

AGENDA

CALIFORNIA TRANSPORTATION COMMISSION

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June 26-27, 2025

Sacramento, CA

Thursday, June 26, 2025

- | | |
|-----------------|--|
| 11:00 AM | Commission Meeting
Sacramento Convention Center
Ballrooms B6, B7, B8
1401 K Street
Sacramento, CA 95814 |
| 1:00 PM | Commission Lunch
Mayahuel
1200 K Street
Sacramento, CA 95814 |
| 5:30 PM | WTS Sacramento Reception
Mix Downtown
1525 L Street
Sacramento, CA 95814 |
| 7:00 PM | Commission Dinner
Cafeteria 15L
1116 15th Street
Sacramento, CA 95814 |

Friday, June 27, 2025

- | | |
|----------------|--|
| 9:00 AM | Commission Meeting
Sacramento Convention Center
Ballrooms B6, B7, B8
1401 K Street
Sacramento, CA 95814 |
|----------------|--|

To participate on Day 1 of the meeting via computer:

Please visit: https://zoom.us/webinar/register/WN_I19nLyDxTx-0IkVfSYQbRQ

There, you will be provided dial in information, instructions for participation, an access code, and audio pin to join the meeting.

If you wish to only listen to day 1 of the meeting:

Phone Number: 669 444 9171, Webinar ID: 938 1372 0104 Access Code: 290074

To participate on Day 2 of the meeting via computer:

Please visit: https://zoom.us/webinar/register/WN_o8Xz3J2pSU6CXhfTDLGOZQ

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If you wish to only listen to day 2 of the meeting:

Phone Number: 669 444 9171, Webinar ID: 918 5399 0819 Access Code: 519745

NEXT REGULARLY SCHEDULED CTC MEETING (Subject to Change):

CTC Meeting – August 14-15, 2025 in San Diego, CA

NOTICE: We welcome comments from the public as a part of each item at this meeting. Times identified on the following agenda are estimates only. The Commission has the discretion to take up agenda items out of sequence and on either day of the two-day meeting. The Commission may adjourn earlier than estimated on either day.

Unless otherwise noticed in the specified book item, a copy of this meeting notice, agenda, and related book items will be posted 10 calendar days prior to the meeting on the California Transportation Commission (Commission) Website: www.catc.ca.gov. Questions or inquiries about this meeting may be directed to the Commission staff at (916) 654-4245, 1120 N Street (MS-52), Sacramento, CA 95814. The Commission will provide assistive services including translation and interpretation in multiple languages, real-time captioning, transcription, large print, digital audio and/or video recordings, as well as Commission meeting materials in an accessible format for the visually impaired. To obtain services or copies in one of these alternate formats or languages, please contact us at (916) 654-4245 or ctc@catc.ca.gov. Arrangements should be made as soon as possible but no later than at least five working days prior to the scheduled meeting.” (*Las solicitudes de acomodación especial o servicios de interpretación deben hacerse tan pronto como sea posible o por lo menos cinco días antes de la reunión programada*)

Improper comments and disorderly conduct are not permitted. In the event that the meeting conducted by the Commission is willfully interrupted or disrupted by a person or by a group so as to render the orderly conduct of the meeting infeasible, the Chair may order the removal of those individuals who are willfully disrupting the meeting.

*“A” denotes an “Action” item; “I” denotes an “Information” item; “C” denotes a “Commission” item; “D” denotes a “Department” item; “F” denotes a “U.S. Department of Transportation” item; “R” denotes a Regional or other Agency item; and “T” denotes a California State Transportation Agency (CalSTA) item.

For a glossary of frequently used terms and acronyms please visit the Commission website at: https://catc.ca.gov/-/media/ctc-media/documents/about_ctc/acronyms-11-04-21-a11y.pdf

GENERAL BUSINESS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
1	Roll Call	1.1	Justin Hall	I	C
2	Welcome to the Region	1.12	Darren Suen Kacey Lizon	I	R
3	Approval of Minutes for May 15-16, 2025	1.2	Doug Remedios	A	C
4	Commissioner Meetings for Compensation	1.5	Doug Remedios	A	C
5	Committee Member Compensation	1.15	Justin Hall	A	C

PUBLIC FUNDING PARTNER REPORTS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
6	Commission Executive Director • 2026 CTC Meeting Schedule Approval	1.3	Tanisha Taylor	A	C
7	Commissioner and Ex-Officio Reports	1.4	Darnell Grisby	A	C
8	CalSTA Secretary	1.6	Toks Omishakin	I	T
9	Caltrans Director (Acting)	1.7	Mike Keever	I	D
10	FHWA California Division Administrator	1.11	Elissa Konove	I	F
11	Regional Agencies Moderator	1.8	Karl Anderson	I	R
12	Rural Counties Task Force Chair	1.9	Aaron Hoyt	I	R
13	Self-Help Counties Coalition Executive Director	1.10	Keith Dunn	I	R
14	Equity Initiatives Update	1.13	C. Sequoia Erasmus Peggy Martinez	I	C/D/T

POLICY MATTERS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
15	State and Federal Legislative Matters	4.1	Justin Behrens	A	C
16	Budget and Allocation Capacity	4.2	Justin Behrens Keith Duncan	I	D
17	Draft 2026 State Transportation Improvement Program and Aeronautics Account Fund Estimates	4.3	Vincent Ma Garrett Franklin	I	D
18	Draft 2026 State Transportation Improvement Program Guidelines	4.23	Laurie Waters	I	C
19	Commission 2025-27 Strategic Plan	4.4	Paul Golaszewski	A	C
20	2024 Solutions for Congested Corridors Program Adoption – Program of Projects Resolution G-25-41	4.5	Naveen Habib	A	C
21	2024 Trade Corridor Enhancement Program Adoption – Program of Projects Resolution G-25-42	4.6	Beverley Newman-Burckhard	A	C
22	2024 Local Partnership Competitive Program Adoption – Program of Projects Resolution G-25-43	4.7	Leishara Ward	A	C
23	2025 Active Transportation Program Metropolitan Planning Organization Component Adoption for Seven Large Metropolitan Planning Organizations. Resolution G-25-47, Amending Resolution G-25-33	4.26	Anja Aulenbacher	A	C

INFORMATION CALENDAR

Tab	Item Description	Ref#	Presenter	Type*	Agency*
24	<u>Informational Reports on Allocations Under Delegated Authority</u> -- Emergency G-11 Allocations (2.5f.(1)): \$119,629,000 for 28 projects. -- SHOPP Safety Sub-Allocations (2.5f.(3)): \$27,201,000 for five projects.	2.5f.		I	D

Monthly Reports on the Status of Contract Award for:

Tab	Item Description	Ref#	Presenter	Type*	Agency*
25	State Highway Projects, per Resolution G-06-08	3.2a		I	D
26	Local Assistance STIP Projects, per Resolution G-13-07	3.2b		I	D
27	Local Assistance ATP Projects, per Resolution G-15-04	3.2c		I	D
28	Pre-Construction SHOPP Support Allocations, per Resolution G-06-08	3.3		I	D

Quarterly Reports – FY 2024-25 – Third Quarter

Tab	Item Description	Ref#	Presenter	Type*	Agency*
29	Local and Regional Agency Notice of Intent to Expend Funds on State Transportation Improvement Program Projects Prior to Commission Allocation, Per Senate Bill 184	3.4		I	C
30	Fiscal Year 2024-25 Caltrans Finance Report	3.5		I	D
31	Third Quarter – Report on Fiscal Year 2024-25 Right of Way Annual Allocation	3.6		I	D
32	Aeronautics – Acquisition and Development and Airport Improvement Program	3.7		I	D
33	Third Quarter – Project Delivery Report – Fiscal Year 2024-25	3.8		I	D
34	Final Right of Way Expenditures Report for STIP projects at Construction Contract Acceptance	3.9		I	D
35	Report of Final Expenditures for STIP Projects at Closeout	3.17		I	D
36	Quarterly Report – Local Assistance Annual Allocation for the Period Ending March 31, 2025	3.10		I	D
37	Preliminary Close-Out Report on the 2024-25 Minor Program Annual Allocation.	3.15		I	D
38	Fiscal Year 2024-25 – Third Quarter - Airspace Program Report	3.16		I	D

Other Reports:

Tab	Item Description	Ref#	Presenter	Type*	Agency*
39	Update on the Minor B project list under the 2024-25 SHOPP Minor Construction Program.	3.1		I	D
40	Second Quarter - Balance Report on AB 1012 "Use It or Lose It" Provision for Federal Fiscal Year 2023 Unobligated RSTP and CMAQ Funds	3.11		I	D
41	Transit and Intercity Rail Capital Program (TIRCP) – Program Update	4.30		I	D

BEGIN CONSENT CALENDAR

Tab	Item Description	Ref#	Presenter	Type*	Agency*
42	Approval of Project for Future Consideration of Funding: 08-San Bernardino County Hatch Road and Sullivan Road Trail Project New asphalt pavement to include construction of Class II bicycle lanes and a graded shoulder/pedestrian walkway for approximately 2.15 miles. (MND) (PPNO 1302) (ATP) Resolution E-25-39 <i>(Related Item under Ref. 2.5w.(1))</i>	2.2c.(2)		A	C

Tab	Item Description	Ref#	Presenter	Type*	Agency*
43	<p><u>LPP (Formulaic) – Allocation Amendment</u> Request to amend the State-Administered LPP (Formulaic) Route 94/125 Connector project, on the State Highway System, in San Diego County, to deallocate \$147,000 in Right of Way (RW) Support and \$3,000,000 in RW Capital due to the funds lapsing. (PPNO 0356) Resolution LPP-A-2425-37, Amending Resolution LPP-A-2021-01 <i>(Related Item under Ref. 2.1a.(3))</i></p>	2.5s.(8)		A	D
44	<p><u>STIP Amendment for Action:</u> The Department, Calaveras County, the Metropolitan Transportation Commission, Fresno County, and the San Diego Association of Governments propose to amend the 2024 STIP to reflect changes to ten projects in various counties:</p> <ul style="list-style-type: none"> • SR 4 Wagon Trail Realignment Project (Eastern Segment) (PPNO 3546) in Calaveras County. Reprogram \$1,042,000 in CON from FY 2027-28 to Right of Way (RW) 2025-26. • Treat Boulevard Corridor Improvements (PPNO 0294E) in Contra Costa County. Reprogram \$1,600,000 in CON funds from FY 2025-26 to 2026-27. • SR 4 Operational Improvements - Phase 1 (PPNO 0299A) in Contra Costa County. Reprogram \$7,500,000 in PS&E funds from FY 2025-26 to 2026-27. • Innovate 680 Coordinated Adaptive Ramp Metering (CARM) Project (PPNO 0521L) in Contra Costa County. Reprogram to combine \$3,200,000 CON Support and \$21,800,000 CON, totaling \$25,000,000 CON. There is no increase to STIP. • I-80/Central Avenue - Phase 2 (Local Road Realignment) (PPNO 2025H) in Contra Costa County. Reprogram \$1,873,000 in CON funds from FY 2025-26 to 2026-27. • Silverado Trail Five-Way Intersection Improvements (PPNO 1483B) in Napa County. Reprogram \$1,153,000 in RW from FY 2025-26 to 2026-27. • US 101 Managed Lanes Project, North of Interstate 380 (I-380 to SM/SF County Line) (PPNO 0658M) in San Mateo County. Reprogram \$1,700,000 in RW from FY 2025-26 to 2027-28. • Central Bikeway (PPNO 2091L) in Santa Clara County. Deprogram \$5,500,000 in PS&E FY 2027-28. • South Fresno SR 99 Corridor Project: North Avenue Interchange (PPNO 6288) in Fresno County. Reprogram \$42,346,000 in CON funds from FY 2025-26 to 2027-28. • SR 94/125 Auxiliary Lanes (PPNO 0356) in San Diego County. Reprogram \$85,000,000 in CON funds from FY 2026-27 to 2025-26. <p>STIP Amendment 24S-03 <i>(Related Items under Ref. 2.5d.(3) and 2.5s.(8))</i></p>	2.1a.(3)		A	D

Tab	Item Description	Ref#	Presenter	Type*	Agency*
45	<p><u>STIP Amendment for Action:</u> The Department and the Los Angeles County Metropolitan Transportation Authority propose to amend the 2024 STIP to reflect changes to four projects in Los Angeles County:</p> <ul style="list-style-type: none"> • Soto Street Roadway Widening between Multnomah Street and Mission Road (PPNO 5961). Deprogram \$9,148,000 from CON in FY 2026-27. • Ford Boulevard Traffic Corridor Improvement Project (N-S) (PPNO 6433). Program \$1,000,000 to CON in FY 2026-27. • Valley Boulevard Multi-Modal Transportation Improvement Project (PPNO 5960). Program \$1,648,000 to CON in FY 2026-27. • Northeast Los Angeles Active Transportation Project & Transit Connectivity Enhancements (PPNO 6432). Program \$6,500,000 to CON in FY 2026-27. <p>STIP Amendment 24S-05</p>	2.1a.(4)		A	D
46	<p><u>State Transportation Improvement Program (STIP) Amendment for Action:</u> The Department and the Metropolitan Transportation Commission propose to amend the 2024 STIP to reflect changes to two projects in Sonoma County:</p> <ul style="list-style-type: none"> • State Route 121 and 8th Street East Intersection Improvement Project (PPNO 2091P). Reprogram \$3,100,000 in Plans, Specifications, and Estimate (PS&E) funds from Fiscal Year (FY) 2027-28 to 2025-26. • West County Trail Gap Closures (PPNO 5156Q). Reprogram \$3,100,000 in Construction (CON) from FY 2025-26 to 2027-28. <p>STIP Amendment 24S-04</p>	2.1a.(2)		A	D
47	<p><u>STIP Amendment for Action:</u> The Department and the San Diego Association of Governments propose to amend the East Otay Mesa Land Port of Entry – Construction project (PPNO 0999I) in San Diego County, to split out a portion of the scope to two new projects:</p> <ul style="list-style-type: none"> • East Otay Mesa Land Port of Entry Technology Package (PPNO 0999J). Program \$36,000,000 to CON in FY 2025-26. • Otay Mesa East Port of Entry Construction – Underground Utilities and Grading Work Package (PPNO 0999K). Program \$114,000,000 to CON in FY 2025-26. <p>STIP Amendment 24S-06 <i>(Related Items under Ref. 2.5s.(15) and 4.13)</i></p>	2.1a.(5)		A	D

Tab	Item Description	Ref#	Presenter	Type*	Agency*
48	<p><u>Approval of Four Projects for Future Consideration of Funding:</u></p> <p>01-01-Men-1, PM 75.47/84.10 Westport Culverts Project Culvert improvements on State Route 1 in Mendocino County, from Post Miles 75.47 to 84.10 starting 1.5 miles south of Westport. (MND) (EA 0K170) (SHOPP Minor A) Resolution E-25-35</p> <p>02-04-Sm-1, PM 17.4/18.2 San Gregorio Creek Bridge Project Seismic Restoration of the San Gregorio Creek Bridge on State Route 1 in Unincorporated San Mateo County from Post mile 17.4 to Post mile 18.2. (MND) (PPNO 2021J) (SHOPP) Resolution E-25-36</p> <p>03-04-Son-101, PM 21.0/21.8 Santa Rosa U.S. Highway 101 Bicycle and Pedestrian Overcrossing Construct a 14.5-foot clear width, Class I shared-use bicycle/pedestrian overcrossing spanning U.S. Highway 101 at PM 21.5 in the City of Santa Rosa in Sonoma County. (MND) (PPNO 2354) (STIP) Resolution E-25-37</p> <p>04-05-Sb-135, PM 0.00/0.73 Los Alamos Connected Community Project Construct pedestrian and bicycle infrastructure to connect residential areas to the downtown community corridor and to Olga Reed Elementary School on State Route 135 from 0.1 mile south of the U.S. Highway State Route 101/135 separation to post mile 0.73 in Santa Barbara County. (ND) (PPNO 3111A) (ATP) Resolution E-25-38 <i>(Related Item under Ref. 2.5w.(2))</i></p>	2.2c.(1)		A	D
49	<p><u>STIP – Allocation Amendment</u> Request to amend the locally-administered STIP Kammerer Rd Extension (Connector Segment A) project, off the State Highway System, in Sacramento County, to revise the description and outputs. There is no change to the allocation amount. (PPNO 5844) Resolution FP-24-92, Amending Resolution FP-24-12</p>	2.5c.(7)		A	D

CTC MEETING
AGENDA
June 26-27, 2025

Tab	Item Description	Ref#	Presenter	Type*	Agency*
50	<p>01-04-Ala-880, PM 30.47/31.61 04-Ala-260, PM R0.78/R1.90 Oakland Alameda Access Project Improve connectivity and accessibility between Alameda and Interstate 880 by way of State Route 260. (FEIR Addendum) (PPNO 0044D) (STIP) (LPP) Resolution E-25-41A <i>(Related Items under Ref. 2.5s.(6) and 2.8b.(3))</i></p> <p>02-05-Slo-101, PM 16.0/R22.5 Pismo Congestion Relief Pilot Project Widen the inside shoulder of U.S. Highway 101 through Pismo Beach to provide a part-time travel lane, in San Luis Obispo County. (FEIR Addendum) (PPNO 2653) (STIP) (TCEP) Resolution E-25-42A</p> <p>03-07-LA-10, PM 44.9/48.3 08-SBd-10, PM 0.0/R37.0 Interstate 10 Corridor Project Construct additional lanes on a portion of Interstate 10 in Los Angeles and San Bernardino Counties. (FEIR Addendum) (PPNO 0134K and PPNO 1325) (STIP) (TCEP) (LPP) Resolution E-25-43A <i>(Related Item under Ref. 2.5s.(5))</i></p> <p>04-08-Riv-60, PM 28.03/30.42 State Route 60/Potrero Boulevard New Interchange Project Construct new interchange on State Route 60 at Potrero Boulevard in Riverside County. (MND Addendum) (PPNO 1321) (TCEP) Resolution E-25-44A <i>(Related Item under Ref. 2.5s.(17))</i></p> <p>05-12-Ora-5, PM 21.3/R25.8 Interstate 5 (I-5) Improvement Project from Interstate 405 (I-405) to State Route 55 (SR-55) Widen I-5 between I-405 and SR-55 in Orange County. (MND Addenda) (PPNO 2743) (STIP) (LPP) Resolution E-25-45A</p>	2.2c.(4)		A	D
51	<p><u>One Relinquishment Resolution</u> 07-LA-72-PM 0.0/6.5 Right of way on State Route 72 (Whittier Boulevard) from Lockheed Avenue to Valley Home Avenue, in the city of Whittier. Resolution R-4121 <i>(Related Item under Ref. 2.5b.(1))</i></p>	2.3c.		A	D
52	3 Resolutions of Necessity	2.4b.		A	D
8 Ayes	Resolutions C-22556 through C-22558				
53	<p>Director's Deeds Items 1 through 7 Excess Lands - Return to State \$1,580,140</p>	2.4d.		A	D

Tab	Item Description	Ref#	Presenter	Type*	Agency*
54	<u>Multi-funded Coordinated Border Infrastructure Program (CBI) and Trade Corridor Enhancement Program (TCEP) – Allocation Amendment</u> Request to amend the locally-administered multi-funded CBI and TCEP East Otay Mesa Land Port of Entry - Segment 3 project, on the State Highway System, in San Diego County, to adjust the CBI Federal funding and revise the outputs. There is no change to the allocation amount. (PPNO 0999C) Resolution FA-24-106, Amending Resolution FA-23-107	2.5e.(13)		A	D
55	<u>Multi-funded STIP and Proposition 1B State Route 99 (SR 99) – Allocation Amendment</u> Request to amend the State-Administered multi-funded STIP and Proposition 1B SR 99 Delano to Pixley 6-Lane with Pavement Rehabilitation Project, on the State Highway System, in Kern and Tulare counties, to revise the Regional Transportation Planning Agency (RTPA), add Tulare County information, the county shares note, and revise the Regional Improvement Program and Interregional Improvement Program split amount. There is no change to the allocation amount. (PPNO 7072) Resolution FA-24-107, Amending Resolution FA-24-48	2.5e.(14) - 2.5v.(1)		A	D
56	<u>Multi-funded STIP and Local Partnership Program (LPP) (Formulaic and Competitive) – Allocation Amendment</u> Request to amend the State-Administered STIP and LPP (Competitive and Formulaic) Oakland Alameda Access Project, on the State Highway System, in Alameda County, to revise the Postmiles. There is no change to the allocation amount. (PPNO 0044D) Resolution FP-24-93, Amending Resolution FP-24-46 Resolution LPP-A-2425-28, Amending Resolution LPP-A-2425-14 <i>(Related Items under Ref. 2.2c.(4) and 2.8b.(3))</i>	2.5s.(6)		A	D
57	<u>LPP (Formulaic) – Allocation Amendment</u> Request to amend the locally-administered LPP (Formulaic) SMART Rail Operations Equipment Expansion (Phase 3) Transit project, in various counties, to deallocate \$216,000 to reflect project savings. (PPNO 2318J) Resolution LPP-A-2425-29, Amending Resolution LPP-A-2122-18	2.6s.(5)		A	D
58	<u>Active Transportation Program (ATP) – Allocation Amendment</u> Request to amend eight locally-administered ATP projects, off the State Highway System, in Orange County, to split the original allocations between two Budget Authority fiscal years. There is no change to the allocation amounts. Resolution FATP-2425-21, Amending Resolution FATP-1819-06, FATP-1819-14, FATP-1920-07	2.5w.(4) - 2.5w.(11)		A	D

CTC MEETING
AGENDA
June 26-27, 2025

Tab	Item Description	Ref#	Presenter	Type*	Agency*
59	<p><u>ATP – Allocation Amendment</u> Request to amend 13 locally-administered ATP projects, off the State Highway System, to rescind a combined total of \$9,372,000 from ATP Cycles 3A, 4, 5, and 6 to reflect project cancellations.</p> <p>Cycle 3A – Resolution FATP-2425-22, Amending Resolutions FATP-1718-19, FATP-1819-06, FATP-1819-14, FATP-2021-10, FATP-2122-23, and FATP-2223-23</p> <p>Cycle 4 – Resolution FATP-2425-23, Amending Resolution FATP-1920-19</p> <p>Cycle 5 – Resolution FATP-2425-24, Amending Resolutions FATP-2122-01, FATP-2122-13, and FATP-2324-28</p> <p>Cycle 6 – Resolution FATP-2425-25, Amending Resolution FATP-2223-31 (<i>Related Item under Ref. 4.27 and 4.28</i>)</p>	<p>2.5w.(12) - 2.5w.(24)</p>		A	D
60	<p><u>Transit and Intercity Rail Capital Program (TIRCP) – Amendment Allocation</u> Request to amend the TIRCP (2022:11) Fresno County Rural Transit Agency Resiliency Hub (Resiliency Hub Structure with Inductive Charging) component, in Fresno County, to rescind the full amount of \$30,000 from the Project Approval and Environmental Document phase due to a third-party contract not being awarded within the Timely Use of Funds deadline. (PPNO CP089) Resolution TIRCP-2425-68, Amending Resolution TIRCP-2223-36</p>	2.6g.(4)		A	D
61	<p><u>TIRCP – Allocation Amendment</u> Request to amend the TIRCP (2018:27) Southern California Optimized Rail Expansion (SCORE) (El Monte Siding and Station Improvements) component, in Los Angeles County, to rescind the full amount of \$525,000 from the Right of Way phase due to not being able to expend the funds within the Timely Use of Funds deadline. (PPNO CP033Y) Resolution TIRCP-2425-70S, Amending Resolution TIRCP-2122-30S</p>	2.6g.(6)		A	D
62	<p>Approval of two State Highway Operation and Protection Program Baseline Agreements Resolution SHOPP-P-2425-07B</p>	4.12		A	C

Tab	Item Description	Ref#	Presenter	Type*	Agency*
63	<p>Request to amend three TIRCP (2022:01) ATN FAST (Family of Advanced Solutions for Transit): Revolutionizing Transit for a Global Audience components, in Orange County, to rescind a combined total of \$13,776,000 from the CON phase, due to third-party contracts not being awarded within the TUF deadline as follows:</p> <ul style="list-style-type: none"> • \$877,000 from the Purchase 7 (10 passenger) ZEV vans for EVE: New Airport Connection & Night Owl component. (PPNO CP086) (2.6g.(3a)) Resolution TIRCP-2425-65, Amending Resolution TIRCP-2223-82 • \$313,000 from the Purchase 10 (15 passenger) ZEV Microtransit for FRAN) component. (PPNO CP086A) (2.6g.(3b)) Resolution TIRCP-2425-66, Amending Resolution TIRCP-2223-82 • \$12,586,000 from the Purchase of 15 - 40' Zero-Emission Buses for ART Fleet Conversion component. (PPNO CP086C) (2.6g.(3c)) Resolution TIRCP-2425-67, Amending Resolution TIRCP-2223-82 <p><i>(Related Item under Ref. 2.6g.(1))</i></p>	2.6g.(3)		A	D
64	<p><u>TIRCP – Allocation Amendment</u> Request to amend the TIRCP (2020:17) Expansion of Water Emergency Transportation Authority Ferry Service (Shoreside Charging Infrastructure) (2022:17) San Francisco Zero Emissions High-Frequency Ferry Network (Shoreside Charging Infrastructure) component, in San Francisco County, to revise the project title and deallocate \$12,381,000 from the Construction (CON) phase due to the component being split into three total components. (PPNO CP075A) Resolution TIRCP-2425-69, Amending Resolution TIRCP-2324-58</p> <p><i>(Related Item under Ref. 2.8v.(12))</i></p>	2.6g.(5)		A	D
65	<p><u>TIRCP – Allocation Amendment</u> Request to amend the TIRCP (2018:26) SMART Larkspur to Windsor Corridor (Windsor to Healdsburg) component, in Sonoma County, to revise the component name. There is no change to the allocation amount. (PPNO CP041A) Resolution TIRCP-2425-71, Amending Resolution TIRCP-2425-14</p> <p><i>(Related Item under Ref. 2.6g.(1))</i></p>	2.6g.(7)		A	D
66	<p><u>State Highway Operation and Protection Program (SHOPP) – Time Extension Amendment</u> Request to amend the previously approved project completion time extension for the SHOPP Facilities project, to revise the county from Los Angeles to San Bernardino. (PPNO 3005R) Waiver 25-150, Amending Waiver 25-52</p>	2.8v.(15)		A	D
67	Approval of the Aeronautics' Capital Improvement Plan 2025-2034	4.11		A	D

CTC MEETING**AGENDA****June 26-27, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
68	ATP – Amendment for Approval Amendment to the 2023 Active Transportation Program to approve a Funding Distribution Change for the City of Cudahy - Salt Lake Avenue Pedestrian Accessibility Project, in Los Angeles County. (PPNO 6171) Resolution G-25-46, Amending Resolution G-23-49	4.22		A	D
69	Approval of the 2021 Active Transportation Program Baseline Agreement Amendment for the Bob Jones Trail Gap Closure Project Resolution ATP-P-2425-15B	4.32		A	C

END OF CONSENT CALENDAR**POLICY MATTERS**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
70	Adoption of the Interim Timely Use of Funds Policy for the 2024 and 2025 Southern California Fires Resolution G-25-51 <i>(Related Items under Ref. 2.5w.(1))</i>	4.31	Laurie Waters	A	C
71	Assembly Bill 744 – Draft Transportation Data Program Proposal	4.10	Sheila Ennes	I	C
72	Vehicle Weight Safety Study Task Force Update (Assembly Bill 251)	4.8	Cayla McDonell	I	C
73	Transportation Asset Management Plan – 2025 Annual Performance Benchmark Report	4.25	Tim Sobelman Mike Johnson	I	D
74	Adoption of the 2025 State Route-710 Local Alternative Transportation Improvement Program Resolution G-25-50	4.29	Rebecca Light	A	C
75	Active Transportation Program Amendment Resolution G-25-48, Amending Resolutions G-16-32, G-17-04, G-17-29, G-19-12, G-21-30, G-21-46 and G-23-49 <i>(Related Items under Ref. 2.5w.(12) – 2.5w.(24) and 4.28)</i>	4.27	Elika Changizi	A	C
76	Amendment to the 2025 Active Transportation Program Statewide and Small Urban and Rural Components Resolution G-25-49, Amending Resolution G-24-76 <i>(Related Items under Ref. 2.5w.(12) – 2.5w.(24) and 4.27)</i>	4.28	Anja Aulenbacher	A	C

ENVIRONMENTAL MATTERS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
77	Approval of Project for Future Consideration of Funding: 09-Mno-395, PM 76.00/80.60 Bridgeport Rehab Rehabilitate pavement, replace nonstandard highway facilities, and make other improvements on U.S. Route 395 from post miles 76.00 to 80.60 in the community of Bridgeport in Mono County. Specifically, it would rehabilitate pavement, bring shoulders to standard widths, upgrade guardrails, and improve grading, culverts, and pedestrian facilities. (FEIR) (PPNO 2648) (SHOPP) Resolution E-25-40 <i>(Related Item under Ref. 2.5e.(9))</i>	2.2c.(3)	Cherry Zamora Jeremiah Ketchum	A	C/D

ALLOCATIONS AND SUPPLEMENTAL FUNDS REQUEST**Projects with costs that exceed the Programmed Amount by More than 20 Percent**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
78	Request for \$107,000,000 (55.0 percent increase) allocation in Construction Capital and \$11,000,000 (61.0 percent increase) allocation in Construction Support, for the SHOPP Roadway Rehabilitation project on United States Highway 101, in San Francisco County. (EA 1Q820, PPNO 2904K) Resolution FP-24-85 (Related Item under Ref. 2.1a.(1))	2.5d.(2)	Tim Sobelman Dina El-Tawansy	A	D
79	Request for \$14,000,000 (64.7 percent increase) allocation in Pre-Construction Support, Project Approval and Environmental Document (PA&ED) phase, for the locally-administered STIP Central Bikeway project on State Route 82 and local roads, in Santa Clara County. (EA 3X340, PPNO 2091L) Resolution FP-24-86 (Related Item under Ref. 2.1a.(3))	2.5d.(3)	Jaeden Gales Dina El-Tawansy	A	D
80	Request for \$2,835,000 (30.3 percent increase) allocation in Construction Capital and \$838,000 (zero percent increase) allocation in Construction Support, for the SHOPP Major Damage (Permanent Restoration) project on State Route 1, in Monterey County. (EA 1K020, PPNO 2850) Resolution FP-24-87 (Related Item under Ref. 2.8v.(4))	2.5d.(4)	Tim Sobelman Scott Eades	A	D
81	Request for \$5,150,000 (64.0 percent increase) allocation in Pre-Construction Support, Plans, Specifications, and Estimate (PS&E) phase and \$2,520,000 (117.4 percent increase) allocation in Right of Way (RW) Support phase, for the SHOPP Pavement Preservation project on State Route 1, in Los Angeles County. (EA 36150, PPNO 5625) Resolution FP-24-88 (Related Items under Ref. 2.1a.(1) and 2.8d.(1))	2.5d.(5)	Tim Sobelman Gloria Roberts	A	D
82	Request for \$19,318,000 (51.9 percent increase) allocation in Construction Capital and \$2,140,000 (zero percent increase) allocation in Construction Support, for the SHOPP Roadway Rehabilitation project on Interstate 5, in Los Angeles County. (EA 37080, PPNO 5733) Resolution FP-24-89	2.5d.(6)	Tim Sobelman Gloria Roberts	A	D
83	Request for \$33,863,000 (35.7 percent increase) allocation in Construction Capital and \$4,770,000 (15.0 percent increase) allocation in Construction Support, for the SHOPP Maintenance Facilities project on State Route 88, in Alpine County. (EA 1H560, PPNO 3433) Resolution FP-24-90 (Related Item under Ref. 2.8d.(1))	2.5d.(7)	Tim Sobelman Grace Magsayo	A	D
84	Request for \$2,600,000 (36.8 percent increase) allocation in Construction Capital and \$724,000 (14.9 percent increase) allocation in Construction Support, for the SHOPP Transportation Management Systems project on various State Routes, in various counties. (EA 1L860, PPNO 3515) Resolution FP-24-91	2.5d.(8)	Tim Sobelman Grace Magsayo	A	D

Capital Outlay Support (COS) Supplemental Fund Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
85	Request for an additional \$1,600,000 (42.8 percent increase) in Pre-Construction Support for the Plans, Specifications, and Estimate (PS&E) phase, for the SHOPP Pavement Preservation project on United States Highway 101, in Santa Barbara County. (EA 1J900, PPNO 2798) Resolution FA-24-94	2.5e.(4)	Tim Sobelman Scott Eades	A	D
86	Request for an additional \$620,000 (37.3 percent increase) in Pre-Construction Support for the PS&E phase, for the SHOPP Major Damage (Permanent Restoration) project on State Route 1, in Monterey County. (EA 1K010, PPNO 2853) Resolution FA-24-95	2.5e.(5)	Tim Sobelman Scott Eades	A	D
87	Request for an additional \$300,000 (5.3 percent increase) in Pre-Construction Support for the PS&E phase, for the SHOPP Pavement Preservation project on Interstate 405, in Los Angeles County. (EA 35320, PPNO 5484) Resolution FA-24-96 <i>(Related Item under Ref. 2.8d.(1))</i>	2.5e.(8)	Tim Sobelman Gloria Roberts	A	D
88	Request for an additional \$2,095,000 (301.4 percent increase) in Pre-Construction Support for the PS&E phase, for the SHOPP Roadway Rehabilitation project on United States Highway 395, in Mono County. (EA 37460, PPNO 2648) Resolution FA-24-97 <i>(Related Item under Ref. 2.2c.(3))</i>	2.5e.(9)	Tim Sobelman Ryan Dermody	A	D
89	Request for an additional \$158,000 (6.8 percent increase) in Pre-Construction Support for the PS&E phase, for the SHOPP Pavement Preservation project on State Routes 4 and 89, in Alpine County. (EA 0J720, PPNO 1044) Resolution FA-24-98	2.5e.(10)	Tim Sobelman Grace Magsayo	A	D
90	Request for an additional \$250,000 (12.7 percent increase) in Pre-Construction Support for the PS&E phase, for the SHOPP Pavement Preservation project on State Route 59, in Merced County. (EA 0V690, PPNO 3439) Resolution FA-24-99 <i>(Related Item under Ref. 2.8d.(1))</i>	2.5e.(11)	Tim Sobelman Grace Magsayo	A	D
91	Request for an additional \$14,800,000 (105.7 percent increase) in Pre-Construction Support for the PS&E phase, for the SHOPP Bridge Rehabilitation and Replacement project on State Route 75 and Interstate 5, in San Diego County. (EA 43063, PPNO 1352) Resolution FA-24-100	2.5e.(12)	Tim Sobelman Ann Fox	A	D

Capital – Supplemental Fund Allocation (Award)

Tab	Item Description	Ref#	Presenter	Type*	Agency*
92	Request for an additional \$3,104,000 (22.3 percent increase) in Construction Capital for the SHOPP Promoting Resilient Operations for Transformative, Efficient, and Cost-saving Transportation (PROTECT) Program project on Interstate 80, in Placer and Nevada counties. (EA 0J560, PPNO 5142) Resolution FA-24-101	2.5e.(1)	Tim Sobelman Sergio Aceves	A	D

Capital – Supplemental Fund Allocations (Complete Construction)

Tab	Item Description	Ref#	Presenter	Type*	Agency*
93	Request for an additional \$1,000,000 (50.0 percent increase) in Construction Support for the SHOPP Operational Improvements project on State Route 99, in Sacramento County. (EA 1H630, PPNO 6930) Resolution FA-24-102	2.5e.(2)	Tim Sobelman Sergio Aceves	A	D
94	Request for an additional \$1,700,000 (27.4 percent increase) in Construction Support for the SHOPP Pavement Preservation project on United States Highway 101, in Santa Barbara County. (EA 1H860, PPNO 2700) Resolution FA-24-103	2.5e.(3)	Tim Sobelman Scott Eades	A	D
95	Request for an additional \$7,200,000 (46.3 percent increase) in Construction Support for the STIP Route 46/41 Wye project on State Routes 41 and 46, in San Luis Obispo County. (EA 3307C, PPNO 0226K) Resolution FA-24-104	2.5e.(6)	Jaeden Gales Scott Eades	A	D
96	Request for an additional \$1,269,000 (35.7 percent increase) in Construction Support for the SHOPP Roadway Rehabilitation project on State Route 41, in Madera County. (EA 0R210, PPNO 6708) Resolution FA-24-105	2.5e.(7)	Tim Sobelman Michael Navarro	A	D

Transit and Intercity Rail Capital Program (TIRCP) Supplemental Fund Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
97	Request for an additional \$1,000,000 (1.2 percent increase) for the CON phase of the TIRCP (2018:13) Peninsula Corridor Electrification Expansion Project (Battery Electric Multiple Unit Demonstration Trainset) component, in various counties. (PPNO CP039A) Resolution TIRCP-2425-72S	2.6g.(2)	Kat Kim Dee Lam	A	D
98	Request for an additional \$894,000 (30 percent increase) for the RW phase of the TIRCP (2018:27) Southern California Optimized Rail Expansion (SCORE) (Simi Valley Double Track) component, in Ventura County. (PPNO CP033G) Resolution TIRCP-2425-75S	2.6g.(8)	Kat Kim Dee Lam	A	D

PROGRAM UPDATES**State Highway Operation and Protection Program (SHOPP)**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
99	<u>SHOPP Amendments for Approval:</u> Request to: --Add 35 new projects into the 2024 SHOPP. (2.1a.(1a)) --Develop 1 Long Lead project. (2.1a.(1c)) --Revise 97 projects currently programmed in the 2024 SHOPP. (2.1a.(1d), 2.1a.(1e) and 2.1a.(1f)) SHOPP Amendment 24H-012 <i>(Related Items under Ref. 2.5b.(1), 2.5b.(2), 2.5d.(2) 2.5d.(5), and 2.8a.(1))</i>	2.1a.(1)	Jon Pray James R. Anderson	A	D
100	<u>SHOPP Amendments for Approval:</u> Request to: --Revise one project currently programmed in the 2024 SHOPP. (2.1a.(6d)) SHOPP Amendment 24H-013 <i>(Related Item under Ref. 2.5b.(5))</i>	2.1a.(6)	Jon Pray James R. Anderson	A	D

Trade Corridor Enhancement Program (TCEP)

Tab	Item Description	Ref#	Presenter	Type*	Agency*
101	<u>TCEP – Project Scope Amendments</u> The Department and bp Products North America propose to amend two Cycle 3 Trade Corridor Enhancement Program – EV Oasis South A project components, in San Bernardino County, to amend the scope. <ul style="list-style-type: none"> • Petro Ontario component. (PPNO 6199A) • TA Barstow component. (PPNO 6199C) Resolution TCEP-P-2425-08 <i>(Related Items under Ref. 2.1s.(2) and 2.8a.(18))</i>	2.1s.(1)	Beverley Newman-Burckhard Dee Lam	A	D
102	<u>TCEP – Project Scope Amendments</u> The Department and bp Products North America propose to amend three Cycle 3 Trade Corridor Enhancement Program – EV Oasis South B project components, in Kern County, to amend the scope. <ul style="list-style-type: none"> • TA Wheeler Ridge component. (PPNO 6240A) • Petro Wheeler Ridge component. (PPNO 6240B) • TA Buttonwillow component. (PPNO 6240C) Resolution TCEP-P-2425-09 <i>(Related Items under Ref. 2.1s.(1) and 2.8a.(18))</i>	2.1s.(2)	Beverley Newman-Burckhard Dee Lam	A	D
103	<u>2022 Trade Corridor Enhancement Program Amendment</u> Amendment to the Otay Mesa East Port of Entry Project Resolution G-25-44, Amending Resolution G-25-37 <i>(Related Items under Ref. 2.1a.(5) and 2.5s.(15))</i>	4.13	Beverley Newman-Burckhard	A	C

Local Partnership Program (LPP)

Tab	Item Description	Ref#	Presenter	Type*	Agency*
104	<u>Local Partnership Formulaic Program Amendment</u> <ul style="list-style-type: none"> • Deprogram a total of \$79,000 from three projects in Alameda County. • Deprogram \$3,147,000 from the State Route 94/State Route 125 South to East Connector Project in San Diego County. • Amend the Roadway Rehabilitation, Street Light & Street Sign Replacement project in Sacramento County to update the project title. • Add two projects in Alameda and Sonoma Counties and program \$2,462,000. Resolution LPP-P-2425-09, Amending Resolution LPP-P-2425-08	4.15	Kayla Giese	A	C
105	<u>2024 Local Partnership Formulaic Program Funding Distribution Amendment:</u> <ul style="list-style-type: none"> • Add \$392,000 incentive funding for the City of Albany in Fiscal Year 2025-26. • Add \$5,000,000 incentive funding for Napa Valley Transportation Authority in Fiscal Year 2025-26. Resolution LPP-P-2425-10, Amending Resolution G-24-65	4.16	Kayla Giese	A	C

Active Transportation Program (ATP)

Tab	Item Description	Ref#	Presenter	Type*	Agency*
106	<u>ATP – Project Scope Amendment</u> The City of Riverside proposes to amend the ATP Mitchell Avenue Sidepath Gap Closure project, in Riverside County, to amend the scope. (PPNO 1310) Resolution ATP-P-2425-10	4.17	Elika Changizi Dee Lam	A	D

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Tab	Item Description	Ref#	Presenter	Type*	Agency*
107	<u>ATP – Project Scope Amendment</u> The City of San Mateo proposes to amend the Delaware Street Safe Routes to School Corridor project, in San Mateo County, to amend the scope and segment the project. (PPNO 2353) Resolution ATP-P-2425-11 <i>(Related Item under Ref. 2.5w.(1))</i>	4.18	Elika Changizi Dee Lam	A	D
108	<u>ATP – Project Scope Amendment</u> The Transportation Agency for Monterey County proposes to amend the Fort Ord Regional Trail and Greenway: California Avenue Segment project, in Monterey County, to amend the scope. (PPNO 3130A) Resolution ATP-P-2425-13	4.20	Elika Changizi Dee Lam	A	D

ALLOCATIONS**Annual Allocations**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
109	<u>Local Assistance (State) – Annual Allocation</u> Request of \$228,908,000 in State Funds for Local Assistance Annual Allocation for Fiscal Year 2025-26. Resolution FM-24-02	2.5h.	Jaeden Gales Dee Lam	A	D
110	<u>Right of Way – Annual Allocation</u> Request of \$125,000,000 for the Fiscal Year 2025-26 Right of Way Annual Allocation. Resolution FM-24-03	2.5m.	Tim Sobelman René Fletcher	A	D
111	<u>Minor Program Annual Allocation</u> Request of \$250,000,000 for 2025-26 Minor Program Annual Allocation for Capital and Support. Resolution FM-24-04	2.5n.	Jon Pray James R. Anderson	A	D
112	<u>Aeronautics Program Annual Allocation</u> Requests to allocate \$2,500,000 for the Fiscal Year 2025-26 Aeronautics Annual Allocation to Match Federal Airport Improvement Program Grants. Resolution FDOA-2025-01	2.7a.	Elika Changizi Tarek Tabshouri	A	D

SHOPP Minor Program Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
113	Request \$1,250,000 for one District Minor project. Resolution FP-24-94	2.5a.	Jon Pray James R. Anderson	A	D

SHOPP Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
114	Request \$367,528,000 for 25 SHOPP projects. Resolution FP-24-95 <i>(Related Items under Ref. 2.1a.(1), 2.3c., and 2.5w.(1))</i>	2.5b.(1)	Jon Pray James R. Anderson	A	D
115	Request of \$57,507,000 for 43 2024 SHOPP preconstruction project phases for environmental support, design, and R/W support. Resolution FP-24-96 <i>(Related Item under Ref. 2.1a.(1))</i>	2.5b.(2)	Jon Pray James R. Anderson	A	D
116	Request of \$387,560,000 for the SHOPP Major Damage (Permanent Restoration) project, in Mariposa County. (PPNO 3636) Resolution FP-24-105 <i>(Related Item under Ref. 2.1a.(6))</i>	2.5b.(5)	Jon Pray James R. Anderson	A	D

SHOPP Right of Way Capital Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
117	Request an allocation of \$11,276,000 for the Right of Way Capital phase for the SHOPP Major Damage Restoration project, on State Route 1, in San Mateo County. (PPNO 2917N) Resolution FP-24-97	2.5b.(3)	Jon Pray James R. Anderson	A	D

STIP Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
118	Request of \$8,297,000 for nine locally-administered STIP and Planning, Programming, and Monitoring projects, off the State Highway System. Resolution FP-24-99	2.5c.(3)	Jaeden Gales Dee Lam	A	D

STIP – Advance Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
119	Request of \$4,192,000 for 10 locally-administered STIP Planning, Programming, and Monitoring projects, off the State Highway System, programmed in Fiscal Year 2025-26. Resolution FP-24-101	2.5c.(6)	Jaeden Gales Dee Lam	A	D

LPP (Formulaic) – Advance Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
120	Request of \$311,000 for the locally-administered LPP (Formulaic) Bowker Road Rehabilitation project, off the State Highway System, in Imperial County, programmed in Fiscal Year 2025-26. (PPNO 1615) Resolution LPP-A-2425-32	2.5s.(4)	Kayla Giese Dee Lam	A	D

Multi-Funded LPP (Formulaic), STIP, and TIRCP Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
121	Request of \$727,267,000 for the locally-administered multi-funded LPP (Formulaic), STIP, and TIRCP East San Fernando Valley Transit Corridor project, in Los Angeles County. (PPNO 4296) Resolution LPP-A-2425-35 Resolution FP-24-102 Resolution TIRCP-2425-73 (Related Item under Ref. 2.6g.(1))	2.6s.(4)	Kayla Giese Dee Lam	A	D

LPP (Competitive) Transit Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
122	Request of \$3,000,000 for the locally-administered LPP (Competitive) Southeast Los Angeles Transit Improvements Project (Bus Shelters) in Los Angeles County. (PPNO LP011B) Resolution LPP-A-2425-36	2.6s.(3)	Leishara Ward Dee Lam	A	D

Multi-Funded LPP (Formulaic) and TCEP – Advance Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
123	Request of \$87,731,000 for the locally-administered multi-funded LPP (Formulaic) and TCEP Interstate 10 Corridor Freight and Managed Lane Project: I-15 to Sierra Avenue, Contract 2A, on the State Highway System, in San Bernardino County, programmed in Fiscal Year 2025-26. (PPNO 1325) Resolution LPP-A-2425-33 Resolution TCEP-A-2425-24 (Related Item under Ref. 2.2c.(4))	2.5s.(5)	Sheila Ennes James R. Anderson	A	D

TCEP Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
124	Request of \$38,713,000 for two locally-administered TCEP projects, off the State Highway System. Resolution TCEP-A-2425-25	2.5s.(3)	Sheila Ennes Dee Lam	A	D
125	Request of \$33,500,000 for the locally-administered TCEP SR-60/Potrero Boulevard Interchange Phase II project, on the State Highway System, in Riverside County. (PPNO 1321) Resolution TCEP-A-2425-26 <i>(Related Item under Ref. 2.2c.(4))</i>	2.5s.(17)	Sheila Ennes James R. Anderson	A	D

TCEP Rail Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
126	Request of \$30,528,000 for the locally-administered TCEP San Dieguito Double Track Phase 1 Rail project, in San Diego County. (PPNO LP003) Resolution TCEP-A-2425-27	2.6s.(2)	Sheila Ennes James R. Anderson	A	D

Multi-Funded TCEP and Infrastructure for Rebuilding America (INFRA) Program Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
127	Request of \$56,400,000 for the locally-administered multi-funded TCEP and INFRA Otay Mesa East Port of Entry Technology Package project, on the State Highway System, in San Diego County, programmed in Fiscal Year 2025-26. (PPNO 0999J) Resolution TCEP-A-2425-29 Resolution FP-24-104 <i>(Related Items under Ref. 2.1a.(5) and 4.13)</i>	2.5s.(15)	Sheila Ennes James R. Anderson	A	D

SCCP Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
128	Request of \$4,150,000 for three locally-administered SCCP projects, off the State Highway System. Resolution SCCP-A-2425-04	2.5s.(7)	Naveen Habib Dee Lam	A	D

SCCP Transit Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
129	Request of \$45,507,000 for two locally-administered SCCP Transit projects. Resolution SCCP-A-2425-05	2.6s.(1)	Naveen Habib Dee Lam	A	D

Multi-Funded LPP (Formulaic) and SCCP Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
130	Request of \$63,275,000 for the locally-administered Multi-funded LPP (Formulaic) and SCCP Mid County Parkway: Ramona Expressway project, off the State Highway System, in Riverside County. (PPNO 1276) Resolution LPP-A-2425-34 Resolution SCCP-A-2425-03 <i>(Related Item under Ref. 2.8(9))</i>	2.5s.(12)	Naveen Habib Dee Lam	A	D

ATP Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
131	Request of \$64,166,000 for 28 locally-administered ATP projects, off the State Highway System. Resolution FATP-2425-26 <i>(Related Items under Ref. 2.2c.(2), 2.5b.(1), 2.8a.(21), 4.18, and 4.31)</i>	2.5w.(1)	Elika Changizi Dee Lam	A	D

ATP – Advance Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
132	Request of \$1,044,000 for the State-Administered ATP Los Alamos Connected Community Project, on the State Highway System, in Santa Barbara County, programmed in Fiscal Year 2025-26. (PPNO 3111A) Resolution FATP-2425-27 <i>(Related item under Ref. 2.2c.(1))</i>	2.5w.(2)	Elika Changizi James R. Anderson	A	D
133	Request of \$1,319,000 for five locally-administered ATP projects, off the State Highway System, programmed in Fiscal Year 2025-26. Resolution FATP-2425-28	2.5w.(3)	Elika Changizi Dee Lam	A	D

TIRCP Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
134	Request of \$487,122,000 for 17 TIRCP projects. Resolution TIRCP-2425-74 <i>(Related Items under Ref. 2.6g.(3), 2.6g.(7), and 2.6s.(4))</i>	2.6g.(1)	Kat Kim Dee Lam	A	D

TIME EXTENSION REQUESTS**SHOPP Time Extensions**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
135	Request to extend the period of project allocation for 105 SHOPP projects, per SHOPP Guidelines. Waiver 25-95 <i>(Related Item under Ref. 2.1a.(1))</i>	2.8a.(1)	Ahmed Ghonim James R. Anderson	A	D
136	Request to extend the period of contract award for three SHOPP projects, per SHOPP Guidelines. Waiver 25-96	2.8b.(1)	Ahmed Ghonim James Anderson	A	D
137	Request to extend the period of project completion for six SHOPP projects, per SHOPP Guidelines. Waiver 25-97 <i>(Related Item under Ref. 2.8c.(3))</i>	2.8c.(1)	Ahmed Ghonim James Anderson	A	D
138	Request to extend the period of project development expenditure for 17 SHOPP projects, per SHOPP Guidelines. Waiver 25-98 <i>(Related Items under Ref. 2.5e.(8), 2.5d.(5), 2.5e.(11), 2.5d.(7), and 2.8v.(2))</i>	2.8d.(1)	Ahmed Ghonim James R. Anderson	A	D

STIP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
139	Request to extend the period of project allocation for the locally-administered STIP I-280/Wolfe Road Interchange Improvement project, on the State Highway System, in Santa Clara County, per STIP Guidelines. (PPNO 2091M) Waiver 25-99	2.8a.(2)	Jaeden Gales James R. Anderson	A	D
140	Request to extend the period of project allocation for seven locally-administered STIP projects, off the State Highway System, per STIP Guidelines. Waiver 25-100	2.8a.(3)	Jaeden Gales Dee Lam	A	D
141	Request to extend the period of contract award for the locally-administered STIP Wescott Road Rehabilitation project, off the State Highway System, in Colusa County, per STIP Guidelines. (PPNO 3185) Waiver 25-101	2.8b.(2)	Jaeden Gales Dee Lam	A	D

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Tab	Item Description	Ref#	Presenter	Type*	Agency*
142	Request to extend the period of project development expenditure for five locally-administered STIP projects, off the State Highway System, per STIP Guidelines. Waiver 25-102	2.8d.(2)	Jaeden Gales Dee Lam	A	D

Multi-Funded STIP, TIRCP, and Proposition 1B Intercity Rail Improvement Program Time Extension

Tab	Item Description	Ref#	Presenter	Type*	Agency*
143	Request to extend the period of project allocation for the locally-administered multi-funded STIP, TIRCP, and Proposition 1B Intercity Rail Improvement Program Central Coast Layover Facility Rail project, in San Luis Obispo County, per STIP Guidelines. (PPNO 2195) Waiver 25-103	2.8a.(4)	Jaeden Gales Dee Lam	A	D

Multi-Funded LPP (Formulaic) and STIP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
144	Request to extend the period of project allocation for the State-Administered multi-funded LPP (Formulaic) and STIP Excelsior Expressway project, on the State Highway System, in Kings and Fresno counties, per LPP (Formulaic) and STIP Guidelines. (PPNO 6705) Waiver 25-104	2.8a.(5)	Kayla Giese James R. Anderson	A	D
145	Request to extend the period of contract award for the State-Administered multi-funded LPP (Formulaic and Competitive) and STIP Oakland Alameda Access Project, on the State Highway System, in Alameda County, per LPP (Formulaic and Competitive) and STIP Guidelines. (PPNO 0044D) Waiver 25-105 <i>(Related Items under Ref. 2.2c.(4) and 2.5s.(6))</i>	2.8b.(3)	Leishara Ward James R. Anderson	A	D
146	Request to extend the period of project development expenditure for the locally-administered multi-funded LPP (Formulaic) and STIP State Route 132 West Freeway/Expressway Phase II Project, on the State Highway System, in Stanislaus County, per LPP (Formulaic) and STIP Guidelines. (PPNO 0944A) Waiver 25-106	2.8d.(3)	Kayla Giese James R. Anderson	A	D

LPP (Formulaic and Competitive) Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
147	Request to extend the period of project allocation for two locally-administered LPP (Formulaic) projects, off the State Highway System, per LPP (Formulaic) Guidelines. Waiver 25-107	2.8a.(6)	Kayla Giese Dee Lam	A	D
148	Request to extend the period of project allocation for three locally-administered LPP (Competitive) Southeast Los Angeles Transit Improvements Project components, in Los Angeles County, per LPP (Competitive) Guidelines. Waiver 25-108	2.8a.(7)	Leishara Ward Dee Lam	A	D
149	Request to extend the period of project allocation for the locally-administered LPP (Competitive) Gold Line Light Rail Platform Modifications - Phase 2 project, in Sacramento County, per LPP (Competitive) Guidelines. (PPNO LP008) Waiver 25-109	2.8a.(8)	Leishara Ward Dee Lam	A	D

CTC MEETING**AGENDA****June 26-27, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
150	Request to extend the period of contract award for the locally-administered LPP (Competitive) SamTrans Emission Zero Project, in San Mateo County, per LPP (Competitive) Guidelines. (PPNO LP010) Waiver 25-110	2.8b.(4)	Leishara Ward Dee Lam	A	D
151	Request to extend the period of project completion for the State-Administered LPP (Formulaic) San Benito Route 156 Improvement Project, on the State Highway System, in San Benito County, per LPP (Formulaic) Guidelines. (PPNO 0297) Waiver 25-111	2.8c.(2)	Kayla Giese James R. Anderson	A	D
152	Request to extend the period of project development expenditure for the State-Administered LPP (Formulaic) 805/15 Transit Priority Connectors project, on the State Highway System, in San Diego County, per LPP (Formulaic) Guidelines. (PPNO 1500) Waiver 25-112	2.8d.(4)	Kayla Giese James R. Anderson	A	D

Multi-Funded LPP (Formulaic) and SCCP Time Extension

Tab	Item Description	Ref#	Presenter	Type*	Agency*
153	Request to extend the period of project allocation for the locally-administered multi-funded LPP (Formulaic) and SCCP Mid County Parkway: Ramona Expressway project, off the State Highway System, in Riverside County, per LPP (Formulaic) and SCCP Guidelines. (PPNO 1276) Waiver 25-113 <i>(Related Item under Ref. 2.5s(12))</i>	2.8a.(9)	Naveen Habib Dee Lam	A	D

Multi-Funded SCCP and STIP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
154	Request to extend the period of project allocation for the locally-administered multi-funded SCCP and STIP Leesdale Passing Siding Rail project, in Ventura County, per SCCP and STIP Guidelines. (PPNO 9887) Waiver 25-114	2.8a.(10)	Naveen Habib Dee Lam	A	D
155	Request to extend the period of project completion for the State-Administered multi-funded SCCP and STIP South Coast 101 HOV-Padaro (Segment 4B) project, on the State Highway System, in Santa Barbara County, per SCCP and STIP Guidelines. (PPNO 7101D) Waiver 25-115 <i>(Related Item under Ref. 2.8c.(1))</i>	2.8c.(3)	Naveen Habib James R. Anderson	A	D

Multi-Funded SCCP and ATP Time Extension

Tab	Item Description	Ref#	Presenter	Type*	Agency*
156	Request to extend the period of project allocation for the locally-administered multi-funded SCCP and ATP East Bay Greenway Multimodal, North Segment Project Phase 1, on the State Highway System, in Alameda County, per SCCP and ATP Guidelines. (PPNO 2364) Waiver 25-116	2.8a.(11)	Naveen Habib James R. Anderson	A	D

SCCP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
157	Request to extend the period of project allocation for two locally-administered SCCP Transit components, in Los Angeles County, per SCCP Guidelines. Waiver 25-117	2.8a.(12)	Naveen Habib Dee Lam	A	D

CTC MEETING**AGENDA****June 26-27, 2025**

158	Request to extend the period of project allocation for the locally-administered SCCP Light Rail Realignment (Sacramento Valley Light Rail Station Realignment) component, in Sacramento County, per SCCP Guidelines. (PPNO CP094H) Waiver 25-118	2.8a.(13)	Naveen Habib Dee Lam	A	D
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Multi-Funded TCEP and STIP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
159	Request to extend the period of project allocation for the State-Administered multi-funded TCEP and STIP Rt 49, Corridor Improvements, La Barr-McKnight, SB Truck Climbing Lane project, on the State Highway System, in Nevada County, per TCEP and STIP Guidelines. (PPNO 4117) Waiver 25-119	2.8a.(14)	Kenneth Lopez James R. Anderson	A	D
160	Request to extend the period of project allocation for the State-Administered multi-funded TCEP and STIP Fix 5 Cascade Gateway / I-5 Improvements Shasta project, on the State Highway System, in Shasta and Tehama counties, per TCEP and STIP Guidelines. (PPNO 3597) Waiver 25-120	2.8a.(15)	Kenneth Lopez Dee Lam	A	D
161	Request to extend the period of project allocation for the State-Administered multi-funded TCEP and STIP Five Cities Multimodal Transportation Network Enhancement Project, on the State Highway System, in San Luis Obispo County, per TCEP and STIP Guidelines. (PPNO 2653) Waiver 25-121	2.8a.(16)	Kenneth Lopez Dee Lam	A	D

TCEP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
162	Request to extend the period of project allocation for two State-Administered TCEP projects, off the State Highway System, per TCEP Guidelines. Waiver 25-123 <i>(Related Items under Ref. 2.1s.(1) and 2.1s.(2))</i>	2.8a.(18)	Kenneth Lopez Dee Lam	A	D

ATP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
163	Request to extend the period of project allocation for 44 locally-administered ATP projects, off the State Highway System, per ATP Guidelines. Waiver 25-126 <i>(Related Item under Ref. 2.5w.(1))</i>	2.8a.(21)	Elika Changizi Dee Lam	A	D
164	Request to extend the period of contract award for four locally-administered ATP projects, off the State Highway System, per ATP Guidelines. Waiver 25-127 <i>(Related Item under Ref. 2.8d.(5))</i>	2.8b.(5)	Elika Changizi Dee Lam	A	D
165	Request to extend the period of project completion for two locally-administered ATP projects, off the State Highway System, per ATP Guidelines. Waiver 25-128	2.8c.(4)	Elika Changizi Dee Lam	A	D
166	Request to extend the period of project development expenditure for two locally-administered ATP projects, off the State Highway System, per ATP Guidelines. Waiver 25-129 <i>(Related Item under Ref. 2.8b.(5))</i>	2.8d.(5)	Elika Changizi Dee Lam	A	D

Local Transportation Climate Adaptation Program (LTCAP) Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
167	Request to extend the period of project allocation for two locally-administered LTCAP projects, off the State Highway System, per LTCAP Guidelines. Waiver 25-130	2.8a.(22)	Rebecca Light Dee Lam	A	D

TIRCP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
168	Request to extend the period of contract award for the TIRCP (2022:21) Sonoma Regional Bus and Rail Connectivity Improvements (Purchase 4 Electric Buses) component, in Sonoma County, per TIRCP Allocation Policy. (PPNO CP100A) Waiver 25-131	2.8b.(6)	Kat Kim Dee Lam	A	D
169	Request to extend the period of project completion for the TIRCP (2018:01) Purchase Zero Emission High Capacity Buses to Support Transbay Tomorrow and Clean Corridors Plan (Facilities and Bus Charging Infrastructure) component, in Alameda County, per TIRCP Allocation Policy. (PPNO 2320B) Waiver 25-132	2.8c.(5)	Kat Kim Dee Lam	A	D
170	Request to extend the period of project development expenditure for the TIRCP (2022:4) Sacramento Valley Station (SVS) Transit Center: Priority Projects (H Street Cycle Track Project On-Street Component), in Sacramento County, per TIRCP Allocation Policy. (PPNO CP094A) Waiver 25-133	2.8d.(6)	Kat Kim Dee Lam	A	D
171	Request to extend the period of project development expenditure for the (2022:6) TIRCP Making a Beeline for Electrification (Parking Deck Build Enhancements) component, in Los Angeles County, per TIRCP Allocation Policy. (PPNO CP082) Waiver 25-149	2.8d.(7)	Kat Kim Dee Lam	A	D

POST-FACT TIME EXTENSION

Tab	Item Description	Ref#	Presenter	Type*	Agency*
172	Post-Fact request to amend the period of project completion for the SHOPP Transportation Management Systems project, in San Joaquin County, as an exception to the SHOPP Guidelines. (PPNO 3230) Waiver 25-135, Amending Waiver 24-147	2.8v.(1)	Ahmed Ghonim James R. Anderson	A	D

TIME EXTENSION AMENDMENTS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
173	Request to amend the period of project allocation for 10 SHOPP projects, as an exception to the SHOPP Guidelines. Waiver 25-136, Amending Waivers 24-125, 24-199, and 25-16 <i>(Related Item under Ref. 2.8d.(1))</i>	2.8v.(2)	Ahmed Ghonim James R. Anderson	A	D
174	Request to amend the period of project completion for three SHOPP projects, as an exception to the SHOPP Guidelines. Waiver 25-137, Amending Waivers 25-18, 25-23, and 25-89	2.8v.(3)	Ahmed Ghonim James R. Anderson	A	D

CTC MEETING
AGENDA
June 26-27, 2025

Tab	Item Description	Ref#	Presenter	Type*	Agency*
175	Request to amend the period of project development expenditure for three SHOPP projects, as an exception to the SHOPP Guidelines. Waiver 25-138, Amending Waivers 23-155, 24-03, 24-27 and 24-148 <i>(Related Item under Ref. 2.5d.(4))</i>	2.8v.(4)	Ahmed Ghonim James R. Anderson	A	D
176	Request to amend the period of project development expenditure for the State-Administered LPP (Formulaic) Marin Sonoma Narrows (MSN) Contract B7 project, on the State Highway System, in Marin County. (PPNO 0361M) Waiver 25-140, Amending Waiver 24-99	2.8v.(6)	Leishara Ward James R. Anderson	A	D
177	Request to amend the period of project development expenditure for the State-Administered multi-funded TCEP and Proposition 1B SR 99 South Madera 6 Lane project, on the State Highway System, in Madera County. (PPNO 6297) Waiver 25-142, Amending Waiver 24-67	2.8v.(8)	Kenneth Lopez James R. Anderson	A	D
178	Request to amend the period of project allocation for the locally-administered TCEP Interstate 605/Valley Boulevard Interchange Improvements Project, on the State Highway System, in Los Angeles County. (PPNO 4382) Waiver 25-143, Amending Waiver 24-64	2.8v.(9)	Kenneth Lopez James R. Anderson	A	D
179	Request to amend the period of project allocation for the locally-administered TCEP Stockton Diamond Grade Separation Rail project, in San Joaquin County. (PPNO 9883) Waiver 25-125, Amending Waiver 24-80	2.8v.(16)	Beverley Newman-Burckhard Dee Lam	A	D
180	Request to amend the period of contract award for the TIRCP (2020:17) Expansion of Water Emergency Transportation Authority Ferry Service (Shoreside Charging Infrastructure Treasure Island Electrification) component, in San Francisco County. (PPNO CP075A) Waiver 25-146, Amending Waiver 24-191 <i>(Related Item under Ref. 2.6g.(5))</i>	2.8v.(12)	Kat Kim Dee Lam	A	D
181	Request to amend the period of project completion for the TIRCP (2018:10) Los Angeles Region Transit System Integration and Modernization Program (Network Integration) component, in Los Angeles County. (PPNO CP030) Waiver 25-147, Amending Waiver 23-160	2.8v.(13)	Kat Kim Dee Lam	A	D
182	Request to amend the period of project completion for the TIRCP (2018:13) Peninsula Corridor Electrification Expansion Project (Bike Parking Enhancements) component, in various counties. (PPNO CP039) Waiver 25-134, Amending Waiver 23-186	2.8v.(14)	Kat Kim Dee Lam	A	D

OTHER MATTERS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
	Public Comment	6	Darnell Grisby	I	C

ADJOURN

Highway and Mass Transportation Financial Matters

Program	Current Meeting Proposed Allocations (June 26-27, 2025)	FY 2024-2025 Year to Date Total (May 31, 2025)
SHOPP Allocations	\$1,552,447,000	\$5,846,642,000
STIP Allocations	\$215,356,000	\$260,949,000
Senate Bill 1 Allocations	\$438,059,000	\$457,366,000
Proposition 1B Allocations		\$9,556,000
Proposition 1A Allocations		
ATP Allocations	\$66,529,000	\$235,625,000
LTCAP Allocations		\$12,683,000
TIRCP Allocations	\$973,830,000	\$789,022,000
Short-Line Railroad Allocations		
Aeronautics Allocations	\$2,500,000	
Local Assistance Annual Allocation	\$228,908,000	\$2,633,257,000
Waterborne Ferry Program Allocation		\$3,547,000
Grand Total:	\$3,477,629,000	\$10,248,647,000
Total Jobs Created:	38,254	112,735
Total De-Allocations:	\$39,447,000	\$309,533,000

CALIFORNIA TRANSPORTATION COMMISSION

1120 N Street (MS-52)
Sacramento, CA 95814
(916) 654-4245

CTC Website: <http://www.catc.ca.gov>

Mr. Darnell Grisby– Chair
1120 N Street, MS-52
Sacramento, CA 95814

Mr. Jason Elliott
1120 N Street, MS-52
Sacramento, CA 96814

Ms. Clarissa Falcon – Vice Chair
1120 N Steet, MS-52
Sacramento, CA 95814

Mr. Carl Guardino
1120 N Street, MS-52
Sacramento, CA 95814

Jay Bradshaw
1120 N Street, MS-52
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Ms. Adonia E. Lugo, Ph.D.
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Mr. Joe Cruz
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Ms. Zahirah Mann
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Ms. Lee Ann Eager
1120 N Street, MS-52
Sacramento, CA 95814

Mr. Bob Tiffany
1120 N Street, MS-52
Sacramento, CA 95814

Ex Officio Members

The Honorable Dave Cortese
Member of the Senate
1021 O Street, Suite 6630
Sacramento, CA 95814

The Honorable Lori D. Wilson
Member of the Assembly
1021 O Street, Suite 5150
Sacramento, CA 95814

Executive Director

Tanisha Taylor
1120 N Street, MS-52
Sacramento, CA 95814
(916) 654-4245

WELCOME TO THE REGION

A VERBAL PRESENTATION ON THIS ITEM
WILL BE MADE AT THE CALIFORNIA
TRANSPORTATION COMMISSION MEETING.

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 1.2, Action

Prepared By: Douglas Remedios,
Deputy Director, Administration and Financial Management (Interim)

Published Date: June 13, 2025

Subject: Approval of Minutes for May 15-16, 2025 Commission Meeting

Recommendation:

Staff recommends that the California Transportation Commission (Commission) approve the meeting minutes for the May 15-16, 2025 Commission meeting.

Issue:

California Code of Regulations, Title 21 California Administrative Code section 8012, requires that:

The Commission shall keep accurate minutes of all meetings and make them available to the public. The original copy of the minutes is that signed by the executive secretary and is the evidence of taking any action at a meeting. All resolutions adopted at a meeting shall be entered in the text of the minutes by reference.

In compliance with Title 21 California Administrative Code section 8012, the Commission's Operating Procedures dated December 4, 2019, require that as an order of business, at each regular meeting of the Commission, the minutes from the last meeting shall be approved by the Commission.

Attachments:

Attachment A: May 15-16, 2025, Commission meeting minutes

MINUTES
CALIFORNIA TRANSPORTATION COMMISSION
May 15-16, 2025
San Francisco, CA

Thursday, May 15, 2025

12:00 PM

Commission Meeting
Bay Area Metro Center – Chambers
375 Beale Street
San Francisco, CA 94105

Friday, May 16, 2025

9:00 AM

Commission Meeting
Bay Area Metro Center – Chambers
375 Beale Street
San Francisco, CA 94105

For a more thorough review of this meeting, please visit:

<https://www.youtube.com/channel/UCASI3qyTEuhZffC13RbG4xQ>

*“A” denotes an “Action” item; “I” denotes an “Information” item; “C” denotes a “Commission” item; “D” denotes a “Department” item; “F” denotes a “U.S. Department of Transportation” item; “R” denotes a Regional or other Agency item; and “T” denotes a California State Transportation Agency (CalSTA) item.

For a glossary of frequently used terms and acronyms please visit the Commission website at:

https://catc.ca.gov/-/media/ctc-media/documents/about_ctc/acronyms-11-04-21-a11y.pdf

GENERAL BUSINESS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
1	Roll Call	1.1	Doug Remedios	I	C
Commissioner		Attendance			
Chair Darnell Grisby		Present			
Vice Chair Clarissa Falcon		Present			
Commissioner Jay Bradshaw		Present			
Commissioner Joseph Cruz		Present (Remotely Day 1)			
Commissioner Lee Ann Eager		Present (Remotely)			
Commissioner Jason Elliott		Present (Remotely Day 1)			
Commissioner Carl Guardino		Present			
Commissioner Adonia Lugo		Present			
Commissioner Zahirah Mann		Present			
Commissioner Bob Tiffany		Present			
Ex-Officio's		Attendance			
Senator Dave Cortese, Ex-Officio		Present (Remotely)			
Assembly Member Lori Wilson, Ex-Officio		Absent			

TOTAL - Present: 11 Absent: 1

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
2	Welcome to the Region	1.12	Andrew Fremier Sue Noack	I	R

Metropolitan Transportation Commission Chair Sue Noack and Executive Director Andrew Fremier presented this informational item.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
3	Approval of Minutes for March 20-21 and April 16, 2025 Commission meetings and April 10, 2025 Joint Meeting	1.2	Doug Remedios	A	C

Tabs 3-5 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
4	Commissioner Meetings for Compensation	1.5	Doug Remedios	A	C

Tabs 3-5 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
5	Committee Member Compensation	1.15	Justin Hall	A	C

Tabs 3-5 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

PUBLIC FUNDING PARTNER REPORTS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
6	Commission Executive Director	1.3	Tanisha Taylor	A	C

Commission Executive Director Tanisha Taylor presented this item. No action was taken.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
7	Commissioner and Ex-Officio Reports	1.4	Darnell Grisby	A	C

Chair Grisby, Vice Chair Falcon and Ex-Officio Cortese provided a report for this item. No action was taken.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
8	CalSTA Secretary	1.6	Toks Omishakin	I	T

California State Transportation Agency Secretary Toks Omishakin presented this informational item.

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
9	Caltrans Chief Deputy Director	1.7	Michael Keever	I	D

California Department of Transportation Chief Financial Officer Steven Keck presented this informational item.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
10	FHWA California Division Administrator	1.11	Elissa Konove	I	F

Item was withdrawn

Tab	Item Description	Ref#	Presenter	Type*	Agency*
11	Regional Agencies Moderator	1.8	Karl Anderson	I	R

Regional Agencies Moderator Karl Anderson presented this informational item.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
12	Rural Counties Task Force Chair	1.9	Aaron Hoyt	I	R

Rural Counties Task Force Chair Aaron Hoyt presented this informational item.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
13	Self-Help Counties Coalition Executive Director	1.10	Keith Dunn	I	R

Item was withdrawn

Tab	Item Description	Ref#	Presenter	Type*	Agency*
14	Equity Initiatives Update	1.13	C. Sequoia Erasmus Keri Akemi-Hernandez	I	C/D/T

Commission Associate Deputy Director Sequoia Erasmus and Interagency Equity Advisory Committee Member Keri Akemi-Hernandez presented this informational item.

RIGHT-OF-WAY MATTERS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
15 8 Ayes	<u>Resolutions of Necessity - Appearance</u> --08-SBd-210-PM R24.2 Raghibir I. Kamboh and Kamlesh Kamboh, trustees of the family trust of Raghibir I. Kamboh and Kamlesh Kamboh dated August 14, 2008, as to Parcels 1, 2, and 3 Resolution C-22540 --08-SBd-210-PM R24.2 Mobil Oil Corporation, as to Parcel 4 Resolution C-22541	2.4a.(1)	Tim Sobelman Rizwan Tanvir	A	D

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Falcon **Recused:** None **Absent:** Elliott

Vote result: 9-0

Ayes: Bradshaw, Cruz, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
16 8 Ayes	<u>Resolution of Necessity - Appearance</u> --04-Mrn-1-PM 28.56 Andrea Meghrouni-Brown, Trustee of the Andrea Meghrouni-Brown Revocable Trust Dated March 24, 2023, as to an Undivided 50% Interest and Dave E. Moser and Barbara W. Moser, Trustees of the David and Barbara Moser 1998 Family Trust Dated September 3, 1998, as amended as to an Undivided 50% interest, as to Parcels 63817-1, 2, 3, 4 Resolution C-22555	2.4a.(2)	Tim Sobelman Rizwan Tanvir	A	D

Recommendation: Approval**Action Taken:** Approved**Motion:** Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Elliott**Vote result:** 9-0**Ayes:** Bradshaw, Cruz, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None**POLICY MATTERS**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
17	State and Federal Legislative Matters	4.1	Justin Behrens	A	C

Recommendation: Approval**Action Taken:** Approved**Motion:** Guardino **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott**Vote result:** 8-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** NoneChanges to this item were listed on the "Changes to CTC Agenda" handout as follows:YELLOW MEETING HANDOUT – Attachment A only

Tab	Item Description	Ref#	Presenter	Type*	Agency*
18	Budget and Allocation Capacity	4.2	Justin Behrens Keith Duncan	I	D

Commission Deputy Director Justin Behrens and Caltrans Division Chief for Budgets Keith Duncan presented this informational item.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
19	2026 State Transportation Improvement Program and Aeronautics Account Fund Estimates – Final Assumptions	4.3	Vincent Ma Garrett Franklin	A	D

Recommendation: Approval**Action Taken:** Approved**Motion:** Bradshaw **Second:** Falcon **Recused:** None **Absent:** Cruz and Elliott**Vote result:** 8-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
20	Adoption of Final 2025 State and Local Transportation System Needs Assessment	4.4	Frances Dea-Sanchez	A	C

Recommendation: Approval**Action Taken:** Approved**Motion:** Bradshaw **Second:** Guardino **Recused:** None **Absent:** Cruz and Elliott**Vote result:** 8-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None

Speakers:

Vanessa Schoenewald – San Bernardino County Transportation Authority

Changes to this item were listed on the “Changes to CTC Agenda” handout as follows:

YELLOW MEETING HANDOUT – Book Item Memorandum and Attachment

Tab	Item Description	Ref#	Presenter	Type*	Agency*
21	Innovations in Transportation Caltrain Efforts to Enhance the San Francisco Bay Area's Multimodal Network	4.8	Destiny Preston Michelle Bouchard	I	C

Commission Assistant Deputy Director Destiny Preston and Caltrain Executive Director Michelle Bouchard presented this informational item.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
22	Approval of Amendments to the Commission's Conflict of Interest Code	4.9	Brandy Fleming	A	C

Item was withdrawn.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
23	Adoption of the 2025 Active Transportation Program Metropolitan Planning Organization Component for the San Diego Association of Governments Resolution G-25-33 <i>(Related Item under Ref. 2.5w.(3))</i>	4.10	Anja Aulenbacher	A	C

Recommendation: Approval**Action Taken:** Approved**Motion:** Falcon **Second:** Eager **Recused:** None **Absent:** Cruz and Elliott**Vote result:** 8-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None

INFORMATION CALENDAR

Tab	Item Description	Ref#	Presenter	Type*	Agency*
24	<u>State Transportation Improvement Program (STIP) Amendment for Notice:</u> The Department and the Metropolitan Transportation Commission propose to amend the 2024 STIP to reflect changes to two projects in Sonoma County: <ul style="list-style-type: none"> • State Route 121 and 8th Street East Intersection Improvement Project (PPNO 2091P). Reprogram \$3,100,000 in Plans, Specifications, and Estimate (PS&E) funds from Fiscal Year (FY) 2027-28 to 2025-26. • West County Trail Gap Closures (PPNO 5156Q). Reprogram \$3,100,000 in Construction (CON) from FY 2025-26 to 2027-28. STIP Amendment 24S-04	2.1b.(2)		I	D

This item was presented as part of the Information Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
25	<u>STIP Amendment for Notice:</u> The Department and the Southern California Association of Governments propose to amend the 2024 STIP to reflect changes to four projects in Los Angeles County: <ul style="list-style-type: none"> • Soto Street Roadway Widening between Multnomah Street and Mission Road (PPNO 5961). Deprogram \$9,148,000 from CON in FY 2026-27. • Ford Boulevard Traffic Corridor Improvement Project (N-S) (PPNO 6433). Program \$1,000,000 to CON in FY 2026-27. • Valley Boulevard Multi-Modal Transportation Improvement Project (PPNO 5960). Program \$1,648,000 to CON in FY 2026-27. • Northeast Los Angeles Active Transportation Project & Transit Connectivity Enhancements (PPNO 6432). Program \$6,500,000 to CON in FY 2026-27. STIP Amendment 24S-05	2.1b.(3)		I	D

This item was presented as part of the Information Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
26	<u>STIP Amendment for Notice:</u> The Department and the San Diego Association of Governments propose to amend the East Otay Mesa Land Port of Entry – Construction project (PPNO 0999I) in San Diego County, to split out a portion of the scope to two new projects: <ul style="list-style-type: none"> • East Otay Mesa Land Port of Entry Technology Package (PPNO 0999J). Program \$36,000,000 to CON in FY 2025-26 • East Otay Mesa Land Port of Entry Construction Package 1 (PPNO 0999K). Program \$114,000,000 to CON in FY 2025-26 STIP Amendment 24S-06	2.1b.(4)		I	D

This item was presented as revised as part of the Information Calendar.

Changes to this item were listed on the “Changes to CTC Agenda” handout as follows:

YELLOW MEETING HANDOUT

Tab	Item Description	Ref#	Presenter	Type*	Agency*
27	<p>STIP Amendment for Notice:</p> <p>The Department, Calaveras County, the Metropolitan Transportation Commission, Fresno County, and the San Diego Association of Governments propose to amend the 2024 STIP to reflect changes to ten projects in various counties:</p> <ul style="list-style-type: none"> • SR 4 Wagon Trail Realignment Project (Eastern Segment) (PPNO 3546) in Calaveras County. Reprogram \$1,042,000 in CON from FY 2027-28 to Right of Way (RW) 2025-26. • Treat Boulevard Corridor Improvements (PPNO 0294E) in Contra Costa County. Reprogram \$1,600,000 in CON funds from FY 2025-26 to 2026-27. • SR4 Operational Improvements - Phase 1 (PPNO 0299A) in Contra Costa County. Reprogram \$7,500,000 in PS&E funds from FY 2025-26 to 2026-27. • Innovate 680 Coordinated Adaptive Ramp Metering (CARM) Project (PPNO 0521L) in Contra Costa County. Reprogram to combine \$3,200,000 CON Support and \$21,800,000 CON, totaling \$25,000,000 CON. There is no increase to STIP. • I-80/Central Avenue - Phase 2 (Local Road Realignment) (PPNO 2025H) in Contra Costa County. Reprogram \$1,873,000 in CON funds from FY 2025-26 to 2026-27. • Silverado Trail Five-Way Intersection Improvements (PPNO 1483B) in Napa County. Reprogram \$1,153,000 in RW from FY 2025-26 to 2026-27. • US 101 Managed Lanes Project, North of Interstate 380 (I-380 to SM/SF County Line) (PPNO 0658M) in San Mateo County. Reprogram \$1,700,000 in RW from FY 2025-26 to 2027-28. • Central Bikeway (PPNO 2091L) in Santa Clara County. Deprogram \$5,500,000 in PS&E FY 2027-28. • South Fresno SR 99 Corridor Project: North Avenue Interchange (PPNO 6288) in Fresno County. Reprogram \$42,346,000 in CON funds from FY 2025-26 to 2027-28. • SR 94/125 Auxiliary Lanes (PPNO 0356) in San Diego County. Reprogram \$85,000,000 in CON funds from FY 2026-27 to 2025-26. <p>STIP Amendment 24S-03 (Related Items under Ref. 2.5c.(4), 2.8a.(9), and 2.8d.(2))</p>	2.1b.(1)		I	D

This item was presented as part of the Information Calendar.

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
28	Informational Reports on Allocations Under Delegated Authority -- Emergency G-11 Allocations (2.5f.(1)): \$200,223,000 for 47 projects. -- SHOPP Safety Sub-Allocations (2.5f.(3)): \$13,234,000 for three projects. -- Minor G-19-05a Allocations (2.5f.(4)): \$8,891,000 for nine projects.	2.5f		I	D

This item was presented as revised as part of the Information Calendar.

Changes to this item were listed on the "Changes to CTC Agenda" handout as follows:

In the Book Item Attachment 2.5f.(1):

Project 15 – EA 02-1K900 – Various counties – revise the project location as follows: In Plumas, Shasta, Tehama, and Butte Counties, at various locations and add the following information: PPNO 02-3929, the CON ENG RMRA Budget information 505-3290 RMRA and the CON ENG RMRA amount \$200,000

Project 22 – EA 04-3X050/PPNO 04-2929G – Sonoma County – revise the Initial G-11 Allocation date from 01/01/2011 to 02/26/2025

Project 43 – EA 08-1R500/PPNO 08-3025P – San Bernardino County – revise the Initial G-11 Allocation date from 01/01/2011 to 02/26/2025

Monthly Reports on the Status of Contract Award for:

Tab	Item Description	Ref#	Presenter	Type*	Agency*
29	State Highway Projects, per Resolution G-06-08	3.2a		I	D

This item was presented as revised as part of the Information Calendar.

Changes to this item were listed on the "Changes to CTC Agenda" handout as follows:

In the Book Item Attachment:

Project 13 – EA 07-34850/PPNO 07-5392 – Revise the Award Deadline from 6/30/2025 to 9/30/2025

Tab	Item Description	Ref#	Presenter	Type*	Agency*
30	Local Assistance STIP Projects, per Resolution G-13-07	3.2b		I	D

This item was presented as part of the Information Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
31	Local Assistance ATP Projects, per Resolution G-15-04	3.2c		I	D

This item was presented as part of the Information Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
32	Pre-Construction SHOPP Support Allocations, per Resolution G-06-08	3.3		I	D

This item was presented as part of the Information Calendar.

Other Reports:

Tab	Item Description	Ref#	Presenter	Type*	Agency*
33	Update on the Minor B project list under the 2024-25 SHOPP Minor Construction Program.	3.1		I	D

This item was presented as part of the Information Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
34	Transit and Intercity Rail Capital Program (TIRCP) Semi-Annual Report	3.13		I	D

This item was presented as part of the Information Calendar.

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
35	Notice of the Draft Aeronautics Capital Improvement Plan for 2025-2034	4.7		I	D

This item was presented as part of the Information Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
36	TIRCP – Program Update	4.30		I	D

This item was presented as part of the Information Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
37	<u>Independent Office of Audits and Investigations:</u> 2023-24 Annual Summary of Findings and Recommendations	4.5		I	C

This item was presented as part of the Information Calendar.

BEGIN CONSENT CALENDAR

Recommendation: Approval as revised

Action Taken: Approved

Motion: Bradshaw **Second:** Falcon **Recused:** Mann **Absent:** None

Vote result: 9-0

Ayes: Bradshaw, Cruz, Eager, Elliott, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
38	<u>Approval of Eight Projects for Future Consideration of Funding:</u> <u>01-02-Sha-5</u> , PM 58.0/67.019 02-Sis-5, PM 0.0/2.7 Flume Creek CAPM Project Perform pavement, drainage, and safety improvements on Interstate 5 between Post Miles (PM) 58.0 and 67.019 in Shasta County, and PM 0.0 to 2.7 in Siskiyou County. (MND) (PPNO 3777) (SHOPP) Resolution E-25-23 <i>(Related Item under Ref. 2.5b.(2))</i> <u>02-03-ED-89</u> , PM 24.4/25.3 Meeks Creek Bridge Replacement Project Replacement of Meeks Creek Bridge, creek restoration, fish and wildlife connectivity improvements, enhancement of bicycle and pedestrian infrastructure, and Transportation Management System elements on State Route 89 in El Dorado County, from PM 24.4 to 25.3. (ND) (PPNO 3473) (SHOPP) Resolution E-25-24 <i>(Related Item under Ref. 2.5b.(2))</i> <u>03-03-Sac-12</u> , PM 0.70/6.11 Sac 12 Terminus Safety Project Improve safety of the traveling public, install a concrete median barrier, provide standard width inside shoulders, improve left turn lane access at Terminus Road, rehabilitate drainage systems, and install Transportation Management System elements on State Route 12 in Sacramento County, from PM 0.70 to 6.11, east of Solano County and west of San Joaquin County. (ND) (PPNO 5963) (SHOPP) Resolution E-25-25 <i>(Related Item under Ref. 2.5b.(2))</i>	2.2c.(1)		A	D

CTC MEETING

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Tab	Item Description	Ref#	Presenter	Type*	Agency*
	<p>04-05-Scr-9, PM 0.046/7.500 South Santa Cruz 9 Capital Preventative Maintenance Project Roadway and drainage rehabilitation and infrastructure improvement on State Route 9 between Santa Cruz and Felton, from PM 0.046 to 7.500 in Santa Cruz County. (MND) (PPNO 2879) (SHOPP) Resolution E-25-26 <i>(Related Item under Ref. 2.5b.(2))</i></p> <p>05-06-Fre-Var, PM Var/Var Fresno County Culvert Improvements Repair or replace existing blocked and defective drainage across multiple state routes and Interstate 5 at various post miles in Fresno County. (MND) (PPNO 7071) (SHOPP) Resolution E-25-27 <i>(Related Item under Ref. 2.5b.(2))</i></p> <p>06-08-SBd-138, PM T16.20/R17.35 State Route 138 Curve Revision Realign State Route 138, from 1.0 mile east of Interstate 15 to just west of Hog Ranch Creek Bridge (PM T16.20 to R17.35) near the City of Hesperia in San Bernardino County. (MND) (PPNO 3019X) (SHOPP) Resolution E-25-29 <i>(Related Item under Ref. 2.5b.(2))</i></p> <p>07-10-Alp-4, PM Var/Var 10-Alp-207, Var/Var Alpine County State Route 4 Drainage System Restoration Rehabilitate existing drainage systems at 29 locations on State Routes 4 and 207 in Alpine County. (MND) (PPNO 3511) (SHOPP) Resolution E-25-30 <i>(Related Item under Ref. 2.5b.(2))</i></p> <p>08-10-Sta-120, PM 8.94/9.54 Wamble Road Left-Turn Channelization Project Install left-turn channelization on State Route 120 turning onto Wamble Road, east of the City of Oakdale in Stanislaus County. (MND) (PPNO 3540) (SHOPP) Resolution E-25-31</p>				

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
39	<p><u>Approval of Eight Projects for Future Consideration of Funding:</u></p> <p>01-01-DN-101, PM 35.8/36.5 Dr. Fine Bridge Replacement Project Replacement of Dr. Fine Bridge on United States Highway 101 in Del Norte County from postmile (PM) 35.8 to 36.5.(FEIR Addendum) (PPNO 0100V) (SHOPP) Resolution E-25-34A <i>(Related Item under Ref. 2.5e.(2))</i></p>	2.2c.(2)		A	D

<p>02-04-Mrn-101, PM 18.6/27.7 04-Son-101, 0.0/7.1 Marin Sonoma Narrows (MSN) HOV Widening Project Relieve recurrent congestion along 16.1 miles of United States Highway 101 in Marin County from PM 18.6 to PM 27.7 and in Sonoma County from PM 0.0 to PM 7.1. Widen and realign existing mainline to improve overall traffic operations and access. (FEIR Addendum) (PPNO 0361M) (SCCP) (LPP) (STIP) Resolution E-25-36A <i>(Related Items under Ref. 2.5s.(9)/2.5v.(2), 2.5s.(10), 2.8c.(3), and 2.8v.(5))</i></p> <p>03-04-Sol-80, PM 10.8/17.0 04-Sol-680, PM 10.0/13.1 04-Sol-12, PM 1.7/L2.8 04-Sol-12, PM L1.8/4.8 Interstate 80/Interstate 680/State Route 12 Interchange Project: I-80 Westbound Cordelia Commercial Vehicle Enforcement Facility (CCVEF) Refinements Remove the existing Interstate 80 Westbound Truck Scales facility and construct a new more efficient facility that complies with current California Highway Patrol and Caltrans DES Architecture standards. Modify on- and off-ramps from westbound State Route 12E and westbound Interstate 80 to accommodate the truck traffic to and from the new facility. (FEIR Addendum) (PPNO 8273C) (TCEP) (STIP) Resolution E-25-37A <i>(Related Item under Ref. 2.5s.(6))</i></p> <p>04-05-Mon-68, PM 0.2/15.7 State Route 68 Drainage Improvements Repair or replace culverts, replace lights near Fairground Road, and install two new traffic census stations on State Route 68 from post miles 0.2 to 15.7 in Monterey County. (MND Addendum) (PPNO 2787) (SHOPP) Resolution E-25-38A</p> <p>05-05-SB-101, PM 21.3/21.9 U.S. Route 101 San Jose Creek Bridge Replacement Replace the existing San Jose Creek Bridges (Number 51-0163 L/R) with a single-span bridge on United States Highway 101 at post mile 21.6 in Santa Barbara County. (MND Addendum) (PPNO 2649) (SHOPP) Resolution E-25-39A <i>(Related Item under Ref. 2.5e.(7))</i></p> <p>06-05-SCr-9, PM 8.5/25.5 Upper Drainages Erosion Control Improvements Upgrade drainage systems along State Route 9 in Santa Cruz and San Mateo Counties from Scenic Way in Ben Lomond to 0.25 mile north of the Summit Meadows trailhead parking lot. (MND Addendum) (PPNO 2627) (SHOPP) Resolution E-25-40A <i>(Related Items under Ref. 2.5e.(6) and 2.8v.(4))</i></p>				
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	<p>07-06-Fre-99, PM 21.2/24.4 El Dorado to Clinton Rehabilitation Project Rehabilitate the pavement, improve existing interchange spacings and structures, and replace interchanges on State Route 99 from post miles 21.2 to 24.4 in Fresno County. (FEIR Addendum) (PPNO 6949) (SHOPP) Resolution E-25-41A (Related Item under Ref. 2.5e.(13))</p> <p>08-10-Cal-12, PM 17.1/17.5 Calaveras County Bridge Replacement Replace the North Fork Calaveras River Bridge next to its current alignment, and realign State Route 12 from post miles 17.1 to 17.5 in Calaveras County. (MND Addendum) (PPNO 3288) (SHOPP) Resolution E-25-42A (Related Item under Ref. 2.5b.(1))</p>				
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This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
40	<u>Approval of Project for Future Consideration of Funding:</u> 03-Sacramento County Dos Rios Light Rail Station Construction of light rail station. (MND) (PPNO CP160) (TIRCP) Resolution E-25-32 (Related Item under Ref. 2.6g.(1))	2.2c.(3)		A	C

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
41	<u>Approval of Project for Future Consideration of Funding:</u> 06-Tulare County Dinuba Active Transportation Program Improvements Project Construct Class 2 and Class 4 bike lanes/routes, pavement enhancements, sidewalks, curb ramps, crossing improvements, pedestrian signal heads, flashing beacons, and traffic calming medians. (MND) (PPNO 8024A) (ATP) Resolution E-25-34 (Related Item under Ref. 2.5w.(1))	2.2c.(6)		A	C

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
42	<u>Three Relinquishment Resolutions</u> 01-Hum-101-PM 57.0/59.1 Right of way along United States Highway 101 on Fowler Lane, Sandy Prairie Road, and Drake Hill Road in the county of Humboldt. Resolution R-4118 03-Sac-5-PM 24.4/25.8 Right of way along Interstate 5 on Bannon Street and Garden Highway in the city of Sacramento. Resolution R-4119 11-SD-5-PM 23.7 Right of way along Interstate 5 on Santa Fe Street in the city of San Diego Resolution R-4120	2.3c.		A	D

This item was presented and approved as part of the Consent Calendar.

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May 15-16, 2025

Tab	Item Description	Ref#	Presenter	Type*	Agency*
43 8 Ayes	<u>7 Resolutions of Necessity</u> Resolutions C-22542, C-22545, C-22546, C-22550, and C-22552 through C-22554	2.4b.		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
44	Director's Deeds Items 1 through 21 Excess Lands - Return to State: \$448,300	2.4d.(1)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
45 8 Ayes	<u>Rescinding Resolution of Necessity</u> --Resolution CR-170 - Union Pacific Railroad (Outfront Media, LLC) 03-Yub-70-PM-15.0 <i>(Related Items under Ref. 2.2c.(5), 2.4e.(2), and 2.5e.(3))</i>	2.4e.(1)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
46 8 Ayes	<u>Rescinding Resolution of Necessity</u> --Resolution CR-171 - Union Pacific Railroad (Outfront Media, LLC) 03-Yub-70-PM-15.1 <i>(Related Items under Ref. 2.2c.(5), 2.4e.(1), and 2.5e.(3))</i>	2.4e.(2)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
47	<u>STIP – Allocation Amendment</u> Request to amend the locally-administered SR 4 Wagon Trail Realignment Project (Eastern Segment), on the State Highway System, in Calaveras County, to revise the Recipient. There is no change to the allocation amount. (PPNO 3546) Resolution FP-24-83, Amending Resolution FP-22-12 <i>(Related Items under Ref. 2.1b.(1), 2.8a.(9), and 2.8d.(2))</i>	2.5c.(4)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
48	<u>Local Partnership Program (LPP) (Formulaic) – Allocation Amendment</u> Request to amend the locally-administered LPP (Formulaic) I-215/Placentia Avenue Interchange Project, on the State Highway System, in Riverside County, to deallocate \$4,136,000 from the CON phase to reflect project savings and revise the Recipient. (PPNO 3013G) Resolution LPP-A-2425-20, Amending Resolution LPP-A-1920-20 <i>(Related Item under Ref. 4.17)</i>	2.5s.(7)		A	D

This item was presented and approved as revised as part of the Consent Calendar.

Changes to this item were listed on the “Changes to CTC Agenda” handout as follows:

In the Book Item Attachment:

Revise the contribution from other sources amount from \$32,363,000 to \$36,499,000

CTC MEETING
MINUTES
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Tab	Item Description	Ref#	Presenter	Type*	Agency*
49	<u>Multi-Funded Solutions for Congested Corridors Program (SCCP) and LPP (Formulaic) – Allocation Amendment</u> Request to amend the State-Administered multi-funded SCCP and LPP (Formulaic) Santa Barbara U.S. 101 Multimodal Corridor Project - 4E South -City of Santa Barbara, on the State Highway System, in Santa Barbara County, to update the project split note. There is no change to the allocation amount. (PPNO 7101G) Resolution SCCP-A-2425-01, Amending Resolution SCCP-A-2324-07 Resolution LPP-A-2425-19, Amending Resolution LPP-A-2324-39	2.5s.(8)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
50	<u>Multi-Funded SCCP and STIP – Allocation Amendment</u> Request to amend the State-Administered multi-funded SCCP and STIP Marin Sonoma Narrows (MSN) Contract B7 project, on the State Highway System, in Marin County, to allow non-proportional spending. There is no change to the allocation amount. (PPNO 0361M) Resolution SCCP-A-2425-02, Amending Resolution SCCP-A-2122-02 Resolution FP-24-82, Amending Resolution FP-21-08 <i>(Related Items under Ref. 2.2c.(2), 2.5s.(10), 2.8c.(3) and 2.8v.(5))</i>	2.5s.(9) / 2.5v.(2)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
51	<u>LPP (Formulaic) – Allocation Amendment</u> Request to amend the State-Administered LPP (Formulaic) Marin Sonoma Narrows (MSN) Contract B7 project, on the State Highway System, in Marin County, to allow non-proportional spending. There is no change to the allocation amount. (PPNO 0361M) Resolution LPP-A-2425-27, Amending Resolution LPP-A-2122-25 <i>(Related Items under Ref. 2.2c.(2), 2.5s.(9)/2.5v.(2), 2.8c.(3) and 2.8v.(5))</i>	2.5s.(10)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
52	<u>Transit and Intercity Rail Capital Program (TIRCP) – Allocation Amendment</u> Request to amend the Increased Rail Service to Roseville, Service Optimization, and Standby Power Investments component, in various counties, to deallocate \$4,000 from the CON phase to reflect component savings and to revise the project title to include the component name. (PPNO CP023) Resolution TIRCP-2425-57, Amending Resolution TIRCP-1617-09 <i>(Related Item under Ref. 2.6g.(2))</i>	2.6g.(3)		A	D

This item was presented and approved as part of the Consent Calendar.

CTC MEETING
MINUTES
May 15-16, 2025

Tab	Item Description	Ref#	Presenter	Type*	Agency*
53	TIRCP – Allocation Amendment Request to amend the TIRCP Increased Rail Service to Roseville, Service Optimization, and Standby Power Investments component, in various counties, to rescind the full amount of \$200,000 from the CON phase due to a third-party contract not being awarded within the Timely Use of Funds deadline and to revise the project title to include the component name. (PPNO CP023) Resolution TIRCP-2425-56, Amending Resolution TIRCP-1617-08 <i>(Related Item under Ref. 2.6g.(3))</i>	2.6g.(2)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
54	TIRCP – Allocation Amendment Request to amend the TIRCP (2018:13) Peninsula Corridor Joint Powers Board (Battery Electric Multiple Unit Demonstration Trainset) component, in various counties, to revise the project title and to include the request to extend the completion of construction an additional 18 months beyond the 36-month Timely Use of Funds deadline, as originally requested by the implementing agency. There is no change to the allocation amount. (PPNO CP039A) Resolution TIRCP-2425-58, Amending Resolution TIRCP-2324-16	2.6g.(4)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
55	TIRCP – Allocation Amendment Request to amend the TIRCP (2020:4) Inglewood Transit Connector Project, in Los Angeles County, to rescind a combined total amount of \$216,452,000 from the RW phase of two components, due to a change in the delivery plan as follows: <ul style="list-style-type: none"> • \$75,200,000 from the Inglewood Transit Connector Phase 1 component (PPNO CP062A) (2.6g.(5a)) Resolution TIRCP-2425-59, Amending Resolution TIRCP-2324-49 • \$141,252,000 from the Inglewood Transit Connector Phase 2 component (PPNO CP062B) (2.6g.(5b)) Resolution TIRCP-2425-60, Amending Resolution TIRCP-2324-69 <i>(Related Item under Ref. 2.6g.(1))</i>	2.6g.(5)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
56	Approval of the 2023 Active Transportation Program Baseline Agreement for the Coachella Valley Arts & Music Line project Resolution ATP-P-2425-07B	4.12		A	C

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
57	Approval of a State Highway Operation and Protection Program Baseline Agreement for the construction of new maintenance station and the development of an equipment shop site for District 5. Resolution SHOPP-P-2425-06B	4.13		A	C

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
58	<p>LPP (Formulaic) – Allocation Amendment</p> <p>Request to amend three locally-administered LPP (Formulaic) Transit projects, in Alameda County, to deallocate a combined total of \$79,000 from the Construction phase to reflect project savings as follows:</p> <ul style="list-style-type: none"> • \$49,000 from the Division 2 Maintenance Roof Replacement Project. (PPNO 2320C) (2.6s.(4a)) Resolution LPP-A-2425-21, Amending Resolution LPP-A-2021-30 • \$2,000 from the D6 Security Enhancements Phase 2 project. (PPNO 0090B) (2.6s.(4b)) Resolution LPP-A-2425-22, Amending Resolution LPP-A-2223-05 • \$28,000 from the D4 Guard Booths and Gates Replacement project. (PPNO 0090D) (2.6s.(4c)) Resolution LPP-A-2425-23, Amending Resolution LPP-A-2223-05 	2.6s.(4)		A	D

This item was presented and approved as part of the Consent Calendar.

Tab	Item Description	Ref#	Presenter	Type*	Agency*
59	<p>Revised Senate Bill 1 Accountability and Transparency Guidelines</p> <p>Resolution G-25-34, Amending Resolution G-23-28</p>	4.11		A	C

This item was presented and approved as part of the Consent Calendar.

END OF CONSENT CALENDAR

RIGHT-OF-WAY MATTERS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
60	<p>Airspace Lease - Request to Directly Negotiate with Zero Emissions Transit in Los Angeles County</p> <p><i>(Unanimous Approval Required)</i></p>	2.4c.(1)	Ahmed Ghonim René Fletcher	A	D

Item was withdrawn

Tab	Item Description	Ref#	Presenter	Type*	Agency*
61	<p>Airspace Lease – Request to Directly Negotiate with Laketricity USA, Inc. in Napa County</p> <p><i>(Unanimous Approval Required)</i></p>	2.4c.(2)	Ahmed Ghonim René Fletcher	A	D

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Speakers:

Keri Akemi-Hernandez – Member of the public

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
62	Director's Deeds - Roberti Items 1 through 3 Excess Lands - Return to State: \$133,218	2.4d.(2)	Ahmed Ghonim René Fletcher	A	D

Recommendation: Approval**Action Taken:** Approved**Motion:** Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott**Vote result:** 8-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None**ENVIRONMENTAL MATTERS**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
63	<u>Approval of Project for Consideration of Funding:</u> 03-Yub-70, PM 14.8/15.7 State Route 70 Binney Junction Roadway Rehabilitation and Complete Streets Project Replace Binney Junction Underpass and Marysville Railroad Underpass, make operational and safety improvements which include a turn pocket and 2 auxiliary lanes on State Route 70 from post miles 14.8 to 15.7 in Yuba County. (FEIR) (PPNO 9820) (SHOPP) Resolution E-25-43 <i>(Related Items under Ref. 2.4e.(1), 2.4e.(2), and 2.5e.(3))</i>	2.2c.(5)	Cherry Zamora Jeremiah Ketchum	A	C/D

Recommendation: Approval**Action Taken:** Approved**Motion:** Bradshaw **Second:** Falcon **Recused:** Mann **Absent:** Cruz and Elliott**Vote result:** 7-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, and Tiffany**Nays:** None**Abstained:** NoneChanges to this item were listed on the "Changes to CTC Agenda" handout as follows:

In the Book Item Attachment A:

Under 1.1, in the bullet point, third line, revise and provide 5 lanes to make operational and safety improvements which include a turn lane and 2 auxiliary lanes

In the Book Item Attachment B:

Under Project Description, first and second line, revise and provide 5 lanes to make operational and safety improvements which include a turn lane and 2 auxiliary lanes

In the Book Item Attachment D:

Under BACKGROUND, in the bullet point, second line, revise and provide 5 lanes to make operational and safety improvements which include a turn lane and 2 auxiliary lanes

PROGRAM UPDATES

Tab	Item Description	Ref#	Presenter	Type*	Agency*
64	Project Delivery Update	3.8	Tim Sobelman Sujaya Kalainesan	I	D

Commission Chief Engineer Tim Sobelman and Caltrans' Acting Division Chief for Project Management Sujaya Kalainesan presented this informational item.

ALLOCATIONS AND SUPPLEMENTAL FUNDS REQUEST**Projects with costs that exceed the Programmed Amount by More than 20 Percent**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
65	Request for \$329,000 (13.4 percent decrease) allocation in Construction Capital and \$580,000 (262.5 percent increase) allocation in Construction Support, for the SHOPP Bridge Rehabilitation and Replacement project on State Route 99, in Madera County. (EA 0V121, PPNO 6857A) Resolution FP-24-74	2.5d.(1)	Tim Sobelman Michael Navarro	A	D

Tabs 65-68 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
66	Request for \$7,542,000 (71.4 percent increase) allocation in Construction Capital and \$1,500,000 (15.4 percent increase) allocation in Construction Support, for the SHOPP Safety Improvements project on State Routes 43 and 46, in Kern County. (EA 0X770, PPNO 6982) Resolution FP-24-75	2.5d.(2)	Tim Sobelman Michael Navarro	A	D

Tabs 65-68 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
67	Request for \$8,414,000 (197.3 percent increase) allocation in Construction Capital and \$2,057,000 (64.8 percent increase) allocation in Construction Support, for the SHOPP Americans with Disabilities Act Curb Ramps project on United States Highway 395, in Inyo County. (EA 37450, PPNO 2647) Resolution FP-24-76	2.5d.(3)	Tim Sobelman Ryan Dermody	A	D

Tabs 65-68 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
68	Request for \$22,689,000 (52.3 percent increase) allocation in Construction Capital and \$3,375,000 (68.8 percent increase) allocation in Construction Support, for the SHOPP Materials Laboratories project on United States Highway 395, in Inyo County. (EA 37480, PPNO 2649) Resolution FP-24-77	2.5d.(4)	Tim Sobelman Ryan Dermody	A	D

Tabs 65-68 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Capital Outlay Support (COS) Supplemental Fund Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
69	Request for an additional \$560,000 (110.9 percent increase) in Pre-Construction Support for the PA&ED phase, for the SHOPP Pavement Preservation project on United States Highway 101, in Humboldt County. (EA 0L310, PPNO 2551) Resolution FA-24-73	2.5e.(1)	Tim Sobelman Matthew Brady	A	D

Tabs 69, 71-73 and 76-78 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Guardino **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
70	Request for an additional \$570,000 (11.4 percent increase) in Pre-Construction Support for the Plans, Specifications, and Estimate (PS&E) phase, for the SHOPP Roadway Rehabilitation project on Interstate 80, in Placer County. (EA 3H590, PPNO 5131) Resolution FA-24-74 <i>(Related Item under Ref. 2.8v.(1))</i>	2.5e.(5)	Tim Sobelman Sergio Aceves	A	D

Tabs 70, 74 and 75 voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
71	Request for an additional \$2,099,000 (126.1 percent increase) in Pre-Construction Support for the PA&ED phase, for the SHOPP Roadway Rehabilitation project on United States Highway 101, in San Benito County. (EA 1J840, PPNO 2776) Resolution FA-24-75 <i>(Related Item under Ref. 2.8d.(1))</i>	2.5e.(9)	Tim Sobelman Scott Eades	A	D

Tabs 69, 71-73 and 76-78 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Guardino **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
72	Request for an additional \$990,000 (29.5 percent increase) in Pre-Construction Support for the PA&ED phase, for the SHOPP Bridge Rehabilitation and Replacement project on United States Highway 101, in Monterey County. (EA 1K880, PPNO 2904) Resolution FA-24-76 <i>(Related Item under Ref. 2.8d.(1))</i>	2.5e.(10)	Tim Sobelman Scott Eades	A	D

Tabs 69, 71-73 and 76-78 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Guardino **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
73	Request for an additional \$400,000 (17.7 percent increase) in Pre-Construction Support for the PS&E phase, for the SHOPP Safety Improvements project on State Route 9, in Santa Cruz County. (EA 1M400, PPNO 3012) Resolution FA-24-77	2.5e.(11)	Tim Sobelman Scott Eades	A	D

Tabs 69, 71-73 and 76-78 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Guardino **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
74	Request for an additional \$5,100,000 (28.3 percent increase) in Pre-Construction Support for the PS&E phase and \$750,000 (34.9 percent increase) for the Right of Way (RW) Support phase, for the SHOPP Roadway Rehabilitation project on State Route 99, in Fresno County. (EA 0W800, PPNO 6949) Resolution FA-24-78 <i>(Related Item under Ref. 2.2c.(2))</i>	2.5e.(13)	Tim Sobelman Michael Navarro	A	D

Tabs 70, 74 and 75 voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
75	Request for an additional \$240,000 (10.4 percent increase) in Pre-Construction Support for the PS&E phase and \$210,000 (38.9 percent increase) for the RW Support phase, for the SHOPP Roadway Rehabilitation project on State Route 155, in Kern County. (EA 0W810, PPNO 6960) Resolution FA-24-79 <i>(Related Item under Ref. 2.8d.(1))</i>	2.5e.(14)	Tim Sobelman Michael Navarro	A	D

Tabs 70, 74 and 75 voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** Mann **Absent:** Cruz, Eager and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
76	Request for an additional \$250,000 (13.9 percent increase) in Pre-Construction Support for the PS&E phase, for the SHOPP Maintenance Facilities project on State Route 99, in Fresno County. (EA 0X430, PPNO 6973) Resolution FA-24-80	2.5e.(15)	Tim Sobelman Michael Navarro	A	D

Tabs 69, 71-73 and 76-78 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Guardino **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING
MINUTES
May 15-16, 2025

Tab	Item Description	Ref#	Presenter	Type*	Agency*
77	Request for an additional \$1,200,000 (36.6 percent increase) in Pre-Construction Support for the PS&E phase and \$1,010,000 (89.4 percent increase) for the RW Support phase, for the SHOPP Bridge Rehabilitation and Replacement project on State Route 1, in Los Angeles County. (EA 32090, PPNO 4983) Resolution FA-24-81 <i>(Related Item under Ref. 2.8a.(1))</i>	2.5e.(17)	Tim Sobelman Gloria Roberts	A	D

Tabs 69, 71-73 and 76-78 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Guardino **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
78	Request for an additional \$293,000 (7.4 percent increase) in Pre-Construction Support for the PS&E phase, for the SHOPP Pavement Preservation project on State Route 34, in Ventura County. (EA 35590, PPNO 5515) Resolution FA-24-82	2.5e.(18)	Tim Sobelman Gloria Roberts	A	D

Tabs 69, 71-73 and 76-78 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Guardino **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Capital – Supplemental Fund Allocations (Award)

Tab	Item Description	Ref#	Presenter	Type*	Agency*
79	Request for an additional \$2,578,000 (30.3 percent increase) in Construction Capital for the SHOPP Promoting Resilient Operations for Transformative, Efficient, and Cost-saving Transportation Program project on State Route 9, in Santa Cruz County. (EA 1G950, PPNO 2627) Resolution FA-24-83 <i>(Related Items under Ref. 2.2c.(2) and 2.8v.(4))</i>	2.5e.(6)	Tim Sobelman Scott Eades	A	D

Tabs 79-81 and 83-88 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
80	Request for an additional \$1,824,000 (23.6 percent increase) in Construction Capital for the SHOPP Maintenance Facilities project on State Route 74, in Riverside County. (EA 1J320, PPNO 3011L) Resolution FA-24-84	2.5e.(19)	Tim Sobelman Catalino Pining III	A	D

Tabs 79-81 and 83-88 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
81	Request for an additional \$1,919,000 (34.6 percent increase) in Construction Capital for the SHOPP Safety Improvements project on Interstate 40, in San Bernardino County. (EA 1M040, PPNO 3020J) Resolution FA-24-85 <i>(Related Item under Ref. 2.8b.(1))</i>	2.5e.(20)	Tim Sobelman Catalino Pining III	A	D

Tabs 79-81 and 83-88 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Right of Way Capital Supplemental Fund Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
82	Request for an additional \$10,000,000 (65.4 percent increase) in Pre-Construction RW Capital, for the SHOPP Roadway Rehabilitation project on State Route 70, in Yuba County. (EA 0H160, PPNO 9820) Resolution FA-24-86 <i>(Related Items under Ref. 2.4e.(1), 2.4e.(2), and 2.2c.(5))</i>	2.5e.(3)	Tim Sobelman Sergio Aceves	A	D

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Falcon **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Changes to this item were listed on the "Changes to CTC Agenda" handout as follows:

In the Book Item Memorandum:

On page 1, revise the Subject as follows: Supplemental funds allocation for the right of way capital phase

Capital – Supplemental Fund Allocations (Complete Construction)

Tab	Item Description	Ref#	Presenter	Type*	Agency*
83	Request for an additional \$5,750,000 (43.1 percent increase) in Construction Support for the SHOPP Bridge Rehabilitation and Replacement project on United States Highway 101, in Del Norte County. (EA 43640, PPNO 0100V) Resolution FA-24-87 <i>(Related Item under Ref. 2.2c.(2))</i>	2.5e.(2)	Tim Sobelman Matthew Brady	A	D

Tabs 79-81 and 83-88 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
84	Request for an additional \$21,000,000 (140.0 percent increase) in Construction Capital and \$3,500,000 (100.0 percent increase) in Construction Support for the SHOPP Major Damage (Permanent Restoration) project on United States Highway 50, in Sacramento County. (EA 0N620, PPNO 6265) Resolution FA-24-88	2.5e.(4)	Tim Sobelman Sergio Aceves	A	D

Tabs 79-81 and 83-88 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
85	Request for an additional \$1,355,000 (30.8 percent increase) in Construction Support for the SHOPP Bridge Rehabilitation and Replacement project on United States Highway 101, in Santa Barbara County. (EA 1H430, PPNO 2649) Resolution FA-24-89 <i>(Related Item under Ref. 2.2c.(2))</i>	2.5e.(7)	Tim Sobelman Scott Eades	A	D

Tabs 79-81 and 83-88 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
86	Request for an additional \$3,480,000 (9.6 percent increase) in Construction Capital for the SHOPP Pavement Preservation project on United States Highway 101, in Monterey County. (EA 1H690, PPNO 2679) Resolution FA-24-90	2.5e.(8)	Tim Sobelman Scott Eades	A	D

Tabs 79-81 and 83-88 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
87	Request for an additional \$1,853,000 (6.0 percent increase) in Construction Capital and \$1,882,000 (53.0 percent increase) in Construction Support for the SHOPP Pavement Preservation project on State Route 33, in Kern County. (EA 0Y130, PPNO 6978) Resolution FA-24-92	2.5e.(16)	Tim Sobelman Michael Navarro	A	D

Tabs 79-81 and 83-88 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
88	Request for an additional \$4,900,000 (55.1 percent increase) in Construction Capital and \$700,000 (23.4 percent increase) in Construction Support for the SHOPP Transportation Management Systems project at various locations, in Orange County. (EA 0P671, PPNO 2859C) Resolution FA-24-93 <i>(Related Item under Ref. 2.8v.(3))</i>	2.5e.(21)	Tim Sobelman Lan Zhou	A	D

Tabs 79-81 and 83-88 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Bradshaw **Rescued:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

PROGRAM UPDATES**State Highway Operation and Protection Program**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
89	<u>SHOPP Amendments for Approval:</u> Request to: --Add 43 new projects into the 2024 SHOPP. (2.1a.(1a), and 2.1a.(1b)) --Revise 40 projects currently programmed in the 2024 SHOPP. (2.1a.(1d), 2.1a.(1e), and 2.1a.(f)) SHOPP Amendment 24H-010 <u>(Related Items under Ref. 2.5b.(1) and 2.5b.(2))</u>	2.1a.(1)	Jon Pray James R. Anderson	A	D

Tabs 89-91 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Falcon **Recused:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Speakers:

Norbert Dall – Albion Bridge Stewards

Changes to this item were listed on the “Changes to CTC Agenda” handout as follows:

MEETING HANDOUT – Letters to the CTC

In the Book Item Attachment 2.1a.(1d):

Project 33 – EA 11-43023/PPNO 11-1311 – San Diego County – In the project split note, revise PPNO 1670 to 1617

Project 34 – EA 11-43281/PPNO 11-1617 – San Diego County – In the project split note, revise PPNO 1670 to 1617

Tab	Item Description	Ref#	Presenter	Type*	Agency*
90	<u>SHOPP Amendments for Approval</u> --Revise 19 projects currently programmed in the 2024 SHOPP. (2.1a.(2d) SHOPP Amendment 24H-011	2.1a.(2)	Jon Pray James R. Anderson	A	D

Tabs 89-91 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Falcon **Recused:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
91	Request for an additional \$200,000,000, from \$704,000,000 to \$904,000,000, for the Fiscal Year 2024-25 Major Damage (Emergency Restoration) Reservation Resolution G-25-35, Amending Resolution G-25-29	4.14	Jon Pray Bobi Hettick	A	D

Tabs 89-91 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Falcon **Recused:** None **Absent:** Cruz, Eager, and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Trade Corridor Enhancement Program (TCEP)

Tab	Item Description	Ref#	Presenter	Type*	Agency*
92	<u>2020 Trade Corridor Enhancement Program Amendment</u> Amendment to the Stockton Diamond Grade Separation Project Resolution G-25-36, Amending Resolution G-21-69 <i>(Related Item under Ref. 2.6s.(5))</i>	4.15	Beverley Newman-Burckhard	A	C

Tabs 92 and 106 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
93	<u>2022 Trade Corridor Enhancement Program Amendment</u> Amendment to the Port of Oakland Microgrid Project Resolution G-25-37, Amending Resolution G-25-30	4.16	Beverley Newman-Burckhard	A	C

Recommendation: Approval

Action Taken: Approved

Motion: Mann **Second:** Falcon **Recused:** Bradshaw **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Local Partnership Program (LPP)

Tab	Item Description	Ref#	Presenter	Type*	Agency*
94	<u>Local Partnership Formulaic Program Amendments</u> <ul style="list-style-type: none"> • Deprogram \$4,136,000 from the Interstate-215/Placentia Avenue Project in Riverside County. • Add five projects in Riverside, Orange, and Alameda Counties and program \$39,211,000. Resolution LPP-P-2425-08, Amending Resolution LPP-P-2425-07 <i>(Related Item under Ref. 2.5s.(7))</i>	4.17	Kayla Giese	A	C

Recommendation: Approval**Action Taken:** Approved**Motion:** Tiffany **Second:** Bradshaw **Recused:** None **Absent:** Cruz, Eager and Elliott**Vote result:** 7-0**Ayes:** Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None**Active Transportation Program (ATP)**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
95	ATP – Project Scope Amendment The City of Oakland proposes to amend the scope of the ATP East Oakland Neighborhood Bike Routes project, in Alameda County. (PPNO 2346) Resolution ATP-P-2425-09	4.22	Anja Aulenbacher Dee Lam	A	D

Recommendation: Approval**Action Taken:** Approved**Motion:** Bradshaw **Second:** Mann **Recused:** None **Absent:** Cruz, Eager and Elliott**Vote result:** 7-0**Ayes:** Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None**ALLOCATIONS****SHOPP Allocations**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
96	Request of \$2,632,000 for two District Minor projects. Resolution FP-24-78	2.5a.	Jon Pray James R. Anderson	A	D

Tabs 96-98 were voted together

Recommendation: Approval**Action Taken:** Approved**Motion:** Falcon **Second:** Bradshaw **Recused:** None **Absent:** Cruz, Eager and Elliott**Vote result:** 7-0**Ayes:** Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
97	Request of \$539,245,000 for 18 SHOPP projects. Resolution FP-24-79 <i>(Related Items under Ref. 2.1a.(1), 2.2c.(2), and 2.5w.(2))</i>	2.5b.(1)	Jon Pray James R. Anderson	A	D

Tabs 96-98 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Falcon **Second:** Bradshaw **Recused:** None **Absent:** Cruz, Eager and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Changes to this item were listed on the "Changes to CTC Agenda" handout as follows:

In the Book Item Memorandum:

Under ISSUE, RECOMMENDATION, and BACKGROUND, revise \$539,245,000 to \$503,915,000 and 18 to 16

Under FINANCIAL RESOLUTION, revise \$468,771,000 to \$433,441,000 and 18 to 16

In the Book Item Attachment:

Project 5 – EA 04-2Q720 – Alameda County – revise the project location as follows: In the cities of San Leandro, Oakland, and Alameda, from Route 112 (Davis Street) to Encinal Avenue and add the following information: PPNO 04-2027G, the CON ENG RMRA Budget information 505-3290 RMRA and the CON ENG RMRA amount \$398,000

Project 8 – EA 07-37190/PPNO 07-5743 – Los Angeles County – \$12,583,000 – Withdrawn prior to the CTC meeting

Project 9 – EA 07-37290/PPNO 07-5806 – Los Angeles County - \$22,747,000 – Withdrawn prior to the CTC meeting

Project 14 – EA 10-0X740/PPNO 10-3288 – revise the CON ENG Program Code from 20.10.201.110 to 20.10.201.116 and the CONST Program Code from 20.20.201.110 to 20.20.201.116

Tab	Item Description	Ref#	Presenter	Type*	Agency*
98	Request of \$48,878,000 for 36 2024 SHOPP preconstruction project phases for environmental support, design, and R/W support. Resolution FP-24-80 <i>(Related Items under Ref. 2.1a.(1) and 2.2c.(1))</i>	2.5b.(2)	Jon Pray James R. Anderson	A	D

Tabs 96-98 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Falcon **Second:** Bradshaw **Recused:** None **Absent:** Cruz, Eager and Elliott

Vote result: 7-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

STIP Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
99	Request of \$2,631,000 for seven locally-administered STIP Planning, Programming, and Monitoring projects, off the State Highway System. Resolution FP-24-81	2.5c.(3)	Laurie Waters Dee Lam	A	D

Tabs 99 and 100 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Mann **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

STIP Transit Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
100	Request of \$40,749,000 for the locally-administered STIP Buses, 58 ZEBs, Proj #2 (Tier II) Transit project, in Los Angeles County. (PPNO 5738) Resolution MFP-24-04	2.6a.	Laurie Waters Dee Lam	A	D

Tabs 99 and 100 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Tiffany **Second:** Mann **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

LPP (Formulaic) Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
101	Request of \$2,000,000 for two locally-administered LPP (Formulaic) projects, off the State Highway System. Resolution LPP-A-2425-25	2.5s.(3)	Kayla Giese Dee Lam	A	D

Tabs 101 and 102 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Mann **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

LPP (Formulaic) Transit Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
102	Request of \$1,615,000 for the locally-administered LPP (Formulaic) Lake Merritt UPS Replacement and Relocation Transit project, in Alameda County. (PPNO 2364F) Resolution LPP-A-2425-26 <i>(Related Item under Ref. 2.6g.(1))</i>	2.6s.(2)	Kayla Giese Dee Lam	A	D

Tabs 101 and 102 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Mann **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

TCEP Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
103	Request of \$1,018,000 for the locally-administered TCEP Sacramento County WattEV Innovative Freight Terminal (SWIFT) project, off the State Highway System, in Sacramento County. (PPNO 1821) Resolution TCEP-A-2425-18	2.5s.(4)	Sheila Ennes Dee Lam	A	D

Tabs 103-105 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
104	Request of \$30,000,000 for the locally-administered TCEP US 395 – Phase 2 Freight Mobility and Safety Project-CON Mainline, on the State Highway System, in San Bernardino County. (PPNO 3019L) Resolution TCEP-A-2425-19	2.5s.(5)	Sheila Ennes James R. Anderson	A	D

Tabs 103-105 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Changes to this item were listed on the “Changes to CTC Agenda” handout as follows:

In the Book Item Attachment:

Add 05/01/2025 for the Right of Way Certification date

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
105	Request of \$129,000,000 for the State-Administered TCEP Westbound I-80 Cordelia Commercial Vehicle Enforcement Facility project, on the State Highway System, in Solano County. (PPNO 8273C) Resolution TCEP-A-2425-20 <i>(Related Item under Ref. 2.2c.(2))</i>	2.5s.(6)	Sheila Ennes James R. Anderson	A	D

Tabs 103-105 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Multi-Funded TCEP and TIRCP Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
106	Request of \$12,000,000 for the locally-administered multi-funded TCEP and TIRCP (2024:21) Bridging Rail Initiatives, Technology, and Education (BRITE) (South Stockton Yard Crossover) project, in San Joaquin County. (PPNO CP163) Resolution TCEP-A-2425-17 Resolution TIRCP-2425-62 <i>(Related Item under Ref. 4.15)</i>	2.6s.(5)	Beverley Newman-Burckhard Dee Lam	A	D

Tabs 92 and 106 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Multi-Funded TCEP and TIRCP – Advance Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
107	Request of \$ 92,321,000 for two locally-administered multi-funded TCEP and TIRCP (2024:16) Coastal Rail Infrastructure Resiliency Project components, in Orange County, programmed in Fiscal Year 2025-26. Resolution TCEP-A-2425-21 Resolution TIRCP-2425-64	2.6s.(3)	Beverley Newman-Burckhard Dee Lam	A	D

Recommendation: Approval as revised**Action Taken:** Approved**Motion:** Mann **Second:** Falcon **Recused:** None **Absent:** Cruz and Elliott**Vote result:** 8-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** NoneChanges to this item were listed on the “Changes to CTC Agenda” handout as follows:

In the Book Item Attachment:

Project 1 – PPNO CP171A – Orange County – Add 05/12/2025 for the Coastal Development Permit date

Project 2 – PPNO CP171B – Orange County – Add 05/12/2025 for the Coastal Development Permit date

TIRCP Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
108	Request of \$205,124,000 for nine TIRCP projects. Resolution TIRCP-2425-63 <i>(Related Items under Ref. 2.2c.(3), 2.6g.(5), and 2.6s.(2))</i>	2.6g.(1)	Kat Kim Dee Lam	A	D

Recommendation: Approval**Action Taken:** Approved**Motion:** Tiffany **Second:** Falcon **Recused:** None **Absent:** Cruz and Elliott**Vote result:** 8-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None

Speakers:

Lisa Trifiletti – City of Inglewood

Changes to this item were listed on the “Changes to CTC Agenda” handout as follows:

MEETING HANDOUT - Letters to the CTC

In the Book Item Attachment:

Project 5 – Los Angeles County – revise the PPNO from CP062C to CP062E

ATP Allocations

Tab	Item Description	Ref#	Presenter	Type*	Agency*
109	Request of \$22,865,000 for 17 locally-administered ATP projects, off the State Highway System. Resolution FATP-2425-18 <i>(Related Items under Ref. 2.2c.(6) and 2.8a.(17))</i>	2.5w.(1)	Jaeden Gales Dee Lam	A	D

Tab 109-111 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Falcon **Second:** Mann **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Changes to this item were listed on the “Changes to CTC Agenda” handout as follows:

In the Book Item Attachment:

Project 6 – PPNO 8024A – Tulare County – Add 05/14/2025 for the NEPA date

Tab	Item Description	Ref#	Presenter	Type*	Agency*
110	Request of \$7,470,000 for the State-Administered ATP Happy Camp Complete Streets Project, on the State Highway System, in Siskiyou County. (PPNO 3537) Resolution FATP-2425-19 <i>(Related Item under Ref. 2.5b.(1))</i>	2.5w.(2)	Jaeden Gales James R. Anderson	A	D

Tab 109-111 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Falcon **Second:** Mann **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

ATP-Advance Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
111	Request of \$5,681,000 for the locally-administered ATP Bayshore Bikeway Segment 5 Connections project, off the State Highway System, in San Diego County, programmed in Fiscal Year 2025-26. (PPNO 1624) Resolution FATP-2425-20 <i>(Related Item under Ref. 4.10)</i>	2.5w.(3)	Jaeden Gales Dee Lam	A	D

Tab 109-111 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Falcon **Second:** Mann **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Changes to this item were listed on the “Changes to CTC Agenda” handout as follows:

In the Book Item Attachment:

Add 04/23/2025 for the NEPA date and add 05/09/2025 for the Right of Way Certification date

Waterborne Ferry Program Allocation

Tab	Item Description	Ref#	Presenter	Type*	Agency*
112	Request of \$3,547,000 for the Waterborne Ferry Program in the San Francisco Bay Area for Fiscal Year 2025-26. Resolution MFP-24-05	2.6d.	Rebecca Light Dee Lam	A	D

Recommendation: Approval**Action Taken:** Approved**Motion:** Guardino **Second:** Bradshaw **Recused:** None **Absent:** Cruz and Elliott**Vote result:** 8-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None**TIME EXTENSION REQUESTS****SHOPP Time Extensions**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
113	Request to extend the period of project allocation for nine SHOPP projects, per SHOPP Guidelines. Waiver 25-50 <i>(Related Item under Ref. 2.5e.(17))</i>	2.8a.(1)	Ahmed Ghonim James R. Anderson	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval**Action Taken:** Approved**Motion:** Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott**Vote result:** 8-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** NoneChanges to this item were listed on the "Changes to CTC Agenda" handout as follows:

In the Book Item Memorandum:

Under ISSUE and RECOMMENDATION, revise nine SHOPP projects to three SHOPP projects

In the Book Item Attachment:

Project 1 – EA 03-0J510/PPNO 03-3918 – Withdrawn prior to the CTC meeting

Project 3 – EA 04-4AC80/PPNO 04-2804G – Withdrawn prior to the CTC meeting

Project 4 – EA 04-0J632/PPNO 04-0480X – Withdrawn prior to the CTC meeting

Project 6 – EA 04-2Q770/PPNO 04-2028S – Withdrawn prior to the CTC meeting

Project 7 – EA 07-32090/PPNO 07-4983 – Withdrawn prior to the CTC meeting

Project 8 – EA 07-36460/PPNO 07-5624 – Withdrawn prior to the CTC meeting

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
114	Request to extend the period of contact award for five SHOPP projects, per SHOPP Guidelines. Waiver 25-51 <i>(Related Items under Ref. 2.5e.(20) and 2.8v.(4))</i>	2.8b.(1)	Ahmed Ghonim James R. Anderson	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
115	Request to extend the period of project completion for seven SHOPP projects, per SHOPP Guidelines. Waiver 25-52	2.8c.(1)	Ahmed Ghonim James R. Anderson	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Changes to this item were listed on the "Changes to CTC Agenda" handout as follows:

In the Book Item Attachment:

Project 5 – EA 08-0R420/PPNO 08-3005R – revise the County from Los Angeles to San Bernardino

Tab	Item Description	Ref#	Presenter	Type*	Agency*
116	Request to extend the period of project development expenditure for 18 SHOPP projects, per SHOPP Guidelines. Waiver 25-53 <i>(Related Items under Ref. 2.5e.(9), 2.5e.(10), and 2.5e.(14))</i>	2.8d.(1)	Ahmed Ghonim James R. Anderson	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

STIP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
117	Request to extend the period of project allocation for the locally-administered STIP Santa Claus Lane Railroad Crossing and Beach Access Improvements Phase 2 project, on the State Highway System, in Santa Barbara County, per STIP guidelines. (PPNO 3214) Waiver 25-54	2.8a.(2)	Laurie Waters James R. Anderson	A	D

Tabs 117, 122, 127, 134, 139, 140, 142, and 147 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Falcon **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
118	Request to extend of period of project allocation for 10 locally-administered STIP projects, off the State Highway System, per STIP Guidelines. Waiver 25-55	2.8a.(3)	Laurie Waters Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Changes to this item were listed on the "Changes to CTC Agenda" handout as follows:

In the Book Item Attachment:

Project 9 – PPNO 3650 – Amador County – per Staff Recommendation revise the following:

Number of Months requested from 6 Months to 12 Months

Extended Deadline from 12/31/2025 to 06/30/2026

In the first paragraph, first line, revise 6-month to 12-month

In the last paragraph, revise 6-month to 12-month and December 31, 2025 to June 30, 2026

Project 10 – PPNO 3653 – Amador County – per Staff Recommendation revise the following:

Number of Months requested from 6 Months to 12 Months

Extended Deadline from 12/31/2025 to 06/30/2026

In the first paragraph, first line, revise 6-month to 12-month

In the last paragraph, revise 6-month to 12-month and December 31, 2025 to June 30, 2026

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
119	Request to extend the period of project completion for the locally-administered STIP Bus Acquisition #1, 40 ZEBs Transit project, in Los Angeles County, per STIP Guidelines. (PPNO 5430) Waiver 25-57	2.8c.(2)	Laurie Waters Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
120	Request to extend the period of project development expenditure for two locally-administered STIP projects, on the State Highway System, per STIP Guidelines. Waiver 25-58 <i>(Related Items under Ref. 2.1b.(1), 2.5c.(4), and 2.8a.(9))</i>	2.8d.(2)	Laurie Waters James R. Anderson	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
121	Request to extend the period of project development expenditure for two locally-administered STIP projects, off the State Highway System, per STIP Guidelines. Waiver 25-59	2.8d.(3)	Laurie Waters Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
122	Request to extend the period of project development expenditure for the locally-administered STIP Elk Grove to Philips Siding Rail Operational and Capacity Improvements Project, in Sacramento County, per STIP Guidelines. (PPNO 9893) Waiver 25-60	2.8d.(4)	Laurie Waters Dee Lam	A	D

Tabs 117, 122, 127, 134, 139, 140, 142, and 147 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Falcon **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Multi-Funded STIP and SCCP Time Extension

Tab	Item Description	Ref#	Presenter	Type*	Agency*
123	Request to extend the period of project completion for the State-Administered multi-funded STIP and SCCP Marin Sonoma Narrows (MSN) Contract B7 project, on the State Highway System, in Marin County, per STIP and SCCP Guidelines. (PPNO 0361M) Waiver 25-62 <i>(Related Items under Ref. 2.2c.(2), 2.5s.(9)/2.5v.(2), 2.5s.(10), and 2.8v.(5))</i>	2.8c.(3)	Naveen Habib James R. Anderson	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

SCCP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
124	Request to extend the period of project allocation for three locally-administered SCCP Rail projects, in Ventura County, per SCCP Guidelines. Waiver 25-73	2.8a.(12)	Naveen Habib Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
125	Request to extend the period of project completion for the locally-administered SCCP PSGC Phase 1 - South Placer Transit - Five Electric Buses project, off the State Highway System, in Placer County, per SCCP Guidelines. (PPNO 1535A) Waiver 25-74	2.8c.(7)	Naveen Habib Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Multi-Funded SCCP and ATP Time Extension

Tab	Item Description	Ref#	Presenter	Type*	Agency*
126	Request to extend the period of project allocation for the locally-administered multi-funded SCCP and ATP Santa Paula Trail, East Ventura Station to e/o Wells Road project, off the State Highway System, in Ventura County, per SCCP and ATP Guidelines. (PPNO 6194) Waiver 25-75	2.8a.(13)	Naveen Habib Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

LPP (Competitive and Formulaic) Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
127	Request to extend the period of project allocation for the locally-administered LPP (Competitive) Hazel Avenue/US 50 Interchange project, on the State Highway System, in Sacramento County, per LPP (Competitive) Guidelines. (PPNO 6222) Waiver 25-63	2.8a.(6)	Leishara Ward James R. Anderson	A	D

Tabs 117, 122, 127, 134, 139, 140, 142, and 147 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Falcon **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
128	Request to extend the period of project allocation for two locally-administered LPP (Formulaic) projects, off the State Highway System, per LPP (Formulaic) Guidelines. Waiver 25-64	2.8a.(7)	Leishara Ward Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
129	Request to extend the period of project completion for the locally-administered LPP (Formulaic) Replacement of 30 Buses from 2009 Transit project, in San Mateo County, per LPP (Formulaic) Guidelines. (PPNO 2141A) Wavier 25-65	2.8c.(9)	Leishara Ward Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
130	Request to extend the period of project completion for the locally-administered LPP (Competitive) US 101/De La Cruz Boulevard/Trimble Road Interchange Improvements project, on the State Highway System, in Santa Clara County, per LPP (Competitive) Guidelines.(PPNO 0462J) Waiver 25-66	2.8c.(4)	Leishara Ward James R. Anderson	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
131	Request to extend the period of project completion for two locally-administered LPP (Competitive) projects, off the State Highway System, per LPP (Competitive) Guidelines. Waiver 25-67	2.8c.(5)	Leishara Ward Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

TCEP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
132	Request to extend the period of project allocation for the three locally-administered TCEP projects, on the State Highway System, per TCEP Guidelines. Waiver 25-68 <i>(Related Items under Ref. 2.1b.(1), 2.5c.(4), and 2.8d.(2))</i>	2.8a.(9)	Kenneth Lopez James R. Anderson	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
133	Request to extend the period of allocation for seven locally-administered TCEP projects, off the State Highway System, per TCEP Guidelines. Waiver 25-69	2.8a.(10)	Kenneth Lopez Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
134	Request to extend the period of project allocation for the locally-administered TCEP Union Pacific (Fresno Subdivision) Ceres to Turlock Double-Tracking Rail Project, in Stanislaus County, per TCEP Guidelines. (PPNO T0020) Waiver 25-70	2.8a.(11)	Kenneth Lopez Dee Lam	A	D

Tabs 117, 122, 127, 134, 139, 140, 142, and 147 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Falcon **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
135	Request to extend the period of contract award for the locally-administered TCEP US 395 - Phase 2 Freight Mobility and Safety Project – Zero-Emission Procurement Component, off the State Highway System, in San Bernardino County, per TCEP Guidelines.(PPNO 1323A) Waiver 25-71	2.8b.(2)	Kenneth Lopez Dee Lam	A	D

Tabs 113-116, 118-121, 123-126,128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
136	Request to extend the period of project completion for the locally-administered TCEP Port of San Diego Tenth Avenue Marine Terminal Beyond Compliance Environmental Enhancement Project, off the State Highway System, in San Diego County, per TCEP Guidelines. (PPNO T0013) Waiver 25-72	2.8c.(6)	Kenneth Lopez Dee Lam	A	D

Tabs 113-116, 118-121, 123-126,128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

LTCAP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
137	Request to extend the period of project allocation for three State-Administered LTCAP projects, on the State Highway System, per LTCAP Guidelines. Waiver 25-76	2.8a.(14)	Rebecca Light Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
138	Request to extend the period of project allocation for six locally-administered LTCAP projects, off the State Highway System, per LTCAP Guidelines. Waiver 25-77	2.8a.(15)	Rebecca Light Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

ATP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
139	Request to extend the period of project allocation for two State-Administered ATP projects, on the State Highway System, per ATP Guidelines. Waiver 25-78 <i>(Related Item under Ref. 2.8d.(5))</i>	2.8a.(16)	Jaeden Gales James R. Anderson	A	D

Tabs 117, 122, 127, 134, 139, 140, 142, and 147 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Falcon **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
140	Request to extend the period of project allocation for 58 locally-administered ATP projects, off the State Highway System, per ATP Guidelines. Waiver 25-79 <i>(Related Items under Ref. 2.5w.(1) and 2.8d.(6))</i>	2.8a.(17)	Jaeden Gales Dee Lam	A	D

Tabs 117, 122, 127, 134, 139, 140, 142, and 147 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Falcon **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Changes to this item were listed on the "Changes to CTC Agenda" handout as follows:

In the Book Item Attachment:

Project 4 – PPNO 2355 – revise the project title as follows: 6th Street Pedestrian Safety Project to West Oakland Link of the Bay Skyway

Project 52 – PPNO 1491 – PS&E phase – revise the Agency Name to City of Imperial Beach

Project 53 – PPNO 1491 – RW phase – revise the Agency Name to City of Imperial Beach

Tab	Item Description	Ref#	Presenter	Type*	Agency*
141	Request to extend the period of contract award for two ATP projects, off the State Highway System, per ATP Guidelines. Waiver 25-80	2.8b.(3)	Jaeden Gales Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
142	Request to extend the period of project development expenditure for four State-Administered ATP projects, on the State Highway System, per ATP Guidelines. Waiver 25-81 <i>(Related Item under Ref. 2.8a.(16))</i>	2.8d.(5)	Jaeden Gales James R. Anderson	A	D

Tabs 117, 122, 127, 134, 139, 140, 142, and 147 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Falcon **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
143	Request to extend the period of project development expenditure for 12 locally-administered ATP projects, off the State Highway System, per ATP Guidelines. Waiver 25-82 <i>(Related Item under Ref. 2.8a.(17))</i>	2.8d.(6)	Jaeden Gales Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

TIRCP Time Extensions

Tab	Item Description	Ref#	Presenter	Type*	Agency*
144	Request to extend the period of contract award for the TIRCP (2022:12) Expanding Transit Services and Introducing Zero-Emission Fleets on California's North Coast (Hydrogen Infrastructure) component, in Humboldt County, per TIRCP Allocation Policy. (PPNO CP101A) Waiver 25-83	2.8b.(4)	Kat Kim Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
145	Request to extend period of project completion for the TIRCP (2020:6) LBT/UCLA Electric Commuter Express (Vehicle Procurement) component, in Los Angeles County, per TIRCP Allocation Policy. (PPNO CP064) Waiver 25-84	2.8c.(8)	Kat Kim Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
146	Request to extend the period of project development expenditure for the TIRCP San Dieguito to Sorrento Valley Double Track and Realignment project, in San Diego County, per TIRCP Allocation Policy. (PPNO CP084) Waiver 25-85	2.8d.(7)	Kat Kim Dee Lam	A	D

Tabs 113-116, 118-121, 123-126, 128-133, 135-138, 141, and 143-146 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

TIME EXTENSION AMENDMENTS

Tab	Item Description	Ref#	Presenter	Type*	Agency*
147	Request to amend the period of project allocation for five SHOPP projects, as an exception to the SHOPP Guidelines. Waiver 25-87, Amending Waivers 24-125, 24-165, and 24-199 (Related Item under Ref. 2.5e.(5))	2.8v.(1)	Ahmed Ghonim James R. Anderson	A	D

Tabs 117, 122, 127, 134, 139, 140, 142, and 147 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Bradshaw **Second:** Falcon **Recused:** Mann **Absent:** Cruz, Eager, and Elliott

Vote result: 6-0

Ayes: Bradshaw, Falcon, Grisby, Guardino, Lugo, and Tiffany

Nays: None

Abstained: None

Changes to this item were listed on the "Changes to CTC Agenda" handout as follows:

In the Book Item Attachment:

Project 2 – EA 06-0X290/PPNO 06-6927 – Fresno County – revise the following:

Number of Months Requested from 12 months to 14 months

Extended deadline from 6/30/2025 to 8/31/2025

In the first paragraph, revise an additional 1 month from 11 months to 12 months to an additional 3 months from 11 months to 14 months

In the second paragraph, revise the last sentence as follows:

Remove: Now that the delay has been resolved, additional time is needed to prepare the project to achieve RTL by May 5, 2025.

Replace with: The team continued to process documents for the RTL requirements. The Critical Path Method (CPM) construction schedule took longer than expected to finalize, which delayed the RTL for the project. The CPM schedule was finalized on May 2, 2025 reflecting more working days (WD) than programmed. The project achieved RTL on May 5, 2025 and will be ready for a construction allocation at the August 2025 Commission meeting.

In the last paragraph, revise an additional one month to an additional three months and June 30, 2025 to August 31, 2025.

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
148	Request to amend the period of contract award for the SHOPP PROTECT Program project, in San Diego County, as an exception to the SHOPP Guidelines. (PPNO 1315) Waiver 25-88, Amending Waiver 25-22	2.8v.(2)	Ahmed Ghonim James R. Anderson	A	D

Tabs 148 -150 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Falcon **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
149	Request to amend the period of project completion for two SHOPP projects, as an exception to the SHOPP Guidelines. Waiver 25-89, Amending Waivers 23-133 and 25-18 <i>(Related Item under Ref. 2.5e.(21))</i>	2.8v.(3)	Ahmed Ghonim James R. Anderson	A	D

Tabs 148 -150 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Falcon **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

Tab	Item Description	Ref#	Presenter	Type*	Agency*
150	Request to amend the period of project development expenditure for three SHOPP projects, as an exception to SHOPP Guidelines. Waiver 25-90, Amending Waivers 24-53, 25-19 and 25-24 <i>(Related Items under Ref. 2.2c.(2), 2.8b.(1) and 2.5e.(6))</i>	2.8v.(4)	Ahmed Ghonim James R. Anderson	A	D

Tabs 148 -150 were voted together

Recommendation: Approval

Action Taken: Approved

Motion: Falcon **Second:** Tiffany **Recused:** None **Absent:** Cruz and Elliott

Vote result: 8-0

Ayes: Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany

Nays: None

Abstained: None

CTC MEETING**MINUTES****May 15-16, 2025**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
151	Request to amend the period of project development expenditure for the State-Administered LPP (Formulaic) Marin Sonoma Narrows (MSN) Contract B7 project, on the State Highway System, in Marin County. (PPNO 0361M) Waiver 25-91, Amending Waiver 24-99 <i>(Related Items under Ref. 2.2c.(2), 2.5s.(9)/2.5v.(2), 2.5s.(10), and 2.8c.(3))</i>	2.8v.(5)	Leishara Ward James R. Anderson	A	D

Item was withdrawn

Tab	Item Description	Ref#	Presenter	Type*	Agency*
152	Request to amend the period of project completion for the TIRCP (2016:9) Peninsula Corridor Electrification Project (Procurement of Electric Multiple Unit Vehicles) component, in various counties. (PPNO CP022) Waiver 25-93, Amending Waiver 24-46	2.8v.(7)	Kat Kim Dee Lam	A	D

Recommendation: Approval**Action Taken:** Approved**Motion:** Falcon **Second:** Mann **Recused:** None **Absent:** Cruz and Elliott**Vote result:** 8-0**Ayes:** Bradshaw, Eager, Falcon, Grisby, Guardino, Lugo, Mann, and Tiffany**Nays:** None**Abstained:** None**OTHER MATTERS**

Tab	Item Description	Ref#	Presenter	Type*	Agency*
	Public Comment	6	Darnell Grisby	I	C

Speakers:

Aleto Dupree – Member of the Public

ADJOURN

Day 1 adjourned in memory of fallen transportation workers and Chairman Bo Mazzetti of the Rincon Band of Luiseño Indians Tribal Council.

Tanisha Taylor, Executive Director

Date

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 1.5, Action

Prepared By: Douglas Remedios
Deputy Director, Administration and Financial Management (Interim)

Published Date: June 13, 2025

Subject: Commissioner Meetings for Compensation

Recommendation:

Staff recommends that the California Transportation Commission (Commission) approve the Commissioner meetings for compensation as provided below.

1. Meetings for Compensation – April 2025 (Attachment A)
2. Amended Meetings for Compensation – March 2025 (Attachment B)

Issue:

Pursuant to Government Code Section 14509, each member of the Commission shall receive compensation of one hundred dollars (\$100) per day, but not to exceed eight hundred dollars (\$800) per month, for any Commission business authorized by the Commission during any month, when a majority of the Commission approves the compensation by a recorded vote. The need for up to eight days per diem per month is unique to the Commission in that its members must evaluate projects and issues throughout the state to carry out its responsibilities.

Attachments:

- Attachment A: Meetings for Compensation – April 2025
- Attachment B: Amended Meetings for Compensation – March 2025

MEETINGS FOR COMPENSATION

April 2025

Jay Bradshaw

No compensation requested.

Joseph Cruz

No meetings reported at this time.

Lee Ann Eager

April 1 – Teleconference with Jerry Dyer and Georgeanne White re: State Route 99 project. San Diego

April 4 – Attended a tour of Laguna Canyon Road. Laguna Beach

April 7 – Video conference with Commission staff re: Commission, Air Resources Board, and Housing and Community Development Department joint meeting briefing. San Diego

April 10 – Attended the Commission, Air Resources Board, and Housing and Community Development Department joint meeting. Sacramento

April 11 – Speaker at the San Joaquin Valley Policy conference. Merced

April 16 – Attended emergency Commission meeting. San Diego

April 18 – Teleconference with staff from the City of Fresno and Caltrans District 6 re: State Route 99 project. San Diego

April 21 – Teleconference with Tanisha Taylor re: Commission matters. San Diego

April 25 – Teleconference with Michael Navarro re: Kern County project. San Diego

April 25 – Teleconference with Ahron Hakimi re: State Route 99 and Kern County projects. San Diego

Jason Elliott

April 10 – Attended the Commission, Air Resources Board, and Housing and Community Development Department joint meeting. Sacramento

April 16 – Attended emergency Commission meeting. Sacramento

Clarissa Falcon

April 1 – Video conference with Darnell Grisby, Carl Guardino and Tanisha Taylor re: Commission matters. San Diego

April 4 – Attended a tour of Laguna Canyon Road. Laguna Beach

April 8 – Video conference with Darnell Grisby, Carl Guardino and Tanisha Taylor re: Commission matters. San Diego

April 10 – Attended the Commission, Air Resources Board, and Housing and Community Development Department joint meeting. Sacramento

April 16 – Attended emergency Commission meeting. San Diego

April 22 – Video conference with Darnell Grisby, Carl Guardino and Tanisha Taylor re: Commission matters. San Diego

April 24 – Attended the Imperial County Town Hall. El Centro

April 25 – Attended the Imperial County Town Hall. El Centro

Darnell Grisby

No meetings reported at this time.

Carl Guardino

- April 1 – Video conference with Darnell Grisby, Clarissa Falcon and Tanisha Taylor re: Commission matters. San Jose
- April 7 – Video conference with Commission staff re: Commission, Air Resources Board, and Housing and Community Development Department joint meeting briefing. San Jose
- April 8 – Video conference with Darnell Grisby, Clarissa Falcon and Tanisha Taylor re: Commission matters. San Jose
- April 14 – Video conference with Commission staff re: Tracy Local Transportation Climate Adaptation Program Lammers Road Extension project. San Jose
- April 15 – Video conference with Darnell Grisby, Clarissa Falcon and Tanisha Taylor re: Commission matters. San Jose
- April 16 – Attended emergency Commission meeting. San Jose
- April 17 – Meeting with Fred Keeley re: Santa Cruz transportation improvements. San Jose
- April 22 – Video conference with Darnell Grisby, Clarissa Falcon and Tanisha Taylor re: Commission matters. San Jose

Adonia Lugo

No meetings reported at this time.

Zahirah Mann

No meetings reported at this time.

Bob Tiffany

- April 7 – Video conference with Commission staff re: joint meeting briefing. Hollister
- April 10 – Attended the Measuring Transportation Impacts workshop #2. Sacramento
- April 10 – Attended the Commission, Air Resources Board, and Housing and Community Development Department joint meeting. Sacramento
- April 16 – Attended emergency Commission meeting. Hollister
- April 18 – Meeting with Supervisor Angela Curro re: State Route 21 and other county issues. Tres Pinos
- April 21 – Video conference with Tanisha Taylor re: Commission matters. Hollister
- April 24 – Attended the Imperial County Town Hall meeting. El Centro
- April 25 – Attended the Imperial County Town Hall meeting. El Centro
- April 30 – Meeting with City of Salinas staff re: Salinas transportation issues. Salinas

AMENDED MEETINGS FOR COMPENSATION

March 2025

Carl Guardino

- March 1 – Video conference with Darnell Grisby, Clarissa Falcon and Tanisha Taylor re: Commission matters. San Jose
- March 3 – Teleconference with Jim Beall re: State Route 152. San Jose
- March 10 – Meeting with Carolyn Gonot and Tim Maguire re: BART phase II. San Jose
- March 13 – Video conference with Darnell Grisby, Clarissa Falcon and Tanisha Taylor re: Commission matters. San Jose
- March 17 – Video conference with Commission staff re: agenda briefing. San Jose
- March 17 – Video conference with Metropolitan Transportation Commission staff re: Commission agenda briefing. San Jose
- March 20 – Attended March Commission meeting day 1. Long Beach
- March 21 – Attended March Commission meeting day 2. Long Beach
- March 21 – Teleconference with Secretary Omishakin re: Commission matters. Long Beach
- March 24 – Meeting with local elected officials from Half Moon Bay, Pacifica, and Campbell re: regional transportation priorities. San Jose
- March 25 – Video conference with Darnell Grisby, Clarissa Falcon and Tanisha Taylor re: Commission matters. San Jose

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 1.15, Action

Prepared By: Justin Hall
Commission Clerk (Acting)

Published Date: June 13, 2025

Subject: Committee Member Compensation

Recommendation:

Staff recommends that the California Transportation Commission (Commission) approve compensation for Interagency Equity Advisory Committee (EAC) members and Road Charge Technical Advisory Committee (RC TAC) members as provided in Attachments A, B, and C.

Issue:

Pursuant to California Government Code Section 14509.5, members of advisory committees to the Commission are eligible for reimbursement of expenses incurred in connection with their service. Committee members shall receive a per diem of one hundred dollars (\$100) for each day spent conducting authorized committee duties during any month, when a majority of the Commission approves the compensation by a recorded vote.

The Interagency Equity Advisory Committee Charter, Article 3.7, requires the Commission approve all requests for eligible per diem requests for Committee members. Per diem compensation requests for Commission approval in Attachments A, B, and C are for April 2025, May 2025, and amendments to March 2025.

Background:

In its 2022 Annual Report to the Legislature, the Commission recommended legislation to allow it to compensate members of its advisory committees, including the newly formed Interagency Equity Advisory Committee. This recommendation was intended to recognize the value of advisory committee members' time commitment and to alleviate financial barriers for qualified and willing individuals to serve on a committee. Senate Bill 125 (Committee on Budget and Fiscal Review, Chapter 54, 2023) implemented the Commission's recommendation.

Attachments:

- Attachment A: Committee Member Compensation – April 2025
- Attachment B: Committee Member Compensation – May 2025
- Attachment C: Amended Committee Member Compensation – March 2025

MEETINGS FOR COMPENSATION

April 2025

Regular Road Charge Technical Advisory Committee Meeting

April 18 – Road Charge Technical Advisory Committee Meeting. (Committee Members Eric Sauer and Lori Wilson were absent. All other RC TAC members attended all or part of the meeting.)

Asha Agrawal

No additional Committee duties reported at this time.

Lisa Bartlett

No additional Committee duties reported at this time.

Terry Benzel

No additional Committee duties reported at this time.

Jay Bradshaw

No compensation requested.

Steve Finnegan

No additional Committee duties reported at this time.

Jim Madaffer

No additional Committee duties reported at this time.

Pam O'Connor

No additional Committee duties reported at this time.

Robert Poythress

No additional Committee duties reported at this time.

Misty Rickwalt

No additional Committee duties reported at this time.

Eric Sauer

No Committee duties reported at this time.

Stacy Atkins Salazar

No additional Committee duties reported at this time.

Lee Tien

No additional Committee duties reported at this time.

Lori Wilson

No compensation requested.

Regular EAC Meeting Activities

There was no EAC meeting

Keri Akemi-Hernandez

April 1 – Draft and submit Vision and Mission.

April 3 – EAC Member debrief meeting.

April 7 – Workplace Violence Prevention Training.

April 7 – Vision and Mission Workshop.

April 8 – Review proposed Vision, Mission and Preface to offer final recommendations.

April 9 – Review proposed Vision, Mission and Preface to offer final recommendations.

April 11 – Meeting with EAC Board Chair Martha Armas-Kelly to review final Vision, Mission, and Preface recommendations.

April 11 – Submit final recommended revision for proposed Vision, Mission and Preface.

April 15 – EAC new Member orientation.

April 21 – EAC Spotlight Presentation discussion with CTC staff.

April 24 – EAC Spotlight Presentation - work on talking points.

April 25 – EAC Spotlight Presentation - work on talking points.

April 29 – EAC Spotlight Presentation - completed presentation and submitted to staff.

Lisa Marie Alvarado

April 21 – Senate Bill 1 project application review.

Martha Armas-Kelly

April 7 – Leadership call with Interagency staff.

April 16 – Leadership call with Interagency staff.

April 24 – Attended Imperial County Town Hall day 1.

April 25 – Attended Town Hall tour in Imperial County.

April 30 – Leadership call with Interagency staff.

David Azevedo

No compensation requested.

Abimael Chavez-Hernandez

No Committee duties reported at this time.

Ana Gonzalez

April 7 – Leadership call with Interagency staff.

April 16 – Leadership call with Interagency staff.

April 30 – Leadership call with Interagency staff.

Gloria Huerta

No Committee duties reported at this time.

Naomi Iwasaki

No compensation requested.

Peggy Martinez

April 13 – Workplace Violence Prevention Training

April 17 – Met with CTC staff to discuss Senate Bill 1 application review.

April 20 – Senate Bill 1 application review.

Andres Ramirez

No Committee duties reported at this time.

Michelle Rousey

No Committee duties reported at this time.

Florence Simon

No compensation requested.

Randy Torres-Van Vleck

April 3 – Senate Bill 1 evaluation training.

April 4 – EAC Member debrief with interagency staff.

April 9 – Senate Bill 1 application review.

April 11 – Senate Bill 1 application review.

April 21 – Senate Bill 1 application review.

April 22 – Senate Bill 1 application review.

April 24 – Senate Bill 1 application review.

April 25 – Attended Town Hall tour in Imperial County.

April 26 – Senate Bill 1 application review.

April 27 – Senate Bill 1 application review.

Pualani Vazquez

No compensation requested.

Howard Wong

April 14 – Workplace Violence Prevention Training.

April 15 – EAC new Member orientation.

April 15 – STIP guidelines review.

April 23 – Senate Bill 1 application review.

April 24 – Senate Bill 1 application review.

April 25 – Senate Bill 1 application review.

April 26 – Senate Bill 1 application review.

April 27 – Senate Bill 1 application review.

April 28 – Senate Bill 1 application review.

Attachment B

MEETINGS FOR COMPENSATION

May 2025

Regular EAC Meeting

May 14 – Interagency Equity Advisory Committee Business Meeting. (Committee Members Azevedo and Huerta were absent. All other EAC members attended all or part of the meeting.)

May 14 – Interagency Equity Advisory Executive Meeting. (Committee Members Azevedo and Huerta were absent. All other EAC members attended all or part of the meeting.)

Keri Akemi-Hernandez

May 6 –Vision, Mission, Preface (Foundation Principles) check-in with Interagency staff.

May 7 –Vision, Mission, Preface (Foundation Principles): provided definitions, resources and website links.

May 15 – Caltrain Tour and EAC Member Spotlight at CTC Meeting.

Lisa Marie Alvarado

May 13 – Senate Bill 1 project applications review.

May 19 – Workplace Violence Prevention Training.

Martha Armas-Kelly

May 7 – Leadership call with Interagency staff.

May 7 –Vision and Mission Statement discussion with interagency staff.

May 15 – May CTC meeting Caltrain Tour.

May 19 – Workplace Violence Prevention Training.

May 22 – Phone call with interagency lead staff.

May 28 – Leadership call with interagency staff

David Azevedo

No compensation requested.

Abimael Chavez-Hernandez

May 5 – EAC meeting member briefing.

Ana Gonzalez

May 7 – Leadership call with interagency staff.

May 28 – Leadership call with Interagency staff

Gloria Huerta

No Committee duties reported at this time.

Naomi Iwasaki

No compensation requested.

Peggy Martinez

May 1 – Senate Bill 1 application review.
May 5 – EAC meeting agenda briefing.
May 8 – Senate Bill 1 application review.
May 9 – Senate Bill 1 application review.
May 13 – EAC meeting travel.
May 15 – May CTC meeting Caltrain tour.
May 18 – EAC meeting travel.
May 22 – Met with CTC staff to discuss June meeting presentation and logistics.
May 27 – Senate Bill 1 application review.

Andres Ramirez

May 7 – EAC meeting agenda briefing.

Michelle Rousey

No additional Committee duties reported at this time.

Florence Simon

No compensation requested.

Randy Torres-Van Vleck

May 8 – May EAC agenda briefing.

Pualani Vazquez

No compensation requested.

Howard Wong

May 5 – STIP guidelines workshop.
May 5 – EAC meeting agenda briefing.

AMENDED MEETINGS FOR COMPENSATION

MARCH 2025

Martha Armas-Kelly

March 11 – Attended EAC meeting day 1.

March 12 – Attended EAC meeting day 2.

March 25 – EAC meeting debrief with interagency staff.

March 27 – Vision and Mission Statement discussion with interagency staff.

Ana Gonzalez

March 11 – Attended EAC meeting day 1.

March 12 – Attended EAC meeting day 2.

March 24 – EAC meeting debrief with interagency staff.

EXECUTIVE DIRECTOR'S REPORT

A VERBAL PRESENTATION ON THIS ITEM
WILL BE MADE AT THE CALIFORNIA
TRANSPORTATION COMMISSION MEETING.

2026 MEETING SCHEDULE
CALIFORNIA TRANSPORTATION COMMISSION

Proposed June 26, 2025

JANUARY 29(TH) – 30(F), 2026 – SACRAMENTO REGION

FEBRUARY 2026 – NO REGULARLY SCHEDULED COMMISSION MEETING

MARCH 19(TH) – 20(F), 2026 – LOS ANGELES REGION

APRIL 2026 – NO REGULARLY SCHEDULED COMMISSION MEETING

TBD – Joint CTC/CARB/HCD Meeting — TBD

April 23(TH) & 24(F) – Town Hall Meeting – TBD

MAY 14(TH) – 15(F), 2026 – SF BAY AREA

May 13(W) – Executive Equity Advisory Committee meeting – SF BAY AREA

JUNE 25(TH) – 26(F), 2026 – SACRAMENTO REGION

JULY 2026 – NO REGULARLY SCHEDULED COMMISSION MEETING

AUGUST 20(TH) – 21(F), 2026 – SAN DIEGO REGION

SEPTEMBER 2026 – NO REGULARLY SCHEDULED COMMISSION MEETING

September 17(TH) & 18(F) – Town Hall Meeting – TBD

OCTOBER 15(TH) – 16(F), 2026 – CENTRAL VALLEY

NOVEMBER 2026 – NO REGULARLY SCHEDULED COMMISSION MEETING

TBD – Joint CTC/CARB/HCD Meeting – TBD

DECEMBER 3(TH) – 4(F), 2026 – LOS ANGELES REGION

December 2(W) – Executive Equity Advisory Committee meeting – TBD

COMMISSION REPORTS

A VERBAL PRESENTATION ON THIS ITEM
WILL BE MADE AT THE CALIFORNIA
TRANSPORTATION COMMISSION MEETING.

REPORT BY THE STATE TRANSPORTATION
AGENCY SECRETARY
AND/OR UNDERSECRETARY

A VERBAL PRESENTATION ON THIS ITEM
WILL BE MADE AT THE CALIFORNIA
TRANSPORTATION COMMISSION MEETING.

REPORT BY CALTRANS' DIRECTOR
AND/OR DEPUTY DIRECTOR

A VERBAL PRESENTATION ON THIS ITEM
WILL BE MADE AT THE CALIFORNIA
TRANSPORTATION COMMISSION MEETING.

REPORT BY UNITED STATES
DEPARTMENT OF TRANSPORTATION

A VERBAL PRESENTATION ON THIS ITEM
WILL BE MADE AT THE CALIFORNIA
TRANSPORTATION COMMISSION MEETING.

REPORT BY REGIONAL AGENCIES MODERATOR

A VERBAL PRESENTATION ON THIS ITEM
WILL BE MADE AT THE CALIFORNIA
TRANSPORTATION COMMISSION MEETING.

REPORT BY RURAL COUNTIES TASK FORCE CHAIR

A VERBAL PRESENTATION ON THIS ITEM
WILL BE MADE AT THE CALIFORNIA
TRANSPORTATION COMMISSION MEETING.

REPORT BY SELF-HELP COUNTIES COALITION
MODERATOR

A VERBAL PRESENTATION ON THIS ITEM
WILL BE MADE AT THE CALIFORNIA
TRANSPORTATION COMMISSION MEETING.

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: Tanisha Taylor, Executive Director

Reference Number: 1.13 Information

Prepared By: C. Sequoia Erasmus
Associate Deputy Director

Published Date: June 13, 2025

Subject: Interagency Equity Advisory Committee Updates

Summary:

May Committee Meetings Recap

The Interagency Equity Advisory Committee (Committee) met on May 14, 2025. A Business meeting, attended by Committee members, was held during the morning of May 14 to discuss and deliberate next steps on the utilization of subcommittees to accomplish advisory goals associated with the interagency work plan.

An Executive meeting, held during the afternoon of May 14, was attended by Committee members, California Transportation Commission (Commission) Commissioners and Executive Director, the Secretary of the California State Transportation Agency (CalSTA), and the Deputy Director of Equity and Tribal Affairs at the California Department of Transportation (Caltrans). During the Executive meeting, members and executives discussed next steps on a draft mission, vision, and foundational principles for transportation equity, discussed the 2025 Committee work plan, and received an update from the Caltrans Director's Office of Equity and Tribal Affairs regarding Caltrans' community engagement efforts.

Committee members took action during the Business meeting to approve the following three ad hoc subcommittees:

1. Community Engagement Policies and Practices
 - a. This ad hoc subcommittee will focus on community engagement policies and practices, such as the Caltrans Statewide Community Engagement Playbook.
2. Transportation Planning and Program Guidance
 - a. This ad hoc subcommittee will focus on long-range planning efforts, such as Climate Action Plan for Transportation Infrastructure (CAPTI) 2.0 implementation.
3. Data Implementation
 - a. This ad hoc subcommittee will focus on data-driven decision-making tools and practices, supporting equitable investments in transportation infrastructure and programs, such as providing advisory support in Caltrans Equity Index Use Case Development.

Ad hoc subcommittees are anticipated to meet bi-monthly in June, August and November 2025, and in February 2026. Interagency lead staff are working with ad hoc subcommittee members to finalize meeting dates and ad hoc subcommittee meeting agendas

The next full Committee meeting is scheduled to be held virtually on September 5, 2025.

Meeting materials and links to join can be found at the following website:

<https://catc.ca.gov/meetings-events/equity-advisory-committee-2025-meetings>

Committee Member Advisory Involvement in Commission Programs

Participating members have completed their review of the community engagement narratives in project applications for the Commission's Senate Bill 1 Competitive Programs' fourth funding cycle. Committee member input influences funding award recommendations in the upcoming fourth SB 1 funding cycle, which is slated to award over \$1.4 billion to transportation projects at the June 2025 Commission Meeting.

Additionally, Committee members were invited to review the 2026 Draft State Transportation Improvement Program (STIP) Guidelines and provide feedback to staff via member briefings throughout the month of May 2025. Member feedback included adding a glossary of terms into the updated guidelines, which is reflected in the updated draft.

Committee Member Update

Committee member and accessibility consultant Peggy Martinez will provide a presentation on pedestrian safety and accessibility, focusing on those with blindness, low-vision, and other vulnerable populations.

Peggy Martinez, Americans with Disabilities Act Coordinator (ADAC), has worked in disability accessibility since 1996 in access technology, as a business owner, in higher education, and in non-profit management.

In 2015, Peggy launched her second sole proprietorship, Creative Inclusion, in response to requests from designers and planners for her expertise in accessible technology, wayfinding, public transport, recreation, and entertainment. Peggy works with cross-functional teams to address diverse accessibility requests in the built environment and public realm.

Creative Inclusion combines existing legislation and best practices to develop accessibility solutions promoting safety, independence, enjoyment, and dignity, enhancing projects from concept through completion to meet client goals. Clients include architecture and landscape firms, global tech and movie companies, transit-oriented development teams, and state and local transportation agencies.

Services include:

- Accessible walkability and tactile wayfinding advisement
- Inclusive event planning and engagement training
- Disability awareness and customer service training
- Policy development Services
- Usability testing

Background:

The Committee is a priority for the Commission, Caltrans, and CalSTA. The Committee elevates diverse and historically marginalized voices to advise the three agencies on how to achieve meaningful outcomes in transportation equity, environmental justice, and equitable economic opportunities, especially as related to transportation planning and programming.

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.1, Action

Prepared By: Justin Behrens
Deputy Director

Published Date: June 13, 2025

Subject: State and Federal Legislative Matters

Recommendation:

Staff recommends the California Transportation Commission (Commission) accept this report on state and federal legislation.

Issue:

A list of bills monitored by staff pursuant to the Commission's bill monitoring policy is included as Attachment A.

The Commission previously took a support position on Senate Bill 470 (Laird): Bagley-Keene Open Meeting Act: teleconferencing. Commission staff is not recommending any new positions on bills at this time.

Background:

State Legislation

The deadline for bills to pass out of the house of origin was June 6. Policy committees in the Senate and Assembly have until June 18 to consider bills in the second house. The Legislature is scheduled to adjourn for Summer Recess on July 18 and will reconvene on August 18.

Staff continue to discuss bills that may affect the Commission with bill authors, legislative staff, and stakeholders to provide technical assistance.

State Budget

Since the Governor submitted his updated 2025-26 budget proposal on May 14, the budget committees of the Senate and Assembly have met to consider the Administration's proposals. The Legislature must pass a budget for fiscal year 2025-26 by June 15, to take effect at the beginning of the fiscal year (July 1).

Negotiations on the final budget deal are expected to continue until the aforementioned deadline; Commission staff will provide an update on further budget developments at the Commission meeting.

Federal Funding

On May 22, the House of Representatives approved a reconciliation bill pursuant to the budget framework previously approved in February. The reconciliation bill is currently pending in the Senate. If approved by the Senate and signed by the President into law, the bill would:

- Require states to collect annual registration fees of \$250 for electric vehicles and \$100 for hybrid vehicles through federal fiscal year 2035. These fees are estimated to generate total revenues of \$64 billion through 2035 for the federal Highway Trust Fund, which is currently supported by federal excise taxes on motor vehicle fuels and general revenues. The reconciliation bill does not adjust spending levels for programs funded by the Highway Trust Fund, and no estimate of California's expected share of revenues associated with the fees is available at this time.
- Rescind approximately \$3 billion for discretionary grant programs established by the Inflation Reduction Act and administered by the Federal Highway Administration, including the Neighborhood Access and Equity Grant Program, the Low Carbon Transportation Materials Program, and assistance for environmental review for transportation projects. None of the rescissions impact formula funding received by California or funding levels of programs administered by the Commission.

Commission staff continues to monitor actions at the federal level that may impact the availability of federal funds.

Attachments

- Attachment A: Legislation Monitored by Commission Staff

Legislation Monitored by Commission Staff
Active Bills (2025-26 Regular Session)
(As of June 6, 2025)

Bill	Author	Subject	Description	Status
AB 66	Tangipa	California Environmental Quality Act: exemption: egress route projects: fire safety.	Exempts from the California Environmental Quality Act (CEQA) egress route projects undertaken by a public agency to improve emergency access to and evacuation from a subdivision without a secondary egress route under specified conditions until January 1, 2032.	Pending committee referral in Senate
AB 289*	Haney	State highway work zone speed safety program.	Authorizes the California Department of Transportation (Caltrans) to establish a program for automated speed enforcement in state highway construction or maintenance areas until January 1, 2032.	Pending committee referral in Senate
AB 334*	Petrie-Norris	Operators of toll facilities: interoperability programs: vehicle information.	Authorizes operators of toll facilities on federal-aid highways engaged in an interstate interoperability program to provide only the information regarding a vehicle's use of the toll facility that is intended to implement interstate interoperability.	Pending in Senate Transportation Committee
AB 390*	Wilson	Vehicles: highway safety.	Expands the requirement that drivers approach stationary marked Caltrans vehicles with caution to apply to all marked highway maintenance vehicles, as defined.	Pending in Senate Transportation Committee
AB 431	Wilson	Advanced Air Mobility Infrastructure Act.	Requires Caltrans to develop an advanced air mobility plan that includes vertiports, electric aviation charging, and the infrastructure needs of other advances in aviation technology, as specified.	Pending committee referral in Senate
AB 891	Zbur	Transportation: Quick-Build Project Pilot Program.	Establishes the Quick-Build Project Pilot Program within Caltrans's maintenance program to expedite the development and implementation of low-cost projects on the state highway system, as specified, and requires Caltrans to identify and commit to funding a minimum of six quick-build projects by December 31, 2028.	Pending committee referral in Senate
AB 902*	Schultz	Transportation projects: barriers to wildlife movement.	Requires lead agencies to incorporate wildlife passage features into transportation infrastructure projects in connectivity areas, except for projects on the state highway system where Caltrans is the lead agency, as specified.	Pending committee referral in Senate
AB 954	Bennett	State transportation improvement program: bicycle highway pilot program.	Requires Caltrans to prepare a proposal for the development of sites for a pilot program establishing branded networks of bicycle highways that are numbered and signed within 2 of California's major metropolitan areas.	Pending committee referral in Senate

Bill	Author	Subject	Description	Status
AB 975	Gallagher	Lake and streambed alteration agreements: exemptions: culverts and bridges.	Exempts until January 1, 2027, projects to repair or reconstruct a bridge 30 feet long or less, or a culvert 70 feet long or less within the County of Sutter that has been damaged by a natural disaster in, or after, 2021 from provisions required by the Department of Fish and Wildlife.	Pending in Senate Natural Resources and Water Committee
AB 1244	Wicks	California Environmental Quality Act: transportation impact mitigation: Transit-Oriented Development Implementation Program.	Authorizes a project to satisfy CEQA mitigation requirements for transportation impacts by contributing an amount of money per vehicle mile traveled set by the Governor's Office of Land Use and Climate Innovation to the Transit-Oriented Development Implementation Fund, as specified, and authorizes the Department of Housing and Community Development to use those moneys to fund developments located in the same region as the project.	Pending committee referral in Senate
AB 1275	Elhawary	Regional housing needs: regional transportation plan.	Modifies the timeline for the Department of Housing and Community Development to update each region's existing and projected housing need, requires each council of governments to consider the development pattern set forth in the region's sustainable communities strategy of its regional transportation plan in developing the methodology for its regional housing needs allocation, and requires each region's housing needs allocation plan be informed by the applicable sustainable communities strategy, instead of being consistent with that strategy.	Pending hearing in Senate Housing Committee
SB 10	Padilla	Otay Mesa East Toll Facility Act: toll revenues.	Authorizes toll revenues generated by the Otay Mesa East Toll Facility Act to also be used to assist in the maintenance of the South Bay International Boundary and Water Commission sewage treatment facility and additional sanitation infrastructure related to the Tijuana River, consistent with applicable federal and state laws.	Pending committee referral in Assembly
SB 63*	Wiener	San Francisco Bay area: local revenue measure: transportation funding.	Establishes the Transportation Revenue Measure District in Alameda and Contra Costa Counties and the City and County of San Francisco to be governed by the Metropolitan Transportation Commission's governing board, and authorizes the district's board or a qualified voter initiative to impose a retail transaction and use tax to fund operating expenses of specified transit operators and public transportation expenses in the counties comprising the district.	Pending committee referral in Assembly

Bill	Author	Subject	Description	Status
SB 71*	Wiener	California Environmental Quality Act: exemptions: environmental leadership transit projects.	Extends existing CEQA exemptions for certain types of active transportation and transit projects indefinitely and exempts specified other types of transit projects from the requirements of CEQA until January 1, 2032, and specified types of passenger rail projects until January 1, 2040.	Pending committee referral in Assembly
SB 74	Seyarto	Office of Land Use and Climate Innovation: Infrastructure Gap-Fund Program.	Requires the Governor's Office of Land Use and Climate Innovation to establish the Infrastructure Gap-Fund Program to provide grants to local agencies for the development and construction of infrastructure projects, as specified.	Pending in Assembly Local Government Committee
SB 78	Seyarto	Department of Transportation: report: state highway system: safety enhancements.	Requires Caltrans to prepare a report to identify types of safety enhancements that could be implemented on the state highway system, as well as common factors contributing to delays in those enhancements, and strategies to expedite them.	Pending in Assembly Transportation Committee
SB 298	Caballero	State Energy Resources Conservation and Development Commission: seaports: plan: alternative fuels.	Requires the California Energy Commission, in coordination with the California State Transportation Agency (CalSTA), the California Air Resources Board, and the State Lands Commission, to develop a plan for the alternative fuel needs of oceangoing vessels at California's public seaports that would enable the seaports to meet their emissions goals.	Pending committee referral in Assembly
SB 359	Niello	Use Fuel Tax Law: Diesel Fuel Tax Law: exempt bus operation.	Applies the exemptions from the provisions of the Diesel Fuel Tax Law and the Use Fuel Tax Law to counties that own and operate a local transit system, as provided.	Pending in Assembly Revenue and Taxation Committee
SB 419*	Caballero	Hydrogen fuel.	Provides an exemption from the state taxes imposed by the Sales and Use Tax Law for hydrogen fuel.	Pending committee referral in Assembly
SB 445	Wiener	Transportation: planning: complete streets facilities: sustainable transportation projects.	Delays the existing requirement that Caltrans develop and adopt a project intake, evaluation, and encroachment review process for complete streets facilities that are sponsored by a local jurisdiction or transit agency from January 1, 2027, to February 1, 2027.	Pending committee referral in Assembly
SB 470	Laird	Bagley-Keene Open Meeting Act: teleconferencing.	Extends alternative teleconferencing provisions for the Bagley-Keene Open Meeting Act until January 1, 2030.	Pending committee referral in Assembly
SB 569	Blakespear	Department of Transportation: homeless encampments.	Requires Caltrans to establish a dedicated liaison to facilitate communication with local governments and relevant state agencies to address homeless encampments within the state highway system, as specified.	Pending committee referral in Assembly

Bill	Author	Subject	Description	Status
SB 661*	Hurtado	Airports: financial assistance.	Redirects 50% of revenues generated by state sales and use taxes on jet fuel from the General Fund to the Aeronautics Account and requires Caltrans' Division of Aeronautics to annually allocate those funds to qualifying general aviation airports and commercial airports for specified uses, and requires the remaining 50% to be retained by the airport at which the jet fuel is sold to be used only for purposes related to airport operations, capital improvements, maintenance, and other aviation-related infrastructure, as specified..	Pending committee referral in Assembly
SB 695	Cortese	Transportation: climate resiliency: projects of statewide and regional significance.	Requires Caltrans, in consultation with the Commission and CalSTA, to create and submit to the Legislature a prioritized list of projects of statewide and regional significance to better prepare the state for extreme weather-related events, as specified, by July 1, 2026 and annually thereafter.	Pending in Assembly Transportation Committee
SB 741	Blakespear	Coastal resources: coastal development permit: exemption: Los Angeles-San Diego-San Luis Obispo Rail Corridor.	Expands the exemptions from the coastal development permitting process to include certain emergency projects undertaken by a public agency to maintain, repair, or restore existing railroad tracks along the Los Angeles-San Diego-San Luis Obispo Rail Corridor, as provided.	Pending in Assembly Natural Resources Committee
SB 840	Limón	Greenhouse gases: report.	Extends indefinitely the requirement for the Legislative Analyst's Office to annually submit to the Legislature the report on the economic impacts and benefits of the State Air Resources Board's greenhouse gas emissions targets.	Pending committee referral in Assembly

Total Active Measures: 26

Note: Measures marked with “*” were amended since most recent staff report.

Legislation Monitored by Commission Staff
Inactive Bills (2025-26 Regular Session)
(As of June 6, 2025)

Bill	Author	Subject	Description
AB 52	Aguiar-Curry	Native American resources.	Modifies the processes and requirements for consultation with federally recognized tribes and non-federally recognized tribes under the Planning and Zoning Law and CEQA.
AB 612	Rogers	Transportation: Highway Design Manual: emergency response times.	Requires Caltrans to update the Highway Design Manual to direct local governments to consult with local fire departments to ensure road improvements do not negatively impact emergency response times.
AB 707	Soria	San Luis and Delta-Mendota Water Authority: B.F. Sisk Dam Raise and Reservoir Expansion Project.	Appropriates \$455.5 million from the General Fund to the San Luis and Delta-Mendota Water Authority for the purpose of funding the State Highway 152 Route improvements required to complete the B.F. Sisk Dam Raise and Reservoir Expansion Project.
AB 733	Tangipa	Agency Reports.	Requires each state agency that is required to submit one or more reports to the Legislature to submit, by April 1 of each year, a list of all reports the agency has not yet submitted to the Legislature along with a status summary and provides that the Legislature may withhold appropriations for an agency that fails to submit timely reports.
AB 939	Schultz	The Safe, Sustainable, Traffic-Reducing Transportation Bond Act of 2026.	Enacts the Safe, Sustainable, Traffic-Reducing Transportation Bond Act of 2026 which, if approved by the voters, authorizes \$20 billion in bonds to finance transit and passenger rail improvements, local streets and roads, and active transportation projects, zero-emission vehicle investments, transportation freight infrastructure improvements, and grade separations and other critical safety improvements.
AB 1058	Gonzalez, Jeff	Motor Vehicle Fuel Tax Law: suspension of tax.	Suspends the imposition of the tax on motor vehicle fuels for one year and requires the State Controller to transfer an amount equal to the amount collected under the tax in Fiscal Year 2023-24 from the General Fund.
AB 1168	Solache	Department of Transportation: transferred property: City of Lynwood.	Requires Caltrans to release and remove the deed restriction that it imposed on a parcel, previously transferred to the City of Lynwood, that requires a portion of the property to be used exclusively for public purposes for a period of 15 years from the recorded date of the deed.
AB 1268	Macedo	Motor Vehicle Fuel Tax Law: adjustment suspension.	Authorizes the Governor to suspend an inflationary adjustment to the motor vehicle fuel tax upon deciding that the adjustment would impose an undue burden on low-income and middle-class families, as provided.
AB 1343	Gallagher	Local alternative transportation improvement program: Feather River crossing.	Authorizes affected local agencies with canceled planned state transportation facilities over the Feather River in Sutter and Yuba Counties, to develop and file with the Commission a local alternative transportation improvement program in the area that was to be served by the planned facilities.

Bill	Author	Subject	Description
AB 1399	Hoover	Department of Transportation: encroachment permits: broadband facilities.	Requires Caltrans to perform additional actions for an encroachment permit application for a broadband facility, as specified.
AB 1421	Wilson	Vehicles: Road Usage Charge Technical Advisory Committee.	Extends the operation of the California Transportation Commission's Road Usage Charge Technical Advisory Committee until January 1, 2035.
AB 1491	Ta	Transportation: road safety.	States the intent of the Legislature to enact legislation to improve safety on the roads in the state.
ACA 12	Wallis	Road usage charges: vote and voter approval requirements.	Provides that the imposition of a road usage charge to be subject to a 2/3 vote requirement for approval. Requires that any road usage charge be imposed at a uniform rate, and not vary based on any factor. Prohibits the Legislature from imposing both a road usage charge, and either a motor vehicle fuel tax or a tax on the sale, storage, or use of motor vehicle fuels used in vehicles subject to a road usage charge.
SB 73	Cervantes, Sabrina	California Environmental Quality Act: exemptions.	Exempts those projects located in a very low vehicle travel area from CEQA requirements, as defined.
SB 752	Richardson	Sales and use taxes: exemptions: California Hybrid and Zero-Emission Truck and Bus Voucher Incentive Project: transit buses.	Extends the exemption for specified zero-emission technology transit buses from existing sales and use taxes until January 1, 2028.

Total Inactive Measures: 15

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 4.2, Information Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **BUDGET AND ALLOCATION CAPACITY UPDATE**

SUMMARY:

Outlined below is an update for the California Transportation Commission (Commission) concerning transportation funding in the State of California. This information is intended to supplement portions of the verbal presentation on this item.

BACKGROUND:

As of May 31, 2025, the Commission has allocated approximately \$5.8 billion toward 738 projects in Fiscal Year 2024-25. Adjustments totaled approximately negative \$157 million, leaving approximately \$6.6 billion (54 percent) in remaining allocation capacity.

2024-25 Capital Allocations vs. Capacity Summary through May 31, 2025 (\$ in millions)										
	SHOPP	STIP	AERO	LPP	SCCP	TCEP	ATP	TIRCP	BONDS	TOTAL
Allocation Capacity	\$5,392	\$811	\$4	\$401	\$390	\$877	\$1,157	\$3,166	\$66	\$12,263
Total Votes	4,374	259	3	66	0	353	219	500	8	5,782
Authorized Changes ¹	-157	-1	0	0	0	0	0	0	0	-157
Remaining Capacity	\$1,174	\$553	\$1	\$335	\$390	\$524	\$938	\$2,666	\$58	\$6,639

Notes: Amounts may not sum to totals due to independent rounding.

¹ Authorized changes include project increases and decreases through April 30, 2025, pursuant to the Commission's G-12 process and project rescission.

² Total Votes include \$199,366,000 in Supplemental allocations through May 31, 2025.

Through May 31, 2025, the California Department of Transportation has processed changes to capital construction budgets for both the State Transportation Improvement Program (STIP) and the State Highway Operation and Protection Program (SHOPP). The STIP experienced a net decrease of approximately \$612,600 of the programmed amount. This is the result of a net increase of approximately \$297,000 to one project with a total value of \$1 million and a net decrease of approximately \$909,600 to five projects with a total value of \$6 million. The SHOPP experienced a net decrease of approximately \$157 million of the programmed amount. This is the result of a net increase of approximately \$98 million to 84 projects with a total value of \$1.6 billion and a net decrease of approximately \$255 million to 143 projects with a total value of \$2.4 billion. The STIP and SHOPP experienced a net decrease of approximately \$157 million of the programmed amounts. This is the result of a net increase of approximately \$98 million to 85 projects with a total value of \$1.6 billion and a net decrease of approximately \$255 million to 148 projects with a total value of \$2.4 billion. In addition, the Commission has approved supplemental allocations totaling approximately \$199 million through May 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 4.3, Information Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **DRAFT 2026 STATE TRANSPORTATION IMPROVEMENT PROGRAM AND
AERONAUTICS ACCOUNT FUND ESTIMATES**

SUMMARY:

The California Department of Transportation (Department) recommends that the California Transportation Commission (Commission) accept the Draft 2026 State Transportation Improvement Program (STIP) Fund Estimate and the Draft 2026 Aeronautics Account Fund Estimate tables that will be provided at the Commission meeting on June 26-27, 2025. The Department will work with Commission staff to implement necessary changes prior to the scheduled adoption of the Fund Estimates at the Commission's August 2025 meeting. The 2026 STIP Fund Estimate and Guidelines Workshop will be held in July 2025. This will provide an opportunity for Commissioners, Commission staff, and other stakeholders to comment and suggest additional improvements to the Fund Estimates.

BACKGROUND:

Section 14524(a) of the Government Code (GC) requires the Department to submit the STIP Fund Estimate prior to July 15 of each odd-numbered year. The Department will meet this requirement by submitting the Draft 2026 STIP Fund Estimate at the June 2025 Commission meeting. Section 14525(a) of the GC requires the Commission to adopt the STIP Fund Estimate by August 15 of each odd-numbered year. The Commission may elect to delay adoption for up to 90 days after August 15 should it find that forthcoming legislation could significantly impact the Fund Estimate.

The Draft 2026 Fund Estimate program capacities are based on the assumptions approved by the Commission at its May 2025 meeting, and incorporates updated revenue projections included as part of the May Revision to the State's proposed budget released on May 14, 2025. After reviewing the Draft 2026 Fund Estimate, the Commission may choose to revisit the approved assumptions for possible adjustments or updates. The Department

requests that the Commission direct comments to Commission staff so the Department may incorporate feedback into the Fund Estimates.

The Department will continue to work with Commission staff between now and the August 2025 Commission meeting to update information and make any necessary changes to the 2026 Fund Estimates. In the event that budgetary action requires amendment of the assumptions prior to the scheduled August adoption, the Department will immediately inform Commission staff.

Attachment

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.23, Information

Prepared By: Rebecca Light,
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Subject: Draft 2026 State Transportation Improvement Program Guidelines

Summary:

The State Transportation Improvement Program (STIP) is a formulaic program that funds a wide variety of regional and interregional capital projects that improve local roads, support active transportation, and help facilitate the movement of people and goods by improving the state highway system, intercity rail, and transit. The STIP guidelines describe the policy, standards, criteria, and procedures for the development, adoption, and management of the program. The draft 2026 STIP guidelines continue emphasizing partnerships between the California Department of Transportation (Caltrans) and regional agencies in making investment decisions addressing the most critical needs. The draft 2026 STIP guidelines also continue the changes made in the 2024 STIP guidelines, which incorporated the Climate Action Plan for Transportation Infrastructure; added a new community engagement section; and required complete streets elements be incorporated on local streets and highways, where feasible.

Proposed changes to the 2026 STIP Guidelines, which consist primarily of revisions for clarity and consistency, were developed through engagement with Commission stakeholders and the Interagency Equity Advisory Committee (EAC). Commission staff held a workshop with stakeholders to discuss proposed changes on May 5, 2025. One member of the EAC assisted in the review of the guidelines and provided useful feedback on how the guidelines could be more friendly to the public. Per the EAC member's suggestion, a glossary was included with the guidelines to help with clarity.

These changes include the following, as shown in Attachment B:

- **Section 17—Committed/Uncommitted Funds**—This section has been modified to encompass committed and uncommitted funding. Uncommitted funds are identified as those from specific commission programs, which include the Active Transportation Program, and federal discretionary programs. Failure to secure the uncommitted funding will result in the project's deletion from the STIP.

- Section 48—Project Description, Subsection J—has been modified to provide regional flexibility. The corresponding page number in the Regional Transportation Plan may instead identify projects without an identification number.
- Section 50—Fact Sheets, Subsection F—has been added to stipulate that the agency's logo must be included.
- Appendix D—Glossary—Has been included as a reference for acronyms used throughout the guidelines.

After holding a Fund Estimate and Guidelines workshop in July 2025, staff will request the Commission approve the final 2026 STIP Guidelines in August 2025. Between now and adoption, staff will monitor enacted state and federal legislation that may affect the STIP and will incorporate any required changes.

Background:

The STIP is a biennial five-year program adopted by the Commission for state highway improvements, intercity rail, and regional highway and transit improvements. Each new STIP adds two new years to prior programming commitments. The 2024 STIP was adopted in March 2024, and the next STIP must be adopted by April 1, 2026. Under state law, the Commission must adopt the STIP by April 1 of every even-numbered year and may allocate STIP funds only in accordance with the adopted STIP.

Updated guidelines for the STIP are adopted biennially along with the Fund Estimate, which is generally adopted in August of every odd-numbered year. The guidelines are developed in cooperation with Caltrans, regional transportation planning agencies, county transportation commissions, and local agencies in accordance with Government Code Section 14530.1.

The 2026 STIP will cover the five years from 2026-27 through 2030-31. Where applicable, the included projects must be consistent with adopted Regional Transportation Plans, including Sustainable Communities Strategies and the Interregional Transportation Strategic Plan.

Attachments:

- Attachment A: Policies and Procedures Specific to the 2026 State Transportation Improvement Program
- Attachment B: Draft 2026 State Transportation Improvement Program Guidelines

**DRAFT 2026 STATE TRANSPORTATION IMPROVEMENT
PROGRAM GUIDELINES**

INFORMATION ON THIS ITEM WILL BE PROVIDED PRIOR TO
THE JUNE 26-27, 2025 CALIFORNIA TRANSPORTATION
COMMISSION MEETING

2026 PERMANENT STIP GUIDELINES

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I. INTRODUCTION

1. Purpose and Authority

These guidelines describe the policy, standards, criteria, and procedures for the development, adoption, and management of the state transportation improvement program (STIP). They were developed and adopted in cooperation with Caltrans, regional transportation planning agencies, county transportation commissions, and local agencies in accordance with Government Code Section 14530.1 with the following basic objectives:

- Develop and manage the STIP as a resource management document.
- Facilitate transportation decision-making by those who are closest to the transportation problems.
- Recognize that although Caltrans is the owner-operator of the State highway system, the regional agencies have the lead responsibility for resolving urban congestion problems, including those on state highways.
- Provide incentives for regional accountability for the timely use of funds.
- Facilitate the California Transportation Commission and Caltrans's role as guardians of State capital dollars, responsible for determining how best to manage those dollars wisely and cost-effectively.
- Facilitate cooperative programming and funding ventures between regions and between Caltrans and regions.
- Make progress towards regional and statewide goals and objectives in improving the state's multi-modal transportation system.
- Emphasize partnerships between Caltrans and regional agencies in making investment decisions addressing the most critical corridor needs, regardless of mode choice or system condition.
- Mitigate negative environmental and community impacts.
- Align with the State's climate and equity goals, including the Climate Action Plan for Transportation Infrastructure (CAPTI).

The Commission intends to carry out these objectives through its guidelines, stressing accountability, flexibility, and simplicity.

2. Biennial Fund Estimate

By July 15 of each odd-numbered year, Caltrans shall submit a proposed fund estimate for the following five-year STIP period to the Commission. The Commission shall adopt the fund estimate by August 15 of that same year. The assumptions on which the fund estimate is based shall be determined by the Commission in consultation with Caltrans, regional agencies, and county transportation commissions.

3. STIP Adoption

Before April 1 of each even-numbered year, the Commission shall adopt a five-year STIP and submit it to the legislature and the Governor. The STIP shall be a

statement of the Commission's intent for allocation and expenditure of funds for the following five years and a resource management document to assist in the cost-effective planning and utilization of transportation resources. The STIP shall be developed in a manner consistent with the fund estimate, and the total amount programmed in each fiscal year of the STIP shall not exceed the amount specified in the fund estimate. The adopted STIP shall remain in effect until a new STIP is adopted for the next two-year STIP cycle.

4. Amendments to STIP Guidelines

The Commission may amend the adopted STIP guidelines after first giving notice of the proposed amendment and conducting at least one public hearing. The guidelines may not be amended or modified during the period between thirty days following the adoption of the fund estimate and the adoption of the STIP.

5. Federal TIPs and Federal STIP

These guidelines apply only to the transportation programming requirements specified in state statutes. They do not apply to transportation programming requirements specified in federal statutes. Generally, all projects receiving federal transportation funds must be programmed in a federal transportation improvement program (TIP) (for projects in urbanized regions) and the federal State Transportation Improvement Program (FSTIP) for projects outside of Metropolitan Planning Organization boundaries. Metropolitan Planning Organizations are responsible for developing and adopting federal TIPs, and Caltrans is responsible for preparing the federal STIP. Federal TIPs and STIP requirements are specified in federal statutes (Title 23 USC) and regulations (23 CFR part 450).

II. STIP CONTENTS

6. General

The STIP is a biennial document adopted no later than April 1 of each even-numbered year. Each STIP will cover a five-year period and add two new years of programming capacity. Each new STIP will include projects carried forward from the previous STIP, plus new projects and reserves from among those proposed by regional agencies in their regional transportation improvement programs (RTIPs) and by Caltrans in its interregional transportation improvement program (ITIP). State highway project costs in the STIP will include all Caltrans project support costs and all project listings will specify costs for each of the following four components: (1) completion of all permits and environmental studies; (2) preparation of plans, specifications, and estimates; (3) right-of-way acquisition; and (4) construction and construction management and engineering, including surveys and inspection. (See Sections 52 and 57 of these guidelines for guidance on displaying project components and their costs.)

7. County and Interregional Shares

The STIP consists of two broad programs: the regional program, which is funded by 75% of new STIP funding, and the interregional program, which is funded by 25% of new STIP funding. The 75% regional program is further subdivided by formula into county shares. County shares are available solely for projects nominated by regions in their RTIPs. The Caltrans ITIP will nominate only projects for the interregional program. Under restricted circumstances, an RTIP may also recommend a project for funding from the interregional share (see Section 37 of these guidelines).

The 1998 STIP period constituted a single county share period ending 2003-04; later county share periods are discrete 4-year periods, ending 2026-27, 2028-29, etc. Both surpluses and deficits of county and interregional shares carry forward from one period to the next. The Commission will program each new project from a county or interregional share, including Caltrans support costs. (See Sections 58-64 of these guidelines for counting cost changes after initial programming.)

8. Joint Funding from Regional and Interregional Shares

If Caltrans and a regional agency agree, they may recommend that a new project or a project cost increase be jointly funded from county and interregional shares. In that case, the region will nominate the county share in the RTIP, and Caltrans will nominate the interregional share in the ITIP.

9. Prior Year Projects

The STIP shall include projects from the prior STIP that are expected to be advertised before July 1 of the year of adoption, but for which the Commission has not yet allocated funds.

10. 1996 STIP Projects

All 1996 STIP project costs will be funded off the top before the division of new funds between the regional and interregional programs. This grandfathered funding will include Caltrans support costs, and the project cost display for 1996 STIP projects will conform to the same standards used for new STIP projects. Any cost changes to construction or right-of-way capital costs for 1996 STIP projects will be drawn from or credited to county and interregional shares as if they were cost changes to new STIP projects. Caltrans support costs for 1996 STIP projects will be drawn from county and interregional shares only to the extent that they are attributable to a change in project scope since the 1996 STIP. All cost changes will be drawn from or credited to the appropriate regional share. In the ITIP, Caltrans shall report on the budgets for all ongoing grandfathered 1996 STIP projects. This reporting shall include a comparison of actual expenditures to project budgets as reported in the 2012 ITIP.

11. Multi-Modal Corridor

A corridor is defined as a largely linear geographic area that includes various modes of transportation that facilitate the multi-modal movement of people and goods, support the economy, and connect communities, including cross-mode connections. A multi-modal corridor is generally defined by existing and forecasted travel patterns serving a particular travel market or markets affected by similar transportation needs and mobility issues. Origins and destinations, land use, place types, and existing and future development surrounding the transportation infrastructure also influence how the multi-modal corridor is defined. Pursuant to state and federal law¹ and statewide guidance², Caltrans and regional transportation agencies prepare corridor plans to identify multi-modal transportation projects that meet state, regional, and local goals and benefit corridors around the state.

12. Transportation Management System Improvements

The Commission supports implementing and applying transportation management system (TMS) improvements to address highway congestion and manage transportation systems. Under current statutes, Caltrans is the owner-operator of the state highway system and is responsible for the overall management of the state highway system. The regional transportation agencies are responsible for planning and programming transportation strategies, facilities, and improvements that address regional transportation issues and systemwide congestion. The Commission encourages the regions and Caltrans to work together to plan, program, implement, operate, and manage transportation facilities as an integrated system with the objective of meeting state and regional goals while maximizing available transportation resources and overall transportation system performance.

Considering this objective and the respective responsibilities of Caltrans and the regional agencies, it is the Commission's policy that TMS improvements for state highways may be programmed in the State Highway Operations and Protection Program (SHOPP) by Caltrans in consultation with regional agencies if such improvements are part of a region's adopted strategy for addressing system wide congestion. The regions are encouraged to program TMS improvements in their RTIP for STIP programming if timely programming through the SHOPP is not possible because of funding limitations. TMS improvements include, but are not limited to, the following types of projects:

- Transportation Management Centers (TMC), including necessary computer software and hardware.

¹ California Government Code Section 65086, California Streets and Highways Code Sections 2390 – 2397, Title 23 United States Code Section 135, Title 23 Code of Federal Regulations Part 450.320(c).

² 2018 Comprehensive Multimodal Corridor Plan Guidelines: <https://catc.ca.gov/programs/sb1/solutions-for-congested-corridors-program/comprehensive-multimodal-corridor-plan-guidelines>, and Caltrans Corridor Planning Guidebook: <https://dot.ca.gov/programs/transportation-planning/multi-modal-system-planning/system-planning>

- TMC interconnect projects, which allow a TMC to substitute for another TMC during an emergency.
- TMC field elements, such as, but not limited to, traffic sensors, message signs, cameras, and ramp meters, which upgrade the existing facilities and are necessary to facilitate the operation of the TMC.

To maximize the TMS benefits, TMS improvements should be coordinated with other operational improvements, such as freeway ramp/local street access modifications and auxiliary lanes. Before programming a new highway facility for construction, reconstruction, or rehabilitation in the STIP or the SHOPP, regions and Caltrans should fully consider transportation system management plans and needs and include any necessary TMC field elements to support the operation of existing or planned TMCs.

13. Capacity Increasing Highway Operational Improvements

State highway operational improvements, which expand the system's design capacity, are not eligible for SHOPP funds. To the extent such projects address regional issues, the regional agency is responsible for nominating them for STIP programming through the RTIP process. To the extent such projects address interregional issues, Caltrans is responsible for nominating them for STIP programming through the ITIP process. Examples of such projects include:

- 1) High Occupancy Vehicle (HOV) lanes and HOV interchanges.
- 2) Interchange design modifications and upgrades to accommodate traffic volumes significantly larger than the existing facility was designed for.
- 3) Truck or slow vehicle lanes on freeways with six or more mixed flow lanes.

14. Non-Capacity Increasing Highway Operational Improvements

State highway operational improvements that do not expand the system's design capacity, are intended to address spot congestion, and are not directly related to TMCs or TMC field elements, are eligible for the SHOPP. Regions may nominate these types of projects for STIP programming through the RTIP process if timely implementation through the SHOPP is not possible. Examples of such projects include:

- Auxiliary lanes for merging or weaving between adjacent interchanges
- Intersection modifications, including traffic signals
- Slow vehicle lanes on conventional highways and four-lane freeways
- Curve and vertical alignment corrections
- Two-way left turn lanes
- Channelization
- Turnouts
- Chain control and truck brake inspection sites
- Shoulder widening

III. STIP REQUIREMENTS FOR ALL PROJECTS

15. Project Study Reports

A project study report (PSR) or a PSR equivalent is required for all new projects proposed to be added to the STIP. A PSR or PSR equivalent will, at a minimum, define and justify the project scope, cost, and schedule to the satisfaction of the regional agency and Caltrans. Though a PSR or PSR equivalent may focus on the project components proposed for programming, it must provide at least a preliminary estimate of costs for all components. This requirement does not apply to planning, programming, and monitoring projects.

A PSR and PSR equivalent must follow the requirements and standards outlined in the Commission's PSR guidelines.

A registered engineer must prepare a PSR and PSR equivalent and contain the proper approvals, including the approval of the Executive Director, Division Chief, or District Director of the nominating and implementing agencies.

The following is required for PSR equivalents:

- In a rail project where the private railroad is the implementing agency, the signature of the private railroad is not required.
- In a transit or rail procurement project, the Commission's Uniform Transit Application (UTA) may replace a PSR equivalent. All PSR and PSR equivalent requirements and standards apply to the UTA.

The final RTIP or ITIP must include a hard copy or a link to the PSR, PSR equivalent, or UTA.

16. Programming Project Components Sequentially

Project components may be programmed sequentially.

- A project may be programmed for the environmental phase without being programmed for the design phase.
- A project may be programmed for the design phase without being programmed for the right-of-way or construction phase.
- A project may be programmed for the right-of-way phase without being programmed for the construction phase.

The Commission recognizes a particular benefit in programming projects for environmental work only, since project costs and scheduling often cannot be determined with meaningful accuracy until environmental studies have been completed. Prematurely programming post-environmental components can needlessly tie up STIP programming resources while other transportation needs go unmet.

When proposing to program only preconstruction components to a project, Caltrans and the regional agency should demonstrate how they intend to fund the construction of a useable segment, consistent with the regional transportation plan or the Caltrans Interregional Transportation Strategic Plan.

17. Committed and Uncommitted Funds

Committed Funds

The Commission will not program a project or phase of a project in the STIP without a full funding commitment from STIP funds or other committed funds. The Commission considers funds committed when programmed by the Commission, or when the agency with discretionary authority over the funds has committed to the project by ordinance or resolution.

For federal formula funds, including the Surface Transportation Block Grant Program, Highway Improvement Program, Congestion Mitigation and Air Quality, and federal formula transit funds, the commitment may be made through the federal Transportation Improvement Program adoption. For federal discretionary funds, the commitment may be made through the federal approval of a full funding grant agreement or by grant approval.

For projects where the agency is seeking federal discretionary funds such as New Starts or Small Starts for construction, the commitment may take the form of federal acceptance into Accelerated Project Delivery and Development (in the case of Small Starts) with the expectation of federal approval of an Expedited Grant Agreement, or federal approval of a project to enter Engineering (in the case of New Starts) with the expectation of federal approval of a Full Funding Grant Agreement, as long as all funding, excluding STIP funding, is committed to the project. A project programmed before receiving federal approval for construction must receive the federal approval for construction before construction allocation and no later than the end of the first full federal fiscal year after the STIP or STIP amendment is adopted, or the project will be deleted from the STIP.

When proposing to program only preconstruction components for a project, Caltrans or the regional agency should demonstrate how it intends to fund the construction of a useable segment, consistent with the regional transportation plan or the Caltrans interregional transportation strategic plan.

All proposed projects shall submit complete funding plans describing each overall project and/or usable project segment. Each plan shall list Federal, State, and local funding categories by fiscal year over the funding timeframe sought. Rail and transit projects must include how they intend to fund initial operating costs. Moreover, should the project schedule exceed the funding horizon, the amount needed beyond what is currently requested shall be indicated. This information may be incorporated in the project fact sheets (see Section 50 of these guidelines).

Uncommitted Funds

The Commission will program projects with uncommitted funds only from the Solutions for Congested Corridors Program, Trade Corridors Enhancement Program, Local Partnership Program, Local Transportation Climate Adaptation Program, Active Transportation Program, and federal discretionary programs. If the funding commitment is not secured with the adoption of the following programming cycle that covers the applicable programming year for these programs, and alternative funding is not identified within six months, a STIP amendment will be required to delete the projects or substitute the projects for projects that have a full funding plan commitment.

18. Alternative Delivery Methods

Projects using alternative delivery methods, such as design-build, design-sequencing procurement, or the Construction Manager/General Contractor (CMGC) delivery method, should be identified at the time of programming or as soon as possible before allocation. See Section 71 of these Guidelines.

19. Completion of Environmental Process

The Commission may program funding for right-of-way or construction only if it finds that the sponsoring agency will complete the environmental process and can proceed with right-of-way acquisition or construction within the five-year period of the STIP. In compliance with Sections 21102 and 21150 of the Public Resources Code, the Commission will not allocate funds to state or local agencies for design, right-of-way, or construction before environmental clearance documentation under the California Environmental Quality Act. As a matter of policy, the Commission will not allocate funds for design, right-of-way, or construction of a federally funded project before documentation of environmental clearance under the National Environmental Policy Act. Exceptions to this policy may be made when federal law allows for the right-of-way acquisition before completion of the National Environmental Policy Act review.

20. Caltrans/Regional Consultations

Caltrans and regional agencies shall consult in developing the ITIP and the RTIPs. As a part of this consultation, Caltrans will advise regional agencies, as far in advance as possible, of projects that may be included in the ITIP, including the potential for joint funding from county and interregional shares. Caltrans will seek the advice of the regional agencies regarding these projects. Caltrans will also advise the appropriate regional agencies, as far in advance as possible, of any schedule and cost changes for Caltrans-implemented projects funded from regional shares in the STIP.

The consultation should allow regional agencies to consider and advise Caltrans regarding the potential impact of the ITIP on the programming of projects in the RTIP. The Commission encourages Caltrans to assist the regional agencies that are responsible for preparing a Federal TIP by identifying projects that may be

included in the ITIP, recognizing that Federal regulations generally require that a project in a county with an urbanized area be included in the Federal TIP to qualify for Federal funding.

As part of this consultation, each regional agency should seek and consider the advice of Caltrans regarding potential regional program funding for State highway and intercity rail projects and should advise Caltrans, as far in advance as possible, of staff recommendations or other indications of projects that may be or are likely to be included in the RTIP. The consultation should allow Caltrans to consider and advise the regional agency regarding the potential impact of the RTIP on the programming of projects in the ITIP. Where the regional agency prepares a Federal TIP, the consultation should provide for the timely inclusion of State highway projects in the Federal TIP.

Nothing in this section is meant to require that Caltrans or a regional agency make final commitments regarding including particular projects in the ITIP or RTIP before the December 15 submission deadline.

21. Minor Projects

A STIP project does not have a minimum size. The minor reserve in the Caltrans State Highway Operation and Protection Program (SHOPP) is for SHOPP projects only. The Commission will not allocate funds from the SHOPP minor program for STIP projects' enhancements, mitigation, or cost increases.

22. Criteria for Measuring Performance and Cost-Effectiveness

Regions and Caltrans are responsible for developing goals, objectives, and priorities, considering the transportation system's overall performance consistent with federal and state planning requirements. These goals and objectives are incorporated in the region's regional transportation plan (RTP), reflected in the region's RTIP, and similarly in Caltrans' Interregional Transportation Strategic Plan (ITSP) and ITIP. To maximize the state's investments in transportation infrastructure, the Commission's policy is that each RTIP and the ITIP will be evaluated for performance and cost-effectiveness at the regional level and, where applicable, at the project level.

The Commission will evaluate each RTIP and the ITIP based on the following:

- A. A performance evaluation at the regional level and how each RTIP furthers the goals of the region's RTP, and if applicable, its Sustainable Communities Strategies (SCS), and for Caltrans, how the ITIP furthers the objectives of the ITSP.
- B. An evaluation of the cost-effectiveness of the RTIP at the regional level or the ITIP at the statewide level.
- C. Project-specific data on proposed changes to the built environment.

- D. A project-specific benefit evaluation to estimate the project's benefit to the regional system from changes to the built environment is required for:
- a. Projects with a total cost of \$50 million or greater, or
 - b. STIP programming for right-of-way and/or construction of \$15 million or more.

Consistent with Executive Order B-30-15, the project-specific benefit evaluation must include a full life-cycle cost evaluation and consider climate change impacts.

The Commission will consider the evaluations submitted by regions when making decisions on RTIPs as described in Section 65 of these guidelines. The Commission will consider the evaluations submitted by Caltrans when making decisions on the ITIP as described in Section 67 of these guidelines.

The Commission expects these evaluations to be on a life-cycle basis (full cost through the project's life, including maintenance and operation).

A. Regional Level Performance Evaluation

Caltrans and each region that is a Metropolitan Planning Organization (MPO) or within an MPO shall include an evaluation of overall (RTP or California Transportation Plan (CTP)/ITSP level) performance using, as a baseline, the region's or state's existing monitored data (e.g., vehicle miles traveled (VMT), greenhouse gases (GHG), housing and jobs, pavement condition, etc.). To the extent relevant data and tools are available, the following performance measures may be reported:

- Vehicle Miles Traveled (VMT) per capita
- Percent of congested VMT (at or below 35 mph)
- Commute mode share (travel to work or school)
- Percent of distressed state highway lane-miles
- Pavement Condition Index (local streets and roads)
- Percent of highway bridge lane miles in need of replacement or rehabilitation (sufficiency rating of 80 or below)
- Percent of transit assets that have surpassed the Federal Transit Administration (FTA) useful life period
- Highway Buffer Index (the extra time cushion that most travelers add to their average travel time when planning trips to ensure on-time arrival)
- Fatalities and serious injuries per capita
- Fatalities and serious injuries per VMT
- Percent of housing and jobs within 0.5 miles of transit stops with frequent transit service
- Mean commute travel time (to work or school)
- Change in acres of agricultural land
- CO2 emissions reduction per capita
- Accessibility and on-time performance for rail and transit
- Farebox recovery ratio

Regions outside an MPO shall include any of the above measures that the region currently monitors. A region outside an MPO (or a small MPO) may request, and Caltrans shall provide, data on these measures relative to the state transportation system in that region.

Alternatively, a region outside an MPO (or a small MPO) may use the Performance Monitoring Indicators identified in the Rural Counties Task Force's Rural and Small Urban Transportation Planning study dated June 3, 2015. These include:

- Total Accident Cost
- Total Transit Operating Cost per Revenue Mile
- Total Distressed Lane Miles, and
- Land Use Efficiency (total developed land in acres per population)

The evaluation of overall performance shall include a qualitative or quantitative assessment of how effective the RTIP or the ITIP is in addressing or achieving the goals, objectives, and standards that correspond to the relevant horizon years within the region's RTP or Caltrans' ITSP that covers the 5-year STIP period. Caltrans' evaluation of the ITIP shall also address ITIP consistency with the RTPs.

In addition, each region with an adopted SCS shall include a discussion of how the RTIP relates to its SCS. The discussion shall consist of a quantitative or qualitative assessment of how the RTIP will facilitate implementation of the SCS and identify any challenges the region faces in implementing its SCS. The report shall address the portion of the SCS relevant to that region in a region served by a multi-county transportation planning organization. As part of this discussion, each region shall identify any proposed or current STIP projects exempt from Senate Bill (SB) 375.

B. Evaluation of the cost-effectiveness of the RTIP or ITIP

If appropriate and to the extent relevant data and tools are available, Regions shall use the performance measures outlined above to evaluate the cost-effectiveness of projects proposed in the STIP on a regional level. Caltrans shall do so at the statewide level.

C. Project-level outputs

For each new project proposed, the region or Caltrans shall provide data on the proposed changes to the built environment, including but not limited to the items listed below. Such data shall be included in the electronic Project Programming Request (ePPR) (See Appendix A).

For state highway projects:

- New general-purpose lane-miles
- New HOV/High-Occupancy Toll (HOT) lane-miles
- Lane-miles rehabilitated
- New or upgraded bicycle/pedestrian lane/sidewalk miles

- Operation improvements
- New or reconstructed interchanges
- New or reconstructed bridges

For intercity rail and rail/transit projects:

- Additional transit miles or vehicles
- Miles of new track
- Rail crossing improvements
- Station improvements

For local street and road projects:

- New lane-miles
- Lane-miles rehabilitated
- New or upgraded bicycle/pedestrian lane/sidewalk miles
- Operation improvements
- New or reconstructed bridges

D. A project-level benefit evaluation shall be submitted for projects for which construction is proposed, if:

- The total amount of existing and proposed STIP for right-of-way and/or construction of the project is \$15 million or greater, or
- The total project cost is \$50 million or greater.

The project-level benefit evaluation shall address the specific benefits of the proposed project using as many of the following measures as are relevant:

- Change in Vehicle Miles Traveled (VMT) per capita
- Change in percent of congested VMT (at or below 35 mph)
- Change in commute mode share (travel to work or school)
- Change in percent of distressed state highway lane-miles
- Change in Pavement Condition Index (local streets and roads)
- Change in percent of highway bridge lane-miles in need of replacement or rehabilitation (Sufficiency Rating of 80 or below)
- Change in percent of transit assets that have surpassed the FTA useful life period
- Change in highway Buffer Index (the extra time cushion that most travelers add to their average travel time when planning trips to ensure on-time arrival)
- Change in fatalities and serious injuries per capita
- Change in fatalities and serious injuries per VMT
- Change in percent of housing and jobs within 0.5 miles of transit stops with frequent transit service
- Change in mean commute travel time (to work or school)
- Change in acres of agricultural land
- Change in CO2 emissions reduction per capita

- Changes in accessibility and on-time performance
- Change in farebox recovery ratio

The project-level benefit evaluation shall include a Caltrans-generated benefit/cost estimate, including life-cycle costs for projects proposed in the ITIP. For the RTIP, the regions may choose between the Caltrans estimate and their own estimate (explain why the Caltrans estimate was not used). The project-level evaluation must explain how the project is consistent with Executive Order B-30-15, including describing any actions taken to protect the state's most vulnerable populations. The evaluation shall be conducted by each region and by Caltrans before the RTIPs and the ITIP are submitted to the Commission for incorporation into the STIP.

23. Community Engagement

The Commission requires the development of each RTIP and the ITIP to be informed by a robust community engagement process. All agencies shall include documentation of the community engagement activities undertaken to develop the program of projects proposed in each RTIP and the ITIP. This discussion can consist of community engagement conducted during the Regional Transportation Plan or California Transportation Plan development, community engagement conducted during individual project planning, or community engagement conducted for the RTIP and ITIP. Agencies should describe how community feedback was acknowledged and incorporated into each RTIP and the ITIP. This description should demonstrate the linkage between the engagement and the scope of the proposed projects. The documentation should include a summary of the benefits the RTIP or ITIP will have on the community and any potential negative community impacts. If negative community impacts have been identified, describe how these impacts are being mitigated and how the mitigation strategy was developed in coordination with the impacted community.

IV. REGIONAL IMPROVEMENT PROGRAM

24. Submittal of RTIPs

After consulting with Caltrans, each regional agency shall adopt and submit its RTIP to the Commission and Caltrans no later than December 15 of each odd-numbered year. The RTIP will include and separately identify:

- (a) Programming proposals from the county share(s), consistent with the STIP fund estimate and Section 27 of these guidelines. These proposals may include new projects and changes to existing STIP projects within the 5-year STIP period.
- (b) Programming proposals from the county Advance Project Development Element (APDE) share, if identified in the fund estimate, which is treated as an advance of future share (see Sections 42-47).
- (c) Any request to advance a future county share for a larger project.
- (d) Any project recommendations for the interregional share.

- (e) A discussion of the proposed project's impact on other projects planned or underway within the corridor.
- (f) Information on STIP projects (in the RTIP) completed since the last RTIP submittal (see section 78).
- (g) Discuss what regions believe are the most significant interregional highway and intercity rail needs within the region (see section 39), as well as any state routes within the region that might be potential candidates for highways to boulevards. (See section 39).
- (h) A discussion describing how community engagement was performed and the benefits the project will achieve once implemented. The discussion should include potential negative impacts and how these will be mitigated.

After approval by the regional agency Board, each RTIP will be made available electronically by the regional agency on its website, with the link provided to the Commission.

Each RTIP shall be based on the regional transportation plan and, where applicable, the Sustainable Communities Strategy developed and updated pursuant to Government Code Section 65080 and a region-wide assessment of transportation needs and deficiencies. Programming in the RTIP should not be based on a formula for distributing county shares among agencies or geographic areas.

Caltrans may nominate or recommend State highway improvement projects for inclusion in the RTIP for programming from the county share. Caltrans should also identify any additional State highway and intercity rail improvement needs within the region that could reasonably be programmed within the 3 years beyond the end of the current STIP period using revenue assumptions similar to those adopted for the fund estimate. These programming recommendations and this identification of State highway and intercity rail improvement needs should be provided to the regional agency at least 90 days before the due date for submittal of the RTIP or, if a later due date for project nominations is set by the regional agency, before that date. The regional agency has sole authority to accept Caltrans' STIP recommendations for programming in the RTIP. Caltrans shall provide a copy or list of its RTIP recommendations and identify additional state highway and intercity rail needs for each region to the Commission. Each region shall, in its RTIP, include a comparison of the projects in its RTIP and the State highway and intercity rail improvement needs identified by Caltrans, including a discussion of significant differences.

When Caltrans makes its RTIP recommendation and identifies State highway and intercity rail improvement needs, it should also share with the regional agency its plans for SHOPP projects that may be relevant to the region's consideration of RTIP projects. This is apart from the statutory requirement to make a draft of the SHOPP available for review and comment.

25. Project Planning, Programming, and Monitoring

The RTIP may propose to program up to five percent of the county share for project planning, programming, and monitoring (PPM) by the transportation planning agency or, within the Southern California Association of Governments (SCAG) area, by a county transportation commission. If the RTIP proposes programming funds for both SCAG and a county transportation commission, the total will not exceed five percent of the county share.

Funds programmed for this purpose should be spread across the years of the STIP. When allocated by the Commission, the funds will be available to cover the costs of:

- Regional transportation planning, including developing and preparing the regional transportation plan and Sustainable Communities Strategy.
- Project planning, including developing project study reports or major investment studies, is conducted by regional or local agencies in cooperation with regional agencies.
- Program development, including the preparation of RTIPs and studies supporting them.
- Monitoring the implementation of STIP projects, including project delivery, timely use of funds, and compliance with State law and the Commission's guidelines.

Caltrans expenses for these purposes are included in the Department's annual budget and will not be funded through the STIP except when a region reimburses Caltrans for project study reports using funds allocated to that region for PPM.

26. Active Transportation Projects in the RTIP

A region may program bicycle and pedestrian projects in its RTIP, as these projects are eligible for State Highway Account or Federal funds.

Consistent with Caltrans's Complete Streets Action Plan, regions should consider incorporating complete streets elements in all highway projects proposed for funding in the STIP.

For local road improvements, regions should consider incorporating complete street elements into their projects proposed for funding in the STIP.

27. County Shares, Advances, and Reserves

The fund estimate will identify, for each county, (1) the county share for the share period that ends during the current STIP period, (2) the county's proportionate share for the portion of the new four-year period that falls within the current STIP period, and (3) the balance of the estimated share for the four-year period that extends beyond the current STIP period. For the 2026 STIP fund estimate, for example, this means (1) the available share for the period ending 2027-28, (2) the

county's proportionate share for the period beginning 2028-29, and (3) an estimated proportionate share for the period ending 2031-32.

Any region may, in its RTIP, propose projects or project components during the STIP period from all of these shares, including the share for the period that extends beyond the STIP period. Unless the Commission rejects an RTIP, as described in Section 65, the Commission will include in the STIP, at a minimum, all RTIP projects carried forward from the prior STIP and all new RTIP programming proposed within the level of the county share for the share period that ends during the current STIP (i.e., for the 2024 STIP, the share for the period ending 2027-28). Beyond that, as described in Section 66, the Commission may include more or less in the STIP than each region's proportionate share for the new share period. Overall, the Commission may not program more than the available statewide capacity for the STIP period. The RTIP shall identify those projects or components it proposes to program within the STIP period from the share for each four-year period.

As authorized by Streets and Highways Code Section 188.8(j), a region for a county with a population of less than one million may also, in its RTIP, ask the Commission to advance an amount beyond its county share for a larger project. As identified in the Fund Estimate, the requested advance may not exceed 200 percent of the county share for the four-year share period that extends beyond the current STIP period. The RTIP will separately identify the project or components it proposes to program with the advance, following the same display format used for other RTIP projects.

Any region may, in its RTIP, ask to leave all or part of its county share unprogrammed, thus reserving that amount to build up a larger share for a higher-cost project or otherwise to program projects in the county later. The Commission may use funds freed up by these reserves to advance county shares in other counties. With Caltrans's consent, the Commission may also consider advancing county shares by reserving a portion of the interregional share until the next county share period.

28. Federal Match

In its RTIP, a region may propose to program State funds to match federal funds committed to a project. Such projects must meet the eligibility restrictions of the available state funds. For example, a transit project may not use State Highway Account funds to match federal funds unless the project is eligible under Article XIX of the California Constitution. The match for rail rolling stock and bus purchases can only be programmed in the STIP if Public Transportation Account capacity is available.

29. Regional Improvement Program Project Eligibility

Except for project planning, programming, and monitoring, all STIP projects will be capital projects (including project development costs) needed to improve regional

transportation. These projects generally may include, but are not limited to, improving State highways, local roads, public transit (including buses), intercity rail, pedestrian and bicycle facilities, grade separations, transportation system management, transportation demand management, sound walls, intermodal facilities, and safety. Non-capital costs for transportation system management or transportation demand management may be included where the regional agency finds the project to be a cost-effective substitute for capital expenditures. Other non-capital projects (e.g., road and transit maintenance) are not eligible.

In addition to meeting general program standards, all STIP projects must meet eligibility requirements specific to the STIP's funding sources, the State Highway Account, which includes both State and Federal revenues, and the Public Transportation Account. Unless the fund estimate specifies otherwise, a region may propose, in its RTIP, projects to be funded from any of these funding sources, or a combination of them. The Commission will provide and calculate STIP county shares without regard to the individual STIP funding sources.

Except for project planning, programming, and monitoring, RTIP nominations will be consistent with the following statutory sequence of priorities for programming from the State Highway Account:

- Safety improvements on transportation facilities other than State highways where physical changes, other than adding new capacity, would reduce fatalities and the number and severity of injuries. (Safety projects on State highways are programmed in the SHOPP. However, regions may program safety improvements in their RTIP for STIP programming if timely programming through the SHOPP is not possible because of funding limitations.)
- Transportation capital improvements that expand capacity, reduce congestion, or do both. These improvements may include the reconstruction of local roads and transit facilities, non-capital expenditures for transportation systems management, and transportation demand management projects that are cost-effective substitutes for capital expenditures.
- Environmental enhancement and mitigation, including soundwall projects.

Article XIX of the California Constitution permits State revenues in the State Highway Account (SHA) only for State highways, local roads, and fixed guideway facilities.

Article XIX of the California Constitution restricts transit and rail projects that can be funded with nearly all SHA revenues to the "research, planning, construction, and improvement of exclusive public mass transit guideways (and their related fixed facilities), including the mitigation of their environmental effects, the payment for property taken or damaged for such purposes, the administrative costs necessarily incurred in the foregoing purposes, and the maintenance of the structures and the immediate right-of-way for the public mass transit guideways,

but excluding the maintenance and operating costs for mass transit power systems and mass transit passenger facilities, vehicles, equipment, and services.”

Additionally, SHA revenues may not be expended for these purposes “unless such use is approved by a majority of the votes cast on the proposition authorizing such use of such revenues in an election held throughout the county or counties, or a specified area of a county or counties, within which the revenues are to be expended.”

This means, for example, that rail rolling stock and buses may be funded only from Federal revenues in the STIP. The non-Federal match can only be programmed in the STIP if PTA capacity is available for such projects. The match must be provided from a non-STIP source if no Public Transportation Account (PTA) capacity is available.

The Commission's continuing intent is for rehabilitation projects, excluding maintenance, on the local streets and roads system to remain eligible for funding in the STIP. Proposed projects on local highways functionally classified as a local or rural minor collector (nonfederal-aid eligible) are also eligible for STIP funding. However, programming of projects on nonfederal-aid eligible routes shall be limited to the availability of state-only funding as determined by the Commission.

Pursuant to Streets and Highways Code Section 100.15, any new capacity increasing project or a major street or highway lane realignment project proposed in an RTIP and ITIP shall demonstrate that reversible lanes were considered for the project. The lead agency shall demonstrate that reversible lanes were considered when submitting the project's environmental documents for consideration of future funding.

30. Federalizing Transit Projects

In accordance with Federal statutes and regulations, federal highway funds programmed for transit projects must be transferred from the Federal Highway Administration (FHWA) to the FTA for administration when the project or project component is ready to be implemented. To facilitate the transfer and timely use of funds, the Commission encourages the implementing agency or fund applicant to submit grant applications to the FTA requesting a grant number and tentative approval of project eligibility prior to requesting Commission allocation of funds.

Transit-related projects such as parking structures and multi-modal stations should also be transferred to the FTA for administration. However, a transfer is unnecessary on an exception basis when FHWA agrees to administer the funds and the grant application. Proposed exceptions should be discussed and agreed to with Caltrans and FHWA before programming the project in the STIP and documented in the PSR equivalent and ePPR.

31. Increased STIP Funding Participation

An RTIP may propose, from the county share, to increase a project's STIP funding to replace local funding already committed, provided the local funding has not been committed. It will not be expended or encumbered under contract before the Commission allocates STIP funds. The proposal will include the revised basis for cost sharing, as Section 54 of these guidelines specifies.

In those instances when a regional agency seeks additional STIP funding for a previously programmed project and the projected funding increase exceeds any increase in the estimated cost of that project, the board of such regional agency, by resolution of a majority of board members, shall declare in writing that the increase in the STIP funding is not for "back-filling" other non-STIP funds previously committed to the capital project which have already been, or in the future will be, redirected to non-capital activities and purposes.

32. Pooling of County Shares

Two or more regional agencies may agree to consolidate their county shares for two consecutive county share periods into a single county share for both periods. A pooling agreement will become effective for a county share period if each regional agency adopts a resolution incorporating the agreement and submits it to the Commission with its RTIP. Similarly, Sacramento Area Council of Governments (SACOG) may pool the shares of any counties in its region by adopting a resolution and submitting it with its RTIP.

As an alternative to pooling, two regional agencies may agree to accomplish the same purpose by loaning a specified dollar amount from one region's county share to the other during a STIP period, with the loaned amount to be returned in the following county share period. In its RTIP, a regional agency may also propose to contribute all or a portion of its current county share for the programming of a project in another county.

The Metropolitan Transportation Commission may pool its county shares for a STIP period by adopting a resolution and submitting it with its RTIP, provided that the amount of any county share advanced or reserved is not more than 15 percent of the county share identified in the Fund Estimate.

33. Consistency with Land Use Plans and Congestion Management Programs

Projects included in the regional program shall be consistent with the adopted regional transportation plan, and where applicable, the Sustainable Communities Strategy, which state law requires to be consistent with federal planning and programming requirements. The federal requirements (23 U.S.C. 134) include factors to be considered in developing transportation plans and programs, including the likely effect of transportation policy decisions on land use and development and the consistency of transportation plans and programs with the provisions of all applicable short- and long-term land use and development plans.

Congestion Management Programs (CMP) prepared by counties not electing to be exempt from CMP requirements pursuant to Section 65088.3 of the Government Code shall be incorporated by the Regional Agency into the appropriate RTIP before its adoption and submittal to the Commission, pursuant to Government Code Section 65089.2. Projects included in the adopted RTIP shall be consistent with the CMP's capital improvement program. Projects not in the approved CMP shall not be included in the RTIP unless identified and listed separately.

V. INTERREGIONAL IMPROVEMENT PROGRAM

34. General

The interregional improvement program consists of STIP projects funded from the interregional program share, which is 25% of new STIP funding. Caltrans will nominate a program of projects for the interregional share in its Interregional Transportation Improvement Program (ITIP). The interregional program has two parts:

- (a) The first part, funded from up to 10% of new STIP funding, is nominated solely by Caltrans in the ITIP. It is subject to the north/south 40%/60% split and otherwise may include projects anywhere in the State. The projects may include State highway, intercity passenger rail, mass transit guideway, or grade separation projects. Non-capital costs for transportation system management or transportation demand management may be included where Caltrans finds the project to be a cost-effective substitute for capital expenditures.
- (b) The second part, funded from at least 15% of new STIP funding, is not subject to the north/south split. It is limited to intercity rail projects (including Amtrak feeder bus, interregional commuter rail, and grade separation projects) and to improvements outside urbanized areas on interregional road system routes (which are specified in statute). At least 15% of the 15% (or at least 2.25% of new STIP funding) must be programmed for intercity rail projects, including interregional commuter rail and grade separation projects.

Under restricted circumstances, an RTIP may also recommend a project for funding from the second part, described in paragraph (b). See Section 37 of these guidelines.

35. Interregional Program Objectives

The Commission envisions an interregional improvement program that works toward the achievement of the following objectives:

1. Provide access for people and goods to and through all regions of California.
2. Ensure the interregional transportation system is reliable and efficient for moving people, goods, services, and emergency response.
3. Develop and operate a safe interregional transportation system for all travelers by improving public safety and security.

4. Optimize multi-modal connectivity throughout the interregional transportation system for all people.
5. Improve interregional connectivity to enhance California's diverse economy.
6. Improve and manage California's interregional transportation system in an environmentally sensitive, economical, and equitable manner that fosters livable and healthy communities and promotes social equity.
7. Ensure that the proposed investments align with CAPTI and are made in collaboration with local and regional partners.

The Caltrans draft and final ITIP shall be consistent with the ITSP prepared as required by Government Code sections 14524.4(a) and 14524.4(b). The ITSP should address the development of multi-modal corridors, including both the interregional road system and intercity rail in California, and it should define a strategy that extends beyond the STIP. The ITIP shall describe how proposed projects relate to the ITSP and how the proposed projects would implement the objectives listed above. The Commission will evaluate the ITIP and any regional recommendations for the interregional program in light of these objectives, the ITSP, and CAPTI.

The interregional improvement program will include State highway and rail projects (potentially including mass transit guideway and grade separation projects).

For State highways, the interregional program should emphasize an interregional transportation system that provides:

- Access to and through or around all regions of California; and
- Access to California's major interstate and international gateways, including interstate and international border crossings, international airports, and seaports.

The Commission expects the identification and selection of State highway projects for the interregional program to be based on consideration of cost in relationship to the following benefits, with higher priority given to projects with greater net benefit for the investment made:

- Traffic safety, including the potential for reducing fatalities and injuries;
- Reduced travel time and vehicle operating costs for interregional travel;
- Economic benefits to California of expanding interregional commerce through faster and more reliable access between markets;
- Economic benefits to California of expanding interstate and international trade and commerce through faster and more reliable access to California's international airports and seaports; and
- Ability to mitigate adverse environmental impacts.

Commerce includes the movement of people and goods for any economic purpose. It may include extractive industries (such as mining, agriculture, or timber) or recreation.

Projects where investments have been made and have not been completed, to the extent the benefits remain or have increased, Caltrans shall prioritize these projects over new projects to complete the corridor.

There is no expectation that STIP interregional improvements will be evenly spread across the State, and spreading funding among regions is not a Commission objective for the interregional program. The Commission encourages Caltrans and smaller regions (generally with populations less than 250,000) to consider and seek partnerships to jointly fund projects on the interregional road system for the mutual benefit of the region and the state.

For rail, the interregional program should emphasize:

- The preservation and improvement of the existing system of State-sponsored intercity passenger rail and Amtrak feeder bus routes, including compliance with safety and accessibility standards and protection of the State's investment in equipment;
- The reduction of the system's dependence on State operating subsidies;
- The improvement of other passenger rail access between major urban centers, airports, and intercity rail routes that support implementation of the state's rail plan;
- The use of rail grade separations to improve service reliability for both intercity passenger rail and interregional goods movement; and
- coordination and connectivity with the State's planned high-speed rail system.

The Commission expects the identification and selection of rail capital projects for the interregional program (including Amtrak feeder bus, interregional commuter rail, and grade separations) to be based on consideration of cost in relationship to the following benefits, with higher priority given to projects with greater net benefit for the investment made:

- reduced intercity rail running times and operating costs (which may increase demand and reduce the need for operating subsidies);
- improved intercity rail schedule frequency and reliability (which may increase demand and reduce the need for operating subsidies); and
- economic benefits to California of promoting trade and commerce by creating faster and more reliable highway or rail access to markets, including access to California's international airports and seaports.

Caltrans and the Commission may evaluate a project as part of a series of related projects in the same location or corridor for either highways or rail. The evaluation may consider the costs and benefits of the projects as a group. All projects in the group should be included in the ITSP as priorities for near-term funding, whether or not proposed for the STIP.

Where a potential interregional program project may provide substantial local benefits, dividing costs between the regional and interregional programs is

appropriate. In this case, the project's evaluation for the interregional program should be based on the interregional program cost share in relation to the benefits described in this section.

Pursuant to Streets and Highways Code Section 100.15, any new capacity increasing project or a major street or highway lane realignment project proposed in an RTIP and ITIP shall demonstrate that reversible lanes were considered for the project. The lead agency shall demonstrate that reversible lanes were considered when submitting the project's environmental documents for consideration of future funding.

36. Submittal of Caltrans ITIP

After consulting with regional agencies and other local transportation authorities, Caltrans shall submit its draft ITIP to the Commission no later than October 15 of each odd-numbered year. Two hearings, one in the south and one in the north, will be held by November 15 to provide the opportunity for public input regarding projects proposed in the ITIP. Caltrans shall submit its final ITIP, including a summary of the major comments received at the hearings and responses to those comments, to the Commission no later than December 15 of each odd-numbered year. At the same time, Caltrans will transmit a copy of the ITIP to each regional agency. The ITIP will include programming proposals from the interregional share for the five-year STIP period. These proposals may consist of new projects, program reserves, changes to prior STIP interregional program projects, and the interregional share of proposals for jointly funding new projects or cost increases from county and interregional shares.

The ITIP shall include, for each proposed project, information (including assumptions and calculations) to support an objective analysis of interregional program priorities. That information, based on the project study report, shall include:

- an estimate of total project costs, including mitigation costs and support costs;
- an estimate of the time of completion of project construction;
- an estimate of annual project benefits (at project opening) due to vehicle time savings and vehicle operating costs;
- for road projects, an estimate of annual project benefits (at project opening) due to reductions in fatalities and injuries;
- for rail or Amtrak feeder bus projects, an estimate of the project's impact on ridership and the need for operating subsidies;
- a discussion of the proposed project's impact on other projects planned or underway within the corridor; and
- a description of how the project would implement the interregional strategic plan, including a description of its impact on California's economic growth, the interregional distribution of goods, and the environment; and

- for every new project proposed for funding, or a carry-over project with cost changes, a cost-benefit analysis using the California Life-Cycle Benefit/Cost analysis must be included.
- A discussion describes how community engagement was performed and the benefits the project will achieve once implemented. The discussion should include potential negative impacts and how these will be mitigated.

Caltrans should consider nominating, in the ITIP, preconstruction funding for projects that Caltrans intends to propose for one of the competitive programs created under the Road Repair and Accountability Act of 2017 (SB 1).

Caltrans should consider fast-tracking new CAPTI-aligned project nominations in early planning phases. This will be done in collaboration with local and regional partners, and in addition, to the need to continue funding for existing ITIP projects.

The ITIP will be posted on the Department's website, with the link provided to the Commission.

37. Regional Recommendations for the Interregional Program

A regional agency may, in its RTIP, recommend improvements outside urbanized areas on interregional road system routes for funding from the interregional share. Interregional road system routes are defined in statute at Streets and Highways Code Sections 164.10 to 164.20, inclusive. By statute, the Commission may program a regional recommendation for the interregional program only if the Commission "makes a finding, based on an objective analysis, that the recommended project is more cost-effective than a project submitted by [Caltrans]." The Commission cautions regions, especially those with priority needs in both urbanized and non-urbanized areas, that non-urbanized area projects of highest regional priority should be proposed in the RTIP from the county share. The interregional program is not a nonurbanized area program, and the Commission does not intend to use the interregional program to meet most State highway needs in nonurbanized areas. The Commission anticipates programming regional recommendations for funding from the interregional program only when a recommended project constitutes a cost-effective means of implementing the Interregional Transportation Strategic Plan (see Section 35 of these guidelines).

Any regional recommendation for the interregional program shall be made in the RTIP and separate and distinct from the RTIP proposal for programming from the county share(s). Each project nominated in this way must constitute a usable segment of highway. The nomination must be to fund the project fully through the interregional program. The nomination may not be part of a proposal for joint funding between the regional and interregional programs. Joint funding proposals may be made only in concert with Caltrans, with the region proposing the county share in its RTIP and Caltrans proposing the interregional share in the ITIP.

An RTIP proposal for interregional funding should be accompanied by information (including assumptions and calculations) to support the objective analysis that the Commission must make before it can program the project. That information, based on the project study report, shall include:

- an estimate of total project costs, including mitigation costs and support costs;
- an estimate of the time of completion of project construction;
- an estimate of annual project benefits (at project opening) due to vehicle time savings and vehicle operating costs;
- for road projects, an estimate of annual project benefits (at project opening) due to reductions in fatalities and injuries;
- for rail or Amtrak feeder bus projects, an estimate of the project's impact on ridership and the need for operating subsidies;
- a discussion of the proposed project's impact on other projects planned or underway within the corridor; and
- a description of how the project would implement the interregional strategic plan, including its impact on California's economic growth, the interregional distribution of goods, and the environment.

In addition, an RTIP proposal for interregional funding must include a cost-benefit analysis using the California Life-Cycle Benefit/Cost Analysis.

38. Regional Transportation Plan

Projects included in the interregional program shall be consistent with the relevant adopted regional transportation plan(s) and, where applicable, the sustainable communities strategy.

39. Interregional Highway and Intercity Rail Needs

The ITIP shall identify projects that have previously received ITIP funds for pre-construction phases in the last ten years but have not been fully funded through construction.

Each interregional highway and intercity rail investment proposed in the ITIP must include a discussion of how the proposed investment is informed by the most recently approved Interregional Transportation Strategic Plan and how the proposed investments are meeting the need(s) identified in a corresponding corridor plan and the approved Interregional Transportation Strategic Plan.

Robust planning efforts must ensure that proposed investments maximize benefits, including benefits to equity, safety, multimodal travel choices, congestion relief, goods movement, ability to support evacuation, and adaptation to climate change. The Department shall provide, annually by December 31st, an update on system needs across all corridors encompassing the following facilities:

- State highways that are specified in Streets and Highways Code Sections 164.10 through 164.20

- Intercity passenger rail systems

The update should include the status of comprehensive multimodal corridor efforts led by the Department.

40. Active Transportation Projects in the ITIP

Caltrans should consider incorporating complete street elements, where applicable, into all projects proposed for funding. Complete street elements include elements that improve safety for all users, including people using bicycle and pedestrian facilities.

Where a proposed project on the state highway system is connected to a local transportation facility, the Commission encourages partnership with the regional agency to incorporate complete streets.

Caltrans may propose standalone active transportation projects in the ITIP if they improve or enhance interregional movement of people and goods. Examples include:

- First/Last mile connections to the intercity rail or multi-region bus systems.
- Bicycle highways that cross multiple regions along the interregional road system.
- Projects that complete a multi-use trail that crosses multiple regions.

41. Projects and Reserves

The ITIP shall include a complete proposal for the programming of the STIP interregional share which complies with the various statutory restrictions, including: the two parts described in Section 34 of these guidelines (the 10% and 15% parts), the north/south split of the first part, and the 2.25% intercity rail minimum of the second part. Any portion of the interregional share not proposed for a specific project may be proposed as a reserve for future programming. This may include reserves of any kind, including a proposal to reserve a portion of the interregional share for the next share period to free up funding for county share advances.

VI. ADVANCE PROJECT DEVELOPMENT ELEMENT

42. Fund Estimate for Advance Project Development Element

Each fund estimate will identify an amount available pursuant to subdivision (c) of Section 14529.01 of the Government Code for the STIP Advance Project Development Element (APDE), with county and interregional shares identified separately. These APDE amounts are independent of the amounts identified as regular programming capacity.

43. Programming of APDE County and Interregional Shares

Regions and Caltrans may propose projects from their respective county and interregional APDE shares in the RTIPs and ITIP, and they may propose joint regional and interregional APDE funding for a project. The proposal and adoption of projects will be the same as for other STIP projects, except that projects to be programmed through the APDE are limited to the two STIP project development components: (1) environmental and permits, and (2) plans, specifications, and estimates. Projects may not be programmed through the APDE if they are simultaneously programmed to acquire right-of-way (including support) or construct from regular STIP programming capacity. Project development work already programmed in the STIP may not be shifted to the APDE.

44. Program Year

APDE projects will be proposed for programming, adopted into the STIP, and allocated like other projects programmed in the STIP's five fiscal years. APDE local projects, when programmed, are subject to the STIP's timely use of funds provisions.

45. Program Amendments

APDE projects may be amended into the STIP at any time in the same manner as other STIP amendments. The amendments will identify the county or interregional APDE share from which the projects will be funded.

46. Effect on Regular County and Interregional Shares

APDE programming will be treated as an advance of the regular future county or interregional share. However, every county, including a county in a region with a population of over one million, is eligible for APDE programming. If all or a portion of any county or interregional APDE share is not programmed, that amount will become available to program for any STIP purpose in the next STIP. Amounts programmed in the current STIP from an APDE share will be deducted from the regular county or interregional share for the next STIP. The Fund Estimate for the next STIP will include a new APDE fund estimate with new county and interregional APDE shares.

47. APDE Shares May Not Be Exceeded

The programming of a county or interregional APDE share may not exceed the amount identified in the Fund Estimate. A county or interregional APDE share may not be loaned or advanced. However, regional agencies agreeing to pool their regular county shares (Section 32 of these guidelines) may also pool their APDE shares. Any region may program project development work from its regular STIP county share.

VII. DISPLAY OF PROJECT DESCRIPTIONS AND COSTS

48. Project Description

Each new or carryover project proposed for programming in the STIP shall include the following information:

- (a) The name of the agency responsible for project implementation.
- (b) The project title, including a brief nontechnical description of the project location and limits (community name, corridor, street name, etc.), and a phrase describing the type and scope of the project. By definition, the Commission will regard the limits for a rehabilitation project on local streets and roads, including adjacent or nearby streets and roads, thus providing greater flexibility in project scope.
- (c) A Caltrans-provided unique project identification number (PPNO).
- (d) The route number and post-mile limits should be identified for projects on the State highway system. Global positioning system (GPS) coordinates (longitude and latitude) and cross streets should be identified for local projects not on the state highway system.
- (e) The delivery schedule for each of the project's milestones.
- (f) Any appropriate funding restriction or designation, including projects eligible for Public Transportation Account funding, projects requiring state-only funding, or projects requiring Federal funds. Agencies proposing projects requiring state-only funding (including local street and road projects not eligible for federal aid) should recognize that the availability of state-only financing may be limited.
- (g) New and carryover projects shall include the current funding plan, including the total project cost and the source and amounts of local or other non-STIP funds, if any, committed to the project.
- (h) A map showing the project location and corridor.
- (i) The legislative districts where the projects are located.
- (j) The project's identification or page number as reflected in the RTP.

49. State-only Funding

All projects must follow the Federal-Aid Funding Guidelines adopted by the Commission for state-only funding. The Commission will assume that all projects will be qualified for Federal transportation funding unless the RTIP or ITIP designates otherwise. Whenever a region designates a project to be programmed for State-only (non-Federal) funding, the RTIP will explain the reason for this designation. The Commission will not program a state highway project for state-only funding without consulting with Caltrans. Projects programmed without state-only funding designation and later proposed for state-only funding allocations will be subject to Caltrans's recommendation for exception to federal funding prior to Commission approval as described in Section 69 of these guidelines.

50. Fact Sheets

All regions and Caltrans shall submit a one- or two-page fact sheet with their respective RTIP and ITIP. The fact sheet shall include:

- (a) An executive summary of the RTIP and ITIP highlighting the region and the State's top priorities.

- (b) A summary of the most significant benefits the proposed investments will provide to the region(s), including the community's safety, environment, equity, and economic benefits.
- (c) For the regions, a description of how the RTIP is advancing the goals and objectives of the Regional Transportation Plan and, where applicable, the Sustainable Communities Strategy.
- (d) For Caltrans, a description of how the ITIP is advancing the goals and objectives of the ITSP.
- (e) A description of how the RTIPs and the ITIP align with the State's goals. If a region has prepared a Comprehensive Multimodal Corridor Plan, a Bicycle Plan, or any other regional plans, include a discussion of the RTIP's consistency with those plans. Similarly, for Caltrans, the fact sheet should be consistent with the ITIP's plans, which Caltrans may have prepared.
- (f) The agency's logo.

The fact sheet will be posted on the Commission's website and must comply with state and federal web accessibility laws and standards.

51. STIP Database

Caltrans is responsible for developing, upgrading, and maintaining an electronic database record of the adopted STIP and Commission actions that amend the STIP. Caltrans will provide the Commission and the regional agencies with appropriate access to the STIP database to facilitate the STIP's development, analysis, and management.

52. Cost Estimates for Project Components

For each project proposed for programming, the RTIP or ITIP shall list costs separately for each of the four project components:

- (1) environmental studies and permits;
- (2) preparation of plans, specifications, and estimates,
- (3) right-of-way, and
- (4) construction.

The RTIP or ITIP shall list separate costs for Caltrans support and capital outlay for the right-of-way and construction components on Caltrans-implemented projects. This brings the total to six (6) project cost components for Caltrans projects.

For each project component, the amount programmed shall be escalated to the year proposed for programming, based on the current cost estimate updated as of November 1 of the year the RTIP or ITIP is submitted. The standard escalation rate for the STIP shall be the rate specified in the fund estimate for the STIP. Caltrans or a region may use alternative escalation factors for right-of-way or other costs deemed appropriate. STIP costs and non-STIP costs will be displayed separately. For Caltrans-implemented projects programmed in an RTIP, Caltrans

shall provide the region with cost updates at least 90 days prior to the date RTIPs must be submitted to the Commission.

When project design, right-of-way, or construction is programmed before the sponsoring agency completes the environmental process, updated cost estimates shall be submitted in the RTIP or ITIP in the STIP cycle following completion of the environmental process. Cost estimates for project components that are programmed and not allocated shall be updated, as needed, based on the most current cost information during every STIP cycle.

Where a project or project component will be funded from multiple county shares or jointly from the interregional and county shares, the amounts programmed from the different shares will be displayed separately. Amounts programmed for any component shall be rounded to the nearest \$1,000. For jointly funded projects, the county share or ITIP share contribution programmed for a component shall each be rounded to the nearest \$1,000.

53. Authority and Responsibility

For projects on the State highway system, only cost estimates approved by the Caltrans Director or by a person authorized by the Director to approve cost estimates for programming will be used. Only cost estimates approved by the Chief Executive Officer or other authorized officer of the responsible local implementing agency will be used for other projects.

54. Basis for Cost Sharing

Where a project or project component is to be funded from both STIP and non-STIP sources, the ePPR submitted with the RTIP or ITIP shall indicate whether the programming commitment is for a particular dollar amount, a particular percentage of total project cost, or a particular element or item of work. For projects with SHOPP funding, the RTIP shall discuss the SHOPP-eligible components and their current condition.

Where a project or project component is to be jointly funded from the interregional share and a county share or funded from multiple county shares, the ePPR submitted with the RTIP and/or ITIP shall indicate the basis for apportioning cost increases or decreases between the shares. The Commission must approve any changes after adopting the cost-sharing distribution.

Without an alternate cost-sharing arrangement approved by the Commission at the time of allocation, project costs, including increases and savings, will be apportioned in the same percentages as programmed.

Where a project is funded from both STIP and non-STIP sources, and where the Commission has approved non-proportional spending allowing for the expenditure of STIP funds before other funds, (sometimes referred to as sequential spending), the project is not eligible for an increase (supplemental) allocation under the

authority delegated to Caltrans by Commission Resolution G-12 until all other funds committed to the project have been expended.

Where a project is delivered using an alternative delivery method (e.g., public-private partnership), with operation and maintenance included, the operation and maintenance shall not be funded from the STIP or subject to the cost-sharing requirements of this section.

All funding agreements must be consistent with the STIP Guidelines.

55. Program Year for Cost Components

The cost of each project component will be listed in the STIP no earlier than the state fiscal year in which the project component can be delivered, as described below.

(a) Project development

(1) Local agency project development costs for environmental studies and permits will be programmed in the fiscal year during which environmental studies will begin. The fiscal year during which the draft environmental document is scheduled for circulation will be identified in the STIP. Costs for the preparation of plans, specifications, and estimates will be programmed in the fiscal year during which this work will begin. Where appropriate, local agency costs for environmental studies and design may be listed in different fiscal years.

(2) Caltrans project development costs for environmental studies and permits will be programmed in the fiscal year during which the environmental studies begin. Caltrans shall not begin environmental studies until the fiscal year in which project development costs for environmental studies and permits are programmed. The fiscal year during which the draft environmental document is scheduled for circulation will be identified in the STIP. Costs for preparing plans, specifications and estimates will be programmed in the fiscal year during which this work will begin. Caltrans shall not begin preparing plans, specifications, and estimates until California Environmental Quality Act (CEQA) and National Environmental Policy Act (NEPA) (if applicable) are completed and until the fiscal year in which project development costs for preparing plans, specifications, and estimates are programmed.

Caltrans may not begin preconstruction work on a project earlier than the year in which that project component is programmed unless funding becomes available through the delay of other STIP projects. The Commission must approve this through a STIP amendment specified in Section 75 or a time extension defined in Section 74.

Caltrans will report, outside the STIP, on year-by-year expenditures for project development components.

(b) Right-of-way

Right-of-way costs, including Caltrans support costs, will be programmed in the fiscal year during which right-of-way acquisition (including utility relocation) contracts will first be executed.

(c) Construction

Construction costs, including Caltrans construction support costs, will be programmed in the fiscal year during which construction contracts will be advertised or, for Caltrans-implemented projects, when the Ready to List milestone is achieved. All construction costs in or related to a single construction contract should be listed in one fiscal year, regardless of the time construction costs will be paid. Projects requiring separate construction contracts should be listed separately for the STIP, even if they are corridor projects grouped for project development and right-of-way programming, as described in Section 63 of these guidelines.

56. Escalation Adjustments

All projects will count against share balances based on their fully escalated (inflated) costs. All project RTIP and ITIP nominations shall therefore be at costs escalated to the year project delivery is proposed (see Sections 52 and 55 of these guidelines). Cost estimates for project components that are programmed and not allocated shall be updated, as needed, based on the most current cost information during every STIP cycle. A revised ePPR (per Appendix A) shall be submitted for every updated project. Commission staff may make further escalation adjustments, in consultation with Caltrans and regions, to make its staff recommendations and develop the STIP (see Section 68 of these guidelines). Ordinarily, the Commission will apply escalation adjustments only to Caltrans construction costs, not to right-of-way, project development, or local projects.

57. Prior Costs for Grandfathered 1996 STIP Projects

For every Caltrans project carried forward to the 1998 STIP, Caltrans will identify the amount of its expenditures for right-of-way (including support) and project development through the 1997-98 fiscal year. When added to the amounts remaining and programmed for the 1998 STIP period, these amounts will establish the project component base cost for share balance tabulations and adjustments, as described in Sections 58-64 of these guidelines. All other cost changes will be drawn from or credited to the appropriate regional share. In the ITIP, Caltrans shall report on the budgets for all ongoing grandfathered 1996 STIP projects. This reporting shall include a comparison of actual expenditures to project budgets as reported in the 2012 ITIP.

VIII. SHARE BALANCES AND ADJUSTMENTS

58. Long-term balances

With assistance from Caltrans and regional agencies, the Commission will maintain a long-term balance of county and interregional shares, as specified in Streets and Highways Code Section 188.11. The Commission will calculate the cumulative share balances as of the end of the preceding fiscal year and make them available for review by Caltrans and regional agencies by August 15 each year.

59. Local Projects

For share balances, the Commission will allocate the costs counted for local projects (all project work not implemented by Caltrans).

The Commission may approve a downward share adjustment to reflect the voted allocation if the construction contract award allotment is less than 80 percent of the engineer's final estimate. The regional agency should make its request by letter to the Commission no later than three months after the construction contract award date.

No adjustment will be made after the allocation vote for any amount not expended by the local agency. To provide a degree of flexibility to local agencies in administering projects, allocated funds may be shifted between project components to accommodate cost changes within the following limits:

- That agency may also expend any amount allocated to a local agency for environmental studies and permits for plans, specifications, and estimates. Any amount allocated to a local agency for plans, specifications, and estimates may also be expended by that agency for environmental studies and permits.
- Additionally, a local agency may expend an amount allocated for project development, right of way, or construction for another project phase, provided that the total expenditure shifted to a phase in this way is no more than 20 percent allocated for either phase. This means that the amount transferred by a local agency from one phase to another may be no more than 20 percent of whichever of the phases has received the smaller allocation from the Commission.

Shifting allocated funds between components will not impact county share balances, based on the actual amounts allocated for each component.

60. Construction

For share balances, the costs counted for Caltrans construction projects are the engineer's final estimate presented to the Commission for allocation vote.

At the request of Caltrans and with the approval of the regional agency for the county share, the Commission may approve a downward share adjustment of the allocation vote if the construction contract award allotment is less than 80 percent of the engineer's final estimate. The Department, with the approval of the regional

agency, when necessary, should make its request by letter to the Commission no later than three months after the construction contract award date.

No other adjustment will be made after the allocation vote for the award amount or for changes in expenditures, except where the Commission votes a supplemental allocation during or following construction. No share adjustment will be made for supplemental allocations made by Caltrans under the authority delegated by Commission Resolution G-12, except that when a Commission supplemental vote is larger than it otherwise would have been because of a prior G-12 rescission (negative G-12) made by Caltrans, the effect of the negative G-12 will be excluded when counting the Commission's supplemental vote for share balances. The programmed amount will be counted when a project has not been voted.

61. Construction Support

For share balances, the costs counted for Caltrans construction support are the amount identified and presented to the Commission for the allocation vote. No other share adjustment for cost differences less than 120% of the Commission's original allocation will be made. No adjustment will be made for supplemental allocations made by Caltrans under the authority delegated by Commission Resolution G-12. For costs equal to or greater than 120% of the Commission's original allocation, the Commission shall require a supplemental allocation, the full amount of which shall be counted for purposes of share balances.

62. Right-of-Way

For sharing balances, the costs counted for right-of-way on Caltrans implemented projects, including right-of-way support costs, are the amounts programmed for right-of-way in the STIP. No adjustment will be made for cost differences within 20 percent of the amount programmed for right-of-way reported at the time of construction allocation and/or contract acceptance. This flexibility is intended to facilitate the tracking of share balances and is not intended to be permission to overspend a project budget.

For Caltrans projects that achieve right-of-way certifications 1 or 2 at the time of Commission construction allocation, costs will be counted at the time of the vote. For Caltrans projects with a right-of-way certification other than 1 or 2, the reporting of the final estimate may be deferred until the right-of-way certification is updated. In no case shall this deferral exceed 12 months.

To encourage accurate estimates and minimize the manipulation of share balances, the Commission will consider STIP amendments for Caltrans implemented project right-of-way costs only in conjunction with the statewide review of right-of-way costs in the annual right-of-way plan.

63. Project Development

For share balances, the costs counted for Caltrans project development are the amounts programmed for environmental studies and permits, and preparing plans,

specifications, and estimates. No adjustment will be made for cost differences within 20 percent of the amount programmed for project development at the time of construction allocation. This flexibility is intended to facilitate the tracking of share balances and is not intended to be permission to overspend a project budget. To encourage accurate estimates and minimize the manipulation of share balances, the Commission will consider STIP amendments for project development only when the change in total project development costs is 20 percent or more or when changes in project development costs are the result of STIP amendments to change the scope of the project.

64. Federal Earmark Funds

Federal funds earmarked for specific projects that are not subject to federal obligation authority or are accompanied by their obligation authority, either individually or by project group, are not included in the Fund Estimate or programmed in the STIP. Because these funds are made available outside the STIP, they do not count against county or interregional shares. Suppose the sponsor or implementing agency for the earmarked project seeks RTIP or ITIP funding to match the federal earmark funds or to complete funding for the project. In that case, the project becomes a STIP project, and the earmark funds are treated as non-STIP funds.

If federal earmark funds become available for projects already programmed in the STIP, the earmark funds may be used in one of three ways:

- (1) If the STIP project is not fully funded, the earmark funds may be used to help fully fund the project.
- (2) If the project is fully funded, the earmark funds may increase its scope or supplement the state or local funds already committed to the STIP project.
- (3) If earmark funds supplant committed funds, the beneficiary of the tradeoff will be as follows:
 - (a) For projects funded with county or local funds, the county share and/or local funds will be credited with the benefit.
 - (b) For projects funded with interregional share funds, the interregional share will be credited with the benefit.
 - (c) For jointly funded projects, the interregional share, the county share, and/or the local fund will each be credited with the benefit in proportion to their respective funding commitments in the STIP project.

The Commission advises sponsors and implementing agencies for earmark projects that earmark funds are limited in availability for each specified project, or for groups of projects, to annual obligation authority and annual allocation percentages specified in federal statutes. This means that the full amount of federal earmark funds specified in federal statute may not be available for the project at planned implementation. These limitations shall be considered when

determining the amounts of earmarked funds available for the options described in the previous two paragraphs.

IX. COMMISSION ACTION AND ADOPTION

65. Commission Action on RTIP Proposals

The Commission will include all RTIP projects nominated from the county share for the four-year share period that ends during the current STIP (i.e., the period ending in 2030-31 for the 2026 STIP) unless the Commission finds that (a) the RTIP is not consistent with these guidelines, (b) there are insufficient funds to implement the RTIP, (c) there are conflicts with other RTIPs or with the ITIP, (d) a project is not in an approved CMP or is not included in a separate listing in the approved RTIP as provided by Government Code 65082, or (e) the RTIP is not a cost-effective expenditure of State funds. In making its finding, the Commission will consider the cost-effectiveness evaluation of the RTIP submitted by the region as required in Section 19 of these guidelines. If a region nominates only projects with uncommitted funds (see Section 16) for the four-year share period that ends during the current STIP, the Commission may view the RTIP as not a cost-effective use of state funds if there is a significant risk of the projects not receiving the funding commitments. The Commission may also evaluate based on the criteria in Section 19 of these guidelines. If the Commission makes one of those findings, it may entirely reject the RTIP. For the six (6) county SCAG area, the Commission will incorporate or reject each county's RTIP separately. For Metropolitan Transportation Commission (MTC) and SACOG, the Commission will incorporate or reject the multicounty RTIP. For counties that pool county shares, the Commission will incorporate or reject the counties' RTIPs together.

If the Commission proposes to reject an RTIP, it will provide notice to the regional agency not later than 60 days after the date it receives the RTIP. The Commission's Executive Director may provide the notice by letter; the notice does not require formal Commission action. The notice will specify the factual basis for the proposed rejection. The Commission will act on the proposed rejection of an RTIP no later than the adoption of the STIP. No later than 60 days after the Commission rejects an RTIP, it will hold a public hearing on the RTIP in the affected region unless the regional agency proposes to waive the hearing and submit a new RTIP. Whenever the Commission rejects an RTIP, the regional agency may submit a new RTIP. Unless the new RTIP is rejected in the same manner, it will be incorporated into the STIP as a STIP amendment. This amendment will not require a separate 30-day public notice if the new RTIP is limited to projects considered in the STIP hearings or a public hearing on the proposed RTIP rejection.

The Commission may also program projects proposed in the RTIP for funding from the estimated county share for the four-year share period that extends beyond the current STIP (in the 2026 STIP, this is the share period ending 2031-32) or from

advances against future share periods. A decision by the Commission not to program any of these proposed projects does not constitute or require a rejection of the RTIP. Any portion of the county share for the four-year period not programmed in the current STIP will remain available for programming within the same period in the following STIP.

66. Commission Action on Advances and Reserves

In selecting projects for funding beyond the county share for the share period that ends during the current STIP, including advances, the Commission intends to consider regional agency priorities and the extent to which each RTIP includes:

- Projects consistent with Governor's Executive Order B-30-15 (based on documentation submitted in the RTIP).
- Projects implement a cost-effective RTIP, considering the evaluation submitted as required by Section 19 of these guidelines.
- Projects that complete or fund further components of projects included in the prior STIP.
- Grandfathered projects from the 1996 STIP.
- Projects within the corridor that meet identified State highway and intercity rail improvement needs as described in Section 24.
- Projects that leverage federal discretionary funds.
- Projects that leverage discretionary local funds that would otherwise not be spent on a transportation-related purpose.
- Projects that provide regional funding for interregional partnership projects.

If the Commission approves a region's request to advance an amount beyond its county share for the four years to program a larger project, the advance will be deducted from the county share for the following county share period. Suppose the Commission does not approve the advance and does not program the project or project components that the RTIP proposed to program with the advance. In that case, the Commission will reserve any portion of the county share left unprogrammed until the next STIP. This action will not require a rejection of the entire RTIP.

An RTIP request to reserve part or all of a county share until the next STIP or county share period will free up current period funding that the Commission may use to advance county shares in other counties. With Caltrans's consent, the Commission may also consider advancing county shares by reserving a portion of the interregional share until the next county share period.

67. Commission Action on Interregional Program

The Commission will program the interregional share of the STIP from projects nominated by Caltrans in its ITIP or alternative recommendations made by regions in their RTIPs. By statute, the Commission may program a regional recommendation for the interregional program only if the Commission "makes a finding, based on an objective analysis, that the recommended project is more

cost-effective than a project submitted by [Caltrans].” The Commission may decline to program any project it finds inconsistent with these guidelines or not a cost-effective expenditure of State funds. In making its finding, the Commission will consider the cost-effectiveness evaluation of the ITIP submitted by Caltrans as required in Section 19 of these guidelines. The Commission may also evaluate based on the criteria in Section 19 of these guidelines. After a review of the nominated projects, the Commission may elect to leave a portion of the interregional share unprogrammed and reserved for later interregional programming or, with the consent of Caltrans, may reserve a portion of the interregional share for the next share period to free up funding for county share advances.

68. STIP Respreading of Projects

The Commission may program projects, project components and project reserves in fiscal years later than the fiscal years proposed in the RTIP or ITIP if the Commission finds it necessary to do so to ensure the total amount programmed in each fiscal year of the STIP does not exceed the amount specified in the fund estimate as required by Section 14529(e) of the Government Code. In that case, the Commission will compare all projects nominated for the year(s) from which projects will be postponed, considering (1) regional priorities and the leveling of regional shares across the STIP period, and (2) the availability of PTA or other restricted funds by fiscal year.

X. STIP MANAGEMENT

69. Allocation of Funds

The Commission will consider allocating funds for a project or component when it receives an allocation request and recommendation from Caltrans. The Commission will only consider allocating construction and/or construction support funds to projects that are ready to advertise. For ready-to-advertise projects, the Commission expects Caltrans to certify that a project’s plans, specifications, and estimate (PS&E) are complete, environmental and right-of-way clearances are secured, and all necessary permits and agreements (including railroad construction and maintenance) are executed. Projects not ready for advertisement will not be placed on the Commission’s agenda for allocation approval. All construction allocations, *including rail equipment procurements*, are valid for six months from the date of allocation unless the Commission approves an extension (see Section 73 regarding timely use of funds).

When requesting an allocation of funds for the right-of-way or construction of a transit or intercity rail project in which the transit or rail operator will not own the improved facility, the request for allocation must be accompanied by a copy of the executed agreement with the facility owner that details the benefits the operator is to receive following the capital improvements.

All allocations will be made in units of \$1,000, and all allocation requests shall therefore be in units of \$1,000. The request will include determining funding availability and recommending the funding source. The recommendation on the source of funding shall consist of the amounts by fund account, i.e., State Highway Account, Public Transportation Account, or Federal Trust Fund, as well as the fund type within the account, including the type of federal funds. Caltrans' recommendation to the Commission for state-only funding of a project will be made in accordance with Caltrans' current policy for exceptions to federal funding. The final determination of the fund type available for a project will be made in the Commission's allocation of funds. The Commission will approve the allocation only if the funds are available and are necessary to implement the project as programmed in the STIP.

In compliance with Sections 21102 and 21150 of the Public Resources Code, the Commission may not allocate funds to local agencies for design, right-of-way, or construction before environmental clearance documentation under the California Environmental Quality Act (CEQA). As a matter of policy, the Commission will not allocate funds to local agencies for design, right-of-way, or construction of a federally funded project before environmental clearance documentation under the National Environmental Policy Act (NEPA). Exceptions to this policy may be made when federal law allows for the acquisition of right-of-way before completion of NEPA review.

All funds allocated are subject to the timely use of funds provision as described in Section 73 of these guidelines.

The Commission will consider making an allocation that exceeds the amount programmed in the STIP if a region or the interregional program has an adequate unprogrammed share balance or if the Commission finds it can approve an advance to the county share or to the interregional share. Unallocated amounts are available for allocation until the end of the fiscal year in which they are programmed in the STIP. Funds not allocated are subject to the timely use of funds provisions described in Section 73 of these guidelines.

If a project or project component is ready for implementation earlier than the fiscal year that it is programmed in the STIP, the implementing agency may request an allocation in advance of the programmed year. The Commission may allocate in advance of the programmed year if it finds that the allocation will not delay funding availability for other projects.

When a local agency (including a transit agency) is ready to implement a project or project component, the agency will submit a request to Caltrans. Caltrans will review the request, prepare appropriate agreements with the agency, and recommend the request to the Commission for action. After receipt of the application, the typical time required to complete Caltrans review, recommendation, and Commission allocation is 60 days. The specific details and instructions for the allocation, transfer and liquidation of funds allocated to local

agencies are included in the Local Assistance Procedures for Administering Local Projects in the STIP prepared by Caltrans in consultation with the Commission and regional and local agencies. Allowable reimbursable costs are eligible for reimbursement only after the Commission approves the allocation.

70. Allocation of Right of Way Capital for Caltrans implemented projects

Pursuant to Commission Resolution G-01-09, the Commission delegated authority to Caltrans for sub-allocations and/or adjustment authority to streamline and help in the management of right of way. Annually, Caltrans will present for Commission review and acceptance a Right of Way Capital Plan. Only programmed projects may be included in the Right of Way Capital Plan. The annual right-of-way capital allocation will not exceed the amount programmed for capital project costs. Unprogrammed right-of-way commitments, such as post-certification costs and inverse condemnation, are funded through the right-of-way capital fund reservation established in the STIP Fund Estimate.

Prior to the approval of the environmental document, Caltrans is restricted from right-of-way expenditures with the exception of the following activities that may occur during the Project Approval and Environmental Document (PA&ED) phase:

- Permits to enter
- Environmental permit fees
- Positive location of utilities
- Agreements for railroad coordination
- Preliminary title fees

Commitments related to the items listed above are limited to five percent of the overall amount approved in the annual Right of Way Capital Plan.

Beginning in fiscal year 2019-20 or later, projects programmed for right-of-way capital of \$10 million or more, it must receive Commission approval for project-level allocations prior to expenditure of funds. If a project's right-of-way capital initially falls below this threshold but increases to \$10 million or more, an individual project allocation will be required.

Caltrans will provide the Commission quarterly status reports on right-of-way capital plan expenditures. The report shall include a summary, by program, of the funds programmed, allocated, and expended. Changes to the allocation amounts for individual projects must also be included in the quarterly report. Any project identified by Caltrans as needing additional right-of-way capital after the Commission accepts the Right-of-Way Capital Plan will need to be highlighted in the quarterly report, including information on how the additional right-of-way costs will be managed.

71. Allocation of Alternative Delivery Methods

Projects using design-build, design-sequencing procurement, or Construction Manager/General Contractor (CMGC) delivery method shall be identified during programming or before allocation. The Commission will not allocate funds to a project using design-build, design-sequencing, or CMGC procurement without CEQA and NEPA (if applicable) clearance as specified in Section 71 of these guidelines. These project delivery methods will be programmed and allocated in the same manner as projects utilizing design-bid-build delivery. However, schedule, scope, and cost flexibility may be requested and approved consistent with allocation and programming capacity and the timely use of funds rules.

For projects using Design-Build or design-sequencing procurement methods, the allocation for the Plans, Specifications, and Estimate phase may be used to fund up to 30 percent of the Plans, Specifications, and Estimate effort, the right-of-way requirement determination, and the development of the Request for Qualifications and Request for Proposal. Construction support and construction capital allocation must be approved before releasing the Request for Proposal to short-listed Design-Build proposers. After approval of the Design-Build contract, the Design Builder's design costs are funded by the construction capital allocation. The construction support allocation funds Caltrans' oversight of the Design-Builder's design.

For projects using the CMGC delivery method, the Contract Manager's contract costs during the design phase are considered design phase expenditures. Upon award of the construction contract, the contractor shifts to the General Contractor role, and expenditures will be reported as construction phase expenditures.

If a project using the CMGC delivery method is divided into separate work packages for delivery and the packages are unknown at the time of programming, the work packages may be split at the time the initial allocation is requested. A list of the anticipated work packages and their delivery year must accompany the initial construction allocation request. When the first allocation is approved, subsequent construction allocations must provide updated lists for anticipated work packages. A STIP amendment will not be required, provided the scope remains unchanged.

72. SB 184 Reimbursement Allocations

Government Code Section 14529.17, as amended by SB 184 (2007), permits a regional or local agency to expend its funds for a STIP project in advance of the Commission's approval of a project allocation and to be reimbursed for the expenditures after the Commission approves the allocation. However, the statute does not require the Commission to approve an allocation that it would not otherwise approve.

To qualify for reimbursement of expenditures before the Commission approves a project allocation, the regional or local agency must submit a project allocation

request that includes notice of the agency's intent to expend its funds for the project before the allocation approval.

When submitting the original to Caltrans, the regional or local agency should send a copy of the allocation request to the Commission's Executive Director.

The local entity must comply with all legal requirements for the project and any project expenditures, including Federal and State environmental laws. Expenditures for projects programmed for Federal funding still require advance approval of the Federal obligation for the project (E-76). Any local agency must intend to take advantage of the reimbursement provisions of Section 14529.17, understand its obligations, and the risk that is inherently involved.

Only expenditures made by or under contract with a regional or local agency for a project that was and is programmed in the STIP are eligible for reimbursement allocations by the Commission. Project expenditures must be in accordance with the STIP at the time of expenditure and allocation.

The following expenditures are not eligible for reimbursement allocations by the Commission:

- expenditures made before adoption of the project component in the STIP;
- expenditures made before the submittal of the allocation request or before the beginning of the fiscal year for which the project is programmed;
- expenditures that exceed the amount that was or is programmed in the STIP for the particular project component;
- expenditures made by Caltrans;
- expenditures made by a regional or local agency for a project component that was or is programmed for Caltrans implementation;
- expenditures made by a regional or local agency on the State highway system, except in accordance with a project-specific cooperative agreement executed between the local agency and Caltrans; and
- expenditures made by a regional or local agency for a project component that was or is programmed for implementation by another regional or local agency, except in accordance with a project-specific agreement between the two agencies.

The Commission will approve reimbursement allocations only if the regional or local agency submits an allocation request before the first expenditure and the Commission finds no legal impediment to a Commission allocation, other than lack of State budget authority, at the time of expenditure. If, at the time of the allocation request, the Commission finds a lack of sufficient funding available and would otherwise approve the allocation, then the Commission will approve the project for future allocation when funding becomes available. However, even the inclusion of a project in the STIP, the availability of state budget authority, and the lack of specific legal impediment do not obligate the Commission to approve an allocation where the Commission finds that the allocation is not an effective use of state

funds, is inconsistent with the Commission's guidelines or policies, or is inconsistent with state or regional plans.

73. Timely Use of Funds

Project Delivery Deadlines:

- Allocation deadline –
 - For locally implemented projects, all phases programmed are available for allocation until June 30th of the fiscal year in which the funds are programmed.
 - For Caltrans implemented projects, construction, construction support costs, or right of way capital of \$10 million or more, are available for allocation until June 30th of the fiscal year in which the funds are programmed.

Programmed funds not allocated within this deadline will lapse and be deleted from the STIP. The Commission will not immediately make the funds available to the county or interregional share for reprogramming. However, the Commission will adjust the share balance to restore the funds in the next county share period.

- Expenditure deadline – Funds allocated to local projects for environmental, design, right of way, and PPM must be spent by the end of the second fiscal year following the fiscal year in which the funds were allocated. For example, a project allocated in fiscal year 2024-25 must be spent by June 30, 2027. For local projects, the local agency must invoice Caltrans for these costs no later than 180 days after the fiscal year in which the final expenditure occurred.
- Contract Award deadline – Commission policy is that funds allocated for construction, including intercity-rail projects, or for the purchase of equipment must be encumbered by the award of a contract within six (6) months of the date of allocation unless the Commission approves an extension as described in Section 74.
- Construction contract acceptance deadline – After the award of the construction contract, the local agency or Caltrans has up to 36 months to complete (accept) the contract.
 - For local projects, the local agency has 180 days **after** contract acceptance to make the final payment to the contractor or vendor, prepare the final Report of Expenditure and submit the final invoice to Caltrans for reimbursement.
 - Given the flexibility of the planning, programming, and monitoring funds, time extensions will not be considered for these funds.

- Additional time for project completion at the time of allocation – The Commission may approve additional time for project delivery at the time of allocation, which includes the completion of work and the liquidation of funds, to accommodate the proposed expenditure plan for the project, except for funds allocated for planning, programming, and monitoring.
- Federal Transit Administration Transfers – Federal highway transportation funds programmed and allocated for transit projects are considered obligated and are deducted from the state's federal obligation authority balances as soon as they are transferred to the Federal Transit Administration (FTA) as described in Section 30 of these guidelines. Federal funds for such projects will be considered encumbered and expended upon completion of the fund transfer to FTA. State funds allocated to match the federal funds for such projects will be subject to the timely use of funds provisions described in this section. Upon completion of such projects, after FTA notification of final project costs, FHWA will adjust obligation records accordingly. Any federal funds transferred to FTA but not expended will be rescinded as state highway account revenue, with no adjustment to county shares. Any state match funds allocated but not expended will also be rescinded with no adjustment to county shares.

Whenever a contract award does not encumber allocated funds or transfer to FTA or is expended within the deadlines specified above, all unencumbered, not transferred, or unexpended funds from the allocation will be rescinded. The Commission will not adjust the county or interregional share for any unencumbered allocation balance.

Caltrans will provide monthly reports to the Commission on projects that have not been awarded or transferred to FTA within six months of the date of the Commission's allocation.

These provisions for the timely use of funds do not apply to Caltrans project development costs, Caltrans right-of-way support costs, or Caltrans right-of-way capital under \$10 million, which the Commission does not allocate.

Funds allocated to Caltrans for right-of-way capital, as specified in Section 70, must be initiated as expenditures within six months of the allocation. Whenever allocated right-of-way capital funds have not initiated expenditure within six months of allocation, the funds will be rescinded, unless the Commission approves a project expenditure time extension. The Commission will not adjust the county or interregional share for rescinded allocated right-of-way funds.

Caltrans has up to 72 months to complete the scope of work specified in the right-of-way phase.

The Commission will not amend the STIP to delete or change the program year of the funding for any project component programmed in the current fiscal year or earlier, except:

- (1) to reprogram funds from a construction project to later mitigation work required for that project, including landscaping or soundwalls, or
- (2) to reprogram funds from one project to another within an identified multi-modal corridor, as defined in Section 11, or
- (3) where the projects are being delivered using the Construction Management/General Contractor delivery method to deliver early work packages as specified in Section 72.

In either of these cases, the Commission will consider the amendment only if it is proposed concurrently with an allocation of funds programmed for the project in the current fiscal year. These three amendments are adjustments that may be incorporated into the Commission's allocation action. In that case, they do not require the separate notice ordinarily required of STIP amendments.

Where a project or project component will not be ready for allocation as programmed in the current fiscal year, the agency responsible for the project should request an extension of the allocation deadline since the project is not eligible for a STIP amendment.

74. Delivery Deadline Extensions

The Commission may extend a delivery deadline, as described in Section 73, upon the request of the regional agency or the agency responsible for project delivery. No deadline may be extended more than once. However, there are separate deadlines for allocation, for award of a contract, for expenditures for project development or right-of-way, and for project completion, and each project component has its own deadlines. The Commission may consider the extension of each of these deadlines separately.

All requests for project delivery deadline extensions shall be submitted directly to the appropriate Caltrans district at least 60 days prior to the specific deadline for which the particular extension is requested (e.g., 60 days prior to June 30th to request the extension of allocation deadlines). The extension request should describe the specific circumstance that justifies the extension and identify the delay directly attributable to that circumstance. Caltrans will review extension requests and forward them to the Commission for action. Unlike proposed STIP amendments, extension requests do not require a 30-day notice period.

- Allocation time extension (20 months maximum) – The Commission may extend the deadline for allocation of funds only once and only if it finds that an unforeseen and extraordinary circumstance beyond the control of the responsible agency has occurred that justifies the extension. The extension will not exceed the period of delay directly attributed to the extraordinary circumstance and cannot exceed 20 months.

- Contract award time extension (20 months maximum) – The Commission may extend the deadline for contract award only once and only if it finds that an unforeseen and extraordinary circumstance beyond the control of the responsible agency has occurred that justifies the extension. The extension will not exceed the period of delay directly attributed to the extraordinary circumstance and cannot exceed 20 months.
- Project development expenditure time extension (20 months maximum) – The Commission may extend the deadline for each project development expenditure of the environmental, design, and right-of-way phases only once and only if it finds that an unforeseen and extraordinary circumstance beyond the control of the responsible agency has occurred that justifies the extension. The extension will not exceed the period of delay directly attributed to the extraordinary circumstance and cannot exceed 20 months.
- Project completion time extension (20 months maximum) – The Commission may extend the deadline for project completion of the construction phase only once and only if it finds that an unforeseen and extraordinary circumstance beyond the control of the responsible agency has occurred that justifies the extension. The extension will not exceed the period of delay directly attributed to the extraordinary circumstance and cannot exceed 20 months.

For each request to extend the deadline to allocate project construction funds, the agency requesting the extension should submit a project construction STIP history in conjunction with the request. The request should also identify any cost increase related to the delay and how the increase would be funded. The STIP history should note the original inclusion of project construction in the STIP and each project construction STIP amendment, including the amendment date, the dollar amount programmed for construction, and the scheduled year of construction delivery. The Commission intends to review this history when considering a construction allocation extension request.

75. STIP Amendments

The Commission may amend the STIP at the entity's request, either Caltrans or the regional agency that originally nominated the STIP project. The Commission will amend the STIP only after providing at least 30 days of public notice. Projects proposed by amendment will be subject to the same standards and criteria that apply to RTIP and ITIP proposals. Each amendment will designate from which county share(s) or interregional share the project is being funded, and the Commission will adjust share balances accordingly. An amendment may not create or increase a county share deficit unless the Commission can approve an advance of the county share (see Sections 27 and 66 of these guidelines).

All regional requests for STIP amendments shall be submitted directly to the appropriate Caltrans district. For each amendment that would delay the year of

construction, the agency requesting the amendment should submit a project construction STIP history in conjunction with the amendment request. The request should also identify any cost increase related to the delay and how the increase would be funded. The STIP history should note the original inclusion of project construction in the STIP and each prior project construction STIP amendment, including the amendment date, the dollar amount programmed for construction, and the scheduled year of construction delivery. The Commission intends to review this history when considering a STIP amendment that would delay the year of construction.

Caltrans will review proposed amendments and forward them to the Commission for public notice and action. The Commission encourages Caltrans, in cooperation with regions and Commission staff, to develop and implement procedures to standardize and streamline the amendment process and enhance regions' accountability for amendments to projects not administered by Caltrans.

An amendment may change the scope, cost, or program year of any STIP project, except that the Commission will not amend the STIP:

- to change Caltrans right-of-way costs, except in conjunction with the annual right-of-way plan, or to make an adjustment of more than 20 percent in conjunction with the Commission's allocation of project construction funding;
- to delete or change the program year of the funding for any project component after the beginning of the fiscal year for which it is programmed (except for the adjustments at the time of allocation described in Section 73);
- to change Caltrans construction support or project development costs, except when the change in total construction support or project development costs is 20 percent or more, unless the cost change is the result of a STIP amendment to change the scope of the project; or
- to change the programming of any funds after they have been allocated.

76. Approval of AB 3090 Arrangements

Under Government Code Section 14529.7, as amended by Assembly Bill (AB) 3090 (1992), the Commission, the Department, a regional agency, and a local agency may enter into either one of two types of arrangements under which a local agency pays for the delivery of a STIP project with its funds in advance of the year in which the project is programmed. Under the first type of arrangement, the local agency that advances the STIP project has another project or projects of equivalent value programmed in its place, and these arrangements are implemented by a STIP amendment designating the specified dollar amount for an "AB 3090 replacement project" without identifying the specific project to be implemented as the replacement. Under the second type of arrangement, the local agency that advances the STIP project is programmed to receive a direct cash reimbursement. Those arrangements are implemented by a STIP amendment that approves the Department to execute a reimbursement agreement and programs the reimbursement for the fiscal year in which the project was scheduled in the STIP or a later year.

Scheduled project reimbursements have the highest STIP priority among projects programmed within a fiscal year, although reimbursements are subject to the availability of the appropriate fund type. In most cases, reimbursement will be programmed over several years. Additionally, the Department may pay the reimbursements quarterly if so specified in the reimbursement agreement.

The Commission has adopted separate AB 3090 Reimbursement Guidelines (Resolution G-02-13) that describe specific procedures for reimbursement arrangements. The following is the Commission's policy for approving AB 3090 arrangements for either replacement projects or reimbursements.

1. The Commission intends to encourage local agencies that wish to use local funds to advance the delivery of projects programmed for construction in the STIP when State funds are insufficient to support direct project allocations. In doing so, the Commission will consider approving either AB 3090 replacement projects or AB 3090 direct reimbursement arrangements, giving preference to the programming of AB 3090 replacement projects where feasible or to AB 3090 reimbursements using federal funds and the local advance construction process.
2. Where a local agency proposes to use its own funds for early delivery of a project component programmed in the STIP for a future fiscal year, the Commission will consider approval of an AB 3090 replacement project under the following conditions:
 - a. The regional agency approves the arrangement.
 - b. The local agency has identified a local fund source for the project component, and there is a reasonable expectation that the AB 3090 approval will accelerate the construction delivery of an STIP project.
 - c. The local agency commits to award a contract or otherwise begin delivery of the project component within six (6) months of the Commission's approval, with the understanding that the arrangement may be cancelled if that condition is not met. AB 3090 arrangements for construction or the purchase of equipment are valid for six months from the date of approval unless the Commission approves an extension.
 - d. The STIP amendment approving the arrangement will replace the project component with an unidentified replacement project in the same fiscal year.
3. Where a local agency proposes to use its funds for early delivery of a project component programmed in the STIP for a future fiscal year, the Commission will consider approval of an AB 3090 reimbursement only when the following additional conditions are met:
 - a. The regional agency explicitly finds the project to be the region's highest priority among STIP projects programmed for that fiscal year. A regional agency unable to make such a finding shall, in its request for an AB 3090 reimbursement, explain why it cannot make the finding and the relative priority of the STIP projects programmed for that fiscal year.

- b. The Commission determines that reimbursement would be consistent with the fund estimate.
 - c. The source of local funds to deliver the project could not or would not be made available for an AB 3090 replacement project. The request for AB 3090 reimbursement approval shall identify the source of local funds to be used, why the funds would not be available for the STIP project without an AB 3090 direct reimbursement arrangement, and what the funds would be available for if not used for the STIP project.
 - d. Before approving an AB 3090 reimbursement arrangement, the Commission will consider programming the reimbursement in a later fiscal year, consistent with the project's regional and state priority for funding and the projected availability of funds to support other projects. The Commission will not change the reimbursement programming after approval.
 - e. The Commission will not approve AB 3090 reimbursement arrangements intended solely to protect a project from being reprogrammed or to protect a local agency's share of STIP funding.
4. The Commission will also consider approval of an AB 3090 reimbursement arrangement for a project component programmed in the current fiscal year if insufficient funds are available to approve a direct allocation. In this case, the AB 3090 approval will schedule the reimbursement for the next fiscal year or later. In making a current year request for an AB 3090 reimbursement arrangement, the region shall explain why the project cannot be advanced using a reimbursement allocation (as described in section 70).
5. In considering approval of AB 3090 reimbursement arrangements, the Commission intends to ensure that no more than \$200 million in reimbursements is scheduled statewide for any one fiscal year and that no more than \$50 million in reimbursements is scheduled for the projects of any single agency or county for any one fiscal year. The Commission intends to evaluate the limit on AB 3090 reimbursement arrangements biennially as a part of the STIP fund estimate and STIP guidelines. A local agency may request the approval of an AB 3090 reimbursement arrangement that exceeds the aforementioned limits. The Commission will consider such requests on a case-by-case basis. In evaluating such requests, the Commission will weigh the impact that exceeding the limits might have on allocating other STIP projects.

77. Selection of Projects for GARVEE Bonding

If the fund estimate projects the availability of federal funding for the STIP, the Commission may, by STIP amendment, select STIP projects proposed from either an RTIP or the ITIP for accelerated construction through Grant Anticipation Revenue Vehicle (GARVEE) bonding. With the agency's agreement to propose the project, the Commission may designate a STIP project for GARVEE bonding even if the original RTIP or ITIP did not specifically propose GARVEE bonding. The Commission may also select projects programmed in the SHOPP for

accelerated construction through GARVEE bonding. The Commission will select projects for GARVEE bonding that are major improvements to corridors and gateways for interregional travel and goods movement, especially projects that promote economic development and projects that are too large to be programmed within current county and interregional shares or the SHOPP on a pay-as-you-go basis. The Commission expects that, generally, these will be projects that require bond proceeds exceeding \$25 million. Major improvements include projects that increase capacity, reduce travel time, or provide long-life rehabilitation of key bridges or roadways.

Each bond will be structured for debt service payments over a term of not more than 12 years. In designating projects for bonding and scheduling bond sales, the Commission will consider the overall annual debt service limit of 15 percent of Federal revenues.

GARVEE bonds cover only the federally funded portion of a project's cost (generally 88½ percent). GARVEE bonding in California is structured so that the State's future Federal transportation apportionments cover all debt service payments. This requires that the entire non-Federal portion of the project cost (including costs of issuance and interest) be provided on a pay-as-you-go basis at the time of construction. The Commission's policy is that the non-federal portion of project costs will be programmed within the current STIP and SHOPP capacity. Although local funds may be applied to the non-federal share, the ability of a local agency to contribute non-STIP funding will not be a major criterion in the selection of projects for GARVEE bonding.

78. Project Delivery

It is a Commission policy that all transportation funds allocated through the State be programmed and expended in a timely manner to avoid the accumulation of excessive fund balances and the lapse of federal funds. The Commission's goal is that transportation projects programmed against funds allocated through the State be delivered no later than scheduled in the appropriate transportation programming document. For this goal, delivery means allocation or obligation of funds for the programmed project or project component. For projects delivered by Caltrans, the Commission's delivery goal each fiscal year is 90% of the projects programmed in each fiscal year and 100% of the funds programmed in each fiscal year. For projects delivered by agencies other than Caltrans, the Commission's delivery goal each fiscal year is 90% of the projects programmed in each fiscal year and 95% of the funds programmed in each fiscal year.

Caltrans will provide the Commission with status reports on project delivery in October, January, April, and July of each fiscal year for projects to be delivered by Caltrans.

Caltrans and regions will also provide the Commission with a report on completed projects. Caltrans shall report this information at least semiannually. Each regional agency shall, in its RTIP, report on all STIP projects completed between adopting

the RTIP and adopting the previous RTIP. The report shall include a summary, by component and fund type, of the funds programmed, allocated, and expended when the construction contract was accepted. For projects with a total cost of \$50 million or greater or a total STIP programmed amount (in right-of-way and/or construction) of \$15 million or greater, the reports shall also include a discussion of the project benefits that were anticipated before construction, compared to an estimate of the actual benefits achieved. Caltrans or a regional agency may elect to defer the reporting of project benefits if it believes such a deferral is needed to assess the project benefits better. If reporting is deferred, Caltrans or the regional agency shall include a list of all the projects for which reporting has been deferred and indicate when it anticipates reporting.

In consultation with Commission staff, regional agencies, and county transportation commissions, Caltrans will develop a format and content requirement for the reports.

XI. STIP DEVELOPMENT SCHEDULE AND PROCEDURES

79. STIP Development Schedule

The following schedule lists the major milestones for the development and adoption of the STIP:

STIP Milestones	Date
Caltrans presents the Draft Fund Estimate to the California Transportation Commission (CTC).	By July 15 of each odd-numbered year
CTC adopts Fund Estimate.	By August 15 of each odd-numbered year
Caltrans submits the draft ITIP to the CTC.	By October 15 of each odd-numbered year
CTC ITIP hearing – North CTC ITIP hearing – South	By November 15 of each odd-numbered year
Regions submit their RTIPs to CTC Staff.	By December 15 of each odd-numbered year
Caltrans submits its ITIP to CTC Staff.	By December 15 of each odd-numbered year
CTC STIP hearing – North	Jan. – Feb. each even-numbered year
CTC STIP hearing – South	Jan. – Feb. each even-numbered year
CTC publishes staff recommendations.	At least 20 days prior to the adoption of the STIP
CTC adopts the STIP.	By April 1 of each even-numbered year

80. ITIP Hearings

Before Caltrans adopts and submits the final ITIP, the Commission will hold at least two hearings, one in northern and one in southern California, to provide opportunities for public input regarding projects proposed in the ITIP.

81. STIP Hearings

Before the adoption of the STIP, the Commission will hold two STIP hearings for Caltrans and regional agencies, one in northern California and one in

southern California. By statute, the hearings are “to reconcile any objections by any county or regional agency to the department’s program or the department’s objections to any regional program.” The Commission will expect any objections to the Caltrans program or to a regional program to be expressed in terms of the undesirable impact that the program would have on the implementation of the respective agency’s long-range transportation plan(s). The Commission expects regional agencies and Caltrans to discuss how the infrastructure projects included in each program help attain regional and statewide goals, including those in Governor’s Executive Orders B-30-15 B-32-15, and N-19-19, where applicable.

82. Transmittal of RTIPs

By statute, regional agencies must adopt and submit their RTIPs to the Commission and Caltrans no later than December 15 of odd-numbered years. The Commission requests that each region send an electronic copy to Kacey.Ruggiero@catc.ca.gov and one hard copy of its RTIP, addressed to:

Executive Director
California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Caltrans requests that each region send an electronic copy to Sudha.Kodali@dot.ca.gov, OCIP@dot.ca.gov, one hard copy to the appropriate Caltrans District Director, and one hard copy addressed to:

Chief, Division of Financial Programming
Attention: Office of Capital Improvement Program
Department of Transportation
Mail Station 82
P. O. Box 942874
Sacramento, CA 94274-0001

83. Transmittal of Draft and Final ITIP

By statute, Caltrans must submit its draft ITIP and final ITIP to the Commission no later than October 15 and December 15, respectively, of odd-numbered years. The Commission requests that Caltrans post the draft ITIP and the final ITIP on their website and send an electronic copy to Kacey.Ruggiero@catc.ca.gov and one hard copy of the draft ITIP and final ITIP addressed to:

Executive Director
California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

84. Commission Staff Recommendations

Commission staff shall prepare recommendations for the STIP's adoption at least twenty days before the adoption date. The staff recommendations will be made available to the Commission, Caltrans, and the regional agencies.

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XII. APPENDICES

Appendix A: Electronic Project Programming Request

A link to the ePPR tool may be found at:

<https://dot.ca.gov/programs/financial-programming/office-of-capital-improvement-programming-ocip>

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Appendix B: Performance Indicators and Measures

Use the following table B1 to indicate quantitatively the overall regional level performance of your Regional Transportation Plan (RTP), and where applicable, Sustainable Communities Strategy (SCS) or California Transportation Plan, and the Interregional Transportation Strategic Plan (ITSP). For regions outside an MPO, or a small MPO, the second table B1(a) may be used in addition to or as a replacement for B1.

If tables B1 and/or B1(a) are insufficient in indicating how progress towards attaining goals and objectives contained in each RTP, and where applicable, SCS, and the ITSP is assessed and measured, include the following information:

- List your performance measures.
- Provide a quantitative and/or qualitative analysis (include baseline measurement and projected program or project impact).
- State the reason(s) why selected performance measures are accurate and useful in measuring performance. Please be specific.
- Identify any and all deficiencies encountered in as much detail as possible.

For qualitative explanations, state how progress towards attaining goals and objectives contained in each RTP, SCS, and, where applicable, the ITSP is assessed and measured.

B1 Evaluation – Regional Level Performance Indicators and Measures			
Goal	Indicator/Measure	Current System Performance (Baseline)	Projected System Performance (indicate timeframe)
Congestion Reduction	Vehicle Miles Traveled per capita		
	Percent of congested Vehicle Miles Traveled (at or below 35 mph)		
	Commute mode share (travel to work or school)		
Infrastructure Condition	Percent of distressed state highway lane-miles		
	Pavement Condition Index (local streets and roads)		
	Percent of highway bridges by deck area classified in Poor condition		
	Percent of transit assets that have surpassed the FTA useful life period.		
System Reliability	Highway Buffer Index (the extra time cushion that most travelers add to their average travel time when planning trips to ensure on-time arrival)		
Safety	Fatalities and serious injuries per capita		
	Fatalities and serious injuries per Vehicle Miles Traveled		
Economic Vitality	Percent of housing and jobs within 0.5 miles of transit stops with frequent transit service		
	Mean commute travel time (to work or school)		
Environmental Sustainability	Change in acres of agricultural land		
	CO ₂ emissions reduction per capita		

B1(a) Evaluation			
Rural Specific Regional Level Performance Indicators and Measures			
Goal	Indicator/Measure	Current System Performance (Baseline)	Projected System Performance (indicate timeframe)
Congestion Reduction	Vehicle Miles Traveled per capita, area, by facility ownership, and/or local vs tourist		
	Peak Volume/Capacity Ratio or Thresholds (threshold volumes based on HCM 2010)		
	Commute mode share (travel to work or school)		
Transit	Total operating cost per revenue mile		
Infrastructure Condition	Distressed lane-miles, total and percent, by jurisdiction		
	Pavement Condition Index (local streets and roads)		
Safety	Total accident cost per capita and VMT		
Environmental Sustainability	Land Use Efficiency (total developed land in acres per population)		

Agencies may use the following table B2 to evaluate the cost-effectiveness of the RTIP or ITIP.

B2 Evaluation - Cost-Effectiveness Indicators and Measures			
Goal	Indicator/Measure (Per thousand dollars invested)	Current Level of Performance (Baseline)	Projected Performance Improvement (indicate time frame)
Congestion Reduction	Reduce Vehicle Miles Traveled per capita		
	Reduce percent of congested VMT (at or below 35 mph)		
	Change in commute mode share (travel to work or school)		
Infrastructure Condition	Reduce percent of distressed state highway lane-miles		
	Improve Pavement Condition Index (local streets and roads)		
	Reduce percent of highway bridge deck area in Poor Condition		
	Reduce percent of transit assets that have surpassed the FTA useful life period		
System Reliability	Reduce Highway Buffer Index (the time cushion added to average commute travel times to ensure on-time arrival)		
Safety	Reduce fatalities and serious injuries per capita		
	Reduce fatalities and serious injuries per Vehicle Miles Traveled		
Economic Vitality	Increase percent of housing and jobs within 0.5 miles of transit stops with frequent transit service		
	Reduce mean commute travel time (to work or school)		
Environmental Sustainability	Change in acres of agricultural land		
	CO ₂ emissions reduction per capita		

Agencies may use the following table B3 to identify by proposed project, or in summary for all proposed projects, changes to the built environment.

B3 Evaluation - Project Changes or Increased Capacity Benefits			
Project Type Or Mode	Change to the Built Environment	Indicator/ Measure	Benefits or Performance Improvement at Project Completion
State Highway	New general-purpose lane-miles		
	New HOV/HOT lane-miles		
	Lane-miles rehabilitated		
	New or upgrade bicycle lane/sidewalk miles		
	Operational improvements		
	New or reconstructed interchanges		
	New or reconstructed bridges		
Transit or Intercity Rail	Additional transit service miles		
	Additional transit vehicles		
	New rail track miles		
	Rail crossing improvements		
	Station improvements		
Local streets and roads	New lane-miles		
	Lane-miles rehabilitated		
	New or upgrade bicycle lane/sidewalk miles		
	Operational improvements		
	New or reconstructed bridges		

Appendix C: Addendum for LATIP Programs

ADDENDUM to STIP GUIDELINES Local Alternative Transportation Improvement Programs State Routes 84 and 238

Authority and Scope: Government Code Section 14528.56, added by Chapter 291 (AB 1386) of the Statutes of 2009, authorizes the California Transportation Commission (Commission) to incorporate into the state transportation improvement program guidelines additional guidelines specific to the local alternative transportation improvement program, and to adopt guidelines to establish a process to approve advancing a project, if the project is included in the local alternative transportation improvement program approved pursuant to Section 14528.5 or 14528.55 of the Government Code.

The Commission may amend these guidelines any time after first giving notice of the proposed amendments.

Development of the Local Alternative Transportation Improvement Program:

Sections 14528.5 and 14528.55 of the Government Code authorize the development of a local alternative transportation improvement program (TIP) to address transportation problems which were to be addressed by the planned state transportation facilities on State Highway Route 238 in the City of Hayward and Alameda County, and on State Highway Route 84 in the Cities of Fremont and Union City. The City and/or County will act jointly with the transportation planning agency to develop and file the local alternative TIP. Priorities for funding in the local alternative TIPs shall go to projects in the local voter-approved transportation sales tax measure.

The local alternative TIP must be submitted to the Commission prior to July 1, 2010.

All proceeds from the sale of the excess properties, less any reimbursements due to the federal government and all costs incurred in the sale of those excess properties (properties acquired to construct a new alignment for a freeway or expressway bypass to State Highway Route 238 in the City of Hayward and in the County of Alameda, and State Highway Route 84 in the Cities of Fremont and Union City) shall be allocated by the Commission to fund the approved local alternative TIP.

Administration of the Local Alternative TIP: Project funds programmed in the local alternative TIP shall be allocated and expended in the same manner as state funds made available for capital improvement projects in the state transportation improvement program (STIP) adopted by the Commission pursuant to Section 14529 of the Government Code. These funds shall not be subject to the formula distributions specified in Sections 164, 188 and 188.8 of the Streets and Highways Code.

Advancement of a Project in the Local Alternative TIP: A local agency may, with the concurrence of the appropriate transportation planning agency, the Commission, and the Department of Transportation (Department), advance a project included in the local alternative TIP prior to the availability of sufficient funds from the sale of respective excess properties, through the use of its own funds.

Advancement of a project or projects shall not change the priority for funding and delivery of all projects within each respective approved local alternative TIP.

A local agency may enter into an agreement with the appropriate transportation planning agency, the Department, and the Commission to use its own funds to develop, purchase right-of-way for, and construct a transportation project within its jurisdiction that is included in the respective local alternative TIP.

If the local agency uses local voter-approved sales and use tax revenues to advance a project, any reimbursement made shall be used for the same purposes for which the imposition of the sales and use tax is authorized.

Submittal of Advancement Request: Requests shall be submitted to the Department by the applicant in accordance with established timeframes for project amendments to be placed on the agenda for timely consideration by the Commission.

To be considered by the Commission, an advancement request shall:

- Be signed by a duly authorized agent(s) of the applicant agency and the implementing agency if different.
- Include all relevant information as described below.
- Indicate that the implementing agency is ready to start work on the project or project component.
- Have a complete and committed funding plan for the component covered by the advancement request.
- Indicate the anticipated schedule for expenditures and completion of the component.

Content and Format of Advancement Request: The Commission expects a complete request to include, at a minimum, the following information as applicable:

- A letter requesting advancement approval. The request shall include a summary of any concurrent actions needed from the Commission and a discussion of the source(s), amount, and funding commitment to be used to advance the project.
- Alternate local funding source(s) that will be substituted for the local alternative TIP funds and a demonstration of commitment of those funds (e.g., resolution, minute order) from its policy board.
- An expenditure schedule for the component covered by the advancement request.

- If jointly funded with STIP or Proposition 1B funds, a STIP or Proposition 1B allocation request, an AB 3090 request, or a Proposition 1B Letter of No Prejudice (LONP) request must be included.
- Requests to advance right-of-way purchase or construction must include documentation for Commission review of the final environmental document, as appropriate, and approval for future funding consideration.

Review and Approval of Advancement Requests: The Department will review advancement requests for consistency with these guidelines and place the request on the Commission meeting agenda.

Advancement will only be granted for work consistent with the approved project's scope, schedule, and funding.

Upon approval of the advancement, the Department will execute a cooperative agreement or Master Agreement/Program Supplement with the local agency before it can reimburse eligible project expenditures.

Initiation of Work: The project requested to be advanced shall be ready to proceed upon approval. The local agency shall report to the Department/Commission within four months following advancement approval on progress in executing agreements and third-party contracts needed to execute the work.

Allocations: The Commission will allocate funds for the advanced project when scheduled in the local alternative TIP, contingent on sufficient funds being available in the appropriate Special Deposit Fund. Pursuant to the agreement with the local agency, the Department shall reimburse the local agency for the actual cost of developing and constructing the project, including the right-of-way acquisition. Reimbursement of project development costs shall not exceed 20 percent of estimated construction costs, or any lesser amount mutually agreed to by the Department, Commission, and local agency. Interest and other debt service costs are not reimbursable.

In no case will an allocation be made that exceeds the amount of funds available in the respective account established in the Special Deposit Fund from the sale of excess properties from Route 84 or Route 238. The agency advancing the project accepts the risk that sufficient funds may not be realized from the sale of the excess properties to fully reimburse all project costs.

Appendix D: Title VI Requirements

TITLE VI REQUIREMENTS

All projects programmed in the STIP shall comply with the following provisions:

- The implementing agency assumes responsibility and accountability for the use and expenditure of program funds. Applicants and implementing agencies must comply with all relevant federal and state laws, regulations, policies, and procedures.
- The implementing agency will ensure that no person or group(s) of persons shall, on the grounds of race, color, national origin, sex, age, disability, limited English proficiency, or income status, be excluded, or otherwise subject to discrimination, related to projects programmed and allocated by the Commission, regardless of whether the programs and activities are federally funded. The implementing agency will comply with all Federal and State statutes and implementing regulations relating to nondiscrimination.
- A current list of Title VI/nondiscrimination and related authorities is available on the Commission's website at www.catc.ca.gov.

Appendix D: Glossary

- AB – Assembly Bill
- APDE – Advance Project Development Element
- CAPTI – Climate Action Plan for Transportation Infrastructure
- CEQA – California Environmental Quality Act
- CMGC – Construction Manager/General Contractor
- CMP – Congestion Management Program
- CO₂ – Carbon Dioxide
- CTC – California Transportation Commission
- CTP – California Transportation Plan
- ePPR – Electronic Project Programming Request
- FHWA – Federal Highway Administration
- FTA – Federal Transit Administration
- GARVEE – Grant Anticipation Revenue Vehicle
- GHG – Greenhouse Gas
- GPS – Global Positioning System
- HCM 2010 – Highway Capacity Manual 2010
- HOT – High-Occupancy Toll
- HOV – High-Occupancy Vehicle
- ITIP – Interregional Transportation Improvement Plan
- ITSP – Interregional Transportation Strategic Plan
- LONP – Letter of No Prejudice
- MPO – Metropolitan Planning Organization
- MTC – Metropolitan Transportation Commission
- NEPA – National Environmental Protection Act
- PA&ED – Project Approval and Environmental Document Phase
- PPM – Planning, Programming, and Monitoring
- PPNO – Project Programming Number
- PSR – Project Study Report
- PTA – Public Transportation Account
- RTIP – Regional Transportation Improvement Plan
- RTP – Regional Transportation Plan
- SACOG – Sacramento Area Council of Governments
- SB – Senate Bill
- SCAG – Southern California Association of Governments
- SCS – Sustainable Communities Strategy
- SHA – State Highway Account
- SHOPP – State Highway Operations and Protection Program
- STIP – State Transportation Improvement Plan
- TIP – Transportation Improvement Plan
- TMC – Transportation Management Center

- TMS – Transportation Management System
- UTA – Uniform Transit Application
- VMT – Vehicle Miles Traveled

DRAFT

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.4, Action

Prepared By: Paul Golaszewski
Chief Deputy Director

Published Date: June 13, 2025

Subject: Commission 2025-27 Strategic Plan

Recommendation:

California Transportation Commission (Commission) staff recommend the Commission approve the proposed Commission 2025-27 Strategic Plan (Strategic Plan).

Issue:

The proposed Strategic Plan is intended to guide the Commission in the coming years as it carries out its mission to invest in transportation that improves communities, the environment, and the economy, in fulfillment of its vision for a safe, equitable, and multi-modal transportation system for all Californians. The six goals outlined in the Strategic Plan provide a roadmap for executing this mission by: prioritizing safety; elevating community voices; ensuring a healthy environment for all; moving people and goods efficiently; partnering for success, and supporting a team where people continue to thrive.

Background:

At the direction of Commissioners, Commissions staff began developing the proposed Strategic Plan last summer after procuring development assistance from the California State University, Sacramento. In December, a draft mission statement and goals and objectives were presented to Commissioners at a public workshop and to the Interagency Equity Advisory Committee. The proposed Strategic Plan incorporates feedback received during both presentations, as well as additional feedback from Commission staff, partners, and stakeholders.

Attachment:

- Commission 2025-27 Strategic Plan and Staff Values



CTC

**2025-2027
Strategic Plan**

June 2025

INTRODUCTION FROM THE EXECUTIVE DIRECTOR

The investments the California Transportation Commission makes are critically important to improving the safety of everyone who drives, rides, walks, bikes, and rolls. Our investments shape a transportation system that can withstand the changing climate and provide a cleaner environment for future generations. We are using these investments to uplift communities and deliver equitable outcomes. Collectively, they create thousands of jobs and support a vibrant economy that has become the fourth largest in the world.

The Commission's 2025 Strategic Plan is the result of thoughtful input from Commission staff and executives, Commissioners, the Interagency Equity Advisory Committee, and our partners and stakeholders. The Plan will guide the Commission in the coming years as we carry out our mission to invest in transportation that improves communities, the environment, and the economy. The six goals outlined in the Strategic Plan provide a roadmap for executing this mission. Through this plan, we will:

- Prioritize safety
- Elevate community voices
- Ensure a healthy environment for all
- Move people and goods efficiently
- Partner for success, and
- Support a team where people continue to thrive

As we invest nearly \$12 billion annually, we will keep these goals at the forefront of our everyday work and continue our partnerships and collaborative efforts to move towards achieving our vision of a safe, equitable, and multi-modal transportation system for all Californians.



TANISHA TAYLOR
Executive Director



ABOUT THE COMMISSION

The Commission funds highway, local road, transit, intercity passenger rail, active transportation, and general aviation projects throughout California. The Commission consists of 11 voting members and two ex officio, non-voting members. Of the 11 voting members, nine are appointed by the Governor, one is appointed by the Senate Rules Committee, and one is appointed by the Speaker of the Assembly. The two ex officio members are appointed one by the Senate Rules Committee and one by the Speaker of the Assembly.

The Commission holds public meetings throughout California, at which time it reviews and approves projects for funding. The Commission also holds town hall meetings and workshops across California to engage the public and stakeholder groups. The Commission is primarily responsible for developing and coordinating transportation policies (consistent with state and federal laws); funding transportation projects through formulaic and competitive programs; ensuring timely project delivery by requiring agencies to request approval for project scope, schedule, and/or cost changes; evaluating and reporting on performance outcomes; approving applications for toll facilities; and approving various right-of-way matters such as property condemnations and relinquishments of portions of the state highway system to local control.



OUR VISION:

A safe, equitable, and multi-modal transportation system for all Californians.

OUR MISSION:

Invest in transportation that improves communities, the environment, and the economy.

Goals



GOAL 1: PRIORITIZE SAFETY

- **Invest in projects that:**
 - Reduce fatalities and injuries across all modes
 - Ensure safe pedestrian and bike routes
 - Protect California's transportation workforce

GOAL 2: ELEVATE COMMUNITY VOICES

- **Invest in projects that:**
 - Demonstrate meaningful and robust community engagement
 - Respond to needs of vulnerable and marginalized communities
- **Collaborate with the Equity Advisory Committee as a champion of community voices**
- **Educate the public about the role of the CTC**

GOAL 3: ENSURE A HEALTHY ENVIRONMENT FOR ALL

- **Invest in projects that:**
 - Reduce greenhouse gas emissions
 - Improve air quality
 - Prioritize multi-modal options
 - Align transportation and land use
 - Protect wildlife and natural and cultural resources
 - Advance climate resilience

GOAL 4: MOVE PEOPLE AND GOODS EFFICIENTLY

- **Invest in projects that:**
 - Support a vibrant economy
 - Expand accessibility and mobility options, including for individuals with disabilities
 - Reduce congestion
 - Enhance reliable freight throughput

GOAL 5: PARTNER FOR SUCCESS

- **Engage Tribal Governments for meaningful and timely input**
- **Collaborate with partners to minimize cost increases and delays**
- **Communicate how transportation investments benefit the public**

GOAL 6: SUPPORT A TEAM WHERE PEOPLE CONTINUE TO THRIVE

- **Continue to foster a culture where team members are respected and valued**
- **Strengthen HR function and invest in organizational development**
- **Create and communicate opportunities for growth and promotion**

GOAL 1: PRIORITIZE SAFETY

California relies on a complex transportation system to support its residents, businesses, and environment. Ensuring the safety of this system is about protecting lives, preserving public trust, maximizing access, and minimizing disruption.

As California faces new challenges including climate change, aging assets, and increased demand, prioritizing safety becomes ever more urgent. Over the coming years, the Commission will invest in projects that focus on reducing hazards for motorists, pedestrians, cyclists, individuals with disabilities, users of mass transit, and the individuals who build and maintain California's transportation system. The Commission also will prioritize those projects that enhance the personal safety of the communities impacted by the transportation system. By investing in projects that address critical safety needs, the Commission is committed to building and maintaining an infrastructure that supports mobility and economic growth while protecting the well-being of those who use it.



GOAL 2: ELEVATE COMMUNITY VOICES

The Commission recognizes that supporting an equitable and effective transportation system begins with listening to communities. As part of its strategic priority to Elevate Community Voices, the Commission will invest in projects that are shaped by inclusive, transparent, and meaningful public engagement. This includes transportation projects that come before the Commission that are informed by the diverse needs of residents, especially those living in vulnerable and marginalized communities. By supporting dialogue early and often, the Commission aims to build trust and make transportation funding more responsive to community needs.

Over the next three years, the Commission also will focus on public education, helping people understand how transportation decisions are made and how they can actively participate in shaping them. Through this approach, the Commission is not only improving infrastructure—it is serving as a listener, a collaborator, and a facilitator of community-driven change.



GOAL 3: ENSURE A HEALTHY ENVIRONMENT FOR ALL

The Commission's goal to Ensure a Healthy Environment for All will guide its investment decisions to promote environmental sustainability, public health, and social equity. The Commission is committed to supporting transportation projects that reduce pollution, address climate change, and improve quality of life. This includes advancing clean mobility options, promoting active transportation, and reducing greenhouse gas emissions—all while fostering economically and socially resilient communities.

The Commission will invest in projects that contribute to reduced greenhouse gas emissions, improved air quality, and increased multi-modal transportation options that support walking, biking, and public transit. It will support projects that align transportation and land use planning, protect wildlife and natural and cultural resources, and advance climate resilience. Together, these investments will help create a transportation system that supports a healthier, more sustainable future.



GOAL 4: MOVE PEOPLE AND GOODS EFFICIENTLY

The ability to move people and goods quickly, reliably, and safely is critical to maintaining a competitive California economy that ranks at the fourth largest in the world. Efficient transportation systems help reduce travel time, support commerce, and enhance connectivity across communities and industries.

To meet this strategic goal, the Commission will invest in projects that expand accessibility and mobility options, to create a transportation system that is inclusive and serves individuals with disabilities. These investments will allow residents to access jobs, schools, health care services, and to get to a host of other destinations essential for their well-being and quality of life in a reasonable amount of time and at a reasonable cost.

Additionally, the Commission aims to reduce congestion on roadways and enhance reliable freight throughput, improving the flow of goods across key trade corridors. This will support a vibrant economy, by focusing on infrastructure that fuels business activity and job creation. Through these targeted investments, the Commission seeks to create a more efficient and resilient transportation system.



GOAL 5: PARTNER FOR SUCCESS

The Commission wants to ensure that collaboration and coordination are central to its transportation planning and investments. To do this, the Commission is committed to developing and maintaining strong partnerships across all levels of government, as well as with community organizations, industry, and the public.

The Commission will actively engage Tribal Governments to ensure meaningful and timely input in transportation decision-making, respecting sovereignty and honoring unique cultural and historical perspectives. The Commission also will collaborate with project partners to reduce cost increases and avoid unnecessary delays, promoting efficiency and saving taxpayer dollars. Additionally, the Commission is committed to clearly communicating how transportation investments benefit the public, helping residents understand the value of infrastructure improvements and encouraging greater participation in shaping California's transportation future.



GOAL 6: SUPPORT A TEAM WHERE PEOPLE CONTINUE TO THRIVE

The Commission recognizes that its greatest asset is its staff – public servants who work tirelessly to advance the Commission’s mission. The Commission is committed to fostering a workplace where employees continue to thrive.

The Commission will continue its investment into the development of its internal organization, focusing on tools, systems, and strategies that enhance employee engagement and performance and support a resilient and high-performing organization. The Commission will create and clearly communicate opportunities for growth and promotion, to provide team members clear pathways to advance their careers. Through these efforts, the Commission dedicates itself to maintaining a workplace culture rooted in respect, equity, and continuous learning—one where people can grow and thrive, contribute meaningfully, and take pride in serving the public.



CTC Staff Values

Reference No.: 4.4
June 26-27, 2025

IMPACT

We value making a meaningful difference for communities across the state.

We create positive impact through:

- Equity and accessibility in action
- A focus on sustainable and economically beneficial solutions
- Program and policy expertise
- Collaboration for greater reach
- Active engagement with communities and partner organizations

LEADERSHIP

We foster the growth and development of our team as leaders by embracing a growth mindset.

To foster leadership, we:

- Provide opportunities for individual and career advancement
- Support continuous learning for professional growth
- Empower our staff to take initiative and own their work
- Encourage staff to “lead from the seat they’re in”
- Create an environment where diverse perspectives are valued and respected

INTEGRITY AND ACCOUNTABILITY

As public stewards, we conduct ourselves with honesty, consistency, and transparency.

To demonstrate integrity and accountability, we:

- Apply policies, decisions, and expectations consistently
- Take ownership of our actions and their outcomes
- Follow through on our commitments
- Clearly communicate decisions, rationale, processes, and methodologies
- Engage proactively with the public, communities, and partner organizations
- Seek feedback and refine our processes to uphold the highest standards of public service

COLLABORATION AND TEAMWORK

We value working together as a team and with partners to achieve our shared goals.

To achieve effective collaboration and teamwork, we:

- Communicate clearly
- Seek and respect diverse viewpoints to foster a positive work environment where everyone feels valued
- Build trust among our teams and with partners
- Listen actively
- Define roles and responsibilities
- Create a culture of constructive feedback
- Use shared tools and technology effectively
- Adapt to address changing circumstances



Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.5, Action

Prepared By: Naveen Habib
Associate Deputy Director

Published Date: June 13, 2025

Subject: 2024 Solutions for Congested Corridors Program Adoption –
Program of Projects, Resolution G-25-41

Recommendation:

Staff recommends the California Transportation Commission (Commission) adopt the 2024 Solutions for Congested Corridors Program of Projects, as presented in the project list (Attachment B), and consistent with Resolution G-25-41 (Attachment A).

Issue:

The Commission established the 2024 Solutions for Congested Corridors Program as a two-year, \$480,956,000 program covering fiscal years 2025-26 and 2026-27. Commission staff recommends funding seven projects totaling \$482,926,000. The recommended program is over-programmed by \$1,970,000, which will be offset by future project cost savings, consistent with the Solutions for Congested Corridors Program Guidelines. The recommended investments leverage an additional \$13.5 billion in local, state, and federal funds that will create over 180,000 jobs and advance the Commission's goals to support a vibrant economy, elevate community voices, move people and goods efficiently, ensure a healthy environment for all, and partner for success.

The investments recommended in the 2024 Solutions for Congested Corridors Program provide communities with multimodal options to safely and efficiently drive, bike, walk, and take transit to their destinations. Investments include a new passenger rail extension, bicycle and pedestrian safety improvements, bus rapid transit expansion, and system technology and mobility hub upgrades. From transit network upgrades to investments that reduce greenhouse gas emissions by allowing travelers to opt out of congestion, these projects demonstrate cross-agency collaboration, investment in cleaner and multimodal infrastructure, and the evolution of transportation systems across the state that respond to moving people efficiently and safely.

The seven recommended projects uphold the Commission's focus on community involvement and collaboration with the Interagency Equity Advisory Committee and the California Department of Housing and Community Development.

Projects not recommended for funding demonstrated travel benefits but were found less competitive for the following reasons: benefits that did not exceed those of recommended projects; misalignment with program objectives and guidelines; lacking project readiness; unclear or incomplete project application information; or insufficient support for state climate and equity goals.

The recommended project list was posted on the Commission's website on June 6, 2025, and is included as Attachment B.

Recommendations Development

The Commission received 19 project nominations totaling over \$1.2 billion in funding requests. Each project underwent a comprehensive review to assess corridor-level benefits, environmental readiness, public engagement, land use and housing alignment, matching fund commitments, and consistency with regional transportation plans and comprehensive multimodal corridor plans.

Commission staff led evaluations of nominated projects with support from California Department of Transportation (Caltrans) staff, using the criteria identified in the [2024 Solutions for Congested Corridors Program Guidelines](#) to determine the most competitive projects. In addition, California Department of Housing and Community Development staff evaluated land use efficiency and housing benefits and members of the Interagency Equity Advisory Committee evaluated community engagement narratives.

Commission staff welcome continued engagement from nominating agencies and encourage each agency to seek technical assistance to strengthen future submissions. Agencies that have reapplied after receiving such assistance have often seen improved outcomes in subsequent cycles.

Project Highlights

The seven recommended projects continue to shift towards multimodal investments that reduce congestion and support the state's goals on climate, equity, public health, efficient land use, and economic vitality. These projects include:

- Transit and rail investments (including new stations, track extensions, zero-emission transit fleet procurement and infrastructure)
- Active transportation facilities (including multi-use paths, bicycle/pedestrian safety improvements, complete streets)
- Operational improvements (including strategic managed lanes, new bicycle/pedestrian overcrossings bridges, intelligent traffic management technologies)

Brief descriptions of the scope and benefits of all seven projects are provided below.

- **BART Silicon Valley Phase II Extension (BSVII) Project in Santa Clara County (\$75,000,000)** extends BART six miles to Santa Clara via downtown San José, with four new stations and a maintenance facility. This high-capacity rail extension enhances access to jobs and services in one of California's largest employment centers, connects major regional rail lines, and strengthens mobility for disadvantaged and underserved communities in San José.

- **Watsonville-Santa Cruz Multimodal Corridor Program in Santa Cruz County (\$103,700,000)** delivers multimodal improvements across three main routes through investments that span bus-on-shoulder lanes and cross-county transit and coastal bike and pedestrian facility enhancements. The project supports first-and-last-mile connections to schools, jobs, and healthcare, promotes mode shift, and increases climate resilience in a coastal county.
- **Santa Barbara U.S. 101 Multimodal Corridor Project in Santa Barbara County (\$109,020,000)** completes the final segment of a multi-decade U.S. 101 corridor modernization and congestion reduction effort. Incorporates peak-hour rail and bus services, new electric vehicle charging stations, active transportation infrastructure, and high occupancy vehicle lane expansion, improving mobility between Santa Barbara and Ventura counties.
- **Metrolink Sustainable Locomotives Project in Southern California Counties (\$52,606,000)** is a vital regional rail project that will upgrade 12 aged-out and higher-polluting locomotives with cleaner Tier 4 locomotives, sustaining and enhancing Metrolink's ongoing service throughout the greater Southern California region. This project will eliminate more than two billion highway vehicle miles traveled on Southern California's heavily impacted freeways and arterials. With significant reduction in congestion, this project will deliver longstanding community needs for clean, affordable, and reliable Metrolink service, with higher frequencies connecting people to intercounty destinations daily.
- **Sonoma-Marin Area Rail Transit (SMART) Healdsburg Extension Project in Sonoma County (\$62,000,000)** will extend the Sonoma-Marin Area Rail Transit commuter rail to Healdsburg, reducing congestion on U.S. 101, improving multimodal connectivity, and expanding access to affordable housing, tourism, and economic centers. This project includes track upgrades to support both passenger and freight service which supports economic development in the region.
- **San Francisco Municipal Transportation Agency (SFMTA) Train Control Upgrade Project (TCUP) - Phase 1 in San Francisco County (\$41,000,000)** is the critical first phase of a nine-year, multi-phase modernization of the entire "Muni Metro" light rail system with Communications-Based Train Control (CBTC) technology, and will lead to increased tunnel capacity and reductions in transit delay. This project will serve the neighborhoods of Bayview/Hunters Point and Visitacion Valley, providing more cost-effective means of traveling for vulnerable communities and enhancing access to education and employment.
- **I-805 South Managed Lanes Conversion Project (HOV to HOT) in San Diego County (\$39,600,000)** will enhance safety and quality of life in San Diego's South Bay communities and for users connecting to residential and employment centers along the United States-Mexico Border and I-805 Corridor. This project will upgrade 8.5 miles of managed lanes from existing high occupancy vehicle lanes to high occupancy toll lanes using integrated operations and tolling systems. This project will generate toll revenue for investments in transit capital, provide safer and more efficient travel along a

binational corridor and improve access to employment and transit options in San Diego's South Bay communities.

Baseline Agreement Requirements

All funded agencies must comply with the Commission's 2024 Solutions for Congested Corridors Program Guidelines and [Senate Bill 1 \(SB 1\) Accountability and Transparency Guidelines](#).

All projects included in the 2024 Solutions for Congested Corridors Program must enter into a Baseline Agreement approved by the Commission. Baseline Agreement development is initiated upon program adoption. Baseline Agreements stipulate the agreed-upon expected benefits, delivery schedules, project costs, and funding plans. Baseline Agreements identify agencies that must comply with SB 1 reporting requirements and include cost-sharing agreements that detail how cost overruns will be covered financially by project sponsors and implementing agencies. Executed Baseline Agreements serve as benchmarks for comparison to the current status of a project for subsequent reporting purposes. Baseline Agreements must be signed by the Caltrans Director and District Director, Commission's Executive Director, project applicant, and implementing agency.

Background:

Created by SB 1 (Beall, Chapter 5, Statutes of 2017), the Solutions for Congested Corridors Program provides funding to reduce congestion in highly traveled and highly congested corridors through transportation improvements and innovations that expand access to travel choices, stimulate neighborhood vibrancy, preserve local community character, and provide environmental benefits. SB 1 authorizes \$250 million annually to fund the Solutions for Congested Corridors Program.

Beginning December 2023, Commission staff hosted four public workshops to solicit stakeholder feedback to develop the 2024 Solutions for Congested Corridors Program Guidelines. Commission staff prepared the 2024 Solutions for Congested Corridors Program Guidelines in consultation with the Interagency Equity Advisory Committee, Caltrans, California Air Resources Board, California Department of Housing and Community Development, regional transportation planning agencies, local agencies, advocates, and other stakeholders. On August 15, 2024, the Commission adopted the 2024 Solutions for Congested Corridors Program Guidelines, which describe the policy, standards, criteria, and procedures for the development, adoption, and management of the Solutions for Congested Corridors Program.

Commission staff hosted 24 virtual office hour sessions from March to May 2024 to provide technical assistance to applicants for project nominations for the 2024 Solutions for Congested Corridors Program.

Attachments:

- Attachment A: Resolution G-25-41
- Attachment B: 2024 Solutions for Congested Corridors Program - Program of Projects
- Attachment C: Comment Letters

**CALIFORNIA TRANSPORTATION COMMISSION
2024 SOLUTIONS FOR CONGESTED CORRIDORS PROGRAM ADOPTION
PROGRAM OF PROJECTS**

RESOLUTION G-25-41

- 1.1 **WHEREAS**, Senate Bill 1 (SB 1) (Beall, Chapter 5, Statutes of 2017) created the Solutions for Congested Corridors Program to fund projects included in regional comprehensive multimodal corridor plans that reduce congestion, expand access to travel choices, stimulate neighborhood vibrancy, preserve local community character, and provide environmental benefits; and
- 1.2 **WHEREAS**, \$250 million is appropriated annually from the State Highway Account to the Solutions for Congested Corridors Program; and
- 1.3 **WHEREAS**, on August 15, 2024, the Commission adopted the 2024 Solutions for Congested Corridors Program Guidelines, which describe the policy, standards, criteria, and procedures for the development, adoption, and management of the Solutions for Congested Corridors Program; and
- 1.4 **WHEREAS**, the Commission established the 2024 Solutions for Congested Corridors Program as a two-year, \$480,956,000 program; and
- 1.5 **WHEREAS**, on November 18, 2024, the Commission received 19 project nominations, totaling over \$1.2 billion in funding requests; and
- 1.6 **WHEREAS**, Commission staff prepared program recommendations that included \$482,926,000 for 7 projects representing total project costs of \$13.9 billion; and
- 1.7 **WHEREAS**, Commission staff recommendations are consistent with statute and conform to the 2024 Solutions for Congested Corridors Program Guidelines; and
- 1.8 **WHEREAS**, on June 6, 2025, Commission staff recommendations for the 2024 Solutions for Congested Corridors Program were posted on the Commission's website and made available to the Commission, California Department of Transportation, regional transportation agencies, county transportation commissions, and the public; and
- 1.9 **WHEREAS**, the Commission considered Commission staff recommendations and public testimony during its June 2025 Commission meeting.

- 2.1 **NOW THEREFORE BE IT RESOLVED**, that the Commission hereby adopts the 2024 Solutions for Congested Corridors Program, as reflected in Attachment B; and
- 2.2 **BE IT FURTHER RESOLVED**, that the amounts approved for project funding shall be considered a “not-to-exceed” amount, and any cost increases beyond the estimates reflected in the adopted program shall be the responsibility of the nominating agency; and
- 2.3 **BE IT FURTHER RESOLVED**, that projects included in the adopted 2024 Solutions for Congested Corridors Program must comply with the 2024 Solutions for Congested Corridors Program Guidelines and SB 1 Accountability and Transparency Guidelines; and
- 2.4 **BE IT FURTHER RESOLVED**, that projects included in the adopted 2024 Solutions for Congested Corridors Program must enter into a Baseline Agreement approved by the Commission; and
- 2.5 **BE IT FURTHER RESOLVED**, that if a recommended project requests an allocation for project components between the June 2025 and October 2025 Commission meetings, the project applicant must submit a Baseline Agreement for approval by the October 2025 Commission meeting; and that all remaining Baseline Agreements must be approved by the December 2025 Commission meeting; and
- 2.6 **BE IT FURTHER RESOLVED**, that the Commission may remove from the program any project for which a Baseline Agreement is not executed; and
- 2.7 **BE IT FURTHER RESOLVED**, that the Commission will not consider approval of a project allocation without an approved Baseline Agreement; and
- 2.8 **BE IT FURTHER RESOLVED**, that Commission staff, in consultation with the California Department of Transportation and project sponsors, is authorized to make minor technical changes to the 2024 Solutions for Congested Corridors Program to reflect the most current information or clarify the Commission’s programming commitments, and shall seek Commission approval for any substantive changes; and
- 2.9 **BE IT FURTHER RESOLVED**, that the Commission directs Commission staff to post the 2024 Solutions for Congested Corridors Program of Projects on the Commission’s website.

2024 SOLUTIONS FOR CONGESTED CORRIDORS PROGRAM
PROGRAM OF PROJECTS - PROJECTS RECOMMENDED FOR FUNDING (\$1,000s)
RESOLUTION G-25-41

County	Nominating Agency	Implementing Agency	Project Title	Project Description	Congested Corridor	Total Project Cost	Total Recommended	Fiscal Year
Sonoma	Caltrans	Sonoma-Marin Area Rail Transit (SMART) Healdsburg Extension Project			Rt 101	\$ 191,750	\$ 62,000	FY 25-26
Sonoma		Sonoma-Marin Area Rail Transit	Healdsburg Extension Stations and Facilities	14 grade separations / rail crossing improvements 1 new station 5.5 miles of rehabilitated track 5.5 miles of pedestrian / bicycle facilities		\$ 185,750	\$ 56,000	FY 25-26
Sonoma		Sonoma-Marin Area Rail Transit	Healdsburg Extension Zero Emission Switcher Locomotive	1 rail / transit equipment		\$ 6,000	\$ 6,000	FY 25-26
Santa Cruz	Caltrans	Watsonville-Santa Cruz Multimodal Corridor Program			Rt 1	\$ 283,210	\$ 103,700	FY 26-27
Santa Cruz		Caltrans (District 5)	SR 1 - Freedom to State Park Auxiliary Lanes, Bus on Shoulders, and Coastal Rail Trail Segment 12 Project (Contract #1)	3 modified / reconstructed bridges 2.5 operational improvements 2.3 miles of auxiliary lanes 1.15 miles of pedestrian / bicycle facilities 2 sq. ft. of pedestrian bridges		\$ 231,844	\$ 68,574	FY 26-27
Santa Cruz		Santa Cruz County	Soquel Drive Multimodal Project - State Park Drive to Freedom Boulevard (Contract #2)	30 accessible pedestrian signals installations 16 new curb ramps 7 crosswalks 2 intersection / signal improvements 4.8 miles of rehabilitated local roads 4.8 miles of bicycle lanes 1 mile of sidewalk		\$ 26,917	\$ 21,290	FY 26-27
Santa Cruz		Santa Cruz Metropolitan Transit District	Local Roadway Rapid Bus Improvements – Soquel Drive and Highway 152 (Contract #3)	60 transit signal priority intersections 57 real-time transit information enhancements 6 crosswalks 2 intersection / signal improvements 0.6 miles of slow vehicle lanes		\$ 24,449	\$ 13,836	FY 26-27
Santa Barbara	Caltrans	Santa Barbara U.S. 101 Multimodal Corridor Project			Rt 101	\$ 216,092	\$ 109,020	FY 25-26 FY 26-27
Santa Barbara		Caltrans (HQ)	Segment 4E North - Cabrillo Interchange	8 new curb ramps 5 crosswalks 4 ramp modifications 2 crossing islands 2 shoulder widenings 1 interchange modification 1 intersection / signal improvements 3 miles of mixed flow mainline 1.5 miles of HOV/HOT mainline 1.5 miles of mainline shoulders 0.9 miles of ramps and connectors 0.3 miles of sidewalk 0.1 miles of reconstructed local road 0.1 miles of pedestrian / bicycle facilities 24,202 sq. ft. of modified / improved interchanges 290 sq. ft. of new detectable warning surfaces installations 1,439.6 linear feet of new sidewalk 973.2 linear feet of culverts		\$ 140,038	\$ 54,200	FY 25-26
Santa Barbara		City of Santa Barbara	Cabrillo Boulevard Pedestrian Improvements	0.5 miles of new track 0.1 miles of roadway lane 0.1 miles of bicycle lane 16,400 sq. ft. of modified / reconstructed bridges / tunnels 2,100 linear feet of new sidewalk 600 linear feet of culverts		\$ 57,949	\$ 38,403	FY 26-27
Santa Barbara		City of Santa Barbara	Santa Barbara Zero Emission Vehicle Charging Infrastructure	8 DC charging ports		\$ 1,777	\$ 1,662	FY 25-26
Santa Barbara		City of Santa Barbara	Santa Barbara Eastside Active Transportation Improvements	46 crosswalks 33 signs, lights, greenway, or other safety enhancements 0.85 miles of sidewalk 1,650 linear feet of new crosswalk		\$ 10,328	\$ 8,755	FY 26-27
Santa Barbara		Santa Barbara County Association of Governments	VCTC Coastal Express Zero-Emission Buses	3 zero-emission rail cars / transit vehicles		\$ 6,000	\$ 6,000	FY 26-27
San Diego	Caltrans	Caltrans (District 11)	I-805 South Managed Lanes Conversion Project (HOV to HOT)	50 traffic monitoring detection stations	Rt 805	\$ 131,000	\$ 39,600	FY 26-27
San Francisco	Caltrans	City and County of San Francisco	SFMTA Train Control Upgrade Project (TCUP) - Phase 1	1 communication based train control system	Rt 101 Rt 280	\$ 155,175	\$ 41,000	FY 26-27
Various	Southern California Association of Governments	Southern California Regional Rail Authority	Metrolink Sustainable Locomotives Project	12 rail cars / transit vehicles	Various	\$ 150,779	\$ 52,606	FY 25-26
Santa Clara	Santa Clara Valley Transportation Authority	Santa Clara Valley Transportation Authority	BART Silicon Valley Phase II Extension (BSVII) Project	48 rail cars / transit vehicles 4 station improvements 6 miles of new track	Rt 80 Rt 101	\$ 12,745,607	\$ 75,000	FY 25-26

Total 2024 SCCP Projects Programmed	7
Total 2024 SCCP Project Costs	\$ 13,873,613
Total 2024 SCCP Funds Programmed	\$ 482,926
Total 2024 SCCP Funding Capacity	\$ 480,956
Recommendation Oversubscribed	\$ (1,970)



June 9, 2025

Commissioner Darnell Grisby, Chair
California Transportation Commission
1120 N Street MS 52
Sacramento, CA 95814

RE: Central Coast Coalition Support for CTC SB1 Cycle 4 Staff Recommendations

Dear Chair Grisby & Members of the California Transportation Commission:

The Central Coast Coalition strongly supports the California Transportation Commission (CTC) staff recommendations for grant funding allocations from the Senate Bill 1 (SB1) Cycle 4 competitive programs. We thank Commission staff for their collaborative effort to develop program guidelines and select projects during a very short timeframe so that we can expeditiously build these much-needed projects:

Sponsor Agencies	Project Title	SB1 Program(s)	Recommendation
California Department of Transportation and Santa Barbara County Association of Governments	Santa Barbara U.S. 101 Multimodal Corridor Project	Local Partnership Program – Competitive and Solutions for Congested Corridors Program	\$134 million
California Department of Transportation and Santa Cruz County Regional Transportation Commission	Watsonville-Santa Cruz Multimodal Corridor Program	Local Partnership Program – Competitive and Solutions for Congested Corridors Program	\$128.7 million
California Department of Transportation and Transportation Agency for Monterey County	State Route 156 Castroville Boulevard Interchange Project	Local Partnership Program - Competitive and Trade Corridor Enhancement Program	\$90.6 million

The Coalition appreciates the support for recommended projects listed above. The Central Coast also had projects not recommended for Cycle 4 funding, such as the Highway 46/Union Road overpass. This project and others will be updated and revised, and we would appreciate consideration in future SB1 cycles.

As the area's regional transportation planning agencies in San Benito, Santa Cruz, Monterey, San Luis Obispo, and Santa Barbara counties, in partnership with the Association of Monterey Bay Area Governments, we highly value the SB1 revenues. These grants will help deliver truly transformative transportation projects in our region.

We strongly support the CTC staff recommendations and urge the Commission to approve the funding at their June 26, 2025, meeting.

If you have any questions, please contact SBCAG Executive Director, Marjie Kirn, at (805) 600-4497 or by email at mkim@sbcag.org.



Central Coast Coalition
Moving California's Economy

We greatly appreciate our partnership with Commissioners and CTC staff.

Sincerely,

Marjie Kirn, Executive Director
Santa Barbara Association of Governments

Pete Rodgers, Executive Director
San Luis Obispo Council of Governments

Todd Muck, Executive Director
Transportation Agency for Monterey County

Sarah Christensen, Executive Director
Santa Cruz Co. Regional Transportation
Commission

Binu Abraham, Executive Director
San Benito Council of Governments

Maura Twomey, Executive Director
Association of Monterey Bay Area Governments

cc: Mr. Toks Omishakin, Secretary, California State Transportation Agency
Ms. Tanisha Taylor, Executive Director, California Transportation Commission
Mr. Mike Keever, Acting Director, California Department of Transportation
Mr. Paul Golaszewski, Chief Deputy Director, California Transportation Commission
Mr. James Anderson, Chief, Programming, California Department of Transportation
Mr. Scott Eades, District 5 Director, California Department of Transportation
Mr. Matthew Yosgot, Deputy Director, California Transportation Commission
Ms. Naveen Habib, Associate Deputy Director, California Transportation Commission
Ms. Leishara Ward, Associate Deputy Director, California Transportation Commission
Ms. Beverly Newman-Burckhard, Associate Deputy Director, California Transportation Commission
Mr. Darron Hill, Deputy District Director, Asset and Program Project Management, California Department of Transportation
Mr. Joe Erwin, Office Chief, Project Management, California Department of Transportation
Mr. Sarkes Khachek, SBCAG Director of Programming, Coalition Chief of Staff



Western Regional Office
409 13th St.
Suite 600
Oakland, CA 94612
510-992-4662

May 30, 2025

Matthew Yosgott
Deputy Director - SB 1 Programming
California Transportation Commission (CTC)
1120 N Street MS 52
Sacramento, CA 95814

RE: Applications of Merit for SB 1 Funding – Supporting Bay Area Trails

Dear Deputy Director Yosgott,

Rails to Trails Conservancy writes to encourage CTC staff to recommend funding for two active transportation project applications for SB 1 funding in the San Francisco Bay Area under the Solutions for Congested Corridors Program (SCCP) and the Local Partnership Program (LPP) that we have listed below.

RTC is the largest trail organization in the country and is working to connect all people, everywhere by trails and safe active transportation infrastructure. RTC advocates for low-stress, traffic-separated, and multimodal trails, to create new accessible, sustainable, and affordable pathways for mobility.

RTC chairs the Bay Area Trails Collaborative (BATC), which comprises more than 50 organizations, agencies, and businesses seeking to develop a 2,600-mile [regional trail network](#) that would connect the nine-county Bay Area. Two [priority projects](#) for BATC applied for SB 1 funding, the Bay Skyway and SMART Pathway.

We encourage you to recommend funding for the following two projects to progress completion of the Bay Area regional trail network.

1. **SCCP Project 1: Multimodal Bay Skyway.** Given the impactful nature of a transbay multiuse path connecting Oakland and San Francisco, the Bay Skyway is one of 12 high priority projects for BATC. The completion of the Bay Skyway would be transformative for the region, as it creates new avenues for congestion relief, expands commuter choice, and opens greater access to cultural, educational, recreational, and employment opportunities. As the crow flies, the downtown areas of San Francisco and Oakland are a mere 6 miles apart, but



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today there are no direct routes to walk, bike, or roll between these destinations. The Bay Skyway would provide a scenic and stress-free route for transbay commuters, as it is sited along and a key segment of the beautiful San Francisco Bay Trail. RTC is additionally supportive of the Bay Skyway as moderators of the Friends of the Bay Trail, a group of advocates, city & county staff, and other professionals working to advance the 500-mile Bay Trail.

2. **SCCP Project 2 and LPP Project 35: SMART Healdsburg Extension.** When complete, the SMART Pathway will connect bicyclists and pedestrians to jobs, schools, transit hubs, housing and other destinations via a 71.3-mile multiuse trail alongside the SMART commuter rail system in Sonoma and Marin counties. This essential spine of Sonoma and Marin's active transportation network is a BATC priority project. Filling the remaining gaps will result in increased connectivity throughout the region and boost the current usership. Additionally, the SMART Pathway was officially designated as the southern portion of the 307-mile Great Redwood Trail.

We appreciate the opportunity to provide comments and amplify the BATC regional trail network as commission staff reviews the SB 1 applications. We look forward to further engagement with CTC staff and commissioners once the staff recommendations are released.

Thank you for your consideration.

Sincerely,

A handwritten signature in black ink, appearing to read "J. Voremberg".

Jesse Voremberg
jesse@railstotrails.org
Trail Development Manager, Western Region
Rails to Trails Conservancy

A handwritten signature in black ink, appearing to read "Iman Sylvain".

Iman Sylvain, PhD
isylvain@railstotrails.org
Director, Western Region
Rails to Trails Conservancy

ClimatePlan TRANSFORM



May 28, 2025

Matthew Yosgott
Deputy Director - SB 1 Programming
California Transportation Commission (CTC)
1120 N Street MS 52
Sacramento, CA 95814

Re: Applications of Concern for SB 1 Funding (SCCP, TCEP, LPP Programs)

Dear Deputy Director Yosgott,

The undersigned organizations write to express concerns about a number of highway expansion project applications for SB 1 funding under the Solutions for Congested Corridors Program (SCCP), Trade Corridor Enhancement Program (TCEP), and the Local Partnership Program (LPP) that we have listed below. Our concerns about these projects relate to their inconsistency with the Climate Action Plan for Transportation Infrastructure (CAPTI), their likelihood to significantly increase driving, their environmental, climate, and displacement impacts, and their conflicts with California's climate and equity goals. We urge the Commission to scrutinize these projects closely and only fund the components of these projects that reduce driving, improve

multimodal options, and advance the zero-emission fleet transition. Several of the projects listed are opposed locally by organizations in the ClimatePlan network that are also signed on to this letter. We have linked to opposition letters to specific projects in the footnotes.

Solutions for Congested Corridors Program (SCCP)

We are very pleased to see the high number of transformative multimodal and vehicle miles traveled (VMT) reducing projects and components of projects among the list of applications for SCCP this cycle. We particularly appreciate the innovative project applications that aim to improve highway operations in congested corridors without adding new lanes, such as the ***I-805 South Managed Lanes HOV to HOT Conversion Project*** which will reinvest toll revenue into transit and active transportation infrastructure.

We encourage the CTC to only fund projects and project components that reduce VMT and avoid adding new lanes to highways and roadways. Specifically, we urge the CTC not to fund the following highway expansion projects and project components:

1. **SR 1 – Freedom to State Park Auxiliary Lanes¹ (Also an LPP and TCEP application, comments apply to that program as well)** - This auxiliary lane expansion project is a component of the Watsonville-Santa Cruz Multimodal Corridor Project and is opposed by local advocates challenging the project. As articulated in the Sierra Club chapter letter linked in the footnote, this application for SCCP/TCEP/LPP has some multimodal components that enjoy support from the community and we encourage the CTC to fund these components without the auxiliary lanes.
2. **101 Multimodal Corridor Segment 4E North – Cabrillo Interchange (Also an LPP and TCEP application, comments apply to that program as well)** - This expansion project is a component of the Santa Barbara US 101 Multimodal Corridor Project and was opposed by local advocates. This application for SCCP/TCEP/LPP also has many strong multimodal components that enjoy support from the community and we encourage the CTC to fund these components of this project. Since most of the corridor has already been widened, we recognize that restricting lanes in this short segment could decrease air quality. However, the HOV lanes have limited effectiveness because of their limited time duration and operational requirements for only 2 or more occupants. We encourage CTC to fund the HOV component only with expanded HOV operational requirements for additional hours and 3 or more occupants.
3. **I-5 Managed Lanes Multimodal Operational Improvements (Orange County)** - This project adds 16 lane-miles of new HOV lanes that will exacerbate freeway congestion and air quality issues without a clear commitment to invest in transit alternatives. We would support the portion of this project that converts existing HOV lanes to express lanes if those components could be funded separately from the lane expansion components and if toll revenues were committed to providing multimodal alternatives.

¹ [Sierra Club to CTC on SR-1](https://drive.google.com/file/d/1mzuzGdvoWviBsOHhuy8rzwJOPGmgb4ql/view?usp=sharing)
<https://drive.google.com/file/d/1mzuzGdvoWviBsOHhuy8rzwJOPGmgb4ql/view?usp=sharing>

4. **SR 37 Sears Point to Mare Island Improvement Project – Phase 2² (Also an LPP and TCEP application, comments apply to all three programs)** - The sign on letter linked in the footnote demonstrates the local advocate opposition to this “interim” lane expansion project, and states that “the interim project will cost over \$500M and will be under water caused by sea level rise within fifteen years of completion. Continuing the interim project in its current form further delays, rather than advances, the long-term vision for the corridor.” We have serious concerns about investing any state funding in a roadway that may be inoperable as soon as 2040.
5. **I-15 Express Lanes Project Southern Extension (ELPSE)³ (Also an LPP application, comments apply to that program as well)** - This project will add over 60 lane-miles to I-15 in a congested and growing region and does not include multimodal and VMT reducing components. As detailed in the comment letter on this project’s draft environmental impact report linked in the footnote, this project will induce significant VMT and relied on outdated and flawed methodology for estimating air quality and other impacts.
6. **I-680 NB Express Lane Completion Phase 1⁴** - This express lane expansion project is a component of the Innovate 680 project. Though this SCCP application appears to only be requesting funds for the Shared Mobility Hubs and TDM Augmentation components of the larger project, we still want to highlight the concerns raised by Transform in their comment letter to Caltrans regarding flawed analysis in the draft EIR for this project, linked in the footnote.

Trade Corridor Enhancement Program (TCEP)

We are again pleased to see the increase in TCEP applications this cycle that would advance freight electrification, technology, and efficiency at ports and truck parking hubs. We strongly support investing TCEP funding in these types of projects that are well-aligned with CAPTI and reduce harmful impacts to communities that bear the brunt of pollution from the freight industry. We encourage the CTC to focus funding only on those types of projects and avoid funding any projects that increase passenger VMT by adding general purpose lanes to roadways.

In addition to the projects listed above that are applying for both SCCP and TCEP, we have concerns about the following TCEP applications:

1. **SR 60/World Logistics Center Pkwy Interchange** - This project entails a significant expansion and new connection for an existing interchange that will open up land for more warehouse and sprawl development and induce significant new passenger VMT.

² Sign on comment letter SR-37 <https://transformca.org/wp-content/uploads/2024/09/Sign-on-Letter - Oppose-Backdoor-Streamlining-for-HW-37.pdf>

³ [Sign on comment letter I-15 DEIR https://drive.google.com/file/d/1P8Tr5ZF3zWvByCTWY_bwlo957Ab7Q3jM/view?usp=sharing](https://drive.google.com/file/d/1P8Tr5ZF3zWvByCTWY_bwlo957Ab7Q3jM/view?usp=sharing)

⁴ [Sign on comment letter Innovate 680 DIER https://transformca.org/wp-content/uploads/2024/07/I-680-NB-Express-Lanes-Comment-Letter.docx.pdf](https://transformca.org/wp-content/uploads/2024/07/I-680-NB-Express-Lanes-Comment-Letter.docx.pdf)

2. **Konocti Corridor - Segment 2B** - This project will add 16 general purpose lane-miles to convert the existing conventional highway between Kelseyville and Clear Lake to a four-lane expressway and will induce passenger VMT.
3. **Tulare SR 99 Corridor and Paige Avenue Multimodal Interchange Enhancements** - We do not support the capacity-increasing general purpose lane additions to SR 99 which will induce significant passenger and truck VMT. The project as approved further separates South City of Tulare and the neighboring unincorporated community of Maheny Tract by providing the transportation system for the growth of the City's industrial zones which separates the aforementioned communities. The Project does not improve connectivity for pedestrians and cyclists. At the bare minimum the project proposes a shared paved path, not even a sidewalk, between Blackstone St. and Laspina St, which do not extend into the residential communities to the East and West of the project site. We recommend a project alternative which will result in reduced traffic congestion while also improving pedestrian and active transportation options for the community, instead of facilitating industrial growth and highway capacity for freight.
4. **State Route 46 Antelope Grade Corridor Improvements Project** - This project will add five general purpose lane-miles to convert the existing conventional highway over Antelope Grade to a four-lane expressway and induce passenger VMT.
5. **State Route 132 Phase 3A Project** - This project will add 14 new general purpose lane-miles to extend a new expressway that creates a bypass for SR 132 and opens up new land for warehouse and sprawl development and induce new passenger VMT.
6. **State Route 71 Gap Closure Phase 2 Project (*Also an LPP application, comments apply to that program as well*)** - This project will convert an existing arterial highway into an 8-lane freeway, adding a general purpose and HOV lane in each direction. We do not support the highway lane addition portions of this project, which will induce significant passenger VMT and limit the freight benefits. However, we support the components of the project that provide sound walls and noise mitigation, update rail bridges to current standards, and enhance a pedestrian overcrossing.
7. **SR 46 East/Union Road Intersection Improvements** - This project entails a new interchange that will open up land for sprawl development and induce significant new passenger VMT.
8. **680/SR-4 Interchange Improvement (*Also an LPP application, comments apply to that program as well*)** - This project will add a direct connector with two general purpose lanes to the interchange of I-680 and SR-4 without mitigation for increases to passenger VMT.

Local Partnership Program (LPP)

In addition to the projects listed above that have applied for SCCP and/or TCEP in addition to LPP, we have concerns about the following additional application for LPP:

1. **Madera 41 South Expressway Project** - This project will add eight general purpose lane-miles to convert the existing conventional highway to a four-lane expressway, increasing access to land that is rapidly converting to sprawl development and significantly increasing passenger VMT. Rather, we urge the CTC to support projects

which truly meet the purpose of the LPP which includes addressing aging infrastructure, road conditions, active transportation, transit and rail, and ensuring health and safety benefits. Madera County is currently unable to meet existing transportation needs, including maintenance and transit of existing communities. Funding additional roadways for the purpose of sprawl contradicts the purpose of the LPP, will increase emissions, and divert critical funding needed to support the current transportation system in Madera County.

We appreciate the opportunity to provide comments and amplify project concerns from local organizations as commission staff reviews the SB 1 applications. We look forward to further engagement with CTC staff and commissioners once the staff recommendations are released.

Thank you for your attention to our concerns and for your leadership in guiding California toward a more sustainable and equitable transportation future.

Sincerely,

Zack Deutsch-Gross
Transform

Mary Lim, J.D.
Genesis: Interfaith Organizing in the East Bay

Jeanie Ward-Waller
ClimatePlan

Marven Norman
Center for Community Action and Environmental Justice (CCA EJ)

Jared Sanchez
CalBike

Elizabeth Reid-Wainscoat
Center for Biological Diversity

Chance Cutrano
Resource Renewal Institute

Matthew Baker
Planning and Conservation League

Janet Cox
Climate Action California

Sofia Rafikova
Coalition for Clean Air

Heather Deutsch
MOVE Santa Barbara County

Mike Swire
Stop the 101 & 280 Widenings in San Mateo County

Iman Sylvain, PhD
Rails to Trails Conservancy

David Diaz, MPH
Active San Gabriel Valley

Nick Ratto
350 Bay Area Action - Transportation Lead

Carter Rubin
Natural Resources Defense Council (NRDC)

Carter Lavin
Transbay Coalition

Justin Hu-Nguyen
Bike East Bay

David Levitus
LA Forward

Hana Creger
The Greenlining Institute

Aditi (Adi) Varshneya
California Environmental Justice Alliance (CEJA) Action

Emma De La Rosa
Leadership Counsel for Justice and Accountability

Jonathan Matz
Safe Routes Partnership

Marc Vukceovich
Streets For All

Kevin Shin
California Walks

Rick Longinotti
Campaign for Sustainable Transportation

Suzanne Hume
CleanEarth4Kids.org

Jamie Pew
NextGen California

Jorge Rivera
Healing & Justice Center

cc:
Executive Director Tanisha Taylor
Chair Darnell Grisby
Vice-chair Clarissa Falcon

To: California Transportation Commission

From: Residents of Aptos, Santa Cruz County

May 17, 2025

Dear Commissioners,

We live in Aptos, the location of proposed auxiliary lanes on Highway 1. We are among the people who are supposed to benefit from this project, in the form of reduced congestion on Hwy 1. We experience considerable congestion on Hwy 1, but according to the Project EIR, the morning commute will actually get worse in the opening year of the project. The EIR estimates:

- Northbound AM peak average traffic speed declining from 30 miles per hour to 26 mph in the opening year of the project.

The EIR predicts an improvement in travel time for the Southbound PM peak period in the opening year. However, the EIR says nothing about how long that improvement would last, except that there is no improvement by 2045.

The insignificance of congestion relief benefit was predicted by the Tier I EIR (2019) that studied the "TSM Alternative", defined as auxiliary lanes and ramp meters from Santa Cruz to Freedom Blvd, including the Aptos segments in this Project:

- The TSM Alternative "would result in a very slight improvement in traffic congestion when compared to the No Build Alternative" (Draft EIR page 2.1.5-16)

The Project EIR claims that there will be a safety benefit from adding auxiliary lanes. This contradicts the Tier I EIR (2019):

- "The total accident rates overall and by segment in 2035 under the Tier I Corridor TSM Alternative would be the same as the accident rates for the No Build Alternative." (Draft EIR page 2.1.5-17)

The absence of safety benefit predicted by the Tier I EIR makes sense. The Caltrans Highway Design Manual recommends auxiliary lanes for safe merging when the distance between interchanges is less than 2000 feet. The interchanges in this Project are a mile apart.

An important question is *why, if the Tier I EIR predicted "very slight" congestion relief, and no safety benefit, did Santa Cruz County leaders proceed with the auxiliary lanes project?*

Whatever the reason, it is not too late to change course. In Aptos, Hwy 1 runs through a finger of redwood forest that extends from the Forest of Nisene Marks towards the coast. The EIR states that 1100 trees will be cut down for this project, including some grand redwoods. It is a costly loss for a Project that will not succeed in reducing congestion.

These magnificent trees shield us from some of the traffic noise on Hwy 1. The project will reduce our quality of life and the value of our property.

The Commission decided not to fund this project during the last cycle of SB 1 grant funding. Please do so again.

Thank you for your consideration,

Mike Saint
516 Santa Marguarita Dr.

Peter Niboli
345 Moosehead Dr.

Elissa Wagner
528 Encino Dr.

Christina Grant
785 Loma Prieta Dr.

Nathan Bietz
520 Santa Marguarita Dr.

via email

May 2, 2025

Naveen Habib
Associate Deputy Director
California Transportation Commission
Solutions for Congested Corridors Program (SCCP)
1120 N Street, MS-52
Sacramento, CA 95814

RE: Caltrans/Santa Cruz County Regional Transportation Commission (RTC)
SCCP Cycle 4 Project ID#3, Three Projects, Total Request \$98,100,000

Dear Ms. Habib -

Thank you for the opportunity to provide public comment on the fourth cycle of the Solutions for Congested Corridors Program (SCCP), a program of California's comprehensive transportation funding program (SB1, 2017).

The purpose of this letter is to highlight the absence of adequate public transparency about the proposed project to Widen Highway 1 with Auxiliary and Bus-on-Shoulder Lanes between State Park Drive and Freedom Boulevard in Santa Cruz County ("Project"). The Project includes two additional components: install bus transit facilities on Soquel Drive, a parallel county arterial, and complete the Rail Trail Segment 12 per the *Monterey Bay Sanctuary Scenic Trail Master Plan*, 2014. This letter is focused on the Highway 1 Project.

1. 2016 Santa Cruz County Measure D - 30-Year Local Transportation Sales Tax.

The 2016 Measure D ½ cent countywide sales tax is currently accruing about \$23M/year for transportation projects and fixed annual allocations to local jurisdictions and transit providers. Measure D explicitly included adding north and southbound auxiliary lanes on Highway 1 between three interchanges in the Santa Cruz Mid-County area: Soquel Avenue < - > 41st Avenue, Bay Ave./Porter St. < - > Park Avenue, and Park Ave. < - > State Park Drive - skipping over the complex, closely-located interchanges in Soquel Creek Gulch between 41st Avenue and Bay/Porter in the City of Capitola.

In late 2019/early 2020, just as the Covid-19 pandemic was hitting, the RTC and its member agencies acted to administratively amend Measure D (allowed in the Measure) by adding new auxiliary lane segments in the Aptos area between three interchanges beyond the original Measure D limits: State Park Drive < - > Rio Del Mar Blvd. < - > Freedom Blvd. This extension of the Measure D Highway Program has, so far, roughly doubled demand that the Highway program asserts on Measure D revenues.

Since the 2020 amendment, RTC communications have consistently and intentionally sought to mislead the public by re-casting the original three segments in the [Measure D](#) Highway Program as the ones resulting from the administrative amendment: Soquel Avenue < - > 41st Ave., Bay/Porter < - > State Park Drive, and State Park < - > Freedom Blvd. Due to these RTC efforts

within the context of Covid-19, members of the public now largely believe that they voted to widen the highway between Soquel Avenue and Freedom Boulevard, which is not the case.

Aside from other impacts highlighted below, the fiscal impact of the 2020 administrative amendment is that the Measure D program now projects a large cash-flow deficit requiring significant borrowing and debt service.

2. 2019 State Route 1 Auxiliary Lane Bus-on-Shoulder Concept of Operations.

Subsequent to voter approval of Measure D, Santa Cruz Metro and the RTC promoted the idea that in addition to using new auxiliary lanes as a strategy to reduce travel times for bus routes traversing the Santa Cruz Mid-County region, buses also be allowed to use highway shoulders through interchanges. Metro had collaborated with Monterey-Salinas Transit in Monterey County on AB946 (2013) that allowed those transit agencies to study Bus-on-Shoulder (BOS) applications in the Monterey Bay region.

In 2019, prior to the Measure D administrative amendment noted above, a [*Bus-on-Shoulder Concept of Operations*](#) study for Highway 1 was completed by a consultant to the RTC. The study asserted that “[i]f the hybrid auxiliary lanes/BOS project were implemented, Santa Cruz Metro would increase the number of busses, which would result in as many as ten buses per hour using the BOS facilities during a weekday period...using the entire proposed BOS facility between Morrissey and Freedom Boulevards.” (p.3-7) While this assertion made no sense within the context of Santa Cruz and Watsonville urbanized area transit use patterns and needs, the ‘ConOps’ established a Santa Cruz-style rationale for the highway widening projects.

Santa Cruz Metro’s [*Rapid Corridors Report*](#) (January 2024) wisely eliminated implementation of Bus-on-Shoulder operations between the Highway 1/Soquel Drive interchange and State Park/Rio Del Mar Blvd., instead proposing a “Rapid Bus” concept along Soquel Drive, a major county arterial parallel to Highway 1; this project is part of the SCCP Cycle 4 Project application. There is therefore no current plan for buses to use the Auxiliary and Bus-on-Shoulder lanes between Soquel Drive and State Park - the SCCP Cycle 2 funded project. In fact, it was stated at yesterday’s RTC meeting that the RTC is “working on a [Bus-on-Shoulder] plan with Metro” with a timeline of 2026. The Director of Community Bridges, a local paratransit provider, immediately stood up and requested authorization to use the vacant BOS lanes for their paratransit vans. The first segment of the Cycle 2 project opened last month with vibrant red paint on shoulder sections reserved for BOS.

Beyond these oral comments yesterday, to date, no update to the 2019 Bus-on-Shoulder *ConOps* plan has been presented on the RTC’s public agenda and members of the public have not otherwise been informed about Metro and the RTC eliminating BOS from the Cycle 2 project.

3. 2024 Caltrans EIR/EA for SR 1 Auxiliary Lanes and Bus-on-Shoulder Improvements Freedom Blvd. to State Park Drive and Coastal Rail Trail Segment 12 Project, Project Submittal, Splitting the Project, and Lawsuit.

Together with Caltrans and the County of Santa Cruz, the RTC is a primary applicant for [*SCCP Cycle 4 funding for the Project*](#). In February 2024, following Caltrans approval of the environmental document for the highway widening, the RTC voted unanimously to agendize the Caltrans *Final EIR/EA* for public consideration and RTC findings. To date, the Caltrans-sponsored Project environmental review document has not been presented to the RTC or to the

public via the RTC. Additionally, no detailed Project information, beyond one-page Fact Sheets, has been made available for public review and comment about topics such as cost estimates, design, environmental and project development challenges. No item authorizing RTC submittal of an application for SCCP funding to the California Transportation Commission has appeared on a public agenda; the Santa County Board of Supervisors acted to authorize their SCCP Cycle 4 Project application on October 8, 2024.

During a November 7, 2024, RTC Public Hearing advertised for a 5-Year Plan for Measure D and a 'Senate Bill 1 Cycle 4 Update', and amidst acrimonious discussion about cost-overruns accruing to the current Cycle 2 project, Caltrans and the RTC agreed to "split" the proposed Cycle 4 Project in two: Caltrans would be the lead agency for the state highway aspect of the Project and the RTC would be the lead agency for Segment 12 of the Coastal Rail Trail. Since that time, no public RTC agenda has included authorization for the RTC to sign a Memorandum with Caltrans ([11/7/2024 RTC Agenda, Item 27, Exhibit G, p.27-18](#)) as presented at the November 2024 meeting, nor have there been any updates about follow-up items that the Memorandum stated would be finalized "by the end of March 2025."

The Project is the subject of an active lawsuit in Sacramento Superior Court (24WM000051/Campaign for Sustainable Transportation and Sierra Club, Petitioners) that challenges the Project's *Final EIR/EA*, with the most recent filing by Petitioners on April 7, 2025. The *Final EIS/EA* concedes that there is only a "slight" (0.1%) improvement in travel times with the [\\$300M+ project](#) (current Cycle 4 Project estimate per undated RTC Fact Sheet).

4. Comprehensive Multimodal Corridor Plan Required by SCCP Guidelines.

The RTC's 2019 [Unified Corridor Study](#) is still inappropriately identified by the RTC as its *Comprehensive Multimodal Corridor Plan* for the Cycle 4 Project. Please refer to my letter of January 27, 2023 in this regard.

Thank you again for the opportunity to comment.

- Linda Wilshusen
SCCRTC Executive Director, 1985-2005
l-j-w@pacbell.net

CC: SCCRTC, Caltrans District 5

STATE CAPITOL
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COMMITTEES
BUDGET
COMMUNICATIONS AND
CONVEYANCE
TRANSPORTATION
UTILITIES AND ENERGY
WATER, PARKS, AND WILDLIFE
BUDGET SUBCOMMITTEE NO. 4 ON
CLIMATE CRISIS, RESOURCES,
ENERGY, AND TRANSPORTATION

April 9, 2025

Tanisha Taylor, Executive Director
California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814

Subject: Support for the SMART Healdsburg Extension Project Applications for Solutions for Congested Corridors Program/Local Partnership Program Competitive Funds

Dear Executive Director Taylor,

I am writing to express my support for funding the completion of Sonoma-Marín Area Rail Transit District's (SMART) Healdsburg Extension through the grant applications submitted by Caltrans and the Metropolitan Transportation Commission to the Solutions for Congested Corridors Program (SCC) and by SMART to the Local Partnership Competitive Program (LPP-C).

In 2008, the voters of Marin and Sonoma County made a commitment by approving a ¼ cent sales tax measure to provide funding for the construction and operations of a 70-mile rail and pathway service between Larkspur and Cloverdale. Through 2024, the voters have provided \$550 million of their hard-earned money towards that purpose.

With the opening of the SMART Windsor Station upcoming Spring 2025, SMART's system will have 48-miles and 14 stations of passenger rail open in service, with 36-miles of the Great Redwood Trail/SMART Pathway open to the public. This request before you completes the work to Healdsburg and advances the completion of the entire system, including non-motorized path and enhanced freight rail service capabilities system-wide. There are just over four years left in SMART's 2008 sales tax measure. We should make every effort to complete construction of the rest of the SMART system before asking the voters to reauthorize the sales tax to pay for ongoing operations of the system.

I respectfully urge your favorable consideration of these SCC and LPP-C funding requests to match our voters' commitment to a healthier transportation future. Please feel free to contact my office at 707-576-2526 if you have any questions or require additional information.

Sincerely,

CHRIS ROGERS
Assemblymember, Second District

CR: mb



**SANTA CRUZ COUNTY GROUP
of the Ventana Chapter**

P.O. Box 604, Santa Cruz, CA 95061

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California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814
c/o Doug Remedios Douglas.Remedios@catc.ca.gov

December 30, 2024

Dear Commissioners,

The Sierra Club writes to support part of the Santa Cruz County Regional Transportation Commission (SCC RTC) application for transit, bicycle, and pedestrian improvements, and to oppose the part of the application that would expand Highway 1 for auxiliary lanes in Aptos, a project that the Environmental Impact Report (EIR) estimates will actually increase traffic delay in the morning commute and provide only short-term reduction in delay in the afternoon commute.

First, we support the project on Soquel Drive that includes ten miles of protected or buffered bike lanes, closing gaps in sidewalks, ten new pedestrian beacon crossings, and transit signal prioritization at 22 intersections. Soquel Drive was the main artery between Santa Cruz and Watsonville prior to the construction of State Highway 1, and remains the major locus of employment and connection to residential neighborhoods. We also support funding for Segment 12 of the rail trail in Aptos. We know this is a good investment because the parts of the rail trail that have been built are well utilized by bicyclists and pedestrians.

The Sierra Club is disappointed that the SCC RTC's grant application combines these worthy projects with the auxiliary lane project. These projects were also combined in the Cycle 3 grant application in 2022, which did not receive funding. We request that the California Transportation Commission consider funding the Soquel Drive and rail trail portion of the grant, and decline funding for the auxiliary lanes. The fact that Caltrans and the SCC RTC have separated the highway project from the rail trail project may facilitate the separation of grant funds. The auxiliary lane project does not satisfy the state's criteria for grant funding.

1. The project would not reduce congestion.

The Emeritus Executive Director of the SCC RTC (1985-2006), Linda Wilshusen, commented on the EIR for the auxiliary lanes, "The data in the DEIR/EA itself demonstrates no sustained traffic-relief benefit from this Project. The Project does not accomplish the stated Purpose and Need."

The Draft EIR estimates the project will worsen congestion in the morning commute, "*Implementation of the Build Alternative is expected to increase daily Vehicle Hours Traveled and vehicle hours of delay in northbound direction [Watsonville to Santa Cruz]. In the afternoon commute, any improvement in congestion is estimated to be short-lived, "Compared to the No-Build Alternative, the level of service for the Build Alternative improves for the southbound PM peak direction in the year 2025 but no improvements were seen in the year 2045. [Note that the EIR publishes no data for years between 2025 and 2045]*"

The highway widening will add significantly to the cost of the popular Rail & Trail projects in Santa Cruz County. Widening the highway will require the construction of much longer (and therefore more expensive) pedestrian and bicycle bridges over the highway. Even more alarming, the two existing rail bridges that are in perfectly good shape (per the April 2024 bridge analysis) will need to be destroyed and replaced with expensive longer rail bridges if the highway is widened. So, the highway widening, which will increase VMT and GHG emissions and not improve travel time, will delay and add considerable expense to two projects that *would* reduce VMT, reduce GHG emissions, as well as provide potentially speedier ways to travel than our congested highway.

2. The Auxiliary Lanes Project is not a Bus-on-Shoulder project.

The proposed project is not “bus-on-shoulder” as implemented in Minneapolis, Atlanta, Miami, and Cleveland. The defining characteristic of bus-on-shoulder is bus-only lanes. The sole bus-only portions of the proposed project are short segments across two overpasses. The rest of the way buses will travel in auxiliary lanes, sharing the lanes (and traffic delay) with other vehicles. We know from experience that new auxiliary lanes become congested. (See photo of the newest auxiliary lane from Morrissey to Soquel.) The insignificant length of bus-only segments on a congested highway will not reduce bus travel time sufficiently to attract more riders to use transit.

3. The county’s study does not comply with the state’s Comprehensive Multimodal Corridor Guidelines (2018)

The SCC RTC submits that its Unified Corridors Investment Study (2019) satisfies the requirement for a Comprehensive Multimodal Corridor Plan, which must reflect a comprehensive approach to reducing congestion. Far from being comprehensive, the RTC’s final study failed to include feasible short-term transit strategies recommended by its consultant, transit planner Jarrett Walker.

In 2018 Jarrett Walker told the SCC RTC, *“You have a very immediate debate over whether you want to begin providing competitive transit service...For a community of your size and your density, let alone the degree of progressive values that operate in this community, you do not have very much transit...We know that simply a higher level of service would be useful to a lot more people and would be having a lot more benefit particularly in the Santa Cruz-Watsonville corridor.”*

In spite of Walker’s advice, the [Final Unified Corridors Investment Study](#) eliminated the strategy of *increased frequency of transit with express service* on Soquel Dr./Freedom Blvd. [the main artery other than Hwy 1] that was included in the Draft Study (available in the SCC RTC agenda packet for 5/4/2017). The final study also leaves out other transit strategies that were included in the Draft: *transit signal prioritization; queue jumping; faster boarding; relocating parking to improve bike and transit options.*

4. The auxiliary lanes project EIR is deficient and is the subject of litigation.

In 2019, Caltrans certified a Tier I EIR that analyzed a series of auxiliary lanes along the 8.9 mile segment of Hwy 1, including the State Park to Freedom Blvd auxiliary lanes that are the object of this grant application. The Sacramento Superior Court ordered Caltrans to set aside its approval of the Tier I project in a decision filed on August 12, 2022. The Sierra Club and Campaign for Sustainable Transportation have sued Caltrans over the State Park to Freedom Blvd. asserting that the auxiliary lanes are tiered from an EIR that the court invalidated.

The lawsuit also contests Caltrans' claim that this project is exempt from the requirement to analyze and mitigate increases in vehicle miles traveled.

The Solutions to Congested Corridors Program Guidelines asks, "Will the project reduce or minimize vehicle miles traveled while maximizing person throughput in the corridor? If so, how?" Caltrans' claim of exemption from analyzing VMT increases means that this question cannot be adequately addressed.

The Office of Planning and Research (OPR) published the *Technical Advisory on Evaluating Transportation Impacts in CEQA*. It includes auxiliary lanes among "Project types that would likely lead to a measurable and substantial increase in vehicle travel generally include:

- Addition of through lanes on existing or new highways, including general purpose lanes, HOV lanes, peak period lanes, **auxiliary lanes**, or lanes through grade-separated interchanges. [emphasis added]

5. Summary

The Sierra Club supports the part of the SCC RTC application which is for transit, bicycle, and pedestrian improvements, and opposes the part of the application that would expand Highway 1 for auxiliary lanes in Aptos.

Thank you for your consideration,

Michael Guth,
Chair, Santa Cruz Group of the Ventana Chapter of the Sierra Club

cc:

Matthew Yosgott

Deputy Director - Programming - SB1
California Transportation Commission
Email: Matthew.Yosgott@catc.ca.gov

Naveen Habib

Associate Deputy Director
California Transportation Commission
Email: Naveen.Habib@catc.ca.gov



AFFILIATED AGENCIES

*Orange County
Transit District*

*Local Transportation
Authority*

*Service Authority for
Freeway Emergencies*

*Consolidated Transportation
Service Agency*

*Congestion Management
Agency*

December 2, 2024

Ms. Tanisha Taylor
Executive Director
California Transportation Commission
1120 North Street, MS-52
P.O. Box 942873
Sacramento, CA 95814

Dear Ms. Taylor:

On behalf of the Orange County Transportation Authority (OCTA), I strongly support the joint nomination by the Southern California Association of Governments and the Southern California Regional Rail Authority (SCRRA) for the Metrolink Sustainable Locomotives Project. The Solutions for Congested Corridors Program (SCCP) funding will replace 12 aging Tier 2 diesel locomotives with new Tier 4 diesel locomotives, reducing harmful emissions by up to 76 percent. This project is a crucial step toward Metrolink's goal of becoming one of the cleanest regional rail agencies in the United States.

Under the California Air Resources Board In-Use Locomotive Regulation, Metrolink is required to replace its Tier 2 locomotives by January 1, 2031. SCRRA's Alternative Compliance Plan requires the replacement of these 12 locomotives with Tier 4 models and SCCP funding is needed to complete this project.

Without this funding, Metrolink will face a 26 percent reduction in service, impacting millions of riders and adding over two billion vehicle miles traveled to congested highways, along with significant increases in greenhouse gas emissions and other pollutants. Metrolink's "Reimagined" service plan, launched in October 2024, expanded service and connectivity, but this expansion is at risk without the new locomotives.

The project also supports Metrolink's transition to zero-emission operations by utilizing renewable diesel fuel and enabling hybrid locomotive technology. This modernization will improve regional mobility, reduce congestion, and enhance safety, both on the rail and on the highways. We respectfully request the California Transportation Commission to partner with the South Coast Air Quality Management District and the region to help modernize the Metrolink fleet and ensure sustainable, reliable, passenger rail service.

Ms. Tanisha Taylor
December 2, 2024
Page 2

If you have any questions, please contact Rose Casey, Executive Director of Planning, at rcasey@octa.net or (714) 560-5729.

Sincerely,



Darrell E. Johnson
Chief Executive Officer

DEJ:cb

California Legislature



November 19, 2024

The Honorable Tanisha Taylor
Executive Director
California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814

Re: Letter of Support for the BART Silicon Valley Phase II Extension Project

Dear Director Taylor,

I am writing to express my support for the Santa Clara Valley Transportation Authority's (VTA) BART Silicon Valley Phase II (BSVII) Extension project and urge the California Transportation Commission (CTC) to award the requested \$75 million through the Solutions for Congested Corridors Program (SCCP) and \$25 million through the Local Partnership Program (LPP). These crucial funds are essential to advancing a project of this magnitude, which will significantly improve rail connectivity, enhance access to major employment centers and growing urban areas, reduce congestion on major highways and promote sustainable transportation options in Santa Clara County and throughout the Bay Area.

VTA's BSVII project will extend BART service from the Berryessa/North San José station to the City of Santa Clara, adding six miles of new track and constructing four new stations at 28th Street/Little Portugal, Downtown San José, Diridon Station and Santa Clara. This extension will provide a much-needed sustainable alternative to personal vehicle trips, easing traffic congestion and providing reliable, accessible, equitable and affordable transit options for tens of thousands of daily commuters.

Public Benefits of the BSVII Project:

- Traffic Congestion Reduction: The project will move vehicle trips off major highway corridors in the project area, including US 101, I-680 and I-880. The project is identified in the US 101 Comprehensive Multimodal Corridor Plan.
- Rail Connectivity: The project will fulfill the vision of 'Ring the Bay with Rail' and improve intermodal connectivity between the different parts of the Bay Area by linking BART, Caltrain, Capitol Corridor, and Altamont Corridor Express at Diridon Station.
- Addressing Climate Change: The project will reduce approximately 50 tons greenhouse gas (GHG) emissions (2040) by reducing barriers to rail and transit options and reducing the number of vehicle trips in the project corridor.
- Economic Growth and Job Creation: The project will spur economic growth and job creation in the region, creating more than 100,000 direct and indirect jobs. Bay Area commuters will gain

access to 1 million Jobs in Silicon Valley. 2 million people in Santa Clara County will gain access to 3.5 million Bay Area Jobs.

- Equity and Access: The project directly serves priority populations by improving access to affordable travel options and connecting 1.7 million transit dependent riders with equitable access to jobs, educational opportunities and essential services.

As our region continues to grow, the need for sustainable and efficient transit options becomes increasingly urgent. The BSVII project is a key component of the long-term strategy to meet this demand while reducing our reliance on personal vehicles and supporting the state's goals for reducing GHG emissions.

I strongly encourage the CTC to prioritize funding for this regionally significant project. BSVII will deliver lasting benefits to the residents of Santa Clara County and the entire Bay Area by improving mobility, supporting economic development and contributing to a more sustainable future.

Thank you for your consideration.

Sincerely,



Dave Cortese
Senator, 15th District



Aisha Wahab
Senator, 10th District



Ash Kalra
Assemblymember, 25th District



Evan Low
Assemblymember, 26th District



Gail Pellerin
Assemblymember, 28th District

October 29, 2024

California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814



Re: Support for SBCTA ONT Connector Project

Dear Commissioners:

On behalf of Inland Action, Inc, a non-profit, non-partisan corporation of public-spirited leaders who have joined together to be catalysts for the economic well-being of the Inland Empire region of California since 1962, I would like to express our support of the San Bernardino County Transportation Authority's (SBCTA) application to the 2024 Solutions for Congested Corridors Program (SCCP) for the Ontario International Airport (ONT) Connector Project. Since obtaining local control of the airport from Los Angeles in 2016, ONT has been named the fastest growing airport in the United States providing non-stop commercial service to over 26 major airports. This innovative regional project will construct a four-mile subsurface tunnel from ONT to the Metrolink commuter rail station in Rancho Cucamonga and will use "on-demand" autonomous electric shuttles to create a "last mile" connection to ONT.

The ONT Connector will reduce congestion along Interstates 10 and 15 and local streets and roads, encouraging a mode shift from single-occupancy vehicles to public transit for travelers to and from the Ontario International Airport. Additionally, the project will improve air quality by reducing greenhouse gas emissions and air pollution; improve safety; enhance accessibility; promote access for disadvantaged populations; and support economic development, job creation and retention, and efficient land use development in the area.

We appreciate SBCTA's proven performance and continued commitment to delivering quality projects and programs for the benefit of the State and the Inland Empire region of Southern California, and we urge you to fund the SCCP grant application from SBCTA for the ONT Connector Project. Thank you for your consideration of this important project that aims to improve mobility for generations to come.

Sincerely,



Kevin Dyer, Chair



Mike Burrows, Transportation Chair



Julie Michaels, Exec. Dir.

cc: Tanisha Taylor, Executive Director

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.6, Action

Prepared By: Beverley Newman-Burckhard
Associate Deputy Director

Published Date: June 13, 2025

Subject: 2024 Trade Corridor Enhancement Program Adoption – Program of Projects,
Resolution G-25-42

Recommendation:

Staff recommends the California Transportation Commission (Commission) adopt the 2024 Trade Corridor Enhancement Program of Projects, as presented in the project list (Attachment B), and consistent with Resolution G-25-42 (Attachment A).

Issue:

The California Department of Transportation (Caltrans) initially estimated that the 2024 Trade Corridor Enhancement Program would have \$1,071,000,000 in available funding. This estimate was based on state funds from projected diesel excise tax revenues and federal funds from the National Highway Freight Program apportioned through the Infrastructure Investment and Jobs Act. In August 2024, the Commission adopted the 2024 Trade Corridor Enhancement Program Fund Estimate, establishing the program as a two-year, \$900,389,000 program covering fiscal years 2025-26 and 2026-27. The Fund Estimate accounted for \$170,611,000 in supplemental funding and advance programming actions, which reduced the total funding available compared to the original estimate. Since that time, the Commission has approved an additional \$128,867,000 in supplemental funding and advance programming actions, further reducing the total available for programming to \$771,522,000.

Commission staff recommends funding 24 projects totaling \$810,474,000 in 2024 Trade Corridor Enhancement Program funding, and with total project costs of \$2,541,757,000. The recommended program is over-programmed by \$38,952,000, which will be offset by future project cost savings, consistent with the Trade Corridor Enhancement Program Guidelines.

Additionally, two projects previously programmed in the Advance 2024 Trade Corridor Enhancement Program with a cumulative total of \$185,000,000 in funding will be transitioned into the 2024 Trade Corridor Enhancement Program. The projects include the I-80/US 50 Managed Lanes Project in Yolo County, programmed with \$105,000,000 for one component, and the Coastal Rail Infrastructure Resiliency Project, programmed with \$80,000,000 for two components. With this action, the total programmed amount for the 2024 Trade Corridor

Enhancement Program increases to \$995,474,000, with total project costs of \$3,054,960,000. The overprogrammed amount remains unchanged, as projects programmed in the Advance 2024 Trade Corridor Enhancement Program were accounted for in the identified programming capacity.

The recommended investments support the movement of goods by enhancing the infrastructure that connects California's marine ports, inland ports, railyards, and freight corridors, ensuring goods flow efficiently across local, regional, national, and global supply chains. By leveraging an additional \$1.7 billion in local, state, and federal funds, the projects will boost California's global economic competitiveness while generating over 33,000 jobs in communities across the state. The investments support rail expansion and upgrades, shifting more freight from trucks to rail and easing congestion and emissions on California's highways. Collectively, the projects reduce greenhouse gas emissions and improve air quality by reducing idling for trucks, cars, and trains, and by rerouting trucks out of vulnerable neighborhoods.

Continuing the progress made in the previous program cycle, the 2024 program includes \$94.2 million for eight medium- and heavy-duty zero-emission vehicle infrastructure projects that will reduce greenhouse gas emissions and improve air quality for communities. The eight recommended projects will add 509 charging ports, increasing the existing number of available medium- and heavy-duty charging stations by 25 percent statewide.

Safety is a central focus of the investments, which will reduce fatalities and injuries by eliminating at-grade road and rail crossings to prevent collisions, replacing high-collision intersections with roundabouts to reduce vehicle speeds, and upgrading roadway design with better sight distances and safer curves and slopes. The projects will also enhance the safety of people walking and biking by removing freight traffic from neighborhood streets, and by constructing protected multiuse paths and bicycle and pedestrian overcrossings.

The recommended project list was posted on the Commission's website on June 6, 2025, and is included as Attachment B.

Recommendations Development

The Commission received 43 project nominations totaling \$1,347,201,000 in funding requests. Commission staff reviewed all applications for compliance with the screening criteria identified in the [2024 Trade Corridor Enhancement Program Guidelines](#), including compliance with program goals and statute, demonstration of required matching funds (if applicable), inclusion in an adopted regional transportation plan, and consistency with a Sustainable Communities Strategy approved by the California Air Resources Board (if applicable).

Commission staff, in collaboration with Caltrans staff, evaluated project nominations using the criteria identified in the 2024 Trade Corridor Enhancement Program Guidelines. Additionally, members of the Interagency Equity Advisory Committee evaluated community engagement narratives.

The 24 recommended projects demonstrated strong freight system benefits, including increased throughput, velocity, and reliability. Additionally, projects recommended for funding

demonstrated robust complementary benefits including, but not limited to, improved safety, congestion mitigation, economic growth, and enhanced community livability.

Projects not recommended for funding demonstrated benefits but were found to be less competitive for a variety of reasons, including, but not limited to, unclear freight system benefits, limited narrative describing project benefits, unclear or missing information, and lack of project readiness. Commission staff will contact all unsuccessful nominating agencies to hold application debriefs, and to encourage participation in technical assistance opportunities in future program cycles.

Project Highlights

The 24 recommended projects reflect the Commission's commitment to investing in a multimodal freight system that strengthens California's position in the global economy while supporting the state's climate, equity, and public health goals. Collectively, the recommended projects:

- Modernize outdated interchanges with better geometry, turn pockets, roundabouts, and flyovers, which increase freight throughput, reduce delays, and eliminate conflict points between freight vehicles and other road users.
- Divert truck traffic out of neighborhoods and onto trade corridors through new alignments and connectors, thereby reducing air pollution, improving safety in vulnerable communities, and decreasing delays while increasing throughput.
- Enhance rail infrastructure with extended mainline tracks, new crossovers, and on-dock rail expansion, which will increase rail capacity, reduce cargo transport time by facilitating the transfer of cargo from ships to trains efficiently, and promote rail worker safety.
- Replace dangerous at-grade crossings with overpasses or underpasses, which prevent collisions, eliminate long delays and queuing, and enhance the safety of people walking and biking.
- Construct zero-emission freight infrastructure, advancing the state's transition to zero-emission freight while improving air quality in communities.
- Provide complementary multimodal improvements such as Class I multiuse paths physically separate from freight traffic, funding for vanpool and expanded transit, and zero-emission passenger vehicle infrastructure, improving air quality and safety for communities while enhancing access to jobs, education, and critical services.
- Leverage over \$400,000,000 in committed federal discretionary funds.

Specific project examples include:

- **Centennial Corridor Southbound State Route 99 to Westbound State Route 58 Connector Project in Kern County (\$39,900,000):** This project enhances freight connectivity in the Central Valley by constructing a connector ramp from southbound State Route 99 to westbound State Route 58. Currently, freight traffic and travelers must navigate neighborhood streets to make this connection. The project will improve freight throughput, reduce delays, and enhance connectivity to planned inland ports in

Shafter, Mojave, McFarland, and Barstow. Additionally, the project will improve air quality and enhance safety in underserved communities by removing freight traffic from neighborhoods.

- **Gage Zero – Accelerating Zero Emission Fleet Charging on Priority Freight Corridors Project in Alameda, San Bernardino, Fresno, and San Diego Counties (\$20,177,000):** This project will install 146 medium-and heavy-duty zero-emission vehicle charging ports at four sites located in Alameda, Fresno, San Bernardino, and San Diego counties. The sites are located along critical freight corridors identified in the Senate Bill 671 Clean Freight Corridor Efficiency Assessment, including Interstates 5, 10, 15, 580, and 880, as well as State Routes 11, 60, 99, and 905. The charging facilities will be in close proximity to critical freight hubs, including the Port of Oakland, Ontario International Airport, Otay Mesa Port of Entry, and several major warehouses.
- **High Desert Corridor Operational Efficiency Project in San Bernardino County (\$30,770,000):** This project enhances national supply chains by linking the Ports of Los Angeles and Long Beach to inland and national networks. The project will construct two staging tracks, add a third 11.2-mile mainline track, and implement signal improvements. These improvements will reduce freight travel time by allowing trains to pass each other quickly and will increase freight rail capacity. The project will also improve air quality by reducing train idling, enhance safety for rail workers, and improve Metrolink and Amtrak services by reducing delays.
- **Hueneme Road Widening – Edison Drive to Rice Avenue in Ventura County (\$11,340,000):** This project improves freight system efficiency by eliminating a bottleneck on a critical truck route connecting to the Port of Hueneme. The project will widen Hueneme Road from two to four lanes, add turn lanes, and install a paved median. The project will remove approximately 1,000 daily truck trips from neighborhood streets in disadvantaged communities, improving air quality and safety.
- **Long Beach-East Los Angeles Corridor Zero-Emissions Truck Project In Los Angeles County (\$13,653,000):** This project will construct two medium- and heavy-duty zero-emission vehicle charging stations near the Ports of Los Angeles and Long Beach. The project includes the installation of 133 charging ports across the two sites. The stations will be strategically located along critical freight corridors identified in the Senate Bill 671 Clean Freight Corridor Efficiency Assessment, including Interstates 710, 5, and 10, State Routes 47, 60, and 91, and United States Route 101. In addition to the ports, the project will be located near BNSF and Union Pacific railyards, major warehouses, and transload facilities.
- **State Route 84 / United States Route 101 Interchange Reimagined Project in San Mateo County (\$14,165,000):** This project improves freight access to the Port of Redwood City, the San Francisco Bay Area's primary source of construction materials. The project will reduce truck delay and improve reliability by eliminating a five-leg intersection, constructing direct flyover ramps, signalizing ramp intersections, and adding longer turn pockets. The project also improves safety for community members by constructing Class I bike paths that are physically separated from freight traffic, adding new sidewalks, and upgrading rail crossings with new signals and gates.

- **State Route 156 Castroville Boulevard Interchange Project in Monterey County (\$80,300,000):** This project will enhance the movement of fresh produce by replacing a signalized intersection with a grade-separated interchange, removing a key bottleneck along State Route 156. State Route 156 is a critical east-west goods movement corridor connecting United States Route 101 to the Central Coast. Castroville Boulevard will cross over State Route 156 via a new bridge, eliminating conflict points and improving truck speeds and throughput. Additionally, the project adds crosswalks, a Class I multiuse path, and pedestrian islands, safely connecting residents to downtown Castroville, the local high school, and planned affordable housing developments.
- **Tulare Six-lane and Paige Avenue Multimodal Interchange Improvement Project (\$62,670,000):** This project will enhance the movement of agricultural goods in the Central Valley by widening a five-mile segment of State Route 99 from four lanes to six lanes, eliminating a major bottleneck on a critical north-south trade corridor. The project also makes several improvements to nearby local roads, including the addition of roundabouts to eliminate conflict points, the construction of a Class I multiuse path to physically separate pedestrians and bicyclists from freight traffic, and the closure of sidewalk gaps in nearby disadvantaged communities. Additionally, the project will expand transit and vanpool options in the local community, enhancing connectivity to jobs, services, education, and medical facilities.

Baseline Agreement Requirements

All funded agencies must comply with the Commission's adopted 2024 Trade Corridor Enhancement Program Guidelines and [Senate Bill \(SB\) 1 Accountability and Transparency Guidelines](#), which were adopted on May 15, 2025.

All projects included in the 2024 Trade Corridor Enhancement Program must enter into a Baseline Agreement approved by the Commission. Baseline Agreement development is initiated upon program adoption. Baseline Agreements stipulate the agreed-upon expected benefits, delivery schedules, project costs, and funding plans. Baseline Agreements identify agencies that must comply with SB 1 reporting requirements and include cost-sharing agreements that detail how cost overruns will be covered financially by project sponsors and implementing agencies. Executed Baseline Agreements serve as benchmarks for comparison to the current status of a project for subsequent reporting purposes. Baseline Agreements must be signed by the California Department of Transportation Director and District Director, the Commission's Executive Director, the project applicant, and the implementing agency.

Background:

Created by SB 1 (Beall, Chapter 5, Statutes of 2017), the Trade Corridor Enhancement Program provides funding for corridor-based freight projects nominated by local agencies and the state. Implementing legislation, SB 103 (Committee on Budget and Fiscal Review, Chapter 95, Statutes of 2017), directs the Commission to allocate Trade Corridor Enhancement Account funds and federal National Highway Freight Program funds to infrastructure improvements on federally designated Trade Corridors of National and Regional Significance, on the Primary Freight Network, and along other corridors that have a high volume of freight movement.

Beginning in December 2023, the Commission staff hosted five public workshops to solicit feedback on the development of the 2024 Trade Corridor Enhancement Program Guidelines. Commission staff prepared the 2024 Trade Corridor Enhancement Program Guidelines in consultation with the Interagency Equity Advisory Committee, Caltrans, the California State Transportation Agency, the California Air Resources Board, regional transportation planning agencies, metropolitan planning organizations, local agencies, non-governmental organizations, community-based organizations, and freight industry representatives. On August 15, 2024, the Commission adopted the 2024 Trade Corridor Enhancement Program Guidelines, which describe the policy, standards, criteria, and procedures for the development, adoption, and management of the program.

Commission staff hosted 29 virtual office hour sessions from March to May 2024 to provide technical assistance to applicants for project nominations for the 2024 Trade Corridor Enhancement Program.

Attachments:

- Attachment A: Resolution G-25-42
- Attachment B: 2024 Trade Corridor Enhancement Program Recommended Projects
- Attachment C: Comment Letters

**CALIFORNIA TRANSPORTATION COMMISSION
2024 TRADE CORRIDOR ENHANCEMENT PROGRAM ADOPTION
PROGRAM OF PROJECTS**

RESOLUTION G-25-42

- 1.1 **WHEREAS**, Senate Bill 1 (SB 1) (Beall, Chapter 5, Statutes of 2017) created the Trade Corridor Enhancement Program to fund corridor-based freight projects nominated by local agencies and the state; and
- 1.2 **WHEREAS**, SB 103 (Committee on Budget and Fiscal Review, Chapter 95, Statutes of 2017) directs the California Transportation Commission (Commission) to allocate Trade Corridor Enhancement Account funds and federal National Highway Freight Program funds to infrastructure improvements on federally designated Trade Corridors of National and Regional Significance, on the Primary Freight Network, and along corridors that have a high volume of freight movement, as determined by the Commission; and
- 1.3 **WHEREAS**, on August 15, 2024, the Commission adopted the 2024 Trade Corridor Enhancement Program Guidelines, which describe the policy, standards, criteria, and procedures for the development, adoption, and management of the Trade Corridor Enhancement Program; and
- 1.4 **WHEREAS**, the 2024 Trade Corridor Enhancement Program Fund Estimate provided \$900,389,000 in programming capacity over fiscal years 2025-26 and 2026-27; and
- 1.5 **WHEREAS**, since that time, the programming capacity has been reduced to \$771,522,000 due to advance programming and supplemental funding actions; and
- 1.6 **WHEREAS**, the Commission received 43 project nominations with \$1,347,201,000 in funding requests by the November 22, 2024 application deadline; and
- 1.7 **WHEREAS**, Commission staff prepared program recommendations that included \$810,474,000 for 24 projects representing total project costs of \$2,541,757,000; and
- 1.8 **WHEREAS**, the recommended projects exceed the identified programming capacity by \$38,952,000; and
- 1.9 **WHEREAS**, the overprogrammed amount is anticipated to be offset by future project cost savings, consistent with the Trade Corridor Enhancement Program Guidelines; and

- 1.10 **WHEREAS**, two projects previously programmed with \$185,000,000 in the Advance 2024 Trade Corridor Enhancement Program will be transitioned into the 2024 Trade Corridor Enhancement Program, bringing the total recommended programming to \$995,474,000 for 26 projects with total project costs of \$3,054,960,000; and
- 1.11 **WHEREAS**, the transitioned projects include the I-80/US 50 Managed Lanes Project, adopted under Resolution G-24-40 in May 2024 with \$105,000,000, and the Coastal Rail Infrastructure Resiliency Project, adopted under Resolution G-24-70 in October 2024 with \$80,000,000; and
- 1.12 **WHEREAS**, Resolution G-24-70 was subsequently amended by Resolution G-25-32 in April 2025;
- 1.13 **WHEREAS**, Commission staff recommendations are consistent with statute and conform to the 2024 Trade Corridor Enhancement Program Guidelines; and
- 1.14 **WHEREAS**, on June 6, 2025, Commission staff recommendations for the 2024 Trade Corridor Enhancement Program were posted on the Commission's website and made available to the Commission, California Department of Transportation, regional transportation agencies, county transportation commissions, and the public; and
- 1.15 **WHEREAS**, the Commission considered Commission staff recommendations and public testimony during its June 2025 Commission meeting.
- 2.1 **NOW THEREFORE BE IT RESOLVED**, that the Commission hereby adopts the 2024 Trade Corridor Enhancement Program, as reflected in Attachment B; and
- 2.2 **BE IT FURTHER RESOLVED**, that the I-80/US 50 Managed Lanes Project and the Coastal Rail Infrastructure Resiliency Project are hereby moved into the 2024 Trade Corridor Enhancement Program; and
- 2.3 **BE IT FURTHER RESOLVED**, that Resolutions G-24-40, G-24-70, and G-25-32 are hereby rescinded; and
- 2.4 **BE IT FURTHER RESOLVED**, that the amounts approved for project funding shall be considered a "not-to-exceed" amount, and any cost increases beyond the estimates reflected in the adopted program shall be the responsibility of the nominating agency, except for projects nominated by the California Department of Transportation, which may be considered for additional funding on a case-by-case basis as described in Section 8 of the 2024 Trade Corridor Enhancement Program Guidelines; and

- 2.5 **BE IT FURTHER RESOLVED**, that projects included in the adopted 2024 Trade Corridor Enhancement Program must comply with the Trade Corridor Enhancement Program Guidelines and SB 1 Accountability and Transparency Guidelines; and
- 2.6 **BE IT FURTHER RESOLVED**, that projects included in the adopted 2024 Trade Corridor Enhancement Program must enter into a Baseline Agreement approved by the Commission; and
- 2.7 **BE IT FURTHER RESOLVED**, that if a recommended project requests an allocation for project components between the June 2025 and October 2025 Commission meetings, the project applicant must submit a Baseline Agreement for approval by the October 2025 Commission meeting; and that all remaining Baseline Agreements must be submitted by the December 2025 Commission meeting; and
- 2.8 **BE IT FURTHER RESOLVED**, that the Commission may remove from the program any project for which a Baseline Agreement is not executed; and
- 2.9 **BE IT FURTHER RESOLVED**, that the Commission will not consider approval of a project allocation without an approved Baseline Agreement; and
- 2.10 **BE IT FURTHER RESOLVED**, that Commission staff, in consultation with the California Department of Transportation and project sponsors, is authorized to make minor technical changes to the 2024 Trade Corridor Enhancement Program to reflect the most current information or clarify the Commission's programming commitments, and shall seek Commission approval for any substantive changes; and
- 2.11 **BE IT FURTHER RESOLVED**, that the Commission directs Commission staff to post the 2024 Trade Corridor Enhancement Program of Projects on the Commission's website.

2024 TRADE CORRIDOR ENHANCEMENT PROGRAM
RECOMMENDED PROGRAM OF PROJECTS (\$1000s)
RESOLUTION G-25-42

REFERENCE NO.: 4.6
JUNE 26-27, 2025
ATTACHMENT B

County	Freight Region	Project Title	Nominating Agency	Implementing Agency	Project Type	TCEP Federal?	Total Project Cost	Recommended Funding	TCEP Funds by Target		TCEP Funds by Fiscal Year		TCEP Funds by Project Phase				
									TCEP State	TCEP Regional	2025-26	2026-27	PS&E	R/W Sup	R/W	Con Sup	CON
San Mateo	Bay Area/Central Valley	State Route 84 / United States Route 101 Interchange Reimagined Project	Caltrans, Metropolitan Transportation Commission, San Mateo County Transportation Authority, City of Redwood City	Caltrans	Highway		\$ 384,421	\$ 14,165	\$ 5,666	\$ 8,499	\$ -	\$ 14,165	\$ -	\$ -	\$ -	\$ -	\$ 14,165
Tulare	Bay Area/Central Valley	Tulare Six-lane and Paige Avenue Multi-Modal Interchange Improvement Project	Caltrans, Tulare County Association of Governments	Caltrans	Highway		\$ 226,143	\$ 62,670	\$ 25,068	\$ 37,602	\$ -	\$ 62,670	\$ -	\$ -	\$ -	\$ 11,480	\$ 51,190
Los Angeles	Los Angeles/Inland Empire	State Route 71 Gap Closure Project – Phase 2	Caltrans, Los Angeles County Metropolitan Transportation Authority	Caltrans	Highway		\$ 309,400	\$ 80,000	\$ 32,000	\$ 48,000	\$ -	\$ 80,000	\$ -	\$ -	\$ -	\$ 12,300	\$ 67,700
Stanislaus	Bay Area/Central Valley	State Route 132 West Phase 3A Project	Caltrans, Stanislaus Council of Governments	Stanislaus Council of Governments	Highway		\$ 117,560	\$ 67,000	\$ 26,800	\$ 40,200	\$ 66,820	\$ 180	\$ -	\$ -	\$ -	\$ 6,000	\$ 61,000
Stanislaus	Bay Area/Central Valley	SR 132 West Gates to Dakota	Caltrans, Stanislaus Council of Governments	Stanislaus Council of Governments			\$ 117,280	\$ 66,820	\$ 26,620	\$ 40,200	\$ 66,820	\$ -	\$ -	\$ -	\$ -	\$ 6,000	\$ 60,820
Stanislaus	Bay Area/Central Valley	Modesto EV Charging	Caltrans, Stanislaus Council of Governments	Stanislaus Council of Governments			\$ 280	\$ 180	\$ 180	\$ -	\$ -	\$ 180	\$ -	\$ -	\$ -	\$ -	\$ 180
Solano, Sonoma	Bay Area/Central Valley	State Route 37 Sears Point to Mare Island Improvement Project	Caltrans, Metropolitan Transportation Commission, Sonoma County Transportation Authority, Solano Transportation Authority, Napa Valley Transportation Authority	Caltrans	Highway		\$ 251,000	\$ 73,000	\$ 29,200	\$ 43,800	\$ -	\$ 73,000	\$ -	\$ -	\$ -	\$ 15,000	\$ 58,000
Alameda	Bay Area/Central Valley	Alameda County Rail Safety Enhancement Program – Phase A	Caltrans, Alameda County Transportation Commission	Alameda County Transportation Commission	Rail		\$ 96,742	\$ 30,000	\$ 12,000	\$ 18,000	\$ 8,697	\$ 21,303	\$ -	\$ -	\$ -	\$ -	\$ 30,000
Alameda	Bay Area/Central Valley	Alameda County Rail Safety Enhancement Program – Phase A (Package 1)	Caltrans, Alameda County Transportation Commission	Alameda County Transportation Commission			\$ 28,230	\$ 8,697	\$ 3,479	\$ 5,218	\$ 8,697	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 8,697
Alameda	Bay Area/Central Valley	Alameda County Rail Safety Enhancement Program – Phase A (Package 2)	Caltrans, Alameda County Transportation Commission	Alameda County Transportation Commission			\$ 43,780	\$ 14,087	\$ 5,635	\$ 8,452	\$ -	\$ 14,087	\$ -	\$ -	\$ -	\$ -	\$ 14,087
Alameda	Bay Area/Central Valley	Alameda County Rail Safety Enhancement Program – Phase A (Package 3)	Caltrans, Alameda County Transportation Commission	Alameda County Transportation Commission			\$ 24,732	\$ 7,216	\$ 2,886	\$ 4,330	\$ -	\$ 7,216	\$ -	\$ -	\$ -	\$ -	\$ 7,216
Kern	Bay Area/Central Valley	Centennial Corridor Southbound State Route 99 to Westbound State Route 58 Connector Project	Caltrans, Kern Council of Governments	Caltrans	Highway		\$ 78,750	\$ 39,900	\$ 15,960	\$ 23,940	\$ -	\$ 39,900	\$ -	\$ -	\$ -	\$ 7,000	\$ 32,900
Contra Costa	Bay Area/Central Valley	I-680/SR 4 Interchange Improvement – Phase 2A and 4	Contra Costa Transportation Authority	Contra Costa Transportation Authority	Highway		\$ 235,500	\$ 58,000	\$ -	\$ 58,000	\$ 58,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 58,000
Alameda	Caltrans	Prologis Mobility – Freight Logistics Electrifications for Emission-Free Transport Project	Caltrans	Prologis Mobility, Inc.	ZEV		\$ 29,300	\$ 14,650	\$ 14,650	\$ -	\$ 14,650	\$ -	\$ 398	\$ -	\$ -	\$ -	\$ 14,252
Riverside	Los Angeles/Inland Empire	Pennsylvania Avenue Grade Separation Project	Caltrans, City of Beaumont	City of Beaumont	Rail		\$ 74,700	\$ 50,400	\$ 20,160	\$ 30,240	\$ 50,400	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 50,400
Riverside	Los Angeles/Inland Empire	Pennsylvania Avenue Grade Separation	Caltrans, City of Beaumont	City of Beaumont			\$ 73,700	\$ 49,400	\$ 19,160	\$ 30,240	\$ 49,400	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 49,400
Riverside	Los Angeles/Inland Empire	Park and Ride EV Charging	Caltrans, City of Beaumont	City of Beaumont			\$ 1,000	\$ 1,000	\$ 1,000	\$ -	\$ 1,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,000
Los Angeles	Caltrans	EV Realty – South Bay Truck Charging Hub Project	Caltrans	EV Realty, Inc.	ZEV		\$ 18,370	\$ 7,900	\$ 7,900	\$ -	\$ 7,900	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 7,900
San Bernardino	Los Angeles/Inland Empire	High Desert Corridor Operational Efficiency Project	Caltrans	Caltrans	Rail		\$ 135,666	\$ 30,770	\$ 30,770	\$ -	\$ 30,770	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 30,770
Monterey	Central Coast	State Route 156 Castroville Boulevard Interchange Project	Caltrans, Transportation Agency for Monterey County	Caltrans	Highway		\$ 136,101	\$ 80,300	\$ 32,100	\$ 48,200	\$ 80,300	\$ -	\$ -	\$ -	\$ 32,100	\$ 9,100	\$ 39,100
Los Angeles	Los Angeles/Inland Empire	Port of Los Angeles Rail System Efficiency/Emission Reduction Program – Terminal Island Rail Infrastructure Project	Caltrans, Port of Los Angeles	Port of Los Angeles	Rail		\$ 20,700	\$ 13,391	\$ 5,356	\$ 8,035	\$ -	\$ 13,391	\$ -	\$ -	\$ -	\$ 1,750	\$ 11,641
Los Angeles	Los Angeles/Inland Empire	Long Beach-East Los Angeles Corridor Zero-Emissions Truck Project	Los Angeles County Metropolitan Transportation Authority	Forum Mobility, Inc., MN8 Energy Operating Company LLC	ZEV		\$ 38,123	\$ 13,653	\$ -	\$ 13,653	\$ 13,653	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 13,653
Los Angeles	Los Angeles/Inland Empire	Long Beach-East Los Angeles Corridor Zero-Emissions Truck Project (Forum Mobility)	Los Angeles County Metropolitan Transportation Authority	Forum Mobility, Inc.			\$ 28,414	\$ 10,000	\$ -	\$ 10,000	\$ 10,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 10,000
Los Angeles	Los Angeles/Inland Empire	Long Beach-East Los Angeles Corridor Zero-Emissions Truck Project (MN8 Energy)	Los Angeles County Metropolitan Transportation Authority	MN8 Energy Operating Company LLC			\$ 9,709	\$ 3,653	\$ -	\$ 3,653	\$ 3,653	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 3,653
Los Angeles	Los Angeles/Inland Empire	Harbor Scenic Drive Enhancements Project	Caltrans, Port of Long Beach	Port of Long Beach	Local Road		\$ 53,155	\$ 31,900	\$ 12,760	\$ 19,140	\$ -	\$ 31,900	\$ -	\$ -	\$ -	\$ 4,000	\$ 27,900



2024 TRADE CORRIDOR ENHANCEMENT PROGRAM
RECOMMENDED PROGRAM OF PROJECTS (\$1000s)
RESOLUTION G-25-42

REFERENCE NO.: 4.6
JUNE 26-27, 2025
ATTACHMENT B

									TCEP Funds by Target		TCEP Funds by Fiscal Year		TCEP Funds by Project Phase				
County	Freight Region	Project Title	Nominating Agency	Implementing Agency	Project Type	TCEP Federal?	Total Project Cost	Recommended Funding	TCEP State	TCEP Regional	2025-26	2026-27	PS&E	R/W Sup	R/W	Con Sup	CON
Various	Caltrans	Gage Zero – Accelerating Zero Emission Fleet Charging on Priority Freight Corridors Project	Caltrans	Gage Zero LLC	ZEV		\$ 48,622	\$ 20,177	\$ 20,177	\$ -	\$ 20,177	\$ -	\$ 232	\$ -	\$ -	\$ -	\$ 19,945
Alameda	Caltrans	Accelerating Zero Emission Fleet Charging on Priority Freight Corridors - Oakland	Caltrans	Gage Zero LLC			\$ 14,468	\$ 6,004	\$ 6,004	\$ -	\$ 6,004	\$ -	\$ 58	\$ -	\$ -	\$ -	\$ 5,946
San Bernardino	Caltrans	Accelerating Zero Emission Fleet Charging on Priority Freight Corridors - Ontario	Caltrans	Gage Zero LLC			\$ 9,776	\$ 4,057	\$ 4,057	\$ -	\$ 4,057	\$ -	\$ 58	\$ -	\$ -	\$ -	\$ 3,999
Fresno	Caltrans	Accelerating Zero Emission Fleet Charging on Priority Freight Corridors - Fresno	Caltrans	Gage Zero LLC			\$ 14,432	\$ 5,989	\$ 5,989	\$ -	\$ 5,989	\$ -	\$ 58	\$ -	\$ -	\$ -	\$ 5,931
San Diego	Caltrans	Accelerating Zero Emission Fleet Charging on Priority Freight Corridors - San Diego	Caltrans	Gage Zero LLC			\$ 9,946	\$ 4,127	\$ 4,127	\$ -	\$ 4,127	\$ -	\$ 58	\$ -	\$ -	\$ -	\$ 4,069
San Bernardino	Los Angeles/Inland Empire	Baker Boulevard Bridge Zero-Emission Truck Infrastructure Project	San Bernardino County Transportation Authority	San Bernardino County Transportation Authority, San Bernardino County	Bridge Replacement & ZEV		\$ 44,856	\$ 28,912	\$ -	\$ 28,912	\$ 28,912	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 28,912
San Bernardino	Los Angeles/Inland Empire	Baker Boulevard Bridge and Zero-Emission Truck Infrastructure Project - Bridge Component	San Bernardino County Transportation Authority	San Bernardino County			\$ 29,795	\$ 18,369	\$ -	\$ 18,369	\$ 18,369	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 18,369
San Bernardino	Los Angeles/Inland Empire	Baker Boulevard Bridge and Zero-Emission Truck Infrastructure Project - Zero-Emission Component	San Bernardino County Transportation Authority	San Bernardino County Transportation Authority			\$ 15,061	\$ 10,543	\$ -	\$ 10,543	\$ 10,543	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 10,543
Riverside	Los Angeles/Inland Empire	State Route 60 / World Logistics Center Parkway Interchange Replacement Project	Caltrans, City of Moreno Valley	City of Moreno Valley	Highway		\$ 116,000	\$ 25,300	\$ 10,120	\$ 15,180	\$ -	\$ 25,300	\$ -	\$ 1,700	\$ 23,600	\$ -	\$ -
Various	Caltrans	Watt EV – Port to Border California Freight Electrification Project	Caltrans	WattEV, Inc.	ZEV		\$ 27,488	\$ 13,744	\$ 13,744	\$ -	\$ 13,744	\$ -	\$ 272	\$ -	\$ -	\$ -	\$ 13,472
Los Angeles	Caltrans	Port to Border California Freight Electrification (P2B) Project - Long Beach	Caltrans	WattEV, Inc.			\$ 13,744	\$ 6,872	\$ 6,872	\$ -	\$ 6,872	\$ -	\$ 136	\$ -	\$ -	\$ -	\$ 6,736
San Diego	Caltrans	Port to Border California Freight Electrification (P2B) Project - Otay Mesa	Caltrans	WattEV, Inc.			\$ 13,744	\$ 6,872	\$ 6,872	\$ -	\$ 6,872	\$ -	\$ 136	\$ -	\$ -	\$ -	\$ 6,736
Various	Caltrans	BP Pulse – Electric Vehicle Oasis North Project	Caltrans	bp Products North America	ZEV		\$ 28,004	\$ 10,924	\$ 10,924	\$ -	\$ 10,924	\$ -	\$ 424	\$ -	\$ -	\$ -	\$ 10,500
Merced	Caltrans	EV Oasis North - Livingston	Caltrans	bp Products North America			\$ 7,001	\$ 2,731	\$ 2,731	\$ -	\$ 2,731	\$ -	\$ 106	\$ -	\$ -	\$ -	\$ 2,625
Merced	Caltrans	EV Oasis North - Santa Nella	Caltrans	bp Products North America			\$ 7,001	\$ 2,731	\$ 2,731	\$ -	\$ 2,731	\$ -	\$ 106	\$ -	\$ -	\$ -	\$ 2,625
Shasta	Caltrans	EV Oasis North - Redding	Caltrans	bp Products North America			\$ 7,001	\$ 2,731	\$ 2,731	\$ -	\$ 2,731	\$ -	\$ 106	\$ -	\$ -	\$ -	\$ 2,625
Tehama	Caltrans	EV Oasis North - Corning	Caltrans	bp Products North America			\$ 7,001	\$ 2,731	\$ 2,731	\$ -	\$ 2,731	\$ -	\$ 106	\$ -	\$ -	\$ -	\$ 2,625
San Bernardino	Los Angeles/Inland Empire	Interstate 10 / Riverside Avenue Freight Improvement Project	Caltrans, City of Rialto	City of Rialto	Highway		\$ 45,682	\$ 29,800	\$ 11,900	\$ 17,900	\$ -	\$ 29,800	\$ -	\$ -	\$ -	\$ -	\$ 29,800
Alameda	Caltrans	Forum Mobility – Beyond the Dock: Heavy-Duty Electrification of the Port of Oakland Priority Trade Corridors Project	Caltrans	Forum Mobility, Inc.	ZEV		\$ 8,102	\$ 2,578	\$ 2,578	\$ -	\$ 2,578	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 2,578
Ventura	Los Angeles/Inland Empire	Hueneme Road Widening – Edison Drive to Rice Avenue	Ventura County Transportation Commission	Ventura County	Local Road		\$ 17,372	\$ 11,340	\$ -	\$ 11,340	\$ -	\$ 11,340	\$ 840	\$ -	\$ 10,500	\$ -	\$ -
Yolo	Bay Area/Central Valley	I-80/US 50 Managed Lanes Project*	Caltrans	Caltrans	Highway		\$ 199,960	\$ 105,000	\$ 42,000	\$ 63,000	\$ 105,000	\$ -	\$ -	\$ 100	\$ -	\$ -	\$ 104,900
Orange	Los Angeles/Inland Empire	Coastal Rail Infrastructure Resiliency Project*	Orange County Transportation Authority	Orange County Transportation Authority, Southern California Regional Rail Authority	Rail		\$ 313,243	\$ 80,000	\$ -	\$ 80,000	\$ 80,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 80,000
Orange	Los Angeles/Inland Empire	Coastal Rail Infrastructure Resiliency Project - Construction component	Orange County Transportation Authority	Orange County Transportation Authority			\$ 296,747	\$ 75,008	\$ -	\$ 75,008	\$ 75,008	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 75,008
Orange	Los Angeles/Inland Empire	Coastal Rail Infrastructure Resiliency Project - Emergency component	Orange County Transportation Authority	Southern California Regional Rail Authority			\$ 16,496	\$ 4,992	\$ -	\$ 4,992	\$ 4,992	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 4,992

*The I-80/US 50 Managed Lanes Project and the Coastal Rail Infrastructure Resiliency Project were previously programmed in the Advance 2024 Trade Corridor Enhancement Program and are being transitioned into the 2024 Trade Corridor Enhancement Program.

	New	Advance 2024 TCEP	Total
Projects Recommended for Funding	24	2	26
Total Project Costs	\$ 2,541,757	\$ 513,203	\$ 3,054,960
Total TCEP Funding Recommended	\$ 810,474	\$ 185,000	\$ 995,474
Total TCEP Programming Capacity	\$ 771,522	\$ 185,000	\$ 956,522
Total Overprogramming	\$ 38,952	\$ -	\$ 38,952



Reference Number 4.6
June 26-27, 2025
Attachment C

JAY OBERNOLTE
TWENTY THIRD DISTRICT, CALIFORNIA

**COMMITTEE ON ENERGY
AND COMMERCE**
COMMUNICATIONS AND TECHNOLOGY
ENVIRONMENT, MANUFACTURING,
AND CRITICAL MINERALS
HEALTH

**COMMITTEE ON SCIENCE, SPACE
AND TECHNOLOGY**
INVESTIGATIONS AND OVERSIGHT, CHAIRMAN



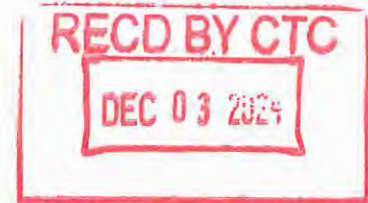
Congress of the United States
House of Representatives

Washington, DC
November 22, 2024

WASHINGTON, D.C. OFFICE:
1029 LONGWORTH HOUSE OFFICE BUILDING
WASHINGTON, DC 20515
TELEPHONE: (202) 225-5861

DISTRICT OFFICE:
9700 SEVENTH AVE., SUITE 201
HESPERIA, CA 92345
TELEPHONE: (760) 247-1815

E-MAIL VIA WEBSITE:
<http://obernolte.house.gov>



The Honorable Tanisha Taylor
Executive Director, California Transportation Commission
1120 N Street, MS-52
Sacramento, CA 95814

RE: Letter of Support for SBCTA's SB 1 TCEP Application for SBCTA's Baker Boulevard Mojave Bridge Replacement and Zero-Emission Truck Charging Station

Dear Executive Director Taylor:

As a member of Congress representing much of the Mojave Desert, I am pleased to support the San Bernardino County Transportation Authority (SBCTA) in their application for funding under the California Transportation Commission's Senate Bill (SB) 1 Trade Corridor Enhancement Program (TCEP). This funding will be used for the Baker Boulevard Mojave Bridge Replacement and Zero-Emission Truck Charging Station.

Baker is a small, unincorporated community located along I-15 approximately 60 miles northeast of Barstow. It serves as a crucial waystation for travelers between Los Angeles and Las Vegas who need food, fuel, and rest. Approximately 40,000 vehicles, including 8,200 trucks, pass by Baker on weekdays, with even higher numbers on weekends. Baker is also a strategic location for a planned charging network for electric Medium and Heavy-Duty trucks. As we see an increase in mandates for electric vehicles, this facility in Baker will help reduce the anxiety for those in the trucking industry, ensuring they can travel their routes confidently while using electric vehicles.

The Baker Boulevard bridge over the Mojave River is a nearly 100-year-old, two-lane timber bridge without shoulders. Its maintenance costs are increasing, and it fails to meet current structural design standards or align with the adjacent roadway segments. The proposal includes a new four-lane bridge with shoulders and a sidewalk. Additionally, a publicly accessible 7.2 megawatt charging station for medium and heavy-duty trucks will be located about 4,000 feet north of the bridge.

This project will improve the local traffic flow for this disadvantaged community, enhance infrastructure resilience, and maintain Baker Blvd. as an alternate route during I-15 closures. It also provides a strategic location for truck charging along I-15, a priority route on CTC's SB 671 Clean Freight Corridor Efficiency Assessment. The County of San Bernardino will be the implementing agency for the bridge, and WattEV will be the implementing vendor for the charging station. I support the efforts of SBCTA, County, and WattEV as they seek to bring these important, resilient, and sustainable improvements to fruition.

Consistent with all rules and regulations, I ask that you give the San Bernardino County Transportation Authority's application to the Trade Corridor Enhancement Program your full and fair consideration. If you have any additional questions, please feel free to contact my office at (202) 225-5861.

Sincerely,


Jay Obernolte
Member of Congress



BUILDING AMERICA®

January 30, 2025

Tanisha Taylor
Executive Director
California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814

**Re: Letter of Support for the Grant Line Road Realignment Project (Project)
Trade Corridor Enhancement Program Application**

Dear Ms. Taylor:

On behalf of Union Pacific Railroad (UP), I am writing in support of San Joaquin County's (County) grant application for the Trade Corridor Enhancement Program (TCEP) for the Grant Line Road Realignment Project (Project). This Project proposes to widen and realign Grant Line Road south of the community of Banta, close the existing at-grade crossing at Banta Road, and add a new four-lane grade-separated crossing over UP tracks. The Project will reduce congestion, improve goods movement, and enhance safety along existing Grant Line Road and in Banta.

Grant Line Road is a two-lane corridor that serves as the main street through the small, rural community of Banta. The growth of distribution centers in eastern Tracy and congestion along Interstate 205 (I-205) has increased vehicle and truck traffic and increased traffic collisions along the corridor. UP's tracks and the adjacent at-grade crossings experience traffic closures due to operations at the adjacent railyard. The proposed improvements address these concerns through the at-grade crossing closure, a new grade separated road over UP, and roadway realignment.

If a grant is awarded, UP will begin to work with the County to ensure the project design and plans meet UP's current engineering and safety standards for aspects of the plans that interface with UP infrastructure. The County will also enter into a preliminary engineering (PE) agreement with UP to reimburse costs associated with reviewing project concepts and designs.

UP requests that the CTC approve San Joaquin County's grant application request.

Sincerely,

Adrian Guerrero

Congress of the United States
Washington, DC 20515

March 6, 2025

The Honorable Sean Duffy
U.S. Department of Transportation
1200 New Jersey Avenue, SE
Washington, D.C. 20590

Dear Secretary Duffy:

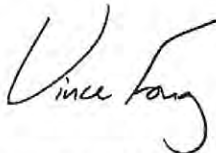
We are writing to express our support for Kern Council of Governments' (Kern COG) application for a Federal-State Partnership for Better Utilizing Investments to Leverage Development (BUILD) grant of \$22 million. If awarded, this funding would support the Centennial Corridor Interchange Connector project in Bakersfield by constructing a new direct freeway-to-freeway connector ramp from southbound State Route (SR) 99 to westbound State Route 58, completing the interchange. This project will enhance mobility and efficiency along SR 99 and SR 58, two of the West Coast's most critical freight corridors.

The Centennial Corridor Interchange Connector is essential for improving regional and national transportation networks, as it links what Kern COG describes as the busiest north-south truck route on the West Coast (SR 99) with the busiest east-west truck route and the only year-round, all-weather route over the Sierra Nevada mountain range (SR 58). This project will not only ease congestion but also reduce the impact of goods movement on communities in Central Bakersfield by diverting truck traffic away from local streets. Kern County's economy continues to expand, driven by its strong agricultural and industrial sectors, and this project will support this growth by facilitating freight movement to and from over 50 distribution, processing, and manufacturing facilities in the southern San Joaquin Valley. Additionally, the project is expected to create 844 construction jobs and support thousands of existing and future industrial jobs.

We firmly support Kern COG's grant application to complete the Centennial Corridor Interchange Connector. Approval of this project aligns with the BUILD Grant Program's mission by enhancing safety, reducing congestion, and strengthening a vital freight corridor essential to regional and national commerce. If you have any questions, please do not hesitate to contact us.

Thank you for your attention to this important matter.

Sincerely,



VINCE FONG
Member of Congress



DAVID G. VALADAO
Member of Congress

ClimatePlan TRANSFORM



May 28, 2025

Matthew Yosgott
Deputy Director - SB 1 Programming
California Transportation Commission (CTC)
1120 N Street MS 52
Sacramento, CA 95814

Re: Applications of Concern for SB 1 Funding (SCCP, TCEP, LPP Programs)

Dear Deputy Director Yosgott,

The undersigned organizations write to express concerns about a number of highway expansion project applications for SB 1 funding under the Solutions for Congested Corridors Program (SCCP), Trade Corridor Enhancement Program (TCEP), and the Local Partnership Program (LPP) that we have listed below. Our concerns about these projects relate to their inconsistency with the Climate Action Plan for Transportation Infrastructure (CAPTI), their likelihood to significantly increase driving, their environmental, climate, and displacement impacts, and their conflicts with California's climate and equity goals. We urge the Commission to scrutinize these projects closely and only fund the components of these projects that reduce driving, improve

multimodal options, and advance the zero-emission fleet transition. Several of the projects listed are opposed locally by organizations in the ClimatePlan network that are also signed on to this letter. We have linked to opposition letters to specific projects in the footnotes.

Solutions for Congested Corridors Program (SCCP)

We are very pleased to see the high number of transformative multimodal and vehicle miles traveled (VMT) reducing projects and components of projects among the list of applications for SCCP this cycle. We particularly appreciate the innovative project applications that aim to improve highway operations in congested corridors without adding new lanes, such as the ***I-805 South Managed Lanes HOV to HOT Conversion Project*** which will reinvest toll revenue into transit and active transportation infrastructure.

We encourage the CTC to only fund projects and project components that reduce VMT and avoid adding new lanes to highways and roadways. Specifically, we urge the CTC not to fund the following highway expansion projects and project components:

1. **SR 1 – Freedom to State Park Auxiliary Lanes¹ (Also an LPP and TCEP application, comments apply to that program as well)** - This auxiliary lane expansion project is a component of the Watsonville-Santa Cruz Multimodal Corridor Project and is opposed by local advocates challenging the project. As articulated in the Sierra Club chapter letter linked in the footnote, this application for SCCP/TCEP/LPP has some multimodal components that enjoy support from the community and we encourage the CTC to fund these components without the auxiliary lanes.
2. **101 Multimodal Corridor Segment 4E North – Cabrillo Interchange (Also an LPP and TCEP application, comments apply to that program as well)** - This expansion project is a component of the Santa Barbara US 101 Multimodal Corridor Project and was opposed by local advocates. This application for SCCP/TCEP/LPP also has many strong multimodal components that enjoy support from the community and we encourage the CTC to fund these components of this project. Since most of the corridor has already been widened, we recognize that restricting lanes in this short segment could decrease air quality. However, the HOV lanes have limited effectiveness because of their limited time duration and operational requirements for only 2 or more occupants. We encourage CTC to fund the HOV component only with expanded HOV operational requirements for additional hours and 3 or more occupants.
3. **I-5 Managed Lanes Multimodal Operational Improvements (Orange County)** - This project adds 16 lane-miles of new HOV lanes that will exacerbate freeway congestion and air quality issues without a clear commitment to invest in transit alternatives. We would support the portion of this project that converts existing HOV lanes to express lanes if those components could be funded separately from the lane expansion components and if toll revenues were committed to providing multimodal alternatives.

¹ [Sierra Club to CTC on SR-1](https://drive.google.com/file/d/1mzuzGdvoWviBsOHhuy8rzwJOPGmgb4ql/view?usp=sharing)
<https://drive.google.com/file/d/1mzuzGdvoWviBsOHhuy8rzwJOPGmgb4ql/view?usp=sharing>

4. **SR 37 Sears Point to Mare Island Improvement Project – Phase 2² (Also an LPP and TCEP application, comments apply to all three programs)** - The sign on letter linked in the footnote demonstrates the local advocate opposition to this “interim” lane expansion project, and states that “the interim project will cost over \$500M and will be under water caused by sea level rise within fifteen years of completion. Continuing the interim project in its current form further delays, rather than advances, the long-term vision for the corridor.” We have serious concerns about investing any state funding in a roadway that may be inoperable as soon as 2040.
5. **I-15 Express Lanes Project Southern Extension (ELPSE)³ (Also an LPP application, comments apply to that program as well)** - This project will add over 60 lane-miles to I-15 in a congested and growing region and does not include multimodal and VMT reducing components. As detailed in the comment letter on this project’s draft environmental impact report linked in the footnote, this project will induce significant VMT and relied on outdated and flawed methodology for estimating air quality and other impacts.
6. **I-680 NB Express Lane Completion Phase 1⁴** - This express lane expansion project is a component of the Innovate 680 project. Though this SCCP application appears to only be requesting funds for the Shared Mobility Hubs and TDM Augmentation components of the larger project, we still want to highlight the concerns raised by Transform in their comment letter to Caltrans regarding flawed analysis in the draft EIR for this project, linked in the footnote.

Trade Corridor Enhancement Program (TCEP)

We are again pleased to see the increase in TCEP applications this cycle that would advance freight electrification, technology, and efficiency at ports and truck parking hubs. We strongly support investing TCEP funding in these types of projects that are well-aligned with CAPTI and reduce harmful impacts to communities that bear the brunt of pollution from the freight industry. We encourage the CTC to focus funding only on those types of projects and avoid funding any projects that increase passenger VMT by adding general purpose lanes to roadways.

In addition to the projects listed above that are applying for both SCCP and TCEP, we have concerns about the following TCEP applications:

1. **SR 60/World Logistics Center Pkwy Interchange** - This project entails a significant expansion and new connection for an existing interchange that will open up land for more warehouse and sprawl development and induce significant new passenger VMT.

² Sign on comment letter SR-37 <https://transformca.org/wp-content/uploads/2024/09/Sign-on-Letter - Oppose-Backdoor-Streamlining-for-HW-37.pdf>

³ [Sign on comment letter I-15 DEIR https://drive.google.com/file/d/1P8Tr5ZF3zWvByCTWY_bwlo957Ab7Q3jM/view?usp=sharing](https://drive.google.com/file/d/1P8Tr5ZF3zWvByCTWY_bwlo957Ab7Q3jM/view?usp=sharing)

⁴ [Sign on comment letter Innovate 680 DIER https://transformca.org/wp-content/uploads/2024/07/I-680-NB-Express-Lanes-Comment-Letter.docx.pdf](https://transformca.org/wp-content/uploads/2024/07/I-680-NB-Express-Lanes-Comment-Letter.docx.pdf)

2. **Konocti Corridor - Segment 2B** - This project will add 16 general purpose lane-miles to convert the existing conventional highway between Kelseyville and Clear Lake to a four-lane expressway and will induce passenger VMT.
3. **Tulare SR 99 Corridor and Paige Avenue Multimodal Interchange Enhancements** - We do not support the capacity-increasing general purpose lane additions to SR 99 which will induce significant passenger and truck VMT. The project as approved further separates South City of Tulare and the neighboring unincorporated community of Maheny Tract by providing the transportation system for the growth of the City's industrial zones which separates the aforementioned communities. The Project does not improve connectivity for pedestrians and cyclists. At the bare minimum the project proposes a shared paved path, not even a sidewalk, between Blackstone St. and Laspina St, which do not extend into the residential communities to the East and West of the project site. We recommend a project alternative which will result in reduced traffic congestion while also improving pedestrian and active transportation options for the community, instead of facilitating industrial growth and highway capacity for freight.
4. **State Route 46 Antelope Grade Corridor Improvements Project** - This project will add five general purpose lane-miles to convert the existing conventional highway over Antelope Grade to a four-lane expressway and induce passenger VMT.
5. **State Route 132 Phase 3A Project** - This project will add 14 new general purpose lane-miles to extend a new expressway that creates a bypass for SR 132 and opens up new land for warehouse and sprawl development and induce new passenger VMT.
6. **State Route 71 Gap Closure Phase 2 Project (*Also an LPP application, comments apply to that program as well*)** - This project will convert an existing arterial highway into an 8-lane freeway, adding a general purpose and HOV lane in each direction. We do not support the highway lane addition portions of this project, which will induce significant passenger VMT and limit the freight benefits. However, we support the components of the project that provide sound walls and noise mitigation, update rail bridges to current standards, and enhance a pedestrian overcrossing.
7. **SR 46 East/Union Road Intersection Improvements** - This project entails a new interchange that will open up land for sprawl development and induce significant new passenger VMT.
8. **680/SR-4 Interchange Improvement (*Also an LPP application, comments apply to that program as well*)** - This project will add a direct connector with two general purpose lanes to the interchange of I-680 and SR-4 without mitigation for increases to passenger VMT.

Local Partnership Program (LPP)

In addition to the projects listed above that have applied for SCCP and/or TCEP in addition to LPP, we have concerns about the following additional application for LPP:

1. **Madera 41 South Expressway Project** - This project will add eight general purpose lane-miles to convert the existing conventional highway to a four-lane expressway, increasing access to land that is rapidly converting to sprawl development and significantly increasing passenger VMT. Rather, we urge the CTC to support projects

which truly meet the purpose of the LPP which includes addressing aging infrastructure, road conditions, active transportation, transit and rail, and ensuring health and safety benefits. Madera County is currently unable to meet existing transportation needs, including maintenance and transit of existing communities. Funding additional roadways for the purpose of sprawl contradicts the purpose of the LPP, will increase emissions, and divert critical funding needed to support the current transportation system in Madera County.

We appreciate the opportunity to provide comments and amplify project concerns from local organizations as commission staff reviews the SB 1 applications. We look forward to further engagement with CTC staff and commissioners once the staff recommendations are released.

Thank you for your attention to our concerns and for your leadership in guiding California toward a more sustainable and equitable transportation future.

Sincerely,

Zack Deutsch-Gross
Transform

Mary Lim, J.D.
Genesis: Interfaith Organizing in the East Bay

Jeanie Ward-Waller
ClimatePlan

Marven Norman
Center for Community Action and Environmental Justice (CCA EJ)

Jared Sanchez
CalBike

Elizabeth Reid-Wainscoat
Center for Biological Diversity

Chance Cutrano
Resource Renewal Institute

Matthew Baker
Planning and Conservation League

Janet Cox
Climate Action California

Sofia Rafikova
Coalition for Clean Air

Heather Deutsch
MOVE Santa Barbara County

Mike Swire
Stop the 101 & 280 Widenings in San Mateo County

Iman Sylvain, PhD
Rails to Trails Conservancy

David Diaz, MPH
Active San Gabriel Valley

Nick Ratto
350 Bay Area Action - Transportation Lead

Carter Rubin
Natural Resources Defense Council (NRDC)

Carter Lavin
Transbay Coalition

Justin Hu-Nguyen
Bike East Bay

David Levitus
LA Forward

Hana Creger
The Greenlining Institute

Aditi (Adi) Varshneya
California Environmental Justice Alliance (CEJA) Action

Emma De La Rosa
Leadership Counsel for Justice and Accountability

Jonathan Matz
Safe Routes Partnership

Marc Vukceovich
Streets For All

Kevin Shin
California Walks

Rick Longinotti
Campaign for Sustainable Transportation

Suzanne Hume
CleanEarth4Kids.org

Jamie Pew
NextGen California

Jorge Rivera
Healing & Justice Center

cc:
Executive Director Tanisha Taylor
Chair Darnell Grisby
Vice-chair Clarissa Falcon



TRANSFORM SPUR ClimatePlan



June 2, 2025

Tanisha Taylor
Executive Director
California Transportation Commission

RE: Trade Corridor Enhancement Program priorities

Dear Ms. Taylor:

On behalf of the undersigned organizations, we are grateful for the opportunity to review projects that are seeking funding from the Commission in the current cycle of Trade Corridor Enhancement Program (TCEP). We write to share the following priorities for what our organizations would like to see from the next TCEP cycle to maximize the benefits of these investments for the health of those living along trade corridors and for the benefit of our State's communities and the environment.

Priorities for the 2025 TCEP Cycle

As Commission staff review projects and make recommendations to the Commission, we ask that you please give the greatest priority to the following kinds of applications:

- Projects that eliminate pollution from the freight sector by investing in zero emissions heavy-duty vehicle charging infrastructure for battery electric trucks. We are encouraged to see so many proposals for truck charging advanced to CTC for TCEP funding, and nearly \$700 million worth of proposals sent to Caltrans for consideration that did not make it into Caltrans's TCEP requests.¹ Zero-emission infrastructure projects are also consistent with the Climate Action Plan for Transportation Infrastructure (CAPTI) principles, helping align CTC's actions with the rest of the state in reducing GHG and air pollutant emissions from the transportation sector.
- Projects that are informed by community input and improve community multimodal mobility infrastructure along trade corridors by helping residents and travelers overcome barriers placed in their communities by major roads, rail facilities, and other infrastructure. We are encouraged to see many projects incorporate elements that support bike, pedestrian, and transit improvements as part of a larger goods movement project.
- Projects that take a fix-it-first approach to investments that maintain existing infrastructure. We were encouraged to see proposals to repair existing facilities to keep them in a state of good repair, instead of expanding roads and bridges that come with additional long-term maintenance cost liabilities.

We ask CTC not to recommend funding projects that would exacerbate health and pollution impacts in communities already burdened by pollution from the freight sector, especially:

¹ <https://dot.ca.gov/programs/esta/zev/tcep-nomination>

- Projects proposing to expand roads and highways resulting in an increase in vehicle miles traveled (VMT). These types of projects have been proven to increase congestion in the long term via induced demand as more drivers are encouraged to use the new highway and road capacity. This in turn can reduce freight throughput as more heavy-duty vehicles are stuck in traffic along with passenger vehicles, reducing the effectiveness of the TCEP program. Avoiding funding projects that increase VMT will ensure CTC aligns with the latest update to the State Transportation Agency's Climate Action Plan for Transportation Infrastructure requiring CTC to update its TCEP guidelines to state that goods movement projects that mitigate their passenger VMT impacts are more competitive for funding.

Lastly, we ask you to consider leveraging investments in the rail network to deliver more benefits.

- Leverage CTC's grantmaking to encourage freight railroads to accelerate the deployment of zero-emissions battery electric and catenary locomotives, and use TCEP to directly fund the necessary overhead catenary wire, charging and substation infrastructure. While relying on freight rail tends to result in fewer GHG emissions, diesel locomotives continue to be a significant source of PM2.5 emissions in communities along freight lines and rail yards. In order to ensure that we are protecting communities near railways from increased exposure to air pollutants, we encourage the CTC to prioritize funding for rail projects that propose using clean locomotive engines.
- Ensure investments in freight railroads are compatible with existing and planned passenger rail infrastructure and service.

As we continue to gather more information about each of the 43 applications, we will follow up to highlight specific areas of opportunity or concern about individual projects.

The Urgency of Addressing the Health Impacts of Diesel Pollution

The Transportation Commission has an opportunity to help Californians breathe easier, especially those who live in communities near goods movement corridors most burdened by diesel pollution. Californians are suffering from dire air pollution, and racial disparities in air pollution exposure mean that "African American, Latino, and Asian Californians are exposed to [21 to 43 percent] more PM2.5 pollution from cars, trucks, and buses than white Californians," according to a study from the Union of Concerned Scientists.² The American Lung Association State of the Air report card highlights that 41 of the state's counties are scoring an F for air pollution and 98 percent of Californians live in a community impacted by unhealthy air³. Continuous failure to comply with the Clean Air Act could result in federal sanctions and the loss of transportation funding, reducing California's ability to fund freight improvement projects.

² <https://blog.ucs.org/dave-reichmuth/pollution-california-people-of-color-bear-burden/>

³ <https://www.lung.org/research/sota/city-rankings/states/california>

The US Environmental Protection Agency describes the impacts of diesel pollution as follows: “Exposure to diesel exhaust can lead to serious health conditions like asthma and respiratory illnesses and can worsen existing heart and lung disease, especially in children and the elderly. These conditions can result in increased numbers of emergency room visits, hospital admissions, absences from work and school, and premature deaths.”⁴

The California Air Resources Board adds: “Diesel engine emissions are believed to be responsible for about 70% of California’s estimated known cancer risk attributable to toxic air contaminants.”⁵

Additionally, addressing air pollution by switching to zero-emissions trucks would provide \$735 billion in public health benefits nationwide through 2050, including reduced hospitalizations and fewer missed workdays. The most heavily impacted California counties could experience tens of billions in health benefits and thousands of lives saved.⁶

We urge CTC to leverage TCEP funding as we recommend above in order to meaningfully address these health inequities faced by California’s most vulnerable communities, improve air quality to reach our state climate goals, and provide economic and workforce benefits statewide.

Policy Basis for Priorities

We believe these priorities are strongly aligned with statute and policy. The Streets and Highways Code section that governs how the Trade Corridors Improvement Fund can be spent identifies the following as eligible:

- “Environmental and community mitigation or efforts to reduce environmental impacts of freight movement, such as projects that reduce noise, overnight truck idling, or truck queues, and advanced traveler information systems such as freight advanced traveler information systems that optimize operations to reduce empty-load trips.”

The same code directs the Commission to adopt guidelines that:

- “[place] emphasis on projects that improve trade corridor mobility and safety while reducing emissions of diesel particulates, greenhouse gases, and other pollutants and reducing other negative community impacts, especially in disadvantaged communities,” and “includes disadvantaged communities measures, as established by the California Environmental Protection Agency pursuant to Section 39711 of the Health and Safety Code, and other tools the commission determines, for evaluating benefits or costs for disadvantaged communities and low-income communities.”⁷

⁴ <https://www.epa.gov/dera/learn-about-impacts-diesel-exhaust-and-diesel-emissions-reduction-act-dera>

⁵ <https://ww2.arb.ca.gov/resources/summary-diesel-particulate-matter-health-impacts>

⁶ <https://www.lung.org/getmedia/e1ff935b-a935-4f49-91e5-151f1e643124/zero-emission-truck-report>

⁷ https://leginfo.legislature.ca.gov/faces/codes_displaySection.xhtml?lawCode=SHC§ionNum=2192

Governor Newsom's Executive Order N-19-19 reinforces these policies by directing the State Transportation Agency to:

- "Align the state's climate goals with transportation spending on planning, programming and mitigation to achieve the objectives of the state's Climate Change Scoping Plan, where feasible," and to
- "Fund transportation options that contribute to the overall health of Californians and reduce greenhouse gas emissions, such as transit, walking, biking, and other active modes."⁸

The State Transportation Agency's Climate Action Plan for Transportation Infrastructure identifies strategy 1.4 "Mainstream Zero-Emissions Vehicle Infrastructure within the Trade Corridor Enhancement Program (TCEP)" by prioritizing:

- "Projects that demonstrate a significant benefit to improving the movement of freight along trade corridors, while also reducing emissions of diesel particulates, greenhouse gases, and other pollutants by creating or improving zero-emission vehicle charging or fueling infrastructure — either within the project itself or within the larger trade corridor."⁹

Additionally, the State Transportation Agency's Climate Action Plan for Transportation Infrastructure 2.0 identifies strategy 2.3 "Update the Trade Corridor Enhancement Program (TCEP) guidelines to state that goods movement projects that mitigate their passenger VMT impacts are more competitive for funding." by prioritizing:

- "Projects that mitigate passenger VMT help reduce emissions of diesel particulates, GHGs, other pollutants, and other negative community impacts. The implementation of this action will commit the CTC to update TCEP program guidelines to emphasize that projects which improve freight mobility and safety while mitigating passenger VMT impacts from their project are more likely to be competitive in the program."¹⁰

In this round of TCEP funding, CTC has an opportunity to align substantial investments with existing state priorities and directives. We urge CTC to seize this opportunity to maximize the environmental, health, and economic co-benefits of investing in zero emissions and VMT-mitigating infrastructure.

Next Steps

Thank you for taking the time to review our priorities. We would be happy to discuss further. We look forward to working with you to ensure that state transportation investments align with helping Californians breathe easier, stabilizing our climate, and supporting the economy.

⁸ <https://www.gov.ca.gov/wp-content/uploads/2019/09/9.20.19-Climate-EO-N-19-19.pdf>

⁹ <https://calsta.ca.gov/-/media/calsta-media/documents/capti-july-2021-a11y.pdf>

¹⁰ <https://calsta.ca.gov/-/media/calsta-media/documents/capti-2025-a11y.pdf>

Best,

Carter Rubin, Director of State Transportation Advocacy
Natural Resources Defense Council

Sofia Rafikova, Policy Advocate
Coalition for Clean Air

Maya Inigo-Anderson, Charge Ahead Campaign Coordinator
Communities for a Better Environment

Laura Deehan, State Director
Environment California

Maurissa Brown, Transportation Equity Program Manager
The Greenlining Institute

Jonathan Matz, California Senior Policy Manager
Safe Routes Partnership

Zack Deutsch-Gross, Policy Director
Transform

Matthew Baker, Policy Director
Planning and Conservation League

Marven Norman, Policy Coordinator
Center for Community Action and Environmental Justice

David Diaz, Executive Director
Active San Gabriel Valley

Suzanne Hume, Educational Director & Founder
CleanEarth4Kids.org

Jared Sanchez, Policy Director
CalBike

Will Barrett, Senior Director, Nationwide Advocacy, Clean Air
American Lung Association

Robert M. Gould, MD, President
San Francisco Bay Physicians for Social Responsibility

Joel Ervice, Associate Director
Regional Asthma Management & Prevention

Eli Lipmen, Executive Director
Move LA

Vanessa Forsythe, R N MSN,
California Nurses for Environmental Health & Justice

Kevin Shin, Co-Executive Director
California Walks

Jeanie Ward-Waller, Interim Director
ClimatePlan

Laura Tolhoff, Transportation Policy Director
SPUR

Ellie Cohen, CEO
The Climate Center

Marc Vukceovich, Director of State Policy
Streets For All

Rick Longinotti, Chair
Campaign for Sustainable Transportation

Andrea Marpillero-Colomina, Policy Advisor
GreenLatinos

Catherine Dodd PhD RN Health Advisor
Families Advocating for Chemical and Toxics Safety FACTS

Jamie Pew, Policy Advisor
NextGen California

Nile Malloy, Climate Justice Director
California Environmental Justice Alliance

Kyle Heiskala, Policy Co-Director
Environmental Health Coalition

Faraz Rizvi

Asian Pacific Environmental Network

Adam Mohabbat, Director
Los Angeles Cleantech Incubator

Aesha Mahmoud, Senior Strategic Campaigner
Jobs to Move America

Adriana Rizzo, Co-Founder
Californians for Electric Rai

Julio Garcia, Executive Director
Rise South City

Deb Banks, Executive Director
SABA (Sacramento Area Bicycle Advocates)

Eli Akira Kaufman, Executive Director
BikeLA

Warren J. Wells, Policy & Planning Director
Marin County Bicycle Coalition

cc:

Matthew Yosgott, Deputy Director, Programming - Senate Bill One
Beverley Newman-Burckhard, Associate Deputy Director Programming - TCEP



June 2, 2025

Tanisha Taylor
Executive Director
California Transportation Commission

RE: 2024 Trade Corridor Enhancement Program Recommendations For Funding

Dear Ms. Taylor:

On behalf of the undersigned organizations, we are grateful for the opportunity to share our recommendations on projects seeking funding from the California Transportation Commission in the current cycle of the Trade Corridor Enhancement Program (TCEP).

The Trade Corridor Enhancement Program serves an important role in supporting and improving the goods movement in California. We strongly believe that this program's goal of increasing economic prosperity and California's commitment to clean air and climate action can coexist. Our recommendations listed below support this vision and we urge you to adopt funding recommendations that not only improve our economy but also protect the health of Californians, promote zero emission goods movement, uplift disadvantaged communities, and improve overall quality of life.

We have attached an appendix to this letter that lists all of the projects we recommend be funded by the Trade Corridor Enhancement Program. In developing this list, we supported projects that achieved any of the following:

1. Projects that eliminate pollution from the freight sector by investing in zero-emission heavy-duty vehicle charging infrastructure for battery electric trucks. We were encouraged to see so many proposals for truck charging advanced to CTC for TCEP funding, and nearly \$700 million worth of proposals sent to Caltrans for consideration that did not make it into Caltrans's TCEP requests. Zero-emission infrastructure projects are also consistent with the Climate Action Plan for Transportation Infrastructure (CAPTI) principles, helping align CTC's actions with the rest of the state in reducing GHG and air pollutant emissions from the transportation sector.
2. Projects that are informed by community input and improve community multimodal mobility infrastructure along trade corridors by helping residents and travelers overcome barriers placed in their communities by major roads, rail facilities, and other infrastructure. We are encouraged to see many projects incorporate elements that support bike, pedestrian, and transit improvements as part of a larger goods movement project.
3. Projects that take a fix-it-first approach to investments that maintain existing infrastructure. We were encouraged to see proposals to keep existing facilities in a state of good repair, instead of expanding roads and bridges that come with additional long-term maintenance liabilities.

We've highlighted below some of the key projects we are excited to see proposed for funding and that we urge you to approve:

- [Gage Zero – Accelerating Zero Emission Fleet Charging on Priority Freight Corridors Project](#) (ID #16) - This project would build four multi-fleet charging hubs in Oakland, Fresno, Ontario, and San Diego. Zero-emission infrastructure is key to encouraging large and small fleets to transition to using cleaner vehicles, helping reduce air pollution and GHG emissions. Additionally, the charging hubs are located in priority population communities, helping reduce the pollution burden for the most vulnerable Californians.
- [LA Metro Long Beach-East Los Angeles Corridor Zero-Emissions Truck Project](#) (ID #43) - This project would build 133 publicly accessible charging ports for zero-emission medium and heavy-duty vehicles. These charging ports will be installed in two EV charging depots, both of which are located along SB 671 Priority Freight Corridors, identified by the CTC as locations in need of additional ZE charging stations.
- [Vaca Valley Parkway/I-505 Multimodal Improvements Project](#) (ID #34) - This project is a

great example of how TCEP funding could be used to improve freight movement, promote safety, and encourage the use of active transportation. This project would replace two intersections with roundabouts, building a new bike/ped overcrossing, four new crosswalks, ADA ramps, and a multi-use trail in the process.

Additionally, in the course of our review, we encountered projects that were not aligned with our organizations' goals of eliminating pollution from the freight sector, while reducing the impacts of the goods movement system on those who live near it and are burdened by truck traffic and pollution.

Those projects, also included in the Appendix, propose to expand roads and highways that will result in a foreseeable increase in vehicle miles traveled (VMT). These types of projects are unlikely to improve goods movement significantly, because induced passenger VMT will re-congest these corridors, while increasing the overall traffic and emissions on these roads and highways. Avoiding funding projects that increase VMT will also ensure early alignment with the latest update to the State Transportation Agency's Climate Action Plan for Transportation Infrastructure, which will require CTC to update its TCEP guidelines to state that goods movement projects that mitigate their passenger VMT impacts will be more competitive for funding. For these reasons, we ask you not to include these types of projects in the 2024 TCEP Program of Projects.

Examples of such projects include:

- [Grant Line Road Safety and Freight Mobility Project](#) (ID #41) - This project would build 3.6 miles of a new 4-lane road in Rancho Cordova. Not only would this project significantly increase local air pollutant and GHG emissions by building a new 4-lane road segment, but this project was also included in the November 2022 Sacramento Measure A ballot initiative, which received a majority No vote, showing a lack of community support for this project.
- [State Route 37 Sears Point to Mare Island Improvement Project](#) - Phase 2 (ID #1) - This project would add a tolled lane to State Route 37 near Vallejo. This project claims to help reduce congestion along the SR 37 corridor, however, studies show that new toll lanes increase VMT and congestion similar to general purpose lanes¹. Additionally, an analysis conducted by Caltrans predicts that portions of SR 37 will be completely flooded by 2050² due to increased sea level rise as a result of climate change, meaning that funding this project will provide little benefit for the state, as the road will soon become too flooded for use.
- [Interstate 680/State Route 4 Interchange Improvement \(Phase 2A and 4\)](#) (ID # 36) - This project would construct a new 2-lane flyover on I-680 in Contra Costa County. While the environmental review document states that this project will reduce emissions by reducing idling on the freeway, this review was completed prior to SB 743 and does not take into

¹ Manville, M. (2024) Induced Travel Estimation Revisited. *UCLA Institute of Transportation Studies*. <https://escholarship.org/uc/item/8m98c8j1#page=42>

² SR37 Project Background. (2025) *Caltrans*. <https://dot.ca.gov/caltrans-near-me/district-4/d4-projects/d4-37-corridor-projects/37-background>

account the induced demand impacts on VMT that will be caused by building two additional lanes, which will result in an overall increase to GHG and air pollutant emissions.

We believe these recommendations are consistent and strongly aligned with statute and policy, which ensures that TCEP can address “environmental and community mitigation or efforts to reduce environmental impacts of freight movement” and “improve trade corridor mobility and safety while reducing emissions of diesel particulates, greenhouse gases, and other pollutants and reducing other negative community impacts, especially in disadvantaged communities.”³

In this round of TCEP funding, CTC has an opportunity to align substantial investments with existing state priorities and directives. We urge CTC to seize this opportunity to maximize the environmental, health, and economic co-benefits by investing in the projects recommended above that will deliver on zero-emission and VMT-mitigating infrastructure.

Thank you for your consideration.

Sincerely,

Carter Rubin
Director of State Transportation Advocacy, NRDC

Sofia Rafikova
Policy Advocate, Coalition for Clean Air

Laura Deehan
State Director, Environment California Research and Policy Center

Debra C. Banks, Ph.D.
Executive Director, Sacramento Area Bicycle Advocates

Robert M. Gould, MD
President, San Francisco Bay Physicians for Social Responsibility

Julio Garcia
Executive Director, Rise South City

Zack Deutsch-Gross
Policy Director, Transform

Joel Ervice
Associate Director, Regional Asthma Management & Prevention

³ https://leginfo.legislature.ca.gov/faces/codes_displaySection.xhtml?lawCode=SHC§ionNum=2192

David Diaz, MPH
Executive Director, Active San Gabriel Valley

Eli Lipmen
Executive Director, Move LA

Warren J. Wells, AICP
Policy & Planning Director, Marin County Bicycle Coalition

Andrea Marpillero-Colomina, PhD
Policy Advisor, GreenLatinos

Brett Slaughenhaupt
Director of LA County Advocacy, Streets Are For Everyone

Jeanie Ward-Waller
Interim Director, ClimatePlan

Clarrissa Cabansagan
Executive Director, Silicon Valley Bicycle Coalition

Matthew Baker
Policy Director, Planning and Conservation League

Mike Swire
Stop the 101 & 280 Widenings in San Mateo County

Jamie Pew
Policy Advisor, NextGen California

Kevin Shin
Co-Executive Director, California Walks

Roman Partida-Lopez
Director of Senior Counsel/Transportation Equity, The Greenlining Institute

Marven Norman
Center for Community Action and Environmental Justice (CCA EJ)

Christy Zamani
Executive Director, Day One

Stuart Wood PhD
Executive Director, Sustainable Claremont

Suzanne Hume
Educational Director & Founder, CleanEarth4Kids.org

Ralph Propper
Environmental Council of Sacramento

Lina Mira
Executive Director, Latino and Latina Roundtable

Eli Akira Kaufman, Executive Director
BikeLA

cc:

Matthew Yosgott, Deputy Director, Programming - Senate Bill One
Beverley Newman-Burckhard, Associate Deputy Director Programming - TCEP

Appendix

Projects Recommended for Inclusion in TCEP Program:

ID #	Project Name
9	<u>Pennsylvania Avenue Grade Separation Project</u>
11	<u>BP Pulse – Electric Vehicle Oasis North Project</u>
12	<u>Tesla – California Truck Electrification Corridor Project</u>
13	<u>Watt EV – Port to Border California Freight Electrification Project</u>
15	<u>Forum Mobility – Beyond the Dock: Heavy-Duty Electrification of the Port of Oakland Priority Trade Corridors Project</u>
16	<u>Gage Zero – Accelerating Zero Emission Fleet Charging on Priority Freight Corridors Project</u>
17	<u>Renewable Properties – Fairway Electric Vehicle Charging Depot Project</u>
18	<u>Voltera – Wilmington Combined Charging Hub Project</u>
19	<u>EV Realty – South Bay Truck Charging Hub Project</u>
21	<u>Prologis Mobility – Freight Logistics Electrifications for Emission-Free Transport Project</u>
33	<u>Shoemaker Bridge Replacement Project</u>
34	<u>Vaca Valley Parkway/I-505 Multimodal Improvements Project</u>
37	<u>Santa Ana Grade Separation Project</u>
40	<u>Bridge Replacement at Las Posas Road and Ventura Boulevard</u>
43	<u>LA Metro Long Beach-East Los Angeles Corridor Zero-Emissions Truck Project</u>

Total Funding: \$238,570,000

Projects Recommended To Not Include in TCEP Program:

ID #	Project Name
1	<u>State Route 37 Sears Point to Mare Island Improvement Project - Phase 2</u>
2	<u>Santa Barbara U.S. 101 Multimodal Corridor</u>
5	<u>Watsonville-Santa Cruz Multimodal Corridor Program - State Park to Freedom Improvement Project</u>
7	<u>State Route 60 / World Logistics Center Parkway Interchange Replacement Project</u>
8	<u>State Route 84 / United States Route 101 Interchange Reimagined Project</u>
10	<u>Konocti Corridor – Segment 2B</u>
22	<u>Interstate 10 / Riverside Avenue Freight Improvement Project</u>
23	<u>Tulare Six-lane and Paige Avenue Multi-Modal Interchange Improvement Project</u>
24	<u>State Route 46 Antelope Grade Corridor Improvements Project</u>
25	<u>State Route 132 West Phase 3A Project</u>
29	<u>State Route 71 Gap Closure Project – Phase 2</u>
30	<u>Centennial Corridor Southbound State Route 99 to Westbound State Route 58 Connector Project</u>
32	<u>SR 46 East/Union Road Intersection Improvements</u>
36	<u>Interstate 680/State Route 4 Interchange Improvement (Phase 2A and 4)</u>
38	<u>Hueneme Road Widening: Edison Drive to Rice Avenue</u>
39	<u>Grant Line Road Realignment Project</u>
41	<u>Grant Line Road Safety and Freight Mobility Project</u>

June 06, 2025

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby:

We are providing this letter to express support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

We support the proposed investments in projects on SR 99 which will help close the existing gaps and create a seamless corridor to three lanes throughout. In doing so, this and other projects will address significant safety issues, increase the efficiency and effectiveness of the movement of goods throughout the state, including vital agricultural products, reduce negative air quality and other environmental impacts, and increase multimodal travel including supporting intercity rail, transit, and bicycle and pedestrian mobility.

SR 99 is the transportation backbone of Tulare County and the San Joaquin Valley and is a critical north-south goods movement artery for the State. It is because of the need for freight mobility and safety for Tulare County, the San Joaquin Valley and the State as a whole that continued investment in projects like these are critical.

Moreover, the San Joaquin Valley is working with the Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, Tule Branch Farms supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,

Raymond Van Beek
Owner, Tule Branch Farms

Cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission



COUNTY OF TULARE

BOARD OF SUPERVISORS

Administration Bldg. 2800 West Burrel Visalia, CA 93291

Telephone: (559) 636-5000

Fax: (559) 615-3009

June 6, 2025

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

RE: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby:

On behalf of the Tulare County Board of Supervisors, I write in support of the California Transportation Commission (CTC) staff recommendation to award \$62.7 million in Trade Corridor Enhancement Program (TCEP) funding for the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This funding will complete the funding for the construction phase set to begin in FY 26/27. The project has already secured \$98 million in federal INFRA funds and will include a local match of \$11.3 million from TCAG's Measure R sales tax.

Investments in SR 99 are essential to closing existing gaps and creating a continuous three-lane corridor. This project will improve safety, enhance goods movement—including critical agricultural freight—reduce environmental impacts, and support multimodal travel, including transit, rail, and active transportation.

SR 99 is the backbone of Tulare County and the San Joaquin Valley, serving as a key north-south freight route for California. Continued investment in projects like this is vital to regional mobility, economic growth, and statewide goods movement.

The San Joaquin Valley, in partnership with Caltrans, is also advancing a new multimodal corridor plan to support sustainable communities, affordable housing, and expanded transportation options. Completing SR 99 is central to that vision.

For these reasons, I strongly support approval of TCEP funding for the Tulare Six-Lane and Paige Avenue Interchange project.

Sincerely,

Pete Vander Poel, Chair
Tulare County Board of Supervisors

cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission

Larry Micari
District One

Pete Vander Poel
District Two

Amy Shuklian
District Three

Eddie Valero
District Four

Dennis Townsend
District Five



June 9, 2025

Commissioner Darnell Grisby, Chair
California Transportation Commission
1120 N Street MS 52
Sacramento, CA 95814

RE: Central Coast Coalition Support for CTC SB1 Cycle 4 Staff Recommendations

Dear Chair Grisby & Members of the California Transportation Commission:

The Central Coast Coalition strongly supports the California Transportation Commission (CTC) staff recommendations for grant funding allocations from the Senate Bill 1 (SB1) Cycle 4 competitive programs. We thank Commission staff for their collaborative effort to develop program guidelines and select projects during a very short timeframe so that we can expeditiously build these much-needed projects:

Sponsor Agencies	Project Title	SB1 Program(s)	Recommendation
California Department of Transportation and Santa Barbara County Association of Governments	Santa Barbara U.S. 101 Multimodal Corridor Project	Local Partnership Program – Competitive and Solutions for Congested Corridors Program	\$134 million
California Department of Transportation and Santa Cruz County Regional Transportation Commission	Watsonville-Santa Cruz Multimodal Corridor Program	Local Partnership Program – Competitive and Solutions for Congested Corridors Program	\$128.7 million
California Department of Transportation and Transportation Agency for Monterey County	State Route 156 Castroville Boulevard Interchange Project	Local Partnership Program - Competitive and Trade Corridor Enhancement Program	\$90.6 million

The Coalition appreciates the support for recommended projects listed above. The Central Coast also had projects not recommended for Cycle 4 funding, such as the Highway 46/Union Road overpass. This project and others will be updated and revised, and we would appreciate consideration in future SB1 cycles.

As the area's regional transportation planning agencies in San Benito, Santa Cruz, Monterey, San Luis Obispo, and Santa Barbara counties, in partnership with the Association of Monterey Bay Area Governments, we highly value the SB1 revenues. These grants will help deliver truly transformative transportation projects in our region.

We strongly support the CTC staff recommendations and urge the Commission to approve the funding at their June 26, 2025, meeting.

If you have any questions, please contact SBCAG Executive Director, Marjie Kirn, at (805) 600-4497 or by email at mkim@sbcag.org.



Central Coast Coalition
Moving California's Economy

We greatly appreciate our partnership with Commissioners and CTC staff.

Sincerely,

Marjie Kirn, Executive Director
Santa Barbara Association of Governments

Pete Rodgers, Executive Director
San Luis Obispo Council of Governments

Todd Muck, Executive Director
Transportation Agency for Monterey County

Sarah Christensen, Executive Director
Santa Cruz Co. Regional Transportation
Commission

Binu Abraham, Executive Director
San Benito Council of Governments

Maura Twomey, Executive Director
Association of Monterey Bay Area Governments

cc: Mr. Toks Omishakin, Secretary, California State Transportation Agency
Ms. Tanisha Taylor, Executive Director, California Transportation Commission
Mr. Mike Keever, Acting Director, California Department of Transportation
Mr. Paul Golaszewski, Chief Deputy Director, California Transportation Commission
Mr. James Anderson, Chief, Programming, California Department of Transportation
Mr. Scott Eades, District 5 Director, California Department of Transportation
Mr. Matthew Yosgot, Deputy Director, California Transportation Commission
Ms. Naveen Habib, Associate Deputy Director, California Transportation Commission
Ms. Leishara Ward, Associate Deputy Director, California Transportation Commission
Ms. Beverly Newman-Burckhard, Associate Deputy Director, California Transportation Commission
Mr. Darron Hill, Deputy District Director, Asset and Program Project Management, California Department of Transportation
Mr. Joe Erwin, Office Chief, Project Management, California Department of Transportation
Mr. Sarkes Khachek, SBCAG Director of Programming, Coalition Chief of Staff



June 9, 2025

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby:

The Tulare Chamber of Commerce, with 668 members representing more than 12,000 associates, is writing to express strong support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which is scheduled to start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding, and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

We support the proposed investments in projects on SR 99 which will help close the existing gaps and create a seamless corridor to three lanes throughout. In doing so, this and other projects will address significant safety issues, increase the efficiency and effectiveness of the movement of goods throughout California (including vital agricultural products), and reduce negative air quality and other environmental impacts. In addition, this project increases multimodal travel, including the support of intercity rail, transit, and bicycle and pedestrian mobility.

SR 99 is the transportation backbone of Tulare County and the San Joaquin Valley and is a critical north-south goods movement artery for the State of California. It is because of the need for freight mobility and safety for Tulare County, the San Joaquin Valley, and the State as a whole, that continued investment in projects like these is critical.

Moreover, the San Joaquin Valley is working with Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

220 E. Tulare Avenue • P.O. Box 1435 • Tulare CA 93275-1435 • 559.686.1547 • Fax 559.686.4915

WWW.TULARECHAMBER.ORG



Page 2 – June 9, 2025

The Honorable Darnell Grisby, Chair, California Transportation Commission

Finally, we would like to add that, as a popular tourist destination, California welcomes millions annually to the three national parks located in the San Joaquin Valley. Offering the safest travel routes for our visitors increases the attractiveness of these amenities which create a substantial economic impact, sustain jobs across hospitality and other industries, and contribute significantly to sales tax revenue.

For the many reasons noted, the Tulare Chamber of Commerce supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,

A handwritten signature in black ink, appearing to read "Donnette Silva Carter".

Donnette Silva Carter, IOM
Chief Executive Officer

Cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission



*Compassionate
Quality
Health Care*

Cartmill Campus -
Administration
1134 East Cartmill Avenue
Tulare, CA 93274

Main Clinic
1101 N Cherry Street
Tulare, CA 93274

Chiropractic Care
1203 N Cherry Street
Tulare, CA 93274

South Tulare
3035 E Bardsley Avenue
Tulare, CA 93274

West Tulare
1500 W Tulare Drive
Tulare, CA 93274

Woodville
16686 Road 168
Woodville, CA 93257

June 9, 2025

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby:

Altura Centers for Health (ALTURA) is this letter to express support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

ALTURA is a Federally Qualified Health Center/Non-Profit clinic. ALTURA had 179,879 visits in Calendar Year (CY 2024) and 32,082 unduplicated patients. Currently employs 288 employees. Also, contracts janitorial, maintenance, and other services. ALTURA patients drive from surrounding Tulare, Kerns, Fresno, and Kings counties.

ALTURA supports the proposed investments in projects on SR 99 which will help close the existing gaps and create a seamless corridor to three lanes throughout. In doing so, this and other projects will address significant safety issues, increase the efficiency and effectiveness of the movement of goods throughout the state, including vital agricultural products, reduce negative air quality and other environmental impacts, and increase multimodal travel including supporting intercity rail, transit, and bicycle and pedestrian mobility.

SR 99 is the transportation backbone of Tulare County and the San Joaquin Valley and is a critical north-south goods movement artery for the State. It is because of the need for freight mobility and safety for Tulare County, the San Joaquin Valley and the State as a whole that continued investment in projects like these are critical.

Moreover, the San Joaquin Valley is working with the Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, the Altura Centers for Health supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,

A handwritten signature in blue ink, appearing to read "Graciela Soto Perez".

Graciela Soto Perez, MHA
Chief Executive Officer

Cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission



June 9, 2025

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Brett Taylor
Mayor

Liz Wynn
Vice Mayor

Steven A. Nelsen
Councilmember

Emmanuel
Hernandez Soto
Councilmember

Brian Poochigian
Councilmember

Dear Chair Grisby,

We are providing this letter to express support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

We support the proposed investments in projects on SR 99 which will help close the existing gaps and create a seamless corridor to three lanes throughout. In doing so, this and other projects will address significant safety issues, increase the efficiency and effectiveness of the movement of goods throughout the state, including vital agricultural products, reduce negative air quality and other environmental impacts, and increase multimodal travel including supporting intercity rail, transit, and bicycle and pedestrian mobility.

SR 99 is the transportation backbone of Tulare County and the San Joaquin Valley and is a critical north-south goods movement artery for the State. It is because of the need for freight mobility and safety for Tulare County, the San Joaquin Valley and the State as a whole, that continued investment in projects like these are critical.

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The Honorable Darnell Grisby
Chair, California Transportation Commission
Page 2
June 9, 2025

Moreover, the San Joaquin Valley is working with Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, the City of Visalia supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,

A handwritten signature in blue ink, appearing to read "Brett Taylor", with a stylized flourish extending to the right.

Brett Taylor, Mayor

Cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission



LANE ENGINEERS, INC.

Civil • Structural • Surveying
P.O. Box 1059
Tulare, CA 93275
Phone: (559) 688-5263 / Fax: (559) 688-8893

June 10, 2025

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and
Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby:

We are providing this letter to express support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

We support the proposed investments in projects on SR 99 which will help close the existing gaps and create a seamless corridor to three lanes throughout. In doing so, this and other projects will address significant safety issues, increase the efficiency and effectiveness of the movement of goods throughout the state, including vital agricultural products, reduce negative air quality and other environmental impacts, and increase multimodal travel including supporting intercity rail, transit, and bicycle and pedestrian mobility.

SR 99 is the transportation backbone of Tulare County and the San Joaquin Valley and is a critical north-south goods movement artery for the State. It is because of the need for freight mobility and safety for Tulare County, the San Joaquin Valley and the State as a whole that continued investment in projects like these are critical.

Moreover, the San Joaquin Valley is working with the Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, Lane Engineers, Inc. supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,

Kevin Nunes
Professional Engineer

Cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission

June 10, 2025

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby:

We are providing this letter to express support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.


We support the proposed investments in projects on SR 99 which will help close the existing gaps and create a seamless corridor to three lanes throughout. In doing so, this and other projects will address significant safety issues, increase the efficiency and effectiveness of the movement of goods throughout the state, including vital agricultural products, reduce negative air quality and other environmental impacts, and increase multimodal travel including supporting intercity rail, transit, and bicycle and pedestrian mobility.

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Moreover, the San Joaquin Valley is working with the Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, I supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,



Renee Soto

Cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission



6/10/25

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby:

We are providing this letter to express support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project, which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding, and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

We support the proposed investments in projects on SR 99, which will help close the existing gaps and create a seamless corridor with three lanes throughout. In doing so, this and other projects will address significant safety issues, increase the efficiency and effectiveness of the movement of goods throughout the state, including vital agricultural products, reduce negative air quality and other environmental impacts, and increase multimodal travel, including supporting intercity rail, transit, and bicycle and pedestrian mobility.

SR 99 is the transportation backbone of Tulare County and the San Joaquin Valley, serving as a critical north-south goods movement artery for the State. Due to the need for freight mobility and safety in Tulare County, the San Joaquin Valley, and the State as a whole, continued investment in projects like these is critical.

Moreover, the San Joaquin Valley is working with Caltrans to develop a new multimodal corridor plan, which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, the Best Western Tulare supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,



Tony Cota

General Manager

Cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission

Town & Country Lodge

1051 N. Blackstone St, Tulare, Ca 93274 P: (559) 688-7537 Reservations: (888) 488-5273
bestwesterntulare.com

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OFFICE OF THE MAYOR

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Dinuba, CA 93618

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F: (559) 591-4246

C: (559) 305-8361

scottharness3@gmail.com

June 10, 2025

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby,

We are providing this letter to express support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

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Moreover, the San Joaquin Valley is working with the Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, the City of Dinuba supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,

A handwritten signature in black ink, reading "Rachel Niero-Guerrero". The signature is written in a cursive, flowing style.

Rachel Niero-Guerrero
Mayor

cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission



TULARE JOINT UNION

High School District

BOARD OF TRUSTEES

Laura Fonseca
Craig Hamilton
Cathy Mederos
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DISTRICT ADMINISTRATION

Lucy Van Scyoc, Ed.D.
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Assistant Superintendent
Human Resources and
Business

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Assistant Superintendent
Student Services and
Special Programs

Kevin Covert
Assistant Superintendent
Curriculum, Technology and
Assessment

Dereck Domingues
Director I
Child Welfare & Safety

June 10, 2025

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby:

We are providing this letter to express support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

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SR 99 is the transportation backbone of Tulare County and the San Joaquin Valley and is a critical north-south goods movement artery for the State. It is because of the need for freight mobility and safety for Tulare County, the San Joaquin Valley and the State as a whole that continued investment in projects like these is critical.

Moreover, the San Joaquin Valley is working with Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, the Tulare Joint Union High School District supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,



Lucy Van Scyoc, Ed.D.
Superintendent



From: [Remedios, Douglas@CATC](mailto:Remedios.Douglas@CATC)
To: [Newman-Burckhard, Beverley@CATC](mailto:Newman-Burckhard.Beverley@CATC); [Lopez, Kenneth@CATC](mailto:Lopez.Kenneth@CATC)
Cc: [Yosgott, Matthew J@CATC](mailto:Yosgott.Matthew.J@CATC)
Subject: FW: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS
Date: Tuesday, June 10, 2025 4:28:13 PM
Attachments: [image002.png](#)

Please see below

Douglas Remedios
Interim Deputy Director, Administration and Financial Management
California Transportation Commission
1120 N Street, MS-52
Sacramento, CA 95814
www.catc.ca.gov

From: Tricia Stever Blattler <pstever@tulcofb.org>
Sent: Tuesday, June 10, 2025 4:18 PM
To: Remedios, Douglas@CATC <Douglas.Remedios@catc.ca.gov>
Subject: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

EXTERNAL EMAIL. Links/attachments may not be safe.



The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby:

Tulare County Farm Bureau represents over 1100 members, specifically farmers and ranchers, and agri-businesses in Tulare County.

We are providing this letter to express support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

We support the proposed investments in projects on SR 99 which will help close the existing gaps and create a seamless corridor to three lanes throughout. In doing so, this and other projects will address significant safety issues, increase the efficiency and effectiveness of the movement of goods throughout the state, including vital agricultural products, reduce negative air quality and other environmental impacts, and increase multimodal travel including supporting intercity rail, transit, and bicycle and pedestrian mobility.

SR 99 is the transportation backbone of Tulare County and the San Joaquin Valley and is a critical north-south goods movement artery for the State. It is because of the need for freight mobility and safety for Tulare County, the San Joaquin Valley and the State as a whole that continued investment in projects like these is critical.

Moreover, the San Joaquin Valley is working with the Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, the Tulare County Farm Bureau supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,

A handwritten signature in black ink, reading "Tricia Stever Blattler". The signature is written in a cursive, flowing style.

Tricia Stever Blattler
Executive Director

Cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission

From: [Remedios, Douglas@CATC](mailto:Remedios.Douglas@CATC)
To: [Newman-Burckhard, Beverley@CATC](mailto:Newman-Burckhard.Beverley@CATC); [Lopez, Kenneth@CATC](mailto:Lopez.Kenneth@CATC)
Cc: [Yosgott, Matthew J@CATC](mailto:Yosgott.Matthew.J@CATC)
Subject: FW: SR99 Support Letter
Date: Tuesday, June 10, 2025 9:22:48 AM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)

Please see below.

Thank you,

Douglas Remedios
Interim Deputy Director, Administration and Financial Management
California Transportation Commission
1120 N Street, MS-52
Sacramento, CA 95814
www.catc.ca.gov

From: Andy Daniels <andy@franksautomotiverepair.com>
Sent: Tuesday, June 10, 2025 9:14 AM
To: Remedios, Douglas@CATC <Douglas.Remedios@catc.ca.gov>
Subject: SR99 Support Letter

EXTERNAL EMAIL. Links/attachments may not be safe.

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Re: Support for TCEP Funding – SR 99 Tulare Six-Lane and Paige Avenue Interchange Project

Dear Chair, Grisby:

On behalf of **Frank's Automotive Inc.**, I am writing to express strong support for the California Transportation Commission (CTC) staff's recommendation to allocate \$62.7 million in Trade Corridor Enhancement Program (TCEP) funding for the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This funding will complete the financial package needed to advance the construction phase, scheduled to begin in FY 2026/27.

The project has already secured \$98 million in federal Infrastructure for Rebuilding America (INFRA) grant funding. The TCEP allocation would be further leveraged by \$11.3 million from the Tulare County Association of Governments' (TCAG) regional sales tax program, Measure R. We commend the CTC's continued commitment to upgrading SR 99—a vital corridor for freight, commuter travel, and agricultural goods movement across the San Joaquin Valley and the broader state. Projects like this are instrumental in eliminating bottlenecks, enhancing safety, improving air quality, and expanding access to multimodal transportation options such as rail, transit, biking, and walking.

SR 99 serves as a central transportation artery in Tulare County and throughout the Valley, playing a crucial role in California's goods movement network. Continued investments in this corridor are essential to improving freight mobility and safety across the region.

In addition, the San Joaquin Valley is actively collaborating with Caltrans to create a new multimodal corridor plan that builds on ongoing efforts to develop sustainable communities, affordable housing, and broader transportation choices. Completing SR 99 is a strategic component of this vision and supports long-term regional and statewide goals.

For these reasons, **Frank's Automotive Inc.** fully supports the approval of the TCEP funding allocation for the SR 99 Tulare Six-Lane and Paige Avenue Interchange project.

Sincerely,

Frank Andy Daniels Jr.

President

Andy Daniels

President, Owner

Franks Automotive Repair, Inc.

1459 S. K St.

Tulare, CA 93274

559.688.7384

559.571.1355 (Direct Line)

www.franksautomotiverepair.com



Virus-free. www.avast.com

California State Senate

CAPITOL OFFICE

1021 O STREET, SUITE 7150
SACRAMENTO, CA 95814
TEL (916) 651-4012

SENATOR.GROVE@SENATE.CA.GOV

SENATOR SHANNON GROVE

TWELFTH SENATE DISTRICT



DISTRICT OFFICES

5701 TRUXTUN AVE., SUITE 150
BAKERSFIELD, CA 93309
TEL (661) 323-0443

567 WEST SHAW AVE., SUITE A-3
FRESNO, CA 93704
TEL (559) 243-8580

June 10, 2025

The Honorable Darnell Grisby, Chair
California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

**Re: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane
and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS**

Dear Chair Grisby,

As a State Senator representing this part of the state, I am pleased to offer my support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

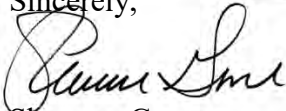
The proposed investments in projects on SR 99 will help close the existing gaps and create a seamless corridor to three lanes throughout. In doing so, this and other projects will address significant safety issues, increase the efficiency and effectiveness of the movement of goods throughout the state, including vital agricultural products, reduce negative air quality and other environmental impacts, and increase multimodal travel including supporting intercity rail, transit, and bicycle and pedestrian mobility.

SR 99 is the transportation backbone of Tulare County and the San Joaquin Valley and is a critical north-south goods movement artery for the state. It is because of the need for freight mobility and safety for Tulare County, the San Joaquin Valley and the state as a whole that continued investment in projects like these are critical.

Moreover, the San Joaquin Valley is working with the Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, I am offering my support for the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project. Should you have any questions, please call my office at (661) 323-0443.

Sincerely,

A handwritten signature in black ink, appearing to read "Shannon Grove", is written over a light gray rectangular background.

Shannon Grove
Senator, 12th District

Cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission



Office of the City Council
411 East Kern, Tulare, CA 93274
559.684.4200 | www.tulare.ca.gov

June 11, 2025

The Honorable Darnell Grisby
Chair, California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Subject: Trade Corridor Enhancement Program (TCEP) Funding: SR 99 Tulare Six-Lane and Paige Ave Interchange – SUPPORT FOR CTC STAFF RECOMMENDATIONS

Dear Chair Grisby,

We are providing this letter to express support for the approval of the California Transportation Commission (CTC) staff recommendations for \$62.7 million of Trade Corridor Enhancement Program (TCEP) funding for the construction of the State Route 99 (SR 99) Tulare Six-Lane and Paige Avenue Interchange project. This would complete the necessary funding for the construction phase of the project which will start in FY 26/27. The California Department of Transportation (Caltrans) and the Tulare County Association of Governments (TCAG) have already received \$98 million in federal competitive Infrastructure for Rebuilding America (INFRA) funding and the recommended TCEP funding is being matched with \$11.3 million of TCAG's regional sales tax, Measure R.

We support the proposed investments in projects on SR 99 which will help close the existing gaps and create a seamless corridor to three lanes throughout. In doing so, this and other projects will address significant safety issues, increase the efficiency and effectiveness of the movement of goods throughout the state, including vital agricultural products, reduce negative air quality and other environmental impacts, and increase multimodal travel including supporting intercity rail, transit, and bicycle and pedestrian mobility.

SR 99 is the transportation backbone of Tulare County and the San Joaquin Valley and is a critical north-south goods movement artery for the State. It is because of the need for freight mobility and safety for Tulare County, the San Joaquin Valley and the State as a whole that continued investment in projects like these are critical.

Moreover, the San Joaquin Valley is working with the Caltrans to develop a new multimodal corridor plan which will enhance and add to the already significant work being done to create sustainable communities, build affordable housing, and increase multimodal opportunities. A completed SR 99 is a vital part of the global strategy for the region.

For these reasons, the City of Tulare supports the approval of TCEP funding for the Tulare Six-Lane and Paige Ave Interchange project.

Sincerely,

Patrick Isherwood
Mayor

Cc: Honorable Commissioners, California Transportation Commission
Tanisha Taylor, Executive Director, California Transportation Commission

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.7, Action

Prepared By: Leishara Ward
Associate Deputy Director

Published Date: June 13, 2025

Subject: 2024 Local Partnership Competitive Program Adoption – Program of Projects,
Resolution G-25-43

Recommendation:

Staff recommends the California Transportation Commission (Commission) adopt the 2024 Local Partnership Competitive Program of Projects, as presented in the project list (Attachment B) and consistent with Resolution G-25-43 (Attachment A).

Issue:

The Commission established the 2024 Local Partnership Competitive Program as a two-year, \$195,670,000 program covering fiscal years 2025-26 and 2026-27. Commission staff recommends funding 11 projects for a total of \$201,655,000. The recommended program is over-programmed by \$5,985,000. This amount is anticipated to be offset by future unused Local Partnership Program incentive funds set aside each fiscal year, or through future project cost savings, consistent with the Local Partnership Competitive Program Guidelines. The projects leverage nearly \$14 billion in additional federal, state, and local funds, and will generate over 180,000 jobs across the state, promoting a vibrant economy and a sustainable, equitable, and innovative transportation system for all Californians.

The recommended investments will reward cities and counties who have each received voter-approval or board action to tax or fee themselves to locally generate more revenue for transportation projects. Together, Local Partnership Program funding, matched with local funding commitments, will provide safer transportation options to residents, workers, and visitors alike, through improvements to rail and transit, bicycle and pedestrian facilities, and road and highway infrastructure. The projects reduce air pollution with zero-emission public transit and active transportation upgrades, creating a healthier environment for Californians. They reduce congestion with interchange and local road improvements that promote safety. They uplift community voices by incorporating community feedback into project designs. And they include improvements to active transportation with dedicated bike lanes, adaptive traffic signal control systems, new sidewalks, and safer intersections to reduce pedestrian, bicyclist, and vehicular conflicts – each of which were supported by the communities that engaged with project sponsors.

The 11 projects recommended for funding were developed with robust in-person public engagement, in multiple languages, and various formats, and responded to community feedback by incorporating the community's input into project design. The Interagency Equity Advisory Committee supported project evaluations by reviewing and rating the community engagement sections of each application, influencing project selection by prioritizing projects that met or exceeded expectations for effective community involvement and incorporating feedback directly into project designs. Ultimately, projects recommended for funding reflected this prioritization.

The recommended Program of Projects was posted on the Commission's website on June 6, 2025, and is included as Attachment B.

Recommendations Development

The Commission received 41 project nominations totaling \$550,956,000 in 2024 Local Partnership Competitive Program funding requests. Each project underwent a comprehensive review to determine its applicant's eligibility status as either a voter-approved or imposed tax or fee jurisdiction.

Evaluation teams comprised of Commission and Caltrans staff, evaluated project nominations using the screening and evaluation criteria identified in the [2024 Local Partnership Competitive Program Guidelines](#), which included community and economic benefits, environmental impacts and environmental clearance, project readiness and deliverability, and alignment with state climate, equity and housing goals.

In addition, California Department of Housing and Community Development staff evaluated the Transportation, Land Use and Housing criterion, and members of the Interagency Equity Advisory Committee evaluated the Community Engagement evaluation criterion.

The 11 recommended projects were selected for their competitiveness across the criteria established in the guidelines and are projected to deliver numerous benefits including:

- Minimized and reduced vehicle miles traveled
- Reduced congestion and improved accessibility
- Improved air quality and reduced greenhouse gas emissions
- Increased ridership for both bus and rail transit system users
- Expanded mobility options and access to key destinations for all travelers

Projects not recommended for funding demonstrated travel benefits but were found less competitive for various reasons: construction start dates falling outside the program cycle; inconsistent or ineligible funding plans; insufficient local match; little or limited community engagement; lower demonstrated benefits relative to cost; or full funding was secured from another SB 1 program.

Commission staff welcome continued engagement from nominating agencies and encourage them to seek technical assistance to strengthen future submissions. Past applicants that have sought technical assistance and re-applied for funding have proven more successful in subsequent program cycles.

Project Highlights

The 11 recommended projects demonstrate alignment with the Climate Action Plan for Transportation Infrastructure and state climate goals by improving and enhancing public health, housing, safety, equity, and economic prosperity for Californians. The recommended Program will minimize or reduce vehicle miles traveled; improve safety measures for motorized and non-motorized users; transition old natural gas buses to battery electric buses; increase mobility and accessibility; improve and replace bridges and interchanges; improve pavement conditions; reduce greenhouse gas emissions; and advance transportation and housing goals through pro-housing designations as determined by the Department of Housing and Community Development.

Among the recommended projects, a sampling that highlight state objectives include:

- **Howard Streetscape Project (\$14,000,000)** in the City of San Francisco, will remove one to two westbound vehicle traffic lanes, install a permanent two-way Class IV bikeway, construct various safety improvements to pedestrian crossings and intersections, new civic amenity zones, and improvements related to urban greening and green infrastructure.
- **I Street Bridge Replacement Project (\$25,000,000)** in the City of Sacramento, will replace an existing two-lane bridge over the Sacramento River, and will connect to the City of West Sacramento, with enhanced approach structures leading to a two-lane bridge containing a new roadway alignment. The new bridge includes a moveable vertical lift center span to accommodate river-borne traffic, Class II buffered bike lanes, and sidewalks along each side of the bridge.
- **Seventh Street Bridge Replacement Project (\$15,000,000)** in the City of Modesto, the project will replace a structurally deficient and functionally obsolete bridge with a new bridge that is raised to meet current hydraulic design requirements. The project provides Class II bike lanes in each direction, a separated multi-use path on the west side of the bridge, a standard sidewalk on the east side, and improves the adjacent intersections.
- **SR 156/Castroville Boulevard Interchange Project (\$10,300,000)** on the west end of the SR 156 corridor in the unincorporated, rural community of Castroville, Monterey County, will construct a new interchange at Castroville Boulevard and State Route 156. The project is a safety, economic development, and congestion relief project in northern Monterey County that will convert the existing at-grade signalized intersection at Castroville Boulevard into a new grade-separated interchange. The project will enhance safety for residents in the low-income, rural community of Castroville, reduce traffic congestion for trucks traveling to and from the region's largest agricultural distribution center, improve access for visitors to the Monterey Peninsula, and improve the quality of life for all communities who use this corridor.
- **Central Ukiah Circulation Improvement Project (\$2,438,000)** in the City of Ukiah, will install replacement water and sewer utilities, reconstruct storm drains, roadways and sidewalks, and construct Americans with Disabilities Act accessible curb ramps and driveways, pedestrian crossings and bike lanes.

- **Streamview Drive Revitalization Phase II Project (\$10,000,000)** in the City of San Diego, on Streamview Drive from 54th Street to College Avenue, this project will provide access improvements for all users: pedestrians, bicyclists, bus riders, and motorists by installing and improving sidewalks, and installing bikes lanes, pop outs, and roundabouts. This project improves safety for all travelers in this corridor.
- **Zero-Emission Bus Project (34 Battery-Electric Bus, Division 7) (\$24,917,000)** in Los Angeles County, the project includes the procurement of 34 new battery-electric buses to replace 34 Compressed Natural Gas buses currently in service on Metro Bus Division 7 transit lines. The 34 new battery-electric buses will have on-board amenities including Wi-Fi and will operate with lower noise and vibration than existing Compressed Natural Gas buses. Additionally, the new battery-electric buses will feature all-door boarding, which is estimated to reduce line running times by 2.5 percent. This project fulfills Metro's commitment to transform bus service by introducing zero-emission buses together with enhanced high-frequency transit corridor infrastructure to improve speed, reliability, and safety in high-priority corridors that serve some of Los Angeles county's most disadvantaged communities.

Baseline Agreement Requirements

All agencies with projects funded in the 2024 Local Partnership Competitive Program must comply with the Commission's 2024 Local Partnership Program Guidelines and [Senate Bill 1 \(SB 1\) Accountability and Transparency Guidelines](#).

All projects included in the 2024 Local Partnership Competitive Program with a total project cost of \$25 million or greater, or a contribution of 2024 Local Partnership Competitive Program funds of \$10 million or greater, must enter into a baseline agreement approved by the Commission. Baseline Agreement development is initiated upon program adoption. Baseline Agreements stipulate the agreed-upon expected benefits, delivery schedules, project costs, and funding plans. Baseline Agreements identify agencies that must comply with SB 1 reporting requirements and include cost-sharing agreements that detail how cost overruns will be covered financially by project sponsors and implementing agencies. Executed Baseline Agreements serve as benchmarks for comparison to the current status of a project for subsequent reporting purposes. Baseline Agreements must be signed by the Caltrans Director and District Director, Commission's Executive Director, project applicant, and implementing agency.

Background:

On April 28, 2017, Governor Brown signed legislation creating the Local Partnership Program (SB 1 [Beall, Chapter 5, Statutes of 2017]). Assembly Bill 115 (Ting, Chapter 20, Statutes of 2017) clarified SB 1 language regarding local and regional transportation agency eligibility and expanded the types of projects eligible for the program. SB 1 authorizes \$200 million annually to fund the Local Partnership Program.

The Local Partnership Program provides funding to counties, cities, districts, and regional transportation agencies in which voters have approved fees or taxes dedicated solely to transportation improvements or that have imposed fees, including uniform developer fees, dedicated solely to transportation improvements.

Beginning in December 2023, Commission staff hosted four public workshops to solicit stakeholder feedback to develop the 2024 Local Partnership Competitive Program Guidelines. Commission staff prepared the 2024 Local Partnership Competitive Program Guidelines in consultation with the Interagency Equity Advisory Committee, Caltrans, California Department of Housing and Community Development, regional transportation planning agencies, local agencies, advocates, and other stakeholders. On August 15, 2024, the Commission adopted the 2024 Local Partnership Competitive Program Guidelines, which describe the policy, standards, criteria, and procedures for the development, adoption, and management of the Local Partnership Competitive Program.

Commission staff hosted 26 virtual office hour sessions from March to May 2024 to provide technical assistance to applicants for project nominations for the 2024 Local Partnership Competitive Program.

Attachments:

- Attachment A: Resolution G-25-43
- Attachment B: 2024 Local Partnership Competitive Program Adoption – Program of Projects
- Attachment C: Comment Letters

**CALIFORNIA TRANSPORTATION COMMISSION
2024 LOCAL PARTNERSHIP COMPETITIVE PROGRAM ADOPTION
PROGRAM OF PROJECTS**

RESOLUTION G-25-43

- 1.1 **WHEREAS**, Senate Bill 1 (SB 1) (Beall, Chapter 5, Statutes of 2017), enacted as the Road Repair and Accountability Act of 2017, created the Local Partnership Program to provide funding to jurisdictions that have sought and received voter-approval of taxes or that have imposed certain fees, including uniform developer fees, which are dedicated solely to transportation improvements for the purpose of funding road maintenance and rehabilitation; and
- 1.2 **WHEREAS**, Assembly Bill 115 (AB 115) (Ting, Chapter 20, Statutes of 2017) clarified SB 1 language regarding local and regional transportation agency eligibility and expanded the types of projects eligible for program funding; and
- 1.3 **WHEREAS**, on August 15, 2024, the California Transportation Commission (Commission) adopted the 2024 Local Partnership Program Guidelines; and
- 1.4 **WHEREAS**, the Commission established the 2024 Local Partnership Competitive Program as a two-year, \$195,670,000 program covering fiscal years 2025-26 and 2026-27; and
- 1.5 **WHEREAS**, on November 20, 2024, the Commission received 41 project nominations, totaling \$575,956,000 in funding requests; and
- 1.6 **WHEREAS**, Commission staff prepared program recommendations that included \$201,655,000 for 11 projects representing total project costs of \$14 billion.
- 1.7 **WHEREAS**, the over-programmed amount totaling \$5,985,000 included in the program recommendations is anticipated to be funded with the redistribution of unused set-aside incentive funds from Fiscal Year 2025-26 and 2026-27 or through future cost savings; and any remaining 2024 Program over-programmed amount will be deducted from the 2026 Program; and
- 1.8 **WHEREAS**, on June 6, 2025, Commission staff recommendations for the 2024 Local Partnership Competitive Program were posted on the Commission's website; and
- 1.9 **WHEREAS**, the Commission considered the staff recommendations and public testimony during its June 26-27, 2025, Commission meeting.

- 2.1 **NOW THEREFORE BE IT RESOLVED**, that the Commission hereby adopts the 2024 Local Partnership Competitive Program, as reflected in Attachment B; and
- 2.2 **BE IT FURTHER RESOLVED**, that the amounts approved for funding shall be considered a "not-to-exceed" amount, and any increases beyond the estimates reflected in the adopted program shall be the responsibility of the nominating agency; and
- 2.3 **BE IT FURTHER RESOLVED**, that projects included in the adopted 2024 Local Partnership Competitive Program must comply with the 2024 Local Partnership Competitive Program Guidelines and the SB 1 Accountability and Transparency Guidelines; and
- 2.4 **BE IT FURTHER RESOLVED**, that projects included in the adopted 2024 Local Partnership Competitive Program with a total project cost of \$25 million or greater, or a total programmed amount of \$10 million or greater, must enter into a Baseline Agreement to be approved by the Commission; and
- 2.5 **BE IT FURTHER RESOLVED**, that if a recommended project requires a Baseline Agreement and requests allocation for project components between the June 2025 and October 2025 Commission meetings, the project applicant must submit a Baseline Agreement for approval by the October 2025 Commission meeting and that any remaining project component Baseline Agreements must be submitted by the December 2025 Commission Meeting; and
- 2.6 **BE IT FURTHER RESOLVED**, that the Commission may remove from the program any project for which a Baseline Agreement is not executed; and
- 2.7 **BE IT FURTHER RESOLVED**, that the Commission will not consider approval of a project allocation without an approved Baseline Agreement; and
- 2.8 **BE IT FURTHER RESOLVED**, that Commission staff, in consultation with the California Department of Transportation and project sponsors, is authorized to make minor technical changes to the 2024 Local Partnership Competitive Program to reflect the most current information or clarify the Commission's programming commitments, and shall seek Commission approval for any substantive changes; and
- 2.9 **BE IT FURTHER RESOLVED**, that the Commission directs Commission staff to post the 2024 Local Partnership Competitive Program of Projects on the Commission's website.

2024 Local Partnership Competitive Program
Program of Projects - Projects Recommended for Funding
(\$1,000s)
Resolution G-25-43

County	Nominating Agency	Project Title	Project Type	Fiscal Year	Total Project Cost	Recommended
Los Angeles	Los Angeles County Metropolitan Transportation Authority	Zero Emission Bus (34 BEBs, Division 7)	Bus purchase - 34 Battery Electric Buses	2026-27	\$ 62,131	\$ 24,917
Mendocino	City of Ukiah	Central Ukiah Circulation Improvement Project	Active Transportation with Stormwater	2025-26	\$ 5,838	\$ 2,438
Monterey	Transportation Agency for Monterey County	SR 156/Castroville Boulevard Interchange	New Interchange	2025-26	\$ 136,101	\$ 10,300
Sacramento, Yolo	City of Sacramento	I Street Bridge Replacement	Bridge replacement with bike and pedestrian facilities	2025-26	\$ 310,780	\$ 25,000
San Diego	City of San Diego	Streamview Drive Revitalization Project Phase II	Complete streets	2025-26	\$ 23,087	\$ 10,000
San Francisco	City & County of San Francisco San Francisco Municipal Transportation Agency	Howard Streetscape Project	Complete Streets with Road Diet	2025-26	\$ 52,683	\$ 14,000
Santa Barbara	Santa Barbara County Association of Governments	Santa Barbara U.S. 101 Multimodal Corridor Project - Segment 4E North -Cabrillo Interchange	Construct HOV lanes and overcrossing	2025-26	\$ 140,038	\$ 25,000
Santa Clara	Santa Clara Valley Transportation Authority	BART to Silicon Valley Phase II Extension	Public Transit - Rail (Extend BART 6 miles)	2025-26	\$ 12,745,607	\$ 25,000
Santa Cruz	Santa Cruz County Regional Transportation Commission	Watsonville-Santa Cruz Multimodal Corridor Program	Parent Project			\$ 25,000
		Child Project #1: State Route 1 - Freedom to State Park Auxiliary Lanes, Bus on Shoulders, and Coastal Rail Trail Segment 12 Project (SB1-Cycle 4: Contract #1)	Auxiliary Lanes	2026-27	\$ 231,844	\$ -
		Child Project #2: Soquel Drive Multimodal Project - State Park Drive to Freedom Boulevard (SB1-Cycle 4: Contract #2)	Complete Streets	2026-27	\$ 26,917	\$ -
		Child Project #3: Local Roadway Rapid Bus Improvements – Soquel Drive and Highway 152 (SB1-Cycle 4: Contract #3)	Public Transit - Bus/Bus Rapid Transit	2026-27	\$ 24,449	\$ -
Sonoma	Sonoma Marin Area Rail Transit	Sonoma-Marin Area Rail Transit District Healdsburg Extension	Public Transit - Rail (SMART 5.5 miles extension)	2025-26	\$ 185,750	\$ 25,000
Stanislaus	Stanislaus Council of Governments	Seventh Street Bridge Replacement	Bridge replacement with bike and pedestrian facilities	2025-26	\$ 134,044	\$ 15,000
*Projects are listed in alphabetical order by county				TOTAL	\$ 14,079,269	\$ 201,655
				Funding Capacity		\$ 195,670
				Over Programmed		(\$5,985)



Congress of the United States
Washington, DC 20515

Reference No.: 4.7
June 26-27, 2025
Attachment C

March 6, 2025

The Honorable Sean Duffy
U.S. Department of Transportation
1200 New Jersey Avenue, SE
Washington, D.C. 20590

Dear Secretary Duffy:

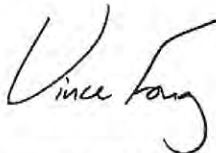
We are writing to express our support for Kern Council of Governments' (Kern COG) application for a Federal-State Partnership for Better Utilizing Investments to Leverage Development (BUILD) grant of \$22 million. If awarded, this funding would support the Centennial Corridor Interchange Connector project in Bakersfield by constructing a new direct freeway-to-freeway connector ramp from southbound State Route (SR) 99 to westbound State Route 58, completing the interchange. This project will enhance mobility and efficiency along SR 99 and SR 58, two of the West Coast's most critical freight corridors.

The Centennial Corridor Interchange Connector is essential for improving regional and national transportation networks, as it links what Kern COG describes as the busiest north-south truck route on the West Coast (SR 99) with the busiest east-west truck route and the only year-round, all-weather route over the Sierra Nevada mountain range (SR 58). This project will not only ease congestion but also reduce the impact of goods movement on communities in Central Bakersfield by diverting truck traffic away from local streets. Kern County's economy continues to expand, driven by its strong agricultural and industrial sectors, and this project will support this growth by facilitating freight movement to and from over 50 distribution, processing, and manufacturing facilities in the southern San Joaquin Valley. Additionally, the project is expected to create 844 construction jobs and support thousands of existing and future industrial jobs.

We firmly support Kern COG's grant application to complete the Centennial Corridor Interchange Connector. Approval of this project aligns with the BUILD Grant Program's mission by enhancing safety, reducing congestion, and strengthening a vital freight corridor essential to regional and national commerce. If you have any questions, please do not hesitate to contact us.

Thank you for your attention to this important matter.

Sincerely,



VINCE FONG
Member of Congress



DAVID G. VALADAO
Member of Congress

TOM MCCLINTOCK
5TH DISTRICT, CALIFORNIA

2256 RAYBURN HOUSE OFFICE BUILDING
WASHINGTON, DC 20515
PHONE: 202-225-2511

EL DORADO HILLS DISTRICT OFFICE
4359 TOWN CENTER BLVD., SUITE 210
EL DORADO HILLS, CA 95762
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[HTTPS://MCCLINTOCK.HOUSE.GOV/](https://mcclintock.house.gov/)



Congress of the United States
House of Representatives
Washington, DC 20515-0504

COMMITTEE ON THE JUDICIARY
CHAIRMAN OF SUBCOMMITTEE ON
IMMIGRATION INTEGRITY, SECURITY, AND
ENFORCEMENT

SUBCOMMITTEE ON
CONSTITUTION AND LIMITED GOVERNMENT

COMMITTEE ON NATURAL RESOURCES

SUBCOMMITTEE ON
FEDERAL LANDS

SUBCOMMITTEE ON
WATER, WILDLIFE, AND FISHERIES

COMMITTEE ON THE BUDGET

November 20, 2024

Director Tanisha Taylor
Executive Director
California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814



Dear Director Taylor,

I write in support of Madera County's application for funding from the Local Partnership Competitive Program administered through the California Transportation Commission. Madera County is seeking funding for its Madera 41 South Expressway Project, which aims to modernize a 4.5-mile-long rural highway, State Route 41, into a facility that provides safe and efficient traffic operations for the region.

State Route 41 is a crucial commuter corridor, connecting rural eastern Madera County to employment centers in urban valley communities leading into the Sierra Nevada. The corridor also plays a significant role in transporting agricultural commodities from the San Joaquin Valley to rail, freight, and port terminals for worldwide distribution. Over the past four years, seven wildfires greater than 5,000 acres have required the use of State Route 41 as an evacuation route. It is also the most utilized travel facility by local, state, national, and international visitors to Yosemite National Park annually. If awarded, it is my understanding that this funding will enable Caltrans and its partners to fulfill this significant project and promote the region's growth objectives.

As the stated purpose of Local Partnership Competitive Program is to provide funding to counties, cities, districts, and regional transportation agencies for transportation improvements, I believe Madera County's application is consistent with the purpose of this grant and highly competitive on its merits.

Thank you for your consideration. Should you have any questions, please contact Paxton Wagner in my office at (202) 225-2511.

Sincerely,

A handwritten signature in blue ink, appearing to read "Tom McClintock".

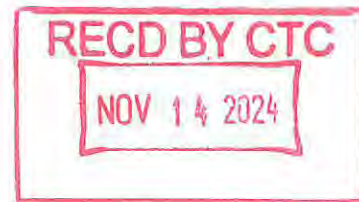
Tom McClintock



CITY OF PERRIS
OFFICE OF THE MAYOR
MICHAEL M. VARGAS

October 31, 2024

The Honorable Carl Guardino
Chair, California Transportation Commission
1120 N Street MS 52
Sacramento, CA 95814



Subject: Support RCTC's Fiscal Year 2025-26 SCCP and LPP Grant

Dear Chair Guardino:

As the Mayor of Perris, I am pleased to support the Riverside County Transportation Commission's (RCTC) Solutions for Congested Corridors Program (SCCP) and Local Partnership Program (LPP) grant applications for the Interstate (I) 15 Express Lanes Project Southern Extension (Project). The full Project extends the existing I-15 Express Lanes an additional 14.5 miles from Cajalco Road in Corona to State Route 74 (Central Avenue) in Lake Elsinore. RCTC is seeking \$85 million from SCCP and \$25 million from LPP under this funding request to advance Segment 1 of the project, which extends approximately five miles from Cajalco Road in Corona to Indian Truck Trail in Temescal Valley.

The full Project will add two tolled express lanes in both directions within the existing I-15 median to facilitate mobility in Riverside County. The Project advances RCTC's mission to provide a safe, interconnected, multimodal transportation system with the following benefits:

- **Improve traffic operations and increase travel time reliability** – Providing new express lanes will enhance the flow of traffic by providing a reliable travel time alternative within the corridor.
- **Expand travel choice** – Augmenting the regional express lanes network encourages carpooling and use of express bus service.
- **Promote safety** – Enabling additional passenger vehicles to travel in dedicated lanes separated from trucks, which rely upon the I-15 corridor to deliver goods from the ports.

Riverside County is the 10th largest county in the nation by population, with rapid growth occurring due to the region's relative housing affordability and economic opportunities. The Project will offset impacts to the I-15 corridor because of population growth, as well as continued growth in the nation's goods movement. The Project addresses competing passenger and

101 North "D" Street
Perris, California 92570
(951) 943-6100

commercial traffic congestion on I-15, bolstering mobility choice and supporting continued economic development.

The I-15 Express Lanes Project Southern Extension will benefit Perris residents and nearby commuters by easing congestion, improving safety, and offering new travel options. By adding tolled express lanes within the I-15 median, the project aims to ensure reliable travel times, support carpooling and express bus services, and separate passenger vehicles from heavy trucks, reducing accident risk. This expansion is vital for accommodating Riverside County's rapid growth, enhancing mobility, and supporting ongoing economic development across the region.

For these reasons, we strongly support RCTC's SCCP and LPP applications for the I-15 Express Lanes Project Southern Extension, Segment 1. For questions regarding this letter of support, please feel free to contact my office.

Sincerely,

A handwritten signature in blue ink, appearing to read "Michael M. Vargas", written in a cursive style.

Michael M. Vargas
Mayor

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1021 O STREET
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SACRAMENTO, CA 95814
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SENATOR.ROTH@SENATE.CA.GOV

California State Senate

SENATOR
RICHARD D. ROTH
THIRTY-FIRST DISTRICT



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THE NEW ECONOMY
CHAIR
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MANUFACTURED HOME
COMMUNITIES
CHAIR

November 12, 2024

The Honorable Carl Guardino
Chair, California Transportation Commission
1120 N Street MS 52
Sacramento, CA 95814

RE: Support for RCTC's Fiscal Year 2025-26 SCCP and LPP Grant for Segment 1 of the I-15 ELPSE Project

Dear Chair Guardino:

I write in strong support for the Riverside County Transportation Commission's (RCTC) Solutions for Congested Corridors Program (SCCP) and Local Partnership Program (LPP) grant applications for the Interstate (I) 15 Express Lanes Project Southern Extension (Project). As you know, Inland Southern California, with over 4.5 million residents, is one of the fastest growing regions in our nation, and is also severely challenged in providing ample transportation infrastructure for those residents.

The full RCTC Project will help address those transportation needs by extending the existing I-15 Express Lanes an additional 14.5 miles from Cajalco Road in Corona to State Route 74 (Central Avenue) in Lake Elsinore. RCTC is seeking \$85 million from SCCP and \$25 million from LPP under this funding request to advance Segment 1 of the project, which extends approximately five miles from Cajalco Road in Corona to Indian Truck Trail in Temescal Valley.

The full Project will add two tolled express lanes in both directions within the existing I-15 median to facilitate mobility in Riverside County, and facilitate the following benefits:

- **Improve traffic operations and increase travel time reliability** – Providing new express lanes enhancing the flow of traffic by providing a reliable travel time alternative within the corridor.
- **Expand travel choice** – Augmenting the regional express lanes network encourages carpooling and use of express bus service.
- **Promote safety** – Enabling additional passenger vehicles to travel in dedicated lanes separated from trucks, which rely upon the I-15 corridor to deliver goods from the ports.

The Project will offset impacts to the I-15 corridor as a result of population growth, as well as continued growth in the nation's goods movement, and address competing passenger and commercial traffic congestion on I-15.

Please contact me if you have any questions regarding my strong support.

Sincerely,

A handwritten signature in blue ink, appearing to read "Richard D. Roth", with a long horizontal flourish extending to the right.

RICHARD D. ROTH
Senator, 31st District



*Congress of the United States
House of Representatives
Washington, D. C. 20515*

*Anna G. Eshoo
Sixteenth District
California*



October 18, 2024

Ms. Tanisha Taylor, Executive Director
California Transportation Commission
1120 N Street
Sacramento, California 95814

Dear Ms. Taylor,

I write in strong support of the City of Sunnyvale's application for funding for the Bernardo Avenue Undercrossing Project through the Local Partnership Program.

Sunnyvale is partnering with the City of Mountain View, which is located in my Congressional District, to construct a crossing for pedestrians and cyclists beneath the Caltrain tracks and Central Expressway near Bernardo Avenue at the of the border of Mountain View and Sunnyvale. This long-planned connection is vital to promoting active transportation in the community, connecting employers and a mobile home park to the north of the tracks with affordable housing, light industrial uses, and an elementary school to the south. The undercrossing will eliminate a detour of one to two miles for those traveling by foot or bicycle, facilitating more direct travel between affordable housing, employers, and public transportation in nearby areas. I'm proud to have recently secured \$2.5 million in federal funding for the design and engineering of this project.

The Bernardo Avenue undercrossing is a priority infrastructure project within Sunnyvale's Vision Zero Plan to reduce traffic fatalities and severe injuries and will provide an important pedestrian connection between the two cities. I urge you to give Sunnyvale's request your full and fair consideration in accordance with all applicable laws and regulations, and I thank you in advance for your consideration of my important request.

Most gratefully,

Anna G. Eshoo
Member of Congress

California Legislature



November 19, 2024

The Honorable Tanisha Taylor
Executive Director
California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814

Re: Letter of Support for the BART Silicon Valley Phase II Extension Project

Dear Director Taylor,

I am writing to express my support for the Santa Clara Valley Transportation Authority's (VTA) BART Silicon Valley Phase II (BSVII) Extension project and urge the California Transportation Commission (CTC) to award the requested \$75 million through the Solutions for Congested Corridors Program (SCCP) and \$25 million through the Local Partnership Program (LPP). These crucial funds are essential to advancing a project of this magnitude, which will significantly improve rail connectivity, enhance access to major employment centers and growing urban areas, reduce congestion on major highways and promote sustainable transportation options in Santa Clara County and throughout the Bay Area.

VTA's BSVII project will extend BART service from the Berryessa/North San José station to the City of Santa Clara, adding six miles of new track and constructing four new stations at 28th Street/Little Portugal, Downtown San José, Diridon Station and Santa Clara. This extension will provide a much-needed sustainable alternative to personal vehicle trips, easing traffic congestion and providing reliable, accessible, equitable and affordable transit options for tens of thousands of daily commuters.

Public Benefits of the BSVII Project:

- **Traffic Congestion Reduction:** The project will move vehicle trips off major highway corridors in the project area, including US 101, I-680 and I-880. The project is identified in the US 101 Comprehensive Multimodal Corridor Plan.
- **Rail Connectivity:** The project will fulfill the vision of 'Ring the Bay with Rail' and improve intermodal connectivity between the different parts of the Bay Area by linking BART, Caltrain, Capitol Corridor, and Altamont Corridor Express at Diridon Station.
- **Addressing Climate Change:** The project will reduce approximately 50 tons greenhouse gas (GHG) emissions (2040) by reducing barriers to rail and transit options and reducing the number of vehicle trips in the project corridor.
- **Economic Growth and Job Creation:** The project will spur economic growth and job creation in the region, creating more than 100,000 direct and indirect jobs. Bay Area commuters will gain

access to 1 million Jobs in Silicon Valley. 2 million people in Santa Clara County will gain access to 3.5 million Bay Area Jobs.

- Equity and Access: The project directly serves priority populations by improving access to affordable travel options and connecting 1.7 million transit dependent riders with equitable access to jobs, educational opportunities and essential services.

As our region continues to grow, the need for sustainable and efficient transit options becomes increasingly urgent. The BSVII project is a key component of the long-term strategy to meet this demand while reducing our reliance on personal vehicles and supporting the state's goals for reducing GHG emissions.

I strongly encourage the CTC to prioritize funding for this regionally significant project. BSVII will deliver lasting benefits to the residents of Santa Clara County and the entire Bay Area by improving mobility, supporting economic development and contributing to a more sustainable future.

Thank you for your consideration.

Sincerely,



Dave Cortese
Senator, 15th District



Aisha Wahab
Senator, 10th District



Ash Kalra
Assemblymember, 25th District



Evan Low
Assemblymember, 26th District



Gail Pellerin
Assemblymember, 28th District



Watsonville
CALIFORNIA

City Manager's Office

"Working with our community to create positive impact through service with heart."

October 28, 2024



Chair Eager
California Transportation Commission
1120 N. Street, MS-52
Sacramento, CA 95814

RE: Support for the Watsonville-Santa Cruz Multimodal Corridor Program, SB1-Cycle 4

Dear CTC Commissioners:

The City of Watsonville is pleased to share its strong support for the Watsonville - Santa Cruz Multimodal Corridor Program (WSC-MCP) Cycle 4 Project and urges the Commission to fund this important project.

Watsonville is a transportation disadvantaged community that depends upon the corridors identified in this project, Soquel Drive and Highway 1, to connect its residents to educational, healthcare, and job centers both in Santa Cruz. Watsonville is a low-income community, and its residents are severely impacted by the constant congestion on these two corridors when traveling to Santa Cruz.

Watsonville residents will directly benefit from the following improvements proposed by this project:

- Elimination of choke points on Highway 1;
- Multi-modal improvements on Soquel Dr;
- Expansion of express bus service for Santa Cruz Metropolitan Transit District (METRO);
- Addition of two new "rapid" METRO routes;
- Installation of new in-lane boarding platforms, shelters, real-time bus arrival displays, and ticket validators.

This project closely aligns with local, regional, state, and federal climate action and resiliency goals. For Watsonville residents, it will enhance their quality of life by improving safety, increasing mobility, reducing congestion, reducing transit travel times and advancing transportation equity. The City of Watsonville is pleased to support this work and thanks the Commission for its consideration of this important project.

Sincerely,



Tamara Vides
Watsonville City Manager

To: California Transportation Commission

From: Residents of Aptos, Santa Cruz County

May 17, 2025

Dear Commissioners,

We live in Aptos, the location of proposed auxiliary lanes on Highway 1. We are among the people who are supposed to benefit from this project, in the form of reduced congestion on Hwy 1. We experience considerable congestion on Hwy 1, but according to the Project EIR, the morning commute will actually get worse in the opening year of the project. The EIR estimates:

- Northbound AM peak average traffic speed declining from 30 miles per hour to 26 mph in the opening year of the project.

The EIR predicts an improvement in travel time for the Southbound PM peak period in the opening year. However, the EIR says nothing about how long that improvement would last, except that there is no improvement by 2045.

The insignificance of congestion relief benefit was predicted by the Tier I EIR (2019) that studied the "TSM Alternative", defined as auxiliary lanes and ramp meters from Santa Cruz to Freedom Blvd, including the Aptos segments in this Project:

- The TSM Alternative "would result in a very slight improvement in traffic congestion when compared to the No Build Alternative" (Draft EIR page 2.1.5-16)

The Project EIR claims that there will be a safety benefit from adding auxiliary lanes. This contradicts the Tier I EIR (2019):

- "The total accident rates overall and by segment in 2035 under the Tier I Corridor TSM Alternative would be the same as the accident rates for the No Build Alternative." (Draft EIR page 2.1.5-17)

The absence of safety benefit predicted by the Tier I EIR makes sense. The Caltrans Highway Design Manual recommends auxiliary lanes for safe merging when the distance between interchanges is less than 2000 feet. The interchanges in this Project are a mile apart.

An important question is *why, if the Tier I EIR predicted "very slight" congestion relief, and no safety benefit, did Santa Cruz County leaders proceed with the auxiliary lanes project?*

Whatever the reason, it is not too late to change course. In Aptos, Hwy 1 runs through a finger of redwood forest that extends from the Forest of Nisene Marks towards the coast. The EIR states that 1100 trees will be cut down for this project, including some grand redwoods. It is a costly loss for a Project that will not succeed in reducing congestion.

These magnificent trees shield us from some of the traffic noise on Hwy 1. The project will reduce our quality of life and the value of our property.

The Commission decided not to fund this project during the last cycle of SB 1 grant funding. Please do so again.

Thank you for your consideration,

Mike Saint
516 Santa Marguarita Dr.

Peter Niboli
345 Moosehead Dr.

Elissa Wagner
528 Encino Dr.

Christina Grant
785 Loma Prieta Dr.

Nathan Bietz
520 Santa Marguarita Dr.

STATE CAPITOL
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SACRAMENTO, CA 94249-0002
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DISTRICT OFFICE
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SANTA ROSA, CA 95404
(707) 576-2526
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E-MAIL

Assemblymember.Rogers@assembly.ca.gov



COMMITTEES
BUDGET
COMMUNICATIONS AND
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UTILITIES AND ENERGY
WATER, PARKS, AND WILDLIFE
BUDGET SUBCOMMITTEE NO. 4 ON
CLIMATE CRISIS, RESOURCES,
ENERGY, AND TRANSPORTATION

April 9, 2025

Tanisha Taylor, Executive Director
California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814

Subject: Support for the SMART Healdsburg Extension Project Applications for Solutions for Congested Corridors Program/Local Partnership Program Competitive Funds

Dear Executive Director Taylor,

I am writing to express my support for funding the completion of Sonoma-Marín Area Rail Transit District's (SMART) Healdsburg Extension through the grant applications submitted by Caltrans and the Metropolitan Transportation Commission to the Solutions for Congested Corridors Program (SCC) and by SMART to the Local Partnership Competitive Program (LPP-C).

In 2008, the voters of Marin and Sonoma County made a commitment by approving a ¼ cent sales tax measure to provide funding for the construction and operations of a 70-mile rail and pathway service between Larkspur and Cloverdale. Through 2024, the voters have provided \$550 million of their hard-earned money towards that purpose.

With the opening of the SMART Windsor Station upcoming Spring 2025, SMART's system will have 48-miles and 14 stations of passenger rail open in service, with 36-miles of the Great Redwood Trail/SMART Pathway open to the public. This request before you completes the work to Healdsburg and advances the completion of the entire system, including non-motorized path and enhanced freight rail service capabilities system-wide. There are just over four years left in SMART's 2008 sales tax measure. We should make every effort to complete construction of the rest of the SMART system before asking the voters to reauthorize the sales tax to pay for ongoing operations of the system.

I respectfully urge your favorable consideration of these SCC and LPP-C funding requests to match our voters' commitment to a healthier transportation future. Please feel free to contact my office at 707-576-2526 if you have any questions or require additional information.

Sincerely,

CHRIS ROGERS
Assemblymember, Second District

CR: mb



November 14, 2024

Ms. Tanisha Taylor
California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814



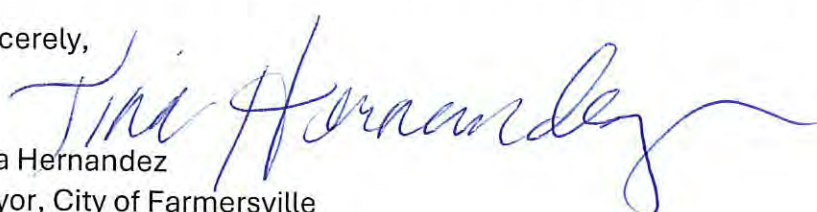
Dear Ms. Taylor:

The City of Farmersville is writing in support of the City of Woodlake's application to the California Transportation Commission's (CTC) 2024 Local Partnership Program (LPP) for the State Route 216 and Mulberry Street Roundabout Project (Project). The application is being submitted in partnership with Caltrans District 6 and the Tulare County Association of Governments (TCAG).

The Project will have a significant and positive impact on the community, enhancing both safety and traffic flow. With new residential, commercial, and industrial developments being designed and constructed in the vicinity of the project area, the intersection will become congested over time. The roundabout will provide safe and sustainable access at the intersection, will accommodate projected traffic associated with foreseeable future development, provide an acceptable Level of Service (LOS), reduce fuel consumption, and reduce vehicular idling time that generates air pollution.

The City of Farmersville appreciates the opportunity to express our strong support. If you have any further questions, please contact Tina Hernandez at 559-747-0458 ex: 1003.

Sincerely,


Tina Hernandez
Mayor, City of Farmersville

1021 O STREET
SACRAMENTO, CA 95814
(916) 651-4003

California State Senate

SENATOR
CHRISTOPHER CABALDON
THIRD SENATE DISTRICT



April 11, 2025

Ms. Tanisha Taylor
Executive Director
California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814

Dear Director Taylor:

I am writing to express my support for the City of Sacramento's request for funding for the I Street Bridge Replacement Project through the 2024 Local Partnership Program (LPP) competitive grant program.

This project will build a new iconic bridge over the Sacramento River between the Cities of Sacramento and West Sacramento. This bridge will provide a crossing of the river that can serve all modes of travel, with wide sidewalks, bike lanes, and vehicle lanes wide enough for buses and emergency response vehicles, all options lacking in the existing 113-year-old I Street Bridge. The bridge will support significant new transit-oriented infill development in both cities and improve accessibility to key destinations for communities on both sides of the Sacramento River, including access to a new Kaiser Permanente hospital currently under construction, and the Sacramento Valley Station. The bridge will also provide another key crossing of the river in the event of the need for emergency evacuations.

The Cities of Sacramento and West Sacramento have been working on this project for many years, have secured state and federal environmental clearance, and will complete final design and obtain all necessary permits this summer. After many years of working with Caltrans to secure a financial commitment, in March of this year, Caltrans pledged \$250 million in federal Highway Bridge Program funding toward the bridge's construction. With the LPP funding that is being requested, the project will finally have sufficient funding guarantees to proceed to construction next spring,

I urge you to support funding for this once-in-a-lifetime project to remove the last barrier to its construction.

Sincerely,

A handwritten signature in black ink, appearing to read "C. Cabaldon".

Christopher Cabaldon
Senator, District 3



California State Senate

SENATOR
ANGELIQUE V. ASHBY, J.D.
EIGHTH SENATE DISTRICT

May 21, 2025

Tanisha Taylor
Executive Director, California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814

Dear Director Taylor,

I am writing to express my support for the City of Sacramento's request for funding for the I Street Bridge Replacement Project through the 2024 Local Partnership Program (LPP) competitive grant program.

The current bridge, located on I Street, is over 100 years old and lacks critical infrastructure. This project will build a new bridge over the Sacramento River that connects Sacramento and West Sacramento. The new bridge will have multiple modes of transportation, such as wide sidewalks, bike lanes, and larger vehicle lanes for increased safety for pedestrians and cyclists.

The bridge will significantly support additional transit-oriented infill development in both cities, and improve the accessibility to key destinations for communities on both sides of the Sacramento River, including the new Kaiser Permanente hospital currently under construction, the Sacramento Valley Station, and Sutter Health Park. The bridge will also be helpful in the event of emergency evacuations.

The City of Sacramento has been working on this project for many years. In March of this year, after diligently working with Caltrans to secure a financial commitment, Caltrans pledged \$250 million in federal Highway Bridge Program funding toward the bridge's construction. This, coupled with the LPP funding that is being requested, the project will finally have the funding to begin construction next spring, resulting in significant long-term economic benefits to the region.

Through this letter, I affirm my support of this project and the economic benefits it will provide to the cities of Sacramento and West Sacramento. Please do not hesitate to contact my office at 916-319-0309 if you have any questions or concerns.

Sincerely,

A handwritten signature in black ink that reads "Angelique Ashby". The signature is fluid and cursive, with a large, sweeping flourish at the end.

ANGELIQUE V. ASHBY
Senator, 8th District

ClimatePlan TRANSFORM



May 28, 2025

Matthew Yosgott
Deputy Director - SB 1 Programming
California Transportation Commission (CTC)
1120 N Street MS 52
Sacramento, CA 95814

Re: Applications of Concern for SB 1 Funding (SCCP, TCEP, LPP Programs)

Dear Deputy Director Yosgott,

The undersigned organizations write to express concerns about a number of highway expansion project applications for SB 1 funding under the Solutions for Congested Corridors Program (SCCP), Trade Corridor Enhancement Program (TCEP), and the Local Partnership Program (LPP) that we have listed below. Our concerns about these projects relate to their inconsistency with the Climate Action Plan for Transportation Infrastructure (CAPTI), their likelihood to significantly increase driving, their environmental, climate, and displacement impacts, and their conflicts with California's climate and equity goals. We urge the Commission to scrutinize these projects closely and only fund the components of these projects that reduce driving, improve

multimodal options, and advance the zero-emission fleet transition. Several of the projects listed are opposed locally by organizations in the ClimatePlan network that are also signed on to this letter. We have linked to opposition letters to specific projects in the footnotes.

Solutions for Congested Corridors Program (SCCP)

We are very pleased to see the high number of transformative multimodal and vehicle miles traveled (VMT) reducing projects and components of projects among the list of applications for SCCP this cycle. We particularly appreciate the innovative project applications that aim to improve highway operations in congested corridors without adding new lanes, such as the ***I-805 South Managed Lanes HOV to HOT Conversion Project*** which will reinvest toll revenue into transit and active transportation infrastructure.

We encourage the CTC to only fund projects and project components that reduce VMT and avoid adding new lanes to highways and roadways. Specifically, we urge the CTC not to fund the following highway expansion projects and project components:

1. **SR 1 – Freedom to State Park Auxiliary Lanes¹ (Also an LPP and TCEP application, comments apply to that program as well)** - This auxiliary lane expansion project is a component of the Watsonville-Santa Cruz Multimodal Corridor Project and is opposed by local advocates challenging the project. As articulated in the Sierra Club chapter letter linked in the footnote, this application for SCCP/TCEP/LPP has some multimodal components that enjoy support from the community and we encourage the CTC to fund these components without the auxiliary lanes.
2. **101 Multimodal Corridor Segment 4E North – Cabrillo Interchange (Also an LPP and TCEP application, comments apply to that program as well)** - This expansion project is a component of the Santa Barbara US 101 Multimodal Corridor Project and was opposed by local advocates. This application for SCCP/TCEP/LPP also has many strong multimodal components that enjoy support from the community and we encourage the CTC to fund these components of this project. Since most of the corridor has already been widened, we recognize that restricting lanes in this short segment could decrease air quality. However, the HOV lanes have limited effectiveness because of their limited time duration and operational requirements for only 2 or more occupants. We encourage CTC to fund the HOV component only with expanded HOV operational requirements for additional hours and 3 or more occupants.
3. **I-5 Managed Lanes Multimodal Operational Improvements (Orange County)** - This project adds 16 lane-miles of new HOV lanes that will exacerbate freeway congestion and air quality issues without a clear commitment to invest in transit alternatives. We would support the portion of this project that converts existing HOV lanes to express lanes if those components could be funded separately from the lane expansion components and if toll revenues were committed to providing multimodal alternatives.

¹ [Sierra Club to CTC on SR-1](https://drive.google.com/file/d/1mzuzGdvoWviBsOHhuy8rzwJOPGmgb4ql/view?usp=sharing)
<https://drive.google.com/file/d/1mzuzGdvoWviBsOHhuy8rzwJOPGmgb4ql/view?usp=sharing>

4. **SR 37 Sears Point to Mare Island Improvement Project – Phase 2² (Also an LPP and TCEP application, comments apply to all three programs)** - The sign on letter linked in the footnote demonstrates the local advocate opposition to this “interim” lane expansion project, and states that “the interim project will cost over \$500M and will be under water caused by sea level rise within fifteen years of completion. Continuing the interim project in its current form further delays, rather than advances, the long-term vision for the corridor.” We have serious concerns about investing any state funding in a roadway that may be inoperable as soon as 2040.
5. **I-15 Express Lanes Project Southern Extension (ELPSE)³ (Also an LPP application, comments apply to that program as well)** - This project will add over 60 lane-miles to I-15 in a congested and growing region and does not include multimodal and VMT reducing components. As detailed in the comment letter on this project’s draft environmental impact report linked in the footnote, this project will induce significant VMT and relied on outdated and flawed methodology for estimating air quality and other impacts.
6. **I-680 NB Express Lane Completion Phase 1⁴** - This express lane expansion project is a component of the Innovate 680 project. Though this SCCP application appears to only be requesting funds for the Shared Mobility Hubs and TDM Augmentation components of the larger project, we still want to highlight the concerns raised by Transform in their comment letter to Caltrans regarding flawed analysis in the draft EIR for this project, linked in the footnote.

Trade Corridor Enhancement Program (TCEP)

We are again pleased to see the increase in TCEP applications this cycle that would advance freight electrification, technology, and efficiency at ports and truck parking hubs. We strongly support investing TCEP funding in these types of projects that are well-aligned with CAPTI and reduce harmful impacts to communities that bear the brunt of pollution from the freight industry. We encourage the CTC to focus funding only on those types of projects and avoid funding any projects that increase passenger VMT by adding general purpose lanes to roadways.

In addition to the projects listed above that are applying for both SCCP and TCEP, we have concerns about the following TCEP applications:

1. **SR 60/World Logistics Center Pkwy Interchange** - This project entails a significant expansion and new connection for an existing interchange that will open up land for more warehouse and sprawl development and induce significant new passenger VMT.

² Sign on comment letter SR-37 <https://transformca.org/wp-content/uploads/2024/09/Sign-on-Letter - Oppose-Backdoor-Streamlining-for-HW-37.pdf>

³ [Sign on comment letter I-15 DEIR https://drive.google.com/file/d/1P8Tr5ZF3zWvByCTWY_bwlo957Ab7Q3jM/view?usp=sharing](https://drive.google.com/file/d/1P8Tr5ZF3zWvByCTWY_bwlo957Ab7Q3jM/view?usp=sharing)

⁴ [Sign on comment letter Innovate 680 DIER https://transformca.org/wp-content/uploads/2024/07/I-680-NB-Express-Lanes-Comment-Letter.docx.pdf](https://transformca.org/wp-content/uploads/2024/07/I-680-NB-Express-Lanes-Comment-Letter.docx.pdf)

2. **Konocti Corridor - Segment 2B** - This project will add 16 general purpose lane-miles to convert the existing conventional highway between Kelseyville and Clear Lake to a four-lane expressway and will induce passenger VMT.
3. **Tulare SR 99 Corridor and Paige Avenue Multimodal Interchange Enhancements** - We do not support the capacity-increasing general purpose lane additions to SR 99 which will induce significant passenger and truck VMT. The project as approved further separates South City of Tulare and the neighboring unincorporated community of Maheny Tract by providing the transportation system for the growth of the City's industrial zones which separates the aforementioned communities. The Project does not improve connectivity for pedestrians and cyclists. At the bare minimum the project proposes a shared paved path, not even a sidewalk, between Blackstone St. and Laspina St, which do not extend into the residential communities to the East and West of the project site. We recommend a project alternative which will result in reduced traffic congestion while also improving pedestrian and active transportation options for the community, instead of facilitating industrial growth and highway capacity for freight.
4. **State Route 46 Antelope Grade Corridor Improvements Project** - This project will add five general purpose lane-miles to convert the existing conventional highway over Antelope Grade to a four-lane expressway and induce passenger VMT.
5. **State Route 132 Phase 3A Project** - This project will add 14 new general purpose lane-miles to extend a new expressway that creates a bypass for SR 132 and opens up new land for warehouse and sprawl development and induce new passenger VMT.
6. **State Route 71 Gap Closure Phase 2 Project (*Also an LPP application, comments apply to that program as well*)** - This project will convert an existing arterial highway into an 8-lane freeway, adding a general purpose and HOV lane in each direction. We do not support the highway lane addition portions of this project, which will induce significant passenger VMT and limit the freight benefits. However, we support the components of the project that provide sound walls and noise mitigation, update rail bridges to current standards, and enhance a pedestrian overcrossing.
7. **SR 46 East/Union Road Intersection Improvements** - This project entails a new interchange that will open up land for sprawl development and induce significant new passenger VMT.
8. **680/SR-4 Interchange Improvement (*Also an LPP application, comments apply to that program as well*)** - This project will add a direct connector with two general purpose lanes to the interchange of I-680 and SR-4 without mitigation for increases to passenger VMT.

Local Partnership Program (LPP)

In addition to the projects listed above that have applied for SCCP and/or TCEP in addition to LPP, we have concerns about the following additional application for LPP:

1. **Madera 41 South Expressway Project** - This project will add eight general purpose lane-miles to convert the existing conventional highway to a four-lane expressway, increasing access to land that is rapidly converting to sprawl development and significantly increasing passenger VMT. Rather, we urge the CTC to support projects

which truly meet the purpose of the LPP which includes addressing aging infrastructure, road conditions, active transportation, transit and rail, and ensuring health and safety benefits. Madera County is currently unable to meet existing transportation needs, including maintenance and transit of existing communities. Funding additional roadways for the purpose of sprawl contradicts the purpose of the LPP, will increase emissions, and divert critical funding needed to support the current transportation system in Madera County.

We appreciate the opportunity to provide comments and amplify project concerns from local organizations as commission staff reviews the SB 1 applications. We look forward to further engagement with CTC staff and commissioners once the staff recommendations are released.

Thank you for your attention to our concerns and for your leadership in guiding California toward a more sustainable and equitable transportation future.

Sincerely,

Zack Deutsch-Gross
Transform

Mary Lim, J.D.
Genesis: Interfaith Organizing in the East Bay

Jeanie Ward-Waller
ClimatePlan

Marven Norman
Center for Community Action and Environmental Justice (CCA EJ)

Jared Sanchez
CalBike

Elizabeth Reid-Wainscoat
Center for Biological Diversity

Chance Cutrano
Resource Renewal Institute

Matthew Baker
Planning and Conservation League

Janet Cox
Climate Action California

Sofia Rafikova
Coalition for Clean Air

Heather Deutsch
MOVE Santa Barbara County

Mike Swire
Stop the 101 & 280 Widenings in San Mateo County

Iman Sylvain, PhD
Rails to Trails Conservancy

David Diaz, MPH
Active San Gabriel Valley

Nick Ratto
350 Bay Area Action - Transportation Lead

Carter Rubin
Natural Resources Defense Council (NRDC)

Carter Lavin
Transbay Coalition

Justin Hu-Nguyen
Bike East Bay

David Levitus
LA Forward

Hana Creger
The Greenlining Institute

Aditi (Adi) Varshneya
California Environmental Justice Alliance (CEJA) Action

Emma De La Rosa
Leadership Counsel for Justice and Accountability

Jonathan Matz
Safe Routes Partnership

Marc Vukceovich
Streets For All

Kevin Shin
California Walks

Rick Longinotti
Campaign for Sustainable Transportation

Suzanne Hume
CleanEarth4Kids.org

Jamie Pew
NextGen California

Jorge Rivera
Healing & Justice Center

cc:
Executive Director Tanisha Taylor
Chair Darnell Grisby
Vice-chair Clarissa Falcon



Western Regional Office
409 13th St.
Suite 600
Oakland, CA 94612
510-992-4662

May 30, 2025

Matthew Yosgott
Deputy Director - SB 1 Programming
California Transportation Commission (CTC)
1120 N Street MS 52
Sacramento, CA 95814

RE: Applications of Merit for SB 1 Funding – Supporting Bay Area Trails

Dear Deputy Director Yosgott,

Rails to Trails Conservancy writes to encourage CTC staff to recommend funding for two active transportation project applications for SB 1 funding in the San Francisco Bay Area under the Solutions for Congested Corridors Program (SCCP) and the Local Partnership Program (LPP) that we have listed below.

RTC is the largest trail organization in the country and is working to connect all people, everywhere by trails and safe active transportation infrastructure. RTC advocates for low-stress, traffic-separated, and multimodal trails, to create new accessible, sustainable, and affordable pathways for mobility.

RTC chairs the Bay Area Trails Collaborative (BATC), which comprises more than 50 organizations, agencies, and businesses seeking to develop a 2,600-mile [regional trail network](#) that would connect the nine-county Bay Area. Two [priority projects](#) for BATC applied for SB 1 funding, the Bay Skyway and SMART Pathway.

We encourage you to recommend funding for the following two projects to progress completion of the Bay Area regional trail network.

1. **SCCP Project 1: Multimodal Bay Skyway.** Given the impactful nature of a transbay multiuse path connecting Oakland and San Francisco, the Bay Skyway is one of 12 high priority projects for BATC. The completion of the Bay Skyway would be transformative for the region, as it creates new avenues for congestion relief, expands commuter choice, and opens greater access to cultural, educational, recreational, and employment opportunities. As the crow flies, the downtown areas of San Francisco and Oakland are a mere 6 miles apart, but



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today there are no direct routes to walk, bike, or roll between these destinations. The Bay Skyway would provide a scenic and stress-free route for transbay commuters, as it is sited along and a key segment of the beautiful San Francisco Bay Trail. RTC is additionally supportive of the Bay Skyway as moderators of the Friends of the Bay Trail, a group of advocates, city & county staff, and other professionals working to advance the 500-mile Bay Trail.

2. **SCCP Project 2 and LPP Project 35: SMART Healdsburg Extension.** When complete, the SMART Pathway will connect bicyclists and pedestrians to jobs, schools, transit hubs, housing and other destinations via a 71.3-mile multiuse trail alongside the SMART commuter rail system in Sonoma and Marin counties. This essential spine of Sonoma and Marin's active transportation network is a BATC priority project. Filling the remaining gaps will result in increased connectivity throughout the region and boost the current usership. Additionally, the SMART Pathway was officially designated as the southern portion of the 307-mile Great Redwood Trail.

We appreciate the opportunity to provide comments and amplify the BATC regional trail network as commission staff reviews the SB 1 applications. We look forward to further engagement with CTC staff and commissioners once the staff recommendations are released.

Thank you for your consideration.

Sincerely,

A handwritten signature in black ink, appearing to read "J. Voremberg".

Jesse Voremberg
jesse@railstotrails.org
Trail Development Manager, Western Region
Rails to Trails Conservancy

A handwritten signature in black ink, appearing to read "Iman Sylvain".

Iman Sylvain, PhD
isylvain@railstotrails.org
Director, Western Region
Rails to Trails Conservancy



KAREN BASS
MAYOR

June 6, 2025

Darnell Grisby, Chair
California Transportation Commission
1120 N Street, MS 52
Sacramento, California 95814

Tanisha Taylor, Executive Director
California Transportation Commission
1120 N Street, MS 52
Sacramento, California 95814

RE: LA River Greenway, West San Fernando Valley Gap Closure (Segments 1 & 2)

Dear Chair Grisby and Executive Director Taylor,

I write again to reiterate my strong support of Metro's request for \$17 million from the Local Partnership Program (LPP) and Solutions for Congested Corridors Program (SCCP) for the LA River Greenway, West San Fernando Valley Gap Closure (Segments 1 and 2) Project. This project will create a 3-mile Class I path that links directly to the Sepulveda Basin Recreation Area—future site of the Valley Sports Park Olympic venue.

This project is critical to Los Angeles' mobility network and readiness for the 2028 Olympic and Paralympic Games. It will provide safe, sustainable transportation options for underserved communities, improve access to 16 schools and major job centers, and support transit connections. Benefits include the creation of 863 new construction jobs, connection of 13,535 jobs via the new corridor, reduction of fatalities and serious injuries, and elimination of 490,000 annual vehicle miles traveled (VMT) and 2,900+ metric tons of greenhouse gas emissions.

Recent construction bids exceeded estimates due to regional fire impacts on material and labor costs, and it is vital that the project receive the full funding requested by Metro to ensure successful delivery. The City, Metro, and regional partners remain fully committed to delivering this long-planned, shovel-ready project in time for the 2028 Games. With your support, this investment will create a lasting legacy for Los Angeles and the San Fernando Valley.

For these reasons, I ask for the Commission's favorable consideration of the full \$17 million request. Thank you for your continued partnership.

Sincerely,

KAREN BASS
Mayor

cc: Stephanie Wiggins, Chief Executive Officer, Los Angeles Metro



Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.26, Action

Prepared By: Anja Aulenbacher
Associate Deputy Director

Published Date: June 13, 2025

Subject: 2025 Active Transportation Program Metropolitan Planning Organization Component Adoption for Seven Large Metropolitan Planning Organizations, Resolution G-25-47, Amending Resolution G-25-33

Recommendation:

Staff recommends the California Transportation Commission (Commission) adopt the 2025 Active Transportation Program Metropolitan Planning Organization (MPO) component for seven of the ten large metropolitan planning organizations as presented in the recommended project list (Attachment B) and consistent with Resolution G-25-47 (Attachment A).

Issue:

Commission staff prepared the recommended 2025 Active Transportation Program MPO component program of projects and posted it on the Commission's website on June 2, 2025. Commission staff recommends funding 34 projects for a total of \$61,344,000 for seven metropolitan planning organizations. Combined, the total project costs are over \$476,014,000. This is the final programming action for the 2025 Active Transportation Program.

The Commission established the 2025 Active Transportation Program as a four-year, \$168,700,000 program (fiscal years 2025-26 through 2028-29). Metropolitan planning organizations select projects through a regional competitive process for 40 percent of the total program funding. The Budget Act of 2024 cut \$400,000,000 from the Active Transportation Program. This reduced available funds for the metropolitan planning organizations' regional call by over 70 percent—from \$227,480,000 to \$67,480,000.

The following seven metropolitan planning organizations submitted a recommended program of projects by the April 22, 2025 deadline.

- Fresno Council of Governments
- Metropolitan Transportation Commission
- Sacramento Area Council of Governments
- Southern California Association of Governments
- San Joaquin Council of Governments

- Stanislaus Council of Governments
- Santa Barbara County Association of Governments

Commission staff reviewed the program submittals to confirm eligibility and consistency with the adopted program guidelines as well as the approved regional guidelines. Throughout the process, Commission staff collaborated with the metropolitan planning organizations to review and finalize the recommendations and request updates before releasing staff recommendations.

At the May 2025 Commission meeting, the Commission adopted the 2025 Active Transportation Program MPO component for the San Diego Association of Governments. The Kern Council of Governments and Tahoe Metropolitan Planning Organization chose not to participate in the 2025 Active Transportation Program MPO component due to limited regional funding available to them (\$1,692,000 and \$270,000, respectively). The 2025 Active Transportation Program Guidelines Amendment (adopted August 2024) allows metropolitan planning organizations receiving less than \$2,000,000 in funding capacity to loan their 2025 Active Transportation Program funding to another metropolitan planning organization, to be repaid in the 2027 Active Transportation Program. The Santa Barbara County Association of Governments worked with Kern Council of Governments and Tahoe Metropolitan Planning Organization, which resulted in Santa Barbara County Association of Governments using their combined \$1,960,000 apportionment to boost its regional funding capacity from \$834,000 to \$2,796,000. The Santa Barbara County Association of Governments will repay an equivalent apportionment of their 2027 Active Transportation Program (Cycle 8) funding to Kern Council of Governments and Tahoe Metropolitan Planning Organization.

Programming Recommendations

The 34 recommended projects include a broad range of active transportation infrastructure improvements, including over 42 miles of new bikeways, 18 miles of new sidewalks, intersection timing improvements, 373 crosswalks, shade trees, 45 bike racks, wayfinding signage, lighting improvements, and Americans with Disabilities Act-compliant curb ramps.

The recommendations include:

- \$60,812,000 for 32 projects that benefit disadvantaged communities. This is 90 percent of the total available funding and far exceeds the minimum 25 percent requirement set by Senate Bill 99 (Committee on Budget and Fiscal Review, Chapter 359, Statutes of 2013) and the 2025 Active Transportation Program Guidelines. Every participating metropolitan planning organization surpassed the 25 percent threshold.
- \$25,472,000 (38 percent) for 19 Safe-Routes-to-School projects.

Evaluation Process

On December 5, 2024, the Commission adopted the 2025 Active Transportation Program Statewide component. Projects located within the boundaries of the metropolitan planning organizations that were not programmed in the Statewide component were forwarded to the respective metropolitan planning organization for regional funding consideration.

In accordance with the 2025 Active Transportation Program Guidelines, metropolitan planning organizations can either delegate project selection to the Commission using the project selection criteria utilized for the Statewide and Small Urban and Rural components, or, with Commission approval, apply their own criteria, scoring weights, minimum funding thresholds, and match requirements. They may also choose to issue a supplemental call for projects.

The Commission approved the 2025 Active Transportation Program Regional Guidelines for seven metropolitan planning organizations. Six issued a supplemental call for projects. Only the Stanislaus Council of Governments used the Statewide selection process, adding its own regional scoring criteria and weights.

Project Highlights

The 34 recommended projects include a range of active transportation infrastructure improvements as well as education and encouragement programs. A few projects are highlighted as examples below.

Fresno Council of Governments

Coalinga Bike and Pedestrian Perimeter Network Gap Closures and Crossings Project in the City of Coalinga (\$1,195,000) is the last phase of the Coalinga Bike and Pedestrian Perimeter Network – an interconnected system of paved Class I trail. This segment of the project will serve vulnerable neighborhoods where access to vehicles is low and obesity and cardiovascular disease rates are higher than the statewide average. Improvements include installation of a Class I trail, new sidewalks, a roundabout, crosswalks with Rectangular Rapid Flashing Beacons, along with sheltered benches, solar lighting, and other trail amenities to create a comfortable place to walk and bike. Once built, the project will connect to the remainder of the Coalinga Bike and Pedestrian Perimeter Network and provide a safe connection to schools, city hall, parks, job centers, and medical care facilities.

Metropolitan Transportation Commission

Verde K-8 Safe Routes to School Project in Contra Costa County (\$4,355,000) will benefit a community in the North Richmond area whose average household income is 55 percent below the statewide median household income. Residents depend on active modes of transportation and public transit. The project will construct widened sidewalks, curb extensions, streetlights and trees, as well as narrow the travel lanes along 0.29 miles of Market Avenue. The proposed improvements will calm traffic and deter semi-trucks from using community streets as a cut-through. This project will also provide a comprehensive safe-routes-to-school education and encouragement program to the North Richmond and Verde K-8 School community members. At completion, this project provides safe access to Verde K-8 School, transit hubs, affordable and senior housing, community centers, parks, and a community garden.

Sacramento Area Council of Governments

Northgate Boulevard Complete Streets Project in the City of Sacramento (\$1,900,000) is located at Northgate Boulevard between Interstate 80 and Del Paso Boulevard which ranks in the top 10 corridors on the City's High Injury Network. Northgate Boulevard also has a significant pollution burden which impacts the community. The neighborhoods surrounding Northgate Boulevard are home to one of the largest youth populations in the City of Sacramento, and nearly 89 percent of students are eligible for the National School Lunch Program. The Active Transportation Program funding will be utilized towards the environmental phase of this project. Future construction includes new Class IV separated bikeways, new and improved sidewalks, new and improved crosswalks and upgrades to existing signals and intersections. This project addresses major active transportation deficiencies and creates safer access to schools, businesses, parks and trails, community recreation areas and health centers.

San Joaquin Council of Governments

Citywide Stockton Bicycle-Pedestrian Connectivity Project in the City of Stockton (\$1,450,000) will create a neighborhood-wide active transportation network with Class I paths, Class II bike lanes, Class IV separated bike lanes, new or enhanced sidewalks, crosswalks, curb ramps, and lighting. Currently, the project area has significant sidewalk gaps and minimal bike lanes, so that bicyclists and pedestrians must share the road with vehicle traffic in many areas. This project will provide residents with a safe connection to schools, a community center, bus stops, shopping, and the only supermarket in town, which 76 percent of residents named as the most important walking and biking destination.

Southern California Association of Governments

Santiago Creek Bike Trail Gap Closure Project in the City of Orange (\$2,302,000) will close the final gap of the 8-mile Santiago Creek Trail by constructing a Class I trail, and pedestrian bridges. The project will also provide a connection to the regional, 66-mile, seamless walking and biking trail system known as the Orange County Loop. The Santiago Creek Bike Trail will provide safe access to schools, a children's museum, businesses, and parks to low-income communities disproportionately impacted with high obesity rates.

Stanislaus Council of Governments

Waterford Safe Routes to School - Washington Road Project in the City of Waterford (\$845,000) will serve a low-income community where many children walk or bike to school. Currently, students walk or bike on uneven dirt next to the road or navigate heavy traffic on a street with no sidewalks or bike lanes. When it rains, to avoid mud and deep puddles, students are forced to use the middle of the street. This project will construct sidewalks, crosswalks, curb ramps, and streetlights, with the goal of providing K-12 students a safe route to school. In addition, this project will also connect residents to trails, parks, and businesses.

Santa Barbara County Association of Governments

Funk Zone Intersection Safety Enhancements Project in the City of Santa Barbara (\$1,300,000) will add curb ramps, crosswalks, new streetlights, all-way stop signs at intersections, and bicycle parking. This project will benefit residents of a community with low vehicle ownership rates and where over 93% of students are eligible for the National School Lunch Program. Residents in this area rely on walking and biking to get to key destinations. The proposed improvements fall in the heart of Santa Barbara's Funk Zone, which is an employment destination for many community members who work at the Funk Zone's many restaurants, wineries, breweries, hotels, and markets.

Background:

The Active Transportation Program was created by Senate Bill 99 (Committee on Budget and Fiscal Review, Chapter 359, Statutes of 2013) and Assembly Bill 101 (Committee on Budget, Chapter 354, Statutes of 2013) to encourage increased use of active modes of transportation. Senate Bill 1 (Beall, Chapter 5, Statutes of 2017) directs additional funding from the Road Maintenance and Rehabilitation Account to the Active Transportation Program. The Active Transportation Program supports projects that increase active transportation trips, improve safety and mobility for non-motorized users, help meet greenhouse gas reduction targets, improve public health, and ensure disadvantaged communities benefit equitably.

Demands for Active Transportation Program funding continue to outpace supply as communities across the state rely on the program to fund critical projects that advance state and regional safety, climate, and equity goals.

The Budget Act of 2024 cut \$400,000,000 from the \$1,049,000,000 one-time General Fund appropriation to the Active Transportation Program in the Budget Act of 2022. To preserve funding commitments for the 2023 Active Transportation Program, the Commission approved a corresponding \$400,000,000 reduction to the 2025 Active Transportation Program at its August 2024 meeting. The revised 2025 Active Transportation Program Fund Estimate provides \$168,700,000 for the 2025 Active Transportation Program, distributed as follows for fiscal years 2025-26 through 2028-29:

- Statewide component: \$84,350,000 (50 percent)
- Small Urban and Rural component: \$16,870,000 (10 percent)
- Metropolitan Planning Organization component: \$67,480,000 (40 percent)

This funding reduction impacted the metropolitan planning organizations' regions. In response, MPOs adapted by collaborating with project sponsors to downsize project scopes, fund only pre-construction phases, or supplement regional Active Transportation Program funds with other sources.

At the December 2024 Commission meeting, the Commission adopted the 2025 Active Transportation Program Statewide and Small Urban and Rural Components.

At the May 2025 Commission meeting, the Commission adopted the 2025 Active Transportation Program Metropolitan Planning Organization component for the San Diego Association of Governments which included three projects totaling \$6,137,000 with combined total project costs of \$31,600,000. One hundred percent of the funding benefits disadvantaged communities.

Attachments:

- Attachment A: Resolution G-25-47
- Attachment B: 2025 Active Transportation Program Metropolitan Planning Organization Component Staff Recommendations
- Attachment C: 2025 Active Transportation Program Metropolitan Planning Organization Component Submittals from Seven Metropolitan Planning Organizations

**CALIFORNIA TRANSPORTATION COMMISSION
Adoption of the 2025 Active Transportation Program
Metropolitan Planning Organization Component
For Seven Large Metropolitan Planning Organizations**

**RESOLUTION G-25-47,
Amending Resolution G-25-33**

- 1.1 **WHEREAS**, pursuant to Streets and Highways Code Sections 2380 - 2385 the California Transportation Commission (Commission) adopts a program of projects for the Active Transportation Program every two years no later than July 1 of each odd-numbered year, with each program covering a period of four fiscal years; and
- 1.2 **WHEREAS**, pursuant to Streets and Highway Code Section 2382 subdivision (c), no less than 25 percent of overall program funds must benefit disadvantaged communities during each program cycle; and
- 1.3 **WHEREAS**, the Commission adopted the 2025 Active Transportation Program Guidelines on March 22, 2024 which describe the policy, standards, criteria, and procedures for the development, adoption, and management of the Active Transportation Program; and
- 1.4 **WHEREAS**, the Commission adopted an amendment to the 2025 Active Transportation Program Guidelines and the 2025 Active Transportation Program Fund Estimate on August 15, 2024; and
- 1.5 **WHEREAS**, the Amended 2025 Active Transportation Program Fund Estimate provided \$168,700,000 for the 2025 Active Transportation Program and apportioned \$84,350,000 (50 percent) to the Statewide component, \$16,870,000 (10 percent) to the Small Urban and Rural component, and \$67,480,000 (40 percent) to the Metropolitan Planning Organization component in fiscal years 2025-26 through 2028-29; and
- 1.6 **WHEREAS**, the Commission adopted the 2025 Active Transportation Program Statewide and Small Urban and Rural components on December 5, 2024; and
- 1.7 **WHEREAS**, the Commission adopted the 2025 Active Transportation Program Metropolitan Planning Organization component for the San Diego Association of Governments which included \$6,137,000 for three projects on May 15, 2025; and

- 1.8 **WHEREAS**, the staff recommendations for the 2025 Active Transportation Program Metropolitan Planning Organization component for seven large metropolitan planning organizations were posted on the Commission's website and made available to the public on June 2, 2025; and
- 1.9 **WHEREAS**, the staff recommendations conform with the 2025 Active Transportation Program Guidelines and other statutory requirements for the Active Transportation Program; and
- 1.10 **WHEREAS**, the Commission considered the staff recommendations and public testimony at its June 26-27, 2025, meeting.
- 2.1 **NOW THEREFORE BE IT RESOLVED**, that the Commission adopts the 2025 Active Transportation Program Metropolitan Planning Organization component for seven large metropolitan planning organizations, which includes \$61,344,000 in funding for thirty-four projects, as reflected in Attachment B; and
- 2.2 **BE IT FURTHER RESOLVED**, that a project included in the adopted 2025 Active Transportation Program Metropolitan Planning Organization component must comply with the Active Transportation Program Guidelines; and
- 2.3 **BE IT FURTHER RESOLVED**, that Commission adoption of the 2025 Active Transportation Program Metropolitan Planning Organization Program of Projects does not authorize an implementing agency to begin work on a project. The implementing agency must not award a contract, and work cannot begin until the Commission has approved allocation of Active Transportation Program Funds for the project and, when applicable, federal approval to begin work has been received; and
- 2.4 **BE IT FURTHER RESOLVED**, that Commission staff, in consultation with the California Department of Transportation and project sponsors, is authorized to make minor technical changes as needed to the 2025 Active Transportation Program Metropolitan Planning Organization component to reflect the most current information, or to clarify the Commission's programming commitments, and shall request Commission approval of any substantive changes; and
- 2.5 **BE IT FURTHER RESOLVED**, that the Commission directs staff to post the 2025 Active Transportation Program Metropolitan Planning Organization component program of projects on the Commission's website.

California Transportation Commission
2025 Active Transportation Program
Metropolitan Planning Organization Component
Staff Recommendations (\$1,000's)

				Resolution G-25-47, Amending Resolution G-25-33															
MPO	Application ID	Project Title	County	Total Project Cost	ATP Funding	25-26	26-27	27-28	28-29	PA&ED	PS&E	R/W	CON	CON NI	Project Type	DAC	SRTS	State Score (Out of 100)	Regional Score #
FCOG	6- Fresno County -1	Easton Sidewalk	Fresno	\$ 7,256	\$ 682	\$ 198	\$ 258	\$ 226		\$ 198	\$ 258	\$ 226			Medium Infrastructure	X	X	81	90
FCOG	6-Coalinga, City of -1	Coalinga Bike and Pedestrian Perimeter Network Gap Closures and Crossings	Fresno	\$ 1,350	\$ 1,195	\$ 86	\$ 452		\$ 657	\$ 86	\$ 57	\$ 395	\$ 657		Small Infrastructure	X		89	83
MTC	4-Contra Costa County-3	Verde K-8 Safe Routes to School	Contra Costa	\$ 5,522	\$ 4,360	\$ 776		\$ 362	\$ 3,222	\$ 276	\$ 362		\$ 3,222	\$ 500	Medium Infrastructure/ Non-infrastructure	X	X	93	98.0
MTC	4-San Rafael, City of-1	Downtown San Rafael North-South Greenway Gap Closure	Marin	\$ 5,444	\$ 4,355		\$ 300	\$ 480	\$ 3,575	\$ 300	\$ 480		\$ 3,575		Medium Infrastructure	X		87	96.8
MTC	4-Solano County-1	Benicia Road Complete Streets Phase 2	Solano	\$ 2,152	\$ 1,630	\$ 168		\$ 1,462			\$ 168		\$ 1,462		Small Infrastructure	X	X	93	96.5
MTC	4-Berkeley, City of-1	Southwest Berkeley Bicycle Boulevards	Alameda	\$ 3,875	\$ 3,430				\$ 3,430				\$ 3,430		Medium Infrastructure	X		92	95.5
MTC	4-Alameda, City of-1	Willie Stargell Avenue Safety Improvements	Alameda	\$ 7,408	\$ 673			\$ 673					\$ 673		Medium Infrastructure	X	X	89	95.3
SACOG	3-Sacramento, City of-3 §	Northgate Boulevard Complete Streets	Sacramento	\$ 69,522	\$ 1,900		\$ 1,900			\$ 1,900					Large Infrastructure	X	X	91	85.7
SACOG	3-West Sacramento, City of-0011 #	Sidewalks and Transportation Equity Program Implementation (Phase 1)	Yolo	\$ 31,050	\$ 2,000	\$ 2,000				\$ 2,000					Large Infrastructure	X		N/A	82.4
SACOG	3-Sacramento County-1	Whitney Avenue Road Diet and Multimodal Improvements	Sacramento	\$ 8,172	\$ 402		\$ 402			\$ 402					Medium Infrastructure	X	X	82	81.5
SACOG	3-Sacramento County-1	Whitney Avenue Non-Infrastructure Program	Sacramento	\$ 56	\$ 50		\$ 50							\$ 50	Non Infrastructure	X	X	82	81.5
SACOG	3-Yolo County-1	Yolo County - State Route 128 and Interstate 505 Interchange and Russell Blvd. - Active Transportation Program Improvements	Yolo	\$ 14,009	\$ 369		\$ 369				\$ 369				Large Infrastructure	X	X	75	76.4
SCAG	11-Imperial County Office of Education-1	Safe Pathways to School	Imperial	\$ 338	\$ 338		\$ 338							\$ 338	Non Infrastructure	X	X	72	92
SCAG	7-Los Angeles County-5	Willowbrook Walk and Roll Pedestrian Safety Enhancements	Los Angeles	\$ 9,990	\$ 7,990	\$ 541		\$ 814	\$ 6,635	\$ 541	\$ 814	\$ 400	\$ 6,235		Medium Infrastructure	X		94	103
SCAG	7-El Monte, City-3 §	Valley Boulevard Complete Street	Los Angeles	\$ 34,505	\$ 4,355	\$ 917		\$ 3,438		\$ 917	\$ 2,521	\$ 917			Large Infrastructure	X		95	102
SCAG	7-Los Angeles, City of-2 §	Ballona Creek Multi-use Path Extension and Connections	Los Angeles	\$ 51,338	\$ 5,090	\$ 2,150	\$ 2,940			\$ 2,150	\$ 2,440	\$ 500			Large Infrastructure	X		89	100
SCAG	7-Southern California Association of Governments-4 #	Covina Town Center Bicycle and Pedestrian Safety and Connectivity (Gap Closure) Quick-Build Roadway	Los Angeles	\$ 790	\$ 222	\$ 222							\$ 222		Quick Build	X		N/A	100
SCAG	7-Southern California Association of Governments-2 #	CSUDH Transportation Demand Management Plan: LA 28 and Beyond	Los Angeles	\$ 284	\$ 284	\$ 284								\$ 284	Plan	X	X	N/A	92
SCAG	7-Los Angeles County-QB1 *	Norwalk Boulevard Vision Zero Quick-Build Pedestrian Safety	Los Angeles	\$ 803	\$ 803	\$ 803							\$ 803		Quick Build	X		84	84
SCAG	12-Santa Ana, City of-3	Heroes Elementary School, Carver Elementary School, Willard Intermediate School, Wilson Elementary School Safe Routes To School	Orange	\$ 23,968	\$ 3,350	\$ 50	\$ 3,300			\$ 50	\$ 3,300				Large Infrastructure	X	X	91	111
SCAG	12-Orange, City of-1	Santiago Creek Bike Trail Gap Closure	Orange	\$ 9,996	\$ 2,302			\$ 2,302					\$ 2,302		Medium Infrastructure	X		83	100
SCAG	7-Southern California Association of Governments-3 #	Move OC: A Vibrant Path to Active Transportation	Orange	\$ 1,000	\$ 416	\$ 416								\$ 416	Plan	X		N/A	93.75
SCAG	8-Coachella, City of-1	Connecting Coachella	Riverside	\$ 22,334	\$ 2,150	\$ 2,150					\$ 2,150				Large Infrastructure	X	X	95	109
SCAG	8-Riverside County-3 §	Monroe Street and Interstate-10 Interchange Active Transportation Improvements	Riverside	\$ 134,012	\$ 2,287	\$ 2,287							\$ 2,287		Large Infrastructure	X		87.5	107.5
SCAG	8-Rialto, City of-1	City of Rialto Safe Routes to School Improvements	San Bernardino	\$ 8,096	\$ 3,934				\$ 3,934				\$ 3,934		Medium Infrastructure	X	X	90	110
SCAG	7-Ventura, City of -1	Eastside Neighborhood Greenway	Ventura	\$ 8,667	\$ 1,410		\$ 280	\$ 1,130		\$ 280	\$ 1,130				Medium Infrastructure	X	X	86	98
SCAG	7-Thousand Oaks, City of-1	State Route 23 and Olsen Road Bike Improvements	Ventura	\$ 826	\$ 92		\$ 92						\$ 92		Small Infrastructure			71	83
SJCOG	10-Stockton-01	Citywide Stockton Bicycle-Pedestrian Connectivity	San Joaquin	\$ 6,494	\$ 1,450				\$ 1,450				\$ 1,450		Medium Infrastructure	X	X	96	119.25

California Transportation Commission
2025 Active Transportation Program
Metropolitan Planning Organization Component
Staff Recommendations (\$1,000's)

Resolution G-25-47, Amending Resolution G-25-33																			
MPO	Application ID	Project Title	County	Total Project Cost	ATP Funding	25-26	26-27	27-28	28-29	PA&ED	PS&E	R/W	CON	CON NI	Project Type	DAC	SRTS	State Score (Out of 100)	Regional Score ‡
STANCOG	10-Waterford, City of-1	Waterford Safe Routes to School - Washington Road	Stanislaus	\$ 845	\$ 845	\$ 5	\$ 95	\$ 745		\$ 5	\$ 55	\$ 40	\$ 745		Small Infrastructure	X	X	66	85
STANCOG	10-Waterford, City of-3	Waterford Safe Routes to School - Welch Street	Stanislaus	\$ 948	\$ 184	\$ 5	\$ 95	\$ 84		\$ 5	\$ 55	\$ 40	\$ 84		Small Infrastructure	X	X	73	82.8
SBCAG	5-Santa Barbara-1 #	Funk Zone Intersection Safety Enhancements	Santa Barbara	\$ 1,300	\$ 1,300	\$ 110	\$ 1,190			\$ 20	\$ 90		\$ 1,190		Small Infrastructure	X		N/A	91.0
SBCAG	5-Santa Maria-1 #	Railroad Avenue and Depot Street Corridor Enhancement	Santa Barbara	\$ 2,793	\$ 1,021		\$ 1,021						\$ 1,021		Small Infrastructure	X	X	N/A	86.5
SBCAG	5-Buellton-1 #	West Highway 246 Pedestrian and Bike Safety Improvements	Santa Barbara	\$ 836	\$ 440		\$ 440						\$ 440		Small Infrastructure		X	N/A	81.0
SBCAG	5-Santa Barbara-2 #	Increasing Secure, Convenient, and Accessible Bicycle Parking in Santa Barbara	Santa Barbara	\$ 35	\$ 35	\$ 35							\$ 35		Small Infrastructure	X		N/A	77.8
				\$ 476,014	\$ 61,344														
	Notes					Abbreviations, Acronyms, and Initialisms													
	‡ Regional scores and ranks are on various scales (and not necessarily out of 100). Individual scoring systems are outlined in each MPO's guidelines.					CON: Construction Phase													
	# Project was not submitted to the state competition.					DAC: Disadvantaged Community													
	§ Project requires baseline agreement.					FCOG: Fresno Council of Governments													
	* Implementing agency for this project is Southern California Association of Governments.					MPO: Metropolitan Planning Organization													
						MTC: Metropolitan Transportation Commission													
						NI: Non-Infrastructure													
						PA&ED: Project Approval and Environmental Documentation Phase													
						PS&E: Plans, Specifications and Estimate Phase													
						R/W: Right-of-Way Phase													
						SACOG: Sacramento Area Council of Governments													
						SBCAG: Santa Barbara County Association of Governments													
						SCAG: Southern California Association of Governments													
						SJCOG: San Joaquin Council of Governments													
						SRTS: Safe Routes to School Project													
						StanCOG: Stanislaus Council of Governments													

Reference No.: 4.26
June 26-27, 2025
Attachment C

2025 Active Transportation Program Metropolitan Planning Organization Component
Submittals from Seven Metropolitan Planning Organizations

- Fresno Council of Governments
- Metropolitan Transportation Commission
- Sacramento Area Council of Governments
- Southern California Association of Governments
- San Joaquin Council of Governments
- Stanislaus Council of Governments
- Santa Barbara County Association of Governments

April 2, 2025

Mrs. Tanisha Taylor, Executive Director
California Transportation Commission
1120 N Street Room 2221 (MS-52)
Sacramento, CA 95814

Attention: Anja Aulenbacher

Subject: Fresno Council of Governments Regional Active Transportation Program of Projects – Cycle 7

Ms. Aulenbacher:

The California Transportation Commission approved the Fresno Council of Governments (Fresno COG) Cycle 7 Regional Competitive Active Transportation Program (ATP) Guidelines on June 29, 2022. The Fresno COG regional competitive ATP call for projects was released on June 17, 2024, and closed on November 20, 2024. The multidisciplinary advisory group (MAG) evaluated ten projects, two of which are being recommended for funding for a total of \$1.877 million. One of the recommended projects are fully funded and one is partially funded project due to limited funding capacity. Fresno COG and the applicant will seek additional funding and ensure project delivery. The Fresno COG Policy Board is anticipated to approve the recommended project list for the Fresno COG regional competitive ATP on February 27, 2025.

The recommended projects include a broad range of active transportation infrastructure, including new sidewalks that are ADA compliant, pedestrian improvements, rectangular rapid flashing beacons, trail amenities, and increase health and safety when accessing frequented locations. One of the projects provides safe routes to schools for students walking and bicycling to school in an unincorporated community. The other project will fill gaps in the current network in a disadvantaged community. The ATP guidelines require that at least 25% of the funds be directed to benefit projects in disadvantaged communities; however, Fresno COG is recommending a project list where 100% of the funds will be directed to benefit projects in disadvantaged communities.

The submittal of the recommended program of projects meets the requirements and furthers the ATP purpose and goals set-forth by the 2025 ATP Guidelines that were adopted by the California Transportation Commission.

Included with this letter are the following attachments:

Attachment A – Fresno COG recommended programming spreadsheet
Attachment B – Complete list of submitted projects and contingency list & applications
Attachment C – Fresno COG Policy Board minute excerpt and resolution approving the recommended program of projects
Attachment D – List of scoring committee members
Attachment E – Description of Unbiased Project Selection Method
Attachment F – Updated Part A4: Project Details Forms
Attachment G – Updated PPR's

If any additional information is needed or should you have any questions or comments, please feel free to call Simran Jhutti at (559) 233-4148 ext. 241.

Attachment C

BEFORE THE
FRESNO COUNCIL OF GOVERNMENTS
RESOLUTION NO. 2025-03

IN THE MATTER OF:

**2025 FCOG ACTIVE TRANSPORTATION
PROGRAM CYCLE 7**

RESOLUTION ADOPTING THE 2025 FCOG
ACTIVE TRANSPORTATION PROGRAM
CYCLE 7 – MPO COMPONENT FUNDING
RECOMMENDATIONS

WHEREAS, Fresno Council of Governments (FCOG) is a Metropolitan Planning Organization (MPO) with and urbanized population greater than 200,000; and

WHEREAS, Streets and Highways Code Section 2381 requires that forty percent of the funds appropriated for the Active Transportation Program (ATP) be distributed to metropolitan planning organizations in urban areas with populations greater than 200,000, in proportion to their relative share of population; and

WHEREAS, FCOG's portion of ATP cycle 7 funds to be awarded through the local MPO component is a total of \$1.87 million for programming years 2025-26, 2026-27, 2027-28 and 2028-29; and

WHEREAS, pursuant to Section 2382(c), no less than 25% of the overall program funds will benefit disadvantaged communities during each program cycle; and

WHEREAS, on January 14, 2025 the ATP Multidisciplinary Advisory Group scored and ranked the submitted projects based on criteria adopted by the FCOG Board on April 25, 2024 with revised guidelines including augmentation of Congestion Mitigation Air Quality (CMAQ) funds on September 26, 2024.

NOW, THEREFORE BE IT RESOLVED, that the Fresno Council of Governments hereby adopts the 2025 FCOG Active Transportation Program Cycle 7 - MPO Component Funding Recommendations, as shown on Attachment 1.

THE FOREGOING RESOLUTION was passed and adopted by the Fresno Council of Governments this 27th day of February, 2025.

AYES: Clovis, Coalinga, Firebaugh, Fowler, Fresno City, Huron, Kerman, Kingsburg, Mendota, Orange Cove, Parlier, Reedley, Sanger, San Joaquin, Selma, Fresno County.

NOES: None

ABSTAIN: None

ABSENT: None

ATTEST:

Signed: 

Alma Beltran, Chair

I hereby certify that the foregoing is a true copy of a resolution of the Fresno Council of Governments duly adopted at a regular meeting dated above.

Signed: 

Robert Phipps, Executive Director



**METROPOLITAN
TRANSPORTATION
COMMISSION**

Bay Area Metro Center
375 Beale Street, Suite 800
San Francisco, CA 94105
415.778.6700
www.mtc.ca.gov

Sue Noack, Chair
Cities of Contra Costa County

April 22, 2025

Stephanie Moulton-Peters,
Vice Chair
Marin County and Cities

Margaret Abe-Koga
Santa Clara County

Eddie Abn
San Francisco Bay Conservation
and Development Commission

Candace Andersen
Contra Costa County

Marilyn Ezzy Ascraft
Cities of Alameda County

Pat Burt
Cities of Santa Clara County

David Canepa
San Mateo County

Dina El-Tawansy
California State
Transportation Agency

Victoria Fleming
Sonoma County and Cities

Dorene M. Giacomini
U.S. Department of Transportation

Alicia John-Baptiste
San Francisco Mayor's Appointee

Rebecca Kaplan
Oakland Mayor's Appointee

Matt Mahan
San Jose Mayor's Appointee

Amber Manfree
Napa County and Cities

Mitch Mashburn
Solano County and Cities

Myrna Melgar
City and County of San Francisco

Nate Miley
Alameda County

Gina Papan
Cities of San Mateo County

Belia Ramos
Association of Bay Area
Governments

Libby Schaaf
U.S. Department of Housing
and Urban Development

Andrew B. Fremier
Executive Director

Alix Bockelman
Chief Deputy Executive Director

Tanisha Taylor, Executive Director
California Transportation Commission
1120 N Street, Room 2221 – MS 52
Sacramento, CA 95814

Attention: Anja Aulenbacher, Associate Deputy Director

Dear Executive Director Taylor:

With this letter, I am pleased to transmit MTC's adopted program of projects for the 2025 Regional Active Transportation Program (rATP), Cycle 7.

The 2025 ATP Fund Estimate, as revised, identified \$14.5 million of programming capacity to the region. Following the competitive Call for Projects, MTC staff received 35 applications requesting \$324 million. Based on scores provided by the CTC from the state evaluation process and a thorough evaluation of MTC's regional supplemental questions, MTC adopted the Cycle 7 rATP on February 26, 2025. The Bay Area regional program includes five projects, with 100% of the funds benefiting disadvantaged communities. MTC requests that the CTC approve MTC's rATP project list in June 2025.

As allowed by our Regional Guidelines adopted in March 2024, MTC also adopted a list of contingency projects in case there are any funding augmentations, project failures or savings. The Contingency List includes eight projects totaling \$135.5 million and is listed in priority order.

Please contact Karl Anderson at (415) 778-6645 if you need further information about our regional program. We look forward to working with you in finalizing the 2025 rATP and delivering the selected projects.

Sincerely,

Theresa Romell
Section Director, Funding Policy and Programs
Metropolitan Transportation Commission

TR:KA

cc: Dee Lam, Caltrans HQ Local Assistance

Attachments: MTC Cycle 7 Regional ATP Program

J:\PROJECT\Funding\ATP\Regional ATP\2025 rATP (Cycle 7)\Final 2025 rATP\01_2025 rATP7 Transmittal Letter.docx

Date: March 27, 2024
W.I.: 1515
Referred by: PAC
Revised: 02/26/25-C

ABSTRACT

Resolution No. 4633, Revised

This resolution adopts the Active Transportation Program (ATP) Regional Program Cycle 7 Guidelines for the San Francisco Bay Area, for submission to the California Transportation Commission (CTC), consistent with the provisions of Senate Bill 99 and Assembly Bill 101.

This resolution includes the following attachments:

- Attachment A – Guidelines: Policies, Procedures, and Project Selection Criteria
- Attachment B – 2025 Regional ATP Program of Projects

This resolution was amended via Commission action on February 26, 2025 to update the funding targets identified in Attachment A, Appendix A-2, to reflect the revised 2025 Active Transportation Program Fund Estimate adopted by the California Transportation Commission on August 15, 2024 and to update Attachment B, 2025 Regional ATP Program of Projects.

Further discussion of these actions is contained in the Summary Sheet to the MTC Programming and Allocations Committee dated March 13, 2024 and February 12, 2025.

Date: March 27, 2024
W.I.: 1515
Referred by: PAC

RE: Adoption of Regional Active Transportation Program (ATP) Cycle 7 Guidelines and Program of Projects

METROPOLITAN TRANSPORTATION COMMISSION
RESOLUTION NO. 4633

WHEREAS, the Metropolitan Transportation Commission (MTC) is the regional transportation planning agency for the San Francisco Bay Area pursuant to Government Code Section 66500 *et seq.*; and

WHEREAS, MTC has adopted and periodically revises, pursuant to Government Code Sections 66508 and 65080, a Regional Transportation Plan (RTP); and

WHEREAS, MTC is the designated Metropolitan Planning Organization (MPO) for the nine-county San Francisco Bay Area region and is required to prepare and endorse a Transportation Improvement Program (TIP) which includes federal funds; and

WHEREAS, MTC is the designated recipient for federal funding administered by the Federal Highway Administration (FHWA) assigned to the MPO/Regional Transportation Planning Agency (RTPA) of the San Francisco Bay Area for the programming of projects (regional federal funds); and

WHEREAS, the California State Legislature passed and the Governor signed into law Senate Bill 99 (Chapter 359, Statutes 2013) and Assembly Bill 101 (Chapter 354, Statutes 2013), establishing the Active Transportation Program (ATP); and

WHEREAS, MTC adopts, pursuant to Streets and Highways Code Section 2381(a)(1), an Active Transportation Program of Projects using a competitive process consistent with guidelines adopted by the California Transportation Commission (CTC) pursuant to Streets and Highways Code Section 2382(a), that is submitted to the CTC and the California Department of Transportation (Caltrans); and

WHEREAS, MTC has developed, in cooperation with CTC, Caltrans, operators of publicly owned mass transportation services, congestion management agencies, countywide

transportation planning agencies, and local governments, guidelines to be used in the development of the ATP; and

WHEREAS, MTC will use state evaluation scores for the statewide application portion, and will assemble a multi-disciplinary evaluation panel to evaluate the regional supplemental application portion, to recommend candidate ATP projects for MTC inclusion in the Active Transportation Program of Projects; and

WHEREAS, the ATP is subject to public review and comment; now, therefore, be it

RESOLVED, that MTC approves the guidelines to be used in the evaluation of candidate projects for inclusion in the ATP, as set forth in Attachment A of this resolution, and be it further

RESOLVED, that MTC approves the Active Transportation Program of Projects, as set forth in Attachment B of this resolution, and be it further

RESOLVED that the Executive Director or designee can make technical adjustments and other non-substantial revisions; and be it further

RESOLVED, that the Executive Director shall forward a copy of this resolution, and such other information as may be required to the CTC, Caltrans, and to such other agencies as may be appropriate.

METROPOLITAN TRANSPORTATION COMMISSION

A handwritten signature in dark ink, appearing to be 'AP' followed by a long horizontal stroke.

Alfredo Pedroza, Chair

The above resolution was entered into by the Metropolitan Transportation Commission at a duly called and noticed meeting held in San Francisco, California and at other remote locations, on March 27, 2024.



1415 L Street,
Suite 300
Sacramento, CA
95814

916.321.9000
sacog.org

Auburn
Citrus Heights
Colfax
Davis
El Dorado County
Elk Grove
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Marysville
Placer County
Placerville
Rancho Cordova
Rocklin
Roseville
Sacramento
Sacramento County
Sutter County
West Sacramento
Wheatland
Winters
Woodland
Yolo County
Yuba City
Yuba County

March 20, 2025

Ms. Tanisha Taylor, Executive Director
California Transportation Commission
1120 N Street, Mail Station 52
Sacramento, CA 95814

Dear Ms. Taylor:

I am pleased to present the Sacramento Area Council of Governments' Six-County Regional Active Transportation Program (SACOG Regional ATP) project list to the California Transportation Commission for adoption. The SACOG Board of Directors unanimously approved \$4,721,000 for projects and a contingent project list in priority order on March 20, 2025.

The projects recommended will support active transportation by improving safety, access, and mobility for bicyclists and pedestrians of all capabilities. The recommended projects support regional and statewide equity goals by investing 100 percent of the funds in providing a meaningful benefit to disadvantaged community residents. Project sponsors for projects recommended for preconstruction funding will continue to seek construction funding through federal, state, regional, and local opportunities.

The attached programming list shows the funding awards and years of allocation for SACOG Regional ATP projects.

If you have any questions or concerns, please contact Summer Lopez at slopez@sacog.org or (916) 340-6228.

Sincerely,

A handwritten signature in blue ink, appearing to read "James Corless", is written over a light blue horizontal line.

James Corless
Executive Director

Attachments:

- 1) Regional ATP Funding Recommendations and Contingency List
- 2) Signed Resolution Approving Regional ATP Recommendations
- 3) Project Scopes and strengths
- 4) Working Group Roster
- 5) Regional ATP Policy Framework
- 6) Regional ATP Evaluation Process
- 7) MPO Programming Spreadsheet
- 8) SACOG Board Packet
- 9) Project Applications of funded projects

Cc: Anja Aulenbacher, California Transportation Commission
Woodrow Deloria, El Dorado County Transportation Commission
Matt Click, Placer County Transportation Planning Agency



SACRAMENTO AREA COUNCIL OF GOVERNMENTS

RESOLUTION NO. 22 – 2025

**APPROVING SACOG 2025 REGIONAL ACTIVE
TRANSPORTATION PROGRAM**

WHEREAS, Streets and Highways Code Section 2384 requires the California Transportation Commission (CTC) adopt a program of projects to receive allocations under the Active Transportation Program (ATP); and

WHEREAS, 40% of ATP funding is distributed to large urbanized areas with funding distributed by population and managed by the Metropolitan Planning Organization (“Regional ATP”); and

WHEREAS, the Sacramento Area Council of Governments (SACOG) is the Metropolitan Planning Organization (MPO) for the Sacramento Metropolitan area and the Yuba City/Marysville Urbanized area; and

WHEREAS, the 2025 ATP Guidelines were adopted by the CTC on March 22, 2024; and

WHEREAS, the CTC adopted the Addendum to the 2025 ATP Guidelines on August 16, 2024; and

WHEREAS, the guidelines describe the policies, standards, criteria, and procedures for the development and management of the 2025 Active Transportation Program funding cycle; and

WHEREAS, SACOG may elect to have a supplemental MPO-specific call for projects for the Regional ATP; and

WHEREAS, SACOG is responsible for developing the guidelines for the Regional ATP, and for recommending proposed projects to the CTC for adoption; and

WHEREAS, the CTC adopted SACOG’s Amended 2025 Regional ATP policies, procedures, and project selection criteria on October 17, 2024; and

WHEREAS, SACOG must use a multidisciplinary advisory group to assist in evaluating Regional ATP project applications; and

WHEREAS, pursuant to Streets and Highway Code Section 2382 subdivision (c), no less than 25 percent of overall program funds must benefit disadvantaged communities during each program cycle; and

WHEREAS, SACOG requires that no less than 40 percent of overall program funds must provide meaningful benefit to disadvantaged communities during each program cycle; and

WHEREAS, SACOG may develop a ranked contingency list of projects to be amended into the ATP in the event of a revised state budget, or if a programmed project is delivered for less or fails; and

WHEREAS, the staff recommendations conform to the 2025 Regional ATP Policy Framework, the 2025 ATP Guidelines, and other statutory requirements for the Active Transportation Program.

NOW THEREFORE, BE IT RESOLVED THAT:

1. The SACOG Board adopts the attached Regional ATP funding recommendations and contingency list for the metropolitan planning area of El Dorado, Placer, Sacramento, Sutter, Yolo, and Yuba counties.
2. The SACOG Executive Director, or his designee, will forward the Regional ATP funding recommendations and contingency list to the CTC for adoption.
3. SACOG staff will prepare the required Metropolitan Transportation Improvement Program (MTIP) amendments.

PASSED AND ADOPTED, this 20th day of March 2025, by the following vote of the Board of Directors:

AYES: Directors Andreatta, Flores (for Bains), Boomgaarden, Bradford, Branscum, Bulahan, Chapdelaine, Clark-Crets, Clerici, Dowdin Calvillo, Early, Houdesheldt, Hume, Jennings, Kozlowski, Loren, Maple, McCully, Sander, Schaefer, Teter, Veerkamp, Vega, Villegas, Vice Chair Gayaldo, and Chair Suen

NOES: None

ABSTAIN: None

ABSENT: Directors Chapman, Desmond, Jones, Kennedy and Rodriguez

Darren Suen

Darren Suen
Chair



James Corless
Executive Director

Signature: Darren Suen
Darren Suen (Apr 6, 2025 22:48 PDT)

Email: dsuen@elkgrovecity.org








SACOG resolution 22-2025 ATP

Final Audit Report

2025-04-07

Created:	2025-03-24
By:	Lanette Espinoza (LEspinoza@sacog.org)
Status:	Signed
Transaction ID:	CBJCHBCAABAAglyZlusxB0RazyJHNVfSsEYJmkSOppGw

"SACOG resolution 22-2025 ATP" History

-  Document created by Lanette Espinoza (LEspinoza@sacog.org)
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SOUTHERN CALIFORNIA
ASSOCIATION OF GOVERNMENTS
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Energy & Environment
Jenny Crosswhite, Santa Paula

Transportation
Tim Sandoval, Pomona

February 19, 2025

Anja Aulenbacher
California Transportation Commission
1120 N Street, MS-52
Sacramento, CA 95814

Subject: SCAG 2025 Regional Active Transportation Program

Dear Ms. Aulenbacher:

SCAG's Regional Council approved the 2025 ATP Regional Guidelines (Regional Guidelines) on April 4, 2024, and approved the Amended Regional Guidelines on September 5, 2024, to reflect the reduced ATP fund estimate. The Regional Guidelines establish a selection process for two categories of projects: (1) Implementation Projects and (2) Planning & Capacity Building Projects.

Implementation Projects: No less than 95 percent of the regional funds were recommended to projects in this category, which includes Infrastructure, Non-infrastructure, and Infrastructure with Non-infrastructure components projects. The selection process for Implementation Projects was the same as in previous cycles and was predominately managed by the six county transportation commissions in the SCAG region. Eligible applicants applied for these funds by submitting an application through the statewide ATP call for projects. Base scores were established through the statewide ATP review process. The Regional Guidelines allow county transportation commissions to prioritize projects by adding up to twenty points to supplement the state-provided base scores. As in previous cycles, the Board of each county transportation commission approved the methodology for assigning the additional points. Total funding available in each county was based on population-based funding targets.

Planning & Capacity Building Projects: No more than five percent of the regional funds were recommended to proposals in this category, which includes Planning, Non-infrastructure, and Quick-Build projects. The projects considered for funding in this category included projects that were submitted through the statewide ATP call for projects using the state's Planning, Non-infrastructure, and Quick-Build applications and Planning and Quick-Build projects submitted through SCAG's supplemental call for projects, administered through the 2024 SCP Active Transportation and Safety call for applications. Total funding available in each county was based on population-based funding targets.

On April 3, 2025 SCAG's Regional Council is anticipated to adopt the project list and contingency project list for the SCAG 2025 Regional Active Transportation Program (see attached draft resolution). The Regional ATP of 15 projects and 113 contingency projects was assembled by combining recommendations from the Implementation and the Planning & Capability Building categories. The

recommended program allocates 97.9 percent of available funds to disadvantaged communities exceeding the statewide minimum requirement of 25 percent. The table below summarizes funding recommendations by county.

ATP Funding by County (\$1,000s)			
	Implementation Projects	Planning & Capacity Building Projects	Total ATP
Imperial	\$338	\$0	\$338
Los Angeles	\$17,435	\$1,291.51*	\$18,726.51
Orange	\$5,652	\$400*	\$6,052
Riverside	\$4,437	*	\$4,437
San Bernardino	\$3,934	*	\$3,934
Ventura	\$1,502	*	\$1,502
SCAG	\$0	\$33.49**	\$33.49
Total	\$33,298	\$1,725	\$35,023

**This table reflects the planning and quick-build projects funded by the Regional ATP and does not reflect the total funding awarded for plans and quick-build projects through the 2024 SCP Active Transportation and Safety call.*

***Approximately two percent of the Planning & Capacity Building funding supports SCAG grant administration.*

Each of the six counties have partially funded projects. In some cases, the partial funding will fund phases of the proposed project, such as environmental or design. In other cases, the local agency will be providing funds to complete the project as proposed and in others the scope has been revised to complete a component of the proposed project. Revised Project Programming Request are attached for all partially funded projects.

Sincerely,

Rachel Om

Rachel Om
Senior Regional Planner
Southern California Association of Governments

RESOLUTION NO. 25-672-3

**A RESOLUTION OF THE SOUTHERN CALIFORNIA
ASSOCIATION OF GOVERNMENTS (SCAG)
APPROVING THE SCAG 2025 REGIONAL ACTIVE TRANSPORTATION
PROGRAM**

WHEREAS, the Southern California Association of Governments (SCAG) is the Metropolitan Planning Organization, for the six county region consisting of Los Angeles, Orange, San Bernardino, Riverside, Ventura, and Imperial counties pursuant to 23 U.S.C. § 134 et seq. and 49 U.S.C. § 5303 et seq.;

WHEREAS, the Active Transportation Program (ATP) was created by Senate Bill 99 (Chapter 359, Statutes of 2013) to encourage increased use of active modes of transportation, such as biking and walking;

WHEREAS, Streets and Highways Code Section 2382(k) allows the California Transportation Commission (Commission) to adopt separate guidelines for the metropolitan planning organizations charged with awarding funds to projects pursuant to Streets and Highways Code Section 2381(a)(1) relative to project selection;

WHEREAS, SCAG adopted the Regional ATP Guidelines on April 4, 2024 and the Amended Regional ATP Guidelines on September 5, 2024 with input from the six Southern California county transportation commissions to govern award of projects funded through the SCAG Regional ATP;

WHEREAS, the 2025 ATP Statewide Guidelines (Resolution G-24-31 and Resolution G-24-66) require metropolitan planning organizations to submit their Regional Program of projects and contingency list to the Commission by April 22, 2025;

WHEREAS, SCAG in collaboration with the six Southern California county transportation commissions has implemented a project selection process that meets the requirements of the 2025 ATP Statewide Guidelines (Resolution G-24-31 and Resolution G-24-66) and SCAG's 2025 ATP Regional Guidelines, and has reached consensus on the SCAG 2025 Regional ATP.

NOW, THEREFORE, BE IT RESOLVED, that the Regional Council of the Southern California Association of Governments does hereby adopt the SCAG 2025 Regional Active Transportation Program.

REGIONAL COUNCIL OFFICERS

President
Curt Hagman
County of San Bernardino

First Vice President
Cindy Allen, Long Beach

Second Vice President
Ray Marquez, Chino Hills

Immediate Past President
Jan C. Harnik, Riverside County
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COMMITTEE CHAIRS

Executive/Administration
Curt Hagman
County of San Bernardino

Community, Economic &
Human Development
David J. Shapiro, Calabasas

Energy & Environment
Jenny Crosswhite, Santa Paula

Transportation
Tim Sandoval, Pomona

BE IT FURTHER RESOLVED THAT:

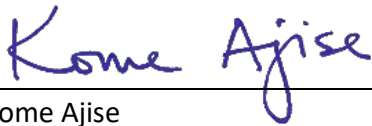
1. The Regional Council directs staff to submit the Regional ATP for the SCAG 2025 Regional ATP to the CTC.
2. The Regional Council defers approval of any further minor revision and administrative amendments to the SCAG 2025 Regional ATP to SCAG's Executive Director or his designee.

PASSED, APPROVED AND ADOPTED by the Regional Council of the Southern California Association of Governments at its regular meeting this 3rd day of April 2025.



Curt Hagman
President, SCAG
County of San Bernardino

Attested by:



Kome Ajise
Executive Director

Approved as to Form:



Jeffery Elder
Chief Counsel



SAN JOAQUIN COUNCIL OF GOVERNMENTS

555 E. Weber Avenue • Stockton, California 95202 • P 209.235.0600 • F 209.235.0438 • www.sjcog.org

To: Tanisha Taylor, CTC Executive Director
Laurie Waters, CTC Deputy Director

Fr: Ryan Niblock, SJCOG Deputy Director

Re: 2025 Active Transportation Program
Project selections from San Joaquin County

Paul Akinjo
CHAIR

Christina Fugazi
VICE CHAIR

Diane Nguyen
EXECUTIVE DIRECTOR

Member Agencies
CITIES OF
ESCALON,
LATHROP,
LODI,
MANTECA,
MOUNTAIN HOUSE,
RIPON,
STOCKTON,
TRACY,
AND
THE COUNTY OF SAN
JOAQUIN

The San Joaquin Council of Government's (SJCOG) ATP Call for Projects made available \$1.45 million from the MPO component of ATP Cycle 7, \$4.232 million of Congestion Mitigation Air Quality funds (CMAQ), and \$9.05 million in Measure K (regional sales tax). Three projects were awarded funding in SJCOG's ATP Call for Projects. Two awarded projects received \$8.238 million in Measure K funds alone, while one project received \$6.494 million from CMAQ, Measure K, and ATP funding sources. The intention is to formalize this recommendation at the June California Transportation Commission (CTC) meeting for the following project:

- Stockton – Citywide Stockton Bicycle-Pedestrian Connectivity Project
 - \$1.450 million ATP funds- \$842,000 State, \$608,000 Federal
 - \$4.232 million Federal CMAQ
 - \$812,000 Measure K

The following required documentation is enclosed:

- List of members in our multidisciplinary advisory group
- Description of unbiased project selection method
- Board resolution approving project(s)
- Completed programming sheet
- List of all projects evaluated and regional competition scores
- Contingency list
- Updated project programming requests

If you have any questions regarding this submittal, please feel free to contact my staff members, Joel Campos at (209) 235-1090 campos@sjcog.org or Jacob Williams at (209) 235-0589 williams@sjcog.org.

Sincerely,

RYAN NIBLOCK
Deputy Director – Programming and Project Delivery
San Joaquin Council of Governments



SAN JOAQUIN COUNCIL OF GOVERNMENTS

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Paul Akinjo

CHAIR

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CITIES OF

ESCALON,

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MOUNTAIN HOUSE,

RIPON,

STOCKTON,

TRACY,

and

THE COUNTY OF SAN

JOAQUIN

MINUTE RESOLUTION NUMBER 2025-01

Whereas, the SJCOG Board of Directors has accepted the staff report under authority of Minute Resolution No. 2025-01 approved by the SJCOG Board of Directors meeting on February 27, 2025.

Agenda Item # 4G.

Approve 2025 Regional Active Transportation Program Cycle 7 Funding:

It was motion/seconded (Zuber/Craig) approval of the evaluation committee's recommendation to Award \$14,732,000 in funding to three projects. Motion passed unanimously by roll call.

A handwritten signature in blue ink, reading "Rosie Gutierrez", written over a horizontal line.

ROSIE GUTIERREZ

Clerk of the Board/Office Administrator

Date: March 4, 2025



STAFF REPORT

- SUBJECT:** Approve \$14.7 Million in Regional Active Transportation Program (ATP) Cycle 7 Funding
- RECOMMENDED ACTION:** Recommend approval of the evaluation committee’s recommendation to Award \$14,732,000 in funding to three projects as shown in Table 2 and a contingency list as shown in Table 3.

SUMMARY:

The San Joaquin Council of Governments (SJCOG) released its Regional Active Transportation Program (ATP) Cycle 7 call for projects on May 20, 2024. A total of \$1.45 million was allocated to the San Joaquin County region via SJCOG by the California Transportation Commission (CTC). The CTC funding was combined with \$4.232 million in Congestion Mitigation and Air Quality funds (CMAQ) and \$9.050 in Measure K Smart Growth, for a total of \$14.732 million for the regional program. A total of 11 applications were submitted to SJCOG’s call for projects totaling \$71,776,513 of funds requested. Those projects are listed in **Table 1**.

An evaluation committee of six volunteers was formed to review and score the submitted applications. Following the review of the projects by the volunteers, SJCOG aggregated the scores and convened the committee to garner feedback and solicit a recommendation from the group. The group agreed upon the recommendation of awarding the funds to the projects listed in **Table 2**, with **Table 3** being a contingency list if more funds are made available before the ATP Cycle 8 call for projects.

RECOMMENDATION:

Recommend approval of the evaluation committee’s recommendation to award \$14,732,000 in funding as shown in Table 2 and a contingency list as shown in Table 3

FISCAL IMPACT:

Assignment of \$14,732,000 in Regional ATP funding.

DISCUSSION:

Applications Submitted

11 projects were submitted as part of the call for projects, requesting a total of \$71,776,513 in funds. Through the combination of ATP, CMAQ, and Measure K funding, a total of \$14,732,000 was made available for this call for projects. Per board approved guidelines, in order to distribute these funds through a competitive process, a multidisciplinary evaluation committee was formed in order to score and evaluate each of the 11 total projects submitted.

Multidisciplinary Evaluation Committee Review and Initial Recommendations

SJCOG staff recruited volunteers to participate in an evaluation committee for the Regional ATP Cycle 7 call for projects. The six-member evaluation committee was comprised of a representative from the San Joaquin Regional Rail Commission, the City of Manteca, the City of Stockton, the City of Tracy, one member from the SJCOG Citizen's Advisory Committee, and SJCOG staff. The six-member committee was given the following tasks:

- Review applications for completeness and confirm project eligibility.
- Evaluate each application using the SJCOG and CTC adopted guidelines.
- Convene to develop a programming recommendation.

The members of the evaluation committee used the guidelines and rubrics approved by the SJCOG Board to carefully review and score each project. SJCOG staff collected the scores from each member, removed the highest and lowest scores from each project, and aggregated the remaining totals. This aggregate value was then combined with the state's overall scoring of each ATP project to produce the final score value, listed in Table 1. The scores were then brought to the evaluation committee for final review and discussion.

Table 1: Regional ATP Cycle 7 Call for Projects Scoring Table

Jurisdiction	Project Name	Total Cost	Requested	Scores
Stockton	Citywide Stockton Bicycle-Pedestrian Connectivity Project	\$6,493,955	\$6,493,955	119.25
Stockton	Downtown Walkability and Bikeability	\$6,997,000	\$6,997,000	112.75
Stockton	Main and Market Complete Streets Project, Phase 2	\$18,264,054	\$4,481,398	112.25
SJ County	Boggs Tract Sustainable Transportation Improvements	\$5,531,000	\$4,424,000	104
Manteca	Manteca Safe Route to School Priority Safety Project	\$8,140,200	\$6,512,160	100.25
SJ County	Burkett Garden Acres	\$4,389,000	\$3,511,000	98.25

Lathrop	East Lathrop Bicycle and Pedestrian Enhancements	\$24,740,000	\$24,740,000	89.5
SJ County	Thornton Community Improvements	\$4,105,000	\$3,079,000	82.5
Tracy	East Schulte Safety and Multimodal Community Corridor	\$6,648,000	\$5,075,000	76.5
SJ County	Country Club Blvd Active Transportation Improvements	\$2,108,000	\$1,897,000	71.5
SJRRC	North Lathrop Transfer Station Project	\$37,911,000	\$4,566,000	60.5
	Total	\$125,327,209	\$71,776,513	

At the conclusion of the evaluation committee discussion, the committee recommended that the projects listed in **Table 2** be awarded funding (as far as the \$14.732 million would allow).

Table 2: Projects Recommended for Programming from the Regional ATP Cycle 7 Call for Projects

Jurisdiction	Project Name	Total Cost	Requested	Awarded
Stockton	Citywide Stockton Bicycle-Pedestrian Connectivity Project	\$6,493,955	\$6,493,955	\$6,493,955
SJ County	Boggs Tract Sustainable Transportation Improvements	\$5,531,000	\$4,424,000	\$4,424,000
Manteca	Manteca Safe Route to School Priority Safety Project	\$8,140,200	\$6,512,160	\$3,814,045
	Total	\$27,162,155	\$24,427,115	\$14,732,000

50% Agency Cap

Per the Regional ATP Cycle 7 guidelines, approved by the SJCOG board, no agency or project may be awarded more than 50 percent of all available funds identified in the call for projects. If a project's total costs exceeds more than 50 percent of the available funds, then the project would only be eligible for partial funding.

For the current cycle, Stockton was awarded a total of \$7,366,000. After fully funding the Citywide Stockton Bicycle-Pedestrian Connectivity Project, a balance of \$872,045 remained. SJCOG contacted Stockton for guidance on allocating the remaining funds. Stockton indicated that delivering a project with this partial award amount would be highly challenging and, as a result, declined the remaining funds. The remaining funds, being the \$872,045, were then made available to the next highest scoring, non-fully funded, project, Manteca's Safe Route to School Priority Safety Project.

Contingency List

The evaluation committee recommended that if new funds become available, the priority would be to completely fund the partially funded projects. Any new funds would still be confined by the regional ATP guidelines regarding the fifty percent cap to one agency or project. See Table 3, below, for the recommended contingency list.

Table 3: Regional ATP Contingency List

Jurisdiction	Project Name	Total Cost	Requested / Remaining Balance	Scores
Manteca	Manteca Safe Route to School Priority Safety Project	\$8,140,200	\$ 2,698,115	100.25
Stockton	Downtown Walkability and Bikeability	\$6,997,000	\$6,997,000	112.75
Stockton	Main and Market Complete Streets Project, Phase 2	\$18,264,054	\$4,481,398	112.25
SJ County	Burkett Garden Acres	\$4,389,000	\$3,511,000	98.25
Lathrop	East Lathrop Bicycle and Pedestrian Enhancements	\$24,740,000	\$24,740,000	89.5
SJ County	Thornton Community Improvements	\$4,105,000	\$3,079,000	82.5
Tracy	East Schulte Safety and Multimodal Community Corridor	\$6,648,000	\$5,075,000	76.5
SJ County	Country Club Blvd Active Transportation Improvements	\$2,108,000	\$1,897,000	71.25
SJRRC	North Lathrop Transfer Station Project	\$37,911,000	\$4,566,000	60.5
	Total	\$113,302,254	\$60,858,558	

COMMITTEE ACTIONS:

Technical Advisory Committee:

The Technical Advisory Committee unanimously approved the recommendation.

Management & Finance:

The Management & Finance Committee unanimously approved the recommendation.

Citizens Advisory Committee:

The Management & Finance Committee unanimously approved the recommendation.

Executive Committee:

The Executive Committee unanimously approved the recommendation.

Prepared by Joel Campos, Senior Regional Planner & Jacob Williams, Assistant Regional Planner



*City of Ceres • City of Hughson • City of Modesto • City of Newman • City of Oakdale • City of Patterson
City of Riverbank • City of Turlock • City of Waterford • County of Stanislaus*

DATE: April 21, 2025
TO: California Transportation Commission
FROM: Blake Dunford, Associate Planner
RE: **Final Programming Recommendation for Cycle 7 of the Active Transportation Program (ATP): Stanislaus Regional Program**

The Stanislaus Council of Governments (StanCOG) is proposing the following programming recommendation for projects in the regional program for Cycle 7 of the Active Transportation Program (ATP).

Based on the amended ATP Fund Estimate, included as Attachment 1, StanCOG may program up to \$1,029,000. The StanCOG Policy Board approved this programming recommendation at their regular meeting held on Wednesday, March 19th at 6:00 p.m. via teleconference and in person at 1111 I Street, Suite 308, Modesto, CA 95354. Resolution 24-27 is included as Attachment 2.

StanCOG staff proposes the following projects for funding.

Agency	Title	Total ATP Funding
City of Waterford	Waterford Safe Routes to School Project – Washington Road	\$845,000
City of Waterford	Waterford Safe Routes to School Project – Welch Street	\$184,000
	Total	\$1,029,000

Detailed information on the funding recommendation, including breakdown by phase, fiscal year, and ATP fund source can be found in Attachment 3. Should the recommended projects be unable to accept funding, StanCOG has developed a contingency list of projects, also noted in Attachment 3.

Background

The Stanislaus Regional Active Transportation Program, hereafter referred to as the 2025 Regional ATP, opened for applications on July 1, 2024, and closed on November 15, 2024. StanCOG staff received four applications for the 2025 Regional ATP.

Agency	Title	Project Category
City of Ceres	Morrow Village Pedestrian Safety Improvements	Medium
City of Waterford	Waterford Safe Routes to School Project – Washington Road	Small
City of Waterford	Waterford Safe Routes to School Project – Welch Street	Small
City of Waterford	City of Waterford Active Transportation Plan	Plan

In line with guidance, projects were scored by a scoring committee. Members of StanCOG 2025 Regional ATP Scoring Committee include:

- Eric Soito, City of Ceres¹
- Sandeep Sandhu, City of Modesto
- Jeff Gravel, City of Oakdale²
- Kathleen Cleek, City of Riverbank
- Bryce Bondi, Stanislaus County
- Dero In, StanRTA
- Blake Dunford, StanCOG

Members met to score the four proposed projects on Thursday, February 13th and Tuesday, February 18th. Scoring methodology is outlined in the adopted StanCOG 2025 Regional ATP Guidelines, posted publicly on the StanCOG website, on the [Transportation Funding webpage](#). A copy of these guidelines is included in Attachment 4. Project ranking with average scores are as follows:

Agency	Title	Average Score
City of Waterford	Waterford Safe Routes to School Project – Washington Road	85.0
City of Waterford	Waterford Safe Routes to School Project – Welch Street	82.8
City of Waterford	City of Waterford Active Transportation Plan	82.0
City of Ceres	Morrow Village Pedestrian Safety Improvements	74.2

Individual scores for each evaluator are provided in Attachment 5.

¹ Eric Soito recused himself during the scoring of the City of Ceres' project and was not present in the virtual room during discussions.

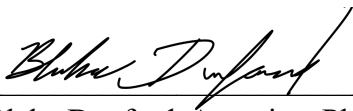
² Due to extraordinary circumstances, Jeff Gravel unfortunately had to step down from the scoring committee shortly before the first meeting. Due to time constraints, a replacement member was unavailable.

The City of Waterford is committed to fulfilling the remaining funding need for the Welch Street project. Updated project information, including Exhibit 25-I (Project Programming Request) and Part A4, have been attached.

Due to StanCOG's adopted guidelines, all eligible regional projects must apply to the state program first. Therefore, no additional applications outside of the state program have been considered.

If you have any questions on this recommendation or the 2025 Regional ATP, please contact Blake Dunford, Associate Planner, at 209-525-4894 or bdunford@stancog.org.

Sincerely,



Blake Dunford, Associate Planner
Stanislaus Council of Governments

Attachments

1. CTC Amended ATP Fund Estimate (PDF)
2. StanCOG Resolution 24-27 (PDF)
3. Regional ATP Programming Spreadsheet (Excel)
4. StanCOG Revised 2025 Regional Active Transportation Program Guidelines (PDF)
5. StanCOG 2025 Regional Active Transportation Program Combined Score Card (Excel)
6. Waterford – Safe Routes to School Washington Road: Updated 25-I and Part A4 (PDF)
7. Waterford – Safe Routes to School Welch Street: Updated 25-I and Part A4 (PDF)

**STANISLAUS COUNCIL OF GOVERNMENTS
RESOLUTION 24-27
APPROVING THE CYCLE 7 REGIONAL ACTIVE TRANSPORTATION PROGRAM
(ATP) PROJECTS**

WHEREAS, the Stanislaus Council of Governments (StanCOG) is a Regional Transportation Planning Agency and a Metropolitan Planning Organization, pursuant to State and Federal designation; and

WHEREAS, the California State Legislature passed and the Governor signed into law Senate Bill 99 (Chapter 359, Statutes 2013) and Assembly Bill 101 (Chapter 354, Statutes 2013), establishing the Active Transportation Program (ATP); and

WHEREAS, StanCOG as the RTPA and MPO for Stanislaus County, is eligible to receive an urban region distribution of ATP funding from the California Transportation Commission (CTC); and

WHEREAS the Stanislaus urban region distribution of ATP funding, also known as the "MPO Component," for Cycle 7 of funding totals \$1,029,000; and

WHEREAS, StanCOG adopted, pursuant to Streets and Highways Code Section 2381(a)(1), ATP Project Selection Guidelines consistent with guidelines adopted by the CTC pursuant to Streets and Highways Code Section 2382(a) on June 19, 2024; and

WHEREAS, the ATP Project Selection Guidelines allow projects submitted by member agencies requesting MPO Component ATP funding to be evaluated by StanCOG in a transparent manner, consistent with guidelines adopted by the CTC pursuant to Streets and Highways Code Section 2382(a); and

WHEREAS, StanCOG's ATP Project Selection Guidelines were submitted to the CTC and approved by the CTC at its June 2024 meeting; and

WHEREAS, a project scoring committee formed by StanCOG evaluated and recommended projects according to the ATP Project Selection Guidelines for inclusion in the StanCOG Active Transportation Program of Projects.

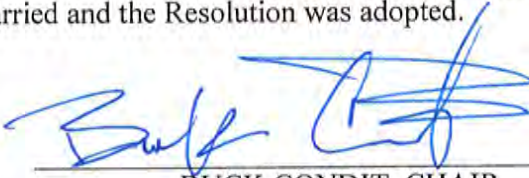
NOW, THEREFORE, BE IT RESOLVED that the Stanislaus Council of Governments hereby approves and adopts the StanCOG MPO Component Project Recommendations for the Cycle 7 Active Transportation Program, attached hereto as Attachment A.

BE IT FURTHER RESOLVED, the StanCOG Executive Director shall forward a copy of this resolution and information pertaining to recommended projects to the CTC, Caltrans, and to such other agencies as may be appropriate, in order for the CTC to approve the recommendation.

BE IT FURTHER RESOLVED that the Executive Director is authorized to make administrative changes as needed, to ensure that the program is implemented in the most efficient and cost-effective manner possible.

THE FOREGOING RESOLUTION was introduced at a regular meeting of the Stanislaus Council of Governments, on the 19th day of March 2025. A motion was made and seconded to adopt the foregoing Resolution. Motion carried and the Resolution was adopted.

MEETING DATE: **March 19, 2025**



BUCK CONDIT, CHAIR

ATTEST:



ROSA DE LEON PARK, EXECUTIVE DIRECTOR

Attachment:

- A. Regional Active Transportation Program Cycle 7 Project List and Score Card

February 21, 2025

Mrs. Tanisha Taylor
Executive Director
California Transportation Commission
1120 N Street Room 2221 (MS-52)
Sacramento, CA 95814

Attention: Laurie Waters

Subject: Santa Barbara County Association of Governments Regional Active Transportation Program of Projects – Cycle 7

Ms. Waters:

The California Transportation Commission approved the Santa Barbara Association of Governments (SBCAG) Cycle 7 Regional Active Transportation Program (ATP) Guidelines on October 2024. The SBCAG's ATP call for projects was released on October 24, 2024, and closed on November 22, 2024. The multidisciplinary advisory group (MAG) evaluated nine projects, four of which are being recommended for funding for a total of \$2.796 million. The SBCAG Board approved the recommended project list for the regional competitive ATP on February 20, 2025.

The projects recommended for funding will provide a broad spectrum of projects to benefit pedestrians and bicyclists, including students walking and bicycling to and from school. The projects not recommended for funding will be placed on a contingency project list. In the event of project delivery failure and/or cost savings from the recommended projects, projects on the contingency list could be offered ATP funding starting with the highest scoring nonfunded project. The contingency list will remain active until the beginning of the next ATP cycle. SBCAG's MPO component funding recommendations meet the statewide ATP guideline requiring that at least 25% of a region's funds benefit disadvantaged communities.

Should you have any questions, please feel free to contact Jaquelin Mata by email at jmata@sbcag.org.

Sincerely,



Marjie Kirn, Executive Director

MEMBER AGENCIES:

Buellton • Carpinteria • Goleta • Guadalupe • Lompoc • Santa Barbara
Santa Maria • Solvang • Santa Barbara County

cc: Sarkes Khachek, skhachek@sbacag.org
Anja Aulenbacher, Anja.Aulenbacher@catc.ca.gov
Jaeden Gales, Jaeden.Gales@catc.ca.gov
Emma Mallonee, Emma.Mallonee@dot.ca.gov

Attachments:

Attachment 1: List of Multi-Disciplinary Advisory Group Members
Attachment 2: Description of Unbiased Project Selection Methodology
Attachment 3: SBACAG Board Resolution Approving MPO Component Funding Recommendations
Attachment 4: Programming Spreadsheet
Attachment 5: List of All Projects, Scores and Contingency List
Attachment 6: Project Programming Request Forms
Attachment 7: Part A4: Project Details Forms

A RESOLUTION OF THE SANTA BARBARA
COUNTY ASSOCIATION OF GOVERNMENTS

ADOPTION OF THE 2025 SANTA BARBARA)
COUNTY ASSOCIATION OF GOVERNMENTS) RESOLUTION NO. 25-08
ACTIVE TRANSPORTATION PROGRAM)
METROPOLITAN PLANNING ORGANIZATION)
COMPONENT FUNDING RECOMMENDATIONS)

WHEREAS, Santa Barbara County Association of Governments (SBCAG) is a Metropolitan Planning Organization (MPO) with an urbanized population greater than 200,000; and

WHEREAS, Streets and Highways Code section 2381 requires that forty percent of the funds appropriated for the Active Transportation Program (ATP) be distributed to metropolitan planning organizations in urban areas with populations greater than 200,000, in proportion to their relative share of population; and

WHEREAS, the California Transportation Commission (CTC) is responsible for administering the ATP and has established ATP Guidelines; and

WHEREAS, on May 16, 2024, as required by the ATP Guidelines SBCAG established SBCAG's 2025 Large MPO ATP Guidelines for Cycle 7 ("Cycle 7 Guidelines") and submitted these guidelines to the CTC for consideration and approval; and

WHEREAS, on September 19, 2024, SBCAG amended its Cycle 7 Guidelines to authorize agreements between SBCAG and the Kern Council of Governments and the Tahoe Regional Planning Agency for SBCAG to receive additional funds from these two agencies for Cycle 7; and

WHEREAS, in October 2024, the CTC approved the Cycle 7 Guidelines as amended; and

WHEREAS, SBCAG's portion of ATP funds allocated by the CTC to be awarded through the 2025 ATP MPO component is \$2,796,000, inclusive of funds from Kern Council of Governments (\$692,000) and from the Tahoe Regional Planning Agency (\$270,000), for programming years 2025/26, 2026/27, 2027/28, and 2028/29; and

WHEREAS, on November 26, 2024, in accordance with the Cycle 7 Guidelines the ATP Multidisciplinary Advisory Group began scoring and ranking the projects based on criteria adopted by the SBCAG Board on October 24, 2024; and

WHEREAS, the four projects recommended for MPO component funding are the West Highway 246 Ped/Bike Safety Improvements Project (\$440,000 in ATP Funds); Railroad Avenue/Depot Street Corridor Enhancement Project (\$1,021,000 in ATP Funds), Funk Zone Intersection Safety Enhancements Project (\$1,300,000 in ATP Funds); and Increasing Secure, Convenient, and Accessible Bicycle Parking in Santa Barbara (\$35,000 in ATP Funds); and

NOW, THEREFORE BE IT RESOLVED, that the Santa Barbara County Association of Governments hereby adopts the SBCAG 2025 ATP MPO Component Recommended Funding and Contingency Project List, as shown on Attachments A and B.

PASSED AND ADOPTED this 20th day of February 2025 by the following vote:

AYES: Lee, Capps, Nelson, Lavagnino, Mosby, Brown, Julian, Silva, Patino, Perotte, Clark and Chair Rowse

NOES:


ABSENT: Hartmann

ABSTAIN:

ATTEST:



Marjie Kirn
Executive Director



Randy Rowse, Chair
Santa Barbara County
Association of Governments
Date: 2.20.25

APPROVED AS TO FORM:

Rachel Van Mullem
County Counsel

By: 

Senior Deputy County Counsel

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5f., Information Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **INFORMATIONAL REPORTS – DELEGATED ALLOCATIONS**
EMERGENCY G-21-11, SHOPP G-03-10 SAFETY, AND MINOR G-19-05a

SUMMARY:

Since the period reported at the last California Transportation Commission (Commission) meeting, the California Department of Transportation (Department) allocated or sub-allocated:

- 2.5f.(1) - \$97,426,000 for construction and \$22,203,000 for construction engineering for 28 emergency construction projects, pursuant to the authority granted under Resolution G-21-11.
- 2.5f.(3) - \$20,378,000 for construction and \$6,823,000 for construction engineering for five safety projects, pursuant to the authority granted under Resolution G-03-10.
- 2.5f.(4) - \$0 for zero State Highway Operation and Protection Program (SHOPP) Minor A projects, pursuant to the authority granted under Resolution G-19-05a.

As of June 1, 2025, the Department has allocated or sub-allocated the following for construction in the Fiscal Year 2024-25:

- 2.5f.(1) - \$610,517,000 for 173 emergency construction projects.
- 2.5f.(3) - \$248,036,000 for 27 safety delegated projects.
- 2.5f.(4) - \$17,199,000 for 17 SHOPP Minor A projects.

BACKGROUND:

The Commission, by Resolution G-11, as amended by Resolution G-21-11, delegated to the Department authority to allocate funds to correct certain situations caused by floods, slides, earthquakes, material failures, slip outs, unusual accidents or other similar events.

This authority is operative whenever such an event:

1. Places people or property in jeopardy.
2. Causes or threatens to cause closure of transportation access necessary for:
 - a. Emergency assistance efforts.
 - b. The effective functioning of an area's services, commerce, manufacture, or agriculture.
 - c. Persons in the area to reach their homes or employment.
3. Causes either an excessive increase in transportation congestion or delay, or an excessive increase in the necessary distances traveled.

Resolution G-11 authorizes the Department to allocate funds for follow-up restoration projects associated with, and that immediately follow an emergency condition response project. Resolution G-11 also requires the Department to notify the Commission, at their next meeting, whenever such an emergency allocation has been made.

On March 30, 1994, the Commission delegated to the Department authority to allocate funds under Resolution G-11, as amended by Resolution G-21-11. This authority allows the Department to begin work without waiting for the next Commission meeting to receive an allocation.

On March 28, 2001, the Commission approved Resolution G-01-10, as amended by resolution G-03-10, delegating to the Department authority to allocate funds for SHOPP safety projects. This authority allows the Department to begin work without waiting for the next Commission meeting to receive an allocation.

Resolution G-19-05a authorizes the Department to sub-allocate funds for Minor projects. At the June 2024 meeting, the funding and project listing for the FY 2024-25 Lump Sum Minor Construction Program was approved by the Commission under Resolution FM-23-06.

The SHOPP, as approved by the Commission, is a four-year program of projects with the total annual proposed expenditures limited to the biennial Commission-approved Fund Estimate. The Commission, subject to monthly reporting and briefings, has delegated to the Department the authority to allocate funds for safety projects and emergency projects. The Department uses prudent business practices to manage the combination of individual project cost increases and savings to meet Commission policies.

In all cases, the delegated authority allows the Department to begin work without waiting for the next Commission meeting to receive an allocation.

The Department has complied with the National Environmental Policy Act and the California Environmental Quality Act requirements in preparing these projects.

Attachments

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
1 \$10,625,000 Del Norte 01-DN-101 12.0/16.5	Near Crescent City, from 0.5 mile south to 4.0 miles north of Wilson Creek Road. On March 14, 2025, a landslide was reported in the area. The landslide, resulting from heavy winter rains, damaged a retaining walls and drainage. The roadway prism has undergone localized variability in settlement due to widespread earth movement causing roadway cracks, undulations, and bumps, thereby affecting the drivability. This project will repair retaining walls, drainage systems and roadway.	01-4957 SHOPP/24-25 CON ENG \$2,125,000 CONST \$8,500,000 0125000013 3,4 0P430	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$2,125,000 \$8,500,000
<u>Performance Measure:</u> 1.0 Location(s)		Emergency		
Construction funds allocated: Initial G-11 Allocation 05/02/2025:		\$10,625,000		
Additional funds allocated: R/W Sup: R/W Cap:		\$25,000 \$25,000		
2 \$6,000,000 Del Norte 01-DN-199 9.1/9.4	Near Gasquet, from 2.0 miles to 1.7 miles east of Hardscrabble Creek Bridge. On February 25, 2025, maintenance crews reported a slipout that resulted in culvert separation and damage to the roadway, embankment, and guardrail. Based on geotechnical investigation, the combination of the steep incline of the embankment, saturated soils, and recent winter storms caused the slope failure and culvert separation. This project will repair the slope with a Mechanically Stabilized Earth (MSE) wall, replace the damaged culvert, and repair the roadway and guardrail.	01-4999 SHOPP/24-25 CON ENG \$1,500,000 CONST \$4,500,000 0125000123 3,4 0Q080	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$1,500,000 \$4,500,000
<u>Performance Measure:</u> 1.0 Location(s)		Emergency		
Construction funds allocated: Initial G-11 Allocation 05/02/2025:		\$6,000,000		
Additional funds allocated: R/W Sup: R/W Cap:		\$25,000 \$25,000		

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
3 \$1,690,000 Humboldt 01-Hum-36 27.9	Near Bridgeville, at 2.6 miles east of Little Larabee Creek Bridge. Heavy rains in mid-February 2025 caused slope erosion on the outlet side of a failed culvert. This resulted in failure of the roadway embankment and damage to the pavement. This project will abandon the existing culvert, install a new culvert, reconstruct the slope, place Rock Slope Protection (RSP), and repair the roadway. <u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/07/2025: \$1,690,000 Additional funds allocated: R/W Sup: \$25,000 R/W Cap: \$25,000	01-4993 SHOPP/24-25 CON ENG \$390,000 CONST \$1,300,000 0125000108 3,4 0Q030 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$390,000 \$1,300,000
4 \$3,300,000 Humboldt 01-Hum-101 R42.3	Near Shively, at 1.0 mile south of Barkdull Road Undercrossing. On March 10, 2025, maintenance forces reported a hole in the paved shoulder along the northbound lane. Upon investigation, it was determined that the culvert at this location had separated. This separation, in combination with heavy rains, caused a large void between the culvert and pavement that extended into the traveled way. This project will replace the existing culvert and repair the roadway. <u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/14/2025: \$3,300,000 Additional funds allocated: R/W Sup: \$25,000 R/W Cap: \$25,000	01-3233 SHOPP/24-25 CON ENG \$800,000 CONST \$2,500,000 0125000130 3,4 0Q130 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$800,000 \$2,500,000

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
5 \$2,000,000 Humboldt 01-Hum-299 R21.0/R22.0	Near Blue Lake, from 1.4 miles to 0.4 miles west of Chezem Road. In February 2025, heavy winter storms caused a 300-foot section of the roadway to sink. The sink spans the full width of the roadway in both eastbound and westbound lanes and has resulted in damage to the roadway prism, pavement, guardrail, and adjacent drainage systems. This project will repair the roadway, replace guardrail, and reconstruct drainage systems. <u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 03/27/2025: \$2,000,000 Additional funds allocated: R/W Sup: \$25,000 R/W Cap: \$25,000	01-4981 SHOPP/24-25 CON ENG \$500,000 CONST \$1,500,000 0125000090 3,4 0P910 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$500,000 \$1,500,000
6 \$2,600,000 Mendocino 01-Men-1 41.8/42.5	Near Whitesboro, from 0.6 mile south to 0.1 mile north of Navarro Ridge Road. On January 22, 2025, following winter rain, construction crews reported that a recently modified slope had failed at several locations. On March 5, 2025, a geotechnical investigation was performed, and it was determined that four sections of the slope require repair. This project will repair and protect the slope sections using a combination of grading and compaction, excavation and backfilling with soil-filled Rock Slope Protection (RSP), installation of subsurface drainage, and replacement of erosion control. <u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 05/02/2025: \$2,600,000 Additional funds allocated: R/W Sup: \$25,000 R/W Cap: \$25,000	01-3231 SHOPP/24-25 CON ENG \$600,000 CONST \$2,000,000 0125000125 3,4 0Q100 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$600,000 \$2,000,000

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
7 \$13,200,000 Mendocino 01-Men-1 0.6	Near Gualala, at Old Stage Road. On February 5, 2025, maintenance forces reported a sinkhole at the intersection of Route 1 and Old Stage Road. The damage was reportedly caused by culvert separation and heavy winter rains. This project will abandon the existing culvert, perform geotechnical studies, install a larger culvert suitable for fish passage requirements, and repair the roadway. <u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/25/2025: \$13,200,000 Additional funds allocated: R/W Sup: \$25,000 R/W Cap: \$25,000	01-4987 SHOPP/24-25 CON ENG \$3,300,000 CONST \$9,900,000 0125000096 3,4 0P960 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$3,300,000 \$9,900,000
8 \$2,000,000 Mendocino 01-Men-101 R86.00/R87.50	Near Cummings, from 1.7 miles south to 0.2 mile south of Route 271; also on Route 271, from 0.2 mile north to 1.2 miles north of Hermitage Road (PM 1.5/2.5). Multiple heavy rainfall events in the winter months of 2024 impacted the region with saturated soils. On March 10, 2024, a large landslide began moving rapidly, resulting in closure of two southbound lanes on Route 101 and full closure of Route 271. Damage at this location has compromised surface drainage, roadway stability, and culverts. This project will stabilize and reconstruct the roadway, install subsurface drainage systems, restore culverts, and place erosion control. Winter storms and heavy rains during February 2025 caused rockfall onto the roadway within the project limits. This resulted in full closure of Route 101 and traffic was detoured on to Route 271. This supplemental is necessary for debris removal, rock fall mitigation, and installation of temporary rock fall catchment fence. <u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/23/2024: \$2,650,000 Supplemental G-11 Allocation 04/07/2025: \$2,000,000 Revised Allocation: \$4,650,000	01-4908 SHOPP/23-24 CON ENG \$0 CONST \$2,000,000 0124000164 3,4 0P090 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$0 \$2,000,000

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
9 \$2,600,000 Mendocino 01-Men-101 97.4	Near Riverdale, at 0.1 mile south of Bridges Creek Bridge. On February 6, 2025, maintenance forces reported a slope and drainage system failure at this location. The damage was the result of heavy rains that overwhelmed the overside drain and caused the slope erosion. This project will reconstruct the slope, place Rock Slope Protection (RSP), and repair the overside drain and roadway. <u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/08/2025: \$2,600,000 Additional funds allocated: R/W Sup: \$25,000 R/W Cap: \$25,000	01-4990 SHOPP/24-25 CON ENG \$600,000 CONST \$2,000,000 0125000102 3,4 0P990 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$600,000 \$2,000,000
10 \$2,000,000 Mendocino 01-Men-128 R28.1	Near Boonville, at 0.1 mile south of Schoenahl Road. On February 18, 2025, maintenance forces reported significant erosion along the Anderson Creek embankment adjacent to the Boonville Maintenance Station. The erosion was caused by increased flow in the creek from heavy winter storms and has resulted in approximately 100 feet of property loss along the maintenance yard perimeter and damage to the perimeter fencing. This project will repair the embankment between the creek and the maintenance yard, reconstruct loss of yard with imported borrow, place Rock Slope Protection (RSP), and repair the perimeter fencing. <u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/11/2025: \$2,000,000 Additional funds allocated: R/W Sup: \$25,000 R/W Cap: \$25,000	01-4997 SHOPP/24-25 CON ENG \$500,000 CONST \$1,500,000 0125000121 3,4 0Q060 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$500,000 \$1,500,000

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
11 \$12,000,000 Mendocino 01-Men-162 16.1/16.2	Near Dos Rios, at 0.8 mile to 0.9 mile east of Laytonville Dos Rios Road. On February 5, 2025, heavy winter rains caused a landslide to occur between two existing retaining walls. The landslide has caused the roadway to crack with a 6-inch horizontal offset, resulting in damage to the existing guardrail, subsurface drainage, and adjacent retaining walls. This project will construct a new soldier pile wall to connect the two existing walls and stabilize the roadway prism, as well as repair the ends of the existing walls, subsurface drainage systems, and roadway.	01-4989 SHOPP/24-25 CON ENG \$2,000,000 CONST \$10,000,000 0125000097 3,4 0P970 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$2,000,000 \$10,000,000
<u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/25/2025: \$12,000,000 Additional funds allocated: R/W Sup: \$25,000 R/W Cap: \$25,000				
12 \$3,570,000 Siskiyou 02-Sis-96 46.6	Near Happy Camp, 2.3 miles east of Gordons Ferry Road. On February 25, 2025, maintenance forces reported multiple cracks in the pavement along a 225-foot section of the roadway. At the time of reporting, the cracks were on average 3 inches wide, with depths of up to 3.5 feet, and showed signs of continuous widening. It was also observed that runoff was percolating into the shoulder and contributing to the pavement failure, despite an existing underdrain system. This project will remove the existing underdrain, install a deeper underdrain, and repair the slope and roadway.	02-3940 SHOPP/24-25 CON ENG \$600,000 CONST \$2,970,000 0225000120 3,4 2K620 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$600,000 \$2,970,000
<u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/17/2025: \$3,570,000				

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
13 \$3,000,000	Near Pulga, from 1.0 mile north of Deadwood Road to 0.3 mile north of Mill Creek Bridge. On February 18, 2025, following heavy winter rain, maintenance forces reported multiple slipout locations within the project limits. Upon further investigation, it was determined that multiple drainage systems in the area were also damaged and in need of repair. This project will replace the damaged culverts, repair the slopes, and place Rock Slope Protection (RSP).	03-2312 SHOPP/24-25 CON ENG \$500,000 CONST \$2,500,000 0325000202 3,4 2N010	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$500,000 \$2,500,000
Butte 03-But-70 36.42/42.35				
<u>Performance Measure:</u> 8.0 Location(s) Construction funds allocated: Initial G-11 Allocation 05/02/2025: \$3,000,000				
14 \$1,340,000	Near Woodville, from 1.3 miles to 1.7 miles north of Horseshoe Hill Road. On January 14, 2025, a roadway slipout had been reported. On January 22, 2025, an investigation found excessive rain from early January 2025 resulted in surface water and over saturated soils that caused a disconnected cross culvert, slope erosion, roadway settlement, and two roadway slipouts. This project will reconstruct the roadway slope embankment and backfill with engineering fill, replace the damaged sections of 2 cross culverts, construct a drainage inlet, repair roadway slipouts and install gabion baskets to stabilize the roadway.	04-2929P SHOPP/24-25 CON ENG \$350,000 CONST \$990,000 0425000224 3,4 3X260	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$350,000 \$990,000
Marin 04-Mrn-1 18.91/19.29				
<u>Performance Measure:</u> 3.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/07/2025: \$1,340,000				

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
15 \$920,000	Near Marin City, at 0.5 mile north of Rodeo Avenue. A heavy storm beginning February 13, 2025 saturated soils, resulting in a landslide the next day, sending mud and debris down the slope, covering the two of four southbound lanes. This project will remove debris from catchment area at the toe of slope and roadway, remove loose rocks and damaged trees from the slope, and clean sediment and debris from the drainage systems.	04-2929S SHOPP/24-25 CON ENG \$200,000 CONST \$720,000 0425000237 3,4 3X360	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$200,000 \$720,000
Marin 04-Mrn-101 3.0				
<u>Performance Measure:</u>				
1.0 Location(s)				
Emergency				
Construction funds allocated:				
Initial G-11 Allocation 04/03/2025: \$920,000				
Additional funds allocated:				
R/W Sup: \$15,000				
R/W Cap: \$30,000				
16 \$870,000	Near Calistoga, at 6.0 miles north of Tubbs Lane. In February 2024, heavy rains caused an embankment slipout at the southbound cut slope, resulting in mud clogging the drainage ditch, and sending mud and debris down the slope, covering the roadway with mud and debris. This project will remove mud and debris, reconstruct slope embankments, regrade an unlined ditch, and place Rock Slope Protection (RSP) to stabilize slope embankments.	04-2930A SHOPP/24-25 CON ENG \$210,000 CONST \$660,000 0425000263 3,4 3X490	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$210,000 \$660,000
Napa 04-Nap-29 45.51				
<u>Performance Measure:</u>				
1.0 Location(s)				
Emergency				
Construction funds allocated:				
Initial G-11 Allocation 04/07/2025: \$870,000				
Additional funds allocated:				
R/W Cap: \$40,000				

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
17 \$5,350,000 Napa 04-Nap-121 19.5	Near Moskowite Corner, at 2.6 miles south of Route 128. Early February 2025 storms brought excessive rainfall that caused water seepage into pavement cracks and oversaturated soils, resulting in pavement settlement, additional pavement cracks, and pavement movement. Additionally, a landslide occurred along the southbound cut slope. This project will clear and remove debris, design and construct a retaining wall, repair pavement by using a combination of sheet piles to avoid soil creep and lightweight Expanding Polyurethane Material (EPM) injections to fill voids, and place Rock Slope Protection (RSP) to stabilize slope embankment.	04-2929M SHOPP/24-25 CON ENG \$1,750,000 CONST \$3,600,000 0425000226 3,4 3X280 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$1,750,000 \$3,600,000
<u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/03/2025: \$5,350,000 Additional funds allocated: R/W Sup: \$40,000 R/W Cap: \$80,000				
18 \$1,623,000 Santa Clara 04-SCI-237 R9.14R	In Milpitas, at eastbound McCarthy Boulevard onramp. On February 24, 2025, a field investigation found tension cracks or separation between the lip of curb and gutter and pavement, measuring 123 feet and 193 feet. The damages were likely caused by storm events filling water into these tension cracks and gradually increasing the depth and width of the cracks while also saturating the adjacent slope embankment. This project will repair pavement by injecting Lightweight Expanded Polyurethane Material (LEPM) to fill voids, and reconstruct the slope embankment.	04-2930C SHOPP/24-25 CON ENG \$421,000 CONST \$1,202,000 0425000270 3,4 3X520 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$421,000 \$1,202,000
<u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/24/2025: \$1,623,000				

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
19 \$3,345,000 Solano 04-Sol-80 6.7	Near Vallejo, at 1.1 mile east of Route 37/80 Separation. Heavy winter storms from early February 2025 brought heavy rains that caused saturated soils and unstable slopes, resulting in a landslide onto the Solano Bike Trail (covering approximately 26,000 square feet), cracked trail pavement, and a full trail closure to public use. This project will haul away and dispose of slide material, reconstruct the slope embankment using engineered buttress fill, and repair drainage systems, pavement, and asphalt dike.	04-2929V SHOPP/24-25 CON ENG \$700,000 CONST \$2,645,000 0425000264 3,4 3X500	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$700,000 \$2,645,000
<u>Performance Measure:</u> 1.0 Location(s)				
Construction funds allocated: Initial G-11 Allocation 05/09/2025: \$3,345,000				
20 \$3,150,000 Sonoma 04-Son-1 38.58	Near Timber Cove, at 1.8 miles north of Pacific View Drive. Heavy storms from late January through mid February 2024 caused saturated slopes, which resulted in washouts, landslides, longitudinal pavement cracking, and pavement settlement. This project will remove the landslide debris, repair slopes, install concrete canvas for slope protection, reset damaged guardrail, reconstruct pavement using sub grade enhancement fabric, and construct new retaining wall using concrete lagging. This supplemental is needed to fund a culvert repair involving greater underground depth and the addition of one work location having a damaged drainage system needing repair.	04-2926T SHOPP/23-24 CON ENG \$150,000 CONST \$3,000,000 0424000308 3,4 1X320	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$150,000 \$3,000,000
<u>Performance Measure:</u> 1.0 Location(s)				
Construction funds allocated: Initial G-11 Allocation 03/21/2024: \$7,460,000 Supplemental G-11 Allocation 05/07/2025: \$3,150,000 Revised Allocation: \$10,610,000				

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
21 \$765,000	Near Noel Heights, at 0.6 mile west of Green Valley Road. Heavy rains from March 13 - 17, 2025 caused over saturated soils and high water creek flows that resulted in slope failure from the roadside above and an undermined toe of slope from a lodged tree stump below near the creek. This project will remove loose soil material, and tree stump, construct a retaining wall, backfill using engineering imported fill, and install cable railing.	04-2930G SHOPP/24-25 CON ENG \$265,000 CONST \$500,000 0425000286 3,4 3X570	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$265,000 \$500,000
<u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/30/2025: \$765,000 Additional funds allocated: R/W Cap: \$30,000				
22 \$13,200,000	Near Malibu, at 0.4 miles east of Las Flores Canyon Road. The Palisades Fire began on January 7, 2025 and quickly spread, burning over 23,700 acres. On January 13, 2025, maintenance and engineering forces investigated the impact the fire had on the Las Flores Maintenance Station and determined it had caused extensive structural and equipment damage. This project will remove and replace the damaged equipment building, fuel island and tank, storage sheds, mechanical, electrical, and wastewater components, and construct a new retaining wall.	07-6301 SHOPP/24-25 CON ENG \$825,000 CONST \$12,375,000 0725000183 3,4 2XR10	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$825,000 \$12,375,000
<u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/15/2025: \$13,200,000 Additional funds allocated: PS&E: \$1,400,000 R/W Sup: \$100,000 R/W Cap: \$100,000				

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocations				
23 \$9,500,000 Los Angeles 07-LA-1 R39.89/40.04	In the city of Los Angeles (Pacific Palisades), from 0.3 mile north of Porto Marina Way to Getty Villa Drive. On January 7, 2025, the Palisades Fire began and spread quickly through the region. The wildfire burned over 23,700 acres and was 100% contained as of February 11, 2025. The Governor issued a State of Emergency Proclamation on January 7, 2025 and the President issued a State of Emergency Declaration on January 8, 2025. The wildfire destroyed state assets and residences and forced evacuations. The wildfire damaged and burned the slopes and the timber lagging retaining wall. This project will reconstruct the retaining wall and the associated drainage systems.	07-6369 SHOPP/24-25 CON ENG \$1,300,000 CONST \$8,200,000 0725000181 3,4 2XS10 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$1,300,000 \$8,200,000
<u>Performance Measure:</u> 1.0 Location(s)				
Construction funds allocated:				
Initial G-11 Allocation 05/05/2025:		\$9,500,000		
Additional funds allocated:				
PS&E:		\$300,000		
R/W Sup:		\$100,000		
R/W Cap:		\$100,000		

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocation				
24 \$235,000 Los Angeles 07-LA-27 3.54/3.60	Near Wildwood, from 0.3 mile south to 0.2 mile south of Fernwood Pacific Drive. On September 30, 2019, a vendor contractor employed by Southern California Edison (SCE) punctured a culvert during installation of a power pole. The following winter storms of 2019 resulted in heavy runoff and erosion of the slope due to the damaged culvert. Multiple attempts by SCE to repair the drainage system failed and debris continued to wash down the side slope onto private property. The private property owner filed a lawsuit against Caltrans and SCE. A settlement was reached on November 5, 2024 in favor of Caltrans. In accordance with the settlement, this project will remove the plastic sheeting on the slope, fill the slope voids, repair the slope with concrete, replace the damaged section of the culvert, line the remainder of the culvert, and repair the curb. This supplemental project will complete the remaining and additional scope identified by Caltrans Legal, and close out the project.	07-6101 SHOPP/24-25 CON ENG \$75,000 CONST \$160,000 0725000103 3,4 2XJ90 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$75,000 \$160,000
<u>Performance Measure:</u> 1.0 Location(s) Construction funds allocated: Initial G-11 Allocation 01/22/2025: \$550,000 Supplemental G-11 Allocation 04/15/2025: \$235,000 Revised Allocation: \$785,000 Additional supplemental funds allocated: R/W Sup: \$25,000 R/W Cap: \$25,000				
25 \$1,000,000 Los Angeles 07-LA-Var Var	In Los Angeles County, on various routes at various locations. On January 7, 2025, high winds started in the area and contributed to fires and also resulted in fallen trees. The winds caused some trees to lose their stability, resulting in either falling or severely leaning. This poses a potential hazard to the traveling public and state and private property. This project will remove damaged and fallen trees at various locations.	07-6298 SHOPP/24-25 CON ENG \$100,000 CONST \$900,000 0725000182 3,4 2XQ60 Emergency	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$100,000 \$900,000
<u>Performance Measure:</u> 22.0 Location(s) Construction funds allocated: Initial G-11 Allocation 04/30/2025: \$1,000,000				

2.5 Highway Financial Matters

Project No. Amount County Dist-Co-Rte Postmile	Location Project Description Allocation History	PPNO Program/Year Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(1) Informational Report - Emergency G-11 Allocation				
26 \$201,000	Near Topaz Lake, from 1.5 to 2.5 miles north of Route 89. On June 24, 2024, field maintenance reported fallen rocks onto the highway from above slopes equipped with a rockfall net. Field maintenance crews have history of cleaning developed rock pockets to release buildup from behind the netting. A 2022 geotechnical report identified rock pockets and small tears in the mesh net but no fallen rocks onto the highway at that time. This project will replace the failed rockfall net. This second supplemental is needed for final payment and to close out the project.	09-5071 SHOPP/24-25 CON ENG \$47,000 CONST \$154,000 0924000077 3,4 40470	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$47,000 \$154,000
Mono 09-Mno-395 118.5/119.5		Emergency		
<u>Performance Measure:</u> 2.0 Location(s)				
Construction funds allocated: Initial G-11 Allocation 09/06/2024: \$2,001,000 Supplemental G-11 Allocation 05/06/2025: \$201,000 Revised Allocation: \$2,202,000				
27 \$11,400,000	Near Pine Valley, at 0.1 mile west of Pine Valley Creek Bridge. On February 13, 2025, maintenance forces responded to an emergency call during a storm event to make temporary pavement repairs on the eastbound Pine Valley Bridge. On February 22, 2025, engineering forces investigated the site and recommended repairs for both the left and right bridges. This project will replace the polyester concrete overlays and joint seals for both bridges.	11-1616 SHOPP/24-25 CON ENG \$1,900,000 CONST \$9,500,000 1125000211 3,4 43282	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$1,900,000 \$9,500,000
San Diego 11-SD-8 R41.8		Emergency		
<u>Performance Measure:</u> 2.0 Location(s)				
Construction funds allocated: Initial G-11 Allocation 04/30/2025: \$11,400,000				
28 \$2,145,000	In Huntington Beach, from 0.1 mile south to 0.2 mile north of Ellis Avenue. On February 11, 2025, maintenance forces discovered a sinkhole at the project location. It was determined that the sinkhole was caused by a culvert that had failed during the recent storm event. This project will abandon the existing culvert, install a new culvert, and repair the adjacent sidewalk.	12-3116 SHOPP/24-25 CON ENG \$495,000 CONST \$1,650,000 1225000093 3,4 0V840	505-3290 RMRA 20.10.201.130 2017-18 802-3290 RMRA 20.20.201.130	\$495,000 \$1,650,000
Orange 12-Ora-39 3.0/3.25		Emergency		
<u>Performance Measure:</u> 1.0 Location(s)				
Construction funds allocated: Initial G-11 Allocation 04/30/2025: \$2,145,000				

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(3) Informational Report - SHOPP Safety Resolution G-03-10 Delegated Allocations				
1 \$6,999,000 Del Norte 01-DN-101 15.6/16.2	Near Crescent City, from 3.0 miles north to 3.6 miles north of Wilson Creek Bridge. Improve cross slope and drainage, widen traveled lane and shoulders, and install guardrail to improve safety. This project will reduce the number and severity of collisions. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	01-1144 SHOPP/24-25 CON ENG \$1,711,000 CONST \$9,328,000 0121000034 3,4 0K950	505-3290 RMRA 001-0890 FTF 20.10.201.010 2017-18 802-3290 RMRA 2024-25 302-0890 FTF 20.20.201.010	\$123,000 <u>\$1,103,000</u> \$1,226,000 \$577,000 <u>\$5,196,000</u> \$5,773,000
<u>Performance Measure:</u> Planned: 0.68, Actual: 0.68 Annual fatal and serious injury collision(s) CEQA - ND, 03/29/2024; Re-validation 04/22/2025 NEPA - CE, 03/29/2024; Re-validation 04/22/2025 Future consideration of funding approved under Resolution E-24-31; May 2024. Allocation Date: 05/28/25				
2 \$7,055,000 Humboldt 01-Hum-101 125.20/R126.0 9	Near Orick, from 0.9 mile south to South Prairie Creek Park Undercrossing. Improve curves, roadway cross slope, and drainage, and widen shoulders to improve safety. This project will reduce the number and severity of collisions. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	01-2536 SHOPP/25-26 CON ENG \$2,721,000 CONST \$5,513,000 0120000035 3,4 0K150	505-3290 RMRA 001-0890 FTF 20.10.201.010 2017-18 802-3290 RMRA 2024-25 302-0890 FTF 20.20.201.010	\$277,000 <u>\$2,498,000</u> \$2,775,000 \$428,000 <u>\$3,852,000</u> \$4,280,000
<u>Performance Measure:</u> Planned: 0.56, Actual: 0.56 Annual fatal and serious injury collision(s) CEQA - CE, 12/29/2023; Re-validation 03/21/2025 NEPA - CE, 12/29/2023; Re-validation 03/21/2025 Allocation Date: 05/28/25				

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5f.(3) Informational Report - SHOPP Safety Resolution G-03-10 Delegated Allocations				
3 \$2,300,000 Sacramento 03-Sac-160 R44.9	In the city of Sacramento, at the onramp and offramp to Del Paso Boulevard. Replace signs and panels, upgrade lighting, install wrong-way driving deterrents, and repave ramps to improve safety. This project will reduce the number and severity of collisions. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	03-7295 SHOPP/24-25 CON ENG \$520,000 CONST \$2,070,000 0323000033 3,4 3J900	505-3290 RMRA 001-0890 FTF 20.10.201.010 2017-18 802-3290 RMRA 2024-25 302-0890 FTF 20.20.201.010	\$52,000 <u>\$468,000</u> \$520,000 \$178,000 <u>\$1,602,000</u> <u>\$1,780,000</u>
<u>Performance Measure:</u> Planned: 0.09, Actual: 0.09 Annual fatal and serious injury collision(s) CEQA - CE, 04/30/2024; Re-validation 02/11/2025 NEPA - CE, 04/30/2024; Re-validation 02/11/2025 Allocation Date: 05/09/25				
4 \$3,937,000 Yolo 03-Yol-16 37.315/37.729	Near Woodland, from 0.2 mile west to 0.2 mile east of County Road 95. Construct left-turn lane at the intersection, and install warning signs and beacons to improve safety. This project will reduce the number and severity of collisions. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	03-8672 SHOPP/25-26 CON ENG \$750,000 CONST \$3,590,000 0323000088 3,4 4J120	505-3290 RMRA 001-0890 FTF 20.10.201.010 2017-18 802-3290 RMRA 2024-25 302-0890 FTF 20.20.201.010	\$75,000 <u>\$675,000</u> \$750,000 \$319,000 <u>\$2,868,000</u> <u>\$3,187,000</u>
<u>Performance Measure:</u> Planned: 0.05, Actual: 0.05 Annual fatal and serious injury collision(s) CEQA - CE, 06/24/2024; Re-validation 03/27/2025 NEPA - CE, 06/24/2024; Re-validation 03/27/2025 Allocation Date: 05/13/25				

2.5 Highway Financial Matters

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type												
2.5f.(3)	Informational Report - SHOPP Safety Resolution G-03-10 Delegated Allocations															
5 \$6,910,000 Merced 10-Mer-140 43.5/43.9	Near Planada, from west of Plainsburg Road to Sutter Street. Intersection control improvements by constructing single-lane roundabout with pedestrian crossings and drainage improvements. This project will improve safety and reduce the number and severity of collisions. <u>Preliminary Engineering</u> <table><tr><td></td><td><u>Budget</u></td><td><u>Expended</u></td></tr><tr><td>PA&ED</td><td>\$1,399,000</td><td>\$1,197,677</td></tr><tr><td>PS&E</td><td>\$1,530,000</td><td>\$1,428,749</td></tr><tr><td>R/W Sup</td><td>\$472,000</td><td>\$319,928</td></tr></table>		<u>Budget</u>	<u>Expended</u>	PA&ED	\$1,399,000	\$1,197,677	PS&E	\$1,530,000	\$1,428,749	R/W Sup	\$472,000	\$319,928	10-3506 SHOPP/23-24 CON ENG \$1,450,000 CONST \$4,520,000 1020000188 3,4 1M170	001-0890 FTF 20.10.201.010 2024-25 302-0890 FTF 20.20.201.010	\$1,552,000 \$5,358,000
	<u>Budget</u>	<u>Expended</u>														
PA&ED	\$1,399,000	\$1,197,677														
PS&E	\$1,530,000	\$1,428,749														
R/W Sup	\$472,000	\$319,928														
<u>Performance Measure:</u> Planned: 0.19, Actual: 0.19 Annual fatal and serious injury collision(s) CEQA - CE, 11/21/2022; Re-validation 03/24/2025 NEPA - CE, 11/21/2022; Re-validation 03/24/2025 Eleven month allocation time extension for CONST and CON ENG approved under Waiver 24-125; August 2024. Allocation Date: 05/13/25																

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.2a., Information Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **STATUS OF CONSTRUCTION CONTRACT AWARD FOR STATE
HIGHWAY PROJECTS**

SUMMARY:

The California Department of Transportation (Department) is presenting this information item to provide the status of construction contract award for projects on the State Highway System allocated in Fiscal Years 2023-24 and 2024-25.

In 2023-24, the California Transportation Commission (Commission) allocated \$4.19 billion to construct 466 State-Administered State Transportation Improvement Program (STIP), and State Highway Operation and Protection Program (SHOPP) projects on the State Highway System. As of May 22, 2025, 461 projects totaling \$3.52 billion have been awarded, 3 projects have lapsed, and the Department plans to re-program their Construction phases. The remaining two projects will be awarded by their extended deadlines.

In 2024-25, the Commission allocated \$2.61 billion to construct 161 State-Administered STIP, and SHOPP projects on the State Highway System. As of May 22, 2025, 98 projects totaling \$703,678,000 have been awarded, 3 projects will be awarded by their extended deadlines, 3 projects have concurrent time extensions, and the remaining 57 projects will be awarded by their original deadlines.

BACKGROUND:

In accordance with STIP and SHOPP Guidelines, projects are required to be ready to proceed to construction within six months of allocation.

Attachment

CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

Reference No.: 3.2a.

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FY 2023-24 Allocations

Month Allocated	No. Projects Voted	Voted Projects (x1000) CONST	No. Projects Awarded	No. Projects Funds Lapse	No. Projects Pending Award	No. Projects Awarded within 6 months
August 2023	127	\$1,696,940	125	2	0	85
October 2023	67	\$322,093	67	0	0	57
December 2023	46	\$298,029	46	0	0	40
January 2024	33	\$358,666	33	0	0	26
March 2024	42	\$330,894	40	1	1	39
May 2024	76	\$615,659	75	0	1	75
June 2024	75	\$565,644	75	0	0	65
Total	466	\$4,187,925	461	3	2	387

Note: 1. Awarded amount reflects total project allocation.
2. Total project amount reflects projects with financial contribution only, delegated safety projects and emergency projects.

FY 2024-25 Allocations

Month Allocated	No. Projects Voted	Voted Projects (x1000) CONST	No. Projects Awarded	No. Projects Funds Lapse	No. Projects Pending Award	No. Projects Awarded within 6 months
August 2024	28	\$399,273	27	0	1	21
October 2024	41	\$468,070	40	0	1	36
December 2024	27	\$291,852	20	0	7	20
January 2025	23	\$573,636	7	0	16	7
March 2025	16	\$275,364	3	0	13	3
May 2025	26	\$599,817	1	0	25	1
Total	161	\$2,608,012	98	0	63	88

Note: 1. Awarded amount reflects total project allocation.
2. Total project amount reflects projects with financial contribution only, delegated safety projects and emergency projects.

"Improving lives and communities through transportation."

Voted Not Awarded Project Status
Four-Month & Time Extension Award Project Status

(Note: All projects are SHOPP unless otherwise noted)

Reference No.: 3.2a.

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Attachment

No.	Dist-EA	PPNO	County	Route	Description	Allocation Date	Allocation Amount x1000	Award Deadline	Project Status
1	04-0Q190	2026G	Sol	80	In and near Fairfield, from west of Green Valley Road to west of North Texas Street, at various locations. Construct permanent Best Management Practices to achieve statewide National Pollutant Discharge Elimination System permit compliance units for trash capture.	8/16/23	\$7,367,000	2/29/24	The Department lapsed this phase and plans to reprogram it.
2	07-32580	5034	LA	1	In the cities of Torrance, Redondo Beach, Hermosa Beach, Manhattan Beach, El Segundo, and Los Angeles, from Paseo De Las Delicias to Dewey Street. Upgrade Americans with Disabilities Act curb ramps and guardrail, repair concrete pavement, reconstruct bus pads, replace asphalt pavement with Portland Cement Concrete at intersections, grind pavement, and place Hot Mix Asphalt and Rubberized Hot Mix Asphalt to rehabilitate pavement. This project will extend the pavement service life and improve ride quality.	8/16/23	\$63,282,000	6/30/24	The Department lapsed this phase and plans to reprogram it.
3	07-34490	5355	LA	103	In the city of Los Angeles, near Wilmington, at the Anaheim Street Overhead No. 53-2627. Upgrade bridge rail to current standards. This project will improve safety and reduce the number and severity of collisions.	3/21/24	\$7,623,000	9/30/24	The Department lapsed this phase and plans to reprogram it.
4	04-3G631	2914W	SCI	9	Near Saratoga, at Saratoga Creek Bridge No. 37-0074. Mitigation project for two years of onsite plant establishment period and plant monitoring for project EA 3G630.	3/21/24	\$400,000	7/31/25	The project will be awarded within the extended deadline.
5	08-1C072	3008C	Riv	74	Near Perris and Hemet, from Route 215 to Route 79 (Winchester Road). Rehabilitate pavement and install Transportation Management System elements. This project will extend the pavement service life and improve ride quality.	5/16/24	\$22,134,000	8/31/25	The project will be awarded within the extended deadline.

Voted Not Awarded Project Status
Four-Month & Time Extension Award Project Status

(Note: All projects are SHOPP unless otherwise noted)

Reference No.: 3.2a.

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Attachment

No.	Dist-EA	PPNO	County	Route	Description	Allocation Date	Allocation Amount x1000	Award Deadline	Project Status
6	12-0R314	4506I	Ora	91	In Anaheim, from Route 55 to 0.7 mile west of Route 90. Upgrade Transportation Management System (TMS) elements, upgrade lighting, and improve safety for highway workers. This project will help to improve corridor management and improve operational efficiency.	8/15/24	\$6,728,000	6/30/25	The project will be awarded within the extended deadline.
7	05-1C8B2	2427X	SB	101	Near Summerland, from 0.9 mile south of South Padaro Lane Undercrossing to 0.6 mile north of Padaro Lane Overcrossing. Environmental mitigation (biological monitoring) for EA 1C822.	12/5/24	\$35,000	6/30/25	The project will be awarded prior to its deadline.
8	07-34850	5392	LA	2	In the cities of Los Angeles, Glendale, and La Canada Flintridge, from Sycamore Avenue to Briartree Drive/Greenridge Drive. Upgrade facilities to Americans with Disabilities Act (ADA) standards.	12/5/24	\$8,021,000	9/30/25	The project will be awarded within the extended deadline.
9	01-0E791	2403M	Hum	254	Near Phillipsville, at 0.7 mile south of Maple Hills Road. Environmental mitigation for revegetation, monitoring, and in-stream restoration for project EA 0E790.	12/5/24	\$4,333,000	6/30/25	The project will be awarded prior to its deadline.
10	08-1M040	3020J	SBd	40	In and near Needles, from Fox Wash Overflow to L Street. Grind and replace pavement with Open Graded Asphalt Concrete (OGAC) to reduce wet weather collisions, install high tension cable barrier, and guardrail.	1/30/25	\$7,308,000	9/30/25	The project will be awarded within the extended deadline.
11	04-2Q740	2027J	Ala	880	In Alameda, San Francisco, and San Mateo Counties, on Routes 80, 101, 880, and 980 at various locations. Install and upgrade Transportation Management System (TMS) elements including Closed Circuit Television (CCTV) cameras, Vehicle Detection Systems (VDS), Changeable Message Signs (CMS), ramp meters, and fiber optic cable.	12/5/24	\$81,171,000	6/30/25	The project has a concurrent time extension on the June 2025 agenda.

Voted Not Awarded Project Status
Four-Month & Time Extension Award Project Status

(Note: All projects are SHOPP unless otherwise noted)

Reference No.: 3.2a.

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Attachment

No.	Dist-EA	PPNO	County	Route	Description	Allocation Date	Allocation Amount x1000	Award Deadline	Project Status
12	04-15500	0064A	Ala	80	In Alameda, Contra Costa, and Solano Counties, on Routes 80, 580, and 980 at various locations; also on local streets from Route 980 to the District 4 Transportation Management Center (TMC). Install fiber optic cable and install and upgrade Transportation Management System (TMS) elements.	12/5/24	\$121,676,000	6/30/25	The project has a concurrent time extension on the June 2025 agenda.
13	05-0N7B2	7102X	SB	101	In Santa Barbara County, near Carpinteria and Summerland from 0.9 miles south of S. Padaro Lane Undercrossing to 0.6 miles north of Padaro Lane Overcrossing. Mitigation monitoring for 05-0N702.	12/5/24	\$35,000	6/30/25	The project will be awarded prior to its deadline.
14	04-0G360	0044D	Ala	260,880	Project is located on Interstate 880 and State Route 260 between 5th Avenue Overhead and 5th and 6th Street Viaduct within the Cities of Oakland and Alameda in Alameda County. The project proposes to improve access along I-880 and in and around the Webster and Posey Tubes (SR-260 tunnels under the Oakland Estuary [Tubes]), downtown Oakland, and the City of Alameda. Proposed improvements also include implementation of follow up landscaping work to restore removed vegetation required by the highway improvements.	12/5/24	\$67,625,000	6/30/25	The project will be awarded prior to its deadline.
15	11-43146	1465	SD	8	In San Diego County, from 0.7 mile west of Flinn Springs Road Undercrossing to 0.2 mile east of Crestwood Road Undercrossing. Biological monitoring for project EA 42210.	1/30/25	\$593,000	7/31/25	The project will be awarded prior to its deadline.
16	11-43164	1477	SD	78	In and near Oceanside, Vista, San Marcos, and Escondido, from Route 5 to North Broadway. Rehabilitate pavement, upgrade facilities to Americans with Disabilities Act (ADA) standards, upgrade bridge rail and Transportation Management System (TMS) elements, replace damaged sign panels, and make operational improvements to reduce delay.	1/30/25	\$126,260,000	7/31/25	The project will be awarded prior to its deadline.

Voted Not Awarded Project Status
Four-Month & Time Extension Award Project Status

(Note: All projects are SHOPP unless otherwise noted)

Reference No.: 3.2a.

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Attachment

No.	Dist-EA	PPNO	County	Route	Description	Allocation Date	Allocation Amount x1000	Award Deadline	Project Status
17	07-34610	5364	LA	1	In Long Beach, at Los Angeles River Bridge No. 53-0341 and De Forest Avenue Undercrossing No. 53-1047. Seismic retrofit, upgrade bridge rails and lighting, and upgrade facilities to Americans with Disabilities Act (ADA) standards.	1/30/25	\$17,527,000	7/31/25	The project will be awarded prior to its deadline.
18	04-0Q130	2022C	SM	1	In and near Half Moon Bay, from south of Wavecrest Road to south of Marine Boulevard. Rehabilitate roadway, upgrade guardrail and Transportation Management System (TMS) elements, rehabilitate drainage systems, upgrade facilities to Americans with Disabilities Act (ADA) standards, and make bicycle improvements. (G13 Contingency)	1/30/25	\$38,847,000	7/31/25	The project will be awarded prior to its deadline.
19	04-1Q582	2024D	SM	101	In various cities, from Santa Clara County line to north of South Airport Boulevard. Rehabilitate pavement, upgrade signs, guardrail, and Transportation Management System (TMS) elements, rehabilitate drainage systems, and upgrade facilities to Americans with Disabilities Act (ADA) standards.	1/30/25	\$154,978,000	7/31/25	The project will be awarded prior to its deadline.
20	04-2Q520	2027C	Sol	29	In Vallejo, from Route 80 to Napa County line (PM 0.0/5.955). Rehabilitate pavement, upgrade facilities to Americans with Disabilities Act (ADA) standards, widen sidewalks, and construct 3.6 miles of Class 2 and 4 bikeways, curb extensions, Light Emitting Diode (LED) light fixtures, and green bikeway markings as complete streets elements.	1/30/25	\$35,713,000	7/31/25	The project will be awarded prior to its deadline.
21	11-42651	1478	SD	5	In the city of San Diego, from 0.5 mile north of Genesee Avenue to 0.2 mile north of Del Mar Heights Road. Biological plant establishment for EA 42650.	1/30/25	\$315,000	7/31/25	The project has a concurrent time extension on the June 2025 agenda.

Voted Not Awarded Project Status
Four-Month & Time Extension Award Project Status

(Note: All projects are SHOPP unless otherwise noted)

Reference No.: 3.2a.

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No.	Dist-EA	PPNO	County	Route	Description	Allocation Date	Allocation Amount x1000	Award Deadline	Project Status
22	01-0J210	1129	DN	101	Near Klamath and Crescent City, from 1.4 miles south of Klamath River Bridge to 0.1 mile south of Humboldt Road/Enderts Beach Road. Rehabilitate pavement and upgrade signs, guardrail, and Transportation Management System (TMS) elements.	1/30/25	\$34,839,000	7/31/25	The project will be awarded prior to its deadline.
23	03-0J560	5142	Pla	80	Near Emigrant Gap, from east of Drum Forebay Overcrossing (OC) to west of Yuba Gap OC (PM 49.3R/R58.7R) and from Nevada County line to west of Troy Undercrossing (PM R62.541R/68.5); also in Nevada County from west of Yuba Gap OC to Placer County line (PM R58.712R/R62.541R). Rehabilitate drainage systems and upgrade Transportation Management System (TMS) elements.	1/30/25	\$15,469,000	7/31/25	The project will be awarded prior to its deadline.
24	07-31960	4972	Ven	1	Near the city of Ventura, at Ventura Overhead No. 52-0040. Replace bridge and make bicycle and pedestrian improvements.	1/30/25	\$56,303,000	7/31/25	The project will be awarded prior to its deadline.
25	05-1F791	2561A	SB	101	Near Buellton, from 0.1 mile south of Nojoqui Creek Bridge No. 51-0018L/R to 0.3 mile south of Santa Rosa Road Overcrossing. Environmental mitigation for bridge rail replacement and widening project EA 1F790.	1/30/25	\$140,000	7/31/25	The project will be awarded prior to its deadline.
26	05-1H440	2650	SLO	1	Near Guadalupe, at the Santa Maria River Bridge No. 49 -0042; also in Santa Barbara County (PM 50.3/50.6). The bridge is scour critical and needs to be replaced. The existing bridge will be used for traffic handling during construction and then demolished. The highway will need to be realigned as a result of the new bridge location. The new bridge will provide standard lane and shoulder widths and include a protected walkway.	1/30/25	\$70,994,000	7/31/25	The project will be awarded prior to its deadline.

Voted Not Awarded Project Status
Four-Month & Time Extension Award Project Status

(Note: All projects are SHOPP unless otherwise noted)

Reference No.: 3.2a.

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No.	Dist-EA	PPNO	County	Route	Description	Allocation Date	Allocation Amount x1000	Award Deadline	Project Status
27	05-1H470	2655	SCr	9	Near Boulder Creek, at San Lorenzo River Bridge No. 36-0052 and Kings Creek Bridge No. 36-0054 (PM 15.49). Replace bridges to maintain standards of safety and reliability.	1/30/25	\$19,735,000	7/31/25	The project will be awarded prior to its deadline.
28	06-0Q432	8106	Tul	190	In and near Porterville, from 0.4 mile west of South Westwood Drive to 0.1 mile east of South Main Street. Construct roundabouts, turn lanes, channelization, and signalized intersections. (Additional contribution: \$5,070,000 RW Cap and \$10,100,000 CONST from Tulare County Association of Governments)	1/30/25	\$3,475,000	7/31/25	The project will be awarded prior to its deadline.
29	06-0U290	6803	Ker	184	Near Bakersfield, from Dunnsmere Street to Breckenridge Road. Upgrade Americans with Disabilities Act (ADA) curb ramps, install bike lanes, cold plane pavement, place Hot Mix Asphalt (HMA) and Rubberized Hot Mix Asphalt (RHMA) pavement, and construct sidewalks, bus turnouts, and improve pedestrian crossings as complete streets elements.	1/30/25	\$36,953,000	7/31/25	The project will be awarded prior to its deadline.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.2b., Information Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **MONTHLY STATUS OF CONSTRUCTION CONTRACT AWARD FOR LOCAL ASSISTANCE STIP PROGRAM PROJECTS, PER STIP GUIDELINES**

SUMMARY:

The California Department of Transportation (Department) is presenting this item for information purposes only. The item provides the status of locally-administered State Transportation Improvement Program (STIP) projects that received a construction allocation in Fiscal Year 2023-24 through 2024-25.

In 2023-24, the California Transportation Commission (Commission) allocated \$64,996,000 to construct 30 locally-administered STIP projects. As of May 22, 2025, 30 projects totaling \$64,996,000 have been awarded.

In 2024-25, the Commission allocated \$12,910,000 to construct 11 locally-administered STIP projects. As of May 22, 2025, six projects totaling \$5,309,000 have been awarded and five projects are scheduled to award by their deadline.

BACKGROUND:

Current STIP Guidelines require projects to be ready to proceed to construction within six months of allocation. The policy also requires the Department to report to the Commission on those projects that have not been awarded within four months of allocation.

Fiscal Year 2023-24 Allocations

Month Allocated	Number of Projects Voted	Voted Projects (in 1000s)	Number of Projects Awarded	Number of Projects Lapse	Number of Projects Pending Award	Number of Projects Awarded within 6 months*
August 2023	15	\$22,721	15	0	0	9
October 2023	1	\$5,745	1	0	0	1
December 2023	1	\$2,285	1	0	0	1
January 2024	2	\$1,965	2	0	0	2
March 2024	4	\$9,770	4	0	0	3
May 2024	1	\$7,362	1	0	0	1
June 2024	6	\$15,148	6	0	0	4
Total	30	\$64,996	30	0	0	21

Fiscal Year 2024-25 Allocations

Month Allocated	Number of Projects Voted	Voted Projects (in 1000s)	Number of Projects Awarded	Number of Projects Lapse	Number of Projects Pending Award	Number of Projects Awarded within 6 months*
August 2024	2	\$2,600	2	0	0	2
October 2024	0	\$0	0	0	0	0
December 2024	3	\$5,030	1	0	2	1
January 2025	4	\$2,774	3	0	1	3
March 2025	1	\$198	0	0	1	0
May 2025	1	\$2,308	0	0	1	0
Total	11	\$12,910	6	0	5	6

Attachment

**Voted Not Awarded Project Status 4-Month
& Time Extension Award Project Status**

Agency Name	Project Title	Dist.- PPNO	Allocation Date	Award Deadline	Allocation Amount (\$1,000's)	Project Status
Lassen County	County Rehab. D Standish Buntingville Road, County Road 301 (A-3)	2-2600	Dec-2024	Jun-2025	\$2,015	The project will be awarded within the deadline.
Lassen County	County Rehab. E Standish Buntingville Road, County Road 301 (A-3)	2-2601	Dec-2024	Jun-2025	\$2,015	The project will be awarded within the deadline.
City of Orland	Road M 1/2 Reconstruction	3-3786	Jan-2025	Jul-2025	\$1,065	The project will be awarded within the deadline.
City of Fort Jones	Bower and Butte Reconstruction	2-2616	Mar-2025	Sep-2025	\$198	The project will be awarded within the deadline.
City of Elk Grove	North Laguna Creek Area Big Horn Boulevard and Franklin Boulevard Improvements	3-5802	May-2025	Nov-2025	\$2,308	The project will be awarded within the deadline.
Grand Total					\$7,601	

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.2c., Information Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **MONTHLY STATUS OF CONSTRUCTION CONTRACT AWARD FOR LOCAL ASSISTANCE ACTIVE TRANSPORTATION PROGRAM PROJECTS, PER ATP GUIDELINES**

SUMMARY:

The California Department of Transportation (Department) is presenting this item for information purposes only. The item provides the status of locally-administered Active Transportation Program (ATP) projects that received a construction allocation in Fiscal Year 2023-24 through 2024-25.

In 2023-24, the California Transportation Commission (Commission) allocated \$197,991,000 to construct 87 ATP projects. As of May 22, 2025, 73 projects totaling \$106,436,000 have been awarded. One project has lapsed and 13 projects have time extensions that are scheduled to award by the extended deadline.

In 2024-25, the Commission allocated \$177,280,000 to construct 44 locally-administered ATP projects. As of May 22, 2025, nine projects totaling \$22,765,000 have been awarded and five projects have a concurrent time extension request on this month's agenda. Three projects have approved time extensions and the remaining 27 are scheduled to award by the deadline.

BACKGROUND:

Current ATP Guidelines require projects to be ready to proceed to construction within six months of allocation. The policy also requires the Department to report to the Commission those projects that have not been awarded within four months of allocation.

FY 2023-24 Allocations

Month Allocated	Number of Projects Voted	Voted Projects (in 1000s)	Number of Projects Awarded	Number of Projects Lapsed	Number of Projects Pending Award	Number of Projects Awarded within 6 months*
August 2023	8	\$8,183	8	0	0	7
October 2023	6	\$5,842	5	1	0	4
December 2023	13	\$26,268	12	0	1	12
January 2024	12	\$32,030	11	0	1	9
March 2024	15	\$62,199	10	0	5	8
May 2024	15	\$31,628	14	0	1	8
June 2024	18	\$31,841	13	0	5	12
Total	87	\$197,991	73	1	13	60

FY 2024-25 Allocations

Month Allocated	Number of Projects Voted	Voted Projects (in 1000s)	Number of Projects Awarded	Number of Projects Lapsed	Number of Projects Pending Award	Number of Projects Awarded within 6 months*
August 2024	1	\$1,497	1	0	0	1
October 2024	2	\$30,423	1	0	1	1
December 2024	11	\$22,240	5	0	6	5
January 2025**	11	\$30,400	2	0	9	2
March 2025	13	\$68,535	0	0	13	0
May 2025	6	\$24,185	0	0	6	0
Total	44	\$177,280	9	0	35	9

Note: Includes all ATP Infrastructure and Non-Infrastructure projects.

* Per Current ATP Guidelines, projects shall award construction within six months of allocation.

** The \$3,764,000 allocated at the January 2025 Commission meeting for the Construction phase of remaining funds allocated at the January 2025 Commission meeting accounts for PPNOs 11-1161A and 11-1161B, under Resolution FATP-2425-13.

Voted Not Awarded Project Status
4-Month & Time Extension Award Project Status

Agency Name	Project Title	Dist.- PPNO	Allocation Date	Award Deadline	Allocation Amount (In 1,000's)	Project Status
City of Santa Monica	Stewart-Pennsylvania Safety Enhancement Project	7-5861B	Oct-2023	Apr-2024	\$25	The project was not awarded within the deadline. Funds lapsed.
San Diego Association of Governments (SANDAG)	University Bikeway	11-1394	Dec-2023	Jun-2025	\$8,561	The project will be awarded within the extended deadline.
Los Angeles County	Unincorporated Los Angeles County Safe Routes to School Plan	07-6177	Jun-2024	Jun-2025	\$750	The project will be awarded within the extended deadline.
Los Angeles County	Lennox Vision Zero Traffic Safety Enhancements	07-6180Q	Jun-2024	Jun-2025	\$1,141	The project will be awarded within the extended deadline.
City of Downey	South Downey Safe Routes to School Phase II Project	07-6192B	Jun-2024	Jun-2025	\$100	The project will be awarded within the extended deadline.
Hoopa Valley Tribe	Hoopa Valley Safe Routes to School Project	01-2440A	Dec-2024	Jun-2025	\$1,180	The project will be awarded within the deadline.
Hoopa Valley Tribe	Hoopa Valley Safe Routes to School Project	01-2440B	Dec-2024	Jun-2025	\$43	The project will be awarded within the deadline.
City of South Lake Tahoe	Pioneer Trail Pedestrian Improvement Project Phase 2	03-6140	Dec-2024	Jun-2025	\$1,200	The project will be awarded within the deadline.
Santa Barbara County	Modoc Road Multimodal Path Gap Closure (Phase II)	05-2992	Dec-2024	Jun-2025	\$2,616	A Concurrent Time Extension has been submitted for the June 2025 meeting.
City of Waterford	Waterford Safe Routes to School Project - Yosemite Boulevard	10-3521	Dec-2024	Jun-2025	\$803	The project will be awarded within the deadline.

Voted Not Awarded Project Status
4-Month & Time Extension Award Project Status

Agency Name	Project Title	Dist.- PPNO	Allocation Date	Award Deadline	Allocation Amount (In 1,000's)	Project Status
City of Los Angeles	LA River Greenway, West San Fernando Valley Gap Closure	07-5663	Jan-2024	Jul-2025	\$18,593	The project will be awarded within the extended deadline.
Alameda County	Mission Boulevard Safe and Complete Streets for Active Transportation	04-2361	Oct-2024	Jul-2025	\$25,000	The project will be awarded within the extended deadline.
City of Willits	City of Willits Rail with Trail Project	01-4726B	Jan-2025	Jul-2025	\$189	The project will be awarded within the deadline.
City of Coalinga	Coalinga Perimeter Multi-Use Trail	06-7025	Jan-2025	Jul-2025	\$660	The project will be awarded within the deadline.
City of Bell Gardens	Bell Gardens Complete Streets Improvements - Phase 1	07-5868	Jan-2025	Jul-2025	\$6,299	The project will be awarded within the deadline.
City of El Monte	Traffic Calming for Parkway Dr/Denholm Dr	07-6042	Jan-2025	Jul-2025	\$4,334	A Concurrent Time Extension has been submitted for the June 2025 meeting.
Cal Poly Pomona	Cal Poly Pomona Campuswide Active Transportation Plan	07-6175	Jan-2025	Jul-2025	\$299	The project will be awarded within the deadline.
City of Colton	Jehue Corridor and Eucalyptus Avenue Class I Bike Paths	08-1257A	Jan-2025	Jul-2025	\$2,079	The project will be awarded within the deadline.
City of Colton	Jehue Corridor and Eucalyptus Avenue Class I Bike Paths	08-1257B	Jan-2025	Jul-2025	\$29	The project will be awarded within the deadline.
City of Wildomar	Bundy Canyon Active Transportation Corridor (BCATC)	08-1286B	Jan-2025	Jul-2025	\$77	A Concurrent Time Extension has been submitted for the June 2025 meeting.

Voted Not Awarded Project Status
4-Month & Time Extension Award Project Status

Agency Name	Project Title	Dist.- PPNO	Allocation Date	Award Deadline	Allocation Amount (In 1,000's)	Project Status
City of Sacramento	Franklin Boulevard Complete Street Project	03-1800	Mar-2024	Sep-2025	\$9,323	The project will be awarded within the extended deadline.
City of San Jose	Better Bikeway/SJ - San Fernando Corridor	04-2331	Mar-2024	Sep-2025	\$8,208	The project will be awarded within the extended deadline.
City of Goleta	San Jose Multipurpose Path	05-2995	Mar-2024	Sep-2025	\$15,290	The project will be awarded within the extended deadline.
City of Wildomar	Bundy Canyon Active Transportation Corridor (BCATC)	08-1286A	Mar-2024	Sep-2025	\$1,377	The project will be awarded within the extended deadline.
City of National City	Bayshore Bikeway – Segment 5	11-1361	Mar-2024	Sep-2025	\$5,421	The project will be awarded within the extended deadline.
City of National City	Bayshore Bikeway Segment 5	11-1526	Jun-2024	Sep-2025	\$2,072	The project will be awarded within the extended deadline.
Imperial County Transportation Commission	Pedestrian Improvements for the Calexico Intermodal Transportation Center	11-1538	Jun-2024	Sep-2025	\$1,073	The project will be awarded within the extended deadline.
Butte County	South Oroville Bike and Ped Connectivity Project	03-6126A	Mar-2025	Sep-2025	\$6,904	The project will be awarded within the deadline.
City of San Jose	Willow-Keyes Complete Streets Improvements	04-2336	Mar-2025	Sep-2025	\$12,926	The project will be awarded within the deadline.
City of Watsonville	Safer Access to Pajaro Valley High School and Beyond	05-0413A	Mar-2025	Sep-2025	\$10,541	The project will be awarded within the deadline.
San Luis Obispo County	Bob Jones Multi-Use Path Gap Closure Segment 1	05-3060	Mar-2025	Sep-2025	\$15,953	The project will be awarded within the deadline.

Voted Not Awarded Project Status
4-Month & Time Extension Award Project Status

Agency Name	Project Title	Dist.- PPNO	Allocation Date	Award Deadline	Allocation Amount (In 1,000's)	Project Status
City of Huron	Citywide Sidewalk Improvements	06-8080	Mar-2025	Sep-2025	\$1,532	The project will be awarded within the deadline.
City of Los Angeles	112 th Street and Flournoy Elementary Schools Safety Improvements Project	07-5579	Mar-2025	Sep-2025	\$4,448	The project will be awarded within the deadline.
City of Lake Elsinore	Murrieta Creek Multi-Use Trail	08-1262	Mar-2025	Sep-2025	\$3,904	The project will be awarded within the deadline.
Riverside County	Riverside County Safe Routes for All – Palm Springs	08-1343	Mar-2025	Sep-2025	\$556	The project will be awarded within the deadline.
Riverside County	Riverside County Safe Routes for All – Cathedral City	08-1344	Mar-2025	Sep-2025	\$745	The project will be awarded within the deadline.
Mariposa County	Mariposa Elementary School Connectivity Project	10-3513	Mar-2025	Sep-2025	\$1,800	The project will be awarded within the deadline.
City of Stockton	Safe Routes to School Sidewalk Gap Closure	10-3462	Mar-2025	Sep-2025	\$1,020	The project will be awarded within the deadline.
City of National City	8 th Street and Roosevelt Ave Active Transportation Corridor, National City	11-1395	Mar-2025	Sep-2025	\$5,185	A Concurrent Time Extension has been submitted for the June 2025 meeting.
City of Anaheim	Citywide Safe Routes to School Sidewalk Gap Closure	12-2196A	Mar-2025	Sep-2025	\$3,021	The project will be awarded within the deadline.
City of Los Angeles	Broadway-Manchester Active Transportation Equity Project	07-5662	May-2024	Nov-2025	\$19,621	The project will be awarded within the extended deadline.
San Diego Association of Governments (SANDAG)	Orange Family Friendly Street Project	11-1443	Dec-2024	Jun-2026	\$4,317	The project will be awarded within the extended deadline.

Voted Not Awarded Project Status
4-Month & Time Extension Award Project Status

Agency Name	Project Title	Dist.- PPNO	Allocation Date	Award Deadline	Allocation Amount (In 1,000's)	Project Status
City of Long Beach	Orange Avenue Backbone Bikeway and Complete Streets Improvements	07-5581	Jan-2025	Jul-2026	\$12,670	The project will be awarded within the extended deadline.
Grand Total					\$221,885	

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.3, Information Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **STATUS OF THE START TIME OF EXPENDITURES FOR PRE-CONSTRUCTION
SUPPORT PHASES FOR SHOPP PROJECTS PER THE TIMELY USE OF FUNDS
POLICY**

SUMMARY:

The California Department of Transportation (Department) is presenting this information item on the status of pre-construction support phases for State Highway Operation and Protection Program (SHOPP) projects. The pre-construction support phases are Project Approval and Environmental Document (PA&ED), Plans, Specifications, and Estimate (PS&E) and Right of Way Support (RW Sup).

In Fiscal Year 2024-25, the California Transportation Commission (Commission) allocated 278 SHOPP pre-construction support phases. As of May 22, 2025, 242 phases have begun expenditure, and 36 phases are scheduled to expend by their respective deadline.

BACKGROUND:

The passage of Senate Bill 1, the Road Repair and Accountability Act, necessitates that the Department and the Commission establish baseline budgets for each pre-construction support phase of each project in the 2020, 2022 and 2024 SHOPP. Government Code Section 14526.5(g) formalized the condition of allocation for pre-construction support phases on or after July 1, 2017 for all SHOPP projects.

The SHOPP Guidelines developed by Commission staff, in partnership with the Department, and adopted by the Commission at the June 2019 meeting, requires that expenditures allocated for SHOPP projects for pre-construction support phases begin accruing expenditures within six months of allocation.

Attachment

Voted Not Expended Project Status
Pre-Construction Allocations for SHOPP Projects
Status for projects nearing six months from allocation

Reference No.: 3.3
June 26-27, 2025
Attachment 1

No.	Dist- EA	PPNO	County- Route	Work Description	Phase	Allocation Date	Start Expenditure Deadline	Allocation Amount (X 1000)	Project Status
1	08-1R010	3023Y	Riv-215	In Perris, from 0.6 mile north of Route 74 (East) to 1.7 miles south of Route 74 (West). Install High Tension Cable Barrier.	PA&ED	01/30/2025	7/31/2025	\$ 1,121	The Department plans to expend by 7/31/2025.
2	08-0P910	3024Z	SBd-10	In Colton, from 0.4 mile east of Mount Vernon Avenue to 0.3 mile west of Route 215. Install guardrail and construct dike.	PA&ED	01/30/2025	7/31/2025	\$ 875	The Department plans to expend by 7/31/2025.
3	04-1W130	2910F	SM-82	In Atherton and Redwood City, from Selby Lane to Brewster Avenue. Repurpose on-street parking for bikeways and reconstruct intersections to include removing slip lanes, installing ladder-style crosswalks, Pedestrian Hybrid Beacon system, and upgrading curb ramps.	PA&ED	01/30/2025	7/31/2025	\$ 1,928	The Department plans to expend by 7/31/2025.
4	08-1R100	3024F	Sbd-38	In and near Yucaipa and Redlands, from 0.4 mile west to 0.4 mile east of Bryant Street. Install traffic signals and advanced flashing beacons.	PA&ED	01/30/2025	7/31/2025	\$ 1,277	The Department plans to expend by 7/31/2025.
5	07-3230U	6406	Ven-1	Near Mussel Shoals, at Willow Creek Bridge No. 52-0003; also near Ojai, on Route 33 at North Fork Matilija Bridges No. 52-0044 (PM 15.82) and No. 52-0173 (PM16.13). Upgrade bridge railings to meet current standards and paint steel components of Bridge No. 52-0173.	PS&E R/W Sup	01/30/2025	7/31/2025	\$ 214	The Department plans to expend by 7/31/2025.
6	04-4Q830	2032G	Son-12	Near Glen Ellen and Kenwood, from 0.9 mile east of Libby Avenue to 0.3 mile east of Aurora Lane. Widen shoulder and construct rumble strips.	PA&ED	01/30/2025	7/31/2025	\$ 1,510	The Department plans to expend by 7/31/2025.
7	05-1S590	3253	Mon-101	Near Chualar and Salinas, from 0.2 mile south of Main Street to 0.9 mile south of Airport Boulevard. Remove at-grade median openings to reduce the frequency and severity of collisions involving crossing and left-turn movements. Remove at-grade median openings to reduce the frequency and severity of collisions involving crossing and left-turn movements.	PS&E R/W Sup	01/30/2025	7/31/2025	\$ 2,099	The Department plans to expend by 7/31/2025.
8	05-1S110	3224	SB-154	Near Los Olivos, from 0.2 mile west to 0.1 mile east of Foxen Canyon Road. Construct roundabout.	PA&ED	01/30/2025	7/31/2025	\$ 1,170	The Department plans to expend by 7/31/2025.
9	05-1R540	3204	SBT-25	Near Hollister, from 0.1 mile north to 1.0 mile north of Santa Ana Road. Install median barrier, pavement surface treatment, and drainage system.	PS&E R/W Sup	01/30/2025	7/31/2025	\$ 1,490	The Department plans to expend by 7/31/2025.

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 3.4 Information

Prepared By: Jaeden Gales,
Staff Services Manager I

Published Date: June 13, 2025

Subject: Local and Regional Agency Notice of Intent to Expend Funds on State Transportation Improvement Program Projects Prior to Commission Allocation, Per Senate Bill 184

Summary:

Government Code Section 14529.17, as amended by Senate Bill 184 (Chapter 462, Statutes of 2007), authorizes a local or regional agency, upon notifying the California Transportation Commission (Commission), to expend funds on a project programmed in the State Transportation Improvement Program (STIP) for which the Commission has not yet made an allocation. Attachment A includes a list of three planning, programming, and monitoring projects programmed in Fiscal Year 2025-26.

Background:

Section 14529.17 is limited to projects programmed in the current fiscal year of the STIP. Notifications received prior to the beginning of the fiscal year are effective on July 1. Notifications received after July 1 are effective the date the Commission receives the notification letter.

The Commission's STIP guidelines, Section 72, directs agencies to submit a copy of the notification and the allocation request to the Commission's Executive Director. The original allocation request should be submitted to the California Department of Transportation at the same time.

The Section 14529.17 authorization does not establish a priority for allocations made by the Commission, nor does it establish a timeframe for when the allocations will be approved by the Commission. Moreover, the statute does not require the Commission approve an allocation it would not otherwise approve.

Section 14529.17 advance expenditures must be eligible for reimbursement in accordance with state laws and procedures. In the event the advance expenditures are determined to be ineligible, the state has no obligation to reimburse those expenditures.

Attachments:

- Attachment A: Notices of Intent to Expend Funds on State Transportation Improvement Program Projects Prior to Commission Allocation

SB 184 Notifications for Local STIP Projects

FISCAL YEAR 2025-26

(\$1,000's)

Includes SB 184 Letters Received Prior to July 1, 2025

#	County	Agency	Rte	PPNO	Project	Date Letter is Effective	Meeting Reported	Planned Allocation	FY 25-26	Project Totals by Component			
										R/W	Const	E & P	PS&E
1	Imperial	ICTC		7200	Planning, Programming, and Monitoring	01-Jul-25	Jun-25	Aug-25	\$ 279		\$ 279		
2	Orange	OCTA		2132	Planning, Programming, and Monitoring	01-Jul-25	Jun-25	Aug-25	\$ 1,030		\$ 1,030		
3	Riverside	RCTC		9803	Planning, Programming, and Monitoring	01-Jul-25	Jun-25	Aug-25	\$ 600		\$ 600		
Total (eligible on July 1, 2025, or from Effective Date of Letter)									\$ 1,909	0	\$ 1,909	0	0

184 since last Commission meeting

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.5, Information Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **FISCAL YEAR 2024-25 – THIRD QUARTER – FINANCE REPORT**

SUMMARY:

The California Department of Transportation (Department) will present the Fiscal Year 2024-25 Finance Report for the third quarter to the California Transportation Commission (Commission) at its June 26-27, 2025 meeting, as an informational item.

BACKGROUND:

The purpose of the quarterly Finance Report is to provide the Commission with the status of capital allocations versus capacity and to report any trends or issues that may require action by the Department or the Commission regarding transportation funding policy, allocation capacity, or forecast methodology to ensure the efficient and prudent management of transportation resources.

Attachment



Department of Transportation Quarterly Finance Report

Third Quarter 2024-25

Department of Transportation
Division of Budgets

The purpose of the Quarterly Finance Report is to provide the California Transportation Commission (Commission) with the status of capital allocations versus capacity and to report any trends or issues that may require action by the California Department of Transportation (Department) or Commission regarding transportation funding policy, allocation capacity, or forecast methodology to ensure the efficient and prudent management of transportation resources. Below is the schedule of dates for the development of prior and future Quarterly Finance Reports.

California Department of Transportation
Quarterly Finance Report
Schedule of Reports

Fiscal Year 2024-25

2023-24 Q4

Close of Quarter6/30/24
Quarterly Report to Commission Staff8/30/24
Presented to Commission.....10/17/24

2024-25 Q1

Close of Quarter9/30/24
Quarterly Report to Commission Staff11/15/24
Presented to Commission.....12/5/24

2024-25 Q2

Close of Quarter12/31/24
Quarterly Report to Commission Staff2/15/25
Presented to Commission.....3/20/25

2024-25 Q3

Close of Quarter3/31/25
Quarterly Report to Commission Staff5/15/25
Presented to Commission.....6/26/25

Fiscal Year 2025-26

2024-25 Q4

Close of Quarter6/30/25
Quarterly Report to Commission Staff8/30/25
Presented to Commission.....10/16/25

Department of Transportation Quarterly Finance Report

Third Quarter 2024-25

EXECUTIVE SUMMARY

2024-25 Capital Allocations vs. Capacity Summary through March 31, 2025 (\$ in millions)

	SHOPP ¹	STIP ¹	AERO	LPP	SCCP	TCEP	ATP	TIRCP	BONDS	TOTAL
Allocation Capacity	\$5,392	\$811	\$4	\$401	\$390	\$877	\$1,157	\$3,166	\$66	\$12,263
Total Votes	3,682	216	3	66	0	166	184	445	8	4,769
Adjustments²	-158	-1	0	0	0	0	0	0	0	-159
Remaining Capacity	\$1,868	\$596	\$1	\$335	\$390	\$711	\$973	\$2,722	\$58	\$7,653

Note: Amounts may not sum to totals due to independent rounding.

¹Proposition 1B Bonds included in totals for 2024-25.

²Adjustments include authorized project increases and decreases pursuant to the Commission's G-12 process.

Through the third quarter of fiscal year 2024-25, the Commission has allocated approximately \$4.8 billion toward 590 projects. Adjustments were negative \$159 million, leaving approximately \$7.7 billion (62 percent) in remaining allocation capacity across multiple programs.

The majority of the Department's main accounts referenced in Appendix C ended the third quarter with fund balances higher than forecast due to expenditures that came in lower than projected. Conversely, the State Highway Account and the Public Transportation Account each ended the third quarter with fund balances that were below forecast.

On March 15, 2025, the President approved a Continuing Resolution to keep the federal government operating through the remainder of the Federal fiscal year (FFY). FFY 2025 represents the fourth year of the five-year Infrastructure Investment and Jobs Act, enacted in November 2021. Year four formula funds (through the Federal Highway Administration) total \$5.8 billion for California.

STATE BUDGET OUTLOOK

On January 10th, Governor Newsom and the Department of Finance released the proposed budget for the upcoming 2025-26 fiscal year, which represents the initial step in the budgetary process that will ultimately lead to approval of the Budget Act in June later this year.

Specific to transportation related revenues during the upcoming 2025-26 fiscal year, the Governor's Budget projects gross state gasoline and diesel excise tax collections of approximately \$7.9 billion and \$1.6 billion, respectively. These amounts represent a one percent decrease to gasoline excise collections and a five percent increase to diesel excise collections compared to previously Enacted Budget projections for the current 2024-25 fiscal year. Compared to 2024-25, Transportation Improvement Fee projections for 2025-26 increased nominally to \$2.5 billion, while diesel sales tax decreased between the same periods by 14 percent to \$1.1 billion. Projections for the Road Improvement Fee, which are fees collected from zero-emission vehicle owners at time of registration, increased by 42 percent to \$182 million between the same periods.

The Budget maintains the entirety of the transportation package included in recent budgets. This includes \$15.4 billion from various funds over multiple years for the following:

- \$7.7 billion for high-priority transit and rail infrastructure projects that will improve rail and transit connectivity between state and local/regional services that are designed to provide options to opt-out of traffic congestion and reduce greenhouse gas emissions. A portion of these funds can also be used to support transit operations.
- \$4.2 billion Proposition 1A for the High-Speed Rail Authority to continue building the 119-mile Central Valley Segment from Madera to just north of Bakersfield.
- \$1.2 billion for projects that improve goods movement on rail and roadways at port terminals, including railyard expansions, new bridges, and zero-emission modernization projects.
- \$1.1 billion for Active Transportation Program projects, the Reconnecting Communities Highways to Boulevards Pilot program, and climate adaptation projects to advance equity and health outcomes.
- \$1.1 billion for the Zero Emission Transit Capital Program.
- \$150 million for grade separation projects that support critical safety improvements and improve traffic and rail movement by separating the vehicle roadway from the rail tracks. The largest contributor of California's greenhouse gas emissions, the transportation sector must align project delivery with climate considerations to reduce both emissions and climate impacts. The Budget demonstrates a continued commitment to address transportation-related climate issues.

With the proposed 2025-26 budget released, the negotiation processes between the Administration and the Legislature have begun, which includes meetings with the Legislative Analyst's Office and legislative staff and holding legislative hearings to discuss what is contained in the budget proposal. These ongoing negotiations will result in a revised proposed budget that will be released in mid-May and then continue until the budget is eventually approved by both the Legislature and the Governor for enactment on July 1st, 2025.

STATE HIGHWAY OPERATION AND PROTECTION PROGRAM (SHOPP)

Fund	Allocation Capacity	Allocations to Date	Adjustments	Net Allocations	Remaining Capacity
SHA	\$150	\$326	\$0	\$326	-\$176
FTF	2,450	2,461	-89	2,372	78
RMRA	2,792	895	-69	826	1,966
Proposition 1B	0	0	0	0	0
Total	\$5,392	\$3,682	-\$158	\$3,524	\$1,868

Notes: Amounts may not sum to totals due to independent rounding.
\$ in millions

Capital Allocations vs. Capacity

The Commission allocated approximately \$3.7 billion toward 315 SHOPP projects through the third quarter. Adjustments totaled negative \$158 million, leaving approximately \$1.9 billion (35 percent) in remaining allocation capacity.

Outlook for Funding & Allocations

State Highway Account (SHA). Through the third quarter of 2024-25, net allocations for SHA SHOPP projects totaled \$326 million, resulting in an overallocation of \$176 million. This is offset by available capacity in other SHOPP funds. SHA SHOPP funds are primarily derived from a percentage of the base excise taxes on gasoline and diesel, in addition to backfilled weight fees.

Federal Trust Fund (FTF). Net allocations totaling approximately \$2.4 billion were committed toward federally eligible SHOPP projects through the third quarter. This leaves \$78 million in remaining allocation capacity.

Road Maintenance and Rehabilitation Account (RMRA). Net allocations of \$826 million have been committed toward SHOPP projects through the third quarter, leaving approximately \$2 billion in remaining allocation capacity. Senate Bill (SB) 1 statutorily requires a fixed portion of RMRA revenue be set aside for certain programs and the remaining balance is split equally between state and local projects.

Proposition 1B. No Proposition 1B SHOPP projects were allocated through the third quarter.

Recommendations

The Department will continue to monitor SHOPP resources and advise the Commission of any concerns or changes.

STATE TRANSPORTATION IMPROVEMENT PROGRAM (STIP)

Fund	Allocation Capacity	Allocations to Date	Adjustments	Net Allocations	Remaining Capacity
SHA	\$406	\$105	-\$1	\$104	\$302
FTF	200	111	0	111	89
PTA	205	0	0	0	205
Proposition 1B STIP	0	0	0	0	0
Total	\$811	\$216	-\$1	\$215	\$596

Notes: Amounts may not sum to totals due to independent rounding.
\$ in millions

Capital Allocations vs. Capacity

The Commission allocated approximately \$216 million toward 84 STIP projects through the third quarter. Adjustments totaled negative \$1 million, leaving approximately \$596 million (73 percent) in remaining capacity.

Outlook for Funding & Allocations

SHA. Through the third quarter, net allocations for SHA STIP projects were \$104 million, leaving approximately \$302 million in remaining allocation capacity. Revenue derived from the incremental portion of the state excise tax on gasoline is the primary funding source of state funds for the STIP.

FTF. Net allocations totaling \$111 million were committed toward federally eligible STIP projects through the third quarter, leaving \$89 million in remaining allocation capacity.

Public Transportation Account (PTA). There were no PTA STIP allocations through the third quarter. Remaining capacity is \$205 million.

Proposition 1B. There were no Proposition 1B STIP allocations through the third quarter.

Recommendations

The Department will continue to monitor STIP resources and advise the Commission of any concerns or changes.

AERONAUTICS (AERO) PROGRAM

Fund	Allocation Capacity	Allocations to Date	Adjustments	Net Allocations	Remaining Capacity
AERO Account	\$3.9	\$2.5	\$0	\$2.5	\$1.4
Total	\$3.9	\$2.5	\$0	\$2.5	\$1.4

Notes: Amounts may not sum to totals due to independent rounding.
\$ in millions

Capital Allocations vs. Capacity

The Commission allocated a lump-sum of \$2.5 million toward the Airport Improvement Program for 2024-25. Through the third quarter, the Department sub-allocated approximately \$997,000 toward 20 projects. There were no Acquisition & Development actions through the third quarter.

Outlook for Funding & Allocations

The 2024 AERO Account Fund Estimate was adopted by the Commission at the August 2023 meeting. In 2019-20, the Department, in collaboration with the Department of Finance developed a budget change proposal to request \$10 million over four years in transfers from the Local Airport Loan Account (LALA) to support the AIP matching grant through fiscal year 2023-24. Beginning fiscal year 2024-25, the AERO Account will no longer receive an annual \$2.5 million transfer of funds from LALA.

Recommendations

The Department will continue to monitor AERO resources and advise the Commission of any changes or issues.

LOCAL PARTNERSHIP PROGRAM (LPP)

Fund	Allocation Capacity	Allocations to Date	Adjustments	Net Allocations	Remaining Capacity
RMRA	\$401	\$66	\$0	\$66	\$335
Total	\$401	\$66	\$0	\$66	\$335

Notes: Amounts may not sum to totals due to independent rounding.
\$ in millions

Capital Allocations vs. Capacity

The Commission allocated \$66 million toward 12 LPP projects through the third quarter. Remaining capacity is \$335 million (83 percent).

Outlook for Funding & Allocations

Final guidelines for the 2024 LPP, which cover fiscal years 2025-26 and 2026-27, were adopted by the Commission during the August 2024 meeting. The two-year program includes a total of \$452 million in funding split between the Formulaic and Competitive programs. The 2024 LPP Formulaic Funding Distribution was adopted by the Commission at the August 2024 meeting. The Initial Program of Projects for the 2024 Formulaic Program was adopted by the Commission at the January 2025 meeting.

Recommendations

The Department will continue to monitor LPP resources and advise the Commission of any changes or issues.

SOLUTIONS FOR CONGESTED CORRIDORS PROGRAM (SCCP)

Fund	Allocation Capacity	Allocations to Date	Adjustments	Net Allocations	Remaining Capacity
SHA	\$390	\$0	\$0	\$0	\$390
Total	\$390	\$0	\$0	\$0	\$390

Notes: Amounts may not sum to totals due to independent rounding.
\$ in millions

Capital Allocations vs. Capacity

There were no allocations to SCCP projects through the third quarter. Remaining capacity is \$390 million.

Outlook for Funding & Allocations

Guidelines for the 2024 cycle of the SCCP were adopted by the Commission at the August 2024 meeting. The 2024 SCCP will provide two years of funding in fiscal years 2025-26 and 2026-27 totaling approximately \$481 million.

Recommendations

The Department will continue to monitor SCCP resources and advise the Commission of any changes or issues.

TRADE CORRIDOR ENHANCEMENT PROGRAM (TCEP)

Fund	Allocation Capacity	Allocations to Date	Adjustments	Net Allocations	Remaining Capacity
TCEA	\$753	\$117	\$0	\$117	\$635
FTF	124	48	0	48	76
Total	\$877	\$166	\$0	\$166	\$711

Notes: Amounts may not sum to totals due to independent rounding.
\$ in millions

Capital Allocations vs. Capacity

The Commission allocated \$166 million toward 13 TCEP projects through the third quarter. Remaining capacity is \$711 million (81 percent).

Outlook for Funding & Allocations

The 2024 TCEP Guidelines and Fund Estimate, which cover fiscal years 2025-26 and 2026-27, were both adopted by the Commission at the August 2024 meeting. The 2024 TCEP will provide two years of funding in fiscal years 2025-26 and 2026-27 totaling approximately \$900 million.

Recommendations

The Department will continue to monitor TCEP resources and advise the Commission of any changes or issues.

ACTIVE TRANSPORTATION PROGRAM (ATP)

Fund	Allocation Capacity	Allocations to Date	Adjustments	Net Allocations	Remaining Capacity
SHA	\$425	\$0	\$0	\$0	\$425
FTF	325	66	0	66	259
RMRA	334	45	0	45	289
General Fund	73	73	0	73	0
Total	\$1,157	\$184	\$0	\$184	\$973

Notes: Amounts may not sum to totals due to independent rounding.
\$ in millions

Capital Allocations vs. Capacity

The Commission allocated \$184 million toward 93 ATP projects through the third quarter, leaving approximately \$973 million (84 percent) in remaining allocation capacity.

Outlook for Funding & Allocations

The Fund Estimate and guidelines for the 2023 ATP were adopted by the Commission at the March 2022 meeting. The sixth cycle of the ATP covers fiscal years 2024-25 through 2026-27. In June 2022, Governor Newsom approved AB 180, which amended the Budget Act of 2021 and appropriated \$1.05 billion in a one-time General Fund augmentation for ATP. This additional funding was captured in the Amended 2023 ATP Fund Estimate adopted by the Commission at the August 2022 meeting. The 2025 ATP guidelines and Fund Estimate were adopted by the Commission at the March 2024 meeting. This seventh cycle of the ATP covers fiscal years 2025-26 through 2028-29. In June 2024, the Governor signed the Budget Act of 2024 and accompanying trailer bill reducing the \$1.05 billion augmentation by \$600 million but restored \$100 million in each of fiscal years 2024-25 and 2025-26 (for a total of \$200 million being restored). An Amended 2025 ATP Fund Estimate, which reflects these changes in state funds, was adopted by the Commission at the August 2024 meeting. The Statewide and Small Urban & Rural components of the 2025 ATP were adopted by the Commission at the December 2024 meeting.

Recommendations

The Department will continue to monitor ATP resources and advise the Commission of any changes or issues.

TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM (TIRCP)

Fund	Allocation Capacity	Allocations to Date	Adjustments	Net Allocations	Remaining Capacity
Cap-and-Trade	\$1,717	\$190	\$0	\$190	\$1,527
PTA	1,046	198	0	198	848
General Fund	403	57	0	57	347
Total	\$3,166	\$445	\$0	\$445	\$2,722

Notes: Amounts may not sum to totals due to independent rounding.
\$ in millions

Capital Allocations vs. Capacity

Through the third quarter, the Commission has allocated \$445 million toward 70 TIRCP projects. This leaves approximately \$2.7 billion (86 percent) in remaining allocation capacity. The Altamont Corridor Express (ACE) Expansion – Lathrop to Merced project, which is not allocated by the Commission and not displayed in the table above, represents an additional \$3.5 million in allocations through the third quarter.

TIRCP projects are typically very large capital projects with a combination of various funding sources in addition to TIRCP funding. The projects require several years to develop and bring to allocation. The complexity of bringing together multiple fund sources for projects can also impact the timing of project allocations. Remaining capacity is due to multi-year funding commitments for programmed projects, including the ACE Train Expansion project.

Outlook for Funding & Allocations

The TIRCP receives approximately 10 percent of the annual proceeds earned by the state through Cap-and-Trade auctions. Annual proceeds are determined by the number of state-owned emission allowances sold at quarterly auctions held in August, November, February, and May. The TIRCP also receives more stable funding through Transportation Improvement Fee (TIF) revenue deposited in the PTA. The TIF is indexed to inflation and adjusted each year in January.

Recommendations

TIRCP revenues are subject to change and are dependent in-part on sales at Cap-and-Trade auctions. The Department will monitor the program and, if necessary, will recommend modifications to the Commission.

PROPOSITION 1A & 1B BONDS

Fund	Allocation Capacity	Allocations to Date	Remaining Capacity
Proposition 1A	\$51	\$0	\$51
CMIA	0	0	0
TCIF	0	0	0
Intercity Rail	3	0	3
Local Bridge Seismic	11	8	3
Grade Separations ¹	0	0	0
Traffic Light Synchronization	0	0	0
Route 99	0	0	0
Short-Line Railroad	1	0	1
Total	\$66	\$8	\$58

Notes: Amounts may not sum to totals due to independent rounding.

\$ in millions

¹Highway-Railroad Crossing Safety Account (HRCSA)

Capital Allocations vs. Capacity

The Commission allocated \$8 million toward three projects through the third quarter, leaving approximately \$58 million (88 percent) in remaining allocation capacity.

Outlook for Funding & Allocations

In the third quarter of 2024-25, the Department received no upfront proceeds for either Proposition 1A or Proposition 1B from the sale of General Obligation (GO) bonds. The State Treasurer's Office issued \$40 million in Commercial Paper (CP) between January 2025 through March 2025.

Through the third quarter of 2024-25, the Department has been issued approximately \$2.8 billion in CP for Proposition 1A and Proposition 1B projects, of which all but \$550 million has been refunded with GO bond proceeds.

Proposition 1B authority is available for allocation in 2024-25, plus estimated authority in future years. These amounts largely consist of authority for the use of potential savings consistent with the Proposition 1B savings policy adopted by the Commission in January 2014. Original allocations are nearly complete for all programs except the Local Bridge Seismic Retrofit Account program, which will continue to make original allocations for several more years. A remaining amount of \$51 million is available for allocation on Proposition 1A connectivity projects.

Recommendations

The Department will continue to monitor Bond resources and advise the Commission of any concerns or changes.

APPENDICES

Appendix A Allocation Capacity and Assumptions

Appendix B Authorized Changes

Appendix C Cash Forecasts

**Forecast Methodology
State Highway Account
Public Transportation Account
Traffic Congestion Relief Fund
Road Maintenance and Rehabilitation Account
Trade Corridor Enhancement Account**

Appendix D Federal Emergency Relief Projects

Appendix E Transportation Loans

Status of Outstanding Transportation Loans, as of March 31, 2025

APPENDIX A – ALLOCATION CAPACITY AND ASSUMPTIONS

2024-25 Final Allocation Capacity By Fund and Program (\$ in millions)

Fund	SHOPP	STIP	AERO	LPP	SCCP	TCEP	ATP	TIRCP	BONDS	TOTAL
SHA	\$150	\$406	\$0	\$0	\$390	\$0	\$425	\$0	\$0	\$1,371
FTF	2,450	200	0	0	0	124	325	0	0	3,099
RMRA	2,792	0	0	401	0	0	334	0	0	3,527
TCEA	0	0	0	0	0	753	0	0	0	753
PTA	0	205	0	0	0	0	0	1,046	0	1,251
AERO	0	0	4	0	0	0	0	0	0	4
General Fund	0	0	0	0	0	0	73	403	0	477
Cap and Trade	0	0	0	0	0	0	0	1,717	0	1,717
Proposition 1A Bonds ¹	0	0	0	0	0	0	0	0	51	51
Proposition 1B Bonds ¹	0	0	0	0	0	0	0	0	15	15
Total Capacity	\$5,392	\$811	\$4	\$401	\$390	\$877	\$1,157	\$3,166	\$66	\$12,263

Note: Amounts may not sum to totals due to independent rounding.

¹Subject to bond sales.

The 2024-25 allocation capacity of approximately \$12.3 billion is based by program on the following:

- The SHOPP SHA, FTF, and RMRA allocation capacities are based on:
 - The 2024-25 Enacted Budget revenue and expenditure estimates,
 - The 2024 STIP Fund Estimate federal receipts and August Redistribution estimates,
 - The SHA prudent cash balance of \$415 million,
 - 2023-24 carryover allocation capacity,
 - SB 1 projections provided by the Department of Finance (DOF)
- The STIP SHA, FTF, and PTA allocation capacities are based on:
 - The 2024-25 Enacted Budget revenue and expenditure estimates,
 - The 2024 STIP Fund Estimate federal receipts and August Redistribution estimates,
 - The SHA prudent cash balance of \$415 million,
 - The PTA prudent cash balance of \$300 million,
 - 2023-24 carryover allocation capacity
- The AERO capacity is based on the 2024 AERO Program Fund Estimate, and 2024-25 Enacted Budget revenue and transfers.
- The LPP capacity is based on the statutory allocation of \$200 million from the RMRA, and approximately \$201 million in carryover capacity.
- The SCCP capacity is based on the statutory allocation of \$250 million from the SHA, and approximately \$140 million in carryover capacity.
- The TCEP capacity is based on the 2024-25 Enacted Budget revenue, estimated Federal funds and 2023-24 carryover capacity.

- The ATP capacity is based on the Amended 2023 ATP Fund Estimate. The ATP capacity also incorporates the following assumptions:
 - State and federal resources are forecasted to remain stable,
 - SB 1 statutory allocation of \$100 million from RMRA,
 - 2023-24 carryover allocation capacity,
 - Net reduction of \$500 million in state funds resulting from changes in the Budget Act, which includes the restoration of \$100 million in General Fund for 2024-25.
- The TIRCP capacity is based on the following assumptions:
 - 2024-25 Enacted Budget revenue in the PTA,
 - Projected Cap-and-Trade proceeds,
 - 2023-24 TIRCP carryover allocation capacity,
 - Reduction due to delays in funding resulting from changes in the Budget Act.
- Bond capacity is based on remaining bond authority, budget authority, and any administrative costs.
 - Proposition 1A and 1B capacities are based on the 2024-25 Enacted Budget and include 2023-24 remaining authority. The bond capacities are also dependent on the sale of sufficient bonds for funding.
 - Allocation capacities for the Highway-Railroad Crossing Safety Account (HRCSA), SHOPP, State Route 99, and TCIF are a result of project close-out and administrative savings.
 - Capacities for Inter-City Rail and the Local Bridge Seismic Retrofit Account (LBSRA) are based on the allocation of originally programmed projects.

APPENDIX B – AUTHORIZED CHANGES

2024-25 Authorized Changes Summary through March 31, 2025 (\$ in millions)

Program	Increase Count	Increase \$	Decrease Count	Decrease \$	Total Count ³	Net \$ Change
SHOPP ¹	75	\$80.1	135	-\$238.5	210	-\$158.4
STIP ²	1	0.3	5	-0.9	6	-0.6
TOTAL	76	\$80.4	140	-\$239.4	216	-\$159.0

Note: Amounts may not sum to totals due to independent rounding.

¹Includes SHOPP G-12s, Proposition 1B Bond G-12s (SHOPP Augmentation) adjustments.

²Includes STIP G-12 and Proposition 1B Bond G-12 (TFA) adjustments.

³May include net zero adjustments.

Summary of Authorized Changes

SHOPP. The Department processed 75 project increases, totaling \$80.1 million (average of \$1.1 million per authorization). The Department also processed 135 project decreases, totaling negative \$238.5 million (average of negative \$1.8 million per authorization). The net change of the 210 allocation adjustments represents a savings of approximately \$158.4 million through the third quarter.

STIP. The Department processed one project increase for \$297,000. The Department also processed five project decreases, totaling negative \$910,000. The net change of the six allocation adjustments represents a savings of approximately \$613,000.

Background

Commission Resolution G-22-12, which amends Resolution G-19-12, (Resolution G-12) allows for the Director of the Department to adjust project allocations within specific limits. It is intended that the Director's approved "decreases" will offset the Director's approved "increases." These authorized changes are known as G-12 authority. This delegation of authority greatly reduces the volume of financial transactions submitted to the Commission and increases the efficiency of the Department in processing changes. Resolution G-12 requires that the Department report on all project capital outlay allocation changes made under this delegation to the Commission's Executive Director on a monthly basis. The Department provides a detailed, project by project, report to Commission staff each month.

APPENDIX C – CASH FORECASTS – FORECAST METHODOLOGY

Methodology and Assumptions

The 24-month cash forecasts for the SHA, PTA, TCRF, RMRA and TCEA are used by the Department to estimate and monitor the cash balance of transportation funds, to determine the level of allocations that can be supported, and to prepare for low or high cash periods. Variances are identified and reported to management and the Commission. If necessary, adjustments are made to capital allocation levels, funding policy, or forecast methodology.

The 2024-25 cash forecasts were based on the following assumptions:

- State Operations projections are based on historical trends and assume an approximately 2.9 percent increase each year, based on the DOF's Price Letter.
- Includes the most current expenditure projections available for Right-of-Way, SHOPP and STIP.
- Capital Outlay and Local Assistance expenditures are based on actual and projected Commission allocations using historical and seasonal construction patterns.
- Monthly adjustments are differences between the Department's accounting system and the State Controller's Office (SCO). These adjustments include short-term loans made to the General Fund (GF), short-term loan repayments, Plans of Financial Adjustments, funds transferred in and out, and reimbursements.
- Annually adjusted contributions to the Transportation Revolving Account.

SHA

- Capacity levels established in the most recent STIP Fund Estimates encouraged the expeditious planning and delivery of projects. The cash balance is anticipated to reflect the ongoing project delivery over the 24-month period and beyond.
- SB 1 provided significant increases to revenue to fund capital outlay transportation programs such as SHOPP.
- Weight fee and excise tax revenue projections provided by the DOF.
- All other revenues are based on historical trends.
- Continued monthly transfers of weight fee revenues to the Transportation Debt Service Fund (TDSF).
- Prudent cash balance of \$415 million.

PTA

- PTA balances reflect the ongoing TIRCP projects, which are typically large, include other funding sources in addition to PTA, and require several years to develop and bring forward for allocation. Consequently, there is a lag before TIRCP projects result in significant cash outflows. Minor fluctuations to the cash balance are expected until TIRCP project spending accelerates, which is anticipated to occur beyond the 24-month period.
- Projected TIRCP programming estimates by the Department's Planning and Modal Program.
- Revenue projections provided by the DOF.
- Prudent cash balance of \$300 million.

TCRF

- No further allocation capacity is available due to cessation of the Traffic Congestion Relief Program pursuant to SB 1. Programmed projects will continue to expend over the 24-month period based on historical patterns.
- Prudent cash balance of \$5 million.

RMRA

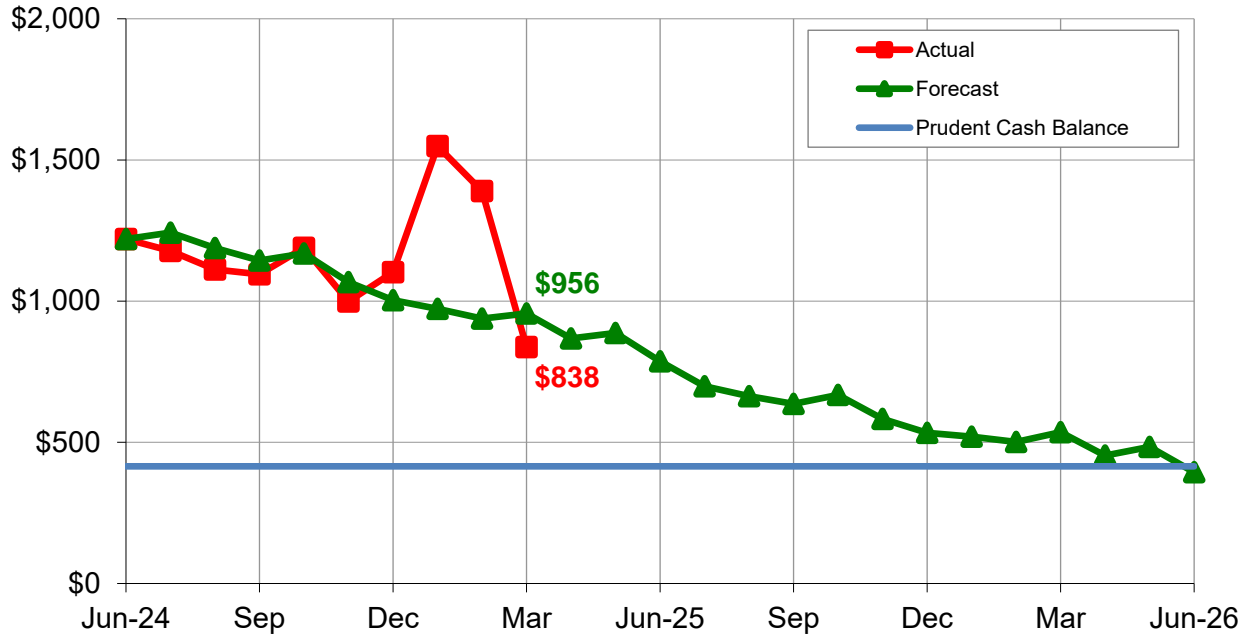
- Capacity levels established in the most recent STIP Fund Estimates accelerated the development and planned delivery of projects, many of which will enter phases requiring increased spending during the 24-month period.
- SB 1 provided significant increased revenue to support new and more recently adopted transportation programs in addition to providing additional funding for programs such as SHOPP.
- Includes projected allocations of emergency SHOPP projects.
- Revenue projections based on data provided by the DOF.
- Prudent cash balance of \$400 million.

TCEA

- Represents state funding for the TCEP, which was created through the passage of SB 1. Programming and allocation capacity are based on the anticipated receipt of revenue each fiscal year, which is expected to result in an increased cash balance over the 24-month period.
- Revenue projections provided by the DOF.
- Forecasted expenditures are based on historical spending patterns.
- Prudent cash balance of \$50 million.

Appendix C – Cash Forecasts – State Highway Account

State Highway Account (SHA) 24-Month Cash Forecast (\$ in millions)



Year-to-Date SHA Summary

The SHA ending cash balance through the third quarter was approximately \$838 million, \$119 million (12 percent) below forecast. The variance is primarily due to adjustments, which represent timing differences between the Department's accounting system and the SCO's accounting system, that were lower than forecast. The sharp decrease in the March fund balance is due to timing of expenditures from the preceding months that were delayed into March. Revenues totaled \$1.3 billion, \$21 million (2 percent) below forecast. Transfers, which primarily comprise of incoming transfers of excise tax revenue, totaled approximately \$2.3 billion, \$204 million (10 percent) above forecast. Expenditures totaled \$4.3 billion, \$63 million (2 percent) above forecast. Adjustments were \$338 million.

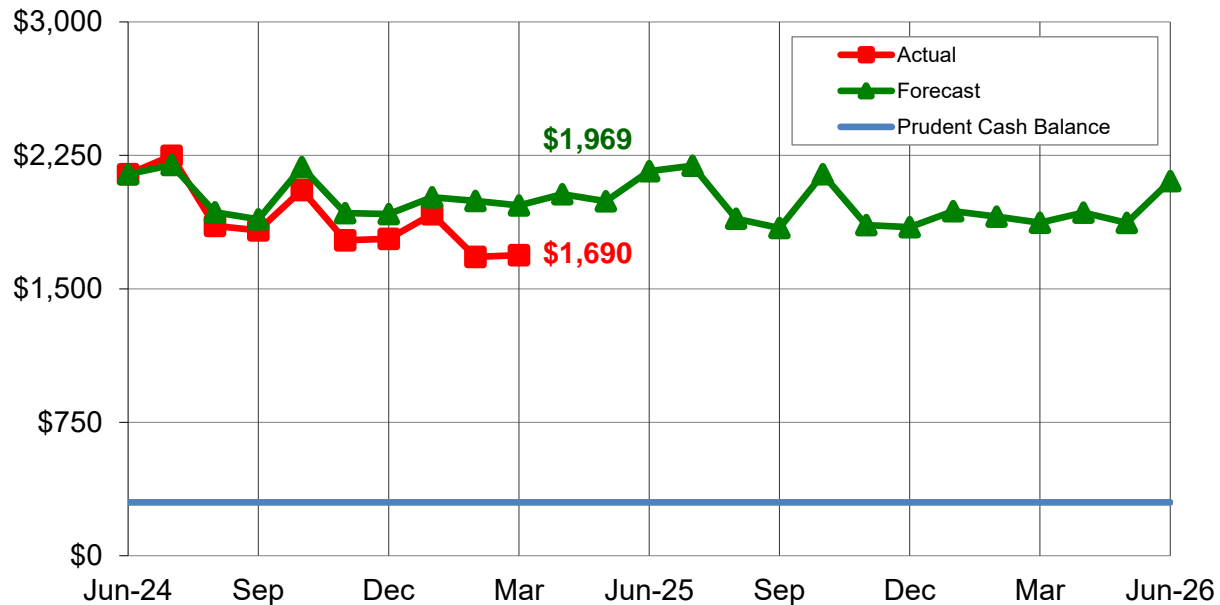
Year-to-Date Reconciliation (\$ in millions)

	Forecast	Actual	Difference	%
Beginning Cash Balance	\$1,220	\$1,220	N/A	
Revenues	1,306	1,285	-21	
Transfers	2,070	2,273	204	
Expenditures	-4,215	-4,278	63	
Adjustments	576	338	-238	
Ending Cash Balance	\$956	\$838	-\$119	-12%

Notes: Amounts may not sum to totals due to independent rounding.

Appendix C – Cash Forecasts – Public Transportation Account

Public Transportation Account (PTA) 24-Month Cash Forecast (\$ in millions)



Year-to-Date PTA Summary

The PTA ending cash balance through the third quarter was approximately \$1.7 billion, \$279 million (14 percent) below forecast. The variance is due to expenditures that were greater than forecast. Revenues totaled approximately \$976 million, \$69 million (7 percent) below forecast. Transfers totaled \$25 million through the third quarter. Expenditures were approximately \$1.7 billion, \$393 million (29 percent) above forecast. Adjustments, which represent timing differences between the Department's accounting system and the SCO's accounting system, were \$289 million.

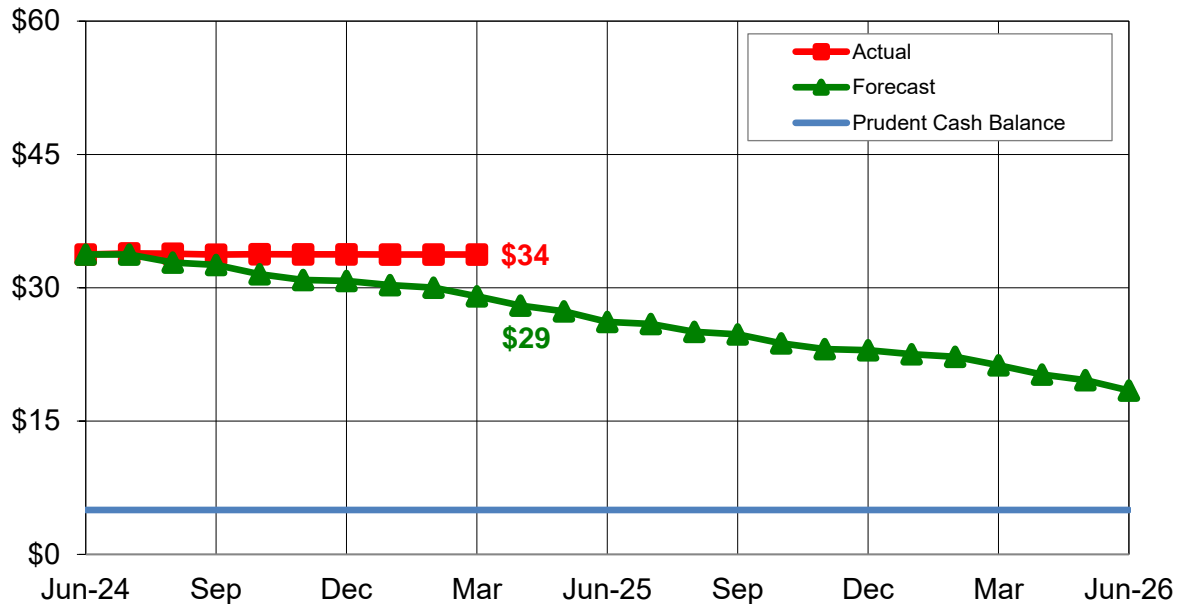
Year-to-Date Reconciliation (\$ in millions)

	Forecast	Actual	Difference	%
Beginning Cash Balance	\$2,144	\$2,144	N/A	
Revenues	1,045	976	-69	
Transfers	18	25	8	
Expenditures	-1,352	-1,745	-393	
Adjustments	114	289	176	
Ending Cash Balance	\$1,969	\$1,690	-\$279	-14%

Notes: Amounts may not sum to totals due to independent rounding.

Appendix C – Cash Forecasts – Traffic Congestion Relief Fund

Traffic Congestion Relief Fund (TCRF) 24-Month Cash Forecast (\$ in millions)



Year-to-Date TCRF Summary

The TCRF ending cash balance through the third quarter was \$34 million, \$5 million (16 percent) above forecast. Expenditures were \$5 million below forecast. Adjustments, which represent timing differences between the Department's accounting system and the SCO's accounting system, were zero.

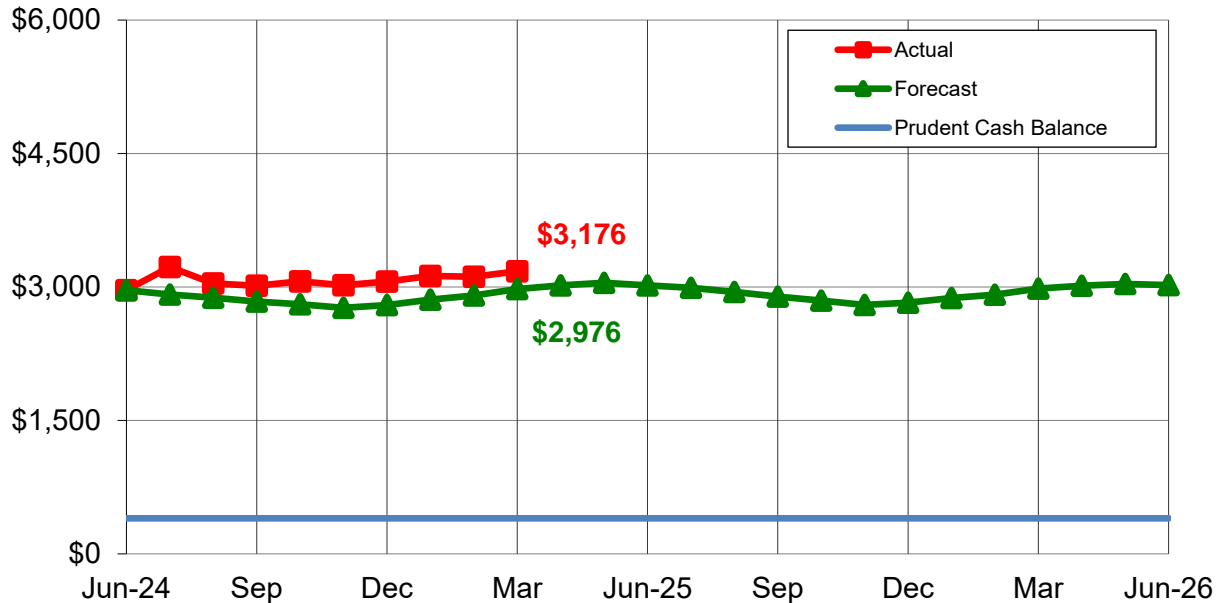
Year-to-Date Reconciliation (\$ in millions)

	Forecast	Actual	Difference	%
Beginning Cash Balance	\$34	34	N/A	
Revenues	0	0	0	
Transfers	0	0	0	
Expenditures	-5	0	-5	
Adjustments	0	0	0	
Ending Cash Balance	\$29	34	5	16%

Note: Amounts may not sum to totals due to independent rounding.

Appendix C – Cash Forecasts – Road Maintenance and Rehabilitation Account

Road Maintenance and Rehabilitation Account (RMRA) 24-Month Cash Forecast (\$ in millions)



Year-to-Date RMRA Summary

The RMRA ending cash balance through the third quarter was approximately \$3.2 billion, \$201 million (7 percent) above the forecast. The variance is primarily due to expenditures that were lower than forecast. Revenues totaled approximately \$1.5 billion, \$12 million (1 percent) above forecast. Transfers totaled \$1.8 billion, \$43 million (3 percent) above forecast. Expenditures were \$2.9 billion, \$209 million (7 percent) below forecast. Adjustments, which represent timing differences between the Department's accounting system and the SCO's accounting system, were negative \$169 million through the third quarter.

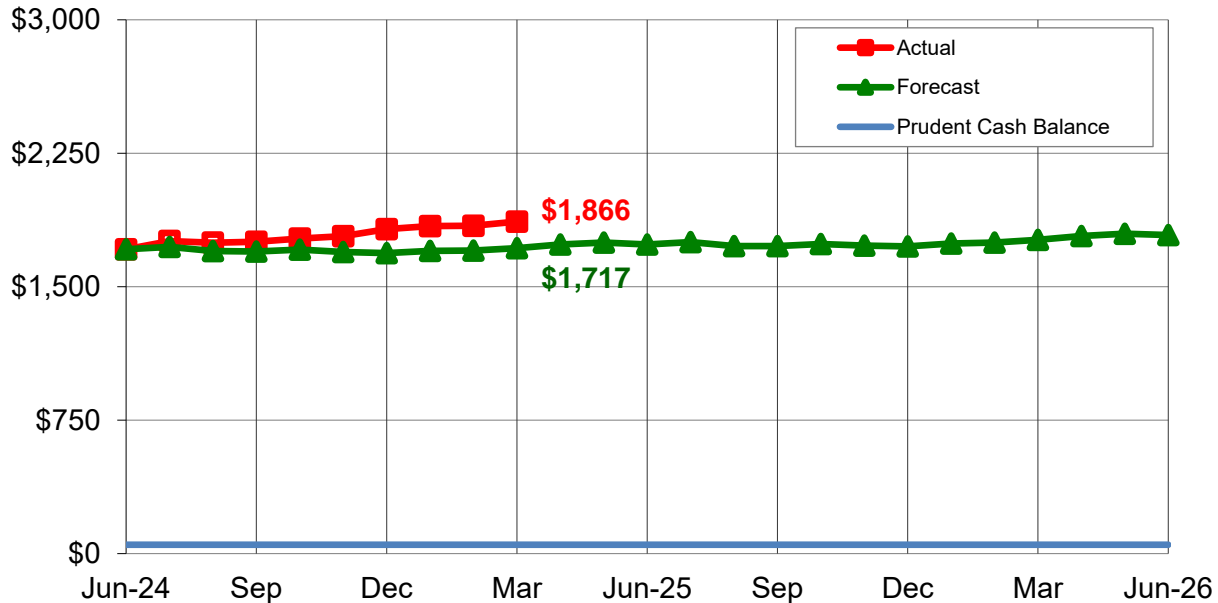
Year-to-Date Reconciliation (\$ in millions)

	Forecast	Actual	Difference	%
Beginning Cash Balance	\$2,965	\$2,965	N/A	
Revenues	1,512	1,524	12	
Transfers	1,706	1,750	43	
Expenditures	-3,102	-2,893	209	
Adjustments	-106	-169	-63	
Ending Cash Balance	\$2,976	\$3,176	\$201	7%

Notes: Amounts may not sum to totals due to independent rounding.

Appendix C – Cash Forecasts – Trade Corridor Enhancement Account

Trade Corridor Enhancement Account (TCEA) 24-Month Cash Forecast (\$ in millions)



Year-to-Date TCEA Summary

The TCEA ending cash balance through the third quarter was approximately \$1.9 billion, \$150 million (9 percent) above the forecast balance. The variance is due to expenditures that were lower than forecast. Through the third quarter, revenues totaled \$58 million, \$14 million (32 percent) above the forecast. Transfers, which are deposits of diesel excise tax revenue, were \$311 million, \$16 million (5 percent) above the forecast. Expenditures were \$207 million, \$120 million (37 percent) below forecast. Adjustments, which represent the timing differences between the Department's accounting system and the SCO's accounting system, were negative \$7 million.

Year-to-Date Reconciliation (\$ in millions)

	Forecast	Actual	Difference	%
Beginning Cash Balance	\$1,710	\$1,710	N/A	
Revenues	44	58	14	
Transfers	295	311	16	
Expenditures	-326	-207	120	
Adjustments	-7	-7	0	
Ending Cash Balance	\$1,717	\$1,866	\$150	9%

Notes: Amounts may not sum to totals due to independent rounding.

APPENDIX D – FEDERAL EMERGENCY RELIEF PROJECTS

The Federal Highway Administration (FHWA) Emergency Relief Program allocates \$100 million on an annual basis to fund repairs and reconstruction of eligible highway damage resulting from declared disasters nationwide. Congress has periodically provided additional funds for the program through supplemental appropriations. The table below represents the remaining Emergency Relief (ER) funding need for approved California disasters. The need for the next 0-6 months is the basis for the formal request of ER funding made to the FHWA. Columns for “Total Federally Eligible Cost of Event” and “ER Funds Previously Allocated” have been included to reflect standard reporting submitted to the FHWA.

EVENT (\$ in thousands)	Total Federal Eligible Cost of Event	ER Funds Previously Allocated	Net Allocation Needs Next 0-6 Months	Net Allocation Needs Beyond 6 Months
CA06-1 December 2005 Statewide Storms	\$587,309	\$578,139	\$9,171	\$0
CA15-2 December 2014 Statewide Storm Damage	\$102,480	\$98,976	\$3,064	\$440
CA16-4 March 5, 2016 Storms	\$117,627	\$115,035	\$2,592	\$0
CA17-1 December 9, 2016 Storms	\$90,357	\$62,540	\$287	\$27,530
CA17-2 January 6, 2017 Storms	\$999,005	\$948,997	\$17,682	\$32,326
CA18-1 October 2017 Wildfires	\$19,873	\$10,082	\$0	\$9,790
CA18-2 October 2017 Napa Fire Complex	\$3,868	\$528	\$0	\$3,341
CA18-3 December 2017 Thomas, Creek and Rye, Lilac Fire and Mudslides	\$40,795	\$39,398	\$0	\$1,397
CA18-6 July 2018 Fires SB, Sis, SD Counties	\$25,274	\$17,113	\$0	\$8,161
CA19-2 January / February Winter Storms	\$275,114	\$170,686	\$16,046	\$88,382
CA19-3 July 2019 Earthquakes	\$709	\$706	\$3	\$0
CA20-2 August 2020 Wildfires	\$81,322	\$75,614	\$1,478	\$4,230
CA20-3 September 2020 Wildfires	\$28,848	\$27,361	\$1,487	\$0
CA21-1 January '21 Winter Storms	\$64,663	\$35,760	\$0	\$28,903
CA21-2 Summer '21 Wildfires	\$1,073	\$56	\$0	\$1,017
CA22-2 October '21 Winter Storms	\$7,156	\$4,515	\$635	\$2,006
CA22-3 December '21 Winter Storms	\$16,086	\$2,347	\$4,903	\$8,836
CA22-4 January '22 Colorado Fire	\$1,617	\$1,562	\$54	\$0
CA23-1 December '22 Storms	\$342,812	\$72,159	\$70,549	\$200,105
CA23-2 December '22 Earthquake	\$4,622	\$939	\$199	\$3,484
CA23-3 February '23 Winter Storms	\$244,483	\$28,906	\$28,103	\$187,474
CA23-4 Tropical Storm Hilary	\$46,250	\$9,099	\$14,569	\$22,582
CA24-1 November '23 LA I-10 Structure Fire	\$20,410	\$3,000	\$19,664	\$0
CA24-2 December 2023 Winter Storms	\$12,539	\$0	\$4,322	\$8,216
CA24-3 February 2024 Winter Storms	\$190,813	\$9,700	\$0	\$181,113
CA24-4 March 2024 Winter Storms	\$45,564	\$0	\$1,017	\$44,548
CA24-5 July 2024 Wildfires	\$1,817	\$0	\$0	\$1,817
TOTAL	\$3,372,484	\$2,313,219	\$195,824	\$865,696

Future federal ER funding of this type can only be used to fund emergency-relief projects and does not represent new capacity, except to the extent that the SHA funds have already been advanced for the emergency projects.

APPENDIX E – TRANSPORTATION LOANS

Status of Outstanding Transportation Loans, as of March 31, 2025 (\$ in millions)

FUND	Original Loan	Loans / Interest Paid-to-Date	Remaining Balance
High-Speed Passenger Train:			
2013-14 Public Transportation Account (PTA) ¹	\$23	\$0	\$23
2014-15 Public Transportation Account (PTA) ¹	31	0	31
Subtotal High-Speed Passenger Train Loans:	\$54	\$0	\$54
Totals:	\$54	\$0	\$54

Note: Amounts may not sum to totals due to independent rounding.

¹Repayment will occur when the PTA is determined to be in need of the funds or when the High-Speed Passenger Train Bond Fund no longer needs the funds.

High-Speed Passenger Train Loans

The 2013-14 Budget Act authorized up to \$26 million in loans from the PTA to the High-Speed Passenger Train Bond Fund to cover support costs incurred by the High-Speed Rail Authority. During 2013-14, a total of \$23 million was loaned, consisting of \$5.4 million in August 2013, \$8.9 million in October 2013, \$5.6 million in March 2014, and \$3 million in June 2014. The 2014-15 Budget Act authorized additional loans of up to \$31.6 million for support costs incurred in 2014-15, including an initial authorization of \$29.3 million and an additional authorization of \$2.3 million. During 2014-15, a total of \$30.9 million was loaned, consisting of \$7.3 million in September 2014, \$7.3 million in December 2014, \$7.3 million in February 2015, \$2.3 million in March 2015, and \$6.7 million in May 2015. No additional loans are anticipated to occur. Pursuant to statute, loans will be repaid when the PTA is determined to be in need of the funds or when the High-Speed Rail Authority no longer needs the funds.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.6, Information Item

Prepared By: René Fletcher, Chief (Acting)
Division of Right of Way and Land Surveys

Subject: **QUARTERLY REPORT – FISCAL YEAR 2024-25 RIGHT OF WAY ANNUAL ALLOCATION – THIRD QUARTER**

SUMMARY:

As of April 1, 2025, approximately \$49.7 million, or 43 percent, of the \$115.4 million allocated by the California Transportation Commission (Commission) for Right of Way (RW) Capital has been committed on State Highway Operation and Protection Program (SHOPP) and State Transportation Improvement Program (STIP) projects. The California Department of Transportation (Department) plans to spend \$46.7 million for the remainder of the fiscal year. The forecasted balance at the end of the fiscal year is \$19 million.

The annual RW Capital Plan approved by the Commission on June 28, 2024, under Resolution FM-23-04, consisted of 548 projects. Funds have been committed on 258, or approximately 47 percent, of the previously identified projects. The RW Capital Plan has been amended to include 147 additional projects that required 2024-25 allocation.

BACKGROUND:

The Department's Division of Right of Way and Land Surveys administers the RW Capital budget under delegated authority from the Commission. The Commission provides an annual allocation consistent with each fiscal year's Budget Act, approved STIP and SHOPP documents, and the RW Capital Plan. The RW Capital Plan includes RW capital outlay costs, plus the specific categories of costs during the Project Approval and Environmental Document (PA&ED) phase, costs exceeding programming, and inverse condemnation costs. The Commission further delegates to the Department the authority to adjust allocation between projects and expenditure categories.

Per Resolution G-19-01, the Department is required to report on a quarterly basis the status of the RW Capital Plan, including a list of the projects added, subtotals of the specific categories, and the balance of the annual allocation.

It is forecasted that \$19 million of the annual allocation balance will not be committed by June 30, 2025. Revised estimates for mitigation capital needs, along with updated right of way impacts and schedules, account for the forecasted balance.

FISCAL YEAR 2024-25 RW CAPITAL ANNUAL ALLOCATION SUMMARY
(Dollars x 1,000)

Capital Projects	Annual Allocation per CTC Resolution FM-23-04	Annual Allocation Encumbered or Expended as of 04/01/2025	Annual Allocation Balance¹
STIP Capital Costs	\$14,449	\$8,991	\$5,458
STIP Costs During the PA&ED Phase ^{2 3}	\$28	\$1	\$27
STIP Costs Exceeding Programming ³	\$9,552	\$2,834	\$6,718
STIP Inverse Costs ³	\$0	\$20	(\$20)
Subtotal STIP	\$24,029	\$11,846	\$12,183
SHOPP Capital Costs	\$79,461	\$24,187	\$55,274
SHOPP Costs During the PA&ED Phase ^{2 3}	\$4,564	\$416	\$4,148
SHOPP Costs Exceeding Programming ³	\$7,127	\$8,106	(\$979)
SHOPP Inverse Costs ³	\$249	\$5,127	(\$4,878)
Subtotal SHOPP	\$91,401	\$37,836	\$53,565
Total	\$115,430	\$49,682	\$65,748

A copy of the detailed [RW Capital Plan](#) is available online.

¹ The forecasted Annual Allocation Balance includes the following fund source adjustments: \$5 million to State STIP funds 25301-0042, \$2.5 million to Federal STIP funds 25301-0890, \$9.5 million to State SHOPP funds 25302-0042, and \$2 million to Federal SHOPP funds 25302-0890.

² As of April 1, 2025, expenditures and encumbrances made during the PA&ED phase total less than 1 percent of the approved allocation. Resolution G-19-01 limits these commitments to 5 percent of the total Annual Allocation.

³ The 2024 STIP Fund Estimate identifies a \$16 million fund reservation for unprogrammed RW Capital costs. As of April 1, 2025, \$16 million has been committed. Approximately \$2.3 million will be reclassified as programmed costs after STIP share adjustments are made at the time of Construction Contract Acceptance.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.7, Information Item

Prepared By: Tarek Tabshouri, Chief
Division of Aeronautics

Subject: **FISCAL YEAR 2024-25 THIRD QUARTER REPORT – AERONAUTICS
ACQUISITION AND DEVELOPMENT PROGRAM AND AIRPORT IMPROVEMENT
PROGRAM**

SUMMARY:

The attached report is the California Department of Transportation's (Department), Division of Aeronautics (Aeronautics) Fiscal Year 2024-25 Third Quarter Report for the Acquisition and Development (A&D) and Airport Improvement Program (AIP) Matching Grant Programs, which will be presented as an informational item at the California Transportation Commission's (Commission) June 2025 meeting.

BACKGROUND:

The Department's A&D and AIP Matching Grants Programs are funded by the Aeronautics Account in the State Transportation Fund. They are prepared in accordance with the California Public Utilities Code (PUC), Sections 21683 and 21706.

The PUC provides that the Department, upon allocation by the Commission, may provide a matching grant to a public entity up to five percent of the amount of a Federal AIP Grant. Each year the Commission approves an annual allocation to match AIP grants. This allocation provides the authority for the Department to subvent matching funds to individual projects as requested by airport sponsors.

The Department's A&D Program is a biennial, two-year program providing State grants to airports for planning, construction, and land acquisition. A&D projects are State funded at 90 percent of the total project cost with a 10 percent local match required. The Department provides the Commission with quarterly reports on the status of all sub-allocations made for State AIP matching grant funds and the status of all projects in the A&D Program.

Attachment:

Third Quarter Report – Aeronautics Acquisition and Development Program and Airport Improvement Program Matching Grants Program



Fiscal Year 2024–25 Third Quarter Report

Division of Aeronautics

Acquisition and Development Grants
And
Airport Improvement Program Matching Grants Programs

Quarterly Report to the
California Transportation Commission

ACQUISITION AND DEVELOPMENT PROGRAM

BACKGROUND:

The California Department of Transportation's (Caltrans), Division of Aeronautics (Aeronautics) Acquisition and Development (A&D) Grants Program is a biennial, two-year program providing State grants to eligible, publicly owned, public-use airports for planning, construction, and land acquisition. The next A&D Program will be developed subsequently to the forthcoming legislative changes that allow for expanded project categories. Therefore, there are currently no programmed projects in the A&D Program.

FEDERAL AIRPORT IMPROVEMENT PROGRAM MATCHING GRANTS

BACKGROUND:

Caltrans' Aeronautics' Airport Improvement Program (AIP) Matching Grants Program (Program) is funded by the Aeronautics Account in the State Transportation Fund. It is prepared in accordance with the California Public Utilities Code (PUC), Sections 21683 and 21706.

Section 21683.1 of the PUC provides that Caltrans, upon allocation by the California Transportation Commission (Commission), may provide a matching grant to a public entity up to five percent of the amount of a federal AIP grant.

Each year the Commission approves an annual allocation to match Federal Aviation Administration (FAA) AIP grants. This allocation provides the authority for Caltrans to subvent matching funds to individual projects as requested by airport sponsors.

Caltrans provides the Commission with quarterly reports on the status of all sub-allocations made for State AIP Matching grant funds. It should be noted that the Aeronautics Account is a continuously appropriated account, and any unused funds revert to the Aeronautics Account for use in future fiscal years.

PROGRAM SUMMARY:

At its June 2024 meeting, the Commission allocated \$2.5 million for the 2024–25 Program. To assist as many airports as possible, a \$150,000 maximum per project is in effect for the current Program. A list of awarded grants and anticipated awarded grant are included in the following pages of this report. As of the third quarter of 2024–25, Caltrans has sub-allocated \$996,815 to 20 projects as detailed on the following spreadsheet:

Federal AIP Matching Grants

Airport	Sponsor	Project Description	Dist	State Grant Date	FAA Grant Amount	State Match Amount	Total Project Cost
Napa	County of Napa	Reconstruct Taxiway K	4	8/29/2024	\$4,392,584	\$150,000	\$4,880,649
Brackett Field	County of Los Angeles	Rehabilitate Runway 8L/26R, Phase 1 Design and Rehabilitate Taxiway N and cross taxiways A, C, E, F, and G.	7	8/31/2024	\$1,384,192	\$69,210	\$1,537,991
Nut Tree	County of Solano	Northeast Taxilanes with Drainage Improvements, Phase 2A	4	9/16/2024	\$2,607,085	\$130,354	\$2,896,762
Blythe	County of Riverside	Installation of Primary Apron Lighting and Control Access Gate, Phase I Design	8	9/13/2024	\$225,000	\$11,250	\$250,000
Camarillo	County of Ventura	Reconstruct Runway 8/26, Phase 1 Design	7	9/13/2024	\$1,830,218	\$91,511	\$2,033,576
Half Moon Bay	County of San Mateo	Acquire and Install Electrical Emergency Generator	4	9/17/2024	\$1,200,000	\$60,000	\$1,403,765
Redlands	City of Redlands	Resurface Runway 08/26	8	9/24/2024	\$750,000	\$37,500	\$833,333
Lake Tahoe	City of South Lake Tahoe	Crack Seal/Seal Coat Parking Apron Phase 1	3	9/27/2024	\$196,813	\$9,841	\$218,681
Riverside	City of Riverside	Reseal Taxiway Pavement (Resurface Twy A, Phase II - construction)	8	10/3/2024	\$791,604	\$39,580	\$879,560
Red Bluff	City of Red Bluff	Rehabilitate South East Apron & Drainage Improvement	2	10/9/2024	\$356,008	\$17,800	\$395,564
Shafter-Minter Field	Minter Field Airport District	Reseal Taxiway Pavement A1, A2, A3 & A4, Reseal Taxiway Twy A, Reseal/Resurface Runway 12-30 Pavement	6	10/9/2024	\$1,089,857	\$54,493	\$1,210,952
Tulelake	County of Modoc	National Environmental Policy Act, Environmental Assessment, Phase IV for Proposed Wildlife Perimeter Fencing	2	10/18/2024	\$465,255	\$23,263	\$516,950
Placerville	County of El Dorado	Install AWOS, Phase 1 - Design	3	10/16/2024	\$69,300	\$3,465	\$77,000
Oceanside	City of Oceanside	Rehabilitate Runway Lighting and Vault Building Phase I - Design	11	10/24/2024	\$136,260	\$6,813	\$151,400
Rogers Field	County of Plumas	Airport Layout Plan Narrative Study, Updated ALP Drawing Set, and Aeronautical Survey with Obstruction Analysis Report	2	10/30/2024	\$257,400	\$12,870	\$286,000
Gansner Field	County of Plumas	Airport Layout Plan Narrative Report with Updated ALP Drawing Set, and Aeronautical Survey with Obstruction Mitigation Plan	2	10/30/2024	\$260,100	\$13,005	\$289,000
Tracy	City of Tracy	Pavement Maintenance Management Plan	10	11/13/2024	\$126,000	\$6,300	\$140,000
Bakersfield	City of Bakersfield	Runway 16-34 Lighting Improvements, Runway 16-34 Rehabilitation, Taxiway "A" Rehabilitation - Construction	6	11/13/2024	\$2,107,944	\$105,397	\$2,342,160
Nevada County	County of Nevada	Reconstruct Access Road to Ramp 4, Reconstruct/Reseal Ramp 4 Taxilanes (Design)	3	1/3/2025	\$83,250	\$4,163	\$92,500
Van Nuys	City of Los Angeles	Reconstruct Taxiways D, E, F, and Q Phase II	7	1/13/2025	\$17,467,200	\$150,000	\$19,408,000
				Total:	\$35,796,070	\$996,815	\$39,843,843

Anticipated Federal AIP Matching Grant Awards*

Airport	Project Description	FAA Grant Amount	State Match Amount
Paso Robles	Update Airport Master Plan	\$530,610	\$26,531
Chino	Construct Taxiway, Extend/Expand Taxiway, Rehabilitate Taxiway, Reconstruct Airport Lighting Vault Equipment	\$1,361,705	\$68,085
Rohnerville	Rehabilitate Runway	\$359,279	\$17,964
Garberville	Reseal Apron Pavement, Reseal Taxiway Pavement, Reseal/Resurface Runway Pavement	\$247,037	\$12,352
	Total Anticipated Matching Grants	\$2,498,631	\$124,932
	Total Awarded Matching Grants	\$35,796,070	\$996,815
	Total Anticipated FY2024-25 AIP Matching Grant Program	\$38,294,701	\$1,121,747

*Preliminary data from the FAA website; Data and grant amounts will change as applications are processed

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.8, Information Item

Prepared By: Sujaya Kalainesan, Chief (Acting)
Division of Project Management

Subject: **CALTRANS' PROJECT DELIVERY QUARTERLY REPORT –
THIRD QUARTER FISCAL YEAR 2024-25**

SUMMARY:

The California Department of Transportation (Department) has submitted its Division of Project Management's Fiscal Year 2024-25 Project Delivery Report, for the third quarter, to the California Transportation Commission (Commission).

BACKGROUND:

The purpose of the quarterly Project Delivery Report is to provide the Commission with project delivery information on transportation projects for which the Department was fully responsible for development and construction management. The report is pursuant to the reporting requirements specified by Government Code Sections 14524.16, 14526.6, and the State Highway Operation and Protection Program (SHOPP) Guidelines Section 57.

Attachment



Third Quarter Fiscal Year 2024-25 Project Delivery Report

**Quarterly Report to the
California Transportation
Commission**



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Executive Summary

Introduction

The California Department of Transportation (Caltrans) delivers transportation capital programs that preserve, protect, and enhance performance of the state highway system. Operational improvement projects help the existing highway system function more efficiently. System preservation projects, such as bridge rehabilitation and pavement rehabilitation, help the highway system last longer and decrease maintenance costs. Safety projects reduce fatalities and serious injuries resulting from traffic accidents.

Purpose

This report provides project delivery information on transportation projects for which Caltrans was fully responsible for development and construction management.

Performance Measures

Measuring and reporting performance on project milestones shows how well Caltrans is meeting its commitments to deliver projects as promised in its primary work programs: the State Transportation Improvement Program (STIP), the State Highway Operation and Protection Program (SHOPP), and for locally funded projects where Caltrans is the implementing agency.

Project Delivery Performance Measure Summary – 3rd Quarter Fiscal Year 2024-25 ¹								
Measure	Year-To-Date thru 3rd Quarter			Annual Commitment		Year-End Projection		Last Five Fiscal Year Average
	Completed	Plan	Percent	Plan	Percent	Forecast	Percent	Percent
Project Approval, Environmental Documents								
Draft Environmental Documents Completed	32	44	73%	51	63%	46	90%	79%
Projects Approved	77	103	75%	117	66%	112	96%	89%
Right of Way: Projects Certified								
Projects Certified	87	151	58%	225	39%	213	95%	88%
Delivery: Projects Designed and Ready for Construction								
Projects Designed and Ready for Construction	64	140	46%	227	28%	218	96%	87%
Capital Value Ready for Allocation (millions) ²	\$1,732	\$3,027	57%	\$4,785	36%	\$4,571	96%	83%
Construction: Projects Constructed								
Projects Constructed	155	226	69%	267	58%	252	94%	79%
Closeout Costs								
State Transportation Improvement Program Costs (millions) ²	\$592 ³	\$667 ⁴	89%	NA	NA	NA	NA	88%
State Highway Operation and Protection Program Costs (millions) ²	\$2,438 ³	\$2,827 ⁴	86%	NA	NA	NA	NA	88%

¹ Milestone completions and Year-End projections are based on end of quarter data and are subject to change for future reporting periods. Year-End projections will vary until 4th quarter results are available after the end of June.

² Dollars are rounded to the nearest million.

³ Total support and capital expenditures.

⁴ Total support and capital approved budget including G-12 and Supplementals.

Project Watch List Summary

The Project Watch List identifies projects deemed "at risk" for budget overruns or schedule delays. Projects are continuously monitored and brought to the attention of managers and transportation stakeholders to resolve or minimize issues affecting the budget, scope, or schedule.

The watch list will change from one quarter to another (projects dropped or added) as supplemental funds are approved, budget risks are mitigated, and schedule risks are resolved. Since the report is prepared quarterly, and in order to keep projects on track to award, projects that have not been included on the watch list may require supplemental funds requests between reports. While this report is intended to reflect information at the end of the reporting period, information for narratives is updated up to the time the report is published to provide the most accurate information possible. The Watch List is current as of May 2, 2025; Appendix A shows the current Project Watch List.

Delivery and Budget Risks

Commission guidelines require Caltrans to deliver projects in the fiscal year in which Construction Capital is programmed. If there is a risk that a project will not be delivered in the programmed fiscal year, Caltrans adds the project to the Watch List. A summary of current delivery risks is provided in the table below. Project-specific delivery risks are provided in the Watch List (Appendix A).

Summary of Delivery Risks

Risk Level	Number of Projects	Programmed Budget \$ (millions)
Very High	38	\$1,502
High	27	\$662
Medium	28	\$1,017
Low	11	\$256
Total	104	\$3,437

Note: 227 projects with a programmed Construction Capital budget of \$4.785 billion are planned for delivery in Fiscal Year 2024-25.

Caltrans balances risk in project budgeting with the need to ensure that an appropriate mix of projects are brought forward in sufficient quantities to use its annual federal obligation authority and other available transportation funding effectively. Complete and reasonable estimates are necessary to avoid undesired consequences, including loss of federal or local funds. Before presenting capital or capital outlay support (COS) budget change requests to the Commission, Caltrans thoroughly examines each request to validate costs and evaluate options. A summary of current budget risks is provided in the table below.

Summary of Potential Supplemental Funds

Budget Risk Type	Projects	Programmed Budget (millions)	Estimated Risk (millions)	Potential Date
Pre-Construction – 142 of 1,044 Total Projects or 14%				
COS Supplementals	50	\$235	\$71	Within 6 months
Greater Than 120% Allocations	85	\$2,100	\$1,500	Within 6 months
Supplemental to Award	7	\$135	\$49	Within 6 months
During Construction – 71 of 775 Total Projects¹ or 9%				
COS Supplementals	37	\$153	\$43	Within 1 year
Supplementals to Complete Construction	32	\$603	\$106	Within 1 year
Partnership Projects - Local Agency Implementing Agency	2	\$35	\$10	After completion
Post-Construction – 21 of 1,046 Total Projects or 2%				
COS Supplementals	1	\$6	\$2	After completion
Supplementals to Closeout	19	\$1,100	\$70	After completion
Right of Way Adjustments	1	\$5	\$4	After completion
Total	234	\$4,372	\$1,855	
Total Risks Versus Active Projects: 234 of 2,925² or 8%				

¹ The estimated total number of STIP and SHOPP projects in Construction as of April 1, 2025, HQs Division of Construction.

² 2,925 is the total number of authorized projects as of April 1, 2025, Statewide Delivery Plan, HQs Division of Project Management.

Project Delivery Milestone Performance Report

Project Approval, Environmental Documents

Draft Environmental Documents Completed Summary

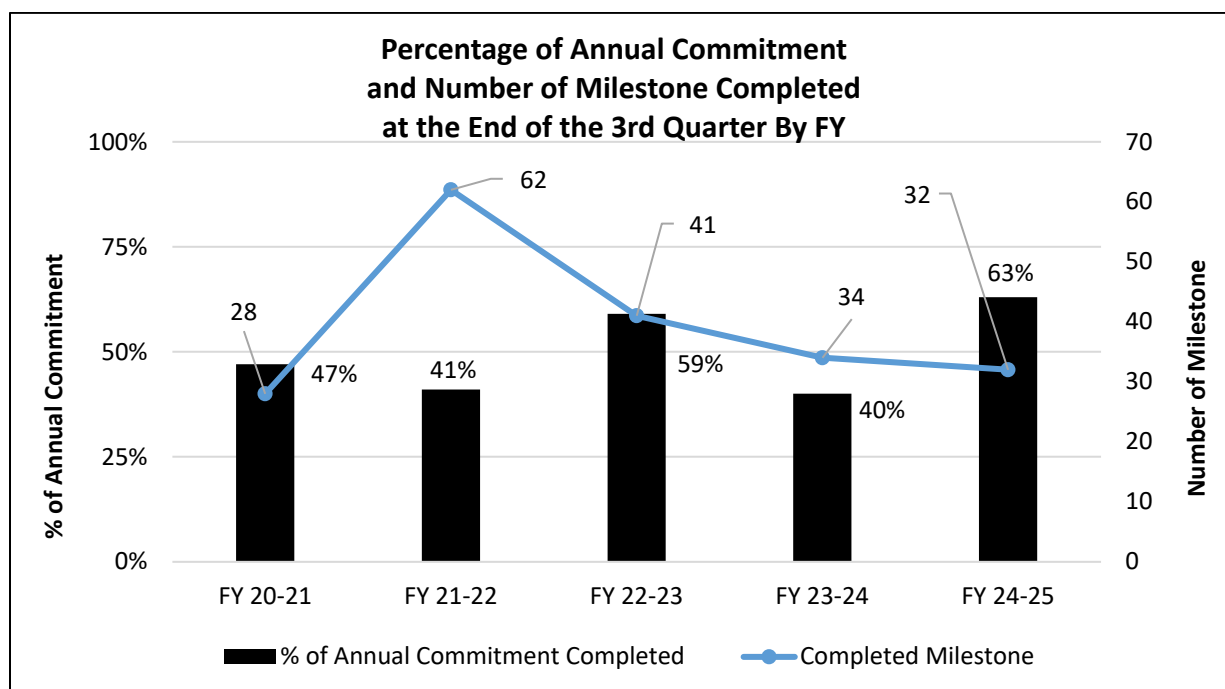
The project team conducts environmental studies to analyze the effect of various project alternatives. The result of the studies is an environmental document. The type of environmental document depends on the significance of the impacts.

In fiscal year 2024-25, Caltrans committed to deliver 51 draft environmental documents. Through the end of the third quarter, fiscal year 2024-25, Caltrans completed a total of 32, or 63 percent of the annual commitment.

Measure: Draft Environmental Documents Completed – 3rd Quarter FY 2024-25

Fiscal Year 2024-25

Year-to-Date thru 3rd Quarter			Annual Commitment		Year-End Projection		Last Five Fiscal Year Average
Completed	Plan	Percent	Plan	Percent	Forecast	Percent	Percent
32	44	73%	51	63%	46	90%	79%



Project Approval Summary

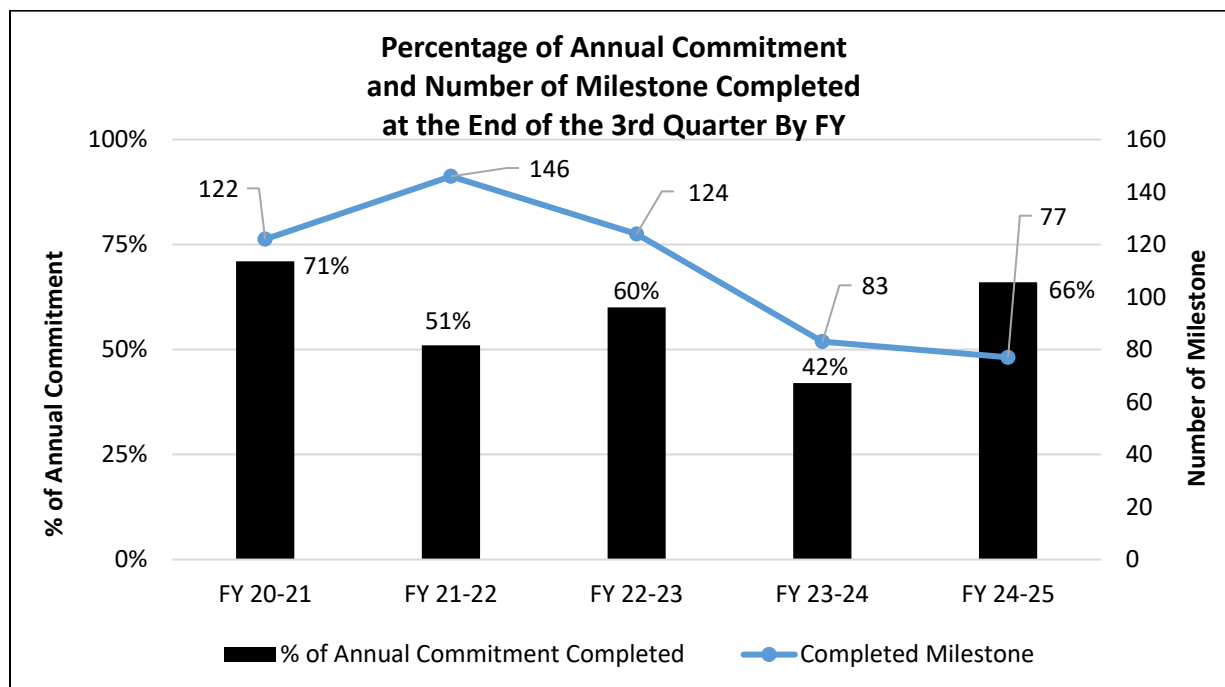
Project approval is also commonly referred to as "PA&ED," which is an abbreviation for the Project Approval and Environmental Document project milestone. Project approval is achieved when the Project Report has been signed. The Project Report includes the selection of the preferred project alternative and includes the project's environmental document.

In fiscal year 2024-25, Caltrans committed to deliver 117 project approvals and environmental documents. Through the end of the third quarter, fiscal year 2024-25, Caltrans approved a total of 77, or 66 percent of the annual commitment.

Measure: Projects Approved, Environmental Documents – 3rd Quarter FY 2024-25

Fiscal Year 2024-25

Year-to-Date thru 3rd Quarter			Annual Commitment		Year-End Projection		Last Five Fiscal Year Average
Completed	Plan	Percent	Plan	Percent	Forecast	Percent	Percent
77	103	75%	117	66%	112	96%	89%



Right of Way: Projects Certified

Projects Certifications Summary

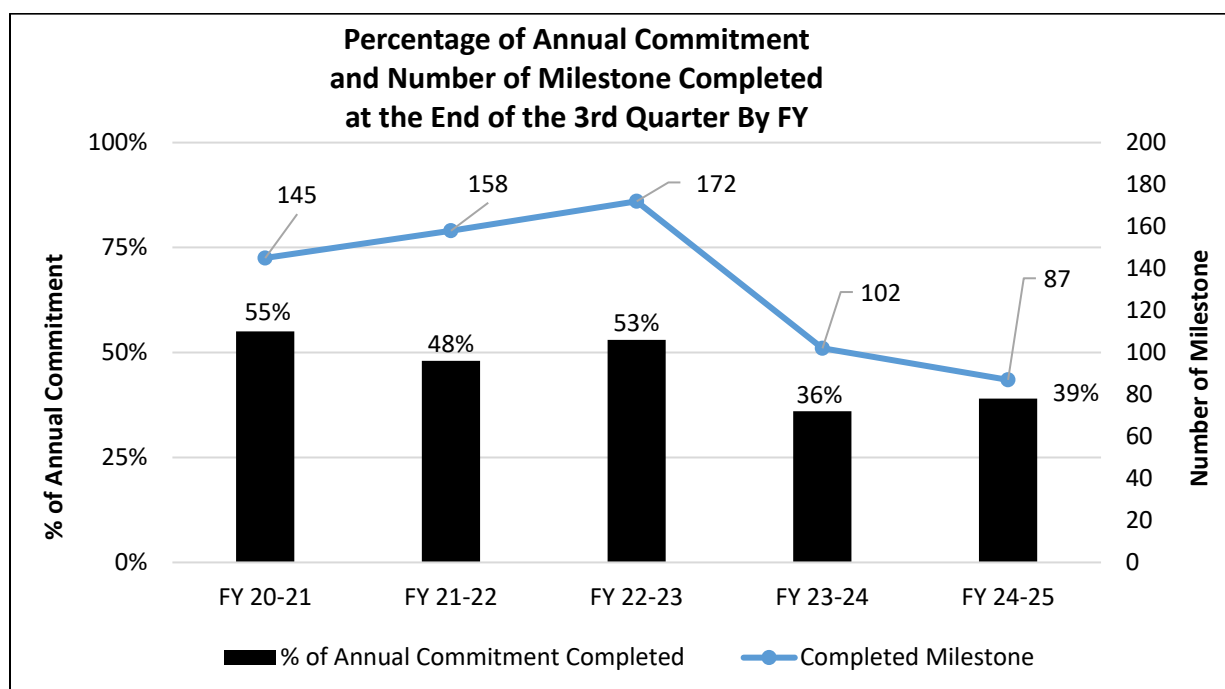
Right of way certification is achieved when all needed properties have been obtained, either by easement or acquisition, and all railroad and utility constraints are cleared.

In fiscal year 2024-25, Caltrans committed to certify right of way for 225 projects. Through the end of the third quarter, fiscal year 2024-25, Caltrans certified a total of 87 projects, or 39 percent of the annual commitment.

Measure: Projects Certified – 3rd Quarter Fiscal Year 2024-25

Fiscal Year 2024-25

Year-to-Date thru 3rd Quarter			Annual Commitment		Year-End Projection		Last Five Fiscal Year Average
Completed	Plan	Percent	Plan	Percent	Forecast	Percent	Percent
87	151	58%	225	39%	213	95%	88%



Delivery: Projects Designed and Ready for Construction

Contract for Delivery Summary

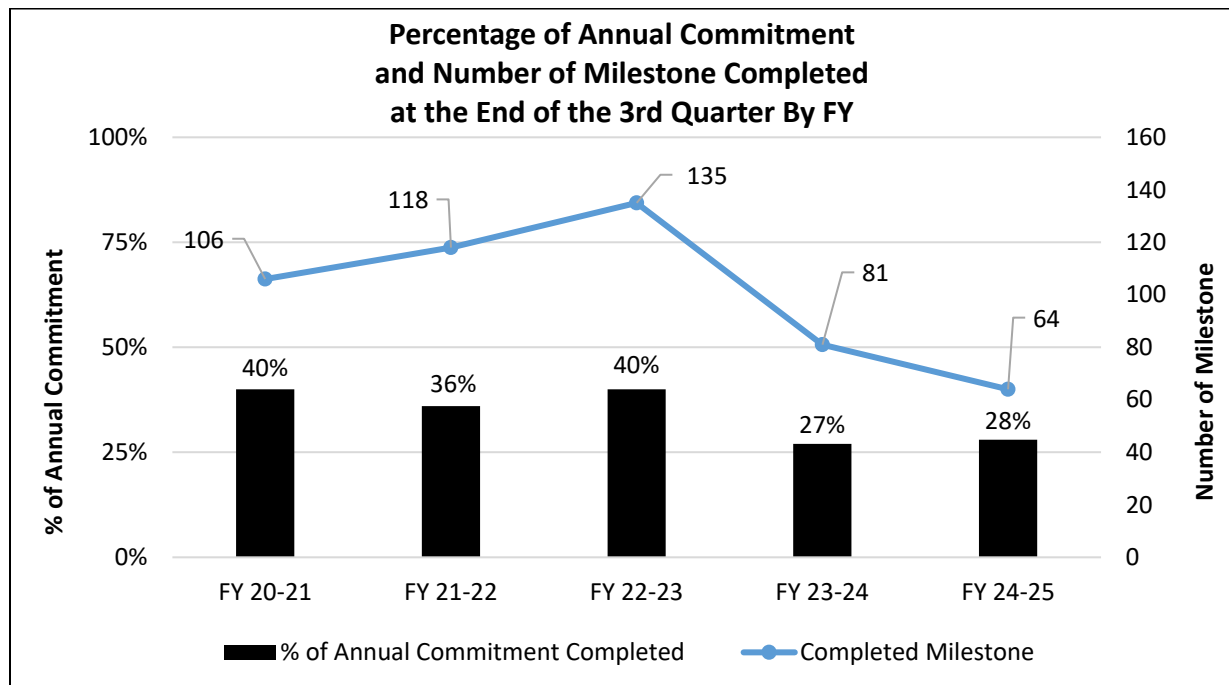
Each year, the Caltrans Director signs a Contract for Delivery with each of our 12 District Directors committing to deliver projects ready for construction. The Contract for Delivery includes a list of major state highway projects for which Caltrans will complete project plans, specifications and estimates and secure rights-of-way and permits in that fiscal year. This allows Caltrans to advertise and award construction contracts and begin construction.

In fiscal year 2024-25, Caltrans committed in the Contract for Delivery to deliver 227 projects ready for construction, valued at \$4.785 billion. Through the end of the third quarter, fiscal year 2024-25, Caltrans delivered 64 projects, or 28 percent of the annual commitment, with an estimated value of \$1.732 billion.

Measure: Projects Designed and Ready for Construction – 3rd Quarter FY 2024-25

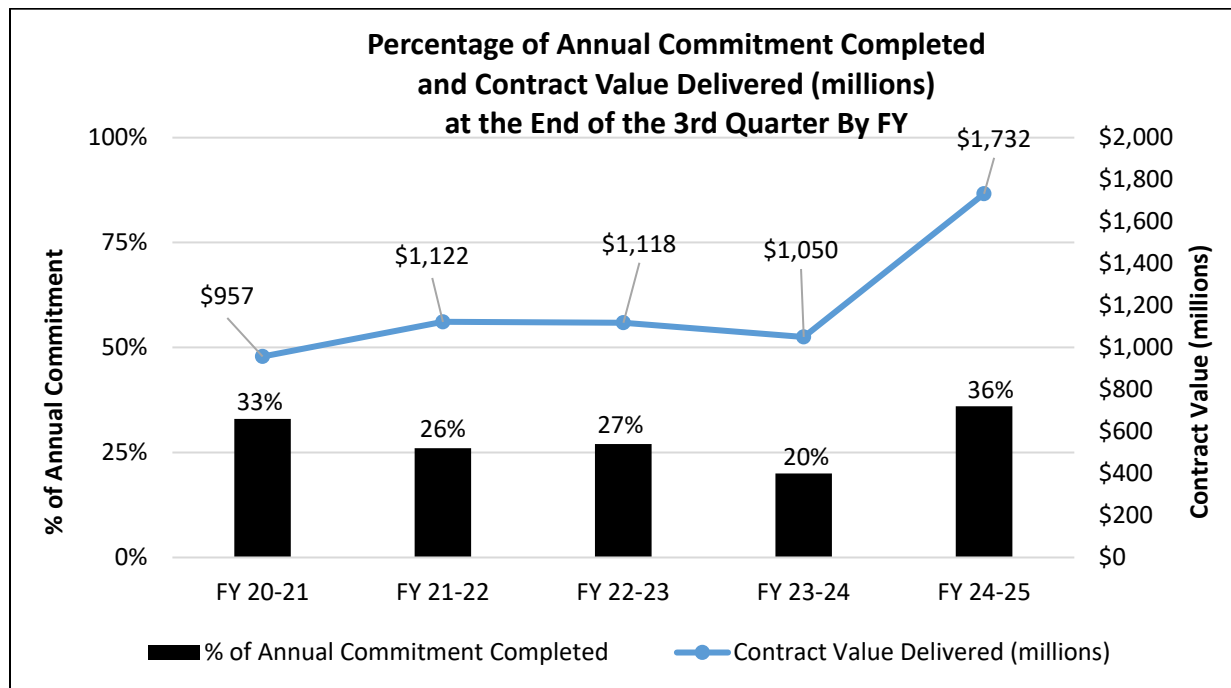
Fiscal Year 2024-25

Year-to-Date thru 3rd Quarter			Annual Commitment		Year-End Projection		Last Five Fiscal Year Average
Completed	Plan	Percent	Plan	Percent	Forecast	Percent	Percent
64	140	46%	227	28%	218	96%	87%



Measure: Contract Value Delivered – 3rd Quarter Fiscal Year 2024-25**Fiscal Year 2024-25 Contract for Delivery (millions)**

Year-to-Date thru 3rd Quarter			Annual Commitment		Year-End Projection		Last Five Fiscal Year Average
Completed	Plan	Percent	Plan	Percent	Forecast	Percent	Percent
\$1,732	\$3,027	57%	\$4,785	36%	\$4,571	96%	83%



Construction: Projects Constructed

Projects Constructed Summary

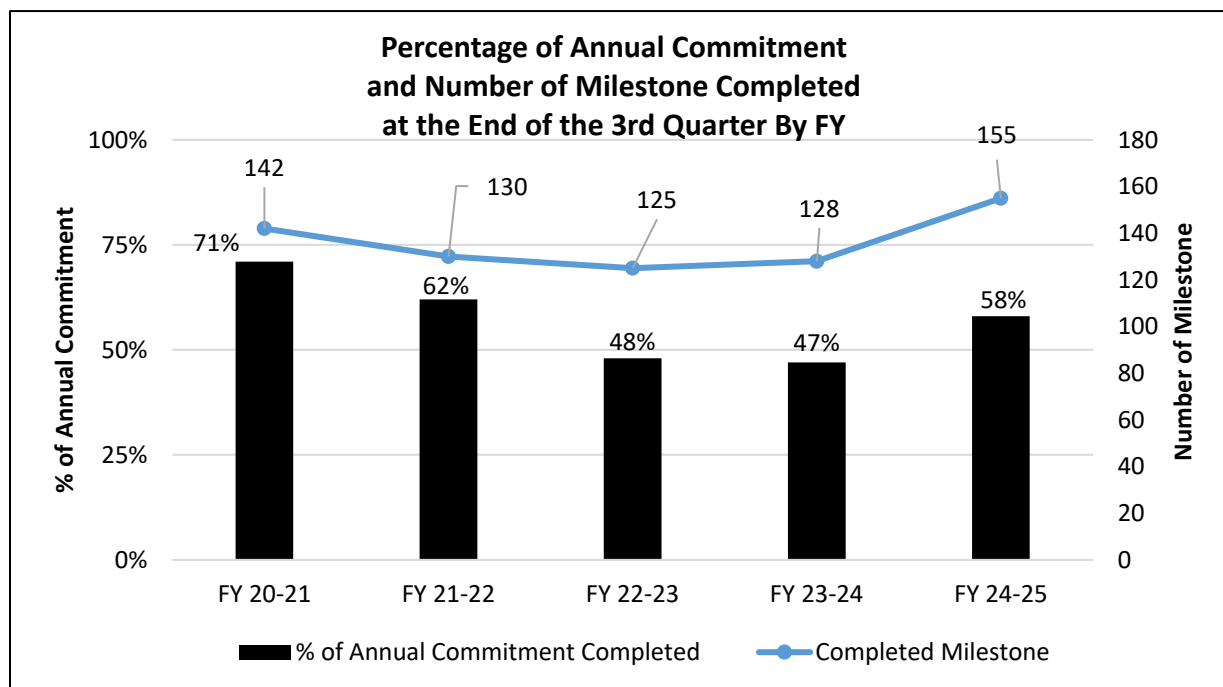
Construction entails building improvements as shown on the contract plans. Caltrans oversees the contractor's work and administers the contract by authorizing payments to the contractor for completed work. The contract is complete when the contract has been accepted by the state resident engineer.

In fiscal year 2024-25, Caltrans committed to complete construction of 267 projects. Through the end of the third quarter, fiscal year 2024-25, Caltrans has completed 155 projects, or 58 percent of the annual commitment.

Measure: Projects Constructed – 3rd Quarter Fiscal Year 2024-25

Fiscal Year 2024-25

Year-to-Date thru 3rd Quarter			Annual Commitment		Year-End Projection		Last Five Fiscal Year Average
Completed	Plan	Percent	Plan	Percent	Forecast	Percent	Percent
155	226	69%	267	58%	252	94%	79%



STIP and SHOPP Closeout Costs

Closeout Costs Summary

Pursuant to State Transportation Improvement Program guidelines and statutory requirements, Caltrans is reporting project closeout by comparing actual costs to final approved budgets. In consultation with Commission staff, project closeout reporting reflects projects where the construction contract was accepted (completed) two quarters ago.

- Through the end of the third quarter, fiscal year 2024-25, Caltrans has closed out 11 State Transportation Improvement Program projects. The final approved budget for these projects was \$667 million. The actual cost to complete these projects was \$592 million, or 89 percent of the final approved budget.
- Through the end of the third quarter, fiscal year 2024-25, Caltrans has closed out 226 State Highway Operation and Protection Program projects. The final approved budget for these projects was \$2.827 billion. The actual cost to complete these projects was \$2.438 billion, or 86 percent of the final approved budget.

Measure: Program Costs – 3rd Quarter Fiscal Year 2024-25

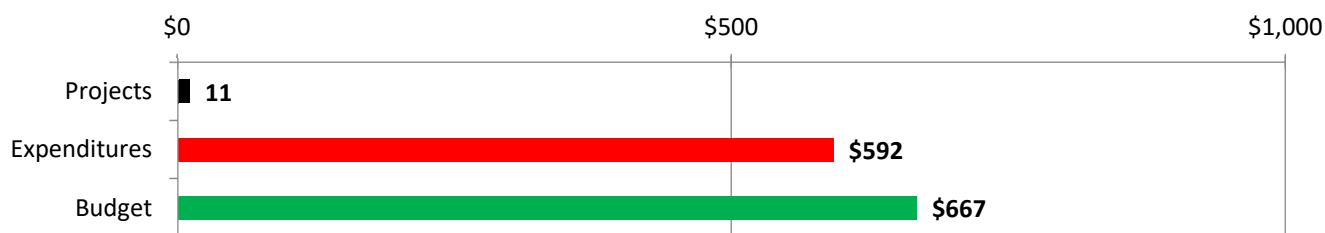
Program	Expended (millions)	Budget (millions)	Savings (millions)	Percent Expended	Last Five Fiscal Year Average
STIP ¹	\$592 ²	\$667 ³	\$75	89%	88%
SHOPP ¹	\$2,438 ²	\$2,827 ³	\$389	86%	88%

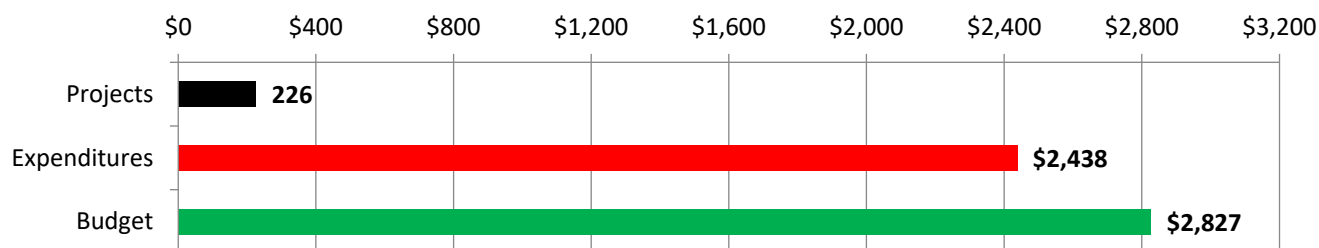
¹ Dollars are rounded to the nearest million.

² Total support and capital expenditures.

³ Total support and capital approved budget including G-12 and Supplementals.

State Transportation Improvement Program Closeout – Program Costs (millions)



State Highway Operation and Protection Program Closeout – Program Costs (millions)
**Commission Initial Allocation, Final Approved Costs, and Expended Costs
for Allocated Construction Components**
SHOPP

Pursuant to Government Code Sections 14526.6 and the State Highway Operations and Protection Program (SHOPP) Guidelines Section 57 adopted in December 2024, this quarterly report provides the following.

- Appendix B: SHOPP projects that include SHOPP funds as well as other contributing funds, programmed, allocated, and expended at the time the construction contract was accepted for the projects.
- Appendix B1: A summary by phase of SHOPP only funds programmed, allocated, and expended at the time the construction contract was accepted for the projects.

Construction costs are calculated six months after the end of construction.

STIP

As required by Government Code 14525.6, the table below provides a comparison between the Commission's initial allocation, final approved state only costs and expended costs for STIP projects that completed construction in the third quarter of Fiscal Year 2024-25. This provides an indication of how adjustments subsequently made after the initial vote (Commission approved supplemental funds or Caltrans delegated funding authority) compared to the initial allocated amounts for each program.

The table below is generated from the projects listed in Appendix C of this report. Construction costs are calculated six months after the end of construction.

STIP Programmed Projects¹ Closeout – Construction Costs (\$1,000s)

Program	Construction Support ²			Construction Capital		
	Initial Allocation	Final Approved Budget ³	Expended	Initial Allocation	Final Approved Budget ³	Expended
Regional Transportation Improvement Program (RTIP) and Interregional Transportation Improvement Program (ITIP)	9,900	10,550	10,102	54,968	65,276	60,218
Solutions for Congested Corridors Program (SCCP)	19,180	19,180	15,159	114,939	114,939	111,215
SHOPP funds on STIP Projects	12,230	13,605	11,342	105,910	79,611	72,152
State Local Partnership Program (SLPP)	10,500	10,500	10,498	9,385	9,385	9,385
Trade Corridors Enhancement Account Program (TCEP)	21,000	21,000	20,627	66,685	66,685	63,380
Trade Corridors Improvement Fund (TCIF)	750	750	748	5,877	5,329	5,329
OTHER ⁴	9,070	10,670	10,618	116,258	122,353	96,964

¹ STIP Programmed Projects often include other fund types needed to complete projects; these include and not limited to: TCIF, SHOPP, CMAQ, Locally Generated, and Federal Discretionary funds. The budget and expenditure from each fund type are based on the best available data at the time.

² Construction Support totals reported: Government Code 14525.5 requires the Commission to allocate construction support for STIP funds and requires Caltrans to report on allocated construction components.

³ Final Approved Budget, excluding right-of-way capital, is the sum of all approved Commission allocations plus delegated G-12 adjustments.

⁴ OTHER includes Locally Generated (i.e., measure funds...) and Federal Discretionary funds. The budget and expenditure are based on the best available data at the time.

Appendix

- (A) Caltrans Third Quarter, Fiscal Year 2024-25 Watch List.
- (B) Caltrans Fiscal Year 2024-25 State Highway Operation and Protection Program Project Closeout. SHOPP projects that includes SHOPP funds as well as other contributing funds for all support and capital phases, programmed, allocated, and expended at the time the construction contract was accepted for projects. Construction costs are calculated six months after the end of construction.

(B1) A summary by phase of SHOPP only funds programmed, allocated, and expended at the time the construction contract was accepted for the projects. Construction costs are calculated six months after the end of construction.
- (C) Caltrans Fiscal Year 2024-25 State Transportation Improvement Program Project Closeout. Construction costs are calculated six months after the end of construction.
- (D) Caltrans Fiscal Year 2024-25 Capital Outlay Support G-12 Request Summary.
- (E) Caltrans Third Quarter, Fiscal Year 2024-25 Retired Risks.
- (F) Construction Capital Closeout Summary.

(A) Caltrans Third Quarter, Fiscal Year 2024-25 Watch List

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Pre-Construction-Delivery Delays: 104 Projects with Programmed Budget of \$3.448 are at risk of missing the Fiscal Year 2024-25 Deliveries									
01-0H140	MEN	101	Hopland ADA	SHOPP	22,726	Fiscal Year Delivery	Medium	Decreased	Right of Way Acquisition and Stakeholder Concerns.
01-0N290	DN	197	Del Norte STAA Improvements	SHOPP	60,000	Fiscal Year Delivery	Medium	New	Construction Site Issues; Design Strategy Changes; and Permits.
02-4F900	SIS	096	Happy Camp Complete Streets	STIP	12,221	Fiscal Year Delivery	Medium	Same as Previous	Complete Streets and Stakeholder Concerns.
02-0J550	SIS	161	Klamath Lake Rehab 2R	SHOPP	14,010	Fiscal Year Delivery	Medium	New	Construction Site Issues and Design Strategy Changes.
02-0J760	TRI	003	Tangle Blue CAPM	SHOPP	28,378	Fiscal Year Delivery	Medium	New	Environmental Document Change.
02-1J870	SIS	097	Grass Lake Maintenance Station	SHOPP	21,390	Fiscal Year Delivery	Very High	Same as Previous	Fiscal Year Delivery; Design Strategy Changes; Stakeholder Concerns; and Unit Price Increases/Market Changes-The project cost estimate was updated at 65% design completion, resulting in a 50% increase in capital cost estimate. As a result of this steep increase in the capital cost estimate, the Project Delivery Team is currently tasked to review the latest estimate for accuracy and evaluate other options that will potentially reduce the cost while still meeting the project's purpose and need. This effort will require additional time; there is a very high risk that the project will miss the fiscal year delivery.
03-4E170	NEV	049	Nev-49 Corridor Improvement Project	STIP	14,816	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition-The project involves parcel condemnation. A Resolution of Necessity (RON) is going to the May 2025 California Transportation Commission (CTC) meeting. Utility relocation requires RON for the parcel. Time frame for the Right of Way Certification approval from FHWA is 8 weeks. The fiscal year delivery is at risk.
03-3H590	PLA	080	Blue Canyon Pavement Rehabilitation	SHOPP	119,772	Fiscal Year Delivery	Very High	Increased	Railroad Involvement-The project has completed the design phase. A draft Construction and Maintenance agreement has been reviewed by the Union Pacific Railroad Real Estate, and it is with the department for review and response. The needed review and coordination time are delaying Right of Way Certification. The fiscal year delivery is at risk. The district plans to request a time extension.
03-4H980	PLA	089	Tahoe City Housing Replacement	SHOPP	5,690	Fiscal Year Delivery	Very High	Same as Previous	Stakeholder Concerns-Fire Marshal approval was received in March 2025 for this fiscal year delivery. Ready-To-List is scheduled for May 2025. District will likely request an allocation time extension at the June 2025 California Transportation Commission Meeting. The fiscal year delivery is at risk.
03-0J400	ED	050	Placerville CAPM	SHOPP	45,770	Fiscal Year Delivery	High	Increased	Design Strategy Changes.
03-0J440	BUT	162	Oroville CAPM	SHOPP	25,613	Fiscal Year Delivery	High	Same as Previous	Railroad Involvement.
03-0J460	COL	005	Maxwell SRRA Rehab	SHOPP	22,250	Fiscal Year Delivery	Very High	Increased	Stakeholder Concerns-A second Fire Marshal review is in progress and comments are pending. Based on the comments being addressed and the number of resubmittals required, additional time is needed; the fiscal year delivery is at risk.
03-0J510	GLE	162	Willows CAPM	SHOPP	22,718	Fiscal Year Delivery	Very High	Same as Previous	Railroad Involvement and Right of Way Acquisition-Timeline for meeting railroad plan review could take longer and right of way certification milestone may be in jeopardy. One county airport parcel will require revised Environmental Document followed by County Board approval and later it would need Federal Aviation Administration approval, which takes 45 days. The fiscal year delivery is at risk.
03-0J520	PLA, NEV	020	Nev/Pla 20 CAPM	SHOPP	30,970	Fiscal Year Delivery	Low	Decreased	Right of Way Acquisition.
03-0J550	PLA	080	Whitmore Maintenance Station Rehab	SHOPP	19,739	Fiscal Year Delivery	High	Same as Previous	Permits.
03-0J910	SUT	099	SUT-99 Lomo Crossing	SHOPP	13,720	Fiscal Year Delivery	Very High	Increased	Railroad Involvement-Plans resubmitted to Union Pacific Railroad (UPRR). The estimated review time by UPRR for the final approval of this project is about 10 months. The fiscal year delivery is at risk.
03-1J160	ED	050	ED 50 CAPM	SHOPP	42,620	Fiscal Year Delivery	Low	Decreased	Right of Way Acquisition.
03-1J630	SAC	099	Willow Slough Replacement	SHOPP	17,520	Fiscal Year Delivery	High	Same as Previous	Permits.
03-3J680	SUT	020	Stafford Way Intersection Safety Improvement	SHOPP	4,910	Fiscal Year Delivery	Low	Decreased	Right of Way Acquisition.
04-1G900	SM	082	82 ADA Burlingame	SHOPP	16,184	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition; Environmental Mitigation; and Utility Conflicts/Relocation-At the project location, about 200 private residents encroach onto the State's Right of Way; the project involves replanting 300 historically registered trees; and utility relocation. The fiscal year delivery is at risk.
04-0G642	MRN	001	Lagunitas Creek Bridge	SHOPP	30,979	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition; Utility Conflicts/Relocation; and Permits-The District took the risk of keeping this project on FY 24/25 contract for delivery, acknowledging environmental/permitting and right of way challenges. However, the project involves longer than anticipated acquisition process and potential condemnation including 10 Temporary Construction Easements as well as 3 out of 10 anticipating condemnations. The project also involves complex utility relocation and permit issues. The fiscal year delivery is at risk.
04-0J550	ALA	084	Scour at Arroyo De La Laguna	SHOPP	45,963	Fiscal Year Delivery	Very High	Same as Previous	Permits; Right of Way Acquisition; and Utility Conflicts/Relocation-The project requires right of way acquisition from the San Francisco Public Utility Commission (SFPUC) and one Temporary Construction Easement from Alameda County. SFPUC water line interfering with new bridge approach slab is also in the process of relocation. The project involves complex permit issues. The fiscal year delivery is at risk.
04-4J860	MRN	101	Multi-asset Interchange Improvement	SHOPP	38,666	Fiscal Year Delivery	Medium	New	Permits.
04-0K810	SM	082	SM 82 Roadway Rehab 3R	SHOPP	121,099	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition; Environmental Mitigation; and Utility Conflicts/Relocation-The project is in the vicinity of project 04-1G900. At the project location, about 200 private residents encroach onto the State's Right of Way; the project also involves utility relocation and replanting 300 historically registered trees. The fiscal year delivery is at risk.
04-1K510	ALA	084	ALA DRAINAGE RESTORATION	SHOPP	13,670	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition-The project needs two parcels and one Permanent Access Easement, which will potentially delay project delivery.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Pre-Construction-Delivery Delays-Continued									
04-0P900	ALA	580	BRIDGE REHABILITATION	SHOPP	38,851	Fiscal Year Delivery	High	Increased	Right of Way Acquisition.
04-0Q020	SF	101	BRIDGE REHABILITATION	SHOPP	44,720	Fiscal Year Delivery	Very High	Increased	Alternative Delivery Method-This is a Construction Manager/General Contractor Construction Manager project. The fiscal year delivery risks include the following: the need to develop adequate Transportation Management Plan (TMP) including possible full freeway closures through downtown San Francisco; coordination with the City and County of San Francisco regarding the TMP and possible detours; and coordination with two adjacent projects' TMPs that may be in construction at the same time. Currently, the updated price for re-packaging for advertisement is a point of dispute. The fiscal year delivery is at risk.
04-0Q120	SF	280	CAPM SF-280	SHOPP	105,313	Fiscal Year Delivery	Very High	Increased	Right of Way Acquisition and Permits-The Pedestrian Overcrossing will need two Temporary Construction Easements (TCEs) from the Bay Area Rapid Transit and one partial TCE from a private property owner. District 4 right of way appraisal started in March 2024 and it is estimated to take at least 18 months to procure the TCEs. Therefore, the district will most likely need to request a 20-month time extension to deliver this project by March 2026. The fiscal year delivery is at risk.
04-0Q480	SM	084	STORM DAMAGE PERMANENT RESTORATION	SHOPP	10,187	Fiscal Year Delivery	Very High	Same as Previous	Permits and Right of Way Acquisition-Right of Way Acquisition of 2 Temporary Construction Easements is at risk. Also, the project involves biological opinion and a number of permits including the California Department of Fish and Wildlife Permit 1600; Water Quality Control Board Permit 401; and the U. S. Army Corps of Engineers Permit 404. The project delivery schedule is at risk.
04-1Q570	SOL	128	Putah Creek, Bridge 23-0099	SHOPP	11,631	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition and Permits-The District took the risk of keeping this project on FY 24/25 contract for delivery. However in June 2024, Right of Way (RW) office learned that 2 privately-owned parcels out of 3, posed a challenge to the acquisitions that led to a potential condemnation process. It was expected that the RW process would take at least 20 months to complete. However, longer than anticipated acquisition process is required for this project. The project also involves complex permit issues. The fiscal year delivery is at risk.
04-1Q620	NAP	121	Napa SR 121 CAPM	SHOPP	48,499	Fiscal Year Delivery	Very High	New	Right of Way Acquisition-The project involves 50 parcels with potential condemnation as well as 153 utility conflicts/ relocations. The fiscal year delivery is at risk.
04-1Q820	SF	101	04-1Q820_SF 101-Roadway Rehab 3R	SHOPP	83,427	Fiscal Year Delivery	Very High	Increased	Design Strategy Changes-The project involves placement of Continuous Reinforced Concrete Pavement (CRCP) that requires more room than traditional pavement replacement strategies. An adequate Traffic Management Plan (TMP) is needed which will likely result in possible full freeway closure to accommodate the placement of CRCP and more coordination between the City and County of San Francisco on the updated TMP. The project delivery will be potentially delayed.
04-2Q420	SON	116	SON 116 Bridge railing systems	SHOPP	9,440	Fiscal Year Delivery	Medium	Same as Previous	Permits.
04-2Q500	MRN	037	Petaluma River Bridge	SHOPP	43,750	Fiscal Year Delivery	Medium	New	Permits.
04-2Q580	SON	101	Santa Rosa Maintenance Station	SHOPP	39,624	Fiscal Year Delivery	Very High	Increased	Right of Way Acquisitions and Permits-The District took the risk of keeping this project on FY 24/25 contract for delivery, acknowledging environmental/permitting and right of way challenges. However, the existing maintenance building is in a dire situation and does not meet current facility design standards. Sonoma County requires 100' creek setback distance from the top of the bank which was not anticipated. This change necessitates re-designing of the filtration basin for discharging stormwater to the Mark West Creek, which in turns results in longer than anticipated permit process. The current year project delivery schedule is at risk.
04-2Q720	ALA	061	PAVEMENT PRESERVATION	SHOPP	26,458	Fiscal Year Delivery	Medium	Decreased	Complete Streets.
04-2Q770	SON	116	Install Signals/Construct Roundabout	SHOPP	15,543	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition; Utility Conflicts/Relocation; and Permits-The District took the risk of keeping this project on FY 24/25 contract for delivery, acknowledging environmental/permitting and right of way challenges. It turns out that the project involves longer than anticipated acquisition process and potential condemnation. Requirements include four parcels from private owners (3 Fee Acquisitions and 1 Temporary Construction Easement) and 1 parcel from the county. The project also involves complex utility relocation and permit issues. The fiscal year delivery is at risk.
04-0AA19	CC	004	CC-4 CAPM HILLCREST OCTO BYRON HWY	SHOPP	43,254	Fiscal Year Delivery	Medium	Decreased	Construction Site Issues; Railroad Involvement; and Utility Conflicts/Relocation.
04-4AA30	NAP	128	Storm Damage	SHOPP	10,611	Fiscal Year Delivery	High	New	Right of Way Acquisition.
04-4AC40	SON	012	Horizontal Alignment Warning Signs	SHOPP	5,698	Fiscal Year Delivery	Medium	Decreased	Permits.
04-4AC70	MRN	101	CAPM	SHOPP	30,732	Fiscal Year Delivery	High	Increased	Right of Way Acquisition and Permits.
04-4AC80	NAP	029	CAPM SR 29	SHOPP	26,847	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition-The District took the risk of keeping this project on FY 24/25 contract for delivery, acknowledging right of way (RW) challenges. However, the project involves compressed schedule to obtain 5 parcels consisting of 1 RW take, 2 Temporary Construction Easements (TCEs), and 2 partial TCEs. All 5 parcels are in appraisals. RW Certification is shifted from May 2025 to October 2026. The district will likely need a 19-month time extension from the California Transportation Commission.
04-2W660	CC	080	I-80/Central Ave UC Ped & Bike	STIP	4,333	Fiscal Year Delivery	Very High	Increased	Design Strategy Changes-Currently, State Potholing Contract is expired and there is a risk of delay by using a consultant task order for potholing. The process of potholing, surveying, and revising the utility plans may take longer than three months due to winter weather and it could delay delivery. The fiscal year delivery is at risk.
04-2Y050	MRN	101	Marin City Second Culvert	SHOPP	20,100	Fiscal Year Delivery	Very High	New	Permits and Right of Way Acquisition-Project has Environmental Permits that require 9 12 months for permit issuance. Also, there are two parcels that need appraisal and acquisition. Right of Way (RW) requested 24 months from RW transmittal to RW certification, placing RW certification at very high risk. Bridge Design schedule is also currently compressed. The fiscal year delivery is at risk.
04-2Y260	ALA	123	RTE 123/ALA Pedestrian Crossing Improvements	SHOPP	6,050	Fiscal Year Delivery	High	New	Design Strategy Changes.
05-1E030	SB	166	Guadalupe ADA	SHOPP	7,639	Fiscal Year Delivery	Medium	Decreased	Right of Way Acquisition and Utility Conflicts/Relocation.
05-1G490	SCR	001	Harkins Bike/Ped OC	STIP, Local	9,314	Fiscal Year Delivery	Low	Decreased	Design Strategy Changes and Utility Conflicts/Relocation.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Pre-Construction-Delivery Delays-Continued									
05-1G680	SLO	101	Five Cities Multimodal Transportation Network Enhancement Project	STIP	81,017	Fiscal Year Delivery	Medium	New	Permits.
05-1H610	SB	001	Solomon Canyon Rumble Strip/Shoulder Widening	SHOPP	28,748	Fiscal Year Delivery	Very High	Same as Previous	Utility Conflicts/Relocation and Right of Way Acquisition-PG&E has deprioritized relocation plans which continues to delay right of way (RW) negotiations and acquisition. Ready-To-List is approaching and RW certification is uncertain. The fiscal year delivery is at risk.
05-1J830	SCR	001, 152	129 Paving, Sign Panels, Lighting, TMS Improvement	SHOPP	17,068	Fiscal Year Delivery	Medium	Increased	Right of Way Acquisition and Complete Streets.
05-1J880	MON	068	Route 68 Drainage Improvements	SHOPP	17,342	Fiscal Year Delivery	High	New	Right of Way Acquisition.
05-1J890	MON	101	Mon 101 Drainage	SHOPP	23,500	Fiscal Year Delivery	Medium	New	Permits; Utility Conflicts/Relocation; and Railroad Involvement.
05-1J900	SB	101	Santa Barbara 101 Pavement Project	SHOPP	67,702	Fiscal Year Delivery	Low	New	Utility Conflicts/Relocation.
05-1K440	MON	101	King City CAPM	SHOPP	27,573	Fiscal Year Delivery	Medium	Decreased	Utility Conflicts/Relocation; Permits; Right of Way Acquisition; and Design Strategy Changes.
05-1K520	SB	154	Hwy 154 Drainage Improvements	SHOPP	15,638	Fiscal Year Delivery	High	Same as Previous	Permits and Right of Way Acquisition.
05-1K680	SLO	101	D5 Maintenance Station Relocation (Phase 1)	SHOPP	76,934	Fiscal Year Delivery	Medium	Increased	Design Strategy Changes.
05-1P210	MON	001	Coastlands II retaining wall	SHOPP	9,499	Fiscal Year Delivery	Low	New	Permits.
06-0S370	FRE	041	EXCELSIOR EXPRESSWAY II	STIP	56,000	Fiscal Year Delivery	High	Same as Previous	Utility Conflicts/Relocation.
06-0V610	KER	119	Pumpkin Center 3R Rehab	SHOPP	59,900	Fiscal Year Delivery	High	Same as Previous	Right of Way Acquisition.
06-0W810	KER	155	Delano SR-155 Rehab (3R)	SHOPP	16,740	Fiscal Year Delivery	Medium	Same as Previous	Railroad Involvement.
06-0W830	KER	033	South Taft Rehab	SHOPP	26,500	Fiscal Year Delivery	Medium	New	Utility Conflicts/Relocation.
06-0W800	FRE	099	El Dorado to Clinton Rehab	SHOPP	401,013	Fiscal Year Delivery	Very High	Same as Previous	Design Strategy Changes; Railroad Involvement; Right of Way Acquisition; and Utility Conflicts/Relocation-The project is a very large rehabilitation project in an urban area utilizing Construction Manager/General Contractor Program. Right of way, railroad, and utility challenges will likely result in fiscal year delivery delay.
06-0X070	TUL	245	Tul 245 Culvert Rehab	SHOPP	17,134	Fiscal Year Delivery	Medium	New	Right of Way Acquisition.
06-0X220	FRE	168	Fre 168 Culvert Rehab	SHOPP	21,093	Fiscal Year Delivery	Medium	New	Permits.
06-0X220	FRE	168	Fre 168 Culvert Rehab	SHOPP	21,093	Fiscal Year Delivery	High	Increased	Utility Conflicts/Relocation; Design Strategy Changes; and Right of Way Acquisition.
06-0W920	KER	005	Grapevine Rehab	SHOPP	105,658	Fiscal Year Delivery	Medium	Increased	Utility Conflicts/Relocation and Design Strategy Changes.
06-0X430	FRE	099	West Ave. Maintenance Station Upgrade	SHOPP	19,620	Fiscal Year Delivery	Low	New	Permits.
06-0Y150	KER	223	Arvin CAPM	SHOPP	1,050	Fiscal Year Delivery	Very High	Increased	Utility Conflicts/Relocation-As a result of the pothole contract approval being delayed, pothole activities will not be completed in time for right of way certification, which in turn would delay Ready-To-List. The fiscal year delivery is at risk.
06-0Y180	MAD	145	Downtown Madera CAPM	SHOPP	18,735	Fiscal Year Delivery	High	Increased	Utility Conflicts/Relocation.
06-0Y410	FRE	180	Dickenson Roundabout	SHOPP	13,595	Fiscal Year Delivery	Very High	Same as Previous	Environmental Document Change and Utility Conflicts/Relocation-This project involves PG&E utility relocations. Caltrans is coordinating with PG&E; however, PG&E may not be able to submit utility relocation plans based on the Caltrans' required schedule. The fiscal year delivery is at risk.
07-32090	LA	001	SGR Bridge Rail Upgrade	SHOPP	42,455	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition and Permits-The project involves two right of way condemnations and one pending private utility relocation. The private utility owner has objected to the project which might involve legal. Also, the project involves a number of permits from various environmental agencies. The fiscal year delivery is at risk.
07-34710	LA	134	LA-134 Overhead Signs	SHOPP	30,184	Fiscal Year Delivery	High	Increased	Design Strategy Changes.
07-35590	VEN	034	CAPM / ADA / Complete Streets	SHOPP	42,227	Fiscal Year Delivery	Very High	Increased	Railroad Involvement and Right of Way Acquisition-Right of Way is having issues with the Union Pacific Railroad (UPRR), Hazardous Waste testing, and possible Resolution of Necessity (RON). UPRR has given plan approval for Lewis Rd Bridge, but it has not yet commented on the memo for the work adjacent to the At-Grade crossings. The fiscal year delivery is at risk.
07-36460	LA	105	LA 105 - Drainage upgrade	SHOPP	10,204	Fiscal Year Delivery	Very High	New	Railroad Involvement and Right of Way Acquisition-During the quality review of the Plans, Specification, and Estimates, the District Railroad Coordinator noted the requirements of securing a wireline agreement from Union Pacific Railroad (UPRR) and a joint revocable permit from the port of Los Angeles (POLA) and the port of Long Beach (POLB) for underground conduit work beneath an existing railroad bridge. Port liaisons estimated a 14-month approval timeline, encompassing both the UPRR agreement and the ports' joint revocable permit. However, Caltrans' legal counsel has opined that existing underpass easements might negate the need for these permits. If the ports' attorneys concur with Caltrans' legal interpretation, the project could proceed with a simple and time saving consent letter; otherwise, the fiscal year delivery is at risk.
07-36300	LA	101	Upgrade TMS elements on LA-101	SHOPP	30,987	Fiscal Year Delivery	High	New	Utility Conflicts/Relocation.
07-36330	LA	405	LA-405 Mobility - TMS	SHOPP	38,283	Fiscal Year Delivery	High	New	Utility Conflicts/Relocation.
07-37310	LA	002	LA-2 TMC Mech	SHOPP	17,768	Fiscal Year Delivery	Very High	Same as Previous	Alternative Delivery Method and Design Strategy Changes-The project involves scope change to ensure full decarbonization to achieve net-zero emission of greenhouse gases for state agency operations; this additional scope has caused additional coordination, which in turn has increased the timeline needed to deliver the project. The fiscal year delivery is at risk.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Pre-Construction-Delivery Delays-Continued									
07-37630	VEN	023	Ven-23 Drainage Near Bardsdale	SHOPP	11,177	Fiscal Year Delivery	Very High	New	Utility Conflicts/Relocation-Due to the potholing contract delays, utility verification for this project was recently completed. The potholing identified that the proposed concrete box culvert would be in conflict with an existing high pressure gas line. The project will require design change that would require additional time to complete. Fiscal year delivery is at risk. The district plans to request a 9-month Construction Funds Allocation Time extension at the June 2025 California Transportation Commission meeting.
07-37640	LA	138	Replace Drainage for Segment 9	SHOPP	6,554	Fiscal Year Delivery	High	New	Permits.
08-1F400	SBD	066	SBD 066 UPGRADE TO STANDARD BRIDGE RAIL	SHOPP	16,623	Fiscal Year Delivery	High	Same as Previous	Railroad Involvement.
08-1G660	SBD	066	SBD 66 CAPM	SHOPP	20,900	Fiscal Year Delivery	High	Same as Previous	Railroad Involvement.
08-1J640	RIV	010	RIV 010 Lane Replacement	SHOPP	73,233	Fiscal Year Delivery	Medium	New	Environmental Document Change and Design Strategy Changes.
08-1J650	RIV	010	RIV 10 PAVEMENT REHAB	SHOPP	81,278	Fiscal Year Delivery	Medium	New	Environmental Document Change; Environmental Mitigation; and Permits.
08-1K940	SBD	038	SBD 38 PAVEMENT REHAB (+IUA CS, FIBER)	SHOPP	17,602	Fiscal Year Delivery	High	New	Design Strategy Changes.
08-1M560	SBD	210	SBD 210 WIDEN E/B OFF RAMP @ WATERMAN AVE OC	SHOPP	13,316	Fiscal Year Delivery	High	New	Right of Way Acquisition.
09-37430	MNO	395	LEE VINING REHAB	SHOPP	27,848	Fiscal Year Delivery	Medium	Decreased	Design Strategy Changes.
09-37450	INY	395	LONE PINE SIDEWALK	SHOPP	6,555	Fiscal Year Delivery	Medium	Decreased	Right of Way Acquisition and Design Strategy Changes.
09-37900	INY	395	MANZANAR PAVEMENT	SHOPP	54,588	Fiscal Year Delivery	Medium	Increased	Utility Conflicts/Relocation and Design Strategy Changes.
10-1F320	SJ	004	SJ 4 CAPM	SHOPP	21,664	Fiscal Year Delivery	Low	New	Design Strategy Changes.
10-1H020	MER	140	MER 140/165 Mobility Improvements	SHOPP	1,776	Fiscal Year Delivery	Very High	New	Utility Conflicts/Relocation-The project requires 4 parcels, PG&E relocation, and Merquin Irrigation District (MID) relocation. The three parcels have been appraised and one parcel is pending response from the owner. PG&E conflict maps have been sent. The team has reached out to PG&E multiple times in regard to the status of the relocation plan with the last correspondence. The time frame for executing a Utility Agreement with PG&E is 12 months. The fiscal year delivery is at risk.
10-1F720	ALP	004	SR4 CULVERT REPLACEMENT	SHOPP	9,065	Fiscal Year Delivery	Very High	Same as Previous	Permits and Right of Way Acquisition-The project involves Right of Way (RW) appraisal delay, which reduced the lead time for RW acquisition. More time is needed to acquire 4 of 17 parcels that are in federal land. These 4 parcels require special use permits. RW needs additional time to acquire these permits. The fiscal year delivery is at risk.
10-1G620	CAL	004	CAL 4 Culvert Replacement	SHOPP	8,127	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition-11 of 16 parcels are still in appraisal. Additional time is needed to acquire the parcels. The fiscal year delivery is at risk.
10-1G270	CAL	049	SR 49 San Andreas Complete Streets	SHOPP	17,365	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition and Utility Conflicts/Relocation-As a result of public engagement efforts during the Project Approval and Environmental Document (PAED), complete street elements were added to the project scope late in the PAED phase. The total number of parcels increased from 34 to 80. The fiscal year delivery is at risk.
10-0J720	ALP	004	SR4 Pavement Anchor Project	SHOPP	47,947	Fiscal Year Delivery	Medium	New	Permit.
10-1G020	ALP	088	Carson TMS	SHOPP	16,891	Fiscal Year Delivery	Low	New	Permits.
10-0V690	MER	059	Merced Pavement Anchor Project	SHOPP	27,554	Fiscal Year Delivery	Very High	Same as Previous	Permits-This project requires a Central Valley Flood Protection Board (CVFPB) Encroachment Permit with Section 408 from the Army Corps of Engineer for the replacement of Mariposa Creek Bridge. The time frame given by the CVFPB is 12-18 months. The fiscal year delivery is at risk.
10-1K620	MER	059	SR 59 Intersection Control	SHOPP	11,483	Fiscal Year Delivery	Very High	Same as Previous	Right of Way Acquisition and Utility Conflicts/Relocation-The project has challenging right of way acquisition and utility conflicts involving the City of Merced and several utility companies including PG&E and AT&T. The fiscal year delivery is at risk.
10-1K820	CAL	026	HWY 26/49 Intersection Control Improvement	SHOPP	18,952	Fiscal Year Delivery	High	Same as Previous	Right of Way Acquisition; Utility Conflicts/Relocation; and Litigation.
10-1M230	SJ	005	SJ I-5 Mathews Rd Intersection Improvements	LOCAL	14,448	Fiscal Year Delivery	High	Same as Previous	Right of Way Acquisition.
10-1J510	MER	099	Chowchilla River CVEF	SHOPP	8,776	Fiscal Year Delivery	High	Increased	Permits.
10-0P924	MPA	140	Alternative Delivery Method (CMGC) Ferguson Rock Avalanche Shed	SHOPP	8,240	Fiscal Year Delivery	Low	Decreased	Alternative Delivery Method.
12-0R312	ORA	091	12-0R312, Rte 091, Multi-Asset Project	SHOPP	24,769	Fiscal Year Delivery	Low	Same as Previous	Stakeholder Concerns.
12-0S051	ORA	005	12-0S051, Rte 5, Multi Asset from I-405 to Yale Ave. OC	SHOPP	36,400	Fiscal Year Delivery	High	Same as Previous	Right of Way Acquisition; Utility Conflicts/Relocation; and Railroad Involvement.
12-0S052	ORA	005	12-0S052, Rte5, Multi Asset Project from Yale Ave OC to SR55	SHOPP	25,819	Fiscal Year Delivery	High	Same as Previous	Right of Way Acquisition.
Pre-Construction-COS Supplementals: 50 projects with a programmed budget of \$235M and risks between \$36M to \$71M									
01-40110	MEN	001	Albion River Bridge	SHOPP	5,500	PA&ED	Low	Same as Previous	Stakeholder Concerns.
01-0L310	HUM	101	Benbow CAPM	SHOPP	505	PA&ED	Very High	Same as Previous	Design Strategy Changes; Funding Constraints; and Stakeholder Concerns-The estimated resource needs for this phase was based on the historical cost data for similar projects at the time of programming. After detailed analysis and external stakeholder consultation, the design was modified from what was originally proposed in the k-phase; additional resources are now needed to complete Project Approval and Environmental Document. In addition, the project needs: more resources for truck lane operations analysis and coordination with the California Highway Patrol; additional metal beam guardrail evaluations; a repair strategy for a deep crack in the roadway structural section (identified in fall 2024); and additional signage for wildlife crossings. The project will likely need supplemental funds.
03-3F540	SAC	099	Lagoon Creek and American River Bridge	SHOPP	50,000	Right of Way Support	Low	Increased	Design Strategy Changes and Right of Way Acquisition.
03-0H160	YUB	070	Marysville Railroad Bridge Rehab (SHOPP)	SHOPP	2,500	Right of Way Support	Low	Same as Previous	Litigation; Design Strategy Changes; Right of Way Acquisition; Railroad Involvement; and Utility Conflicts/Relocation.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Pre-Construction-COS Supplementals-Continued									
03-1H140	BUT	099	Butte 99 Road Rehab In Gridley	SHOPP	510	Right of Way Support	Low	New	Design Strategy Changes and Stakeholder Concerns.
03-1H240	PLA	049	American Canyon Roundabout	SHOPP	585	Right of Way Support	Low	New	Railroad Involvement.
03-3H590	PLA	080	Blue Canyon Pavement Rehabilitation	SHOPP	5,700	PS&E	Very High	New	Design Strategy Changes and Railroad Involvement-Per request from the Union Pacific Railroad (UPRR), the project development team has made modifications to project elements including changes to retaining walls, slopes, and drainage. Since the programmed design resources are not projected to meet the anticipated updated needs, supplemental funds will be likely needed.
03-4H890	ED	050	S. Lake Tahoe Safety Project	SHOPP	750	Right of Way Support	Medium	Same as Previous	Right of Way Acquisition.
03-0I440	BUT	162	Oroville CAPM	SHOPP	540	Right of Way Support	Low	Same as Previous	Design Strategy Changes and Right of Way Acquisition.
03-0J550	PLA	080	Whitmore Maintenance Station Rehab	SHOPP	2,147	PS&E	Low	Same as Previous	Design Strategy Changes.
03-3J790	NEV	080	Donner Summit SRRA ACP Replacement	SHOPP	710	PA&ED	Low	New	Construction Site Issues and Utility Conflicts/Relocation.
04-1G900	SM	082	82 ADA Burlingame	SHOPP	1,200	PS&E	Medium	New	Construction Site Issues.
04-4J820	NAP	121	Tulucay Creek Bridge Replacement	SHOPP	3,248	PS&E	Medium	Same as Previous	Funding Constraints.
04-0K530	ALA	580	0K530 Install Ramp Metering	SHOPP	3,441	PS&E	Low	Decreased	Design Strategy Changes.
04-0K810	SM	082	SM 82 Roadway Rehab 3R	SHOPP	8,181	PS&E	Medium	New	Construction Site Issues.
04-0P910	ALA	680	BRIDGE SCOUR MITIGATION	SHOPP	1,850	PS&E	High	Same as Previous	Design Strategy Changes and Permits.
04-0Q010	SM	001	SM1 Br Seismic+Rail	SHOPP	2,866	PA&ED	Low	Same as Previous	Environmental Mitigation.
04-0Q480	SM	084	STORM DAMAGE PERMANENT RESTORATION	SHOPP	809	PS&E	Medium	New	Environmental Mitigation.
04-1Q620	NAP	121	Napa SR 121 CAPM	SHOPP	2,535	PS&E	Medium	New	Design Strategy Changes.
04-1Q820	SF	101	04-1Q820_SF 101-Roadway Rehab 3R	SHOPP	4,500	PS&E	Medium	New	Alternative Delivery Method and Construction Site Issues.
05-1J840	SBT	101	SBT US101 Pavement Rehabilitation Project	SHOPP	1,665	PA&ED	Very High	Increased	Design Strategy Changes-The project needs additional technical studies and surveys to support the evaluation of the wildlife under crossings to provide hydrological studies and extensive topographical surveys through a densely vegetated and steep creek channel. The project will likely need supplemental funds.
05-1J900	SB	101	Santa Barbara 101 Pavement Project	SHOPP	3,636	PS&E	High	Same as Previous	Design Strategy Changes.
05-1K440	MON	101	King City CAPM	SHOPP	2,021	PS&E	Medium	New	Design Strategy Changes and Funding Constraints.
05-1M400	SCR	009	Felton Pedestrian Safety Improvements	SHOPP	2,263	PS&E	Very High	Same as Previous	Design Strategy Changes and Right of Way Acquisition-Due to late breaking right of way acquisition issues involving PG&E guy wire and condemnation work, certain functional units were required to spend more time than anticipated in the Plans, Specifications, and Estimates phase. The project will likely need supplemental funds.
05-1C361	SB	217	217 San Jose Crk Bridge Mitigation Planting & Monitoring	SHOPP	180	PS&E	Medium	Same as Previous	Design Strategy Changes.
05-1Q450	SB	135	Los Alamos Connected Community Project	STIP	710	PA&ED	Medium	Same as Previous	Funding Constraints and Stakeholder Concerns.
06-0U290	KER	184	Weedpatch Hwy 3R Rehab	SHOPP	3,000	PS&E	Medium	Same as Previous	Railroad Involvement.
06-0W810	KER	155	Delano SR-155 Rehab (3R)	SHOPP	2,300	PS&E	Very High	Increased	Design Strategy Changes; Railroad Involvement; and Utility Conflicts/Relocation-The project involves complex utility and railroad related issues. Late in the Design phase, Union Pacific Railroad provided additional requirements that necessitate potholing and additional staff work. The project will likely need supplemental funds.
06-0W810	KER	155	Delano SR-155 Rehab (3R)	SHOPP	540	Right of Way Support	Very High	Same as Previous	Right of Way Acquisition and Complete Streets-At the time of programming, there were only five right of way parcels identified as part of the scope of work. However, as the project was being developed, it was determined that an additional 21 parcels (both temporary construction easements and permits to enter to construct) were being impacted. This required right of way engineering, appraisals, acquisitions, and utility units to work with more property owners and provide additional information for the design team. The project will likely need supplemental funds.
06-0W830	KER	033	South Taft Rehab	SHOPP	2,100	PA&ED	Medium	Increased	Environmental Mitigation.
06-0W800	FRE	099	El Dorado to Clinton Rehab	SHOPP	2,150	Right of Way Support	Very High	Increased	Right of Way Acquisition and Railroad Involvement-The project was split into two construction contract packages in 2024 in order to maintain the critical path for the construction schedule by greatly reducing the right of way (RW) requirements that are not needed for the first orders of work on the project. The RW team focused on the first package parcel requirements while also still acquiring the balance of about 42 parcels that are extensive. Some parcels experienced changes due to property owner discussions and subsequent alterations. These have contributed to the increased support cost. The project will likely need supplemental funds for RW support.
06-0W800	FRE	099	El Dorado to Clinton Rehab	SHOPP	20,000	PS&E	Very High	Same as Previous	Design Strategy Changes; Alternative Delivery Method; Railroad Involvement; Right of Way Acquisition; Utility Conflicts/Relocation; and Complete Streets-This project is a very large rehabilitation project in an urban area utilizing the Construction Manager/General Contractor alternative delivery method. The project was recently split into two construction contract packages in order to maintain the critical path for the construction schedule by reducing the right of way requirements for the first orders of work. Approximately, five additional months will be needed for the Plans Specifications & Estimates phase, as this time is needed for acquisition of the remaining right of way; the processing of the Construction and Maintenance Agreement for the railroad; and the Design's efforts to split the project into two packages. The additional time and efforts could result in exceeding the allocation amount.
06-0X070	TUL	245	Tul 245 Culvert Rehab	SHOPP	1,095	Right of Way Support	Low	New	Permits; Right of Way Acquisition; Bidding Environment/Award Issues; and Environmental Mitigation.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Pre-Construction-COS Supplementals-Continued									
06-0X430	FRE	099	West Ave. Maintenance Station Upgrade	SHOPP	1,800	PS&E	Very High	New	Permits and Design Strategy Changes-The design package needs to be updated to reflect the requirements from the State Fire Marshall. The changes to the design required more support resources from Structure Design. The project will likely need supplemental funds.
06-0Y410	FRE	180	Dickenson Roundabout	SHOPP	1,700	PS&E	Low	New	Utility Conflicts/Relocation.
06-1E960	MAD	041	SR 41 - CAPM	SHOPP	1,200	PA&ED	Low	New	Construction Site Issues.
07-31350	LA	001	Solstice Creek	SHOPP	2,583	Right of Way Support	Medium	Same as Previous	Right of Way Acquisition.
07-32090	LA	001	SGR Bridge Rail Upgrade	SHOPP	950	Right of Way Support	Very High	Increased	Permits-There are a total of 9 parcels with 8 ownerships impacted and one private utility relocation required by this project. 7 parcels with 6 different ownerships are expected to reach settlement. The remaining 2 parcels are currently proceeding with condemnation. One private utility relocation is pending currently. The private utility owner's protest is under legal review and it will likely result in legal action. The project will likely need supplemental funds for Right of Way Support.
07-32090	LA	001	SGR Bridge Rail Upgrade	SHOPP	2,759	PS&E	Very High	Increased	Permits-This project is at risk due to the permit requirements to obtain the Environmental Certification. The project is located on the San Gabriel River Bridge Channel, which requires the following permits: 408 and 404 permit for the United States Army Corps of Engineers; 1602 Permit from the Department of Fish and Game; 401 Permit for the Clean Water Act; the California Coastal Commission Permit from the California Coastal Commission; and the Coast Guard Permits from the Coast Guards. The permits complications and involved coordination have increased staff time during the project development phase. The project will likely need supplemental funds.
07-33370	LA	138	SR-138 ADA Palmdale	SHOPP	1,401	Right of Way Support	Medium	Same as Previous	Right of Way Acquisition.
07-34650	LA	091	SR-91 OHS & ROADSIDE	SHOPP	2,851	PS&E	Low	Same as Previous	Design Strategy Changes.
07-35320	LA	405	LA-405, Pavement Rehabilitation	SHOPP	4,800	PS&E	Very High	New	Bidding Environment/Award Issues and Unit Price Increases/Market Changes-The lowest bidding is about 24% higher than the Engineer's Estimate. However, the contractor has been deemed non-responsive for failing to complete the sub-contractor's list as required. The department is currently evaluating the remaining two bidders. The project will likely need supplemental funds to complete project development.
07-35590	VEN	034	CAPM / ADA / Complete Streets	SHOPP	3,464	PS&E	Very High	Increased	Design Strategy Changes and Utility Conflicts/Relocation-The project involves utility conflicts and complex drainage issues. Additional design support budget is needed to redesign the drainage system and amend existing task order for the utility consultant.
07-35830	LA	134	Doran Maintenance Station	SHOPP	2,470	PS&E	High	Increased	Design Strategy Changes.
07-37390	LA	710	LA-710 Upgrade Pump Plant	SHOPP	1,679	PS&E	Low	Same as Previous	Design Strategy Changes.
10-1H560	ALP	088	CAPLES LAKE REHABILITATION	SHOPP	2,526	PS&E	Very High	New	Design Strategy Changes-The programmed scope does not match the programmed budget. District is re-evaluating the project scope to potentially reduce or modify it to remain within 120% of the programmed funds. The project will likely need additional funds.
10-0J720	ALP	004	SR4 Pavement Anchor Project	SHOPP	47,947	PS&E	Very High	New	Permits-Due to the extension of plans, Specifications, and Estimates (PS&E) duration to complete the Stanislaus National Forest Construction Encroachment Permit and animal passageways, more resources are needed for: Environmental, Structure; Hydraulics; and Design to provide additional design details and information to the Stanislaus National Forest and to update the PS&E package.
10-0V690	MER	059	Merced Pavement Anchor Project	SHOPP	1,962	PS&E	Very High	Same as Previous	Permits-Due to the extension of the Plans, Specifications, and Estimates phase to complete the 408 Permit, additional resources are needed for Environmental, Hydraulics, and Design to provide design details and information to the Central Valley Flood Protection Board and the U.S. Army Corps of Engineer and to update other environmental permits. The project will likely need supplemental funds.
11-43063	SD	075	SD-075-CORONADO BRIDGE SUICIDE BARRIER	SHOPP	14,000	PS&E	High	Same as Previous	Alternative Delivery Method.
12-0T950	ORA	005	12-0T950, Rte 5, I-5 HOV Operational Improvements	SHOPP	778	PA&ED	Low	Same as Previous	Stakeholder Concerns.
Pre-Construction-Greater Than 120% Allocation: 85 projects with a programmed budget of \$2.10B and risks between \$1B to \$1.5B									
01-40110	MEN	001	Albion River Bridge	SHOPP	13,723	PS&E	Low	Decreased	Stakeholder Concerns and Right of Way Acquisition.
01-0A111	HUM	101	EEL RIVER BRIDGE SEISMIC	SHOPP	6,004	Construction Support	Medium	Same as Previous	Permits.
01-0H570	MEN	101	UKIAH REHAB	SHOPP	47,571	Construction Capital	Medium	New	Design Strategy Changes and Funding Constraints.
01-0J801	HUM	299	Blue Lake Landslide Repair	SHOPP	49,684	Construction Capital	High	Same as Previous	Design Strategy Changes; Unit Price Increases/Market Changes; and Bidding Environment/Award Issues.
01-0K680	MEN	128	Culvert Rehabilitation & Fish Passage	SHOPP	4,306	Construction Support	Medium	New	Construction Site Issues and Fish Passage.
01-0K680	MEN	128	Culvert Rehabilitation & Fish Passage	SHOPP	856	Right of Way Support	Medium	New	Right of Way Acquisition.
02-4F900	SIS	096	Happy Camp Complete Streets	STIP	6,300	Construction Capital	Low	Decreased	Complete Streets and Unit Price Increases/Market Changes.
02-0H920	SHA, TEH	005	Fix 5 Cascade Gateway	STIP	71,000	Construction Capital	Very High	Same as Previous	Unit Price Increases/Market Changes-As a result of unit price increase trends and design refinements, increased engineer's estimate may occur, which would lead to bids greater than the programmed amount.
02-0H730	SIS	096	Scott River Bridge	SHOPP	12,060	Construction Capital	Medium	Same as Previous	Design Strategy Changes and Unit Price Increases/Market Changes.
02-0H730	SIS	096	Scott River Bridge	SHOPP	1,840	PS&E	Medium	Same as Previous	Design Strategy Changes.
02-0J710	SHA	299	Potato Cut	SHOPP	6,199	Construction Capital	Medium	New	Bidding Environment/Award Issues; Design Strategy Changes; and Funding Constraints.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Pre-Construction-Greater Than 120% Allocation-Continued									
02-1J380	SHA	005	Cascade SHOPP	SHOPP	27,449	Construction Capital	Very High	Same as Previous	Design Strategy Changes and Unit Price Increases/Market Changes-As a result of unit price increase trends and additional pavement rehabilitation, increased engineer's estimate may occur, which would lead to bids greater than 120% of the programmed amount.
02-1J870	SIS	097	Grass Lake Maintenance Station	SHOPP	21,390	Construction Capital	Very High	Same as Previous	Construction Site Issues; Design Strategy Changes; and Unit Price Increases/Market Changes-The Advance Planning Study was completed for this project during the Project Approval and Environmental Document phase and a value analysis study has been completed. The two studies revealed the following items were not anticipated and needed during the planning phase: A 200-ft long by 12-ft (maximum) high concrete retaining wall and associated earthwork; the proposed multi-use building needs to be an additional 10-ft deeper to accommodate newer and bigger equipment currently in use; and the need for a fire suppression system in the new building. In addition, inflation for building construction has exceeded escalation rates. The project will likely need additional funds.
02-2J860	SHA	005	Sweetbrier Deck Replacement	SHOPP	6,340	Construction Capital	Low	Decreased	Unit Price Increases/Market Changes.
03-3H590	PLA	080	Blue Canyon Pavement Rehabilitation	SHOPP	93,500	Construction Capital	Very High	Same as Previous	Unit Price Increases/Market Changes and Design Strategy Changes-Due to the need to construct four additional retaining walls and a sound wall that were not anticipated during the project initiation phase, replacement of a bridge in lieu of widening, and increased unit prices due to inflation, the estimated construction capital cost has increased. Greater Than 120% funds will be likely needed.
03-4H980	PLA	089	Tahoe City Housing Replacement	SHOPP	3,600	Construction Capital	Very High	Same as Previous	Unit Price Increases/Market Changes-The last updated estimates revealed 50% cost increases mostly due to the current trend in unit price increases. The project will likely need additional funds.
03-0J550	PLA	080	Whitmore Maintenance Station Rehab	SHOPP	15,579	Construction Capital	Very High	New	Unit Price Increases/Market Changes-The latest cost estimate is about 26% above the Engineer's Estimate. The project will likely need additional funds.
03-0J620	SAC	099, 051	SAC 99/51 Pump Plant Project	SHOPP	17,380	Construction Capital	Very High	Increased	Design Strategy Changes-The project needs structural backfill for the pump plant storage. The most recent structural estimate observed a significant price increase due to the needed structural backfill. The project will likely need additional funds.
03-1J170	NEV, PLA	089	SR 89 CAPM	SHOPP	10,350	Construction Capital	High	Same as Previous	Unit Price Increases/Market Changes.
04-1G900	SM	082	82 ADA Burlingame	SHOPP	9,120	Construction Capital	Medium	Same as Previous	Alternative Delivery Method.
04-0G642	MRN	001	Lagunitas Creek Bridge	SHOPP	17,986	Construction Capital	High	Same as Previous	Environmental Mitigation.
04-4J990	NAP	029	TMDL Listed Pollutant Source Control	SHOPP	1,300	PS&E	Medium	Same as Previous	Right of Way Acquisition; Utility Conflicts/Relocation; and Environmental Mitigation.
04-4J830	NAP	128	Hopper Slough Bridge Replac	SHOPP	12,200	Construction Capital	High	Same as Previous	Environmental Mitigation.
04-0K530	ALA	580	OKS30 Install Ramp Metering	SHOPP	3,596	Construction Support	High	New	Construction Site Issues and Stakeholder Concerns.
04-0K810	SM	082	SM 82 Roadway Rehab 3R	SHOPP	86,161	Construction Capital	Medium	Same as Previous	Alternative Delivery Method.
04-1K720	SON	001	Rehabilitate Culvert	SHOPP	6,481	Construction Capital	Very High	New	Environmental Mitigation-Additional environmental mitigation is needed for each culvert to meet the coastal regulatory agencies' requirements resulting in additional cost to construct each culvert.
04-0P910	ALA	680	BRIDGE SCOUR MITIGATION	SHOPP	7,461	Construction Capital	Very High	Increased	Design Strategy Changes and Fish Passage-Results from recent survey data indicates additional scour at Pier 7 after the past winter storm, resulting in the increase in structure excavation and structure quantiles. The project will likely need additional funds.
04-0Q010	SM	001	SM1 Br Seismic+Rail	SHOPP	12,280	Construction Capital	Medium	Same as Previous	Design Strategy Changes.
04-0Q020	SF	101	BRIDGE REHABILITATION	SHOPP	34,308	Construction Capital	Very High	Increased	Alternative Delivery Method-This is a Construction Manager/General Contractor (CMGC) program project. The initial 95% Plans, Specifications, and Estimates (PS&E) price and the Independent Cost Estimator value show the construction capital cost would exceed the programmed value. If CMGC and Caltrans cannot bring down the agreed price to below the programmed amount, then the project would likely need additional funds.
04-0Q480	SM	084	STORM DAMAGE PERMANENT RESTORATION	SHOPP	5,334	Construction Capital	Very High	New	Unit Price Increases/Market Changes-The project is approaching the end of the design phase; however, the estimated construction capital cost is about 123% of budget. The project will likely need additional funds.
04-1Q820	SF	101	04-1Q820_SF 101-Roadway Rehab 3R	SHOPP	69,027	Construction Capital	High	Increased	Alternative Delivery Method; Design Strategy Changes; and Construction Site Issues.
05-1E030	SB	166	Guadalupe ADA	SHOPP	1,447	Construction Support	Very High	New	Design Strategy Changes and Utility Conflicts/Relocation-This project is located in a historical downtown mixed with residences and a heavy industrial area with retail components. The project involves utility conflicts and ADA compliance issues. As the project design developed, changes had to be made to design strategies to achieve ADA compliance. The project will likely need additional funds.
05-1E030	SB	166	Guadalupe ADA	SHOPP	6,192	Construction Capital	Very High	New	Design Strategy Changes and Unit Price Increases/Market Changes-The construction capital increase is the result of increased materials costs and design changes to meet the ADA requirements.
05-1H610	SB	001	Solomon Canyon Rumble Strip/Shoulder Widening	SHOPP	18,848	Construction Capital	Medium	New	Right of Way Acquisition and Utility Conflicts/Relocation.
05-1J830	SCR	001, 152	129 Paving, Sign Panels, Lighting, TMS Improvement	SHOPP	8,440	Construction Capital	High	Increased	Unit Price Increases/Market Changes and Railroad Involvement.
05-1J890	MON	101	Mon 101 Drainage	SHOPP	13,260	Construction Capital	High	New	Funding Constraints and Unit Price Increases/Market Changes.
05-1J900	SB	101	Santa Barbara 101 Pavement Project	SHOPP	52,677	Construction Capital	High	Same as Previous	Unit Price Increases/Market Changes.
05-1K020	MON	001	Mud Creek Permanent Restoration	SHOPP	2,176	Construction Capital	Very High	New	Unit Price Increases/Market Changes-The latest Engineer's Estimate shows a Construction Capital need greater than 120%. The project will likely need additional funds.
05-31601	MON	156	Castroville Boulevard Interchange	STIP	18,400	Right of Way Capital	Low	Same as Previous	Utility Conflicts/Relocation.
05-31601	MON	156	Castroville Boulevard Interchange	STIP	24,000	Construction Capital	Low	Same as Previous	Bidding Environment/Award Issues.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Pre-Construction-Greater Than 120% Allocation-Continued									
05-1K520	SB	154	Hwy 154 Drainage Improvements	SHOPP	12,979	Construction Capital	High	Same as Previous	Unit Price Increases/Market Changes.
05-1J781	SLO	101	Wellsona Planting Mitigation Project	SHOPP	277	Construction Capital	Very High	Same as Previous	Unit Price Increases/Market Changes-Based on the 95% design/estimate review, the project needs additional funds.
05-1J971	SLO	058	Santa Margarita 58 Landscape	SHOPP	400	Construction Capital	Medium	Decreased	Environmental Mitigation and Unit Price Increases/Market Changes.
06-0V610	KER	119	Pumpkin Center 3R Rehab	SHOPP	26,500	Construction Capital	Medium	Same as Previous	Unit Price Increases/Market Changes and Bidding Environment/Award Issues.
06-0W810	KER	155	Delano SR-155 Rehab (3R)	SHOPP	15,700	Construction Capital	Very High	New	Railroad Involvement and Unit Price Increases/Market Changes-Due to the Union Pacific Railroad (UPRR) requiring a cantilever crossing gate structure and additional lighting at the rail crossing, the construction capital has increased over 120% of the project programming. Additional pavement markings, electrical components and signage are also required to fulfill UPRR's requirements for the cantilever structure.
06-0W800	FRE	099	El Dorado to Clinton Rehab	SHOPP	301,613	Construction Capital	Very High	Same as Previous	Alternative Delivery Method; Unit Price Increases/Market Changes; Utility Conflicts/Relocation; and Design Strategy Changes-This project is a very large rehabilitation project in an urban area utilizing the Construction Manager/General Contractor (CMGC) alternative delivery method. The CMGC and Independent Cost Estimator (ICE) construction estimates are currently significantly higher than the Engineer's Estimate. Following the 60% Opinion of Probable Construction Cost (OPCC) exercise, the Construction Capital programmed amount was adjusted to reflect a balance between the three estimates. A higher level of detail in the 95% set of plans shows that the addition of a pedestrian overcrossing; a water main relocation; and the high inflation all contributed to the higher estimates. The project will likely need additional funds.
06-0X070	TUL	245	Tul 245 Culvert Rehab	SHOPP	8,200	Construction Capital	Low	New	Environmental Mitigation.
06-0X290	FRE	033	Coalinga SR 33 2R Rehab	SHOPP	1,800	Construction Support	High	New	Complete Streets and Funding Constraints.
06-0X430	FRE	099	West Ave. Maintenance Station Upgrade	SHOPP	15,150	Construction Capital	Very High	New	Construction Site Issues-The 100% building design package cost estimate is higher than programmed amount. This is anticipated to increase the Construction Capital cost by greater than 120% over the programmed amount.
06-0X770	KER	043	Wasco SR43/46 Intersection Improvements	SHOPP	4,400	Construction Capital	Very High	New	Unit Price Increases/Market Changes-Construction Capital estimate increased from the programmed amount as project was further refined during the design phase. Escalation of bid item prices was magnified due to project delays. The project will likely need additional funds.
06-0Y180	MAD	145	Downtown Madera CAPM	SHOPP	14,020	Construction Capital	Very High	New	Complete Streets and Unit Price Increases/Market Changes-This project involves pavement rehabilitation and Complete Street improvements on a "main street" in downtown Madera, which is an economically underserved community. The current engineer's estimate has increased beyond the programmed amount. This is attributed to increased unit prices driven by market conditions in the construction industry. It is probable that the district will request a greater than 120% construction allocation.
06-0X950	KIN	041	Kettleman Roundabout	SHOPP	6,464	Construction Capital	High	New	Design Strategy Changes and Unit Price Increases/Market Changes.
06-0W860	MAD	233	Chowchilla CAPM	SHOPP	10,575	Construction Capital	Low	New	Unit Price Increases/Market Changes and Design Strategy Changes.
06-1A470	KER	043	Santa Fe Roundabout	SHOPP	6,065	Construction Capital	High	Same as Previous	Unit Price Increases/Market Changes.
06-1A090	FRE	168	Shaver Lake Viaduct	SHOPP	7,500	Construction Support	Very High	New	Construction Site Issues-After construction staff performed cost analysis, the construction contract's working days were increased from two to four construction seasons which will increase the construction support costs.
06-0V121	MAD	099	Cottonwood Creek Bridge Rehab Landscape Project	SHOPP	160	Construction Support	Very High	New	Environmental Mitigation-The workplan for this project was done based on the preliminary estimate from its parent project. At the time when the project was split from the parent, the extent of resources needed was unknown. The current construction support estimate reflects the refined mitigation requirements from California Department of Fish and Wildlife (CDFW). The project will likely need additional funds.
07-35420	LA	001	PCH CAPM, Malibu Lagoon to Ventura CL	SHOPP	4,400	Construction Support	High	Same as Previous	Design Strategy Changes.
07-35420	LA	001	PCH CAPM, Malibu Lagoon to Ventura CL	SHOPP	23,987	Construction Capital	High	Same as Previous	Unit Price Increases/Market Changes and Bidding Environment/Award Issues.
07-35590	VEN	034	CAPM / ADA / Complete Streets	SHOPP	28,144	Construction Capital	Medium	New	Unit Price Increases/Market Changes.
07-36490	LA	138	LA-138 Roundabout at 60th St W	SHOPP	2,145	Construction Support	Very High	Same as Previous	Construction Site Issues-The funding for Construction Support phase may be inadequate due to changes in project staging and the need to keep the facility operating at one lane each way during construction instead of detouring to local streets. As a result, the number of working days has increased and additional traffic handling and coordination during construction will be required. Additional funds for Construction Support will be likely needed.
07-36490	LA	138	LA-138 Roundabout at 60th St W	SHOPP	6,153	Construction Capital	High	Same as Previous	Unit Price Increases/Market Changes.
07-36060	LA	101	Argyle-Franklin (UC) (53-0680) & Big Tujunga Wash (53-2249)	SHOPP	6,718	Construction Capital	High	Same as Previous	Design Strategy Changes and Unit Price Increases/Market Changes.
07-36060	LA	101	Argyle-Franklin (UC) (53-0680) & Big Tujunga Wash (53-2249)	SHOPP	1,219	Construction Support	High	Same as Previous	Design Strategy Changes and Unit Price Increases/Market Changes.
7-36460	LA	105	LA 105 - Drainage upgrade	SHOPP	5,448	Construction Capital	Very High	Increased	Unit Price Increases/Market Changes-The latest cost estimates are about 60% over the Engineer's Estimates to upgrade 10 wells at the Garfield Pump Station. The project will likely need additional funds.
07-36150	LA	001	PCH CAPM, SM to Malibu Lagoon	SHOPP	1,159	Right of Way Support	Very High	Increased	Design Strategy Changes and Right of Way Acquisition-Due to a recent Pacific Coast Highway (PCH) fatal accident, the department, Local Agencies, and the elected officials consider PCH, within the project limits, as a top safety priority corridor. A recent multi-agencies Road Safety Audit (RSA) was completed and identified a list of safety enhancement recommendations. The project team is asked to incorporate the safety enhancement measures that could be implemented as part of the project. These have increased the Right of Way support activities. The project will likely need additional funds for right of way support.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Pre-Construction-Greater Than 120% Allocation-Continued									
07-36150	LA	001	PCH CAPM, SM to Malibu Lagoon	SHOPP	3,140	PS&E	Very High	Increased	Design Strategy Changes and Right of Way Acquisition-Due to a recent Pacific Coast Highway (PCH) fatal accident, the department, Local Agencies, and the elected officials consider PCH, within the project limits, as a top safety priority corridor. A recent multi-agencies Road Safety Audit (RSA) was completed and identified a list of safety enhancement recommendations. The project team is asked to incorporate the safety enhancement measures that could be implemented as part of the project. These have increased the design activities.
07-37080	LA	005	LA-5 Pavement Rehab in Burbank	SHOPP	12,720	Construction Capital	Very High	Increased	Construction Site Issues and Unit Price Increases/Market Changes-Due to the proximity to Burbank Airport and for public convenience, Stage Construction with 55-Hour Closures were utilized to avoid full freeway closures and to keep 2 lanes always open. Because of the 55-Hour Closures, Rapid Set Concrete was used which drastically increased the unit prices. In addition, recent escalation in unit costs of construction material also directly impacted the project's overall cost increase. The project will likely need additional funds.
07-37640	LA	138	Replace Drainage for Segment 9	SHOPP	6,554	Construction Capital	High	Same as Previous	Design Strategy Changes and Unit Price Increases/Market Changes.
07-37930	LA	002	Angeles Hwy Stormwater	SHOPP	2,156	PS&E	Low	Decreased	Design Strategy Changes.
09-37430	MNO	395	LEE VINING REHAB	SHOPP	2,692	Construction Support	High	Same as Previous	Utility Conflicts/Relocation and Design Strategy Changes.
09-37430	MNO	395	LEE VINING REHAB	SHOPP	18,448	Construction Capital	Medium	Decreased	Unit Price Increases/Market Changes and Bidding Environment/Award Issues.
09-37450	INY	395	LONE PINE SIDEWALK	SHOPP	1,248	Construction Support	Very High	Same as Previous	Design Strategy Changes-The lack of roadway slope was observed/ calculated, and it was determined that drainage would be insufficient, and a more robust drainage system is being designed. Drainage was not included in the original scope of work in the Project Initiation Document. The drainage problem is caused by the sidewalk trapping the water that had previously sheet flowed of the highway. Subsequently, the number of construction working days has increased from 30 days to 140 days; additional construction support is likely needed.
09-37450	INY	395	LONE PINE SIDEWALK	SHOPP	2,830	Construction Capital	Very High	Same as Previous	Design Strategy Changes-The lack of roadway slope was observed, evaluated, and calculated; it was determined that the drainage would be insufficient, and a more robust drainage system is being designed. The project will likely need additional funds.
09-37480	INY	395	D9 LAB	SHOPP	2,000	Construction Support	Very High	New	Design Strategy Changes-The number of working days has increased, which has resulted in increasing the resources needed for construction support activities. The project will likely need additional funds.
10-1G620	CAL	004	CAL 4 Culvert Replacement	SHOPP	8,127	Construction Capital	High	New	Funding Constraints.
10-1H560	ALP	088	CAPLES LAKE REHABILITATION	SHOPP	24,948	Construction Capital	Very High	Same as Previous	Unit Price Increases/Market Changes-The latest Engineer's Estimate was 40.99% higher than the programmed amount due to higher unit prices since the project was programmed.
10-1G270	CAL	049	SR 49 San Andreas Complete Streets	SHOPP	16,640	Construction Capital	Medium	Same as Previous	Design Strategy Changes.
10-0V690	MER	059	Merced Pavement Anchor Project	SHOPP	20,020	Construction Capital	High	Same as Previous	Funding Constraints.
10-0X461	SJ	005	10-0X461 Stockton Channel Viaduct Bridge Replacement /Southbound	SHOPP	216,600	Construction Capital	High	Same as Previous	Funding Constraints.
10-0X462	SJ	005	10-0X462 Stockton Channel Viaduct Bridge Replacement /Northbound	SHOPP	315,900	Construction Capital	High	Same as Previous	Funding Constraints.
10-1L860	MPA	049	Repair/replace old TMS	SHOPP	1,900	Construction Capital	Very High	Same as Previous	Design Strategy Changes-A Maintenance Vehicle Pullout was added to the scope of work for maintenance to access one location. The project cost is currently 137% above the programmed construction capital cost. The project will most likely need additional funds.
10-1M170	MER	140	MER 140 Planada Intersection	SHOPP	4,520	Construction Capital	Medium	New	Construction Site Issues.
10-1J510	MER	099	Chowchilla River CVEF	SHOPP	4,732	Construction Capital	High	Increased	Design Strategy Changes.
12-0S051	ORA	005	12-0S051, Rte 5, Multi Asset from I-405 to Yale Ave. OC	SHOPP	28,930	Construction Capital	Low	Same as Previous	Unit Price Increases/Market Changes and Bidding Environment/Award Issues.
12-0S052	ORA	005	12-0S052, Rte5, Multi Asset Project from Yale Ave OC to SR55	SHOPP	19,869	Construction Capital	High	Increased	Unit Price Increases/Market Changes and Bidding Environment/Award Issues.
Pre-Construction-Supplementals to Award: 7 projects with a programmed budget of \$135M and risks between \$23M to \$49M									
03-0H160	YUB	070	Marysville Railroad Bridge Rehab (SHOPP)	SHOPP	15,300	Right of Way Capital	Very High	Same as Previous	Right of Way Acquisition and Utility Conflicts/Relocation-The project involves coordination with the property owners, utility owners, and railroad. Due to complex right of way issues, the project cost has increased. The project will likely need supplemental funds.
05-1G950	SCR	009	SCR 9 Upper Drainage and Erosion Control Improvements	SHOPP	7,179	Construction Capital	Very High	New	Bidding Environment/Award Issues and Unit Price Increases/Market Changes- The bid opening got pushed out three months due to bidder inquiries regarding crane placement to install a 10' diameter reinforced concrete pipe (RCP) culvert. This inquiry necessitated obtaining a larger temporary construction easement (TCE) than anticipated. In addition, more trees will need to be removed and this required an addendum to the CEQA document. Bids opened recently and the Low bidder was 29% above the Engineer's Estimate. The project will likely need supplemental funds.
07-35320	LA	405	LA-405, Pavement Rehabilitation	SHOPP	88,156	Construction Capital	Very High	New	Bidding Environment/Award Issues and Unit Price Increases/Market Changes-The lowest bidder is about 24% higher than the Engineer's Estimate. However, the contractor has been deemed non-responsive for failing to complete the sub-contractor's list as required. The department is currently evaluating the remaining two bidders. The project will likely need supplemental funds for Construction Capital.
08-1J320	RIV	074	RIV 074 LAKE ELSINORE FACILITY	SHOPP	6,569	Construction Capital	Very High	New	Bidding Environment/Award Issues-The lowest bidder is non-responsive. The 2nd lowest bidder is 24.9% over the Engineer's Estimate, exceeding the G-12 capacity. Supplemental funds are needed to award this project to the lowest bidder.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Pre-Construction-Supplementals to Award-Continued									
08-1M040	SBD	040	SBD 040 OLD PLANE AND OVERLAY	SHOPP	5,550	Construction Capital	Very High	Increased	Bidding Environment/Award Issues and Funding Constraints-Lowest Bidder was 36.1% over the engineers estimate. The team evaluated other options such as scope revision. However, this is a safety project and re-advertising the project would delay this needed safety project to reduce the number of accidents. The project will likely need supplemental funds.
10-1H360	SJ	004	SR 4 OLD RIVER BRIDGE MAINTENANCE	SHOPP	4,150	Construction Capital	Very High	Same as Previous	Bidding Environment/Award Issues-The Department received three bids. The lowest bidder is 50.5% above the Engineer's Estimate. The project will likely need supplemental funds.
10-1H700	CAL	049	SR 49 Angels Camp Complete Streets	SHOPP	7,959	Construction Capital	Medium	New	Unit Price Increases/Market Changes.
During Construction-COS Supplementals: 37 projects with a programmed budget of \$153M and risks between \$32M to \$43M									
01-43640	DN	101	Dr. Fine Bridge Replacement	SHOPP	13,355	Construction Support	Very High	Same as Previous	Construction Site Issues; Permits; and Claims/Arbitration-The contractor has submitted several claims, 3 of which are due to differing site conditions that were discovered during pile construction. Existing bridge foundations were determined to be more exposed than originally anticipated due to river scour, resulting in unforeseen concrete removal work. The large amount of work resulted in increased inspections and Materials Engineering and Testing Services support costs as well. Due to the complexity of the bridge design and construction, environmental mitigation requirements, current claims by the contractor, and the probability of an additional construction season, a supplemental for construction support will be needed to complete construction.
01-0C500	HUM	036	Bridge Rail Replacement - 3 bridges	SHOPP	4,325	Construction Support	Medium	Same as Previous	Design Strategy Changes.
01-0A131	MEN	162	Eel River Bridge Replacement	SHOPP	4,158	Construction Support	Medium	New	Construction Site Issues.
02-1H58U	PLU	070	Cromberg Combined	SHOPP	9,130	Construction Support	Medium	Same as Previous	Unit Price Increases/Market Changes.
03-2F080	YUB	070	Marysville ADA	SHOPP	1,000	Construction Support	Low	New	Utility Conflicts/Relocation and Construction Site Issues.
03-0A570	YUB	020	Browns Valley Rehab	SHOPP	2,500	Right of Way Support	Low	New	Right of Way Acquisition.
03-2F320	YUB	020	Loma Rica Rd./Spring Valley Road Widening & Rehab	SHOPP	950	Right of Way Support	Low	New	Right of Way Acquisition and Utility Conflicts/Relocation.
03-3F060	GLE	162	Butte City Bridge Replacement	SHOPP	1,400	Right of Way Support	Low	New	Right of Way Acquisition and Permits.
03-3F060	GLE	162	Butte City Bridge Replacement	SHOPP	10,000	Construction Support	Very High	Same as Previous	Construction Site Issues; Utility Conflicts/Relocation; and Permits-Previously unknown and unidentified underground utilities have been discovered along with manmade buried objects within the vicinity of the existing and new bridge. The project also has complex permit and construction site issues that need additional support resources. The project will likely need supplemental funds for Construction Support.
03-0H670	SAC	050, 051	Sac 99 Fiber Optics	SHOPP	1,515	Construction Support	Low	New	Design Strategy Changes.
03-1H630	SAC	099	SR 99 Aux Lanes Elk Grove Blvd to Calvine	SHOPP	2,000	Construction Support	Very High	New	Construction Site Issues and Utility Conflicts/Relocation-This project consists of construction of two auxiliary lanes and upgrade of four ramp metering systems. Recent changes such as task orders to support the shortage of state inspectors and resident engineers have made our current budget insufficient. Consultants have a higher billing rate than Caltrans staff and therefore have used more budget than was programmed because the estimate was based on Caltrans staff performing the work. The project will likely need supplemental funds.
03-2H130	SAC	005	Sac 5 Highway Planting Rehab	SHOPP	510	Construction Support	Low	Increased	Construction Site Issues.
03-0J010	PLA	028	PLA-28 Rehab	SHOPP	740	Right of Way Support	Low	New	Design Strategy Changes and Right of Way Acquisition.
04-0A020	SON	001	Gleason Realignment	SHOPP	700	Right of Way Support	High	Increased	Right of Way Acquisition.
04-4A800	ALA	880	Lake Merritt Railroad Bridge Replacement	SHOPP	3,600	Construction Support	High	Same as Previous	Construction Site Issues.
04-2640F	SON	101	MSN Segment C2	LOCAL	12,600	Construction Support	Low	Same as Previous	Construction Site Issues and Environmental Mitigation.
04-3G630	SCL	009	SCL 9 Saratoga Bridge Project	SHOPP	3,100	Construction Support	Medium	Same as Previous	Construction Site Issues.
04-0J560	SCL	101	SCL 101 Br. Rail Replacement/Upgrade	SHOPP	3,050	Construction Support	Medium	Same as Previous	Construction Site Issues and Railroad Involvement.
04-0K800	MRN	101	MRN 101 ADA	SHOPP	120	Right of Way Support	High	Same as Previous	Right of Way Acquisition and Utility Conflicts/Relocation.
04-2K710	ALA	061	ALA-61 CAPM	SHOPP	1,785	Construction Support	Low	Decreased	Construction Site Issues.
04-4K980	ALA	980	D4 HQ Elevators Repair	SHOPP	864	Construction Support	Medium	Decreased	Construction Site Issues and Permits.
05-1C360	SB	217	Hwy 217 San Jose Crk Bridge Replacement	SHOPP	6,782	Construction Support	Medium	Same as Previous	Construction Site Issues and Design Strategy Changes.
05-3307A	SLO	046	Cholame Segment	STIP	11,221	Construction Support	Low	New	Construction Site Issues.
05-1H430	SB	101	San Jose Creek Bridge replacement	SHOPP	4,405	Construction Support	Very High	Same as Previous	Construction Site Issues and Design Strategy Changes-This project is located in one of the most traveled locations in District 5 within an environmentally sensitive creek area. Discharge into waterway has caused permit violation, requiring additional coordination. If additional Construction Support funding is not acquired, the contractor's oversight tasks will be at risk. The project development team closely collaborates as issues arise in construction. Traffic control and stage construction changes have been determined in the field. These issues have increased the project cost. The project will likely need additional funds.
05-1H860	SB	101	Gaviota-Nojoqui CAPM	SHOPP	6,214	Construction Support	High	Same as Previous	Construction Site Issues and Design Strategy Changes.
06-0Q920	KER	099	Union Ave to White Lane 2R Rehab	SHOPP	6,400	Construction Support	Medium	Same as Previous	Construction Site Issues.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
During Construction-COS Supplementals-Continued									
06-0R210	MAD	041	Ranchos Rehab	SHOPP	3,550	Construction Support	Very High	New	Environmental Mitigation; Construction Site Issues; Environmental Document Change; Funding Constraints; and Permits-There are two issues that triggered increased costs: 1) The Contractor's placement of surplus soil and 2) The mitigation for anticipated impacts from the project. During construction, surplus soil was placed by the Contractor, without the State's knowledge, in area identified as an environmental sensitive Area. This damaged the biological and cultural resources. It also violated our Memorandum of Agreement with the State Historic Preservation Office, as well as our Biological Opinion from the United States Fish and Wildlife Service. The project will likely need supplemental funds.
06-0V120	MAD	099	COTTONWOOD CREEK BRIDGE REPLACEMENT (Br. No. 41-0065 L/R&S)	SHOPP	6,100	Construction Support	Low	New	Construction Site Issues.
06-0X060	FRE	198	Fresno 198 Culvert Rehab	SHOPP	2,800	Construction Support	Medium	Same as Previous	Environmental Mitigation.
06-0X380	KER	166	Maricopa Highway CAPM	SHOPP	1,950	Construction Support	Medium	New	Environmental Mitigation and Permits.
06-0Y130	KER	033	Blackwell's Corner CAPM	SHOPP	3,550	Construction Support	Very High	Same as Previous	Environmental Mitigation and Environmental Document Change-An amended California Department of Fish and Wildlife (CDFW) Incidental Take Permit (ITP) was required as a result of multiple sightings of a third protected species, Blunt-Nosed Leopard Lizard (BNLL), not previously found within the project limits during the protocol surveys. CDFW instructed Caltrans to stop construction operation in October 2024 and requested Caltrans to amend the existing ITP to add BNLL as a covered species. Caltrans applied and obtained the amended ITP in January 2025. The amended ITP has additional environmental mitigation for the added species resulting in a slower than normal construction operation due to the need that qualified environmental staff be present during the entire construction operation. The project support cost has increased. The project will likely need supplemental funds.
07-29140	LA	001	TRANCAS CREEK BRIDGE	SHOPP	740	Right of Way Support	Medium	Same as Previous	Right of Way Acquisition and Utility Conflicts/Relocation.
09-21341	INY	395	OLANCHA/CARTAGO 4-LANE	STIP	1,058	Construction Support	Medium	New	Construction Site Issues.
10-0G830	MER	152	MERCED SEISMIC RESTORATION	SHOPP	3,000	Construction Support	Medium	Same as Previous	Construction Site Issues.
10-1H500	CAL	004	Bridge Replacement	SHOPP	10,189	Construction Support	Medium	Increased	Funding Constraints.
10-1G250	MER	033	Mer 33 Curb Ramps	SHOPP	5,114	Construction Support	Medium	Same as Previous	Utility Conflicts/Relocation.
12-0P671	ORA	005, 022, 055, 057, 091, 405	12-0P671 Integrated Corridor Mgmt on 3 State Route	SHOPP	2,990	Construction Support	High	New	Design Strategy Changes.
During Construction-Supplementals to Complete Construction: 32 projects with a programmed budget of \$603M and risks between \$73M to \$106M									
01-0A131	MEN	162	Eel River Bridge Replacement	SHOPP	18,808	Construction Capital	Medium	New	Construction Site Issues.
02-4F900	SIS	096	Happy Camp Complete Streets	STIP	6,300	Construction Capital	Low	Decreased	Complete Streets and Construction Site Issues.
03-3F060	GLE	162	Butte City Bridge Replacement	SHOPP	85,000	Construction Capital	Low	Same as Previous	Permits; Utility Conflicts/Relocation; and Construction Site Issues.
03-3H540	BUT	070	Pulga 70 Profile Change	SHOPP	31,230	Construction Capital	Medium	Same as Previous	Construction Site Issues and Claims/Arbitration.
03-0N620	SAC	050	SAC Translab HVAC repair	SHOPP	15,000	Construction Capital	Very High	New	Construction Site Issues and Design Strategy Changes-The initial scope of the Emergency Directors Order was based on a preliminary assessment conducted by District Maintenance Engineering, Division of Engineering Services Materials Engineering and Testing Services (METS), Translab Facility staff, and Construction staff to evaluate HVAC issues. The review aimed to assess the scope of repairs needed for the 171,195 SF building's non-compliant HVAC framework and to identify critical repairs for non-operational chillers and other components. Following approval of the Emergency Directors Order, ACCO Engineered Systems was selected as the Prime Contractor to address ventilation deficiencies. ACCO's Hygienist conducted a comprehensive building-wide ventilation assessment based on the total air volume needs by reviewing all laboratory operations. The report highlighted significant design flaws in the lab ventilation system that were unknown when the Emergency Directors Order was approved. The project will likely need additional funds.
04-0A020	SON	001	Gleason Realignment	SHOPP	15,398	Right of Way Capital	High	Same as Previous	Environmental Mitigation.
04-4A800	ALA	880	Lake Merritt Railroad Bridge Replacement	SHOPP	7,225	Construction Capital	Very High	Increased	Construction Site Issues and Unit Price Increases/Market Changes-Due to the delay in the utility relocation and issues associated with the Union Pacific Railroad (UPRR) maintenance contract, additional construction capital funds is needed to address construction start delay from August 2022 to June 2025. Caltrans has been coordinating with UPRR to relocate the utilities.
04-2A251	SCL	152	Hecker Pass Planting and ENV mitigation	SHOPP	1,100	Construction Capital	Medium	Same as Previous	Construction Site Issues.
04-0J560	SCL	101	SCL 101 Br. Rail Replacement/Upgrade	SHOPP	9,606	Construction Capital	Medium	Decreased	Construction Site Issues and Railroad Involvement.
04-2J790	SM	084	SM-1/84, Pillaritos/S.Gregorio RSP	SHOPP	4,807	Construction Capital	Medium	Decreased	Permits.
04-4J750	ALA	084	Install Outer Separation Barriers	SHOPP	14,686	Construction Capital	Medium	Same as Previous	Funding Constraints and Construction Site Issues.
04-2K720	CC	004	CC-4 CAPM	SHOPP	55,281	Construction Capital	Medium	Same as Previous	Construction Site Issues.
04-2K950	ALA	580	04-2K950_SF 101_REPLACE MATERIAL'S LAB	SHOPP	22,635	Construction Capital	Medium	New	Construction Site Issues.
04-4K980	ALA	980	04 HQ Elevators Repair	SHOPP	5,626	Construction Capital	Medium	Decreased	Construction Site Issues.
05-1H690	MON	101	Prunedale CAPM	SHOPP	36,207	Construction Capital	Very High	Same as Previous	Construction Site Issues-Additional work is deemed required to complete construction. If the work is not completed, problematic safety issues will remain. The project will likely need supplemental funds.
05-1H860	SB	101	Gaviota-Nojoqui CAPM	SHOPP	61,719	Construction Capital	Low	Same as Previous	Construction Site Issues and Design Strategy Changes.
05-1H960	SB	135	Route 135 Signal Modifications	SHOPP	12,285	Construction Capital	Medium	Same as Previous	Construction Site Issues and Design Strategy Changes.
06-0V760	TUL	099	Warlow SRRA Infrastructure Upgrades	SHOPP	7,000	Construction Capital	Low	Same as Previous	Bidding Environment/Award Issues and Unit Price Increases/Market Changes.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
During Construction-Supplementals to Complete Construction-Continued									
06-0X260	TUL	198	Tul 198 Culvert Rehab	SHOPP	11,088	Construction Capital	Low	New	Construction Site Issues.
06-0X700	TUL	063	Visalia Mall CAPM	SHOPP	11,800	Construction Capital	Medium	New	Construction Site Issues.
06-0Y130	KER	033	Blackwell's Corner CAPM	SHOPP	10,490	Construction Capital	Very High	Same as Previous	Environmental Document Change and Environmental Mitigation-An amended California Department of Fish and Wildlife (CDFW) Incidental Take Permit (ITP) was required as a result of multiple sightings of a third protected species, Blunt-Nosed Leopard Lizard (BNLL), not previously found within the project limits during the protocol surveys. CDFW instructed Caltrans to stop construction operation in October 2024 and requested Caltrans to amend the existing ITP to add BNLL as a covered species. Caltrans applied and obtained the amended ITP in January 2025. The amended ITP has additional environmental mitigation for the added species. The project will likely need supplemental funds to cover the projected increase in Construction Capital cost.
06-0W90U	TUL	216, 198	LOVERS LANE REHAB & IMPROVEMENTS COMBINE	SHOPP	20,900	Construction Capital	Low	New	Utility Conflicts/Relocation.
07-32080	LA	164	SR-164 ADA	SHOPP	5,626	Construction Capital	Low	Decreased	Construction Site Issues.
07-32570	LA	005	Construct Southern Regional Repair Shop	SHOPP	23,400	Construction Capital	Low	Same as Previous	Claims/Arbitration.
10-0G830	MER	152	MERCED SEISMIC RESTORATION	SHOPP	13,661	Construction Capital	Medium	Same as Previous	Construction Site Issues.
10-1C270	STA	005	WESTLEY REST AREA	SHOPP	17,166	Construction Capital	Medium	Same as Previous	Construction Site Issues.
10-0S680	ALP	004	ALPINE CULVERT REHAB	SHOPP	2,598	Construction Capital	Low	Decreased	Claims/Arbitration.
10-1C330	SJ	205	SR205 SMART CORRIDOR - PHASE 2	SHOPP	20,100	Construction Capital	High	New	Construction Site Issues.
10-1F400	SJ	005	SJ Changeable Message Signs	SHOPP	11,110	Construction Capital	Low	Same as Previous	Claims/Arbitration.
12-0H047	ORA	241	12-0H047 SR 241, Capital Preventive Maintenance Project	SHOPP	18,927	Construction Capital	Low	Same as Previous	Funding Constraints and Construction Site Issues.
12-0Q930	ORA	005	12-0Q930 Bridge Project on Route 5 and 133	SHOPP	16,966	Construction Capital	Low	Decreased	Railroad Involvement.
12-0P671	ORA	005, 022, 055, 057, 091, 405	12-0P671 Integrated Corridor Mgmt on 3 State Route	SHOPP	8,888	Construction Capital	High	Same as Previous	Design Strategy Changes.
During Construction-Partnership Projects-Local Agency implementing Agency: 2 projects with a programmed budget of \$35M and risks between \$2M to \$10M									
05-34490	SBT	156	San Benito 156 Improvement Project	STIP	11,000	Construction Support	Low	Same as Previous	Unit Price Increases/Market Changes and Funding Constraints.
10-0E531	CAL	004	Wagon Trail Construction Capital Funding	SHOPP	24,325	Construction Capital	High	Same as Previous	Stakeholder Concerns.
Post-Construction-COS Supplementals: 1 project with a programmed budget of \$6M and risks between \$0.1M to \$2M									
06-36024	TUL	099	Tagus 6-Lane (Combined)	STIP	5,950	PS&E	Very High	Same as Previous	Design Strategy Changes-This project is funded from the STIP (RIP and ITIP). The Plans, Specifications, and Estimates (PS&E) component overran the programmed budget and county STIP shares were debited at the time of construction allocation for the PS&E overrun. However, after the project received the construction allocation, it was determined that the entire project limits needed to be surveyed again due to ground subsidence. This resulted in PS&E expenditures increasing over the debited amount. It is anticipated that county shares will be debited a second time after construction contract acceptance for the overrun from the PS&E component.
Post-Construction-Supplementals to Closeout: 19 projects with a programmed budget of \$1.10B and risks between \$24M to \$70M									
02-37890	SHA	005	Antlers Br Replacement	SHOPP	238,927	Construction Capital	Very High	Same as Previous	Claims/Arbitration-Contractor has submitted multiple claims and they are currently in arbitration. Supplemental funds will be likely needed.
03-1E060	YUB	070	Simmerly Slough (SHOPP)	SHOPP	750	Right of Way Support	Low	Same as Previous	Right of Way Acquisition.
03-1H240	PLA	049	American Canyon Roundabout	SHOPP	1,878	Construction Support	Medium	Same as Previous	Railroad Involvement; Utility Conflicts/Relocation; and Construction Site Issues.
03-1H270	YUB	070	YUB 70 Bridge Widening and Rail Replc	SHOPP	8,046	Construction Capital	Low	Same as Previous	Construction Site Issues.
03-1H270	YUB	070	YUB 70 Bridge Widening and Rail Replc	SHOPP	1,290	Construction Support	Low	Same as Previous	Construction Site Issues.
03-0I700	BUT	032	BUT-32 Soldier Pile Wall	SHOPP	1,560	Construction Support	Low	Same as Previous	Construction Site Issues.
05-1J470	SB	135	Bunny Avenue and North Broadway Ped Improvements	SHOPP	1,696	Construction Capital	Medium	Same as Previous	Claims/Arbitration.
06-44255	KER	046	ROUTE 46 CONV/EXWY SEGMENT 4B	STIP	23,500	Construction Capital	Medium	Decreased	Claims/Arbitration.
06-0Q280	KER	099	Bakersfield 99 Rehab I(North)	SHOPP	86,000	Construction Capital	Low	New	Construction Site Issues.
06-0U430	KER	184	Morning Drive 3R Rehab	SHOPP	1,800	Construction Support	Low	New	Utility Conflicts/Relocation.
06-0V280	KER	184	Kern 184/Sunset Roundabout	SHOPP	530	Right of Way Support	Low	New	Permits.
07-13820	LA	047	Schuyler Heim Bridge Replacement (OS)	SHOPP	79,000	Construction Support	Low	Same as Previous	Construction Site Issues.
07-13820	LA	047	Schuyler Heim Bridge Replacement (OS)	SHOPP	270,200	Construction Capital	Low	Same as Previous	Construction Site Issues.
07-1218W	LA	005	I5 North & Empire	STIP	257,090	Construction Capital	Low	Same as Previous	Claims/Arbitration.
07-29740	LA	105	105/110 lighting	SHOPP	4,920	Construction Capital	Low	Same as Previous	Claims/Arbitration.
07-30370	LA	101	US101 Median Barrier	SHOPP	35,300	Construction Capital	Low	Same as Previous	Construction Site Issues.
07-31170	LA	134	LA-134 Pavement Preservation	SHOPP	5,735	Construction Support	Low	Same as Previous	Construction Site Issues.
07-31200	LA	110	TMS Installations	SHOPP	29,000	Construction Capital	High	Same as Previous	Claims/Arbitration.
10-1G250	MER	033	Mer 33 Curb Ramps	SHOPP	5,170	Construction Capital	Medium	Decreased	Claims/Arbitration.

D-EA	County	Route	Description	Program	Programmed Budget (\$1,000s)	Risk Component	Risk Level	Risk Trend	Risk Description
Post Construction: Right of Way Adjustment: 1 project with programmed budget of \$5M and risks between \$0.1M to \$4M									
05-4482U	SB	101	Linden & Casitas Pass Interchanges	STIP	5,394	Right of Way Capital	Low	Decreased	Utility Conflicts/Relocation and Funding Constraints.

Risk Description

The Watch List describes several common risks that may affect the cost and/or schedule of ongoing projects. In all cases, the risks described in the Watch List were not accounted for at the time of the last Commission action.

The table below provides definitions and examples of each risk description.

Risk Description	Definitions and Examples
Alternative Delivery Method	The project will be delivered using an alternative delivery method, such as Design-Build or Construction Manager/General Contractor, rather than Design-Bid-Build.
Bidding Environment/Award Issues	The bidding environment changes, such as when there are materials shortages, there is a lack of competition, or contractors do not comply with Disadvantaged Business Enterprise requirements. Project may need to be re-advertised.
Claims/Arbitration	The contractor has submitted claims, or the project is in arbitration to resolve claims.
Complete Streets	There is a need to add Complete Streets elements, such as bike lanes/paths or sidewalks, to the project scope.
Construction Site Issues	A site issue that was not identified on the plans or in the specifications, such as differing site conditions or buried man-made objects.
Design Strategy Changes	The design strategy changes based on new information, such as topographical survey results, geotechnical investigation results, environmental study results, or public input. For example a bridge rehabilitation could become a bridge replacement.
Environmental Document Change	The Environmental Document type changes, such as from Categorical Exemption to Initial Study with Negative Declaration, or from Initial Study with Negative Declaration to Environmental Impact Report.
Environmental Mitigation	The need for mitigation is identified or additional mitigation is needed.
Fish Passage	There is a need to address fish passage.
Funding Constraints	A local agency may experience lower sales tax revenues or developer fees, or the State might not be able to fully fund a STIP project, which causes delays.
Litigation	Caltrans is involved in litigation regarding the project unrelated to contractor claims/arbitration.
Permits	The process of obtaining permits (from agencies such as Regional Water Quality Control Boards, Department of Fish and Wildlife, Army Corps of Engineers, Coastal Commission, or local agency encroachment permits) leads to design changes, cost increases, and/or delays.

Risk Description	Definitions and Examples
Railroad Involvement	Development of the railroad Construction and Maintenance agreement results in delays or causes design changes that increase costs.
Right of Way Acquisition	Coordination with property owners to obtain permission to enter or temporary easements, or to acquire parcels, causes delays or results in design changes that could increase costs and/or cause delays.
Stakeholder Concerns	Stakeholders raise issues that cause design changes.
Utility Conflicts/Relocation	Coordination with utility companies, either while identifying the location of utilities or while relocating utilities, causes delays and/or increases costs.
Unit Price Increases/Market Changes	Unit price increases due to current market trends and impact of inflation.

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Project Description			Support (\$1,000's)			Capital (\$1,000's)		Total (\$1,000's)		Completed Within Approved Budget?	Years Delivered: Early, Delayed, or On-time	
			Original Budget ¹	Approved Budget ²	Actual Costs	Approved Budget ²	Actual Costs	Approved Budget ²	Actual Costs			
1st Quarter												
01-0C450	MEN	101	Moss Cove, Irvine Lodge & Empire Camp SRRA's	\$ 3,903	\$ 5,120	\$ 4,757	\$ 9,981	\$ 9,458	\$ 15,100	\$ 14,214	Yes	0 On Time
01-48770	VAR	VAR	HUM-101/MEN-271 Culverts	\$ 2,962	\$ 3,271	\$ 2,315	\$ 2,450	\$ 2,160	\$ 5,721	\$ 4,475	Yes	(1) Delayed
01-0F820	HUM	101	TRINIDAD CAPM	\$ 5,264	\$ 5,852	\$ 4,961	\$ 29,381	\$ 26,142	\$ 35,233	\$ 31,103	Yes	0 On Time
01-0H780	MEN	271	McCOY CREEK SINKS	\$ 4,159	\$ 4,159	\$ 2,435	\$ 2,372	\$ 2,194	\$ 6,531	\$ 4,629	Yes	0 On Time
01-0M750	HUM	101	Earthquake Bridge Repairs	\$ 460	\$ 460	\$ 241	\$ 1,510	\$ 657	\$ 1,970	\$ 897	Yes	0 On Time
02-1H700	TRI	299	Tidy Waters	\$ 3,540	\$ 3,213	\$ 2,290	\$ 2,195	\$ 2,139	\$ 5,408	\$ 4,428	Yes	0 On Time
02-4H440	PLU	070	Plumas 70 Permanent Restoration	\$ 3,320	\$ 6,478	\$ 5,630	\$ 34,764	\$ 29,217	\$ 41,242	\$ 34,847	Yes	0 On Time
02-0K910	SIS	096	SIS 96 PM 0 to 103	\$ 750	\$ 750	\$ 741	\$ 5,100	\$ 4,563	\$ 5,850	\$ 5,304	Yes	0 On Time
03-0H670	SAC	VAR	Sac 99 Fiber Optics	\$ 3,050	\$ 3,832	\$ 3,607	\$ 19,355	\$ 19,143	\$ 23,187	\$ 22,750	Yes	0 On Time
03-4E62U	ED	050	Camino Safety Project Combined	\$ 13,920	\$ 16,763	\$ 15,751	\$ 28,239	\$ 26,760	\$ 45,002	\$ 42,511	Yes	0 On Time
03-1H80U	ED	050	ED 50 Culvert Rehab	\$ 4,560	\$ 5,102	\$ 3,927	\$ 5,318	\$ 4,735	\$ 10,420	\$ 8,662	Yes	0 On Time
03-4J160	ED	VAR	ED Storm Damage	\$ 1,200	\$ 1,200	\$ 780	\$ 6,000	\$ 5,997	\$ 7,200	\$ 6,777	Yes	0 On Time
03-4J500	SAC	051	SAC 51 Fort Sutter	\$ 423	\$ 423	\$ 344	\$ 1,413	\$ 1,264	\$ 1,836	\$ 1,607	Yes	0 On Time
03-4J560	PLA	080	PLA 80 Open-Graded	\$ 1,400	\$ 1,400	\$ 1,096	\$ 7,000	\$ 7,064	\$ 8,400	\$ 8,160	Yes	0 On Time
03-4J720	PLA	VAR	Truckee Storm Damaged Pavement	\$ 1,860	\$ 2,310	\$ 1,345	\$ 7,700	\$ 7,649	\$ 10,010	\$ 8,994	Yes	0 On Time
03-0N470	BUT	099	Bridge Hit #2 Eaton Road OC	\$ 130	\$ 130	\$ 46	\$ 450	\$ 64	\$ 580	\$ 110	Yes	0 On Time
03-0N480	BUT	099	Bridge Hit #1 20th St OC	\$ 200	\$ 200	\$ 66	\$ 800	\$ 263	\$ 1,000	\$ 329	Yes	0 On Time
04-4G380	SON	116	SON 116/Llano Rd LT	\$ 2,440	\$ 6,777	\$ 6,684	\$ 6,703	\$ 5,810	\$ 13,480	\$ 12,493	Yes	0 On Time
04-0J710	SOL	080	sol 80 raise oc	\$ 6,037	\$ 12,122	\$ 11,884	\$ 27,973	\$ 24,830	\$ 40,095	\$ 36,713	Yes	0 On Time
04-2A332	ALA	084	NILES CANYON SAFETY PROJECT	\$ 14,870	\$ 22,535	\$ 14,780	\$ 23,590	\$ 21,078	\$ 46,125	\$ 35,858	Yes	(4) Delayed
04-4J420	MRN	101	Mrn 101 Storm Damage Repair	\$ 2,637	\$ 2,722	\$ 2,685	\$ 1,701	\$ 1,565	\$ 4,423	\$ 4,250	Yes	(1) Delayed
04-0K240	SCL	237	SCL 237_Aux-Lane-EB	\$ 6,890	\$ 6,690	\$ 5,509	\$ 4,888	\$ 3,955	\$ 11,578	\$ 9,465	Yes	0 On Time
04-4K400	ALA	061	ALA Vehicle Speed Feedback Signs	\$ 1,222	\$ 1,686	\$ 1,415	\$ 1,688	\$ 1,436	\$ 3,374	\$ 2,851	Yes	0 On Time
04-0Q710	SOL	505	Storm Damage Permanent Restoration	\$ 1,129	\$ 1,383	\$ 1,267	\$ 632	\$ 436	\$ 2,015	\$ 1,703	Yes	0 On Time
04-3Y070	SCL	680	DO	\$ 800	\$ 1,016	\$ 763	\$ 4,320	\$ 3,814	\$ 5,336	\$ 4,577	Yes	0 On Time
05-0F970	MON	101	North Soledad OH Deck Rehab	\$ 2,900	\$ 6,669	\$ 5,878	\$ 6,270	\$ 4,119	\$ 12,939	\$ 9,996	Yes	(4) Delayed
05-1H220	MON	068	Pacific Grove ADA Pathway	\$ 1,737	\$ 2,031	\$ 1,864	\$ 728	\$ 697	\$ 2,759	\$ 2,560	Yes	0 On Time
05-1J460	MON	068	Hwy 68 Curve Correction	\$ 5,147	\$ 4,476	\$ 1,787	\$ 2,689	\$ 1,141	\$ 7,165	\$ 2,928	Yes	0 On Time
05-1K130	SCR	009	PM 19.97 Hairpin Tieback	\$ 3,998	\$ 4,599	\$ 3,258	\$ 3,301	\$ 2,880	\$ 7,900	\$ 6,138	Yes	0 On Time
05-1P230	VAR	101	Bridge Maintenance	\$ 1,065	\$ 1,094	\$ 657	\$ 1,681	\$ 1,404	\$ 2,775	\$ 2,060	Yes	0 On Time
06-0R190	KER	VAR	Arvin SR 223/184 Roundabout	\$ 1,000	\$ 2,550	\$ 5,251	\$ 7,352	\$ 6,320	\$ 9,902	\$ 11,571	No	2 Early
06-0V280	KER	184	Kern 184/Sunset Roundabout	\$ 3,900	\$ 4,780	\$ 4,287	\$ 4,720	\$ 3,449	\$ 9,500	\$ 7,736	Yes	0 On Time
06-0X760	KER	119	Taft Left Turn Channelization	\$ 3,433	\$ 3,503	\$ 2,875	\$ 1,532	\$ 1,410	\$ 5,035	\$ 4,286	Yes	0 On Time
06-0Y490	FRE	005	Horizontal Curve Warning Sign Update/Upgrade	\$ 2,660	\$ 3,820	\$ 2,756	\$ 6,848	\$ 5,560	\$ 10,668	\$ 8,317	Yes	(1) Delayed
06-1E200	MAD	099	MADERA 99 AND 152, BRIDGE DECK REHAB	\$ 300	\$ 465	\$ 454	\$ 1,177	\$ 1,021	\$ 1,642	\$ 1,476	Yes	0 On Time
07-26080	LA	010	TMDL- 19/20	\$ 8,320	\$ 10,483	\$ 8,931	\$ 11,244	\$ 10,065	\$ 21,727	\$ 18,996	Yes	0 On Time
07-30370	LA	101	US101 Median Barrier	\$ 5,940	\$ 13,227	\$ 11,187	\$ 40,181	\$ 37,076	\$ 53,408	\$ 48,262	Yes	1 Early
07-31040	LA	138	RHMA Overlay on NW LA-138	\$ 2,342	\$ 6,692	\$ 6,097	\$ 20,629	\$ 19,040	\$ 27,321	\$ 25,138	Yes	0 On Time
07-31130	VEN	101	Ven 101 Source Control in Ventura	\$ 698	\$ 2,731	\$ 2,514	\$ 2,387	\$ 2,004	\$ 5,118	\$ 4,518	Yes	0 On Time
07-31150	VEN	126	Stormwater Source Control	\$ 505	\$ 3,212	\$ 2,979	\$ 2,145	\$ 1,901	\$ 5,357	\$ 4,879	Yes	0 On Time
07-33500	LA	110	Bluetooth Detection System	\$ 540	\$ 1,230	\$ 916	\$ 1,062	\$ 772	\$ 2,292	\$ 1,688	Yes	0 On Time
07-34340	LA	039	LA-39/ N. Fork San Gabriel Bridge Replacement	\$ 6,426	\$ 6,426	\$ 5,502	\$ 15,359	\$ 11,211	\$ 21,785	\$ 16,713	Yes	0 On Time
07-35490	LA	001	PCH and Las Flores 010 Safety	\$ 3,308	\$ 2,906	\$ 2,018	\$ 845	\$ 603	\$ 3,751	\$ 2,621	Yes	0 On Time
07-2XC40	LA	101	Bridge Hit (Crib Wall)	\$ 1,000	\$ 832	\$ 745	\$ 3,000	\$ 2,170	\$ 3,832	\$ 2,914	Yes	0 On Time
07-2XC90	LA	005	Mudslide	\$ 500	\$ 223	\$ 111	\$ 3,305	\$ 3,129	\$ 3,528	\$ 3,240	Yes	0 On Time
08-1C720	SBD	015	SBD 15 REGRADE CENTER MEDIAN	\$ 5,020	\$ 6,142	\$ 5,278	\$ 14,237	\$ 13,204	\$ 20,379	\$ 18,482	Yes	(2) Delayed
08-0R141	SBD	040	SBD 40 NEEDLES REGRADE MEDIAN	\$ 9,600	\$ 8,655	\$ 8,261	\$ 16,976	\$ 16,654	\$ 25,631	\$ 24,915	Yes	0 On Time
08-1H820	SBD	138	SBD 138 INSTALL TRAFFIC SIGNALS - TIGER	\$ 3,035	\$ 4,138	\$ 3,903	\$ 3,961	\$ 3,268	\$ 8,099	\$ 7,171	Yes	0 On Time
08-0R302	RIV	111	RIV 111-PALM SPRINGS ADA (PHASE 2)	\$ 7,476	\$ 7,794	\$ 7,078	\$ 4,284	\$ 3,919	\$ 12,078	\$ 10,997	Yes	(2) Delayed
08-1N940	RIV	074	Nickname Required	\$ 680	\$ 680	\$ 499	\$ 2,750	\$ 2,642	\$ 3,430	\$ 3,141	Yes	0 On Time
08-1P640	SBD	002	HURRICANE HILLARY TROPICAL STORM DAMAGE	\$ 1,995	\$ 1,995	\$ 217	\$ 5,700	\$ 1,340	\$ 7,695	\$ 1,557	Yes	0 On Time
08-1P670	VAR	062	HURRICANE HILARY STORM DAMAGE	\$ 220	\$ 220	\$ 46	\$ 850	\$ 752	\$ 1,070	\$ 798	Yes	0 On Time
10-0Y610	MER	099	MERCED 99 GUARDRAIL	\$ 2,854	\$ 2,857	\$ 1,572	\$ 2,608	\$ 2,174	\$ 5,465	\$ 3,746	Yes	0 On Time
10-1E620	STA	099	Replace Drainage Pumps	\$ 3,504	\$ 3,471	\$ 3,203	\$ 6,679	\$ 6,533	\$ 10,150	\$ 9,736	Yes	0 On Time
10-1N780	MER	099	Mer-99 Rehabilitate Four Bridges	\$ 1,055	\$ 1,170	\$ 804	\$ 3,010	\$ 2,364	\$ 4,180	\$ 3,167	Yes	0 On Time

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Project Description				Support (\$1,000's)			Capital (\$1,000's)		Total (\$1,000's)		Completed Within Approved Budget?	Years Delivered: Early, Delayed, or On-time	
				Original Budget ¹	Approved Budget ²	Actual Costs	Approved Budget ²	Actual Costs	Approved Budget ²	Actual Costs			
11-41180	SD	052	SR-52 ROADSIDE SAFETY IMPROVEMENT	\$ 2,051	\$ 2,857	\$ 2,733	\$ 3,769	\$ 1,013	\$ 6,626	\$ 3,746	Yes	0	On Time
11-42870	SD	805	RECYCLED WATER PROJECT / SD I-805	\$ 1,896	\$ 1,894	\$ 1,701	\$ 1,836	\$ 1,379	\$ 3,730	\$ 3,080	Yes	0	On Time
11-42970	SD	005	REPAIR EXISTING LOOP DETECTORS	\$ 5,335	\$ 5,455	\$ 5,261	\$ 8,747	\$ 8,025	\$ 14,202	\$ 13,286	Yes	0	On Time
11-43049	IMP	008	11-IMP-008-BRIDGE REHABILITATION	\$ 1,178	\$ 2,717	\$ 2,492	\$ 8,918	\$ 7,006	\$ 11,635	\$ 9,498	Yes	0	On Time
11-43189	SD	VAR	DO - REPAIR RAMP PAVEMENT	\$ 700	\$ 1,000	\$ 356	\$ 13,000	\$ 12,474	\$ 14,000	\$ 12,830	Yes	0	On Time
12-0P700	ORA	005	Drainage restoration Segunda Deshecha Channel	\$ 2,443	\$ 3,192	\$ 2,619	\$ 3,379	\$ 2,920	\$ 6,571	\$ 5,539	Yes	0	On Time
12-0R290	ORA	022	12-0R290, Rte 22, HFST and Safety Devices Project	\$ 1,107	\$ 1,240	\$ 1,163	\$ 898	\$ 793	\$ 2,138	\$ 1,956	Yes	0	On Time
2nd Quarter													
01-0G610	HUM	101	Fortuna Median Roadside Safety Project	\$ 3,109	\$ 3,326	\$ 2,360	\$ 8,083	\$ 6,013	\$ 11,409	\$ 8,373	Yes	0	On Time
01-0E031	HUM	299	Environmental Mitigation	\$ 160	\$ 160	\$ 5	\$ 300	\$ 0	\$ 460	\$ 5	Yes	1	Early
01-0L040	DN	101	LCG Landslide Repair	\$ 3,050	\$ 3,050	\$ 1,617	\$ 19,350	\$ 19,036	\$ 22,400	\$ 20,654	Yes	0	On Time
01-0M820	MEN	253	Men 253 Emergency Storm Damage Repairs	\$ 525	\$ 525	\$ 344	\$ 1,575	\$ 1,371	\$ 2,100	\$ 1,715	Yes	0	On Time
02-1H570	SHA	299	Burney CAPM	\$ 6,180	\$ 7,152	\$ 6,136	\$ 16,540	\$ 13,964	\$ 23,692	\$ 20,101	Yes	0	On Time
02-3J030	TEH	005	Thomes Creek Bridge MTCE	\$ 1,330	\$ 1,380	\$ 1,017	\$ 4,346	\$ 3,614	\$ 5,726	\$ 4,631	Yes	0	On Time
02-0K150	VAR	070	BUT/PLU 70 Corridor Slides	\$ 1,200	\$ 2,500	\$ 2,061	\$ 28,000	\$ 21,842	\$ 30,500	\$ 23,903	Yes	0	On Time
03-3H390	SAC	005	I-5 Permit Load Mobility Improvement	\$ 67,420	\$ 19,663	\$ 7,666	\$ 7,987	\$ 7,645	\$ 27,650	\$ 15,311	Yes	0	On Time
03-3F071	SAC	051	SR-51 ARB Child Project (CP1)	\$ 900	\$ 1,100	\$ 1,075	\$ 12,000	\$ 11,757	\$ 13,100	\$ 12,831	Yes	0	On Time
03-3F072	SAC	051	SR-51 ARB Child Project (CP2)	\$ 50	\$ 250	\$ 140	\$ 10,900	\$ 10,859	\$ 11,150	\$ 10,999	Yes	0	On Time
03-2J720	ED	050	Caldor Fire 1 SR50	\$ 3,000	\$ 3,000	\$ 1,681	\$ 14,100	\$ 13,842	\$ 17,100	\$ 15,523	Yes	0	On Time
03-4I150	SAC	099	SAC 99 Storm Damage.	\$ 400	\$ 400	\$ 368	\$ 2,000	\$ 1,022	\$ 2,400	\$ 1,390	Yes	0	On Time
03-0L280	ED	050	ED 50 SLT Pavement	\$ 930	\$ 930	\$ 400	\$ 3,100	\$ 3,068	\$ 4,030	\$ 3,468	Yes	0	On Time
04-4G210	NAP	121	HIUCHICA CREEK-REMOVE CULVERTS AND WIDEN ROADWAY	\$ 3,870	\$ 9,031	\$ 8,925	\$ 10,554	\$ 8,458	\$ 19,585	\$ 17,383	Yes	(3)	Delayed
04-4J870	SM	001	SM-1 Pescadero Crk. Br. Rails	\$ 3,408	\$ 3,816	\$ 3,727	\$ 3,288	\$ 3,018	\$ 7,104	\$ 6,745	Yes	(1)	Delayed
04-0K000	NAP	029	SR 29 ADA Compliance	\$ 846	\$ 1,850	\$ 1,828	\$ 1,859	\$ 1,722	\$ 3,709	\$ 3,550	Yes	0	On Time
04-0K080	SCL	VAR	SCL VAR_Accessible Ped. Signal and Countdown Timer	\$ 3,633	\$ 4,295	\$ 4,143	\$ 9,389	\$ 7,315	\$ 13,684	\$ 11,458	Yes	0	On Time
04-2K190	SF	101	SF-101 & SM-1 Baluster Rails	\$ 4,377	\$ 4,737	\$ 4,507	\$ 7,738	\$ 7,415	\$ 12,475	\$ 11,922	Yes	0	On Time
04-2K610	SM	084	SM 84 Peek-A-Boo SPW	\$ 1,586	\$ 3,461	\$ 3,092	\$ 3,334	\$ 2,845	\$ 6,795	\$ 5,938	Yes	(1)	Delayed
04-2K820	ALA	880	ALA-880 Roadside Paving	\$ 1,400	\$ 2,149	\$ 1,917	\$ 3,106	\$ 2,884	\$ 5,255	\$ 4,801	Yes	0	On Time
04-3J062	SM	101	SM101 CAPM Curb Ramps Upgrade		\$ 3,477	\$ 2,119	\$ 19,785	\$ 18,489	\$ 23,262	\$ 20,608	Yes	0	On Time
04-3Y430	NAP	029	DO	\$ 290	\$ 290	\$ 290	\$ 980	\$ 868	\$ 1,270	\$ 1,158	Yes	0	On Time
04-3Y510	ALA	880	DO	\$ 285	\$ 310	\$ 270	\$ 1,593	\$ 1,319	\$ 1,903	\$ 1,589	Yes	0	On Time
05-0J200	SCR	001	Davenport Culvert Replacement	\$ 4,613	\$ 4,995	\$ 3,016	\$ 6,957	\$ 6,780	\$ 11,952	\$ 9,797	Yes	0	On Time
05-0A090	MON	156	Castroville Overhead	\$ 3,100	\$ 5,148	\$ 4,922	\$ 7,300	\$ 6,441	\$ 12,448	\$ 11,363	Yes	(3)	Delayed
05-0G041	SLO	101	North Paso Robles Mitigation Planting	\$ 730	\$ 730	\$ 358	\$ 267	\$ 259	\$ 997	\$ 618	Yes	1	Early
05-1H450	SB	154	Rehab Primavera & La Colina Bridges	\$ 5,301	\$ 5,071	\$ 3,558	\$ 5,509	\$ 5,389	\$ 10,580	\$ 8,947	Yes	1	Early
05-1H810	SBT	025	Rte 25 Curve Alignment Restoration	\$ 4,788	\$ 5,278	\$ 4,240	\$ 6,122	\$ 5,279	\$ 11,400	\$ 9,519	Yes	(2)	Delayed
05-1J480	SBT	025	SR 25/156 Roundabout	\$ 2,965	\$ 3,814	\$ 3,607	\$ 10,935	\$ 10,217	\$ 14,749	\$ 13,824	Yes	0	On Time
05-1Q830	SB	101	SB 192, 150 Storm Damage	\$ 850	\$ 1,050	\$ 819	\$ 3,600	\$ 2,499	\$ 4,650	\$ 3,318	Yes	0	On Time
05-1Q840	SLO	101	District Office Flood Electrical Repairs	\$ 160	\$ 160	\$ 156	\$ 400	\$ 375	\$ 560	\$ 531	Yes	0	On Time
05-1Q870	SB	001	SB Various Storm Damage 1	\$ 650	\$ 650	\$ 527	\$ 3,700	\$ 2,944	\$ 4,350	\$ 3,471	Yes	0	On Time
05-1Q880	SLO	101	District Office Flood Haz Waste Removal	\$ 300	\$ 300	\$ 86	\$ 1,250	\$ 1,240	\$ 1,550	\$ 1,326	Yes	0	On Time
05-1Q930	SCR	009	Remove Hazardous Tree - Snow	\$ 360	\$ 360	\$ 153	\$ 1,500	\$ 1,242	\$ 1,860	\$ 1,395	Yes	0	On Time
05-1R150	MON	198	Roadway Settlement	\$ 900	\$ 900	\$ 86	\$ 3,000	\$ 301	\$ 3,900	\$ 387	Yes	0	On Time
05-1S120	SB	101	Repair Nojoqui Creek Bridge Rail EFA	\$ 100	\$ 100	\$ 72	\$ 550	\$ 99	\$ 650	\$ 171	Yes	0	On Time
06-0S830	FRE	005	Tumey Gulch III	\$ 2,754	\$ 6,785	\$ 6,046	\$ 16,444	\$ 12,214	\$ 23,229	\$ 18,259	Yes	0	On Time
06-0U770	TUL	099	Raine Rest Area Infrastructure Upgrades	\$ 4,562	\$ 4,987	\$ 4,342	\$ 6,967	\$ 6,896	\$ 11,954	\$ 11,238	Yes	1	Early
06-0V780	FRE	VAR	Fre-41;99;168 & 180 Fiber Optic	\$ 4,384	\$ 4,584	\$ 3,844	\$ 13,150	\$ 11,598	\$ 17,734	\$ 15,442	Yes	0	On Time
06-1A690	KER	005	Buttonwillow Median Barrier	\$ 2,590	\$ 2,590	\$ 1,342	\$ 1,469	\$ 1,204	\$ 4,059	\$ 2,546	Yes	0	On Time
07-30480	LA	405	REPLACE DRAINAGE INLETS	\$ 877	\$ 2,531	\$ 2,286	\$ 3,497	\$ 2,909	\$ 6,028	\$ 5,195	Yes	(3)	Delayed
07-31170	LA	134	LA-134 Pavement Preservation	\$ 7,600	\$ 12,887	\$ 11,126	\$ 42,389	\$ 37,184	\$ 55,276	\$ 48,310	Yes	0	On Time
07-31200	LA	110	TMS Installations	\$ 8,845	\$ 11,470	\$ 8,863	\$ 34,315	\$ 30,127	\$ 45,785	\$ 38,989	Yes	0	On Time
07-31740	LA	105	Source Control	\$ 675	\$ 2,814	\$ 1,943	\$ 2,773	\$ 2,308	\$ 5,587	\$ 4,251	Yes	0	On Time
07-31990	LA	005	Roadside safety improv	\$ 2,973	\$ 3,128	\$ 2,580	\$ 3,102	\$ 2,539	\$ 6,230	\$ 5,119	Yes	0	On Time
07-32230	LA	110	Bridge Rail Upgrade (OS)	\$ 3,800	\$ 4,565	\$ 4,063	\$ 4,224	\$ 4,070	\$ 8,789	\$ 8,133	Yes	0	On Time
07-32330	LA	060	TMDL- 20/21	\$ 6,535	\$ 5,955	\$ 4,068	\$ 3,720	\$ 3,185	\$ 9,675	\$ 7,253	Yes	0	On Time
07-32480	LA	057	57 Sunset Xing Worker Safety	\$ 1,456	\$ 1,726	\$ 1,500	\$ 1,350	\$ 1,218	\$ 3,076	\$ 2,717	Yes	1	Early

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			Original Budget ¹	Approved Budget ²	Actual Costs	Approved Budget ²	Actual Costs	Approved Budget ²	Actual Costs			
07-32500	LA	060	TMDL- 20/21	\$ 5,344	\$ 5,344	\$ 4,219	\$ 3,982	\$ 3,718	\$ 9,326	\$ 7,937	Yes	0 On Time
07-32530	LA	118	TMDL- 20/21	\$ 8,370	\$ 6,370	\$ 4,270	\$ 3,713	\$ 3,334	\$ 10,083	\$ 7,604	Yes	0 On Time
07-32570	LA	005	Construct Southern Regional Repair Shop	\$ 8,000	\$ 12,366	\$ 11,650	\$ 22,050	\$ 20,923	\$ 34,416	\$ 32,573	Yes	0 On Time
07-32720	LA	010	I-10 TMS Between SM and DTLA	\$ 7,025	\$ 7,538	\$ 6,590	\$ 15,499	\$ 12,636	\$ 23,037	\$ 19,226	Yes	0 On Time
07-32980	LA	110	I-110 Upgrade OHS	\$ 4,354	\$ 4,454	\$ 3,529	\$ 4,430	\$ 3,571	\$ 8,884	\$ 7,100	Yes	0 On Time
07-35210	LA	138	Left Turn/ADA	\$ 2,709	\$ 2,759	\$ 2,017	\$ 1,246	\$ 1,055	\$ 4,005	\$ 3,072	Yes	(1) Delayed
07-35330	LA	110	Upgrade 3rd St. Bridge Railing with Concrete Barriers	\$ 4,549	\$ 5,331	\$ 3,791	\$ 3,478	\$ 2,861	\$ 8,809	\$ 6,652	Yes	0 On Time
07-3096U	LA	210	LA-210 2R in San Fernando	\$ 30,900	\$ 35,470	\$ 31,308	\$ 160,478	\$ 156,333	\$ 195,948	\$ 187,642	Yes	0 On Time
08-1G280	SBD	010	SBD 10 BRIDGE REHAB	\$ 1,890	\$ 2,735	\$ 2,642	\$ 2,351	\$ 1,431	\$ 5,086	\$ 4,073	Yes	0 On Time
08-1J210	SBD	060	SBD 60 @ RAMONA OC	\$ 20,542	\$ 9,967	\$ 8,147	\$ 10,028	\$ 8,402	\$ 19,995	\$ 16,549	Yes	0 On Time
08-1N930	SBD	018	Nickname Required	\$ 1,210	\$ 1,410	\$ 656	\$ 4,500	\$ 4,351	\$ 5,910	\$ 5,006	Yes	0 On Time
08-1P690	RIV	010	2023 HURRICANE HILARY TROPICAL STORM DAMAGE	\$ 220	\$ 220	\$ 201	\$ 1,100	\$ 1,000	\$ 1,320	\$ 1,201	Yes	0 On Time
08-1P860	SBD	018	Nickname Required	\$ 600	\$ 600	\$ 264	\$ 2,015	\$ 784	\$ 2,615	\$ 1,048	Yes	0 On Time
09-40290	MNO	158	158 JUNE LAKE SLOPE FAILURE DDO	\$ 160	\$ 160	\$ 8	\$ 3,123	\$ 325	\$ 3,283	\$ 332	Yes	0 On Time
10-0Y130	MER	140	MER 140 SAFETY DEVICES	\$ 3,472	\$ 4,302	\$ 3,729	\$ 4,702	\$ 3,005	\$ 9,004	\$ 6,735	Yes	(1) Delayed
10-1C800	SJ	004	SJ BRIDGE MAINTENANCE	\$ 3,222	\$ 4,726	\$ 4,165	\$ 10,729	\$ 10,532	\$ 15,455	\$ 14,697	Yes	0 On Time
10-1H510	SJ	099	SJ 99 ROADSIDE IMPROVEMENTS	\$ 2,676	\$ 2,530	\$ 1,411	\$ 2,384	\$ 1,829	\$ 4,914	\$ 3,240	Yes	0 On Time
10-1N760	SJ	004	Mormon Slough BOH, Bridge Repair	\$ 400	\$ 593	\$ 479	\$ 1,302	\$ 819	\$ 1,895	\$ 1,299	Yes	0 On Time
11-41150	SD	805	ROADSIDE SAFETY	\$ 2,550	\$ 2,921	\$ 2,864	\$ 6,315	\$ 6,151	\$ 9,236	\$ 9,015	Yes	0 On Time
11-41200	IMP	008	IMP, NEAR/IN EL CENTRO, ROADSIDE SAFETY	\$ 1,541	\$ 2,378	\$ 2,338	\$ 5,174	\$ 4,569	\$ 7,552	\$ 6,907	Yes	0 On Time
11-42210	SD	008	I-8 CULVERT REPLACEMENT	\$ 6,778	\$ 4,322	\$ 3,893	\$ 6,753	\$ 6,039	\$ 11,075	\$ 9,932	Yes	(1) Delayed
11-43001	SD	005	I-5 INSTALL HIGH TENSION CABLE BARRIER (HTCB)	\$ 2,565	\$ 2,937	\$ 2,890	\$ 6,733	\$ 6,449	\$ 9,670	\$ 9,339	Yes	0 On Time
11-2N108	SD	005	REPAIR BRIDGE DECK	\$ 1,600	\$ 1,500	\$ 1,206	\$ 1,702	\$ 1,228	\$ 3,202	\$ 2,434	Yes	0 On Time
11-43202	SD	005	DO - REMOVE TREES	\$ 300	\$ 300	\$ 87	\$ 2,000	\$ 1,947	\$ 2,300	\$ 2,034	Yes	0 On Time
12-0H150	ORA	001	12-0H150 PCH Laguna Beach Rehabilitate Pavement & ADA upgrades	\$ 6,150	\$ 6,010	\$ 5,954	\$ 7,964	\$ 7,870	\$ 13,974	\$ 13,824	Yes	0 On Time
12-0R150	ORA	055	12-0R150 55 at Chapman Upgrade Lighting, ADA	\$ 1,700	\$ 2,135	\$ 2,045	\$ 1,800	\$ 1,593	\$ 3,935	\$ 3,638	Yes	0 On Time
12-0R200	ORA	005	12-0R200 Interstate 5 Pavement Rehabilitation Project	\$ 10,242	\$ 9,860	\$ 8,274	\$ 20,410	\$ 19,258	\$ 30,270	\$ 27,532	Yes	0 On Time
12-0T740	ORA	073	12-0T740 Rte. 73 Oso Creek Channel Slope Embankment Repairs	\$ 1,232	\$ 1,232	\$ 911	\$ 3,080	\$ 2,545	\$ 4,312	\$ 3,456	Yes	0 On Time
3rd Quarter												
01-0A110	HUM	101	South Fork Eel River Bridge #04-123	\$ 3,718	\$ 9,743	\$ 8,072	\$ 7,356	\$ 6,088	\$ 17,099	\$ 14,161	Yes	(3) Delayed
01-0H191	HUM	096	BLUFF CREEK WALL	\$ 3,648	\$ 3,648	\$ 2,913	\$ 5,804	\$ 5,143	\$ 9,452	\$ 8,056	Yes	0 On Time
01-0H810	MEN	101	Comminsky Permanent Restoration	\$ 2,944	\$ 4,120	\$ 3,519	\$ 3,385	\$ 3,015	\$ 7,504	\$ 6,534	Yes	0 On Time
01-0M840	HUM	101	Hum 101 Slipout	\$ 800	\$ 5,800	\$ 2,408	\$ 19,900	\$ 14,815	\$ 25,700	\$ 17,224	Yes	0 On Time
02-4H660	SIS	096	SIS Worker Safety	\$ 3,690	\$ 3,520	\$ 2,255	\$ 4,301	\$ 3,811	\$ 7,821	\$ 6,066	Yes	0 On Time
02-1K480	VAR	070	Remove Slide and Provide Traffic control	\$ 600	\$ 600	\$ 180	\$ 3,100	\$ 2,005	\$ 3,700	\$ 2,185	Yes	0 On Time
03-0A570	YUB	020	Browns Valley Rehab	\$ 6,900	\$ 16,950	\$ 15,240	\$ 51,900	\$ 44,372	\$ 68,850	\$ 59,611	Yes	0 On Time
03-0L220	NEV	020	NEV 20 Shoulder Erosion	\$ 900	\$ 900	\$ 467	\$ 3,200	\$ 3,134	\$ 4,100	\$ 3,601	Yes	0 On Time
03-0L260	NEV	080	NEV 80 Rutting 1	\$ 4,930	\$ 5,700	\$ 1,666	\$ 20,839	\$ 20,822	\$ 26,539	\$ 22,488	Yes	0 On Time
04-0J570	MRN	001	Millerton Gulch Br Scour Mitigation	\$ 3,710	\$ 3,710	\$ 2,083	\$ 836	\$ 323	\$ 4,546	\$ 2,405	Yes	(1) Delayed
04-0J642	SON	101	BIG PAVE SEGMENT 2	\$ 18,944	\$ 21,450	\$ 19,013	\$ 68,780	\$ 58,055	\$ 90,230	\$ 77,068	Yes	0 On Time
04-3J140	CC	VAR	APS & Countdown Timers	\$ 3,696	\$ 4,106	\$ 3,951	\$ 7,504	\$ 6,408	\$ 11,610	\$ 10,359	Yes	0 On Time
04-4J390	SF	101	04-4J390_SF 101-MITIGATE WET PAVEMENT	\$ 6,230	\$ 7,030	\$ 5,967	\$ 22,361	\$ 21,299	\$ 29,391	\$ 27,266	Yes	0 On Time
04-4J490	ALA	013	BARRIER SEPARATION	\$ 2,098	\$ 3,224	\$ 2,897	\$ 5,375	\$ 5,149	\$ 8,599	\$ 8,046	Yes	0 On Time
04-4J840	CC	024	Seismic Restoration	\$ 2,650	\$ 4,372	\$ 4,022	\$ 4,310	\$ 3,910	\$ 8,682	\$ 7,932	Yes	0 On Time
04-4J870	SM	001	SM-1 Pescadero Crk. Br. Rails	\$ 3,408	\$ 3,816	\$ 3,728	\$ 3,288	\$ 3,018	\$ 7,104	\$ 6,745	Yes	(1) Delayed
04-0K070	SM	VAR	Install APS(Accessible Ped Signal)	\$ 3,925	\$ 4,672	\$ 4,133	\$ 6,558	\$ 6,114	\$ 11,230	\$ 10,247	Yes	0 On Time
04-0K570	SM	001	Sheet Pile Wall	\$ 1,270	\$ 2,059	\$ 1,822	\$ 1,144	\$ 956	\$ 3,203	\$ 2,778	Yes	0 On Time
04-0K770	ALA	580	ALA CC Curve Warning Signs	\$ 2,110	\$ 2,865	\$ 2,811	\$ 5,547	\$ 4,266	\$ 8,412	\$ 7,077	Yes	0 On Time
04-0Q680	ALA	680	Storm Damage - Permanent Restoration	\$ 2,728	\$ 2,728	\$ 2,273	\$ 1,604	\$ 1,115	\$ 4,332	\$ 3,388	Yes	0 On Time
04-1Q520	SCL	880	SCL-880 Park Ave Bridge Rail Upgrade	\$ 1,920	\$ 1,920	\$ 1,755	\$ 2,017	\$ 1,566	\$ 3,937	\$ 3,321	Yes	0 On Time
04-2Q250	ALA	080	Improve Safety	\$ 2,018	\$ 2,018	\$ 1,970	\$ 2,628	\$ 2,382	\$ 4,646	\$ 4,352	Yes	0 On Time
04-2J801	SF	101	Central Viaduct Structure Painting	\$ 7,500	\$ 7,500	\$ 5,872	\$ 30,993	\$ 26,499	\$ 38,493	\$ 32,371	Yes	0 On Time
04-2Y880	SON	012	DO	\$ 265	\$ 265	\$ 575	\$ 1,235	\$ 1,208	\$ 1,500	\$ 1,783	No	0 On Time
04-3Y020	SON	001	DO	\$ 1,482	\$ 2,302	\$ 2,095	\$ 8,490	\$ 8,088	\$ 10,792	\$ 10,183	Yes	0 On Time
04-3Y320	SON	116	DO	\$ 108	\$ 447	\$ 288	\$ 1,490	\$ 1,265	\$ 1,937	\$ 1,554	Yes	0 On Time
04-3Y490	SOL	037	DO	\$ 2,040	\$ 2,040	\$ 2,044	\$ 6,800	\$ 6,684	\$ 8,840	\$ 8,729	Yes	0 On Time

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				Original Budget ¹	Approved Budget ²	Actual Costs	Approved Budget ²	Actual Costs	Approved Budget ²	Actual Costs		
04-3Y530	SON	001	DO	\$ 400	\$ 1,700	\$ 1,698	\$ 3,885	\$ 3,597	\$ 5,585	\$ 5,295	Yes	0 On Time
04-3Y670	SCL	130	DO	\$ 990	\$ 1,274	\$ 1,002	\$ 3,980	\$ 2,095	\$ 5,254	\$ 3,097	Yes	0 On Time
04-3Y910	SON	101	DO	\$ 650	\$ 650	\$ 809	\$ 2,230	\$ 1,370	\$ 2,880	\$ 2,178	Yes	0 On Time
04-4Y080	SM	001	DO	\$ 234	\$ 234	\$ 287	\$ 780	\$ 470	\$ 1,014	\$ 757	Yes	0 On Time
05-0L722	SLO	001	Old Creek Bridge replacement	\$ 2,594	\$ 7,002	\$ 5,498	\$ 9,781	\$ 8,833	\$ 16,783	\$ 14,331	Yes	(3) Delayed
05-1K070	SCR	017	Jarvis Slide Rock Fence	\$ 3,117	\$ 3,226	\$ 2,053	\$ 2,802	\$ 2,189	\$ 6,028	\$ 4,242	Yes	0 On Time
05-1K720	SLO	041	Morro Rd Soldier Pile Wall	\$ 4,647	\$ 4,647	\$ 3,123	\$ 1,995	\$ 1,633	\$ 6,642	\$ 4,757	Yes	0 On Time
05-1M330	VAR	VAR	ScR Rts 1, 9, 17, 129 SBT 25, 156	\$ 1,470	\$ 1,526	\$ 1,057	\$ 2,311	\$ 1,772	\$ 3,837	\$ 2,829	Yes	0 On Time
05-1P020	SB	VAR	Unsheltered Encampments South	\$ 1,110	\$ 1,510	\$ 1,480	\$ 7,500	\$ 7,271	\$ 9,010	\$ 8,750	Yes	0 On Time
05-1P400	SLO	046	Negranti Pond Embankment Repair	\$ 300	\$ 350	\$ 259	\$ 1,500	\$ 1,334	\$ 1,850	\$ 1,593	Yes	0 On Time
05-1Q820	SLO	VAR	SLO Various Storm Damage 1	\$ 650	\$ 650	\$ 627	\$ 3,300	\$ 2,380	\$ 3,950	\$ 3,007	Yes	0 On Time
05-1Q910	SB	001	Salsipuedes Creek Slip Out	\$ 1,100	\$ 1,100	\$ 443	\$ 5,100	\$ 3,435	\$ 6,200	\$ 3,879	Yes	0 On Time
05-1Q970	SLO	041	SLO Slides and Scour	\$ 750	\$ 750	\$ 505	\$ 3,800	\$ 1,729	\$ 4,550	\$ 2,234	Yes	0 On Time
05-1R120	SB	001	SB Various Storm Damage 2	\$ 550	\$ 550	\$ 274	\$ 3,200	\$ 1,772	\$ 3,750	\$ 2,046	Yes	0 On Time
05-1R160	SB	101	Clark OC Bridge Hit	\$ 500	\$ 550	\$ 515	\$ 3,300	\$ 2,677	\$ 3,850	\$ 3,193	Yes	0 On Time
05-1S150	SLO	101	Extend Drapery System	\$ 160	\$ 160	\$ 155	\$ 900	\$ 781	\$ 1,060	\$ 936	Yes	0 On Time
06-0X270	FRE	005	Panoche CAPM	\$ 8,510	\$ 8,260	\$ 6,052	\$ 33,481	\$ 27,605	\$ 41,741	\$ 33,657	Yes	0 On Time
06-1G040	KER	155	Kern 155 Roadway/Slope/Culvert Repair	\$ 1,500	\$ 1,500	\$ 1,078	\$ 13,595	\$ 7,584	\$ 15,095	\$ 8,662	Yes	0 On Time
07-30240	VEN	101	Ven 101 Padre Juan Rehab	\$ 4,100	\$ 11,070	\$ 10,840	\$ 37,097	\$ 35,391	\$ 48,167	\$ 46,231	Yes	0 On Time
07-31140	LA	101	Rte 101 Stormwater Source Control	\$ 1,349	\$ 3,543	\$ 2,988	\$ 5,410	\$ 4,860	\$ 8,953	\$ 7,848	Yes	0 On Time
07-31790	LA	101	Encino POC Demo	\$ 2,405	\$ 7,769	\$ 7,355	\$ 2,672	\$ 1,920	\$ 10,441	\$ 9,275	Yes	(3) Delayed
07-32440	LA	022	Cold Plane and AC Overlay	\$ 5,172	\$ 5,720	\$ 4,213	\$ 5,854	\$ 5,695	\$ 11,575	\$ 9,908	Yes	0 On Time
07-32850	LA	210	Storm Water Mitigation	\$ 7,461	\$ 6,174	\$ 4,534	\$ 3,404	\$ 2,969	\$ 9,578	\$ 7,502	Yes	0 On Time
07-32880	LA	005	Castaic Weigh Station	\$ 3,770	\$ 3,870	\$ 3,307	\$ 1,547	\$ 1,350	\$ 5,417	\$ 4,657	Yes	0 On Time
07-32970	LA	170	N Hollywood Roadside safety	\$ 2,898	\$ 3,075	\$ 2,411	\$ 2,845	\$ 2,198	\$ 5,920	\$ 4,609	Yes	0 On Time
07-33050	LA	710	Roadside Safety Improvement	\$ 3,456	\$ 3,717	\$ 3,219	\$ 3,015	\$ 2,705	\$ 6,733	\$ 5,925	Yes	0 On Time
07-33250	LA	002	LA 2 MBGR	\$ 6,310	\$ 6,310	\$ 4,445	\$ 17,661	\$ 11,645	\$ 23,971	\$ 16,090	Yes	0 On Time
07-33530	LA	010	Rte 10 Signs, 57 to SBD	\$ 5,248	\$ 3,982	\$ 3,608	\$ 3,149	\$ 2,793	\$ 7,131	\$ 6,402	Yes	0 On Time
07-33650	LA	110	110 Workers Safety	\$ 3,606	\$ 3,946	\$ 2,671	\$ 1,863	\$ 1,712	\$ 5,809	\$ 4,383	Yes	0 On Time
07-33790	LA	134	LA-134 Upgrade TMS	\$ 5,401	\$ 5,422	\$ 3,285	\$ 4,601	\$ 3,559	\$ 10,023	\$ 6,844	Yes	0 On Time
07-34280	LA	014	Storm Damage	\$ 17,182	\$ 9,120	\$ 7,859	\$ 7,882	\$ 7,125	\$ 17,002	\$ 14,984	Yes	(1) Delayed
07-35970	LA	001	LA-1, Signal Improvement	\$ 2,113	\$ 2,139	\$ 1,979	\$ 1,211	\$ 1,057	\$ 3,350	\$ 3,036	Yes	0 On Time
07-0W350	LA	091	91/710 Deck Polyester	\$ 1,256	\$ 1,782	\$ 1,482	\$ 5,022	\$ 4,643	\$ 6,804	\$ 6,125	Yes	0 On Time
07-2XE20	LA	118	Sinkhole	\$ 860	\$ 2,110	\$ 1,944	\$ 14,185	\$ 11,022	\$ 16,295	\$ 12,966	Yes	0 On Time
07-2XF00	LA	405	2XF004-LA 405, PM 16.56/19.74, Abutment	\$ 220	\$ 220	\$ 120	\$ 560	\$ 292	\$ 780	\$ 412	Yes	0 On Time
07-2XG00	LA	002	Washout	\$ 890	\$ 890	\$ 767	\$ 6,760	\$ 6,714	\$ 7,650	\$ 7,482	Yes	0 On Time
08-1G890	RIV	015	RIV 15 IMP SAFETY FOR MTCE	\$ 1,390	\$ 1,744	\$ 1,657	\$ 3,894	\$ 3,140	\$ 5,638	\$ 4,797	Yes	0 On Time
08-1H290	SBD	173	SBD 173 CONSTRUCT SOLDIER PILE WALL	\$ 3,262	\$ 5,973	\$ 5,645	\$ 9,271	\$ 6,991	\$ 15,244	\$ 12,637	Yes	0 On Time
08-1H830	SBD	138	SBD 138 WIDEN MEDIAN AND SHOULDERS	\$ 3,225	\$ 4,039	\$ 3,785	\$ 5,969	\$ 5,490	\$ 10,008	\$ 9,274	Yes	1 Early
08-1J200	SBD	015	SB 15/138 IC SAFETY IMPROVEMENTS	\$ 3,845	\$ 4,578	\$ 4,412	\$ 3,655	\$ 3,340	\$ 8,233	\$ 7,752	Yes	0 On Time
08-1J580	VAR	VAR	RIV/SBD VAR INSTALL ZEV CHARGING STATIONS	\$ 3,013	\$ 3,085	\$ 2,106	\$ 1,757	\$ 1,668	\$ 4,842	\$ 3,773	Yes	0 On Time
08-1J670	RIV	015	RIV 15 UPGRADE CURB RAMPS AND OTHER ADA RELATED ITEMS	\$ 2,806	\$ 3,438	\$ 3,350	\$ 2,486	\$ 2,186	\$ 5,924	\$ 5,535	Yes	0 On Time
08-1K460	RIV	215	RIV 215 S/B & N/B AUX LANE-TIGER	\$ 2,734	\$ 4,186	\$ 4,105	\$ 5,842	\$ 5,580	\$ 10,027	\$ 9,685	Yes	0 On Time
08-1M580	RIV	VAR	RIV 10, 15, 74 PC OVERLAY	\$ 2,088	\$ 2,088	\$ 1,565	\$ 4,362	\$ 3,462	\$ 6,450	\$ 5,027	Yes	0 On Time
08-1P310	RIV	060	Nickname Required	\$ 800	\$ 800	\$ 602	\$ 2,700	\$ 1,286	\$ 3,500	\$ 1,888	Yes	0 On Time
10-1C170	MER	099	SR99 MERCED REHAB	\$ 17,822	\$ 17,972	\$ 16,233	\$ 61,902	\$ 59,080	\$ 79,874	\$ 75,313	Yes	0 On Time
10-1F460	SJ	004	SJ Middle River Bridge Roadway Realignment	\$ 4,814	\$ 5,787	\$ 5,400	\$ 10,441	\$ 9,746	\$ 16,229	\$ 15,146	Yes	(2) Delayed
10-1H600	STA	099	TUOLUMNE RIVER TRASH CONTROL	\$ 2,083	\$ 2,090	\$ 1,700	\$ 608	\$ 529	\$ 2,698	\$ 2,229	Yes	0 On Time
10-1L630	MER	152	SR 152 Median Barrier	\$ 3,348	\$ 3,369	\$ 1,465	\$ 3,133	\$ 2,635	\$ 6,502	\$ 4,100	Yes	0 On Time
10-0P923	MPA	140	Alternative Delivery Method (CMGC) Ferguson Slide Talus Removal	\$ 8,000	\$ 8,000	\$ 6,664	\$ 25,045	\$ 24,211	\$ 33,045	\$ 30,875	Yes	0 On Time
10-1N770	SJ	005	I 580 Separation Bridge MTCE	\$ 590	\$ 645	\$ 520	\$ 1,298	\$ 781	\$ 1,943	\$ 1,302	Yes	0 On Time
11-4Z710	SD	008	I-8 ADA RAMPS	\$ 2,019	\$ 2,938	\$ 2,786	\$ 4,133	\$ 3,730	\$ 7,071	\$ 6,516	Yes	0 On Time
11-43056	SD	067	11-SD-67 RHMA OVERLAY W/ SAMI	\$ 8,692	\$ 9,097	\$ 8,366	\$ 37,450	\$ 36,463	\$ 46,547	\$ 44,829	Yes	0 On Time
11-2N142	SD	125	SR125 AND SR52 BRIDGE PRESERVATION	\$ 1,300	\$ 1,440	\$ 1,213	\$ 2,589	\$ 2,203	\$ 4,029	\$ 3,415	Yes	0 On Time
11-43150	SD	805	DO - REPLACE FAILED CULVERTS	\$ 2,750	\$ 2,750	\$ 1,512	\$ 22,350	\$ 21,784	\$ 25,100	\$ 23,296	Yes	0 On Time
11-43244	SD	015	DO - EMERGENCY - REPAIR STORM DAMAGE	\$ 500	\$ 500	\$ 174	\$ 2,400	\$ 997	\$ 2,900	\$ 1,171	Yes	0 On Time

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Program Project Closeout

Project Description			Support (\$1,000's)			Capital (\$1,000's)		Total (\$1,000's)		Completed Within Approved Budget?	Years Delivered: Early, Delayed, or On-time	
			Original Budget ¹	Approved Budget ²	Actual Costs	Approved Budget ²	Actual Costs	Approved Budget ²	Actual Costs			
11-43249	SD	005	DO - EMERGENCY - REPAIR STORM DAMAGE	\$ 150	\$ 150	\$ 149	\$ 1,000	\$ 987	\$ 1,150	\$ 1,135	Yes	0 On Time
12-0Q270	ORA	057	12-0Q270 Slope Stabilizing at Tonner Cyn	\$ 3,587	\$ 3,587	\$ 3,387	\$ 3,327	\$ 2,912	\$ 6,914	\$ 6,299	Yes	0 On Time
12-0Q390	ORA	055	12-0Q390 Bridge Seismic Restoration	\$ 8,719	\$ 6,352	\$ 6,035	\$ 6,859	\$ 6,373	\$ 13,211	\$ 12,408	Yes	0 On Time
12-0R500	ORA	055	12-0R500 Rt 55 at Hospital Rd. Safety Lighting & Signals	\$ 1,535	\$ 1,909	\$ 1,852	\$ 1,296	\$ 1,205	\$ 3,205	\$ 3,057	Yes	0 On Time
12-0T120	ORA	057	12-0T120 Deck spall repairs and polyester concrete overlay	\$ 1,283	\$ 1,354	\$ 1,199	\$ 2,389	\$ 2,021	\$ 3,743	\$ 3,220	Yes	0 On Time
Totals			\$ 800,628	\$ 904,791	\$ 745,239	\$ 1,922,095	\$ 1,692,751	\$ 2,826,887	\$ 2,437,989			

¹ New project in 2014 Program Document or later. Prior to 2014, the original capital project support budget was calculated on a statewide basis and was not specific to each project component; no data was generated for each project support phase.

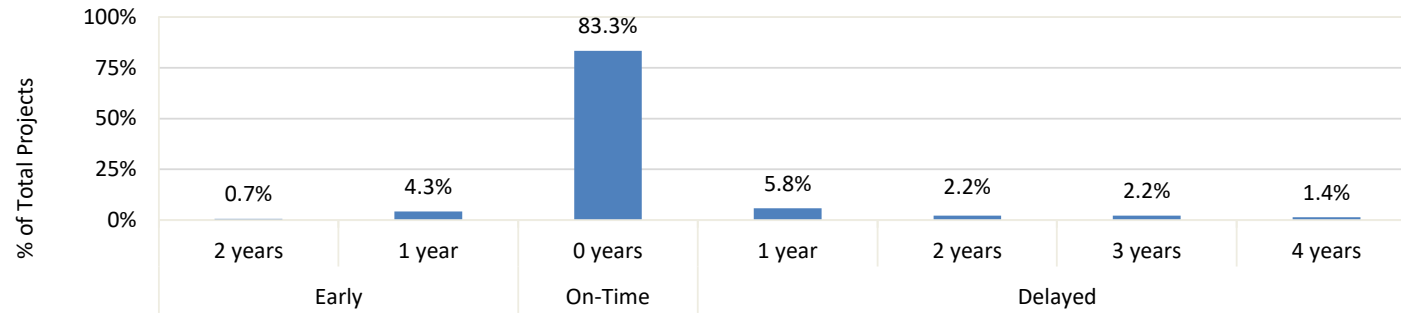
² Approved budget includes fund allocations, any utilized G-12 and supplemental funds, and G-11 amounts for Construction Cap on Emergency Projects. Except for Construction Support, support phase allocations did not begin until June 2017.

**(B) Caltrans Fiscal Year 2024-25
State Highway Operation and Protection
Program Project Closeout**

FY 2024-25 SHOPP Closeout Delivery Year Summary

	Early		On-Time	Delayed			
	2 years	1 year	0 years	1 year	2 years	3 years	4 years
Number of Projects	1	7	195	11	4	6	2
Approved Capital Budget (\$1,000's)	\$ 7,352	\$ 60,542	\$ 1,705,769	\$ 42,328	\$ 35,085	\$ 41,159	\$ 29,860

Distribution of FY 2024-25 SHOPP Closeout Delivery Year



Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
1	01 0A110 HUM 101	South Fork Eel River Bridge #04-123 Near Myers Flat, at South Fork Eel River Bridge No. 04-0123. Seismic retrofit. 20.XX.201.113	PAED	1,198	4,641	3,443	0	0	4,641	3,833	(808)	17/18	20/21	(3)	Delayed	09/30/2024	2	1	1	Bridge(s)
			PSE	1,145	2,072	927	0	0	2,072	2,030	(42)									
			RW Sup	204	218	14	0	0	218	95	(123)									
			Con Sup	1,171	2,812	1,641	0	0	2,812	2,115	(697)									
			RW Cap	44	0	(44)	0	0	290	63	(227)									
			Con Cap	5,565	8,994	3,429	(1,928)	0	7,066	6,026	(1,040)									
			Total	9,327	18,737	9,410	(1,928)	0	17,099	14,161	(2,938)									
2	01 0H191 HUM 096	BLUFF CREEK WALL Near Weitchpec, from 0.6 mile to 0.2 mile west of Bluff Creek Road. Restore roadway and stabilize slope by constructing a retaining wall. 20.XX.201.131	PAED	1,034	1,034	0	0	0	1,034	964	(70)	21/22	21/22	0	On Time	07/22/2024	1	1	1	Location(s)
			PSE	1,159	1,159	0	0	0	1,159	921	(238)									
			RW Sup	63	63	0	0	0	63	30	(33)									
			Con Sup	1,392	1,392	0	0	0	1,392	998	(394)									
			RW Cap	61	0	(61)	0	0	0	0	0									
			Con Cap	6,197	6,464	267	(660)	0	5,804	5,143	(661)									
			Total	9,906	10,112	206	(660)	0	9,452	8,056	(1,396)									
3	01 0H810 MEN 101	Comminsky Permanent Restoration Near Hopland, from 0.8 mile to 1.3 mile north of Geysers Road. Stabilize storm damaged slope by constructing a soldier pile retaining wall and reinforce embankment. 20.XX.201.131	PAED	883	883	0	0	0	883	697	(186)	21/22	21/22	0	On Time	07/25/2024	1	1	1	Location(s)
			PSE	959	959	0	296	250	1,505	1,335	(170)									
			RW Sup	52	52	0	46	0	52	10	(42)									
			Con Sup	1,050	1,680	630	0	0	1,680	1,477	(203)									
			RW Cap	1	0	(1)	0	0	1	0	(1)									
			Con Cap	4,090	4,068	(22)	(685)	0	3,384	3,015	(369)									
			Total	7,035	7,642	607	(343)	250	7,504	6,534	(970)									
4	01 0M840 HUM 101	Hum 101 Slipout Near Cooks Valley, from Humboldt County line to 0.1 mile south of Richardson Grove Undercrossing. Remove debris, stabilize slope, reconstruct roadway, repair drainage, 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	07/10/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	50	0	(50)	0	0	50	17	(33)									
			Con Sup	750	750	0	0	5,000	5,750	2,391	(3,359)									
			RW Cap	50	0	(50)	0	0	0	0	0									
			Con Cap	2,500	2,500	0	0	0	19,900	14,815	(5,085)									
			Total	3,350	3,250	(100)	0	5,000	25,700	17,224	(8,476)									
5	02 4H660 SIS 096	SIS Worker Safety Near Seiad Valley, from east of Ladd Road to 0.7 mile east of Empire Creek Bridge at various locations. Improve highway worker safety by paving Maintenance Vehicle 20.XX.201.235	PAED	1,110	1,110	0	0	0	1,110	1,063	(47)	22/23	22/23	0	On Time	08/26/2024	104	64	0.28	Annual fatal and serious injury collision(s) ⁵
			PSE	910	930	20	0	0	930	674	(256)									
			RW Sup	670	480	(190)	0	0	480	236	(244)									
			Con Sup	1,000	1,000	0	0	0	1,000	282	(718)									
			RW Cap	132	0	(132)	0	0	20	15	(5)									
			Con Cap	4,470	3,900	(570)	381	0	4,281	3,796	(485)									
			Total	8,292	7,420	(872)	381	0	7,821	6,066	(1,755)									

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2. SHOPP support phase allocations did not begin until June 2017. Phases not allocated are shown as zero.
3. Current Approved Budget may not equal Initial Allocated Amount plus G-12 Amount plus Supplemental Allocated Amount because SHOPP support phase allocations did not begin until June 2017. Includes G-11 amounts for Con Cap on Emergency Projects (20.XX.201.130).
4. Applies to new projects in the 2016 SHOPP or later. The Construction Output requirements were first defined and adopted in 2016. Prior to 2016, no Construction Output data was captured.
5. Project involves unit change. See enclosed list of projects with Primary Unit Change.
6. Involves accumulated rounding.
7. See enclosed SHOPP Output Reduction and Reason.

Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
6	02 1K480 VAR 070	Remove Slide and Provide Traffic control Near Pulga, from Butte County line to Route 89; also in Butte County, from 0.2 mile west of Deadwood Road to Plumas County line (PM 35.26/48.076). Remove rocks and 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	23/24	23/24	0	On Time	07/31/2024	2	2	2	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	600	600	0	0	0	600	180	(420)									
			RW Cap	100	0	(100)	0	0	100	9	(91)									
			Con Cap	<u>3,000</u>	<u>3,000</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3,000</u>	<u>1,996</u>	<u>(1,004)</u>									
			Total	3,700	3,600	(100)	0	0	3,700	2,185	(1,515)									
7	03 0A570 YUB 020	Browns Valley Rehab Near Marysville, from Marysville Road to 0.1 mile east of Yuba River Bridge. Rehabilitate roadway. 20.XX.201.120	PAED	3,400	4,300	900	0	0	4,300	3,677	(623)	18/19	18/19	0	On Time	09/03/2024	9.9	9.9	9.8	Lane mile(s) ⁶
			PSE	2,000	2,800	800	0	0	2,800	2,764	(36)									
			RW Sup	1,500	2,500	1,000	450	0	2,950	2,689	(261)									
			Con Sup	6,900	6,900	0	0	0	6,900	6,110	(790)									
			RW Cap	4,400	0	(4,400)	0	0	9,400	4,511	(4,889)									
			Con Cap	<u>38,100</u>	<u>51,400</u>	<u>13,300</u>	<u>(9,000)</u>	<u>0</u>	<u>42,400</u>	<u>39,760</u>	<u>(2,640)</u>									
			Total	56,300	67,900	11,600	(8,550)	0	68,750	59,511	(9,239)									
8	03 0L220 NEV 020	NEV 20 Shoulder Erosion Near Yuba Pass, from 0.5 mile east of Lowell Hill Road to Route 80. Repair eroded shoulder and drainage systems. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	23/24	23/24	0	On Time	08/16/2024	12	12	12	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	900	900	0	0	0	900	467	(433)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>3,000</u>	<u>3,000</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3,200</u>	<u>3,134</u>	<u>(66)</u>									
			Total	3,900	3,900	0	0	0	4,100	3,601	(499)									
9	03 0L260 NEV 080	NEV 80 Rutting 1 In and near Truckee, from the Placer County line to Route 89. Grind pavement, place polyester concrete, replace concrete slabs, replace striping, and rehabilitate drainage 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	23/24	23/24	0	On Time	08/08/2024	35	35	35	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	4,930	4,930	0	0	0	4,930	1,381	(3,549)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>16,790</u>	<u>16,790</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>18,229</u>	<u>18,173</u>	<u>(56)</u>									
			Total	21,720	21,720	0	0	0	23,159	19,554	(3,605)									
10	04 0J570 MRN 001	Millerton Gulch Br Scour Mitigation Near Point Reyes Station, at Millerton Gulch Bridge No. 27-0114. Environmental mitigation for emergency project EA 4K500. 20.XX.201.119	PAED	1,620	1,620	0	0	0	1,620	1,134	(486)	17/18	18/19	(1)	Delayed	07/31/2024	1	0	0	Bridge(s)
			PSE	1,080	1,080	0	0	0	1,080	267	(813)									
			RW Sup	180	180	0	0	0	180	43	(137)									
			Con Sup	830	830	0	0	0	830	639	(191)									
			RW Cap	450	0	(450)	0	0	0	0	0									
			Con Cap	<u>4,020</u>	<u>836</u>	<u>(3,184)</u>	<u>0</u>	<u>0</u>	<u>836</u>	<u>323</u>	<u>(513)</u>									
			Total	8,180	4,546	(3,634)	0	0	4,546	2,405	(2,141)									

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6. Involves accumulated rounding.
7. See enclosed SHOPP Output Reduction and Reason.

Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
11	04 0J642 SON 101	BIG PAVE SEGMENT 2 In and near Windsor, Healdsburg, and Cloverdale, from Old Redwood Highway to 2.0 miles south of Mendocino County line. Roadway rehabilitation. 20.XX.201.122	PAED	5,149	5,149	0	4,849	0	5,149	3,796	(1,353)	18/19	18/19	0	On Time	08/21/2024	43.2	43.2	43.2	Lane mile(s)
			PSE	6,799	6,799	0	0	0	6,799	5,795	(1,004)									
			RW Sup	60	170	110	150	0	320	263	(57)									
			Con Sup	6,936	8,968	2,032	214	0	9,182	9,159	(23)									
			RW Cap	10	0	(10)	0	0	890	126	(764)									
			Con Cap	63,151	78,935	15,784	(11,045)	0	67,890	57,930	(9,960)									
			Total	82,105	100,021	17,916	(5,832)	0	90,230	77,068	(13,162)									
12	04 3J140 CC VAR	APS & Countdown Timers In Contra Costa County, on Routes 4, 24, 80, 123, 242, 580, and 680 at various locations. Install Accessible Pedestrian Signals (APS) systems and pedestrian countdown timers, 20.XX.201.015	PAED	1,108	1,108	0	0	0	1,108	1,093	(15)	21/22	21/22	0	On Time	08/09/2024	333	10	1.11	Annual fatal and serious injury collision(s) ⁵
			PSE	1,108	1,274	166	0	0	1,274	1,274	0									
			RW Sup	80	94	14	0	0	94	90	(4)									
			Con Sup	1,400	1,630	230	0	0	1,630	1,494	(136)									
			RW Cap	20	0	(20)	0	0	150	17	(133)									
			Con Cap	7,487	7,562	75	(208)	0	7,354	6,391	(963)									
			Total	11,203	11,668	465	(208)	0	11,610	10,359	(1,251)									
13	04 4J390 SF 101	04-4J390_SF 101-MITIGATE WET PAVEMENT In the City and County of San Francisco, from San Mateo County line to Route 101/80 Junction. Overlay existing pavement with open graded asphalt, groove concrete 20.XX.201.010	PAED	1,500	2,000	500	0	0	2,000	1,733	(267)	19/20	19/20	0	On Time	07/08/2024	261	261	261	Collision(s) reduced
			PSE	1,700	2,000	300	0	0	2,000	1,846	(154)									
			RW Sup	30	30	0	0	0	30	6	(24)									
			Con Sup	3,000	3,000	0	0	0	3,000	2,382	(618)									
			RW Cap	20	0	(20)	0	0	0	0	0									
			Con Cap	21,970	21,970	0	391	0	22,361	21,299	(1,063)									
			Total	28,220	29,000	780	391	0	29,391	27,266	(2,126)									
14	04 4J490 ALA 013	BARRIER SEPARATION In Oakland, from 0.2 mile north of Lincoln Avenue to 0.2 mile south of Park Boulevard. Install outer separation barrier between Route 13 and frontage roads, widen 20.XX.201.010	PAED	824	824	0	0	0	824	820	(4)	21/22	21/22	0	On Time	07/30/2024	5	5	0.25	Annual fatal and serious injury collision(s) ⁵
			PSE	667	800	133	280	0	1,080	1,050	(30)									
			RW Sup	42	42	0	108	0	150	59	(91)									
			Con Sup	565	1,170	605	0	0	1,170	967	(203)									
			RW Cap	107	0	(107)	0	0	107	3	(104)									
			Con Cap	2,985	5,628	2,643	(360)	0	5,268	5,146	(122)									
			Total	5,190	8,464	3,274	28	0	8,599	8,046	(553)									
15	04 4J840 CC 024	Seismic Restoration In Orinda, at St. Stephens Drive Overcrossing No. 28-0111. Seismic retrofit bridge columns with steel casings, repair bridge deck, apply polyester concrete 20.XX.201.113	PAED	925	925	0	197	0	1,122	1,080	(42)	21/22	21/22	0	On Time	07/25/2024	1	1	1	Bridge(s)
			PSE	1,175	1,375	200	0	0	1,375	1,375	0									
			RW Sup	175	175	0	0	0	175	51	(124)									
			Con Sup	375	1,700	1,325	0	0	1,700	1,516	(184)									
			RW Cap	38	0	(38)	0	0	68	21	(47)									
			Con Cap	2,067	4,542	2,475	(300)	0	4,242	3,890	(352)									
			Total	4,755	8,717	3,962	(103)	0	8,682	7,932	(750)									

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(SHOPP Only Funds Programmed, Allocated, and Expended)

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												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
16	04 4J870 SM 001	SM-1 Pescadero Crk. Br. Rails Near Pescadero, at Pescadero Creek Bridge No. 35-0028. Upgrade bridge rails. 20.XX.201.112	PAED	1,200	1,200	0	0	0	1,200	1,192	(8)	21/22	22/23	(1)	Delayed	09/16/2024	1	840	840	Bridge(s)
			PSE	1,000	1,180	180	0	0	1,180	1,123	(57)									
			RW Sup	8	8	0	0	0	8	3	(5)									
			Con Sup	1,200	1,428	228	0	0	1,428	1,409	(19)									
			RW Cap	260	0	(260)	0	0	260	2	(258)									
			Con Cap	4,434	3,640	(794)	(612)	0	3,028	3,015	(13)									
			Total	8,102	7,456	(646)	(612)	0	7,104	6,745	(359)									
17	04 OK070 SM VAR	Install APS(Accessible Ped Signal) In and near various cities, on Routes 1, 35, 82, 84, 92, 101, 109, 114, 280, and 380 at various locations. Enhance pedestrian safety by upgrading signals with Accessible 20.XX.201.015	PAED	1,250	1,250	0	325	0	1,575	1,482	(93)	21/22	21/22	0	On Time	08/15/2024	63	8	0.91	Annual fatal and serious injury collision(s) ⁵
			PSE	1,250	1,250	0	232	0	1,482	1,475	(7)									
			RW Sup	175	175	0	0	0	175	51	(124)									
			Con Sup	1,250	1,440	190	0	0	1,440	1,125	(315)									
			RW Cap	57	0	(57)	0	0	57	16	(41)									
			Con Cap	5,832	5,732	(100)	769	0	6,501	6,097	(404)									
			Total	9,814	9,847	33	1,326	0	11,230	10,247	(983)									
18	04 OK570 SM 001	Sheet Pile Wall Near Half Moon Bay, at 1.1 miles north of Santa Cruz County line. Restore slope washout by constructing a sheet pile wall and upgrading the drainage system. 20.XX.201.131	PAED	300	350	50	235	0	585	583	(2)	21/22	21/22	0	On Time	08/06/2024	1	1	1	Location(s)
			PSE	400	476	76	248	0	724	720	(4)									
			RW Sup	70	70	0	0	0	70	6	(64)									
			Con Sup	500	500	0	180	0	680	512	(168)									
			RW Cap	2	0	(2)	0	0	4	4	0									
			Con Cap	990	1,183	193	(43)	0	1,140	952	(188)									
			Total	2,262	2,579	317	620	0	3,203	2,778	(425)									
19	04 OK770 ALA 580	ALA CC Curve Warning Signs In Alameda and Contra Costa Counties, on various routes at various locations. Install new and upgrade existing advance warning signs to make standard. This project will 20.XX.201.015	PAED	700	700	0	0	0	700	697	(3)	21/22	21/22	0	On Time	07/16/2024	31	4	0.05	Annual fatal and serious injury collision(s) ⁵
			PSE	700	825	125	283	0	1,108	1,102	(6)									
			RW Sup	10	10	0	0	0	10	10	0									
			Con Sup	700	826	126	221	0	1,047	1,003	(44)									
			RW Cap	108	0	(108)	0	0	0	0	0									
			Con Cap	2,638	3,087	449	(60)	2,520	5,547	4,266	(1,281)									
			Total	4,856	5,448	592	444	2,520	8,412	7,077	(1,334)									
20	04 0Q680 ALA 680	Storm Damage - Permanent Restoration In Fremont, at the northbound offramp to Mission Boulevard. Restore embankment to original condition by stabilizing adjacent slope to prevent future slides. 20.XX.201.131	PAED	750	750	0	0	0	750	450	(300)	21/22	21/22	0	On Time	07/02/2024	1	1	1	Location(s)
			PSE	885	885	0	0	0	885	880	(5)									
			RW Sup	60	60	0	0	0	60	45	(15)									
			Con Sup	1,033	1,033	0	0	0	1,033	897	(136)									
			RW Cap	5	0	(5)	0	0	0	0	0									
			Con Cap	2,908	2,144	(764)	(540)	0	1,604	1,115	(489)									
			Total	5,641	4,872	(769)	(540)	0	4,332	3,388	(944)									

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4. Applies to new projects in the 2016 SHOPP or later. The Construction Output requirements were first defined and adopted in 2016. Prior to 2016, no Construction Output data was captured.
5. Project involves unit change. See enclosed list of projects with Primary Unit Change.
6. Involves accumulated rounding.
7. See enclosed SHOPP Output Reduction and Reason.

Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
21	04 1Q520 SCL 880	SCL-880 Park Ave Bridge Rail Upgrade In San Jose, at Park Avenue Overcrossing No. 37-0127. Upgrade bridge rails and upgrade facilities to Americans with Disabilities Act (ADA) standards. 20.XX.201.112	PAED	418	418	0	0	0	418	403	(15)	22/23	22/23	0	On Time	08/14/2024	398	398	398	Linear feet rail
			PSE	916	916	0	0	0	916	882	(34)									
			RW Sup	57	57	0	0	0	57	51	(6)									
			Con Sup	529	529	0	0	0	529	418	(111)									
			RW Cap	263	0	(263)	0	0	263	9	(254)									
			Con Cap	1,539	1,560	21	194	0	1,754	1,557	(196)									
			Total	3,722	3,480	(242)	194	0	3,937	3,321	(615)									
22	04 2Q250 ALA 080	Improve Safety In Alameda County, on Routes 80, 580, and 680 at various locations. Place High Friction Surface Treatment (HFST) to improve safety in wet conditions. This project will reduce 20.XX.201.010	PAED	640	640	0	0	0	640	607	(33)	21/22	21/22	0	On Time	07/10/2024	213	139	213	Collision(s) reduced
			PSE	674	674	0	0	0	674	663	(11)									
			RW Sup	10	10	0	0	0	10	7	(3)									
			Con Sup	694	694	0	0	0	694	693	(1)									
			RW Cap	12	0	(12)	0	0	0	0	0									
			Con Cap	3,502	4,125	623	(1,497)	0	2,628	2,382	(246)									
			Total	5,532	6,143	611	(1,497)	0	4,646	4,352	(294)									
23	04 2J801 SF 101	Central Viaduct Structure Painting In the City and County of San Francisco, at the Central Viaduct No. 34-0077 from south of 17th Street to S. Van Ness Avenue. Paint superstructure steel members. 20.XX.201.110	PAED	0	0	0	0	0	0	0	0	19/20	19/20	0	On Time	09/10/2024	1	1	1	Bridge(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	7,500	7,500	0	0	0	7,500	5,872	(1,628)									
			RW Cap	153	0	(153)	0	0	0	0	0									
			Con Cap	50,396	50,275	(121)	(19,282)	0	30,993	26,499	(4,494)									
			Total	58,049	57,775	(274)	(19,282)	0	38,493	32,371	(6,122)									
24	04 2Y880 SON 012	DO In Santa Rosa, at Dutton Avenue Undercrossing No. 20-0203L. Replace drainage system and approach slabs. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	08/29/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	265	265	0	0	0	265	575	310									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	1,235	1,235	0	0	0	1,235	1,208	(27)									
			Total	1,500	1,500	0	0	0	1,500	1,783	283									
25	04 3Y020 SON 001	DO Near Jenner, at 0.2 mile north of Russian Gulch Bridge. Repair drainage system and pavement, and stabilize slope by constructing soldier pile retaining wall and 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	07/16/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	1,482	1,482	0	0	820	2,302	2,095	(207)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	4,940	4,940	0	0	0	8,490	8,088	(402)									
			Total	6,422	6,422	0	0	820	10,792	10,183	(609)									

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Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
26	04 3Y320 SON 116	DO Near Sebastopol, on Route 116 at various locations. Replace culvert systems, rebuild the roadway, and fill slopes. Include biological monitoring. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	08/27/2024	5	5	5	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	108	108	0	0	0	108	85	(23)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>360</u>	<u>360</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>360</u>	<u>304</u>	<u>(56)</u>									
			Total	468	468	0	0	0	468	389	(79)									
27	04 3Y490 SOL 037	DO In Vallejo, at Napa River Bridge No. 23-0064. Repair damaged bridge fender system and protect bridge column. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	08/13/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	2,040	2,040	0	0	0	2,040	2,044	4									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>6,800</u>	<u>6,800</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>6,800</u>	<u>6,684</u>	<u>(116)</u>									
			Total	8,840	8,840	0	0	0	8,840	8,729	(111)									
28	04 3Y530 SON 001	DO Near Timber Cove, at 1.5 mile south of Woodside Campground. Repair slipout by installing gabion retaining wall. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	08/27/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	80	0	(80)	0	0	80	98	18									
			Con Sup	320	320	0	0	1,300	1,620	1,599	(21)									
			RW Cap	15	0	(15)	0	0	0	0	0									
			Con Cap	<u>1,305</u>	<u>1,305</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3,885</u>	<u>3,597</u>	<u>(288)</u>									
			Total	1,720	1,625	(95)	0	1,300	5,585	5,295	(290)									
29	04 3Y670 SCL 130	DO Near San Jose, 0.7 mile east of Kincaid Road. Construct retaining wall and repair damaged roadway. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	07/01/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	80	46	(34)									
			Con Sup	990	990	0	0	204	1,194	956	(238)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>3,300</u>	<u>3,300</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3,980</u>	<u>2,095</u>	<u>(1,885)</u>									
			Total	4,290	4,290	0	0	204	5,254	3,097	(2,157)									
30	04 3Y910 SON 101	DO Near Petaluma, at 0.4 mile north of Kastania Road. Stabilize slope and install rock slope protection and drainage system. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	23/24	23/24	0	On Time	09/17/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	650	650	0	0	0	650	809	159									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>2,230</u>	<u>2,230</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>2,230</u>	<u>1,370</u>	<u>(860)</u>									
			Total	2,880	2,880	0	0	0	2,880	2,178	(702)									

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Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
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No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
31	04 4Y080 SM 001	DO Near Pacifica, north of 1st Street. Reconstruct slopes and replace drainage system. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	23/24	23/24	0	On Time	07/16/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	234	234	0	0	0	234	287	53									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	780	780	0	0	0	780	470	(310)									
			Total	1,014	1,014	0	0	0	1,014	757	(257)									
32	05 0L722 SLO 001	Old Creek Bridge replacement Near Morro Bay, at Old Creek Bridge No. 49-0070R. Replace bridge. 20.XX.201.110	PAED	972	1,399	427	340	0	1,739	1,284	(455)	17/18	20/21	(3)	Delayed	08/13/2024	4	1	1	Bridge(s)
			PSE	800	1,900	1,100	0	0	1,900	1,751	(149)									
			RW Sup	22	200	178	0	0	200	107	(93)									
			Con Sup	800	3,163	2,363	0	0	3,163	2,356	(807)									
			RW Cap	43	0	(43)	0	0	450	282	(168)									
			Con Cap	4,200	9,476	5,276	(145)	0	9,331	8,551	(780)									
			Total	6,837	16,138	9,301	195	0	16,783	14,331	(2,452)									
33	05 1K070 SCR 017	Jarvis Slide Rock Fence Near Scotts Valley, at 0.5 mile south of Sugarleaf Road. Stabilize eroded slope by grading, dewatering, and install ingwire mesh drapery system to reduce falling debris. 20.XX.201.131	PAED	721	721	0	0	0	721	438	(283)	21/22	21/22	0	On Time	09/13/2024	1	1	1	Location(s)
			PSE	1,288	1,288	0	0	0	1,288	881	(407)									
			RW Sup	75	75	0	0	0	75	75	0									
			Con Sup	1,033	1,142	109	0	0	1,142	660	(482)									
			RW Cap	50	0	(50)	0	0	50	7	(43)									
			Con Cap	4,271	3,213	(1,058)	(461)	0	2,752	2,182	(570)									
			Total	7,438	6,439	(999)	(461)	0	6,028	4,242	(1,786)									
34	05 1K720 SLO 041	Morro Rd Soldier Pile Wall Near Morro Bay, 3.1 miles north of Route 1. Restore storm damaged slope by constructing a soldier pile wall. 20.XX.201.131	PAED	1,241	1,241	0	0	0	1,241	1,019	(222)	21/22	21/22	0	On Time	08/28/2024	1	1	1	Location(s)
			PSE	1,544	1,544	0	0	0	1,544	1,041	(503)									
			RW Sup	178	178	0	0	0	178	44	(134)									
			Con Sup	1,684	1,684	0	0	0	1,684	1,019	(665)									
			RW Cap	249	0	(249)	0	0	82	20	(62)									
			Con Cap	3,584	2,741	(843)	(828)	0	1,913	1,613	(300)									
			Total	8,480	7,388	(1,092)	(828)	0	6,642	4,757	(1,885)									
35	05 1M330 VAR VAR	SCr Rts 1, 9, 17, 129 SBT 25, 156 In Santa Cruz and San Benito Counties, on Routes 9, 1, 17, 25, 129 and 156 at various locations. Install both centerline and edge line rumble strips and re-stripe with 20.XX.201.010	PAED	0	0	0	0	0	0	0	0	21/22	21/22	0	On Time	09/19/2024	753	753	753	Collision(s) reduced
			PSE	807	807	0	0	0	807	445	(362)									
			RW Sup	35	35	0	0	0	35	9	(26)									
			Con Sup	628	634	6	50	0	684	603	(81)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	3,291	3,084	(207)	(773)	0	2,311	1,772	(539)									
			Total	4,761	4,560	(201)	(723)	0	3,837	2,829	(1,008)									

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4. Applies to new projects in the 2016 SHOPP or later. The Construction Output requirements were first defined and adopted in 2016. Prior to 2016, no Construction Output data was captured.
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												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
36	05 1P020 SB VAR	Unsheltered Encampments South In Santa Barbara County, in and near the cities of Santa Barbara, Lompoc, and Goleta, on Routes 101, 1, 217, and 246 at various locations. Construct hardened fence and 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	21/22	21/22	0	On Time	07/18/2024	20	20	20	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	10	0	(10)	0	0	10	0	(10)									
			Con Sup	1,100	1,100	0	0	400	1,500	1,479	(21)									
			RW Cap	100	0	(100)	0	0	100	3	(97)									
			Con Cap	<u>7,400</u>	<u>7,400</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>7,400</u>	<u>7,267</u>	<u>(133)</u>									
			Total	8,610	8,500	(110)	0	400	9,010	8,750	(260)									
37	05 1P400 SLO 046	Negranti Pond Embankment Repair Near Cambria, from 2.4 miles to 2.8 miles east of North Green Valley Road. Repair roadway, reconstruct slope, construct retaining wall, and place asphalt curb dike. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	07/31/2024	2	2	2	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	300	300	0	0	50	350	259	(91)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>900</u>	<u>900</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>1,500</u>	<u>1,334</u>	<u>(166)</u>									
			Total	1,200	1,200	0	0	50	1,850	1,593	(257)									
38	05 1Q820 SLO VAR	SLO Various Storm Damage 1 In San Luis Obispo County, on routes 1, 41, 58, 101, and 229 at various locations. Repair storm damage, including clearing debris, repairing damaged drainage systems, and 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	09/23/2024	15	15	15	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	650	650	0	0	0	650	627	(23)									
			RW Cap	25	0	(25)	0	0	0	0	0									
			Con Cap	<u>3,300</u>	<u>3,300</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3,300</u>	<u>2,380</u>	<u>(920)</u>									
			Total	3,975	3,950	(25)	0	0	3,950	3,007	(943)									
39	05 1Q910 SB 001	Salsipuedes Creek Slip Out Near Lompoc, at 1.8 miles south of Santa Rosa Road. Construct revetment to restore slope, shoulder, and lane and reconstruct barrier. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	09/11/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	1,100	1,100	0	0	0	1,100	443	(657)									
			RW Cap	20	0	(20)	0	0	0	0	0									
			Con Cap	<u>5,100</u>	<u>5,100</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>5,100</u>	<u>3,435</u>	<u>(1,665)</u>									
			Total	6,220	6,200	(20)	0	0	6,200	3,879	(2,321)									
40	05 1Q970 SLO 041	SLO Slides and Scour In and near Atascadero, from 0.5 mile south of Cerro Alto Road to 0.4 mile north of Old Morro Road; also on Route 101 in the city of San Luis Obispo from 0.3 mile north of Prado 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	08/16/2024	7	7	7	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	750	750	0	0	0	750	505	(245)									
			RW Cap	15	0	(15)	0	0	0	0	0									
			Con Cap	<u>3,800</u>	<u>3,800</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3,800</u>	<u>1,729</u>	<u>(2,071)</u>									
			Total	4,565	4,550	(15)	0	0	4,550	2,234	(2,316)									

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4. Applies to new projects in the 2016 SHOPP or later. The Construction Output requirements were first defined and adopted in 2016. Prior to 2016, no Construction Output data was captured.
5. Project involves unit change. See enclosed list of projects with Primary Unit Change.
6. Involves accumulated rounding.
7. See enclosed SHOPP Output Reduction and Reason.

Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
41	05 1R120 SB 001	SB Various Storm Damage 2 In Santa Barbara County, on Routes 1, 135, and 246 at various locations. Repair storm damage, including drainage systems, slopes, and roadway. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	09/19/2024	16	16	16	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	550	550	0	0	0	550	274	(276)									
			RW Cap	20	0	(20)	0	0	0	0	0									
			Con Cap	<u>2,200</u>	<u>2,200</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3,200</u>	<u>1,772</u>	<u>(1,428)</u>									
			Total	2,770	2,750	(20)	0	0	3,750	2,046	(1,704)									
42	05 1R160 SB 101	Clark OC Bridge Hit Near Santa Maria, at Clark Avenue Overcrossing No. 51-0232. Replace and repair damaged bridge girders and provide traffic control. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	09/04/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	500	500	0	0	50	550	515	(35)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>1,900</u>	<u>1,900</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>3,300</u>	<u>2,677</u>	<u>(623)</u>									
			Total	2,400	2,400	0	0	50	3,850	3,193	(657)									
43	05 1S150 SLO 101	Extend Drapery System Near the city of San Luis Obispo, at 0.2 mile south of Old Stage Coach Road. Extend rockfall drapery system, stabilize slopes, and place erosion control. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	23/24	23/24	0	On Time	09/24/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	160	160	0	0	0	160	155	(5)									
			RW Cap	10	0	(10)	0	0	0	0	0									
			Con Cap	<u>900</u>	<u>900</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>900</u>	<u>781</u>	<u>(119)</u>									
			Total	1,070	1,060	(10)	0	0	1,060	936	(124)									
44	06 0X270 FRE 005	Panoche CAPM Near Mendota, from 1.0 mile south of Kamm Avenue 0.2 mile south of Panoche Road. Pavement rehabilitation, repair culverts and upgrade Transportation Management 20.XX.201.121	PAED	2,000	2,000	0	0	0	2,000	1,815	(185)	21/22	21/22	0	On Time	07/26/2024	46.3	46.3	46.3	Lane mile(s)
			PSE	1,300	850	(450)	0	0	850	758	(92)									
			RW Sup	10	210	200	0	0	210	0	(210)									
			Con Sup	5,200	5,200	0	0	0	5,200	3,479	(1,721)									
			RW Cap	300	0	(300)	0	0	3,100	176	(2,924)									
			Con Cap	<u>35,900</u>	<u>35,951</u>	<u>51</u>	<u>(5,570)</u>	<u>0</u>	<u>30,381</u>	<u>27,429</u>	<u>(2,952)</u>									
			Total	44,710	44,211	(499)	(5,570)	0	41,741	33,657	(8,084)									
45	06 1G040 KER 155	Kern 155 Roadway/Slope/Culvert Repair Near Alta Sierra, from 0.3 mile west of Pascoe Road to 2.8 miles east of Alta Sierra Road. Repair storm damage, including slopes, drainage systems, pavement, and 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	07/23/2024	3	3	8	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	1,500	1,500	0	0	0	1,500	1,078	(422)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>13,595</u>	<u>13,595</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>13,595</u>	<u>7,584</u>	<u>(6,011)</u>									
			Total	15,095	15,095	0	0	0	15,095	8,662	(6,433)									

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5. Project involves unit change. See enclosed list of projects with Primary Unit Change.
6. Involves accumulated rounding.
7. See enclosed SHOPP Output Reduction and Reason.

Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
46	07 30240 VEN 101	Ven 101 Padre Juan Rehab Near Mussel Shoals, from 0.5 mile south of Padre Juan Canyon Road to 0.3 mile north of Punta Gorda Pedestrian Undercrossing. Rehabilitate roadway. 20.XX.201.122	PAED	100	300	200	230	0	530	511	(19)	19/20	19/20	0	On Time	09/11/2024	21.6	21.6	21.6	Lane mile(s)
			PSE	1,700	3,300	1,600	0	0	3,300	3,208	(92)									
			RW Sup	200	200	0	0	0	200	95	(105)									
			Con Sup	2,100	6,340	4,240	700	0	7,040	7,026	(14)									
			RW Cap	70	0	(70)	0	0	150	1	(149)									
			Con Cap	19,270	41,806	22,536	(4,859)	0	36,947	35,390	(1,557)									
			Total	23,440	51,946	28,506	(3,929)	0	48,167	46,231	(1,936)									
47	07 31140 LA 101	Rte 101 Stormwater Source Control In the city of Los Angeles, from 0.1 mile north of Route 110 to 0.1 mile south of North Gower Street. Stabilize soil and erosion control. 20.XX.201.335	PAED	310	510	200	0	0	510	325	(185)	18/19	18/19	0	On Time	08/20/2024	13	53.9	53.9	Acre(s) treated/pollutant
			PSE	440	1,200	760	0	0	1,200	881	(319)									
			RW Sup	19	48	29	0	0	48	37	(11)									
			Con Sup	580	1,043	463	290	452	1,785	1,745	(40)									
			RW Cap	30	0	(30)	0	0	0	0	0									
			Con Cap	3,525	5,132	1,607	278	0	5,410	4,860	(549)									
			Total	4,904	7,933	3,029	568	452	8,953	7,848	(1,105)									
48	07 31790 LA 101	Encino POC Demo In the city of Los Angeles, near the neighborhood of Encino, at the Encino Pedestrian Overcrossing No. 53-1289. Remove pedestrian overcrossing. 20.XX.201.110	PAED	404	1,033	629	303	0	1,336	1,318	(18)	18/19	21/22	(3)	Delayed	08/23/2024	1	0	1	Bridge(s)
			PSE	868	500	(368)	521	0	4,226	4,229	4									
			RW Sup	133	100	(33)	0	0	522	506	(16)									
			Con Sup	1,000	1,685	685	0	0	1,685	1,302	(383)									
			RW Cap	110	0	(110)	0	0	6	6	0									
			Con Cap	6,685	3,548	(3,137)	(9,450)	0	2,666	1,915	(751)									
			Total	9,200	6,866	(2,334)	(8,626)	0	10,441	9,275	(1,165)									
49	07 32440 LA 022	Cold Plane and AC Overlay In Long Beach, from Route 1 to the Orange County line. Cold plane pavement and overlay with Rubberized Hot Mixed Asphalt - Type G (RHMA-G), upgrade curb ramps to 20.XX.201.121	PAED	670	670	0	0	0	670	473	(197)	20/21	20/21	0	On Time	07/05/2024	7.4	7.4	7.4	Lane mile(s)
			PSE	2,523	2,876	353	0	0	2,876	1,604	(1,272)									
			RW Sup	12	14	2	201	0	215	202	(13)									
			Con Sup	1,967	1,859	(108)	100	0	1,959	1,935	(24)									
			RW Cap	126	0	(126)	0	0	55	55	0									
			Con Cap	4,376	5,793	1,417	6	0	5,799	5,639	(160)									
			Total	9,674	11,212	1,538	307	0	11,575	9,908	(1,667)									
50	07 32850 LA 210	Storm Water Mitigation In San Dimas, La Verne, and Claremont, from Amelia Avenue to 0.4 mile east of Monte Vista Avenue. Construct stormwater treatment Best Management Practices 20.XX.201.335	PAED	1,900	1,900	0	0	0	1,900	1,169	(731)	21/22	21/22	0	On Time	08/20/2024	43.5	38.7	40.5	Acre(s) treated/pollutant
			PSE	2,601	2,220	(381)	0	0	2,220	1,323	(897)									
			RW Sup	160	20	(140)	0	0	20	9	(11)									
			Con Sup	2,800	2,034	(766)	0	0	2,034	2,033	(1)									
			RW Cap	69	0	(69)	0	0	213	0	(213)									
			Con Cap	7,490	4,093	(3,397)	(902)	0	3,191	2,969	(222)									
			Total	15,020	10,267	(4,753)	(902)	0	9,578	7,502	(2,076)									

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4. Applies to new projects in the 2016 SHOPP or later. The Construction Output requirements were first defined and adopted in 2016. Prior to 2016, no Construction Output data was captured.
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Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
51	07 32880 LA 005	Castaic Weigh Station In Santa Clarita, from 0.2 mile north of Rye Canyon Road to 0.5 mile south of Route 126 at the northbound Castaic Commercial Vehicle Enforcement Facility (CVEF) (weigh 20.XX.201.321	PAED	550	550	0	0	0	550	491	(59)	20/21	20/21	0	On Time	09/23/2024	1	1	1	Location(s)
			PSE	2,000	2,000	0	0	0	2,000	1,733	(267)									
			RW Sup	20	20	0	0	0	20	19	(1)									
			Con Sup	1,200	1,300	100	0	0	1,300	1,064	(236)									
			RW Cap	10	0	(10)	0	0	36	0	(36)									
			Con Cap	850	1,590	740	(79)	0	1,511	1,350	(161)									
			Total	4,630	5,460	830	(79)	0	5,417	4,657	(760)									
52	07 32970 LA 170	N Hollywood Roadside safety In the city of Los Angeles, near the neighborhood of North Hollywood, from Route 134 to Oxnard Street. Construct Maintenance Vehicle Pullouts (MVPs), place 20.XX.201.235	PAED	526	526	0	0	0	526	363	(163)	21/22	21/22	0	On Time	09/17/2024	84	83	0.32	Annual fatal and serious injury collision(s) ⁵
			PSE	1,313	1,313	0	0	0	1,313	1,193	(120)									
			RW Sup	34	34	0	0	0	34	21	(13)									
			Con Sup	1,025	1,202	177	0	0	1,202	835	(367)									
			RW Cap	41	0	(41)	0	0	57	0	(57)									
			Con Cap	3,262	2,827	(435)	(39)	0	2,788	2,198	(590)									
			Total	6,201	5,902	(299)	(39)	0	5,920	4,609	(1,311)									
53	07 33050 LA 710	Roadside Safety Improvement In Paramount, Lynwood, South Gate, and Bell, from 0.1 mile north of Rosecrans Avenue to Atlantic Boulevard. Construct Maintenance Vehicle Pullouts (MVPs), pave 20.XX.201.235	PAED	613	613	0	261	0	874	681	(193)	21/22	21/22	0	On Time	08/21/2024	36	36	0.57	Annual fatal and serious injury collision(s) ⁵
			PSE	1,578	1,578	0	0	0	1,578	1,438	(140)									
			RW Sup	12	12	0	0	0	12	5	(7)									
			Con Sup	1,253	1,253	0	0	0	1,253	1,096	(157)									
			RW Cap	10	0	(10)	0	0	10	0	(10)									
			Con Cap	2,368	2,552	184	453	0	3,005	2,705	(300)									
			Total	5,834	6,008	174	715	0	6,733	5,925	(808)									
54	07 33250 LA 002	LA 2 MBGR Near Angeles National Forest, from Bay Tree Road to Big Pines Highway at various locations. Upgrade existing guardrail and install new guardrail, and install stormwater 20.XX.201.015	PAED	1,020	1,020	0	0	0	1,020	1,000	(20)	20/21	20/21	0	On Time	09/24/2024	129	24	2.43	Annual fatal and serious injury collision(s) ⁵
			PSE	2,670	2,670	0	0	0	2,670	1,532	(1,138)									
			RW Sup	20	20	0	0	0	20	17	(3)									
			Con Sup	2,600	2,600	0	0	0	2,600	1,896	(704)									
			RW Cap	28	0	(28)	0	0	28	0	(28)									
			Con Cap	16,507	17,809	1,302	(176)	0	17,633	11,645	(5,988)									
			Total	22,845	24,119	1,274	(176)	0	23,971	16,090	(7,881)									
55	07 33530 LA 010	Rte 10 Signs, 57 to SBD In Pomona and Claremont, from Route 57 to the San Bernardino County line. Replace overhead signs. 20.XX.201.170	PAED	588	588	0	0	0	588	380	(208)	20/21	20/21	0	On Time	09/27/2024	599	61	7	Sign(s) ⁷
			PSE	2,700	1,445	(1,255)	75	0	1,520	1,492	(28)									
			RW Sup	60	30	(30)	0	0	30	21	(9)									
			Con Sup	1,900	1,594	(306)	250	0	1,844	1,715	(129)									
			RW Cap	50	0	(50)	0	0	0	0	0									
			Con Cap	7,823	2,930	(4,893)	219	0	3,149	2,793	(356)									
			Total	13,121	6,587	(6,534)	544	0	7,131	6,402	(729)									

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												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
56	07 33650 LA 110	110 Workers Safety In and near the city of Los Angeles, from 0.2 mile south of Route 1 to 0.2 mile south of 228th Street. Construct Maintenance Vehicle Pullouts (MVPs), rock blankets, 20.XX.201.235	PAED	730	730	0	0	0	730	414	(316)	21/22	21/22	0	On Time	09/18/2024	38	29	0.25	Annual fatal and serious injury collision(s) ⁵
			PSE	1,701	1,950	249	0	0	1,950	1,196	(754)									
			RW Sup	46	52	6	0	0	52	10	(42)									
			Con Sup	1,129	1,214	85	0	0	1,214	1,051	(163)									
			RW Cap	42	0	(42)	0	0	20	6	(14)									
			Con Cap	<u>2,128</u>	<u>2,240</u>	<u>112</u>	<u>(397)</u>	<u>0</u>	<u>1,843</u>	<u>1,706</u>	<u>(137)</u>									
			Total	5,776	6,186	410	(397)	0	5,809	4,383	(1,426)									
57	07 33790 LA 134	LA-134 Upgrade TMS In the cities of Los Angeles, Glendale, and Pasadena, from Route 170 to Routes 210/710; also on Route 2 at Route 134 (PM R18.7), on Route 10 at Routes 5/10 20.XX.201.315	PAED	454	454	0	0	0	454	161	(293)	21/22	21/22	0	On Time	09/23/2024	69	48	48	Field element(s)
			PSE	2,840	2,840	0	0	0	2,840	1,928	(912)									
			RW Sup	154	154	0	0	0	154	32	(122)									
			Con Sup	1,953	1,974	21	0	0	1,974	1,164	(810)									
			RW Cap	13	0	(13)	0	0	0	0	0									
			Con Cap	<u>7,559</u>	<u>4,791</u>	<u>(2,768)</u>	<u>(190)</u>	<u>0</u>	<u>4,601</u>	<u>3,559</u>	<u>(1,042)</u>									
			Total	12,973	10,213	(2,760)	(190)	0	10,023	6,844	(3,179)									
58	07 34280 LA 014	Storm Damage In Santa Clarita, from Newhall Avenue to Placerita Canyon Road. Permanent restoration of side slope storm damage by constructing three catchment walls to 20.XX.201.131	PAED	1,870	1,870	0	0	0	1,870	1,394	(476)	20/21	21/22	(1)	Delayed	08/01/2024	1	1	1	Location(s)
			PSE	4,222	3,000	(1,222)	0	0	3,000	2,877	(123)									
			RW Sup	3,543	250	(3,293)	0	0	250	190	(60)									
			Con Sup	7,547	4,000	(3,547)	0	0	4,000	3,397	(603)									
			RW Cap	3,590	0	(3,590)	0	0	5	2	(3)									
			Con Cap	<u>24,410</u>	<u>9,062</u>	<u>(15,348)</u>	<u>(1,185)</u>	<u>0</u>	<u>7,877</u>	<u>7,123</u>	<u>(754)</u>									
			Total	45,182	18,182	(27,000)	(1,185)	0	17,002	14,984	(2,018)									
59	07 35970 LA 001	LA-1, Signal Improvement In the city of Los Angeles, in the neighborhood of Wilmington, at the intersection of Wilmington Boulevard. Upgrade traffic signal with protected left-turn 20.XX.201.010	PAED	444	444	0	0	0	444	376	(68)	21/22	21/22	0	On Time	09/23/2024	41	41	41	Collision(s) reduced
			PSE	912	912	0	0	0	912	868	(44)									
			RW Sup	14	14	0	0	0	14	12	(2)									
			Con Sup	743	769	26	0	0	769	723	(46)									
			RW Cap	48	0	(48)	0	0	48	21	(27)									
			Con Cap	<u>1,135</u>	<u>1,084</u>	<u>(51)</u>	<u>79</u>	<u>0</u>	<u>1,163</u>	<u>1,036</u>	<u>(127)</u>									
			Total	3,296	3,223	(73)	79	0	3,350	3,036	(314)									
60	07 0W350 LA 091	91/710 Deck Polyester In Long Beach, at LA River (W91 - N710 & S710) Bridge No. 53-2143F. Replace portions of the bridge deck and apply polyester concrete overlay to ensure the safety and 20.XX.201.119	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	08/23/2024	1	1	1	Bridge(s)
			PSE	588	588	0	200	0	788	712	(76)									
			RW Sup	10	10	0	0	0	10	1	(9)									
			Con Sup	658	973	315	0	0	973	757	(216)									
			RW Cap	10	0	(10)	0	0	0	0	0									
			Con Cap	<u>4,350</u>	<u>3,975</u>	<u>(375)</u>	<u>0</u>	<u>1,047</u>	<u>5,022</u>	<u>4,643</u>	<u>(379)</u>									
			Total	5,616	5,546	(70)	200	1,047	6,793	6,114	(679)									

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5. Project involves unit change. See enclosed list of projects with Primary Unit Change.
6. Involves accumulated rounding.
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Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
61	07 2XE20 LA 118	Sinkhole Near Chatsworth, at Iverson Road. Repair sinkhole and replace failed culvert. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	08/06/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	50	0	(50)									
			Con Sup	860	860	0	0	1,200	2,060	1,944	(116)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	3,440	3,440	0	0	0	14,185	11,022	(3,163)									
			Total	4,300	4,300	0	0	1,200	16,295	12,966	(3,329)									
62	07 2XF00 LA 405	2XF004-LA 405, PM 16.56/19.74, Abutment In Torrance, Lawndale, Redondo Beach, and Hawthorne, from Route 91 (Artesia Boulevard) to 135th Street. Repair damaged bridge abutment slopes and fill voids caused 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	07/12/2024	7	7	7	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	220	220	0	0	0	220	120	(100)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	560	560	0	0	0	560	292	(268)									
			Total	780	780	0	0	0	780	412	(368)									
63	07 2XG00 LA 002	Washout Near La Canada Flintridge, from 2.4 miles west of Upper Big Tujunga Canyon Road to 1.5 miles west of Mount Mooney Road. Remove slide debris, perform rock scaling, 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	09/26/2024	2	2	2	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	50	0	(50)	0	0	50	0	(50)									
			Con Sup	840	840	0	0	0	840	767	(73)									
			RW Cap	50	0	(50)	0	0	0	0	0									
			Con Cap	2,760	2,760	0	0	0	6,760	6,714	(46)									
			Total	3,700	3,600	(100)	0	0	7,650	7,482	(168)									
64	08 1G890 RIV 015	RIV 15 IMP SAFETY FOR MTCE In Murrieta and Wildomar, from south of Route 215 to 0.3 mile north of Clinton Keith Road. Enhance highway worker safety by installing Maintenance Vehicle Pullouts 20.XX.201.235	PAED	356	356	0	0	0	356	345	(11)	22/23	22/23	0	On Time	09/06/2024	33	32	69.6	Location(s)
			PSE	434	434	0	243	0	677	662	(15)									
			RW Sup	11	11	0	0	0	11	8	(3)									
			Con Sup	589	700	111	0	0	700	641	(59)									
			RW Cap	10	0	(10)	0	0	10	0	(10)									
			Con Cap	3,830	4,259	429	(376)	0	3,884	3,140	(743)									
			Total	5,230	5,760	530	(132)	0	5,638	4,797	(841)									
65	08 1H290 SBD 173	SBD 173 CONSTRUCT SOLDIER PILE WALL Near Cedar Glen, at 0.1 mile south of Hospital Road. Realign roadway, construct soil nail wall, and replace guard rail with concrete barrier mounted on soldier pile wall. 20.XX.201.131	PAED	713	713	0	271	0	984	858	(126)	19/20	19/20	0	On Time	09/13/2024	1	1	1	Location(s)
			PSE	1,080	1,285	205	315	0	1,600	1,525	(75)									
			RW Sup	93	75	(18)	78	0	153	124	(29)									
			Con Sup	1,376	2,760	1,384	476	0	3,236	3,139	(97)									
			RW Cap	10	0	(10)	0	0	768	15	(753)									
			Con Cap	2,345	6,580	4,235	(405)	2,328	8,503	6,976	(1,527)									
			Total	5,617	11,413	5,796	735	2,328	15,244	12,637	(2,608)									

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Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
66	08 1H830 SBD 138	SBD 138 WIDEN MEDIAN AND SHOULDERS Near Pinon Hills, from Los Angeles County line to 0.6 mile west of Phelan Road; also in Los Angeles County, from 0.1 mile west of the San Bernardino County line to the San 20.XX.201.010	PAED	850	850	0	285	0	1,135	1,054	(81)	21/22	20/21	1	Early	08/08/2024	55	55	55	Collision(s) reduced
			PSE	800	1,200	400	0	0	1,200	1,191	(9)									
			RW Sup	75	75	0	0	0	75	11	(64)									
			Con Sup	1,500	1,629	129	0	0	1,629	1,528	(101)									
			RW Cap	10	0	(10)	0	0	218	182	(36)									
			Con Cap	<u>5,890</u>	<u>6,517</u>	<u>627</u>	<u>(769)</u>	<u>0</u>	<u>5,749</u>	<u>5,306</u>	<u>(443)</u>									
			Total	9,125	10,271	1,146	(484)	0	10,005	9,272	(733)									
67	08 1J200 SBD 015	SB 15/138 IC SAFETY IMPROVEMENTS Near Hesperia, from 0.1 mile north of the Route 138 to Gish Overhead. Extend deceleration lane leading to southbound offramp. 20.XX.201.010	PAED	775	775	0	150	0	925	917	(8)	20/21	20/21	0	On Time	09/04/2024	44	44	44	Collision(s) reduced
			PSE	1,190	1,366	176	0	0	1,366	1,346	(20)									
			RW Sup	40	40	0	0	0	40	9	(31)									
			Con Sup	1,840	2,097	257	150	0	2,247	2,139	(108)									
			RW Cap	125	0	(125)	0	0	0	0	0									
			Con Cap	<u>4,400</u>	<u>4,225</u>	<u>(175)</u>	<u>(570)</u>	<u>0</u>	<u>3,655</u>	<u>3,340</u>	<u>(315)</u>									
			Total	8,370	8,503	133	(270)	0	8,233	7,752	(481)									
68	08 1J580 VAR VAR	RIV/SBD VAR INSTALL ZEV CHARGING STATIONS Near Banning and Blythe, at various Safety Roadside Rest Areas (SRRA); also in San Bernardino County near Yucaipa at various locations. Install Zero-Emission Vehicle 20.XX.201.999	PAED	793	793	0	0	0	793	454	(339)	22/23	22/23	0	On Time	09/13/2024	6	4	4	Location(s)
			PSE	1,118	1,280	162	0	0	1,280	1,057	(223)									
			RW Sup	62	62	0	0	0	62	29	(33)									
			Con Sup	1,040	950	(90)	0	0	950	565	(385)									
			RW Cap	20	0	(20)	0	0	20	8	(12)									
			Con Cap	<u>6,381</u>	<u>1,957</u>	<u>(4,424)</u>	<u>(220)</u>	<u>0</u>	<u>1,737</u>	<u>1,659</u>	<u>(77)</u>									
			Total	9,414	5,042	(4,372)	(220)	0	4,842	3,773	(1,068)									
69	08 1J670 RIV 015	RIV 15 UPGRADE CURB RAMPS AND OTHER ADA RELATED ITEMS In and near Murrieta, from Murrieta Hot Springs Road to Bundy Canyon Road. Upgrade facilities to Americans with Disabilities Act (ADA) standards. 20.XX.201.361	PAED	763	763	0	68	0	831	831	0	22/23	22/23	0	On Time	09/17/2024	26	26	28	Curb ramp(s)
			PSE	1,013	1,163	150	0	0	1,163	1,152	(11)									
			RW Sup	34	34	0	0	0	34	19	(15)									
			Con Sup	996	1,100	104	310	0	1,410	1,348	(62)									
			RW Cap	18	0	(18)	0	0	18	13	(5)									
			Con Cap	<u>1,900</u>	<u>2,234</u>	<u>334</u>	<u>234</u>	<u>0</u>	<u>2,468</u>	<u>2,173</u>	<u>(294)</u>									
			Total	4,724	5,294	570	612	0	5,924	5,535	(388)									
70	08 1K460 RIV 215	RIV 215 S/B & N/B AUX LANE-TIGER Near the city of Riverside, from 0.2 mile north of Alessandro Boulevard to 0.2 mile south of Eucalyptus Avenue. Construct auxiliary lanes in the northbound and 20.XX.201.010	PAED	699	699	0	0	0	699	698	(1)	20/21	20/21	0	On Time	09/12/2024	221	221	221	Collision(s) reduced
			PSE	762	876	114	288	0	1,164	1,119	(44)									
			RW Sup	22	22	0	0	0	22	13	(9)									
			Con Sup	1,251	1,179	(72)	318	804	2,301	2,275	(26)									
			RW Cap	24	0	(24)	0	0	24	6	(18)									
			Con Cap	<u>6,015</u>	<u>5,107</u>	<u>(908)</u>	<u>711</u>	<u>0</u>	<u>5,818</u>	<u>5,574</u>	<u>(244)</u>									
			Total	8,773	7,883	(890)	1,316	804	10,027	9,685	(342)									

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4. Applies to new projects in the 2016 SHOPP or later. The Construction Output requirements were first defined and adopted in 2016. Prior to 2016, no Construction Output data was captured.
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Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
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No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial	G-12 Adjust (\$1,000s)	Supplmt	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended	Delivery Year	CCA Date	Construction Output ⁴							
						Initial Pgmd Amount (\$1,000s)		Allocated Amount (\$1,000s)			Current Approved Budget (\$1,000s)			Original	Actual	Difference	Trend	Original	RTL	CCA	Primary Unit
71	08 1M580 RIV VAR	RIV 10, 15, 74 PC OVERLAY In Riverside County, on Routes 10, 15, and 74 at various locations. Apply polyester concrete overlay to bridge decks and replace joint seals. 20.XX.201.119	PAED	528	528	0	0	0	528	504	(24)	22/23	22/23	0	On Time	09/19/2024	5	5	5	Bridge(s)	
			PSE	460	460	0	0	0	460	389	(71)										
			RW Sup	10	10	0	0	0	10	9	(1)										
			Con Sup	1,090	1,090	0	0	0	1,090	659	(431)										
			RW Cap	20	0	(20)	0	0	20	0	(20)										
			Con Cap	4,110	4,444	334	(102)	0	4,342	3,462	(880)										
			Total	6,218	6,532	314	(102)	0	6,450	5,022	(1,428)										
72	08 1P310 RIV 060	Nickname Required Near Moreno Valley, at Theodore Street Overcrossing Bridge No. 56-0488. Replace damaged bridge girders and provide traffic control. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	22/23	22/23	0	On Time	07/31/2024	1	1	1	Location(s)	
			PSE	0	0	0	0	0	0	0	0										0
			RW Sup	10	0	(10)	0	0	10	0	(10)										
			Con Sup	790	790	0	0	0	790	602	(188)										
			RW Cap	10	0	(10)	0	0	10	0	(10)										
			Con Cap	2,690	2,690	0	0	0	2,690	1,286	(1,404)										
			Total	3,500	3,480	(20)	0	0	3,500	1,888	(1,612)										
73	10 1C170 MER 099	SR99 MERCED REHAB In and near the city of Merced, from 0.4 mile south of Childs Avenue to 0.3 mile south of Franklin Road. Roadway rehabilitation. 20.XX.201.122	PAED	1,600	2,100	500	770	0	2,510	2,178	(332)	19/20	19/20	0	On Time	08/14/2024	19.6	19.6	18.8	Lane mile(s) ⁷	
			PSE	3,700	3,700	0	0	0	3,700	2,874	(826)										
			RW Sup	122	122	0	0	0	122	30	(92)										
			Con Sup	12,400	10,400	(2,000)	1,240	0	11,640	11,151	(489)										
			RW Cap	95	0	(95)	0	0	95	17	(78)										
			Con Cap	46,000	50,903	4,903	4,626	6,278	61,807	59,063	(2,744)										
			Total	63,917	67,225	3,308	6,636	6,278	79,874	75,313	(4,561)										
74	10 1F460 SJ 004	SJ Middle River Bridge Roadway Realignment Near Holt, from Middle River Bridge to 0.5 mile east of Middle River Bridge. Realign curve, widen shoulders, upgrade guardrail, and install flashing beacons. 20.XX.201.010	PAED	1,081	1,081	0	0	0	1,081	821	(260)	19/20	21/22	(2)	Delayed	07/30/2024	50	62	50	Collision(s) reduced ⁷	
			PSE	1,453	1,453	0	345	0	1,798	1,753	(45)										
			RW Sup	274	274	0	227	0	501	488	(13)										
			Con Sup	2,006	2,006	0	401	0	2,407	2,339	(68)										
			RW Cap	1,358	0	(1,358)	0	0	3,458	2,988	(471)										
			Con Cap	7,335	8,512	1,177	(1,529)	0	6,983	6,758	(225)										
			Total	13,507	13,326	(181)	(556)	0	16,229	15,146	(1,083)										
75	10 1H600 STA 099	TUOLUMNE RIVER TRASH CONTROL In Modesto, near Zeff Road Undercrossing. Construct stormwater Best Management Practices (BMPs) and a trash full capture device. This project will maximize pollution 20.XX.201.335	PAED	416	416	0	0	0	416	316	(100)	23/24	23/24	0	On Time	07/31/2024	18	19	19	Acre(s) treated/pollutant	
			PSE	938	938	0	40	0	978	976	(2)										
			RW Sup	38	5	(33)	0	0	5	1	(4)										
			Con Sup	691	691	0	0	0	691	407	(284)										
			RW Cap	0	0	0	0	0	0	0	0										
			Con Cap	1,061	695	(366)	(88)	0	608	529	(78)										
			Total	3,144	2,745	(399)	(48)	0	2,698	2,229	(469)										

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												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
76	10 1L630 MER 152	SR 152 Median Barrier Near Los Banos, from 0.8 mile west Basalt Road to Route 5. Construct cable median barrier to improve safety. This project will reduce the number and severity of collisions. 20.XX.201.010	PAED	1,010	1,010	0	0	0	1,010	500	(510)	23/24	23/24	0	On Time	08/30/2024	58	58	58	Collision(s) reduced
			PSE	1,070	1,194	124	0	0	1,194	667	(527)									
			RW Sup	138	25	(113)	0	0	25	22	(3)									
			Con Sup	1,130	1,140	10	0	0	1,140	276	(864)									
			RW Cap	110	0	(110)	0	0	110	27	(83)									
			Con Cap	6,407	5,000	(1,407)	(1,978)	0	3,023	2,608	(415)									
			Total	9,865	8,369	(1,496)	(1,978)	0	6,502	4,100	(2,401)									
77	10 0P923 MPA 140	Alternative Delivery Method (CMGC) Ferguson Slide Talus Removal Near El Portal and Yosemite National Park, 0.5 miles west of South Fork Merced River. Early Work Package No. 1 for Ferguson Slide Construction Manager/General 20.XX.201.131	PAED	0	0	0	0	0	0	0	0	20/21	20/21	0	On Time	09/30/2024	0	0	0	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	8,000	8,000	0	0	0	8,000	6,664	(1,336)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	27,000	22,717	(4,283)	2,327	0	25,045	24,211	(835)									
			Total	35,000	30,717	(4,283)	2,327	0	33,045	30,875	(2,170)									
78	10 1N770 SJ 005	I 580 Separation Bridge MTCE Near Lathrop, at the southbound Route 5 to westbound Route 580 Separation Bridge No. 29-0242L. Apply polyester concrete overlay to bridge deck, replace joint seals, repair 20.XX.201.119	PAED	120	120	0	0	0	120	75	(45)	22/23	22/23	0	On Time	07/02/2024	1	1	1	Bridge(s)
			PSE	180	200	20	35	0	235	234	(1)									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	290	290	0	0	0	290	212	(78)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	1,300	1,369	69	(71)	0	1,298	781	(517)									
			Total	1,890	1,979	89	(36)	0	1,943	1,302	(641)									
79	11 42710 SD 008	I-8 ADA RAMPS In the city of San Diego, from Mission Center Road to College Avenue. Construct and upgrade pedestrian curb ramps to Americans with Disabilities Act (ADA) 20.XX.201.361	PAED	326	326	0	0	0	326	319	(7)	21/22	21/22	0	On Time	07/29/2024	18	17	10	Curb ramp(s) ⁷
			PSE	1,148	1,148	0	314	0	1,462	1,349	(113)									
			RW Sup	42	0	(42)	0	0	0	0	0									
			Con Sup	545	865	320	285	0	1,150	1,117	(33)									
			RW Cap	25	0	(25)	0	0	0	0	0									
			Con Cap	1,107	2,573	1,466	0	1,560	4,133	3,730	(402)									
			Total	3,193	4,912	1,719	599	1,560	7,071	6,516	(555)									
80	11 43056 SD 067	11-SD-67 RHMA OVERLAY W/ SAMI In and near Poway, from 0.1 mile south of Lakeside Avenue to Route 78. Rehabilitate asphalt pavement, drainage system restoration, curb ramp upgrades, guardrail 20.XX.201.120	PAED	807	807	0	0	0	807	243	(564)	20/21	20/21	0	On Time	09/06/2024	49.8	49.8	49.8	Lane mile(s)
			PSE	2,690	2,690	0	220	0	2,910	2,771	(139)									
			RW Sup	85	85	0	0	0	85	74	(11)									
			Con Sup	5,110	5,250	140	0	0	5,250	5,234	(16)									
			RW Cap	326	0	(326)	0	0	326	70	(256)									
			Con Cap	26,891	32,000	5,109	3,316	1,808	37,124	36,393	(731)									
			Total	35,909	40,832	4,923	3,536	1,808	46,502	44,785	(1,717)									

1. Phases with no programmed amount are shown as zero.
2. SHOPP support phase allocations did not begin until June 2017. Phases not allocated are shown as zero.
3. Current Approved Budget may not equal Initial Allocated Amount plus G-12 Amount plus Supplemental Allocated Amount because SHOPP support phase allocations did not begin until June 2017. Includes G-11 amounts for Con Cap on Emergency Projects (20.XX.201.130).
4. Applies to new projects in the 2016 SHOPP or later. The Construction Output requirements were first defined and adopted in 2016. Prior to 2016, no Construction Output data was captured.
5. Project involves unit change. See enclosed list of projects with Primary Unit Change.
6. Involves accumulated rounding.
7. See enclosed SHOPP Output Reduction and Reason.

Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
81	11 2N142 SD 125	SR125 AND SR52 BRIDGE PRESERVATION In La Mesa, at Panorama Drive Undercrossing No. 57-0309; also in the city of San Diego at San Diego River - Hollins Lake Bridge No. 57-0983R (PM 13.6). Apply 20.XX.201.119	PAED	250	250	0	0	0	250	192	(58)	22/23	22/23	0	On Time	07/19/2024	2	2	2	Bridge(s)
			PSE	400	440	40	0	0	440	413	(27)									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	650	750	100	0	0	750	608	(142)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>2,100</u>	<u>2,627</u>	<u>527</u>	<u>(39)</u>	<u>0</u>	<u>2,589</u>	<u>2,203</u>	<u>(386)</u>									
			Total	3,400	4,067	667	(39)	0	4,029	3,415	(613)									
82	11 43150 SD 805	DO - REPLACE FAILED CULVERTS In and near Chula Vista, from 0.2 mile south to 0.1 mile north of H Street. Replace culverts. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	21/22	21/22	0	On Time	08/30/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	2,750	2,750	0	0	0	2,750	1,512	(1,238)									
			RW Cap	100	0	(100)	0	0	100	15	(85)									
			Con Cap	<u>13,750</u>	<u>13,750</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>22,250</u>	<u>21,769</u>	<u>(481)</u>									
			Total	16,600	16,500	(100)	0	0	25,100	23,296	(1,804)									
83	11 43244 SD 015	DO - EMERGENCY - REPAIR STORM DAMAGE In the city of San Diego, at the interchange with Route 94. Backfill eroded slopes, remove debris, and replace failed concrete panels in drainage channel. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	23/24	23/24	0	On Time	07/25/2024	1	1	1	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	500	500	0	0	0	500	174	(326)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>2,400</u>	<u>2,400</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>2,400</u>	<u>997</u>	<u>(1,403)</u>									
			Total	2,900	2,900	0	0	0	2,900	1,171	(1,729)									
84	11 43249 SD 005	DO - EMERGENCY - REPAIR STORM DAMAGE In San Diego County on various routes. Repair damaged guardrail and end treatments. 20.XX.201.130	PAED	0	0	0	0	0	0	0	0	23/24	23/24	0	On Time	08/23/2024	26	26	26	Location(s)
			PSE	0	0	0	0	0	0	0	0									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	150	150	0	0	0	150	149	(1)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>1,000</u>	<u>1,000</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>1,000</u>	<u>987</u>	<u>(13)</u>									
			Total	1,150	1,150	0	0	0	1,150	1,135	(15)									
85	12 0Q270 ORA 057	12-0Q270 Slope Stabilizing at Tonner Cyn Near Brea, at Toner Canyon offramp. Construct a new detention basin as a Best Management Practice (BMP) to reduce Trash Total Maximum Daily Load (TMDL) to 20.XX.201.335	PAED	887	887	0	0	0	887	838	(49)	21/22	21/22	0	On Time	07/29/2024	14.9	29.8	29.8	Acre(s) treated/pollutant
			PSE	1,200	1,200	0	0	0	1,200	1,166	(34)									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	1,500	1,500	0	0	0	1,500	1,383	(117)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	<u>6,797</u>	<u>4,618</u>	<u>(2,179)</u>	<u>(1,291)</u>	<u>0</u>	<u>3,327</u>	<u>2,912</u>	<u>(415)</u>									
			Total	10,384	8,205	(2,179)	(1,291)	0	6,914	6,299	(615)									

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3. Current Approved Budget may not equal Initial Allocated Amount plus G-12 Amount plus Supplemental Allocated Amount because SHOPP support phase allocations did not begin until June 2017. Includes G-11 amounts for Con Cap on Emergency Projects (20.XX.201.130).
4. Applies to new projects in the 2016 SHOPP or later. The Construction Output requirements were first defined and adopted in 2016. Prior to 2016, no Construction Output data was captured.
5. Project involves unit change. See enclosed list of projects with Primary Unit Change.
6. Involves accumulated rounding.
7. See enclosed SHOPP Output Reduction and Reason.

Appendix B1 - Caltrans Fiscal Year 2024-25, Quarter 3, Project Closeout
State Highway Operation and Protection Program
(SHOPP Only Funds Programmed, Allocated, and Expended)

No.	District EA County Route	Title Location and Type of Work Program Code	Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)	Delivery Year				CCA Date	Construction Output ⁴			
												Original	Actual	Difference	Trend		Original	RTL	CCA	Primary Unit
86	12 0Q390 ORA 055	12-0Q390 Bridge Seismic Restoration In Costa Mesa, at the Route 55/73 Connector Overcrossing Bridge No. 55-538F (PM 4.74). Seismic restoration. 20.XX.201.113	PAED	1,048	1,048	0	0	0	1,048	884	(164)	20/21	20/21	0	On Time	09/25/2024	3	1	1	Bridge(s)
			PSE	2,279	2,279	0	0	0	2,279	2,253	(26)									
			RW Sup	11	0	(11)	0	0	0	0	0									
			Con Sup	5,381	2,750	(2,631)	275	0	3,025	2,898	(127)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	15,456	8,933	(6,523)	805	0	6,859	6,373	(487)									
			Total	24,175	15,010	(9,165)	1,080	0	13,211	12,408	(804)									
87	12 0R500 ORA 055	12-0R500 Rt 55 at Hospital Rd. Safety Lighting & Signals In Newport Beach, at Hospital Road. Modify signals, add safety lighting and guardrail, and upgrade facilities to Americans with Disabilities Act (ADA) standards. 20.XX.201.010	PAED	300	300	0	110	0	410	389	(21)	21/22	21/22	0	On Time	08/05/2024	14	14	0.02	Annual fatal and serious injury collision(s) ⁵
			PSE	635	635	0	264	0	899	893	(5)									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	600	600	0	0	0	600	570	(30)									
			RW Cap	4	0	(4)	0	0	0	0	0									
			Con Cap	916	1,053	137	243	0	1,296	1,205	(91)									
			Total	2,455	2,588	133	617	0	3,205	3,057	(147)									
88	12 0T120 ORA 057	12-0T120 Deck spall repairs and polyester concrete overlay In Anaheim, at Stadium Overhead No. 55-0399. Apply polyester concrete overlay to bridge deck and repair spalls. (Bridge Deck Preservation) 20.XX.201.119	PAED	171	171	0	0	0	171	150	(21)	22/23	22/23	0	On Time	08/02/2024	1	1	1	Bridge(s)
			PSE	392	443	51	0	0	443	408	(35)									
			RW Sup	0	0	0	0	0	0	0	0									
			Con Sup	720	720	0	0	0	720	640	(80)									
			RW Cap	0	0	0	0	0	0	0	0									
			Con Cap	2,294	2,294	0	95	0	2,389	2,021	(368)									
			Total	3,577	3,628	51	95	0	3,723	3,218	(504)									
Totals			PAED	52,615	59,464	6,849	8,395	0	62,650	51,481	(11,168)									
			PSE	81,864	86,619	4,755	4,197	250	94,271	81,463	(12,808)									
			RW Sup	9,460	6,910	(2,550)	1,261	0	8,877	6,158	(2,719)									
			Con Sup	150,519	159,151	8,632	5,460	10,280	174,891	144,187	(30,704)									
			RW Cap	13,702	0	(13,702)	0	0	21,197	8,716	(12,481)									
			Con Cap	686,795	720,294	33,499	(63,582)	15,541	730,037	644,059	(85,979)									
			Total	994,955	1,032,438	37,483	(44,269)	26,071	1,091,923	936,065	(155,858)									

1. Phases with no programmed amount are shown as zero.
2. SHOPP support phase allocations did not begin until June 2017. Phases not allocated are shown as zero.
3. Current Approved Budget may not equal Initial Allocated Amount plus G-12 Amount plus Supplemental Allocated Amount because SHOPP support phase allocations did not begin until June 2017. Includes G-11 amounts for Con Cap on Emergency Projects (20.XX.201.130).
4. Applies to new projects in the 2016 SHOPP or later. The Construction Output requirements were first defined and adopted in 2016. Prior to 2016, no Construction Output data was captured.
5. Project involves unit change. See enclosed list of projects with Primary Unit Change.
6. Involves accumulated rounding.
7. See enclosed SHOPP Output Reduction and Reason.

Appendix B1-Continued

SHOPP Output Primary Unit Change

From Appendix B1, this table shows each SHOPP project that involves Primary Unit change.

Project ID	DISTRICT	EA5	PROGCODE	Co	Rte	Nickname	Output at RTL	Output at CCA	Primary Unit	Primary Unit Change
0219000001	02	4H660	20.XX.201.235	SIS	096	SIS Worker Safety	64	0.28	Annual fatal and serious injury collision(s)	Unit Changed From Collision(s) reduced to Annual fatal and serious injury collision(s).
0415000091	04	3J140	20.XX.201.015	CC	VAR	APS & Countdown Timers	10	1.11	Annual fatal and serious injury collision(s)	Unit Changed From Collision(s) reduced to Annual fatal and serious injury collision(s).
0415000356	04	4J490	20.XX.201.010	ALA	013	BARRIER SEPARATION	5	0.25	Annual fatal and serious injury collision(s)	Unit Changed From Collision(s) reduced to Annual fatal and serious injury collision(s).
0416000033	04	0K070	20.XX.201.015	SM	VAR	Install APS(Accessible Ped Signal)	8	0.91	Annual fatal and serious injury collision(s)	Unit Changed From Collision(s) reduced to Annual fatal and serious injury collision(s).
0416000137	04	0K770	20.XX.201.015	ALA	580	ALA CC Curve Warning Signs	4	0.05	Annual fatal and serious injury collision(s)	Unit Changed From Collision(s) reduced to Annual fatal and serious injury collision(s).
0716000204	07	32970	20.XX.201.235	LA	170	N Hollywood Roadside safety	83	0.32	Annual fatal and serious injury collision(s)	Unit Changed From Collision(s) reduced to Annual fatal and serious injury collision(s).
0716000214	07	33050	20.XX.201.235	LA	710	Roadside Safety Improvement	36	0.57	Annual fatal and serious injury collision(s)	Unit Changed From Collision(s) reduced to Annual fatal and serious injury collision(s).
0716000270	07	33250	20.XX.201.015	LA	002	LA 2 MBGR	24	2.43	Annual fatal and serious injury collision(s)	Unit Changed From Collision(s) reduced to Annual fatal and serious injury collision(s).
0716000401	07	33650	20.XX.201.235	LA	110	110 Workers Safety	29	0.25	Annual fatal and serious injury collision(s)	Unit Changed From Collision(s) reduced to Annual fatal and serious injury collision(s).
1218000101	12	0R500	20.XX.201.010	ORA	055	12-0R500 Rt 55 at Hospital Rd. Safety Lighting & Signals	14	0.02	Annual fatal and serious injury collision(s)	Unit Changed From Collision(s) reduced to Annual fatal and serious injury collision(s).

Appendix B1-Continued

SHOPP Output Reduction and Reason

From Appendix B1, this table shows each SHOPP project that has output reduction at project completion (CCA) from what was planned at the time of vote (RTL). The table provides the reasons for the reduction and describes applicable corrective actions as needed.

Project ID	DISTRICT	EA5	PROGCODE	Co	Rte	Nickname	Output at RTL	Output at CCA	Primary Unit	Reason for Output Reduction from RTL to CCA
0716000363	07	33530	20.XX.201.170	LA	010	Rte 10 Signs, 57 to SBD	61	7	Sign(s)	Asset Management Tool database reports Overhead Signs for projects with Signs Panel and Lighting. Currently, there is no way to change the Asset Management Tool as programming summary uses a logic based on the program code.
1014000144	10	1C170	20.XX.201.122	MER	099	SR99 MERCED REHAB	19.6	18.8	Lane mile(s)	The programmed performance output was entered prior to the implementation of the new pavement worksheet. District has generated new Construction Completion Acceptance tracking forms for internal approvals to correct the data in the Asset Management Tool to show 19.2 Lane Miles. There is a 0.4-mile reduction due to differences in calculating pavement lane miles.
1016000139	10	1F460	20.XX.201.010	SJ	004	SJ Middle River Bridge Roadway Realignment	62	50	Collision(s) reduced	The Post-Programming Changes in the Asset Management Tool database were not reviewed and approved by Asset Management, which in turn did not update the final output to 62. District has generated new Construction Completion Acceptance tracking forms for internal approvals to correct the data in the Asset Management Tool database to show 62 Collision(s) reduced.
1116000154	11	42710	20.XX.201.361	SD	008	I-8 ADA RAMPS	17	10	Curb ramp(s)	Due to conflict with a minor project already in place, decision was made to remove one location from the project plans. Thus, ADA Curb Ramp(s) Output had to be reduced during construction to resolve the project development conflict with the minor project. An approved Construction Contract Change order has documented this change.

Appendix B1- Summary Totals, Fiscal Year 2024-25

Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)
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Q1

PAED	42,668	44,197	1,529	1,991	600	53,437	39,318	(14,119)
PSE	58,324	62,836	4,512	3,854	1,920	75,096	67,429	(7,666)
RW Sup	10,075	8,908	(1,167)	1,092	150	12,651	9,131	(3,520)
Con Sup	86,538	90,004	3,466	5,762	5,898	109,962	95,148	(14,814)
RW Cap	19,503	0	(19,503)	0	0	11,026	7,446	(3,580)
Con Cap	<u>440,343</u>	<u>407,701</u>	<u>(32,642)</u>	<u>(32,220)</u>	<u>29,338</u>	<u>450,367</u>	<u>398,335</u>	<u>(52,033)</u>
Total	657,451	613,646	(43,805)	(19,521)	37,906	712,539	616,808	(95,731)

Q2

PAED	56,123	56,811	688	3,383	0	62,117	40,066	(22,051)
PSE	111,935	81,555	(30,380)	7,433	2,287	101,864	85,569	(16,295)
RW Sup	10,182	5,751	(4,431)	1,457	50	8,091	4,591	(3,500)
Con Sup	149,199	108,044	(41,155)	7,003	3,091	137,778	118,549	(19,229)
RW Cap	13,554	0	(13,554)	0	63	12,854	1,484	(11,370)
Con Cap	<u>693,302</u>	<u>490,088</u>	<u>(203,214)</u>	<u>(19,542)</u>	<u>22,660</u>	<u>680,584</u>	<u>619,958</u>	<u>(60,626)</u>
Total	1,034,295	742,249	(292,046)	(267)	28,151	1,003,287	870,217	(133,070)

Phase	Initial Pgmd Amount (\$1,000s) ¹	Initial Allocated Amount (\$1,000s) ²	Initial Allocated minus Initial Pgmd Amount (\$1,000s)	G-12 Adjust Amount (\$1,000s)	Supplmt Allocated Amount (\$1,000s)	Current Approved Budget (\$1,000s) ³	Expended Amount (\$1,000s)	Expended minus Current Approved Budget (\$1,000s)
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Q3

PAED	52,615	59,464	6,849	8,395	0	62,650	51,481	(11,168)
PSE	81,864	86,619	4,755	4,197	250	94,271	81,463	(12,808)
RW Sup	9,460	6,910	(2,550)	1,261	0	8,877	6,158	(2,719)
Con Sup	150,519	159,151	8,632	5,460	10,280	174,891	144,187	(30,704)
RW Cap	13,702	0	(13,702)	0	0	21,197	8,716	(12,481)
Con Cap	<u>686,795</u>	<u>720,294</u>	<u>33,499</u>	<u>(63,582)</u>	<u>15,541</u>	<u>730,037</u>	<u>644,059</u>	<u>(85,979)</u>
Total	994,955	1,032,438	37,483	(44,269)	26,071	1,091,923	936,065	(155,858)

Year-To-Date Totals

PAED	151,406	160,472	9,066	13,769	600	178,204	130,866	(47,338)
PSE	252,123	231,010	(21,113)	15,484	4,457	271,231	234,462	(36,769)
RW Sup	29,717	21,569	(8,148)	3,810	200	29,619	19,881	(9,738)
Con Sup	386,256	357,199	(29,057)	18,224	19,269	422,630	357,884	(64,746)
RW Cap	46,759	0	(46,759)	0	63	45,077	17,646	(27,431)
Con Cap	<u>1,820,440</u>	<u>1,618,083</u>	<u>(202,357)</u>	<u>(115,344)</u>	<u>67,539</u>	<u>1,860,989</u>	<u>1,662,351</u>	<u>(198,638)</u>
Total	2,686,701	2,388,333	(298,368)	(64,057)	92,128	2,807,749	2,423,090	(384,659)

¹ Phases with no programmed amount are shown as zero.² SHOPP support phase allocations did not begin until June 2017. Phases not allocated are shown as zero.³ Current Approved budget may not equal Initial Allocated Amount plus G-12 Amount plus Supplemental Allocated Amount because SHOPP support phase allocations did not begin until June 2017. Includes G-11 amounts for Con Cap on Emergency Projects (20.XX.201.130).

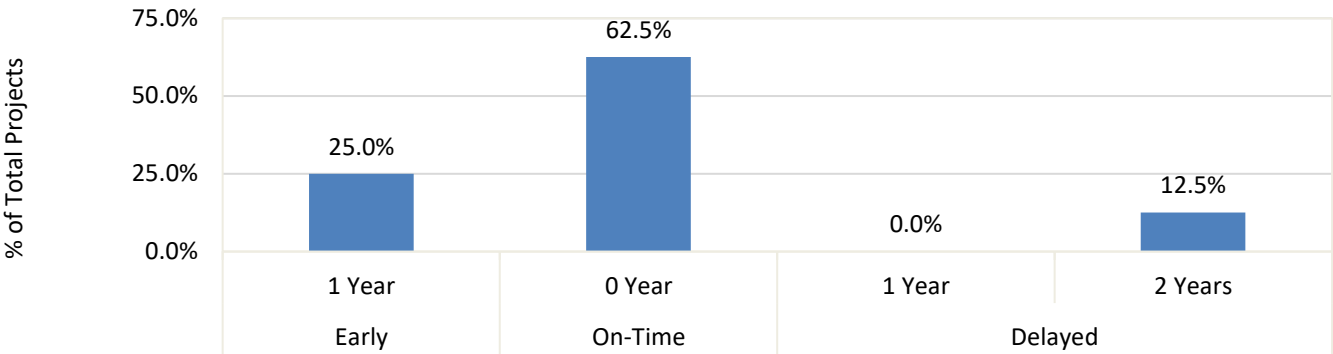
(C) Caltrans Fiscal Year 2024-25 State Transportation Improvement Program ¹ Project Closeout														
Project Description				Support (\$1,000's)			Capital (\$1,000's)		Total (\$1,000's)		Delivery Year			
				Original Budget ²	Approved Budget ³	Actual Costs	Approved Budget ³	Actual Costs	Approved Budget ³	Actual Costs	Original	Actual	Years Early, Delayed, or On-time	
1st Quarter														
03-4F38U	YUB	070	Yuba 70 Laurellen to Butte County Line SHOPP & ST	\$ 42,222	\$ 32,740	\$ 28,741	\$ 79,141	\$ 73,089	\$ 111,881	\$ 101,829	20/21	20/21	0	On Time
11-28883	SD	125	SR-125 SB TO WB SR-905 CONNECTOR	\$ 9,857	\$ 9,857	\$ 9,851	\$ 28,046	\$ 18,688	\$ 37,903	\$ 28,539	19/20	19/20	0	On Time
12-0C110	ORA	057	12-0C110 SR 57 Reconfigure Ramp	\$ 3,900	\$ 18,427	\$ 18,935	\$ 61,456	\$ 57,075	\$ 79,883	\$ 76,011	18/19	18/19	0	On Time
2nd Quarter														
03-3H93U	VAR	070	BUT/YUB 70 Seg 3 Combined	\$ 14,300	\$ 18,740	\$ 15,890	\$ 47,636	\$ 43,575	\$ 66,376	\$ 59,465	20/21	20/21	0	On Time
04-3G474	CC	580	Remove Travelers, Paint Structural Steel & Repair	\$ 15,000	\$ 16,550	\$ 15,141	\$ 68,359	\$ 67,701	\$ 84,909	\$ 82,842	19/20	18/19	1	Early
04-3G488	SM	092	SM-Hayward Bridge	\$ 3,320	\$ 4,920	\$ 4,822	\$ 13,618	\$ 11,454	\$ 18,538	\$ 16,276	22/23	21/22	1	Early
05-0N73U	SB	101	Carpinteria to Santa Barbara Widening Project - Segm	\$ 18,430	\$ 18,430	\$ 14,417	\$ 107,672	\$ 104,148	\$ 126,102	\$ 118,565	19/20	19/20	0	On Time
11-05639	SD	011	OTAY MESA EAST PORT OF ENTRY SITE PREPARATION	\$ 10,600	\$ 12,460	\$ 12,459	\$ 64,916	\$ 35,717	\$ 77,376	\$ 48,176	19/20	21/22	-2	Delayed
3rd Quarter														
05-1G410	SB	101	Santa Claus Lane Bike Path	\$ 1,750	\$ 1,283	\$ 1,005	\$ 12,331	\$ 11,993	\$ 13,614	\$ 12,998	22/23	22/23	0	On Time
10-0Q122	MER	099	10-0Q122: SB LIVINGSTON MEDIAN WIDENING	\$ 9,300	\$ 8,250	\$ 9,558	\$ 29,802	\$ 26,426	\$ 38,052	\$ 35,984	21/22	21/22	0	On Time
11-08028	IMP	098	SR-98 WIDENING LANES	\$ 3,650	\$ 3,098	\$ 3,092	\$ 9,185	\$ 8,211	\$ 12,282	\$ 11,303	19/20	19/20	0	On Time
Totals				\$ 132,329	\$ 144,755	\$ 133,912	\$ 522,162	\$ 458,077	\$ 666,917	\$ 591,988				
¹ Includes projects with one or more components funded from the State Transportation Improvement Program funds. Includes all contributor funds on all project components.														
² New projects in 2014 Program Document or later. Prior to 2014, the original capital project support budget was calculated on a statewide basis and was not specific to each project component; no data was generated for each project support phase.														
³ Budget information is included, if expenditures are reflected in State data systems. Excludes local budgets for work implemented by local agencies. Capital Budget consists of Construction and Right of Way Capital.														

(C) Caltrans Fiscal Year 2024-25
State Transportation Improvement Program
Project Closeout

FY 2024-25 STIP Closeout Delivery Year Summary

	Early	On-Time	Delayed	Delayed
	1 Year	0 Year	1 Year	2 Year
Number of Projects	2	8	0	1
Approved Capital Budget (\$1,000's)	\$ 81,977	\$ 375,269	\$ -	\$ 64,916

Distribution of FY 2024-25 STIP Closeout Delivery Year Summary



**(D) Caltrans Fiscal Year 2024-25, Third Quarter
Capital Outlay Support G-12 Request Summary**

The table below summarizes G-12 funding requests for the Capital Outlay Support program through the end of Q3, FY 2024-25.

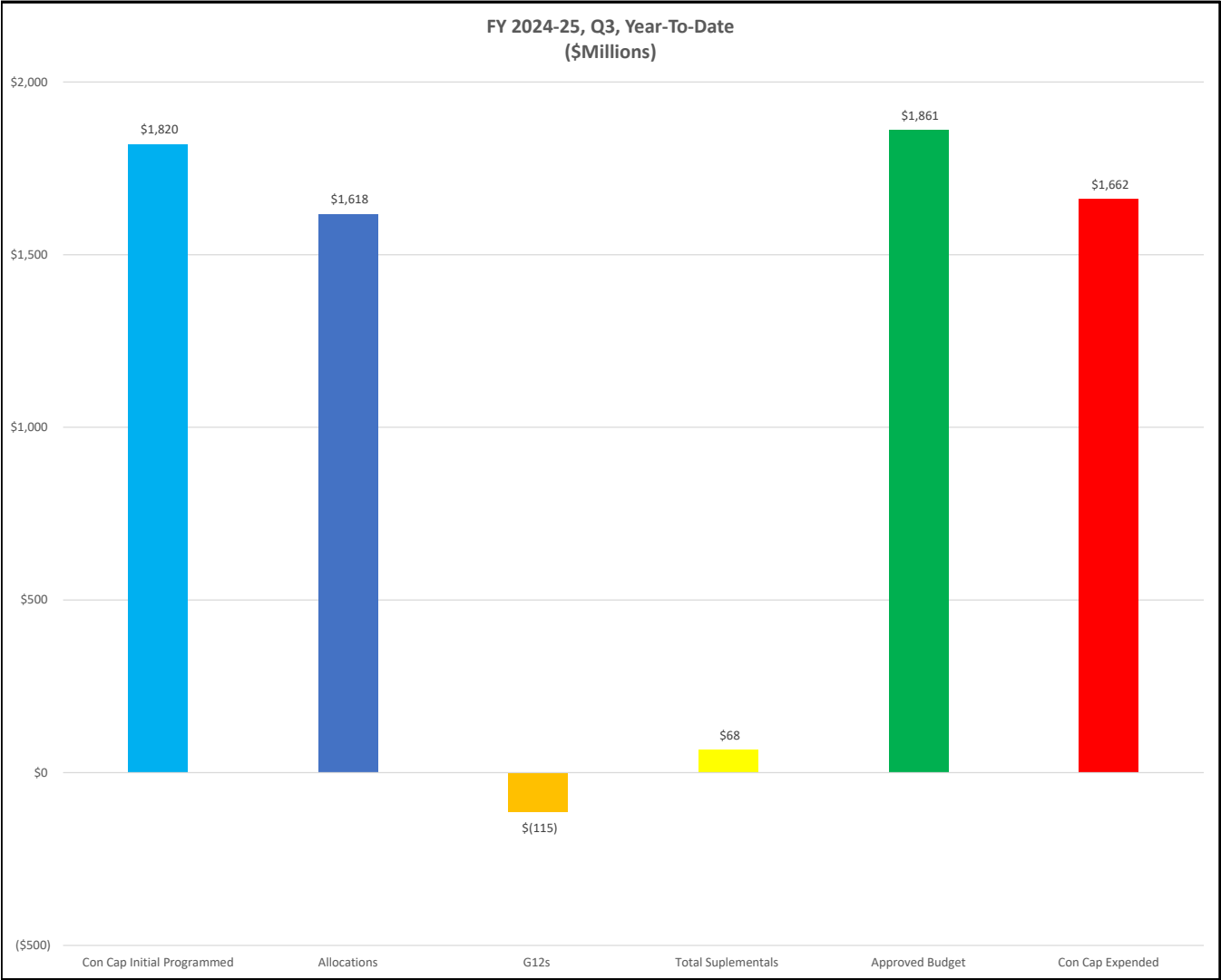
Month	SHOPP		STIP		Total	
	No. of Projects	G-12 Amount	No. of Projects	G-12 Amount	No. of Projects	G-12 Amount
July	21	\$ 4,826,700	0	\$ -	21	\$ 4,826,700
August	24	\$ 5,029,500	1	\$ 50,000	25	\$ 5,079,500
September	18	\$ 3,587,300	1	\$ 700,000	19	\$ 4,287,300
October	29	\$ 5,525,300	0	\$ -	29	\$ 5,525,300
November	15	\$ 5,350,900	1	\$ 1,500,000	16	\$ 6,850,900
December	24	\$ 4,770,400	1	\$ 336,000	25	\$ 5,106,400
January	17	\$ 6,167,800	0	\$ -	17	\$ 6,167,800
February	16	\$ 3,800,000	0	\$ -	16	\$ 3,800,000
March	28	\$ 7,588,500	0	\$ -	28	\$ 7,588,500
G-12 Year-To-Date	192	\$ 46,646,400	4	\$ 2,586,000	196	\$ 49,232,400

(E) Caltrans Third Quarter, Fiscal Year 2024-25 Retired List

D-EA	County	Route	Description	Program	Programed Budget (\$1,000s)	Risk Component	Risk Trend	Risk Description
Pre-Construction-Delivery Year Delays Risks								
02-4F900	SIS	096	Happy Camp Complete Streets	STIP	1,101	Fiscal Year Delivery	Retired	Resolved Issues; on track for delivery.
03-2J070	BUT	099	Butte 99 Keefer Slough	SHOPP	6,035	Fiscal Year Delivery	Retired	Delivered.
04-15500	ALA, CC, SOL	080, 580, 980	Install 26 Miles of Fiber Optics Trunk Line, Hubs and TOS/TMS Elements	SHOPP	93,346	Fiscal Year Delivery	Retired	Delivered.
04-1Q620	NAP	121	Napa SR 121 CAPM	SHOPP	48,499	Fiscal Year Delivery	Retired	No Delivery Issues, risk had been entered by error.
04-2Q430	SON	128	Sonoma SR 128 CAPM	SHOPP	43,239	Fiscal Year Delivery	Retired	No Delivery Issues, risk had been entered by error.
04-4AA30	NAP	128	Storm Damage	SHOPP	10,611	Fiscal Year Delivery	Retired	No Delivery Issues, risk had been entered by error.
04-1Q721	CC	680	INSTALL TOS, RM & FIBER ON 580,680 & 780 In Ala, CC Cos -Child Pro-1	SHOPP	103,635	Fiscal Year Delivery	Retired	Delivered.
05-1H440	SLO	001	Santa Maria River Bridge Replacement	SHOPP	50,836	Fiscal Year Delivery	Retired	Delivered.
06-0H220	MAD	099	South Madera 6-Lane	STIP	111,660	Fiscal Year Delivery	Retired	Received a 12-Month Time Extension.
06-0U290	KER	184	Weedpatch Hwy 3R Rehab	SHOPP	35,355	Fiscal Year Delivery	Retired	Delivered.
06-0X570	KER	005	Tejon SRRA Water & Wastewater Upgrades	SHOPP	13,460	Fiscal Year Delivery	Retired	Resolved Issues; on track for delivery.
07-28630	LA	138	SR-138 Seg 13	STIP	90,600	Fiscal Year Delivery	Retired	Resolved Issues; on track for delivery.
10-1F320	SJ	004	SJ 4 CAPM	SHOPP	21,664	Fiscal Year Delivery	Retired	Resolved Issues; on track for delivery.
10-1M230	SJ	005	SJ I-5 Mathews Rd Intersection Improvements	LOCAL	14,448	Fiscal Year Delivery	Retired	Resolved Issues; on track for delivery.
10-1L860	MPA	049	Repair/replace old TMS	SHOPP	4,226	Fiscal Year Delivery	Retired	Resolved Issues; on track for delivery.
Pre-Construction-COS Supplementals								
01-46480	HUM	101	US101 STAA Operational Improvement Project	SHOPP	692	PS&E	Retired	Supplemental Funds Request has been approved.
02-1J380	SHA	005	Cascade SHOPP	SHOPP	2,037	PS&E	Retired	Supplemental Funds Request has been approved.
04-0P920	ALA	680	BRIDGE REHABILITATION	SHOPP	1,810	PS&E	Retired	Resolved issues; no need for Supplemental Funds Request.
04-2Q720	ALA	061	PAVEMENT PRESERVATION	SHOPP	2,032	PS&E	Retired	Resolved issues; no need for Supplemental Funds Request.
04-0W060	MRN	101	Trash Capture install BMP	SHOPP	1,378	PA&ED	Retired	Resolved issues; no need for Supplemental Funds Request.
05-1H610	SB	001	Solomon Canyon Rumble Strip/Shoulder Widening	SHOPP	1,942	PS&E	Retired	Supplemental Funds Request has been approved.
07-34710	LA	134	LA-134 Overhead Signs	SHOPP	1,682	PS&E	Retired	Supplemental Funds Request has been approved.
07-36090	VEN	033	VEN-33 CAPM	SHOPP	1,388	PA&ED	Retired	Supplemental Funds Request has been approved.
12-0R320	ORA	055	12-0R320 Rte 55 Multi-Asset Project	SHOPP	9,540	PS&E	Retired	Resolved issues; no need for Supplemental Funds Request.
12-0R311	ORA	091	12-0R311 State Route 91 Multi-Asset Project	SHOPP	3,560	PS&E	Retired	Resolved issues; no need for Supplemental Funds Request.
12-0R313	ORA	091	12-0R313, Rte 091, Multi-Asset Project	SHOPP	2,660	PS&E	Retired	Resolved issues; no need for Supplemental Funds Request.
12-0R315	ORA	091	12-0R315, Rte 091, Multi-Asset Project	SHOPP	1,080	PS&E	Retired	Resolved issues; no need for Supplemental Funds Request.
Pre-Construction-Greater Than 120% Allocation								
01-40141	MEN	001	Salmon Creek Sandblast Waste Abatement	SHOPP	475	Right of Way Support	Retired	Resolved issues; no need for Greater Than 120%.
01-40141	MEN	001	Salmon Creek Sandblast Waste Abatement	SHOPP	2,000	PS&E	Retired	Resolved issues; no need for Greater Than 120%.
02-3H770	TEH	099	Upgrade Guardrail and TMS	SHOPP	33,270	Construction Capital	Retired	Resolved issues; no need for Greater Than 120%.
04-2W660	CC	080	I-80/Central Ave UC Ped & Bike	STIP	677	PS&E	Retired	Resolved issues; no need for Greater Than 120%.
05-1H440	SLO	001	Santa Maria River Bridge Replacement	SHOPP	32,640	Construction Capital	Retired	Greater than 120% has been approved.
06-0U290	KER	184	Weedpatch Hwy 3R Rehab	SHOPP	22,400	Construction Capital	Retired	Greater than 120% has been approved.
07-34610	LA	001	PCH @ LAR Bridge Seismic Retrofit	SHOPP	8,800	Construction Capital	Retired	Greater than 120% has been approved.
07-39060	LA	014	SR-14 HOV Re-striping Project	SHOPP	2,025	PS&E	Retired	Greater than 120% has been approved.
08-1L180	RIV	091	RIV 91 RIVERSIDE PAVEMENT REHAB	SHOPP	20,256	Construction Capital	Retired	Resolved issues; no need for Greater Than 120%.
11-43146	SD	008	LANDSCAPE MITIGATION FOR 42210	SHOPP	50	Construction Capital	Retired	Greater than 120% has been approved.
11-43164	SD	078	PAVEMENT REHAB	SHOPP	85,300	Construction Capital	Retired	Greater than 120% has been approved.

D-EA	County	Route	Description	Program	Programed Budget (\$1,000s)	Risk Component	Risk Trend	Risk Description
Pre-Construction-Supplementals to Award								
04-3Q850	ALA	084	Niles Canyon Flashing Beacons	SHOPP	5,524	Construction Capital	Retired	Supplemental Funds Request has been approved.
06-0W79U	TUL	099	Delano to Pixley Mainline Improvement	SHOPP	94,482	Construction Capital	Retired	Supplemental Funds Request has been approved.
07-35560	LA	105	MTC Equip Trn Center Under 105	SHOPP	12,123	Construction Capital	Retired	Supplemental Funds Request has been approved.
08-0G850	RIV	010	RIV 10 UPGRADE CACTUS CITY SRRA	SHOPP	36,730	Construction Capital	Retired	Supplemental Funds Request has been approved.
08-1J540	SBD	015	SBD 15 BRIDGE RAIL UPGRADE	SHOPP	11,655	Construction Capital	Retired	Supplemental Funds Request has been approved.
09-37470	INY	395	BISHOP PAVEMENT	SHOPP	15,307	Construction Capital	Retired	Supplemental Funds Request has been approved.
11-43090	SD	805	BRIDGE PRESERVATION - 43090K	SHOPP	8,937	Construction Capital	Retired	Supplemental Funds Request has been approved.
12-0R314	ORA	091	12-0R314, Rte 091, Multi-Asset Project	SHOPP	6,148	Construction Capital	Retired	Resolved issues; no need for Supplemental Funds Request.
12-0R32U	ORA	055	12-0R32U, Rte 055, Multi-Asset / Safety Project	SHOPP	131,703	Construction Capital	Retired	Resolved issues; no need for Supplemental Funds Request.
12-0R57U	ORA	405	12-0R57U Rte. 405 Reh pvmt, Indscpng-install WIM-add stations	SHOPP	59,067	Construction Capital	Retired	Resolved issues; no need for Supplemental Funds Request.
During Construction-COS Supplementals								
04-4J890	SCL	082	CAPM	SHOPP	1,974	Construction Support	Retired	Supplemental Funds Request has been approved.
04-4J89U	SCL	082	82 CAPM & ADA	SHOPP	1,854	Construction Support	Retired	Supplemental Funds Request has been approved.
06-0U500	KER	005	Wheeler Ridge CAPM	SHOPP	2,100	Construction Support	Retired	Supplemental Funds Request has been approved.
06-0V280	KER	184	Kern 184/Sunset Roundabout	SHOPP	970	Construction Support	Retired	Resolved issues; no need for Supplemental Funds Request.
10-1C790	MER	005	MER John Erreca Roadside Rest Area	SHOPP	4,965	Construction Support	Retired	Resolved issues; no need for Supplemental Funds Request.
During Construction-Supplementals to Complete Construction								
01-43480	MEN	001	Pudding Creek Bridge	SHOPP	9,338	Construction Capital	Retired	Resolved issues; no need for Supplemental Funds Request.
01-0A131	MEN	162	Eel River Bridge Replacement	SHOPP	7,972	Construction Capital	Retired	Resolved issues; no need for Supplemental Funds Request.
04-4H050	SON	012	SON 12 Bridge Scour Mitigation	SHOPP	10,752	Construction Capital	Retired	Resolved issues; no need for Supplemental Funds Request.
04-4J890	SCL	082	CAPM	SHOPP	26,065	Construction Capital	Retired	Supplemental Funds Request has been approved.
04-4J89U	SCL	082	82 CAPM & ADA	SHOPP	33,858	Construction Capital	Retired	Supplemental Funds Request has been approved.
06-0U500	KER	005	Wheeler Ridge CAPM	SHOPP	16,800	Construction Capital	Retired	Supplemental Funds Request has been approved.
07-34320	LA	405	majSlope Stability	SHOPP	14,357	Construction Capital	Retired	Resolved issues; no need for Supplemental Funds Request.
08-0G691	SBD	018	SBD 18 RELINE OR REPLACE CULVERTS	SHOPP	5,679	Construction Capital	Retired	Resolved issues; no need for Supplemental Funds Request.
10-1C790	MER	005	MER John Erreca Roadside Rest Area	SHOPP	29,418	Construction Capital	Retired	Resolved issues; no need for Supplemental Funds Request.
12-0R920	ORA	090	12-0R920 Rt 90 Safety Improvements SB 57 Off Ramp & at Kraemer	SHOPP	1,193	Construction Capital	Retired	Supplemental Funds Request has been approved.
Post-Construction-Supplementals to Closeout								
06-1E200	MAD	099	MADERA 99 AND 152, BRIDGE DECK REHAB	SHOPP	180	Construction Support	Retired	Resolved issues; no need for Supplemental Funds Request.
10-0S740	SJ	088	SJ/ALP 88 DRAINAGE SYSTEM	SHOPP	4,090	Construction Capital	Retired	Supplemental Funds Request has been approved.

(F) Construction Capital Closeout¹ Summary
SHOPP Only Funds²



¹Project closeout reflects projects where construction contract was complete two quarters ago.
²Appendix B, 226 SHOPP Completed Projects at Closeout, Q3, Year-To-Date, FY2024-25.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.9, Information Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **FINAL RIGHT OF WAY EXPENDITURES REPORT FOR STATE
TRANSPORTATION IMPROVEMENT PROGRAM PROJECTS AT
CONSTRUCTION CONTRACT ACCEPTANCE**

SUMMARY:

The California Department of Transportation (Department) is presenting the Final Right of Way Expenditures Report at Construction Contract Acceptance (CCA) for four State Transportation Improvement Program (STIP) projects to the California Transportation Commission (Commission) as an informational item.

BACKGROUND:

Current STIP Right of Way reporting policy, adopted by the Commission in August 2014, requires the reporting of Department State-Administered STIP Right of Way Capital and Support expenditures at the time of CCA. This policy is consistent with Senate Bill 853, which was signed by the Governor in June 2014.

The attached spreadsheet contains the final expenditure estimate for four STIP projects that have reached the CCA milestone. The Department has notified the regional transportation planning agencies of this report.

Attachment

**Final Right of Way Expenditure Report for STIP projects
at Construction Contract Acceptance
June 2025**

Reference No. 3.9
June 26-27, 2025
Attachment
Page 1 of 1

Project	Dist	Co	Rte	PPNO	R/W Estimate at Construction Allocation (R/W Support + Capital) (x \$1,000)		Final R/W cost at Construction Contract Acceptance (CCA) (R/W Support + Capital) (x \$1,000)	County/Interregional Share Adjustments Compared to original programmed amount (R/W Support + Capital) (x \$1,000)			Construction Contract Acceptance (CCA) Date
1	03	Butt	70	9801B							June 2024
	Project Title				Programmed	Estimated Expenditures	Final Cost (CCA)	Previous Share Adjustment at allocation	Total Share adjustment at CCA	Net Share Adjustment	
	SR 70 Passing Lanes (Segment 3)										
	Program			RIP	\$1,400	\$1,118	\$1,042	\$282 (Credit)	\$76 (Credit)	\$358 (Credit)	
				IIP	\$1,400	\$1,118	\$1,042	\$282 (Credit)	\$76 (Credit)	\$358 (Credit)	

Project	Dist	Co	Rte	PPNO	R/W Estimate at Construction Allocation (R/W Support + Capital) (x \$1,000)		Final R/W cost at Construction Contract Acceptance (CCA) (R/W Support + Capital) (x \$1,000)	County/Interregional Share Adjustments Compared to original programmed amount (R/W Support + Capital) (x \$1,000)			Construction Contract Acceptance (CCA) Date
2	03	Yuba	70	9824							January 2024
	Project Title				Programmed	Estimated Expenditures	Final Cost (CCA)	Previous Share Adjustment at allocation	Total Share adjustment at CCA	Net Share Adjustment	
	Passing Lanes Segment 4 & 5 Project										
	Program			IIP	\$5,500	\$5,487	\$5,830	\$0 (< 20 %)	\$0 (< 20 %)	\$0 (< 20 %)	

Project	Dist	Co	Rte	PPNO	R/W Estimate at Construction Allocation (R/W Support + Capital) (x \$1,000)		Final R/W cost at Construction Contract Acceptance (CCA) (R/W Support + Capital) (x \$1,000)	County/Interregional Share Adjustments Compared to original programmed amount (R/W Support + Capital) (x \$1,000)			Construction Contract Acceptance (CCA) Date
3	06	Kern	46	3386E							March 2025
	Project Title				Programmed	Estimated Expenditures	Final Cost (CCA)	Previous Share Adjustment at allocation	Total Share adjustment at CCA	Net Share Adjustment	
	Route 46 Expressway Segment 4C										
	Program			RIP	\$3,350	\$3,085	\$3,685	\$0 (< 20 %)	\$0 (< 20 %)	\$0 (< 20 %)	

Project	Dist	Co	Rte	PPNO	R/W Estimate at Construction Allocation (R/W Support + Capital) (x \$1,000)		Final R/W cost at Construction Contract Acceptance (CCA) (R/W Support + Capital) (x \$1,000)	County/Interregional Share Adjustments Compared to original programmed amount (R/W Support + Capital) (x \$1,000)			Construction Contract Acceptance (CCA) Date
4	10	Merced	99	0161B							August 2024
	Project Title				Programmed	Estimated Expenditures	Final Cost (CCA)	Previous Share Adjustment at allocation	Total Share adjustment at CCA	Net Share Adjustment	
	Livingston Widening Southbound										
	Program			IIP	\$300	\$128	\$89	\$172 (Credit)	\$39 (Credit)	\$211 (Credit)	

IIP Interregional Improvement Program
RIP Regional Improvement Program

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.17, Information Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REPORT OF FINAL EXPENDITURES FOR STIP PROJECTS AT CLOSEOUT**

SUMMARY:

The California Transportation Commission (Commission) is presenting this as an informational item to report final support expenditures for four State Transportation Improvement Program (STIP) projects.

BACKGROUND:

Current STIP Guidelines under Resolution G-23-58, adopted by the California Transportation Commission (Commission) on August 16, 2023, stipulate that the Commission will maintain a long-term balance of county shares and interregional shares, as specified in Streets and Highways Code Section 188.11. Typically, share balance adjustments for final project development and right of way are reported to the Commission at the time of construction allocation. However, these projects were programmed for pre-construction components only and do not require a Commission action. Final expenditure details for these projects are listed on the attached spreadsheet. The Department has officially notified the regional transportation planning agencies of this report.

Attachment

Report of Final Expenditures for STIP Projects (\$ in 1,000)

Dist	Co	Rte	PPNO	PA&ED		PS&E		R/W Support		R/W	
2	TRI	3	3528								
Project Title				Programmed	Final Expenditures	Programmed	Final Expenditures	Programmed	Final Expenditures	Programmed	Final Expenditures
Trinity 3 Turnouts											
Program		RIP		\$150	\$150						
Comments				This project was unprogrammed from the STIP due to unfavorable fund estimates in 2016, and the same scope of work was successfully completed under SHOPP Minor A Project 02-2H510, with final closeout on 01/27/2020.							

Dist	Co	Rte	PPNO	PA&ED		PS&E		R/W Support		R/W	
6	KER	46	3412								
Project Title				Programmed	Final Expenditures	Programmed	Final Expenditures	Programmed	Final Expenditures	Programmed	Final Expenditures
Wasco 4-Lane											
Program			RIP	\$2,070	\$2,241						
Comments				Project is included in Kern COG's RTP as fiscally unconstrained, indicating it remains a regional priority but currently lacks committed funding for implementation.							

Dist	Co	Rte	PPNO	PA&ED		PS&E		R/W Support		R/W	
9	KER	58	2664								
Project Title				Programmed	Final Expenditures	Programmed	Final Expenditures	Programmed	Final Expenditures	Programmed	Final Expenditures
SR 58 Truck Climbing Lance											
Program			RIP-COVID	\$2,272	\$1,130						
Comments				No adjustment to the Kern County share will be made.							

Dist	Co	Rte	PPNO	PA&ED		PS&E		R/W Support		R/W	
11	SD	5	0615								
Project Title				Programmed	Final Expenditures	Programmed	Final Expenditures	Programmed	Final Expenditures	Programmed	Final Expenditures
Route 5 Widening for HOV, Mixed flow & Aux Lanes											
Program		RIP		\$9,730	\$9,455						
Comments				Construction is funded with local funds.							

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.10, Information Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **QUARTERLY REPORT – LOCAL ASSISTANCE ANNUAL ALLOCATION FOR
THE PERIOD ENDING MARCH 31, 2025**

SUMMARY:

As of March 31, 2025, approximately \$690 million, or 25 percent, of the \$2.8 billion allocated by the California Transportation Commission (Commission) for Fiscal Year 2024-25 has been sub-allocated to 566 local projects. The majority of the sub-allocations (approximately \$437 million) are for 331 projects in the following seven categories:

- Special Programs (Discretionary Programs, High Priority Projects, Demonstration Projects, Emergency Relief, & Miscellaneous Programs) – 148 projects, \$128 million
- Bridge Formula Program (BFP) – 19 projects, \$120 million
- National Highway Performance Program (NHPP) – 66 projects, \$77 million
- Highway Safety Improvement Plan (HSIP) – 19 projects, \$21 million
- Congestion Mitigation & Air Quality Improvement Program (CMAQ) – 48 projects, \$41 million
- Freeway Service Patrol (FSP) – 16 projects, \$25 million
- Freeway Service Patrol – Senate Bill 1 (FSP – SB1) – 15 projects, \$25 million

The remaining \$253 million was sub-allocated for 235 projects in other categories (as referenced with an asterisk on the attachment).

BACKGROUND:

The California Department of Transportation's (Department) Division of Local Assistance administers the local assistance subvention budget under delegated authority from the Commission. The Commission provides an annual allocation consistent with each State Fiscal Year's Budget Act. The Commission further delegates to the Department the authority to adjust allocations between categories, and the Department reports to the Commission if transfers in or out of an expenditure category exceed 10 percent of its allocation, per Commission Resolution G-01-08.

Attachment

FY 2024-25 LOCAL ASSISTANCE ANNUAL ALLOCATION
Period March 31, 2025
(Dollars in Thousands)

Reference No.: 3.10
June 26-27, 2025
Attachment

Fund Description	Total Sub-Allocation			Commission Allocation			Allocation Balance			Percent of Allocation Sub-Allocated	Number of Projects
	State	Federal	Total	State	Federal	Total	State	Federal	Total	Total	Total
Local Administered & Miscellaneous Programs											
Surface Transportation Block Grant Program (STBGP)		47,535	47,535		493,694	493,694		446,159	446,159	10%	45
Highway Infrastructure Program (HIP)			0		13,500	13,500 *		13,500	13,500	0%	0
Congestion Mitigation & Air Quality Improvement Program (CMAQ)		41,349	41,349		481,330	481,330		439,981	439,981	9%	48
Special Programs (Discretionary Programs, High Priority Projects, Demonstration Projects, Emergency Relief, & Miscellaneous Programs)		127,734	127,734		727,038	727,038		599,304	599,304	18%	148
Carbon Reduction Program (CRP)		30,163	30,163		55,005	55,005 *		24,842	24,842	55%	16
Promoting Resilient Operations for Transformative, Efficient, and Cost-Saving Transportation Program (PROTECT)			0		60,700	60,700 *		60,700	60,700	0%	
Community Project Funding/Congressionally Directed Spending Program (CPFCDs)		47,083	47,083		99,940	99,940 *		52,857	52,857	47%	17
Surface Transportation Block Grant Program (STBGP) State Match and Exchange	72,680		72,680	74,198		74,198	1,518		1,518	98%	139
Miscellaneous Unassigned Local Programs			0	3,250		3,250 *	3,250		3,250	0%	
Freeway Service Patrol (FSP)	25,479		25,479	25,479		25,479 *	0		0	100%	16
Freeway Service Patrol - Senate Bill 1 (FSP - SB1)	24,669		24,669	25,000		25,000 *	331		331	99%	15
Bridge Programs											
Bridge Inspection and Seismic Retrofit			0	827		827 *	827		827	0%	
Bridge Formula Program (BFP)		120,194	120,194		280,000	280,000		159,806	159,806	43%	19
National Highway Performance Program (NHPP)		76,864	76,864		338,050	338,050		261,186	261,186	23%	66
Rail Programs											
Railroad Grade Separations	10,000		10,000	15,000		15,000 *	5,000		5,000	67%	2
Railroad Grade Crossing Protection Maintenance			0	4,000		4,000 *	4,000		4,000	0%	
Railroad Grade Crossing Protection		2,488	2,488		24,000	24,000 *		21,512	21,512	10%	6
Safety Programs											
Highway Safety Improvement Program (HSIP)		20,891	20,891		60,000	60,000		39,109	39,109	35%	19
Senate Bill 137 Exchange - HSIP	1,471		1,471	30,000		30,000 *	28,529		28,529	5%	2
Total Local Assistance Subvention Funds	134,299	514,301	648,600	177,754	2,633,257	2,811,011	43,455	2,118,956	2,162,411	23%	558
Federal Transit Administration (FTA) Transfers		41,671	41,671								8
Total Local Assistance including FTA Transfers	134,299	555,972	690,271	177,754	2,633,257	2,811,011	43,455	2,118,956	2,162,411	25%	566

Notes

Allocations for State funds reflect the June 27-28, 2024 Commission meeting vote, Item 2.5h., Resolution FM-23-05.
Allocations for federal funds reflect the October 17-18, 2024 Commission meeting vote, Item 2.5h., Resolution FM-24-01.
Allocation Balance is the difference between Commission Allocation and Total Sub-Allocation.
Total Sub-Allocation amounts reflect data from the accounting system as of March 31, 2025.
FTA Transfers are \$976,000.00 for STBGP, \$36,150,104 CMAQ, and \$4,544,896 CRP.
In accordance with Commission Resolution G-01-08, the Department reports when total transfers in or out of an expenditure category exceed 10 percent of its allocation.
* Indicates programs that were not discussed in book item Reference 3.10.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.15, Information Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **PRELIMINARY CLOSE-OUT REPORT ON THE FISCAL YEAR 2024-25 MINOR PROGRAM ANNUAL ALLOCATION**

SUMMARY:

As of May 20, 2025, the California Transportation Commission (Commission) allocated and the California Department of Transportation (Department) sub-allocated \$123,503,000 in construction capital and expended \$95,611,000 in capital outlay support for the 2024-25 State Highway Operation and Protection Program (SHOPP) Minor Program, which constitutes 88 percent of the approved \$250,000,000 allocation.

BACKGROUND:

At its October 2019 meeting, the Commission approved Resolution G-19-05a delegating to the Department the sub-allocation and adjustment authority for minor construction capital projects. Resolution G-19-05a, allows the Department to sub-allocate funding and advertise projects without waiting for Commission meetings to receive an allocation. However, Commission approval is required for Minor A projects not previously identified in the Department's approved list.

The Minor Program is a one-year program reserved for low-cost capital projects that are SHOPP eligible. Minor projects must be awarded in the fiscal year in which they were allocated. Effective February 1, 2024, Minor B projects have a construction limit up to \$461,000. Minor A projects have a construction limit ranging from \$461,001 up to \$1,250,000.

The Commission approved an allocation for 2024-25 of \$250,000,000 (\$148,786,000 for construction capital and \$101,214,000 for capital outlay support). As of May 20, 2025, the Department has utilized:

Construction Capital

Minor A Projects

- delegated by the Department \$20,787,000
- allocated by the Commission \$7,679,000

Minor B \$93,622,000

Right of Way \$1,108,000

Day Labor \$307,000

Capital Outlay Support \$95,611,000

Total **\$219,114,000**

In total, the Department has expended \$219,114,000 of the approved \$250,000,000 for the 2024-25 Minor Program.

The Department intends to submit to the Commission the final 2024-25 SHOPP Minor Program Close-Out Report at the October 2025 Commission meeting.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.16, Information Item

Prepared By: René Fletcher, Chief (Acting)
Division of Right of Way and Land Surveys

Subject: **FISCAL YEAR 2024-25 – THIRD QUARTER – AIRSPACE PROGRAM REPORT**

SUMMARY:

In accordance with Resolution G-25-14, adopted by the California Transportation Commission (Commission) in January 2025, the California Department of Transportation (Department) will present its Airspace Program Quarterly Report for the third quarter of Fiscal Year 2024-25, covering the period from January 1, 2025, to March 31, 2025. This will be an informational item at the Commission's meeting on June 26-27, 2025.

BACKGROUND:

Resolution G-25-14 (Procedure for Leasing Airspace to Private Entities) and Resolution G-25-24 (Procedures for Leasing Airspace to Public entities) require the Department to report to the Commission on its progress towards ensuring compliance as it relates to airspace leases, the Department's guidelines and procedures, and all applicable State/Federal laws and regulations. Compliance with the aforementioned items requires that the Department enforce airspace lease terms, conduct regular inspections, and promptly take the necessary administrative and legal actions to safeguard the transportation system.

Furthermore, under Resolution G-25-14, the Department will report to the Commission each quarter with details on the status of each airspace site, any instances of identified unauthorized right-of-way encroachments, and efforts undertaken for management and monitoring to ensure compliance. The report will highlight any safety risks or violations discovered during inspections, safety incidents, and actionable plans to promptly address any issues. Additionally, the quarterly report will provide an overview of the Department's efforts in providing the necessary guidance and training to ensure proper execution of airspace leases, promptly addressing identified safety risks and violations, and managing and monitoring airspace sites.

Attachment



Department of Transportation Airspace Program Quarterly Report

**Fiscal Year (FY) 2024-25
Third Quarter
(Reporting Period Ending March 31, 2025)**

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Department of Transportation Airspace Program Report FY 2024-25, Third Quarter

1 Executive Summary

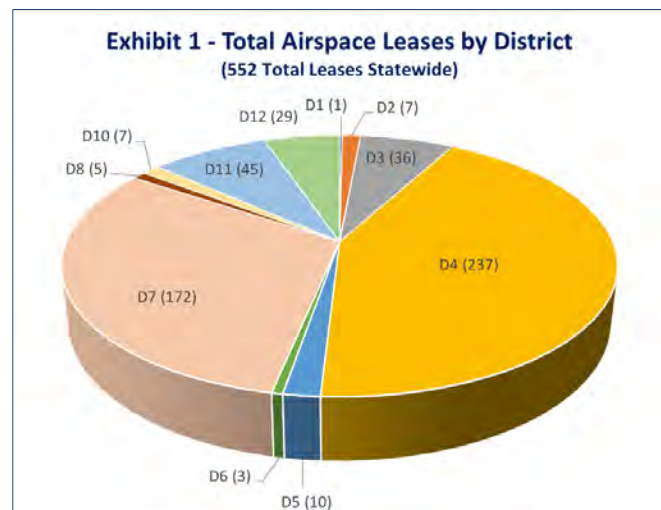
The Department of Transportation (Department) managed 552 airspace sites throughout the state, which are properties within operating right of way that support a secondary use without undue interference to highway operations. The Department is committed to upholding public safety and safeguarding the traveling public through effective and efficient management of its airspace program. To this end, the Department is providing the Commission with a report on the status and progress of statewide airspace sites, including the status of policy changes to the program. Furthermore, the Department has taken greater measures to increase transparency and accountability by enhancing the management of various aspects of the airspace leasing program and implementing quality control mechanisms to ensure compliance with state and federal laws, the Resolutions G-25-14 and G-25-24, and the Department's policies and guidance.

The sections to follow provide additional analyses of key elements of airspace leases, management and oversight, including inspection status and actions the Department has taken to address lease violations, safety risks, and other continued efforts to improve the airspace program. A separate detailed report of each airspace lease site has been provided to the Commission Staff.

2 Statewide Airspace Leases

In FY 2024-25, during the third quarter reporting period ending March 31, 2025, the Department managed a total of 552 airspace leases across 11 districts (Exhibit 1). Although 55 leases were vacated during the reporting period, recent reanalysis of the database holding airspace lease information led to the identification of an additional 55 actively managed sites, resulting in the total leases statewide remaining at 552.

District 4 accounted for 43% of the total statewide airspace leases, with District 7 managing the next highest number of leases at 31% of the statewide total.

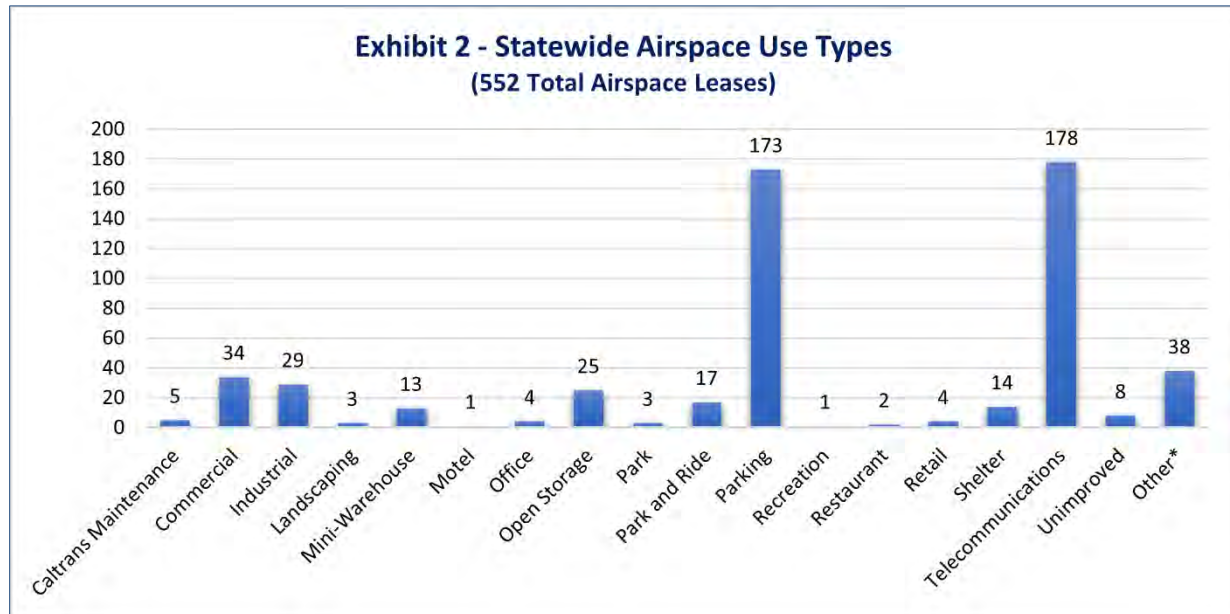


Note: There were no leases in District 9 during this reporting period.

2.1 Types of Airspace Uses

In this reporting period, the Department leased 64% of its airspace sites for parking and telecommunications combined. The Department has completed the review of all parking sites and ensured proper use designation, which identified 25 active open storage sites.

Other uses and the number of sites leased for each use are illustrated in Exhibit 2. Please refer to Table 1 for various use types by district.



* Other uses may include tower crane weathering, construction tie backs, temporary landscaping, public agency use of right of way, etc.

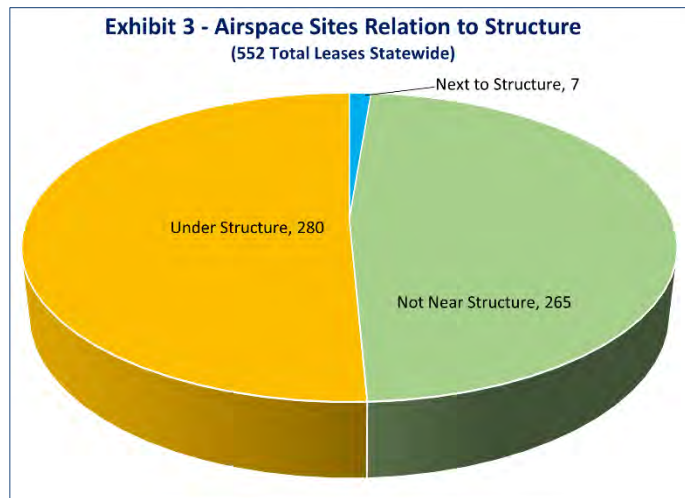
Table 1 - Airspace Use Types by District

District	Route(s)	Caltrans Maintenance	Commercial	Industrial	Landscaping	Mini-Warehouse	Motel	Office	Open Storage	Park	Park & Ride	Parking	Recreation	Restaurant	Retail	Shelter	Telecom	Unimproved	Other	Total
D1	255																		1	1
D2	5, 36, 299	1															4		2	7
D3	Various	1							1			22			1	1	5	3	2	36
D4	Various		30	2	1	2			14	1	1	84				8	72	1	21	237
D5	17, 101, 154																10			10
D6	41, 180									2		1								3
D7	Various	3	4	27	1	8	1		9		5	48				1	57	4	4	172
D8	Various																2		3	5
D9	None																			0
D10	4, 5							1				2				2			2	7
D11	Various				1	3		3	1		11	12		2	2	2	6		2	45
D12	Various											4	1		1		22		1	29
Total		5	34	29	3	13	1	4	25	3	17	173	1	2	4	14	178	8	38	552

2.2 Airspace Site in Relation to Structure

The Department has 51% of the total airspace sites located under the state highway structures, and 48% are not near a structure (Exhibit 3). Sites that are over a structure are not currently captured in the Department's database. However, the Department intends to capture this information in future database enhancements.

The Department management of airspace sites is directed by *Chapter 15 – Airspace*, of the Right of Way Manual. Prior guidance did not differentiate the different standards of rules for sites located under, over, or next to a structure. However, new policies have recently been implemented to clarify leasing in areas over, under, or next to a structure. These new policies and procedures are reflected in the Right of Way Manual effective January 2025. HQ Division of Right of Way and Land Surveys (RWLS) has issued guidance to the districts that reflected the newly implemented policy changes, which may impact historical legacy tenants who have been in place since the 1970's. The Department continues to closely coordinate with its Legal Division, ensure close communication with the tenants near structures, and may exercise the option to cancel leases, if necessary.



2.3 Third Quarter Spotlight: Homeless Shelters

The airspace program has 10 temporary homeless shelters allowed by Streets & Highways Code (SHC) 104.30 in the following counties: Alameda, Sacramento, San Francisco, Santa Clara, and Los Angeles. These shelters range from tiny homes, portable buildings with approximately 20 beds, to larger, more traditional shelter improvements. Additionally, the department has 4 permanent structures allowed by other SHC sections outside of 104.30 located in San Diego and San Joaquin Counties.

2.4 Status of Lease Agreements and Auctions

As of March 31, 2025, 128 leases have expired, which represented approximately 23% of the total leases. This was a reduction of 7 expired leases from the second quarter reporting of 135 expired leases. Expired leases are in holdover¹ status while the Department is implementing programmatic changes to the airspace program. It should be noted that telecommunication sites are excluded from this count as their leases have options to renew, and the expiration dates on record do not reflect these available options. The Department continues to review all district files and is anticipated to be completed by July 31, 2025.

¹ Holdover is when the lease has expired and the Department continues to accept rent payment from the tenant. The lease is still valid but is on a month-to-month basis.

Determining which leases may be cancelled may take an additional 9-12 months for legal review and the unlawful detainer² process. The remaining sites with expired leases will be scheduled for public auction or presented to the CTC for approval of direct negotiations in an upcoming CTC meeting, with the goal of being in compliance by January 2026. Meanwhile, expired leases will remain in holdover until compliance is achieved within the next calendar year.

Since the Department is preparing for future auctions, there were no bid auctions during the third quarter. There was one submittal to the CTC for direct negotiations during this reporting period.

3 Airspace Site Inspections

HQ RWLS is responsible for developing policies and procedures, providing oversight of the airspace program and guidance to the districts for site inspections of airspace properties, and overseeing other site management activities. Each district airspace office is responsible for the compliance of leased airspace sites within its area. This includes conducting routine inspections of the sites, whereby developed leased sites (with structures) are inspected at least quarterly and non-developed leased sites (parking lots, wireless locations, parks, etc.) are inspected at least annually.

Site inspections are conducted by both district staff and the State Fire Marshal (SFM), a program within the Department of Forestry and Fire Protection (CAL FIRE). District staff conducts quarterly or annual inspections for general lease violations, hazards, and unauthorized uses. The SFM conducts site inspections per their authority according to their inspection schedule, when requested by the Department, or if potential fire code violations or fire hazards are identified by the district staff.

During the reporting period, 68 sites (12%) were designated as quarterly inspection, which less than the quarter before due to a tenancy being vacated. The remaining 484 sites (88%) were designated as annual inspection (Exhibit 4). The designation of inspection cycle may change in response to changes in site use and policies, and the Department will continue to address these changes in an ongoing basis.



² An Unlawful Detainer, commonly called an eviction, is a court action used when the tenant will not peaceably vacate a property.

3.1 Site Inspection Status & Plan

The Department completed 182 site inspections during the reporting period. In addition, there were 47 sites overdue for inspection, of which 29 were quarterly inspection sites. The decrease in overdue inspections compared to the second quarter was largely due to an increase in the number of sites inspected.

In the fourth quarter, the Department plans to conduct 142 annual site inspections in addition to the 68 quarterly, that are due for inspection. The Department remains committed to bringing all overdue inspections to compliance immediately and will continue to coordinate with districts on timely inspections to reduce the number of overdue site inspections to zero. Furthermore, the Department will reassess the inspection schedules to establish a more balanced inspection plan for each quarter in the next fiscal year.

3.2 Inspection Findings & Enforcement Actions

Of the 182 site inspections conducted by district staff during the reporting period, 180 (99%) passed district inspection. Two failed initial inspections due to minor violations, such as weed abatement, materials stored, and litter.

As of March 31, 2025, the Department also had 17 leased sites with outstanding failed inspections by the SFM, a decrease from the 22 sites reported in the second quarter. The failed SFM inspections were due to various reasons, which included fire protection and life safety issues, permitting issues, electrical issues, combustibles, etc. Enforcement actions have been initiated on the 17 sites, summarized as follows:

- Seven sites have initiated corrective actions or are in the process of taking corrective actions. These sites will be re-inspected within 90 days from the date of notice. Failed re-inspections will result in the cancellation of leases.
- Seven sites have been issued a notice to correct and have made progress to correct violations, though some issues may be uncorrectable; see additional information below.
- Three sites have ongoing inspections due to multiple buildings.

Upon a failed inspection, district staff take a series of actions to address the violations. These actions include, but are not limited to, providing guidance to the tenant after failed inspections, issuing notices to correct, and potentially initiating the unlawful detainer process. Additionally, HQ RWLS meets with the SFM monthly to discuss various issues and solutions, such as high-risk concerns, obtain the status of necessary permits, or reinspection needed to clear deficiencies.

HQ RWLS and the SFM have met this quarter to review the outstanding failures, specifically the seven sites identified above. Many of the issues noted in the failed SFM reports are the result of permitting issues that are uncorrectable or cost-prohibitive for the tenant. These sites have active leases; therefore, the Department must address these issues through a multidisciplinary review committee and request funding to

remove the improvements if corrections are unachievable.

4 Program Enhancement Measures

The Department has taken significant measures to enhance the management and oversight of the Airspace Program, which includes the following:

1. **Statewide Training:** HQ RWLS has conducted all three phases of airspace training to district airspace staff. Each phase was conducted twice and focused on specific topics of airspace management. All trainings were recorded and posted on the Department intranet to be available anytime. The trainings were as follows:
 - a. Phase 1, conducted in October 2024, comprised of inspection processes and procedures, communication protocol with lessee, lease violation enforcement actions and resolution process, legal action process, and elevating unresolved violations.
 - b. Phase 2, conducted in December 2024, comprised of the competitive bidding process and new lease agreement process.
 - c. Phase 3, conducted in January 2025, comprised of delinquent actions and the resolution process (late fees, penalties, interest charges, delinquencies, unlawful detainer actions, etc.) and communication protocol with the lessee on delinquency.
2. **Task Force:** The Department is in the process of establishing a task force to spot-check district airspace sites, in partnership with the HQ Division of Maintenance, Office of Structure Maintenance & Investigations, SFM, and District Maintenance staff. This task force is anticipated to be established by the end of FY 2024/25. An annual site review plan will subsequently be developed and implemented to enhance HQ oversight of the airspace program.
3. **Auctions:** The Department continues to track expired and upcoming expiring leases and monitor district plans for conducting auctions for these sites on a quarterly basis. There were no reported auctions in the third quarter.
4. **Quality Enhancement Joint Review (QEJR):** The Department has developed a QEJR Plan to review current district operations and ensure quality work products that are consistent with current policies and guidance. The QEJR will begin in July 2025 and will be conducted at least annually with each district or sooner on an ongoing basis.
5. **Enforcement:** The Department has begun tracking airspace enforcement actions and status (inspections, delinquencies, violations, legal actions, etc.) on a quarterly basis and closely coordinated with district staff to ensure areas of concern are addressed timely.
6. **Executive Reporting:** The Department will deliver an Airspace Progress Report to district executive management, Project Delivery Deputy Director, and

Chief Deputy Director on the status and progress of various aspects of district lease management and enforcement actions.

5 Policies, Manuals, & Guidance Updates

Since November 11, 2023, the Department has implemented significant changes to airspace policies and manual guidance. Some of these changes are listed below.

1. **District Weekly Reporting:** Districts are required to report weekly to HQ RWLS on site-specific actions taken during the week, including but not limited to, district annual inspections, SFM inspections, SFM inspection results, and notices of correction or termination issued.
2. **Enforcement Actions:** All corrective notices must be in writing and include the date the corrections must be completed. A follow-up action must be taken by district staff on that date, such as a re-inspection. If corrections are not completed by the follow-up date, the district must proceed with termination of tenancy or appropriate proceedings in consultation with Department Legal.
3. **New Prohibitions:** Following Federal Highway Administration (FHWA) clarification of its guidance on prohibited materials, the Department sent notices to all airspace tenants in April 2024 that included a list of materials prohibited on the Department airspace sites. District airspace managers will review leased sites for the presence of prohibited materials during routine inspections and take immediate actions according to the latest policy and guidance.
4. **Applications and Credit Checks:** The Department updated the lease application for potential tenancies, which now requires review of the applicant's credit history and the applicant certifying they have had no bankruptcies or unlawful detainers in the last 10 years.
5. **Pause on New Leases and Lease Renewals:** In the past quarter, with all staff trained in the auction and CTC processes, districts are able to begin processing new leases. Districts have begun to address and plan for future auctions, with goal of being in compliance with new policies by January 31, 2026.
6. **Open Storage Prohibition:** The Department currently has 25 open storage leases, of which 18 sites are underneath a structure. The Department is reviewing the 18 lease agreements and may consider terminating the leases. As part of the new programmatic policy changes, no new open storage leases will be executed for a site located, over, under, or next to a structure, except for uses that are permitted by policy, such as construction staging and leases to government entities.

7. **Holdovers:** In this reporting period, 128 of the total airspace leases were in holdover, of which 123 have inspections that are up to date, and the remaining five sites were overdue for inspection but are expected to complete inspection in the fourth quarter. Telecommunication sites were excluded from the holdover count as their leases have the option to renew, and the expiration dates on record do not reflect these available options. Leases in holdover will be addressed by cancellation or renewed through the renewal process.
8. **Unlawful Detainers (UDs):** During the reporting period, three UD were filed. Previously, UD requests required District Director approval. New policy changes streamlined the approval process by lowering the approval delegation for UD requests to the District Right of Way Manager. This will increase efficiency by addressing and remedying tenancy issues in a timely manner.
9. **Insurance Certification:** The Department will verify that all insurance certifications are current. As part of the recent programmatic changes, all sites must have a combined insurance policy of \$20M coverage (general liability plus umbrella) for any leases under, over, or next to a structure as the lease dictates. The new insurance coverage is effective March 1, 2025. Lessees that do not comply by this date may be subject to termination.
10. **Subleasing:** The district must request approval from HQ RWLS if a tenant requests a sublease that is not allowed under the lease agreement. The districts have been notified to survey all their leases to identify any existing subleases not allowed under the agreement. HQ RWLS will continue to coordinate closely with the districts to resolve any outstanding subleases to ensure compliance with the current policy, and the district will take immediate corrective actions for any non-compliant subleases.

The Department did not receive any sublease requests during the reporting period. There was one sublease request from the first quarter that is currently awaiting approval from FHWA on the programmatic changes and the sublease request.

11. **Action Plans:** The Department is aggressively inspecting all properties to ensure compliance with lease terms and conditions. Violations are being addressed through written notices to comply; some notices to terminate agreements are pending. HQ RWLS convenes a statewide weekly hot topics meeting to review changes in the airspace leasing program and address district challenges. A monthly meeting is also held with the State Fire Marshal and HQ RWLS to identify where increased oversight is required.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.1, Information Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **UPDATE ON THE MINOR B PROJECT LIST UNDER THE 2024-25 STATE
HIGHWAY OPERATION AND PROTECTION PROGRAM - MINOR
CONSTRUCTION PROGRAM**

SUMMARY:

The California Transportation Commission (Commission) approved Resolution G-19-05a which authorizes the California Department of Transportation (Department) to sub-allocate funds for State Highway Operation and Protection Program (SHOPP) Minor B projects. At its June 2024 meeting, the Commission approved the funding and project listing for the 2024-25 SHOPP Minor Construction Program under Resolution FM-23-06.

In accordance with approval of the SHOPP Minor Construction Program, the Department has the authority to modify the SHOPP Minor B project list, with the understanding that these actions will be reported to the Commission at the next scheduled Commission meeting. The Department uses prudent business practices to manage the combination of individual project cost increases and savings to meet Commission policies.

At the May 2025 meeting, the Department presented an update to the Minor B project list to the Commission. However, since the May 2025 meeting, the Department has added 13 projects and removed 7 projects from the approved SHOPP Minor B project list.

Therefore, the Department is presenting this update to the SHOPP Minor B project list, as an information item to the Commission.

BACKGROUND:

The delegated authority under Resolution G-19-05a allows the Department to begin work without waiting for the next Commission meeting to receive an allocation.

The Minor B project limit is established to be consistent with the lower limit of the State Contract Act. The State Contract Act is reviewed each even numbered year by the Department of Finance and adjusted to be consistent with change in the annual California Construction Index. Effective February 1, 2024, Minor B projects have a construction limit up to \$461,000.

Attachment

Update to the 2024-25 Minor B Program
Districts 01-12
(As of: June 2025)
(x\$1,000)

Minor B Projects - ADD

No.	District	County	Route	Postmile	Project Location/Description	EA	Project ID	Program Code	Activity Category	Construction (Life of Project)	Right of Way (Life of Project)	Support (Life of Project)	Total (Life of Project)	Performance Value	Performance Measure
1	01	HUM	101	112.3	Near Trinidad, 0.24 mile south of Kane Road. Repair storm drainage.	0Q070	0125000122	201.130	Other Program Objectives	\$290	\$5	\$105	\$400	1.0	Location(s)
2	02	PLU	284	6.2	In Plumas County, 0.6 mile south of Little Last Chance Creek Bridge. Perform rock slope scalation operations and repair pavement.	2K650	0225000130	201.130	Other Program Objectives	\$122	\$0	\$25	\$147	1.0	Location(s)
3	03	YOL	16	11.98	Near Guinda, at Forest Avenue. Install culvert and drainage inlet, and install striping.	1N830	0325000118	201.130	Other Program Objectives	\$461	\$0	\$130	\$591	1.0	Location(s)
4	03	SIE	49	1.96	Near Camptonville, 1.6 miles north of Forest 47 Road. Install rockfall mitigation, stabilize slope and remove debris.	2N000	0325000201	201.130	Other Program Objectives	\$300	\$0	\$200	\$500	1.0	Location(s)
5	03	SAC	99	8.77	Near Elk Grove, at the McConnell Overhead No. 24-0392. Regrade slope, install AC dike and replace joint seal and barrier.	2N040	0325000210	201.130	Other Program Objectives	\$420	\$0	\$100	\$520	1.0	Location(s)
6	06	KER	46	50.27	In Wasco, at Poplar Avenue. Install hybrid beacon system, traffic signs, and traffic striping and pavement markings.	1F591	0625000138	201.015	Other Program Objectives	\$40	\$0	\$20	\$60	0.00	Annual fatal and serious injury collision(s)
7	06	FRE	180	98.34/120.0	Near Dunlap, 2.9 miles east of Old Oak Road to 0.07 mile east of Forest Route 13S95. Repair slope.	1H660	0625000198	201.130	Other Program Objectives	\$190	\$0	\$76	\$266	2.00	Location(s)
8	09	MNO	158	R2.8/3.2	In June Lake, from Cherokee Lane to Lyle Terrace Road. Repair slope.	40420	0924000068	201.131	Other Program Objectives	\$458	\$0	\$45	\$503	1.0	Location(s)
9	09	KER	14	58.2/62.3	Near Inyokern, 0.4 mile north of Isabella Walker Pass Road to 0.3 mile south Bow Avenue. Repair slope.	42700	0925000062	201.130	Other Program Objectives	\$449	\$10	\$110	\$569	1.0	Location(s)
10	11	SD	5	R15.115/R15.41	In San Diego, 0.2 south of Broadway to eastbound Route 94 Connector. Stabilize slope and install slope pavement.	3A986	1125000222	201.130	Other Program Objectives	\$460	\$0	\$90	\$550	1.0	Location(s)
11	11	SD	94	3.25	In San Diego, east of Route 15. Replace overhead sign.	3A982	1125000227	201.130	Other Program Objectives	\$150	\$0	\$30	\$180	1.0	Location(s)
12	11	SD	76	M2.75	In Oceanside, near Foussat Road. Tree removal.	3A983	1125000229	201.130	Other Program Objectives	\$120	\$0	\$24	\$144	1.0	Location(s)
13	12	ORA	55	10.4	In Tustin, at 4th Street. Repair guardrail.	0V860	1225000097	201.130	Other Program Objectives	\$116	\$0	\$47	\$163	1.0	Location(s)
Total										\$3,576	\$15	\$1,002	\$4,593		

Minor B Projects - REMOVE

No.	District	County	Route	Postmile	Project Location/Description	EA	Project ID	Program Code	Activity Category	Construction (Life of Project)	Right of Way (Life of Project)	Support (Life of Project)	Total (Life of Project)	Performance Value	Performance Measure
1	06	KER	46	50.27	In Wasco, at Poplar Avenue. Install Flashing Beacon, hybrid beacon, and bulbout. Financial Contribution Only (FCO) to the city of Wasco.	1F590	0623000052	201.378	Supplementary	\$80	\$0	\$10	\$90	2.0	Curb ramp(s)
2	07	LA	60	19.0/21.0	In the city of Rowland Heights, on Route 60 from Stoner Creek Undercrossing to E. Walnut Drive S. Restore landscape and bioswales.	1Q060	0722000165	201.335	Other Program Objectives	\$330	\$0	\$750	\$1,080	19.3	Acre(s) treated/pollutant
3	07	LA	10	20.6/26.9	In Los Angeles County, on Route 10 from 0.2 mile west of North Eastern Avenue Undercrossing to Rosemead Boulevard. Restore landscape and bioswales.	1Q070	0722000166	201.335	Other Program Objectives	\$330	\$0	\$400	\$730	2.4	Acre(s) treated/pollutant
4	07	LA	60	R29.5	In Pomona, on Route 60 near Garey Avenue. Rehabilitate existing drainage system.	1Q480	0723000058	201.151	Supplementary	\$200	\$0	\$300	\$500	1.0	Culvert(s) (ea)
5	07	VEN	33	0.0/57.508	In Ventura County, on Routes 23, 33, and 150 at various locations. Install/upgrade signs, striping, and pavement markings.	1Q760	0723000159	201.015	Other Program Objectives	\$460	\$0	\$180	\$640	1.0	Location(s)
6	07	LA	60	R22.2	In Diamond Bar, at westbound Lemon Avenue onramp. Construct drainage channel.	2Q040	0725000014	201.151	Supplementary	\$170	\$0	\$230	\$400	0.0	Culvert(s) (ea)
7	11	IMP	8	R13.3/R13.4	In Imperial County, near Ocotillo. Upgrade changeable message signs.	3A538	1122000160	201.315	TMS	\$388	\$0	\$155	\$543	1.0	Location(s)
Total										\$1,958	\$0	\$2,025	\$3,983		

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 3.11, Information Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **SECOND QUARTER – BALANCE REPORT ON AB 1012 “USE IT OR LOSE IT”
PROVISION FOR FEDERAL FISCAL YEAR 2023 UNOBLIGATED RSTP AND
CMAQ FUNDS**

SUMMARY:

As of March 31, 2025, the Regional Surface Transportation Program (RSTP) has approximately \$1.5 million, and the Congestion Mitigation and Air Quality Improvement Program (CMAQ) has approximately \$4.6 million subject to reprogramming on November 1, 2025.

BACKGROUND:

The Intermodal Surface Transportation Efficiency Act was enacted in 1991 and was in effect for six years. During that time, the Regions only obligated 87 percent of their Federal funding. The next Federal Highway Act, known as the Transportation Equity Act for the 21st Century (TEA-21), was signed into law in 1998. During the first two years of TEA-21, the Regions' obligation of Federal funds declined to 41 percent. By October 1999, the Regions had accumulated a \$1.2 billion backlog in Federal apportionments and \$854 million in Obligation Authority (OA).

Assembly Bill (AB) 1012 was enacted on October 10, 1999 (Chapter 783, Statutes of 1999), with a goal of improving the delivery of transportation projects and addressing the backlog of the Regions' federal apportionments and OA. AB 1012 states that RSTP and CMAQ funds not obligated within the first three years of Federal eligibility are subject to reprogramming by the California Transportation Commission in the fourth year to prevent the funds from being lost by the State.

The annual notice to the Regions, under AB 1012 “Use It or Lose It” provisions for Federal Fiscal Year 2023 (October 1, 2022, through September 30, 2023), was released on

December 16, 2024. The total 2023 funds identified as subject to reprogramming under the provisions of AB 1012 were approximately \$8.6 million. This included approximately \$3.1 million of RSTP funds and approximately \$5.5 million of CMAQ funds. As of March 31, 2025, the RSTP amount subject to reprogramming has decreased to \$1.5 million and the CMAQ amount has decreased to \$4.6 million.

The California Department of Transportation (Department) is responsible for monitoring and reporting unobligated balances. Each month, the Department provides notification to the Regions of the unobligated RSTP and CMAQ balances that have one year remaining under the AB 1012 guidelines.

Attachments

Apportionment Status Report
CMAQ and RSTP
as of March 31, 2025

Reference No.: 3.11
June 26-27, 2025
Attachment 1

AB 1012
Balances entering the 3rd Year
(from FFY 2023*)
Regional Report Summary

*Previously referred to as Cycle 26

Region	CMAQ Unobligated 03/31/2025 Delivery Balance ¹	CMAQ Amount Subject to AB 1012 Reprogramming 11/01/2025 ²	RSTP Unobligated 03/31/2025 Delivery Balance ¹	RSTP Amount Subject to AB 1012 Reprogramming 11/01/2025 ²
Butte	328,756	-	-	-
Fresno	25,393,484	-	31,891,841	210,591
Kern	25,052,134	311,348	22,006,781	-
Kings	5,909,753	720,327	-	-
Madera	10,313,043	-	1	-
Merced	6,977,609	-	(1)	-
Monterey ³	-	-	-	-
S. F. Bay Area (MTC)	80,479,684	-	107,438,633	-
Sacramento (SACOG)	19,619,381	-	32,983,890	-
San Benito ³	-	-	-	-
San Diego	16,598,621	-	47,028,163	-
San Joaquin	19,789,936	-	30,031,226	-
San Luis Obispo	8,182,790	-	-	-
Santa Barbara ³	-	-	6,201,093	-
Santa Cruz ³	-	-	-	-
Southern CA (SCAG)	447,371,382	-	521,328,057	-
Stanislaus	10,630,344	-	22,654,852	1,243,180
Tahoe	852,161	-	1,363,564	-
Tulare	8,352,578	-	-	-
Rural Counties	9,023,871	3,540,184	43,822	-
TOTAL	694,875,527	4,571,859	822,971,922	1,453,771

Footnotes:

Balances in the 3rd year (October 1, 2024) are subject to reprogramming on November 1, 2025. These balances include the FFY 2023 “Actual” Apportionments (dated November 3, 2023), the FFY 2024 “Actuals” dated (January 9, 2025), and the FFY 2025 “Estimated” Apportionments (dated January 9, 2025).

¹ Indicates all apportionments not yet obligated.

² Totals reflect balances in the 3rd year.

³ These Regions are in air quality attainment and cannot use unobligated CMAQ apportionments, which are deobligations of closed out projects. It is anticipated that any CMAQ balance that accumulates in a Region in air quality attainment will be included in a future CMAQ rescission or transferred to another Region that over-delivered prior to the end of the current FFY.

Apportionment Status Report
CMAQ and RSTP
as of March 31, 2025

Reference No.: 3.11
June 26-27, 2025
Attachment 2

AB 1012
Balances entering the 3rd Year
(from FFY 2023*)
Rural Report Summary

*Previously referred to as Cycle 26

Region	CMAQ Unobligated 03/31/2025 Delivery Balance ¹	CMAQ Amount Subject to AB 1012 Reprogramming 11/01/2025 ²	RSTP Unobligated 03/31/2025 Delivery Balance ¹	RSTP Amount Subject to AB 1012 Reprogramming 11/01/2025 ²
Rural County Information:				
Alpine	-	-	-	-
Amador ³	381,200	-	43,821	-
Calaveras	449,226	-	-	-
Colusa	-	-	-	-
Del Norte	-	-	-	-
El Dorado	-	-	-	-
Glenn	-	-	-	-
Humboldt	-	-	-	-
Inyo	-	-	-	-
Lake	-	-	-	-
Lassen	-	-	-	-
Mariposa	2,072,346	1,703,219	(3)	-
Mendocino	-	-	-	-
Modoc	-	-	-	-
Mono	-	-	-	-
Nevada	2,341,679	-	1	-
Placer	-	-	-	-
Plumas	-	-	-	-
Shasta	-	-	-	-
Sierra	-	-	-	-
Siskiyou	-	-	-	-
Tehama	2,698,937	1,836,965	-	-
Trinity	-	-	-	-
Tuolumne ³	1,080,483	-	-	-
Rural Combined Totals:	9,023,871	3,540,184	43,819	-

Footnotes:

Balances in the 3rd year (October 1, 2024) are subject to reprogramming on November 1, 2025. These balances include the FFY 2023 “Actual” Apportionments (dated November 3, 2023), the FFY 2024 “Actuals” dated (January 9, 2025), and the FFY 2025 “Estimated” Apportionments (dated January 9, 2025).

¹ Indicates all apportionments not yet obligated.

² Totals reflect balances in the 3rd year.

³ These Regions were previously in air quality attainment as of the beginning of FFY 2016 and did not receive any CMAQ funding. Due to the 2015 FFY Ozone factor, these Regions are back in non-attainment areas at the beginning of FFY 2019, and consequently receive FFY 2023 and future FFYs CMAQ funding.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 4.30, Information Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM –
PROGRAM ALLOCATION PLAN UPDATE**

SUMMARY:

The California Department of Transportation (Department) is presenting to the California Transportation Commission (Commission) the Program Allocation Plan Update for the California State Transportation Agency's (CalSTA) Transit and Intercity Rail Capital Program (TIRCP). This update includes the list of awarded projects, separated by components, with allocations and programming projections from award years 2016, 2018, 2020, 2022, 2023, 2024, and Assembly Bill (AB) 180.

BACKGROUND:

The TIRCP was created to fund transformative capital improvements that modernize California's intercity rail, bus (including feeder buses to intercity rail services, as well as vanpool services that are eligible to report as public transit to the Federal Transit Administration), ferry, and rail transit systems (collectively referred to as transit services or systems inclusive of all aforementioned modes unless otherwise specified) to achieve all of the following policy objectives, as established in Section 75220(a) of the Public Resources Code (PRC):

- Reduce emissions of greenhouse gases
- Expand and improve transit service to increase ridership
- Integrate the rail service of the state's various rail operations, including integration with the high-speed rail system
- Improve transit safety

Additionally, Section 75221(c) of the PRC establishes a programmatic goal to provide at least 25 percent of available funding to projects that provide direct, meaningful, and assured benefits to disadvantaged communities, consistent with the objectives of Chapter 830 of the Statutes of 2012. AB 1550 (Chapter 369, Statutes of 2016) provides additional requirements related to ensuring investments meet the needs of priority populations, a term encompassing disadvantaged communities, low-income communities, and low-income households. Investments made by TIRCP are expected to collectively meet or exceed the requirements in AB 1550.

AB 180 (Chapter 44, Schedule 1), Item 0521-131-0001 of Section 2.00, amended the Budget Act of 2021, appropriating funds to 0276-TIRCP. Of the funds appropriated in Schedule (1), \$300,000,000 was made available for project development activities related to rail realignment capital projects for high-priority intercity rail corridors located primarily in the coastal zone, as defined in subdivision (a) of Section 30103 of the PRC.

On January 31, 2023, CalSTA awarded \$2,537,695,000 to “Existing TIRCP Projects Leveraging Federal and Local Funds Reserve” throughout California. The projects awarded funding, received awards in previous TIRCP Cycles 2, 3, and 4. The additional State resources will help fill funding gaps to complete construction while protecting and leveraging significant Federal and local investments.

On April 24, 2023, CalSTA awarded \$690,254,000 in General Fund funding to 28 new public state-funded transportation projects to expand transit and passenger rail service throughout the state.

On July 6, 2023, CalSTA awarded \$350,000,000 in funding for the High Priority Grade Crossing Improvements and Separation Program through the Ports and Freight Infrastructure Program and the TIRCP. Pursuant to the Budget Act of 2024, \$200,000,000 in funding for the High Priority Grade Crossing Improvements and Separation Program was reverted. AB 173 (Chapter 53, Statutes of 2024) requires coordination by CalSTA with the Department and the Commission to identify available funding in state transportation programs that may be used to support grade separation projects that were previously awarded funding under specified transportation programs but had that funding reverted pursuant to the Budget Act of 2024. Updates to the TIRCP Program Allocation Plan and the affected high-priority grade crossing improvement projects will be forthcoming as this analysis continues.

On October 23, 2024, CalSTA awarded \$1,333,342,000 to 27 new public transportation projects to expand transit and passenger rail service throughout the state.

Programming Actions:

The Program Allocation Plan for the TIRCP is adjusted to address project or program needs and is used to plan and track the allocation of projects as reflected in the enclosed attachment.

Attachments A, B, C, D, E, F, G and H

Attachments A, B, C, D, E, F, G and H

4.30

Transit and Intercity Rail Capital Program (TIRCP) –
Program Update

MEETING MATERIALS FOR THIS ITEM WILL BE
PROVIDED PRIOR TO THE JUNE 26-27, 2025
CALIFORNIA TRANSPORTATION COMMISSION MEETING.

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 2.2c.(2) Action

Prepared By: Cherry Zamora
Associate Deputy Director

Published Date: June 13, 2025

Subject: Approval of Project for Future Consideration of Funding – Mitigated Negative Declaration for the Hatch Road and Sullivan Road Trail Project, Resolution E-25-29

Recommendation:

Staff recommends the California Transportation Commission (Commission), as a Responsible Agency, accept the Mitigated Negative Declaration for the Hatch Road and Sullivan Road Trail Project (Project) in the City of Twentynine Palms in San Bernardino County and approve the Project for future consideration of funding.

Issue:

The City of Twentynine Palms (City) is the California Environmental Quality Act (CEQA) lead agency for the Project. The Project is located on Hatch Road and Sullivan Road, between Twentynine Palms Highway to the north and Adobe Road to the east, in the City of Twentynine Palms in San Bernardino County. The Project includes new asphalt pavement to include construction of Class II bicycle lanes and a graded shoulder/pedestrian walkway for approximately 2.15 miles of the corridor. Existing paved roadways and walkways will be incorporated into the final design.

For all projects that are seeking funding through a program under the purview of the Commission, full compliance with CEQA is required. The Commission will not allocate funds to projects for design, right-of-way, or construction until the environmental document is complete, and the Commission has approved the environmentally cleared project for future funding consideration.

Background:

On April 21, 2025, the City adopted the Mitigated Negative Declaration and found that the Project would not have a significant impact on the environment with the implementation of mitigation measures. On May 12, 2025, the City confirmed that the environmental document remains valid, there are no newly identified impacts or substantial increase in the severity of an impact requiring mitigation, and the environmental document is consistent with the Project

scope of work programmed by the Commission. Impacts that require mitigation measures in order to be reduced to less than significant levels relate to biological resources, cultural resources, and tribal cultural resources.

Mitigation measures include construction worker training for avoidance of sensitive natural resources; pre-construction Burrowing Owl surveys; a pre-construction Desert Tortoise survey; a nesting bird survey; and construction monitoring by qualified archaeologists and Native American representatives.

The Project is estimated to cost \$3,480,000 and is fully funded through the construction phase with Active Transportation Program - State (\$3,480,000) funds.

Construction is estimated to begin in Fiscal Year 2026-27.

Attachments:

- Attachment A: Resolution E-25-29
- Attachment B: Notice of Determination
- Attachment C: Project Location Map

**CALIFORNIA TRANSPORTATION COMMISSION
Resolution for Future Consideration of Funding**

**8 – San Bernardino County
Resolution E-25-29**

- 1.1 WHEREAS, the City of Twentynine Palms (City) has completed a Mitigated Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the Hatch Road and Sullivan Road Trail Project (Project) in the City of Twentynine Palms in San Bernardino County; and
- 1.2 WHEREAS, the City has certified that the Mitigated Negative Declaration has been completed pursuant to CEQA and the CEQA Guidelines; and
- 1.3 WHEREAS, the Project is located on Hatch Road and Sullivan Road, between Twentynine Palms Highway to the north and Adobe Road to the east, in the City of Twentynine Palms in San Bernardino County; and
- 1.4 WHEREAS, the Project includes new asphalt pavement to include construction of Class II bicycle lanes and a graded shoulder/pedestrian walkway for approximately 2.15 miles of the corridor. Existing paved roadways and walkways will be incorporated into the final design; and
- 1.5 WHEREAS, the California Transportation Commission (Commission), as a Responsible Agency, has considered the information contained in the Mitigated Negative Declaration; and
- 1.6 WHEREAS, impacts that require mitigation measures in order to be reduced to less than significant levels relate to biological resources, cultural resources, and tribal cultural resources, and certain measures were adopted to address these impacts; and
- 1.7 WHEREAS, on April 21, 2025, the City adopted the Mitigated Negative Declaration and found that the Project would not have a significant effect on the environment with implementation of mitigation measures; and
- 1.8 WHEREAS, on May 12, 2025, the City confirmed that the Mitigated Negative Declaration remains valid, there are no newly identified impacts or substantial increase in the severity of an impact requiring mitigation, and the environmental document is consistent with the Project scope of work programmed by the Commission; and
- 1.9 WHEREAS, the Commission, as a Responsible Agency, has considered the information contained in the Mitigated Negative Declaration.
- 2.1 NOW, THEREFORE, BE IT RESOLVED that the Commission does hereby accept the Mitigated Negative Declaration and approves the above-referenced Project for future consideration of funding.

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: Hatch Road and Sullivan Road Trail Project

2025030675

Richard Pedersen

(760) 801-7615

State Clearinghouse Number

Lead Agency Contact Person

Area Code/Telephone

Project Location (include county): The project is located on Hatch Road and Sullivan Road, between Twentynine Palms Highway to the north and Adobe Road to the east, in the City of Twentynine Palms in San Bernardino County.

Project Description: The project includes new asphalt pavement to include construction of Class II bicycle lanes and a graded shoulder/pedestrian walkway for approximately 2.15 miles of the corridor. Existing paved roadways and walkways will be incorporated into the final design.

This is to advise that the California Transportation Commission has approved the above-described
(☐ Lead Agency / ☒ Responsible Agency)
project on June 26-27, 2025, and has made the following determinations regarding the above-described project:

1. The project (☐ will / ☒ will not) have a significant effect on the environment.
2. ☐ A Final Environmental Impact Report was prepared for this project pursuant to the provisions of CEQA.
☒ A Mitigated Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (☒ were / ☐ were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (☒ was / ☐ was not) adopted for this project.
5. A Statement of Overriding Considerations (☐ was / ☒ was not) adopted for this project.
6. Findings (☐ were/ ☒ were not) made pursuant to the provisions of CEQA.

The above-identified document with comments and responses and record of project approval is available to the General Public at: Twentynine Palms City Hall, 6136 Adobe Road, Twentynine Palms, CA 92277.

TANISHA TAYLOR

Executive Director
California Transportation Commission

Signature (Public Agency)

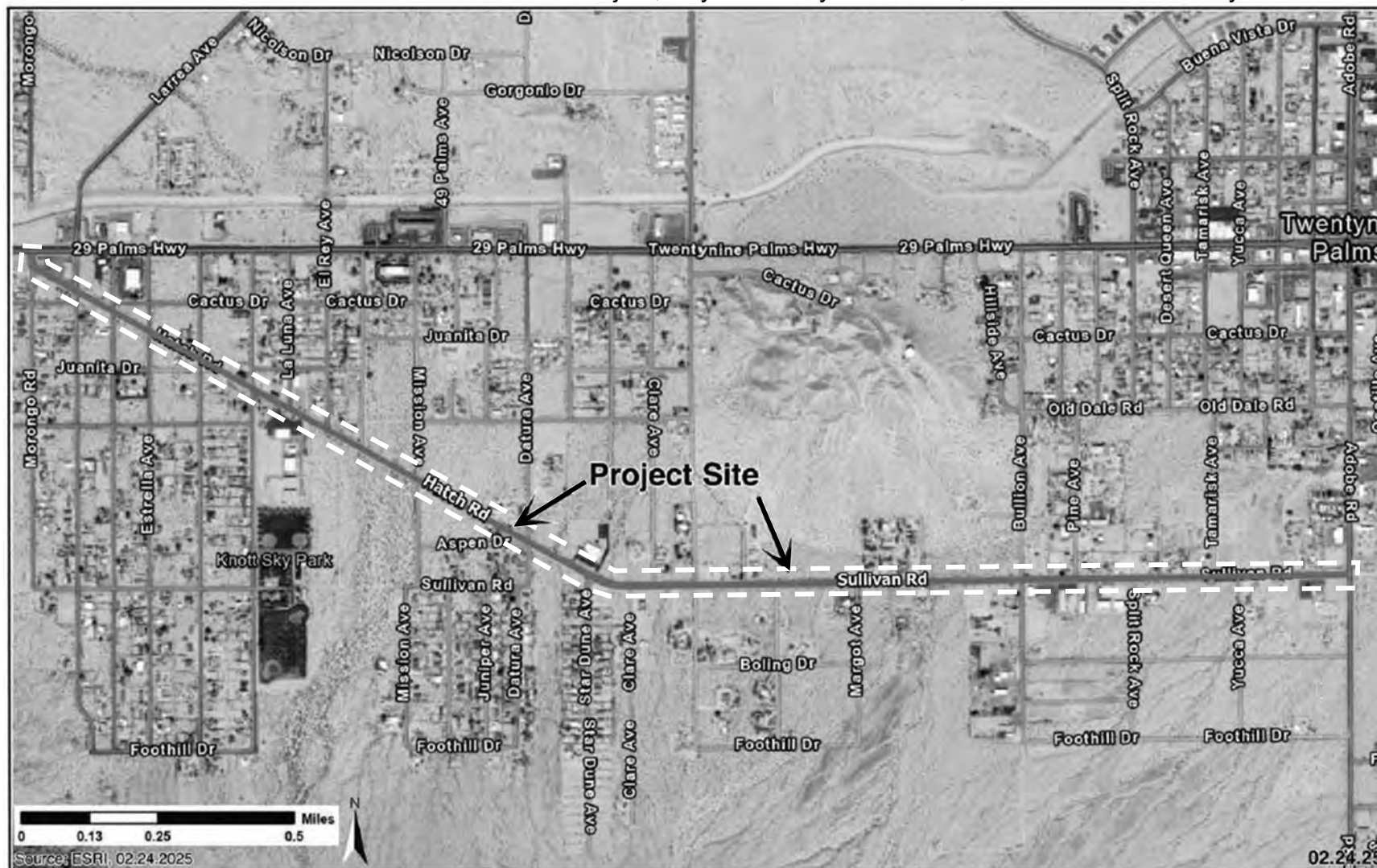
Date

Title

Date received for filing at OPR:

Project Location Map

Hatch Road and Sullivan Road Trail Project, City of Twentynine Palms, San Bernardino County



MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5s.(8), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **ALLOCATION AMENDMENT FOR A STATE-ADMINISTERED SENATE BILL 1
LOCAL PARTNERSHIP PROGRAM (FORMULAIC) PROJECT, ON THE STATE
HIGHWAY SYSTEM
RESOLUTION LPP-A-2425-37, AMENDING RESOLUTION LPP-A-2021-01**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for the State-Administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) Route 94/125 Connector project (PPNO 0356), on the State Highway System, in San Diego County, to deallocate \$147,000 in Right of Way (RW) Support and \$3,000,000 in RW Capital due to the funds lapsing?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation amendment for the State-Administered SB 1 LPP (Formulaic) Route 94/125 Connector project (PPNO 0356), on the State Highway System, in San Diego County, to deallocate \$147,000 in RW Support and \$3,000,000 in RW Capital due to the funds lapsing.

BACKGROUND:

In August 2020, the Commission approved an allocation of \$1,000,000 in RW Support and \$3,000,000 in RW Capital for the State-Administered SB 1 LPP (Formulaic) Route 94/125 Connector project (PPNO 0356), under Resolution LPP-A-2021-01, with the requirement to expend these funds by June 30, 2023. In May 2023, the Commission granted a 12-month extension for project development expenditures under Waiver 23-51, which extended the deadline to spend these funds to June 30, 2024. The Department was unable to spend all of

these funds by the extended deadline. As a result, these funds have lapsed and need to be deallocated.

The proposed changes are reflected in strikethrough and bold in the attached revised vote box. There is a concurrent State Transportation Improvement Program amendment on this month's Commission agenda.

FINANCIAL RESOLUTION:

Be it Resolved, that the SB 1 LPP (Formulaic) Route 94/125 Connector project (PPNO 0356) is hereby amended to deallocate \$147,000 in RW Support and \$3,000,000 in RW Capital due to the funds lapsing, in accordance with the attached revised vote box.

Attachment

2.5 Highway Financial Matters

Project #	PPNO			
Allocation Amount	Program/Year			
Recipient	Phase			
RTPA/CTC	Prgm'd Amount	Budget Year		
County	Project ID	Item #		
Dist-Co-Rte	Advantage Phase	Fund Type		Amount by
Postmile	EA	Program Code		Fund Type

2.5s.(8) Allocation Amendment - State-Administered LPP (Formulaic) Project**Resolution LPP-A-2425-37**
Amending Resolution LPP-A-2021-01

1	Route 94/125 Connector. In Lemon Grove at Route 94/125 Interchange.		11-0356		
\$4,000,000	Construct southbound Route 125 to eastbound Route 94 freeway connector		LPP-F/20-21	505-3290 RMRA	\$1,000,000
\$853,000	ramp.		R/W Sup	20.10.724.000	\$853,000
			\$1,000,000		
Department of	<u>Outcome/Outputs:</u>	<u>Unit</u>	<u>Total</u>	2017-18	
Transportation	Operational improvement(s)	Each	1	801-3290 RMRA	\$3,000,000
<u>SANDAG</u>				20.20.724.000	\$0
San Diego	CEQA - ND, 06/11/2015				
11-SD-94	NEPA - FONSI, 06/11/2015				
10.0/11.8					
	Future consideration of funding approved under		1100000078		
	Resolution E-16-58; August 2016.		2, 9		
			14665		
	Related LPP Program Amendment approved under				
	Resolution LPP-P-2021-02; August 2020.				
	Contribution from other sources: \$7,946,000				
	Concurrent STIP Amendment under 24S-03; June 2025.				
	<u>Amend Resolution LPP-A-2021-01 to deallocate \$147,000 in RW</u>				
	<u>Support and \$3,000,000 in RW Capital due to the funds lapsing.</u>				

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.1a.(3), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **STIP AMENDMENT 24S-03 (ACTION)**
PPNO 3546 – CALAVERAS COUNTY
PPNO 0294E – CONTRA COSTA COUNTY
PPNO 0299A – CONTRA COSTA COUNTY
PPNO 0521L – CONTRA COSTA COUNTY
PPNO 2025H – CONTRA COSTA COUNTY
PPNO 1483B – NAPA COUNTY
PPNO 0658M – SAN MATEO COUNTY
PPNO 2091L – SANTA CLARA COUNTY
PPNO 6288 – FRESNO COUNTY
PPNO 0356 – SAN DIEGO COUNTY

ACTION UPDATE:

- *Update Amendment Box: Changing Construction Year to 2028-29 for the SR 4 Operational Improvements – Phase 1 Project (PPNO 0299A).*
- *Update Amendment Box: Changing the implementing agency for the CON phase from the Department to the City of Napa, also Right of Way Support and Construction Support were moved to Right of Way Capital and Construction Capital for the Silverado Trail Five-Way Intersection Improvements project (PPNO 1483B).*
- *Update Amendment Box: SR 94/125 Auxiliary Lanes (PPNO 0356) updating the LPP-F funds from \$2,978,000 to \$3,000,000 in Right of Way and \$43,000 to \$147,000 in Right of Way Support.*

ISSUE:

Should the California Transportation Commission (Commission) approve State Transportation Improvement Program (STIP) Amendment 24S-03?

RECOMMENDATION:

The California Department of Transportation (Department), Calaveras County, the Metropolitan Transportation Commission (MTC), Fresno County, and the San Diego Association of Governments (SANDAG) requests that the Commission approve STIP Amendment 24S-03. This item was noticed at the Commission's May 2025 meeting.

The Department, Calaveras County, MTC, Fresno County, and SANDAG propose to amend the 2024 STIP to address the programming proposal changes shown in the background section.

BACKGROUND:

The SR 4 Wagon Trail Realignment (Eastern Segment) project (PPNO 3546) is currently programmed in the 2024 STIP with \$1.042 million in Regional Improvement Program (RIP) funds for the Construction (CON) phase in Fiscal Year 2027-28. The Calaveras Council of Governments is requesting an amendment to the 2024 STIP to reprogram the \$1.042 million from CON programmed from 2027-28 to the Right of Way (RW) phase in 2025-26. The project has also received Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) funding for the RW phase in 2024-25. This project received an allocation time extension for the TCEP funds through June 30, 2026 at the May 2025 Commission meeting, and proposes to use STIP funds as the match source. This amendment is necessary to align the STIP funding schedule with the updated TCEP timeline.

The Treat Boulevard Corridor Improvements project (PPNO 0294E) is currently programmed in the 2024 STIP with \$1,600,000 in RIP funds for the CON phase in 2025-26. However, due to significant delays encountered during the engineering review, the project team requires additional time to complete more technical studies as a part of the National Environmental Policy Act clearance process. Additional time is also needed to follow the Project Delivery Quality Management Assessment Process, which involves extensive review and increased coordination. In order to utilize these STIP funds in a timely manner, Contra Costa County Transportation Authority (CCTA) requests to reprogram these funds from 2025-26 to 2026-27.

The SR 4 Operational Improvements – Phase 1 project (PPNO 0299A) is currently programmed in the 2024 STIP with \$7,500,000 in RIP funds for the Plans, Specifications, and Estimate (PS&E) phase in 2025-26. However, additional time is needed to comply with SB 743, which requires agencies to evaluate a project's environmental impacts using Vehicle Miles Traveled under the California Environmental Quality Act (CEQA). This process necessitates increased coordination. In order to utilize these STIP funds in a timely manner, CCTA requests to reprogram these funds from 2025-26 to 2026-27.

The Innovate 680 Coordinated Adaptive Ramp Metering (CARM) project (PPNO 0521L) is currently programmed in the 2024 STIP with \$3,200,000 in RIP funds for the CON Support phase and \$21,800,000 in RIP funds for the CON phase in 2025-26. However, as result of

pre-construction phases, and to streamline the project delivery process in the CON phase, CCTA requests that the \$3,200,000 in CON Support funds be reprogrammed to the CON phase in 2025-26. CCTA also requests updating the project sponsor from the Department to CCTA, augmenting the project scope to extend the project limits, and requesting state-only funds for \$15,000,000 for the future CON phase allocation.

The I-80/Central Avenue – Phase 2 (Local Road Realignment) project (PPNO 2025H) is currently programmed in the 2024 STIP with \$1,873,000 in RIP funds for the CON phase in 2025-26. However, due to additional time needed for Pacific Gas & Electric Company (PG&E) utility undergrounding work and ongoing negotiations related to condemnation proceedings, construction cannot begin as scheduled. Therefore, CCTA requests a 1-year delay to coordinate with PG&E before entering the CON phase, with reprogramming the phase from 2025-26 to 2026-27.

The Silverado Trail Five-Way Intersection Improvements project (PPNO 1483B) is currently programmed in the 2024 STIP with \$1,153,000 in RIP funds for the RW Capital phase in 2025-26. However, additional time is needed to account for delays during the environmental phase. Specifically, due to the project's impacts on the Napa Historic District, which now requires a more in-depth Environmental Impact Report/Environmental Assessment, the Napa Valley Transportation Authority (NVTa) requests a 1-year delay to work with the Department and project partners to complete the environmental phase before beginning the RW phase. NVTa requests reprogramming the phase from 2025-26 to 2026-27.

The US 101 Managed Lanes Project, North of Interstate 380 (I-380 to SM/SF County Line) project (PPNO 0658M) is currently programmed in the 2024 STIP with \$1,700,000 in RIP funds for the RW Capital phase in 2025-26. However, additional time is needed to account for environmental document review delays during the environmental phase. The City/County Association of Governments of San Mateo County (C/CAG) requests a 2-year delay to work with the Department and project stakeholders to complete the environmental phase before beginning the RW phase. C/CAG requests reprogramming the phase from 2025-26 to 2027-28.

The Central Bikeway project (PPNO 2091L) is currently programmed in the 2024 STIP with \$8,500,000 in RIP funds for the Project Approval and Environmental Document (PA&ED) phase in 2024-25, and \$22,238,000 in RIP funds for the PS&E phase in 2027-28. However, costs have increased in the PA&ED phase due to two main reasons. First, the cost estimates used for programming were generated in early 2023 and have since been impacted by a high level of inflation. Second, Santa Clara Valley Transportation Authority (VTA) needs to increase the level of design and public outreach. As a result, Santa Clara VTA is advancing some of the engineering work originally planned for the PS&E phase into the PA&ED phase to reduce the risk of scope changes during the PS&E phase. Santa Clara VTA requests to deprogram \$5,500,000 from the PS&E phase from 2027-28. This adjustment will allow the environmental work to proceed without delay while keeping the PS&E phase fully funded. As a result, Santa

Clara VTA requests deprogramming to accommodate for the environmental phase allocation in June 2025.

The South Fresno SR 99 Corridor Project: North Avenue Interchange (PPNO 6288) is currently programmed in the 2024 STIP with \$42,346,000 in RIP funds for the CON phase in 2025-26. The project will construct an interchange on SR 99 at North Avenue. However, an ongoing CEQA-related lawsuit has created significant uncertainty. Although the Department initially prevailed, the appellate court remanded the case, delaying resolution and key pre-construction activities such as coordination with the Railroad and PG&E for RW Certification. Due to these delays, the Department recommends reprogramming the CON phase funds from 2025-26 to 2027-28 to allow time for legal resolution and completion of critical coordination efforts.

The SR 94/125 Auxiliary Lanes project (PPNO 0356) is currently programmed in the 2024 STIP with \$85,000,000 in RIP funds for the CON phase in 2026-27. Initially, the project intended to build auxiliary lanes on SR 94 and SR 125 in addition to a connector. However, high contaminated groundwater levels were encountered at the planned connector's location, which would require continuous dewatering and treatment before discharging. Therefore, a project change request was processed in March 2025 to down-scope the project by removing the Connector work, which will now be delivered under PPNO 1541, and moving forward with the auxiliary lanes project scope. As a result, the SR 94/125 Auxiliary Lanes project will be ready to start construction earlier, and the Department recommends reprogramming the funds from the CON phase from 2026-27 to 2025-26.

The proposed funding plan for these projects is shown on the following tables:

REVISE: SR 4 Wagon Trail Realignment Project (Eastern Segment) (PPNO 3546)

County		District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor				
Calaveras County		10	3546	1Q290	CO	2027-28	16.300	18.769	4				
Implementing Agency: (by component)		PA&ED		Calaveras County			PS&E		Calaveras County				
		R/W		Calaveras County			CON		Calaveras County				
RTPA/CTC:		Calaveras County											
Project Title:		SR 4 Wagon Trail Realignment Project (Eastern Segment)											
Location:		The project is located on State Route 4 between the town of Copperopolis and the City of Angels Camp in Calaveras County.											
Description:		The Eastern Segment of the SR 4 Wagon Trail Realignment Project consists of constructing SR 4 on a new alignment between Stallion Way and approximately 1.6 miles west of the State Route 4/49 Junction near Stockton Road.											
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	5,542	4,500	0	0	0	1,042	0	0	1,042	0	4,500	0	0
Change	0	0	0	1,042	0	(1,042)	0	1,042	(1,042)	0	0	0	0
Proposed	5,542	4,500	0	1,042	0	0	0	1,042	0	0	4,500	0	0
State SBI TCEP													
Existing	5,250	0	5,250	0	0	0	0	5,250	0	0	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	5,250	0	5,250	0	0	0	0	5,250	0	0	0	0	0
Local Funds													
Existing	2,250	0	2,250	0	0	0	0	2,250	0	0	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	2,250	0	2,250	0	0	0	0	2,250	0	0	0	0	0
Other Fed													
Existing	42,958	0	0	0	0	42,958	0	0	42,958	0	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	42,958	0	0	0	0	42,958	0	0	42,958	0	0	0	0
Total													
Existing	56,000	4,500	7,500	0	0	44,000	0	7,500	44,000	0	4,500	0	0
Change	0	0	0	1,042	0	(1,042)	0	1,042	(1,042)	0	0	0	0
Proposed	56,000	4,500	7,500	1,042	0	42,958	0	8,542	42,958	0	4,500	0	0

REVISE: Treat Boulevard Corridor Improvements project (PPNO 0294E)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Contra Costa County	04	0294E		LA	2026-27								
Implementing Agency: (by component)	PA&ED	Contra Costa County				PS&E	Contra Costa County						
	R/W	Contra Costa County				CON	Contra Costa County						
RTPA/CTC:	Metropolitan Transportation Commission												
Project Title:	Treat Boulevard Corridor Improvements												
Location:	In unincorporated Walnut Creek and portion of the City of Walnut Creek, on Treat Boulevard, from North Main Street to Jones Road.												
Description:	Construct Class IV protected bicycle lanes and pedestrian enhancements along the Treat Boulevard Corridor, including separated bikeways, a new shared use path, enhancing existing refuge islands and crosswalks, closing free right-turn lanes, and improving traffic signal hardware and timing.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	1,600	0	0	1,600	0	0	0	0	1,600	0	0	0	0
Change	0	0	0	(1,600)	1,600	0	0	0	0	0	0	0	0
Proposed	1,600	0	0	0	1,600	0	0	0	1,600	0	0	0	0
Local Funds													
Existing	1,019	419	0	600	0	0	0	0	600	194	225	0	0
Change	3,464	(103)	1,201	(600)	2,966	0	0	100	2,366	122	876	0	0
Proposed	4,483	316	1,201	0	2,966	0	0	100	2,966	316	1,101	0	0
Total													
Existing	2,619	419	0	2,200	0	0	0	0	2,200	194	225	0	0
Change	3,464	(103)	1,201	(2,200)	4,566	0	0	100	2,366	122	876	0	0
Proposed	6,083	316	1,201	0	4,566	0	0	100	4,566	316	1,101	0	0

REVISE: SR 4 Operational Improvements – Phase 1 project (PPNO 0299A)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Contra Costa County	04	0299A	1J031	CO	2026-27 2028-29	15.500	18.500	4					
Implementing Agency: (by component)	PA&ED	Contra Costa Transportation Authority (CCTA)				PS&E	Contra Costa Transportation Authority (CCTA)						
	R/W	Contra Costa Transportation Authority (CCTA)				CON	Contra Costa Transportation Authority (CCTA)						
RTPA/CTC:	Metropolitan Transportation Commission												
Project Title:	SR4 Operational Improvements - Phase 1												
Location:	In the cities of Concord and Pittsburg.												
Description:	Construct various operational improvements in the eastbound direction between Port Chicago Interchange and San Marco Boulevard interchange.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	7,500	0	0	7,500	0	0	0	0	0	0	7,500	0	0
Change	0	0	0	(7,500)	7,500	0	0	0	0	0	0	0	0
Proposed	7,500	0	0	0	7,500	0	0	0	0	0	7,500	0	0
Local Funds													
Existing	2,200	2,200	0	0	0	0	0	0	0	2,200	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	2,200	2,200	0	0	0	0	0	0	0	2,200	0	0	0
Future Need													
Existing	61,800	0	0	0	61,800	0	0	225	61,575	0	0	0	0
Change	0	0	0	0	(61,800)	225	61,575	0	0	0	0	0	0
Proposed	61,800	0	0	0	0	225	61,575	225	61,575	0	0	0	0
Total													
Existing	71,500	2,200	0	7,500	61,800	0	0	225	61,575	2,200	7,500	0	0
Change	0	0	0	(7,500)	(54,300)	225	61,575	0	0	0	0	0	0
Proposed	71,500	2,200	0	0	7,500	225	61,575	225	61,575	2,200	7,500	0	0

REVISE: Innovate 680 Coordinated Adaptive Ramp Metering (CARM) Project (PPNO 0521L)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Contra Costa	04	0521L	0Q960	CO	2025-26	0.000	19.800	680					
Implementing Agency: (by component)	PA&ED	Contra Costa Transportation Authority (CCTA)				PS&E	Contra Costa Transportation Authority (CCTA)						
	R/W	Contra Costa Transportation Authority (CCTA)				CON	Contra Costa Transportation Authority (CCTA)						
RTPA/CTC:	Metropolitan Transportation Commission												
Project Title:	Innovate 680 Coordinated Adaptive Ramp Metering (CARM) Project												
Location:	In Contra Costa County, on I-680 from south of the Alcosta Blvd Interchange to north of the Willow Pass Rd Interchange.												
Description:	Construct ramp modification & metering in the northbound direction at selected interchanges, and ITS and traffic detection improvements in the median and northbound lanes.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	25,000	0	0	25,000	0	0	0	0	21,800	0	0	0	3,200
Change	0	0	0	0	0	0	0	0	3,200	0	0	0	(3,200)
Proposed	25,000	0	0	25,000	0	0	0	0	25,000	0	0	0	0
Other State													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	9,970	0	0	9,970	0	0	0	0	9,970	0	0	0	0
Proposed	9,970	0	0	9,970	0	0	0	0	9,970	0	0	0	0
Local Funds - Locally Generated Funds													
Existing	5,721	2,450	0	3,271	0	0	0	0	3,271	0	2,450	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	5,721	2,450	0	3,271	0	0	0	0	3,271	0	2,450	0	0
Local Funds - Locally Generated Funds													
Existing	1,580	1,580	0	0	0	0	0	0	0	1,580	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	1,580	1,580	0	0	0	0	0	0	0	1,580	0	0	0
Other Fed													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	49,780	0	0	49,780	0	0	0	0	49,780	0	0	0	0
Proposed	49,780	0	0	49,780	0	0	0	0	49,780	0	0	0	0
Total													
Existing	32,301	4,030	0	28,271	0	0	0	0	25,071	1,580	2,450	0	3,200
Change	59,750	0	0	59,750	0	0	0	0	62,950	0	0	0	(3,200)
Proposed	92,051	4,030	0	88,021	0	0	0	0	88,021	1,580	2,450	0	0

REVISE: I-80/Central Avenue – Phase 2 (Local Road Realignment) project (PPNO 2025H)

County		District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor				
Contra Costa County		04	2025H		LA	2026-27							
Implementing Agency: (by component)		PA&ED	Richmond, City of				PS&E		Richmond, City of				
		R/W	Richmond, City of				CON		Richmond, City of				
RTPA/CTC:		Metropolitan Transportation Commission											
Project Title:		I-80/Central Avenue - Phase 2 (Local Road Realignment)											
Location:		In Richmond/El Cerrito, On Central Avenue from east of I-80 to San Mateo Street, On San Mateo Street from Central Avenue to Pierce Street, On Pierce Street from Central Avenue to San Mateo Avenue, and the extension of San Mateo Street to Pierce Street.											
Description:		Realign and reconfigure roadways.											
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	6,373	4,500	0	1,873	0	0	0	4,500	1,873	0	0	0	0
Change	0	0	0	(1,873)	1,873	0	0	0	0	0	0	0	0
Proposed	6,373	4,500	0	0	1,873	0	0	4,500	1,873	0	0	0	0
RSTP													
Existing	2,800	0	0	2,800	0	0	0	0	2,800	0	0	0	0
Change	0	0	0	(2,800)	2,800	0	0	0	0	0	0	0	0
Proposed	2,800	0	0	0	2,800	0	0	0	2,800	0	0	0	0
Local Funds - Locally Generated Funds													
Existing	1,800	1,100	0	700	0	0	0	1,100	700	0	0	0	0
Change	0	0	0	(700)	700	0	0	0	0	0	0	0	0
Proposed	1,800	1,100	0	0	700	0	0	1,100	700	0	0	0	0
Local Funds - Locally Generated Funds													
Existing	2,700	2,500	0	200	0	0	0	0	200	500	2,000	0	0
Change	0	0	0	(200)	200	0	0	0	0	0	0	0	0
Proposed	2,700	2,500	0	0	200	0	0	0	200	500	2,000	0	0
Total													
Existing	13,673	8,100	0	5,573	0	0	0	5,600	5,573	500	2,000	0	0
Change	0	0	0	(5,573)	5,573	0	0	0	0	0	0	0	0
Proposed	13,673	8,100	0	0	5,573	0	0	5,600	5,573	500	2,000	0	0

REVISE: Silverado Trail Five-Way Intersection Improvements project (PPNO 1483B)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Napa County	04	1483B	0J890	CO	2025-26	7.200	7.400	121					
Implementing Agency: (by component)	PA&ED	Napa, City of				PS&E	Napa, City of						
	R/W	Napa, City of				CON	Caltrans Napa, City of						
RTPA/CTC:	Metropolitan Transportation Commission												
Project Title:	Silverado Trail Five-Way Intersection Improvements												
Location:	In the city of Napa along SR 121 at the intersection of Silverado Trail (SR 121), East Ave., Third Street, Coombsville Rd.												
Description:	Construct a roundabout(s) improvement with better multimodal access.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	1,153	0	0	1,153	0	0	0	1,153	0	0	0	0	0
Change	0	0	0	(1,153)	1,153	0	0	0	0	0	0	0	0
Proposed	1,153	0	0	0	1,153	0	0	1,153	0	0	0	0	0
Other State													
Existing	3,900	1,080	0	2,820	0	0	0	490	2,480	420	510	0	0
Change	0	(660)	0	(2,820)	0	3,480	0	(490)	1,000	0	(510)	0	0
Proposed	3,900	420	0	0	0	3,480	0	0	3,480	420	0	0	0
RSTP													
Existing	2,000	0	0	2,000	0	0	0	0	2,000	0	0	0	0
Change	0	0	0	(2,000)	2,000	0	0	2,000	(2,000)	0	0	0	0
Proposed	2,000	0	0	0	2,000	0	0	2,000	0	0	0	0	0
Local Funds - Locally Generated Funds													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	8,208	3,098	500	2,910	1,700	0	0	600	0	4,098	3,510	0	0
Proposed	8,208	3,098	500	2,910	1,700	0	0	600	0	4,098	3,510	0	0
Local Funds - Locally Generated Funds													
Existing	6,098	3,098	2,000	1,000	0	0	0	1,000	0	3,098	2,000	0	0
Change	(6,098)	(3,098)	(2,000)	(1,000)	0	0	0	(1,000)	0	(3,098)	(2,000)	0	0
Proposed	0	0	0	0	0	0	0	0	0	0	0	0	0
Local Funds - Locally Generated Funds													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	19,680	0	0	0	1,847	17,833	0	1,847	17,833	0	0	0	0
Proposed	19,680	0	0	0	1,847	17,833	0	1,847	17,833	0	0	0	0
Total													
Existing	13,151	4,178	2,000	6,973	0	0	0	2,643	4,480	3,518	2,510	0	0
Change	21,790	(660)	(1,500)	(4,063)	6,700	21,313	0	2,957	16,833	1,000	1,000	0	0
Proposed	34,941	3,518	500	2,910	6,700	21,313	0	5,600	21,313	4,518	3,510	0	0

REVISE: US 101 Managed Lanes Project, North of Interstate 380 (I-380 to SM/SF County Line) project (PPNO 0658M)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
San Mateo County	04	0658M	0W150	CO	2027-28	19.200	26.100	101					
Implementing Agency: (by component)	PA&ED	San Mateo City/County Association of Governments				PS&E	San Mateo City/County Association of Governments						
	R/W	Caltrans				CON	Caltrans						
RTPA/CTC:	Metropolitan Transportation Commission												
Project Title:	US 101 Managed Lanes Project, North of Interstate 380. (I-380 to SM/SF County Line)												
Location:	In the cities of San Bruno, South San Francisco, and Brisbane.												
Description:	Construct managed lanes In the northbound and southbound directions of United States Highway 101 (US 101) from 1 mile south of the US 101/Interstate 380 (I-380) Interchange in San Mateo County to the San Mateo/San Francisco County Line.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	7,177	5,477	0	1,700	0	0	0	1,700	0	0	5,477	0	0
Change	0	0	0	(1,700)	0	1,700	0	0	0	0	0	0	0
Proposed	7,177	5,477	0	0	0	1,700	0	1,700	0	0	5,477	0	0
Local Funds													
Existing	181,533	10,150	30,673	0	0	140,710	0	0	140,710	10,150	30,673	0	0
Change	58,300	0	(30,673)	0	0	(51,737)	140,710	58,300	0	0	0	0	0
Proposed	239,833	10,150	0	0	0	88,973	140,710	58,300	140,710	10,150	30,673	0	0
Future Need													
Existing	100,000	0	0	0	0	100,000	0	0	100,000	0	0	0	0
Change	28,970	0	0	0	0	(100,000)	128,970	0	28,970	0	0	0	0
Proposed	128,970	0	0	0	0	0	128,970	0	128,970	0	0	0	0
Total													
Existing	288,710	15,627	30,673	1,700	0	240,710	0	1,700	240,710	10,150	36,150	0	0
Change	87,270	0	(30,673)	(1,700)	0	(150,037)	269,680	58,300	28,970	0	0	0	0
Proposed	375,980	15,627	0	0	0	90,673	269,680	60,000	269,680	10,150	36,150	0	0

REVISE: Central Bikeway project (PPNO 2091L)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Santa Clara County	04	2091L	3X340	CO				82					
Implementing Agency: (by component)	PA&ED	Santa Clara Valley Transportation Authority				PS&E	Santa Clara Valley Transportation Authority						
	R/W	Santa Clara Valley Transportation Authority				CON	Santa Clara Valley Transportation Authority						
RTPA/CTC:	Metropolitan Transportation Commission												
Project Title:	Central Bikeway												
Location:	In the cities of San Jose and Santa Clara.												
Description:	The Central Bikeway project will construct a Class IV physically separated bikeway. Its alignment will start at the intersection of El Camino Real (SR 82) and Lawrence Expressway, and run along El Camino Real, The Alameda, Hedding Street, North 10th Street/North 11th Street couplet, Taylor Street, and Mabury Road.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	30,738	0	8,500	0	0	22,238	0	0	0	8,500	22,238	0	0
Change	(5,500)	0	0	0	0	(5,500)	0	0	0	0	(5,500)	0	0
Proposed	25,238	0	8,500	0	0	16,738	0	0	0	8,500	16,738	0	0
Total													
Existing	30,738	0	8,500	0	0	22,238	0	0	0	8,500	22,238	0	0
Change	(5,500)	0	0	0	0	(5,500)	0	0	0	0	(5,500)	0	0
Proposed	25,238	0	8,500	0	0	16,738	0	0	0	8,500	16,738	0	0

REVISE: South Fresno SR 99 Corridor Project: North Avenue Interchange project (PPNO 6288)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Fresno County	06	6288	0H240	CO	2027-28	15.800	17.600	99					
Implementing Agency: (by component)	PA&ED	Caltrans				PS&E	Caltrans						
	R/W	Caltrans				CON	Caltrans						
RTPA/CTC:	Council of Fresno County Governments												
Project Title:	South Fresno SR 99 Corridor Project: North Avenue Interchange												
Location:	In Fresno County, in Fresno, from Central Avenue Overcrossing to 0.4 mile north of North Avenue.												
Description:	Modify interchange.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	45,346	3,000	0	42,346	0	0	0	0	33,446	3,000	0	0	8,900
Change	0	0	0	(42,346)	0	42,346	0	0	0	0	0	0	0
Proposed	45,346	3,000	0	0	0	42,346	0	0	33,446	3,000	0	0	8,900
Local Funds - Locally Generated Funds													
Existing	31,800	16,100	0	15,700	0	0	0	8,100	15,700	0	5,800	2,200	0
Change	0	0	0	(15,700)	0	15,700	0	0	0	0	0	0	0
Proposed	31,800	16,100	0	0	0	15,700	0	8,100	15,700	0	5,800	2,200	0
Local Funds - Locally Generated Funds													
Existing	35,154	700	0	34,454	0	0	0	0	34,454	0	300	400	0
Change	0	0	0	(34,454)	0	34,454	0	0	0	0	0	0	0
Proposed	35,154	700	0	0	0	34,454	0	0	34,454	0	300	400	0
Total													
Existing	112,300	19,800	0	92,500	0	0	0	8,100	83,600	3,000	6,100	2,600	8,900
Change	0	0	0	(92,500)	0	92,500	0	0	0	0	0	0	0
Proposed	112,300	19,800	0	0	0	92,500	0	8,100	83,600	3,000	6,100	2,600	8,900

REVISE: SR 94/125 Auxiliary Lanes project (PPNO 0356)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
San Diego County	11	0356	14665	CO	2025-26	13.200	R14.900	125					
Implementing Agency: (by component)	PA&ED	Caltrans				PS&E	Caltrans						
	R/W	Caltrans				CON	Caltrans						
RTPA/CTC:	San Diego Association of Governments												
Project Title:	SR 94/125 Auxiliary Lanes												
Location:	In San Diego County in and near La Mesa on State Route 94 from Spring Street undercrossing to 0.1 mile east of Kenwood Drive undercrossing and on State Route 125 from 0.2 mile south of Spring Street undercrossing to 0.2 mile north of Lemon Avenue undercrossing.												
Description:	Construct NB SR-125, SB SR-125, and EB SR-94 auxiliary lanes.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	98,948	13,948	0	0	85,000	0	0	0	71,000	0	13,948	0	14,000
Change	0	0	0	85,000	(85,000)	0	0	0	0	0	0	0	0
Proposed	98,948	13,948	0	85,000	0	0	0	0	71,000	0	13,948	0	14,000
TCRP (Committed)													
Existing	6,352	6,352	0	0	0	0	0	1,352	0	5,000	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	6,352	6,352	0	0	0	0	0	1,352	0	5,000	0	0	0
State SB1 LPP													
Existing	4,000	4,000	0	0	0	0	0	3,000	0	0	0	1,000	0
Change	(3,147)	(3,147)	0	0	0	0	0	(3,000)	0	0	0	(147)	0
Proposed	853	853	0	0	0	0	0	0	0	0	0	853	0
RSTP													
Existing	6,000	6,000	0	0	0	0	0	3,587	0	0	1,673	740	0
Change	0	0	0	0	0	0	0	(2,000)	0	0	1,700	300	0
Proposed	6,000	6,000	0	0	0	0	0	1,587	0	0	3,373	1,040	0
Local Funds - Locally Generated Funds													
Existing	26	26	0	0	0	0	0	26	0	0	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	26	26	0	0	0	0	0	26	0	0	0	0	0
Local Funds - Locally Generated Funds													
Existing	2,136	2,136	0	0	0	0	0	1,101	0	275	0	760	0
Change	(320)	(320)	0	0	0	0	0	(35)	0	0	0	(285)	0
Proposed	1,816	1,816	0	0	0	0	0	1,066	0	275	0	475	0
Local Funds - Locally Generated Funds													
Existing	1,778	1,778	0	0	0	0	0	0	0	1,528	250	0	0
Change	(250)	(250)	0	0	0	0	0	0	0	0	(250)	0	0
Proposed	1,528	1,528	0	0	0	0	0	0	0	1,528	0	0	0
Total													
Existing	119,240	34,240	0	0	85,000	0	0	9,066	71,000	6,803	15,871	2,500	14,000
Change	(3,717)	(3,717)	0	85,000	(85,000)	0	0	(5,035)	0	0	1,450	(132)	0
Proposed	115,523	30,523	0	85,000	0	0	0	4,031	71,000	6,803	17,321	2,368	14,000

FINANCIAL RESOLUTION:

Be it Resolved, that the Commission does hereby amend the 2024 STIP to reprogram the \$1.042 million RIP funds from CON programmed from 2027-28 to RW in 2025-26 for the SR 4 Wagon Trail Realignment (Eastern Segment) project (PPNO 3546); to reprogram \$1,600,000 in RIP funds for the CON phase from 2025-26 to 2026-27 for the Treat Boulevard Corridor Improvements project (PPNO 0294E); to reprogram \$7,500,000 in RIP funds for the PS&E phase from 2025-26 to 2026-27 for the SR 4 Operational Improvements – Phase 1 project (PPNO 0299A); to reprogram the \$3,200,000 in CON Support funds to the CON phase in 2025-26 and augment the project scope to extend the project limits, and request state-only funds for \$15,000,000 for the future CON phase allocation for the Innovate 680 Coordinated Adaptive Ramp Metering (CARM) project (PPNO 0521L); to reprogram \$1,873,000 in RIP funds for the CON phase from 2025-26 to 2026-27 for the I-80/Central Avenue – Phase 2 (Local Road Realignment) project (PPNO 2025H); to reprogram \$1,153,000 in RIP funds for the RW Capital phase from 2025-26 to 2026-27 for the Silverado Trail Five-Way Intersection Improvements project (PPNO 1483B); to reprogram \$1,700,000 in RIP funds for the RW Capital phase from 2025-26 to 2027-28 for the US 101 Managed Lanes Project, North of Interstate 380 (I-380 to SM/SF County Line) project (PPNO 0658M); to deprogram \$5,500,000 from the PS&E phase from 2027-28 to accommodate for the environmental phase allocation in June 2025 for the Central Bikeway project (PPNO 2091L); to reprogram \$42,346,000 in RIP funds for the CON phase from 2025-26 to 2027-28 to allow time for legal resolution and completion of critical coordination efforts for the South Fresno SR 99 Corridor Project: North Avenue Interchange (PPNO 6288); to reprogram \$85,000,000 in RIP funds for the CON phase from 2026-27 to 2025-26 for the SR 94/125 Auxiliary Lanes project (PPNO 0356), in various counties.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.1a.(4), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **STIP AMENDMENT 24S-05 (ACTION)**
PPNO 5961 – LOS ANGELES COUNTY
PPNO 6433 – LOS ANGELES COUNTY
PPNO 5960 – LOS ANGELES COUNTY
PPNO 6432 – LOS ANGELES COUNTY

ACTION UPDATE:

- *Update Amendment Box: Changing the implementing agency from Los Angeles County to the City of Los Angeles for the Valley Boulevard Multi-Modal Transportation Improvement Project (PPNO 5960).*
- *Update Background: Specifying the requested fund type and the total CON as RIP funds for the Valley Boulevard Multi-Modal Transportation Improvement Project (PPNO 5960).*

ISSUE:

Should the California Transportation Commission (Commission) approve the requested State Transportation Improvement Program (STIP) Amendment 24S-05?

RECOMMENDATION:

The California Department of Transportation (Department) and the Los Angeles County Metropolitan Transportation Authority (LA Metro) request that the Commission approve STIP Amendment 24S-05. This item was noticed at the Commission's May 2025 meeting.

The Department and LA Metro propose to amend the 2024 STIP to deprogram the Regional Improvement Program (RIP) funds from the Construction Capital (CON) phase of the Soto Street Roadway Widening between Multnomah Street and Mission Road project (PPNO 5961) and add these funds to program the CON phase of the Ford Boulevard Traffic Corridor Improvement Project (N-S) (PPNO 6433), change the implementing agency from Los Angeles County to the City of Los Angeles and add RIP funds to the CON phase of the

Valley Boulevard Multi-Modal Transportation Improvement Project (PPNO 5960), and program the CON phase of the Northeast Los Angeles Active Transportation Project & Transit Connectivity Enhancements project (PPNO 6432).

No	Project PPNO	Phase	Fiscal year	Programming amount (\$)	Deprogramming amount (\$)	Element
1.	5961	CON	2026-27	0	9,148,000	Local Assistance
2.	6433	CON	2026-27	1,000,000	0	Local Assistance
3.	5960	CON	2026-27	1,648,000	0	Local Assistance
4.	6432	CON	2026-27	6,500,000	0	Local Assistance
Total				9,148,000	9,148,000	

BACKGROUND:

The Soto Street Roadway Widening between Multnomah Street and Mission Road project (PPNO 5961) is currently programmed in the 2024 STIP with \$26,330,000 in RIP funds for the CON phase in Fiscal Year 2026-27. LA Metro requests to deprogram \$9,148,000 of these funds to accommodate the City of Los Angeles's (City) need to advance the projects that are included in this amendment.

The Ford Boulevard Traffic Corridor Improvement Project (PPNO 6433) is a new project proposed that requires programming \$1,000,000 in RIP funds to the CON phase in 2026-27 to fully fund the project.

The Valley Boulevard Multi-Modal Transportation Improvement Project (PPNO 5960) is currently programmed in the 2024 STIP with \$17,872,000 in RIP funds for the CON phase in 2026-27. However, to move the project forward, LA Metro is proposing to program an additional \$1,648,000 of RIP funds to the CON phase. This will bring the total RIP CON funds for this project in 2026-27 to \$19,520,000. The implementing agency is also being revised from Los Angeles County to the City of Los Angeles.

The Northeast Los Angeles Active Transportation Project & Transit Connectivity Enhancements project (PPNO 6432) is a new project. In order to move the project forward, LA Metro is proposing to program \$6,500,000 in RIP funds into its CON phase in 2026-27.

**REVISE: Soto Street Roadway Widening between Multnomah Street and Mission Road
Project (PPNO 5961)**

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Los Angeles County	07	5961		LA	2026-27								
Implementing Agency: (by component)	PA&ED	Los Angeles, City of				PS&E	Los Angeles, City of						
	R/W	Los Angeles, City of				CON	Los Angeles, City of						
RTPA/CTC:	Los Angeles Metropolitan Transportation Commission												
Project Title:	Soto Street Roadway Widening between Multnomah Street and Mission Road												
Location:	The Project is located in the northeastern part of Los Angeles, along Soto Street between Multnomah Street and Mission Road. Soto Street serves as a corridor connecting the Cities of Pasadena, South Pasadena, and Alhambra to the north and west, with the communities of Lincoln Heights, Boyle Heights, and El Sereno to the east and south. The Project is surrounded by the neighborhoods of Lincoln Heights in the south and west, Montecito Heights in the north, and El Sereno in the east.												
Description:	Widen the existing Soto Street roadway from Multnomah Street to Mission Road, to increase capacity and would be configured to include the following: Two southbound traffic lanes; Two northbound traffic lanes; Two (one in each direction) protected bicycle lanes; One median; new and wider sidewalk on the west side, new sidewalk on the east side of Soto Street and widened roadway additional 20 feet for a total width of 90 feet.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	26,330	0	0	0	26,330	0	0	0	26,330	0	0	0	0
Change	(9,148)	0	0	0	(9,148)	0	0	0	(9,148)	0	0	0	0
Proposed	17,182	0	0	0	17,182	0	0	0	17,182	0	0	0	0
RSTP													
Existing	4,000	4,000	0	0	0	0	0	0	0	0	4,000	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	4,000	4,000	0	0	0	0	0	0	0	0	4,000	0	0
Local Funds - Locally Generated Funds													
Existing	5,330	5,330	0	0	0	0	0	329	4,250	751	0	0	0
Change	(5,330)	(5,330)	0	0	0	0	0	(329)	(4,250)	(751)	0	0	0
Proposed	0	0	0	0	0	0	0	0	0	0	0	0	0
Local Funds - City Funds													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	7,040	3,275	0	3,765	0	0	0	115	6,661	176	88	0	0
Proposed	7,040	3,275	0	3,765	0	0	0	115	6,661	176	88	0	0
Local Funds - Prop "C" Funds													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	7,447	2,055	2,000	3,392	0	0	0	214	4,746	487	2,000	0	0
Proposed	7,447	2,055	2,000	3,392	0	0	0	214	4,746	487	2,000	0	0
Total													
Existing	35,660	9,330	0	0	26,330	0	0	329	30,580	751	4,000	0	0
Change	9	0	2,000	7,157	(9,148)	0	0	0	(1,991)	(88)	2,088	0	0
Proposed	35,669	9,330	2,000	7,157	17,182	0	0	329	28,589	663	6,088	0	0

ADD: Ford Boulevard Traffic Corridor Improvement Project (N-S) (PPNO 6433)

County		District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor				
Los Angeles County		07	6433		LA	2026-27							
Implementing Agency: (by component)		PA&ED		Los Angeles County				PS&E		Los Angeles County			
		R/W		Los Angeles County				CON		Los Angeles County			
RTPA/CTC:		Los Angeles Metropolitan Transportation Commission											
Project Title:		Ford Boulevard Traffic Corridor Improvement Project (N-S)											
Location:		This project is located in the San Gabriel Valley subregion. Jurisdictions include the City of Monterey Park, Department, and the unincorporated areas of Los Angeles County.											
Description:		Install new communication infrastructure and upgrade traffic signal infrastructure for 5 intersections on Ford Boulevard between Floral Drive and 3rd Street. This project will complete Traffic Signal Synchronization (TSSP) improvements at 7 intersections on Ford Boulevard between Floral Drive and Olympic Boulevard. Other project components include Intelligent Transportation System (ITS) improvements.											
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	1,000	0	0	0	1,000	0	0	0	1,000	0	0	0	0
Proposed	1,000	0	0	0	1,000	0	0	0	1,000	0	0	0	0
CMAQ													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	1,363	0	511	0	852	0	0	0	852	0	511	0	0
Proposed	1,363	0	511	0	852	0	0	0	852	0	511	0	0
Total													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	2,363	0	511	0	1,852	0	0	0	1,852	0	511	0	0
Proposed	2,363	0	511	0	1,852	0	0	0	1,852	0	511	0	0

**REVISE: Valley Boulevard Multi-Modal Transportation Improvement Project
(PPNO 5960)**

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor						
Los Angeles County	07	5960		LA	2026-27									
Implementing Agency: (by component)	PA&ED	Los Angeles County Los Angeles, City of				PS&E	Los Angeles County Los Angeles, City of							
	R/W	Los Angeles County Los Angeles, City of				CON	Los Angeles County Los Angeles, City of							
RTPA/CTC:	Los Angeles Metropolitan Transportation Commission													
Project Title:	Valley Boulevard Multi-Modal Transportation Improvement Project													
Location:	The project is located along a 4.5-mile corridor through the Lincoln Heights, Boyle Heights, and El Sereno communities, connecting the I-710 freeway to Union Station via Valley Blvd, Mission Road, and Cesar Chavez Ave.													
Description:	The project has been designed to include: Dedicated Bus Lane during Peak Periods, New Class IV Bike Facility, Local Public Park Expansions, Standard Width Sidewalks, Raised Medians with Green Space, On-Street Parking, Railroad Quiet Zones, Curb Modifications and New Street Lighting. The project will also upgrade/improve the 32 existing bus shelters.													
(DOLLARS IN THOUSANDS)														
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component						
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp	
RIP														
Existing	27,304	0	0	9,432	17,872	0	0	0	17,872	0	9,432	0	0	
Change	1,648	0	0	0	1,648	0	0	0	1,648	0	0	0	0	
Proposed	28,952	0	0	9,432	19,520	0	0	0	19,520	0	9,432	0	0	
RSTP														
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0	
Change	20,000	8,000	4,000	8,000	0	0	0	4,000	6,000	6,000	4,000	0	0	
Proposed	20,000	8,000	4,000	8,000	0	0	0	4,000	6,000	6,000	4,000	0	0	
CMAQ														
Existing	2,500	2,500	0	0	0	0	0	0	0	2,500	0	0	0	
Change	(2,500)	(2,500)	0	0	0	0	0	0	0	(2,500)	0	0	0	
Proposed	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total														
Existing	29,804	2,500	0	9,432	17,872	0	0	0	17,872	2,500	9,432	0	0	
Change	19,148	5,500	4,000	8,000	1,648	0	0	4,000	7,648	3,500	4,000	0	0	
Proposed	48,952	8,000	4,000	17,432	19,520	0	0	4,000	25,520	6,000	13,432	0	0	

ADD: Northeast Los Angeles Active Transportation Project & Transit Connectivity Enhancements Project (PPNO 6432)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Los Angeles County	07	6432		LA	2026-27								
Implementing Agency: (by component)	PA&ED	Los Angeles, City of				PS&E	Los Angeles, City of						
	R/W	Los Angeles, City of				CON	Los Angeles, City of						
RTPA/CTC:	Los Angeles Metropolitan Transportation Commission												
Project Title:	Northeast Los Angeles Active Transportation Project & Transit Connectivity Enhancements												
Location:	In the City of Los Angeles, on Figueroa St from S. Ave 60 to Meridian St, on Meridian St from Figueroa St to N. Ave 63, and on N. Ave 63 from Meridian St. to Ruby St.												
Description:	Reconstruct sidewalks and driveways, curb extensions, median island/pedestrian plaza upgrades, ADA compliant access ramps, improved transit furniture and stops, high visibility crosswalks, speed feedback signs, and other wayfinding signage. The project also includes pedestrian lighting, traffic signal upgrades, including High-Intensity Activated Crosswalk (HAWK) signals, and new street trees and enhanced landscaping.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	6,500	0	0	0	6,500	0	0	0	6,500	0	0	0	0
Proposed	6,500	0	0	0	6,500	0	0	0	6,500	0	0	0	0
RSTP													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	5,000	1,125	0	3,875	0	0	0	0	3,875	250	875	0	0
Proposed	5,000	1,125	0	3,875	0	0	0	0	3,875	250	875	0	0
Total													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	11,500	1,125	0	3,875	6,500	0	0	0	10,375	250	875	0	0
Proposed	11,500	1,125	0	3,875	6,500	0	0	0	10,375	250	875	0	0

FINANCIAL RESOLUTION:

Be it Resolved, that the Commission does hereby amend the 2024 STIP to deprogram the RIP funds from the CON phase of the Soto Street Roadway Widening between Multnomah Street and Mission Road project (PPNO 5961) and add these funds to program the CON phase of the Ford Boulevard Traffic Corridor Improvement Project (N-S) (PPNO 6433), change the implementing agency from Los Angeles County to the City of Los Angeles and add RIP funds to the CON phase of the Valley Boulevard Multi-Modal Transportation Improvement Project (PPNO 5960), and program the CON phase of the Northeast Los Angeles Active Transportation Project & Transit Connectivity Enhancements project (PPNO 6432), in Los Angeles County.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.1a.(2), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **STIP AMENDMENT 24S-04 (ACTION)**
PPNO 2091P – SONOMA COUNTY
PPNO 5156Q – SONOMA COUNTY

ISSUE:

Should the California Transportation Commission (Commission) approve the requested State Transportation Improvement Program (STIP) Amendment 24S-04?

RECOMMENDATION:

The California Department of Transportation (Department), the Sonoma County Transportation Authority (SCTA), and the Metropolitan Transportation Commission (MTC) request that the Commission approve STIP Amendment 24S-04. This item was noticed at the Commission's May 2025 meeting.

The Department, SCTA, and MTC propose to amend the 2024 STIP to reprogram the Regional Improvement Program (RIP) funds from the Plans, Specifications, and Estimate (PS&E) phase of the State Route 121 and 8th Street East Intersection Improvement Project (PPNO 2091P) and reprogram RIP funds for the Construction Capital (CON) phase of the West County Trail Gap Closures project (PPNO 5156Q), in Sonoma County.

BACKGROUND:

The State Route 121 and 8th Street East Intersection Improvement Project (PPNO 2091P) is currently programmed in the 2024 STIP with \$3,100,000 in RIP funds for the PS&E phase in Fiscal Year 2027-28. SCTA requests to advance the funding to 2025-26 to allow for the expedited delivery of the project, to best utilize available funding for Sonoma County. The project was originally programmed in the 2024 STIP with SCTA as the implementing agency for Project Approval and Environmental Document (PA&ED) and the Department for PS&E. However, due to safety considerations and the need to accelerate delivery, SCTA and the

District agreed it would be more efficient for SCTA to also lead the PS&E phase using their current design consultant. This consultant is already preparing the PA&ED phase, and their contract can be amended to include PS&E, ensuring continuity and avoiding delays associated with transitioning design responsibilities to the Department. Therefore, this amendment updates the implementing agency for the PS&E phase from the Department to the SCTA.

The West County Trail Gap Closures project (PPNO 5156Q) is currently programmed in the 2024 STIP with \$3,100,000 in RIP funds for the CON phase in 2025-26. However, due to environmental delays, SCTA requests reprogramming to 2027-28.

The proposed funding plan for these projects is shown on the following tables:

REVISE: State Route 121 and 8th Street East Intersection Improvement Project (PPNO 2091P)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
Sonoma County	04	2091P	1Y170	CO	2027-28	7.800	8.400	121					
Implementing Agency: (by component)	PA&ED	Sonoma County Transportation				PS&E	Sonoma County Transportation						
	R/W	Caltrans				CON	Caltrans						
RTPA/CTC:	Metropolitan Transportation Commission												
Project Title:	State Route 121 and 8th Street East Intersection Improvement Project												
Location:	In Sonoma County, near Schellville, on State Route 121 from 0.1 miles west of 8th Street East to 0.2 miles east of 8th Street East.												
Description:	Roadway and intersection improvements to State Route 121 (Route 121) at the intersection of Route 121 and 8th Street East, located south of the City of Sonoma at approximately Postmile 8.1. Two build alternatives that maximize intersection safety and increase multimodal access, while achieving acceptable operations, are being considered: a signal and a roundabout. Both alternatives also propose Class II Bikeways (Bike Lanes) along Route 121 and Class I Bike Paths (Shared Use Paths). The project also proposes high visibility crossings and pedestrian refuges.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	6,500	0	0	0	0	6,500	0	2,400	0	0	3,100	1,000	0
Change	0	0	0	3,100	0	(3,100)	0	0	0	0	0	0	0
Proposed	6,500	0	0	3,100	0	3,400	0	2,400	0	0	3,100	1,000	0
Federal Disc.													
Existing	1,500	1,500	0	0	0	0	0	0	0	1,500	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	1,500	1,500	0	0	0	0	0	0	0	1,500	0	0	0
Future Need													
Existing	16,900	0	0	0	0	0	16,900	0	14,000	0	0	0	2,900
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	16,900	0	0	0	0	0	16,900	0	14,000	0	0	0	2,900
Total													
Existing	24,900	1,500	0	0	0	6,500	16,900	2,400	14,000	1,500	3,100	1,000	2,900
Change	0	0	0	3,100	0	(3,100)	0	0	0	0	0	0	0
Proposed	24,900	1,500	0	3,100	0	3,400	16,900	2,400	14,000	1,500	3,100	1,000	2,900

REVISE: West County Trail Gap Closures project (PPNO 5156Q)

County		District	PPNO		EA	Element	Const. Year		PM Back	PM Ahead	Route/Corridor		
Sonoma County		04	5156Q			LA	2027-28						
Implementing Agency: (by component)		PA&ED		Sonoma County				PS&E		Sonoma County			
		R/W		Sonoma County				CON		Sonoma County			
RTPA/CTC:		Metropolitan Transportation Commission											
Project Title:		West County Trail Gap Closures											
Location:		Located along Green Valley Road between Atascadero Creek and Ross Road in Graton and second segment is located on the north side of Occidental Road starting at the intersection of Highway 116 and continuing westward to the existing West County Trail near Sebastopol.											
Description:		Construct two Class I bike path segments in the unincorporated area of Sonoma County. The completed project will close a 0.20-mile and 0.90-mile gap, respectively.											
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
RIP													
Existing	3,100	0	0	3,100	0	0	0	0	3,100	0	0	0	0
Change	0	0	0	(3,100)	0	3,100	0	0	0	0	0	0	0
Proposed	3,100	0	0	0	0	3,100	0	0	3,100	0	0	0	0
Local Funds													
Existing	1,385	585	0	800	0	0	0	0	800	165	300	120	0
Change	0	0	0	(800)	0	800	0	0	0	0	0	0	0
Proposed	1,385	585	0	0	0	800	0	0	800	165	300	120	0
Total													
Existing	4,485	585	0	3,900	0	0	0	0	3,900	165	300	120	0
Change	0	0	0	(3,900)	0	3,900	0	0	0	0	0	0	0
Proposed	4,485	585	0	0	0	3,900	0	0	3,900	165	300	120	0

FINANCIAL RESOLUTION:

Be it Resolved, that the Commission does hereby amend the 2024 STIP to reprogram the RIP funds from the PS&E phase of the State Route 121 and 8th Street East Intersection Improvement Project (PPNO 2091P) and reprogram RIP funds for the CON phase of the West County Trail Gap Closures project (PPNO 5156Q), in Sonoma County.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.1a.(5), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **STIP AMENDMENT 24S-06 (ACTION)**
PPNO 0999I – SAN DIEGO COUNTY
PPNO 0999J – SAN DIEGO COUNTY
PPNO 0999K – SAN DIEGO COUNTY

ACTION UPDATE: *Update Recommendation, Background, Amendment Box and Financial Resolution: Changing project title of East Otay Mesa Land Port of Entry Construction Package 1 (PPNO 0999K) to Otay Mesa East Port of Entry Construction – Underground Utilities and Grading Work Package (PPNO 0999K).*

ISSUE:

Should the California Transportation Commission (Commission) approve the requested State Transportation Improvement Program (STIP) Amendment 24S-06?

RECOMMENDATION:

The California Department of Transportation (Department), and the San Diego Association of Governments (SANDAG) request that the Commission approve STIP Amendment 24S-06. This item was noticed at the Commission's May 2025 meeting.

The Department and SANDAG propose to amend the East Otay Mesa Land Port of Entry – Construction project (PPNO 0999I) in San Diego County, to split out a portion of the scope to two new child projects: East Otay Mesa Land Port of Entry Technology Package (PPNO 0999J), to be programmed with \$36,000,000 of Infrastructure for Rebuilding America (INFRA) Grant funds, and Otay Mesa East Port of Entry Construction – Underground Utilities and Grading Work Package (PPNO 0999K), to be programmed with \$114,000,000 of INFRA Grant funds.

BACKGROUND:

Pursuant to Section 164.1 of the California Streets and Highways Code, INFRA funds shall be programmed, allocated, and expended in the same manner as other federal funds made available for capital improvement projects in the STIP.

On April 3, 2025, SANDAG, the Department, and the Federal Highway Administration officially completed an agreement obligating \$150 million in INFRA grant funding for construction of the East Otay Mesa Land Port of Entry (POE) project. This project is located near the City of San Diego on State Route (SR) 11, 1.9 miles east of Sanyo Avenue Undercrossing (Mexico border).

The United States and Mexico continue to improve existing border infrastructure, but increasing demands on today's border crossings in the San Diego-Baja California region impede mobility. Insufficient capacity at these border crossings, coupled with average wait times of two hours, costs both countries billions of dollars annually in foregone economic output.

To help solve this problem, the SR 11/East Otay Mesa Land POE will be the nation's first transformative land POE to integrate cutting-edge technologies that will revolutionize border crossings. These technologies will reduce wait times, traffic congestion, and vehicular emissions while enhancing cross-border travel and trade. Using variable tolls to provide a 20-minute average wait time goal, the POE will provide a new relief valve while managing traffic demand, resulting in decreased congestion and wait times for all of the region's POEs. The new East Otay Mesa Land POE will improve regional security and safety, bolster the binational economy, improve mobility and efficiency, reduce greenhouse gas emissions, and foster innovative technology solutions, all while delivering a 10-to-1 return on investment for the San Diego region. Through managed operations and complete corridor integrations currently being implemented with the region's California Sustainable Freight Action Plan border pilot project, the new POE will be essential for a fully integrated regional transportation system.

In January, and May 2012, the Commission approved programming the East Otay Mesa Land Port of Entry - Segment 3 project (PPNO 0999C) for the Plans, Specifications, and Estimate, Right of Way, and Construction phases. In June, and August 2024, the Commission approved to amend the 2022 Trade Corridor Enhancement Program and the 2024 STIP to revise and program two new child projects related to the POE. The East Otay Mesa Land Port of Entry Early Work Package project (PPNO 0999H) constructs an initial portion of the project with the needed infrastructure to support federal staff and inspections and connect them to the completed SR 11, and moving the entire Construction phase work from PPNO 0999C to the East Otay Mesa Land Port of Entry – Construction project (PPNO 0999I), to construct the POE federal facility, an Information Technology System, and tolls. The East Otay Mesa Land Port of Entry - Segment 3 project (PPNO 0999C) is the parent project that is performing the project Plans, Specifications, and Estimate phase, in addition to the Right of Way activities.

To facilitate delivering this high-profile project, the implementing agency is proposing to split out part of the East Otay Mesa Land Port of Entry – Construction project (PPNO 0999I) scope to a new child project, East Otay Mesa Land Port of Entry Technology Package project (PPNO 0999J), to procure long-lead items critical to enhancing the efficiency of the East Otay Mesa Land POE project.

The implementing agency is also proposing to split out a portion of the East Otay Mesa Land Port of Entry – Construction project (PPNO 0999I) scope to a new child project, Otay Mesa East Port of Entry Construction – Underground Utilities and Grading Work Package (PPNO 0999K), to perform fine grading (included imported borrow), installation of site perimeter security fence, installation of wet and dry utilities pipe/conduits, and procurement of long-lead items such as border fence, high mast site lighting, and inspection booths. There is a concurrent Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) and Coordinated Border Infrastructure Program Allocation and SB 1 TCEP Programming Amendment on this month's Commission agenda.

This amendment proposes to:

- Amend the scope to separate and deliver the Technology Package and Construction Package 1 from the overall Construction phase. (PPNO 0999I)
- Add a new project: East Otay Mesa Land Port of Entry Technology Package with \$36,000,000 of INFRA funds. (PPNO 0999J)
- Add a new project: Otay Mesa East Port of Entry Construction – Underground Utilities and Grading Work Package with \$114,000,000 of INFRA funds. (PPNO 0999K)

The proposed funding plan for the projects is shown below.

REVISE: East Otay Mesa Land Port of Entry – Construction project (PPNO 0999I)

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
San Diego County	11	0999I	05641	CO	2025-26	2.700	2.700	11					
Implementing Agency: (by component)	PA&ED	Caltrans				PS&E	San Diego Association of Governments (SANDAG)						
	R/W	Caltrans				CON	San Diego Association of Governments (SANDAG)						
RTPA/CTC:	San Diego Association of Governments												
Project Title:	East Otay Mesa Land Port of Entry - Construction												
Location:	Near San Diego on Route 11 at 1.9 miles east of Sanyo Avenue Undercrossing (Mexico border).												
Description:	Construct Port of Entry federal facility, Information Technology System (ITS), and tolling.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
State SB1 TCEP - Trade Corridors Enhancement Account - CO, LCO - State Share													
Existing	41,857	0	41,857	0	0	0	0	0	41,857	0	0	0	0
Change	(35,400)	0	(35,400)	0	0	0	0	0	(35,400)	0	0	0	0
Proposed	6,457	0	6,457	0	0	0	0	0	6,457	0	0	0	0
State SB1 TCEP - Trade Corridors Enhancement Account - CO, LCO - Regional Share													
Existing	49,659	0	49,659	0	0	0	0	0	49,659	0	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	49,659	0	49,659	0	0	0	0	0	49,659	0	0	0	0
Federal Disc. - Infrastructure For Rebuilding America (INFRA) Grant													
Existing	150,000	0	150,000	0	0	0	0	0	150,000	0	0	0	0
Change	(150,000)	0	(150,000)	0	0	0	0	0	(150,000)	0	0	0	0
Proposed	0	0	0	0	0	0	0	0	0	0	0	0	0
Federal Disc. - Corridor Border Infrastructure													
Existing	71,000	0	0	71,000	0	0	0	0	71,000	0	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	71,000	0	0	71,000	0	0	0	0	71,000	0	0	0	0
Local Funds													
Existing	285,913	0	285,913	0	0	0	0	0	285,913	0	0	0	0
Change	0	0	0	0	0	0	0	0	0	0	0	0	0
Proposed	285,913	0	285,913	0	0	0	0	0	285,913	0	0	0	0
Total													
Existing	598,429	0	527,429	71,000	0	0	0	0	598,429	0	0	0	0
Change	(185,400)	0	(185,400)	0	0	0	0	0	(185,400)	0	0	0	0
Proposed	413,029	0	342,029	71,000	0	0	0	0	413,029	0	0	0	0

ADD: East Otay Mesa Land Port of Entry Technology Package (PPNO 0999J)
(Split from (PPNO 0999I))

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
San Diego County	11	0999J	05643	CO	2025-26	2.700	2.700	11					
Implementing Agency: (by component)	PA&ED	Caltrans				PS&E	San Diego Association of Governments (SANDAG)						
	R/W	Caltrans				CON	San Diego Association of Governments (SANDAG)						
RTPA/CTC:	San Diego Association of Governments												
Project Title:	East Otay Mesa Land Port of Entry Technology Package												
Location:	Near San Diego on Route 11 at 1.9 miles east of Sanyo Avenue Undercrossing.												
Description:	Otay Mesa East technology package to enhance efficiency and throughput at the POE.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
State SBI TCEP - Trade Corridors Enhancement Account - CO, LCO - State Share													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	20,400	0	20,400	0	0	0	0	0	20,400	0	0	0	0
Proposed	20,400	0	20,400	0	0	0	0	0	20,400	0	0	0	0
Federal Disc. - Infrastructure For Rebuilding America (INFRA) Grant													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	36,000	0	0	36,000	0	0	0	0	36,000	0	0	0	0
Proposed	36,000	0	0	36,000	0	0	0	0	36,000	0	0	0	0
Total													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	56,400	0	20,400	36,000	0	0	0	0	56,400	0	0	0	0
Proposed	56,400	0	20,400	36,000	0	0	0	0	56,400	0	0	0	0

ADD: Otay Mesa East Port of Entry Construction – Underground Utilities and Grading Work Package (PPNO 0999K) (Split from (PPNO 0999I))

County	District	PPNO	EA	Element	Const. Year	PM Back	PM Ahead	Route/Corridor					
San Diego County	11	0999K	5644	CO	2025-26	2.700	2.700	11					
Implementing Agency: (by component)	PA&ED	Caltrans				PS&E	San Diego Association of Governments (SANDAG)						
	R/W	Caltrans				CON	San Diego Association of Governments (SANDAG)						
RTPA/CTC:	San Diego Association of Governments												
Project Title:	East Otay Mesa Land Port of Entry Construction Package 1 Otay Mesa East Port of Entry Construction - Underground Utilities and Grading Work Package												
Location:	Near San Diego on Route 11 at 1.9 miles east of Sanyo Avenue Undercrossing.												
Description:	Fine grading (including imported borrow), installation of site perimeter security fence, installation of wet and dry utilities pipe/conduits, and procurement of long lead items such as border fence, high mast site lighting, and inspection booths.												
(DOLLARS IN THOUSANDS)													
FUND	TOTAL	Project Totals by Fiscal Year						Project Totals by Component					
		Prior	24/25	25/26	26/27	27/28	28/29+	R/W	CON	PA&ED	PS&E	R/W Supp	CON Supp
State SB1 TCEP - Trade Corridors Enhancement Account - CO, LCO - State Share													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	15,000	0	15,000	0	0	0	0	0	15,000	0	0	0	0
Proposed	15,000	0	15,000	0	0	0	0	0	15,000	0	0	0	0
Federal Disc. - Infrastructure For Rebuilding America (INFRA) Grant													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	114,000	0	0	114,000	0	0	0	0	114,000	0	0	0	0
Proposed	114,000	0	0	114,000	0	0	0	0	114,000	0	0	0	0
Total													
Existing	0	0	0	0	0	0	0	0	0	0	0	0	0
Change	129,000	0	15,000	114,000	0	0	0	0	129,000	0	0	0	0
Proposed	129,000	0	15,000	114,000	0	0	0	0	129,000	0	0	0	0

FINANCIAL RESOLUTION:

Be it Resolved, that the Commission does hereby amend the 2024 STIP to amend the East Otay Mesa Land Port of Entry – Construction project (PPNO 0999I), to split out a portion of the scope to two new child projects: East Otay Mesa Land Port of Entry Technology Package (PPNO 0999J), to be programmed with \$36,000,000 of INFRA Grant funds, and Otay Mesa East Port of Entry Construction – Underground Utilities and Grading Work Package (PPNO 0999K), to be programmed with \$114,000,000 of INFRA Grant funds, in San Diego County.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.2c.(1), Action Item

Prepared By: Jeremy Ketchum, Chief
Division of Environmental Analysis

Subject: **APPROVAL OF PROJECTS FOR FUTURE CONSIDERATION OF FUNDING
RESOLUTIONS E-25-35, E-25-36, E-25-37, E-25-38**

ISSUE:

Should the California Transportation Commission (Commission), as a responsible agency, approve attached resolutions E-25-35, E-25-36, E-25-37, and E-25-38?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission, as a responsible agency, approve attached resolutions E-25-35, E-25-36, E-25-37, and E-25-38.

BACKGROUND:

[01]-01-Men-1, PM 75.47/84.10

Resolution E-25-35

The attached resolution proposes to approve for future consideration of funding the following project for which a Mitigated Negative Declaration (MND) has been completed:

- Westport Culverts Project: State Route (SR) 1 in Mendocino County. Improve drainage systems and reduce erosion. (EA 0K170)

The project is located on SR 1 from Post Mile (PM) 75.47 to 84.10, in Mendocino County. The Department proposes to improve drainage systems and reduce erosion. Specifically, the project would replace five culvert systems. The project is currently programmed in the 2022 State Highway Operation and Protection Program (SHOPP) Minor A. The total programmed amount which includes Right of Way (Support and Capital), and Construction (Support and Capital) is \$4,049,000. Construction will begin in Fiscal Year 2026-27. The scope, as described for the

preferred alternative, is consistent with the project scope as programmed by the Commission in the 2022 SHOPP Minor A.

A copy of the MND has been provided to Commission staff. The project will result in less than significant impacts to the environment after mitigation. Project implementation may have significant impacts on biological and aquatic resources. However, mitigation measures will reduce any potential impacts on the environment to less than significant. These measures include, but are not limited to, off-site mitigation that may include purchase of credits from the Mendocino Coast Mitigation Bank and a permittee-responsible mitigation project known as Sholars Bog. As a result, an MND was completed for this project.

Attachment 1

[02]-04-Sm-1, PM 17.4/18.2

Resolution E-25-36

The attached resolution proposes to approve for future consideration of funding the following project for which an MND has been completed:

- San Gregorio Creek Bridge Project: SR 1 in San Mateo County. Perform a seismic restoration of the San Gregorio Creek Bridge. The alternative selected would replace the existing bridge with a new bridge featuring pedestrian and bicycle facilities. (PPNO 2021J)

The project is located on SR 1 from PM 17.4 to 18.2, in San Mateo County. The Department proposes to perform a seismic restoration of the San Gregorio Creek Bridge. The alternative selected is replacing the existing bridge with a new bridge featuring pedestrian and bicycle facilities. The project is currently programmed in the 2024 SHOPP. The total programmed amount which includes Right of Way (Support and Capital), and Construction (Support and Capital) is \$21,859,000. Construction will begin in 2027-28. The scope, as described for the preferred alternative, is consistent with the project scope as programmed by the Commission in the 2024 SHOPP.

A copy of the MND has been provided to Commission staff. The project will result in less than significant impacts to the environment after mitigation. Project implementation may have significant impacts on biological resources, specifically, wetlands and special-status species. However, mitigation measures will reduce any potential impacts on the environment to less than significant. These measures include, but are not limited to, compensatory mitigation for wetlands and special-status species. As a result, an MND was completed for this project.

Attachment 2

[03]-04-Son-101, PM 21.0/21.8
Resolution E-25-37

The attached resolution proposes to approve for future consideration of funding the following project for which an MND has been completed:

- Santa Rosa U.S. Highway 101 Bicycle and Pedestrian Overcrossing: United States Highway (US) 101 in Sonoma County. Construct a 14.5-foot-wide bicycle and pedestrian, Americans with Disabilities Act (ADA)-compliant, Class I shared-use overcrossing spanning US 101 in Santa Rosa. The project would include a 5-foot-wide walking lane and 8-foot-wide bicycle path with possible mode separation provided by a curb/barrier and fencing. (PPNO 2354)

The project is located on US 101 from PM 21.0 to 21.8, in Sonoma County. The Department proposes to construct a 14.5-foot-wide bicycle and pedestrian, ADA-compliant, Class I shared-use overcrossing spanning US 101 in Santa Rosa. The project would include a 5-foot-wide walking lane and 8-foot-wide bicycle path with possible mode separation provided by a curb/barrier and fencing. The project is currently programmed in the 2022 State Transportation Improvement Program (STIP). The total programmed amount, which includes Right of Way (Capital) and Construction (Capital) is \$23,050,000 in STIP, Regional Improvement Program. Construction began in 2023-24. The scope, as described for the preferred alternative, is consistent with the project scope as programmed by the Commission in the 2022 STIP.

A copy of the MND has been provided to Commission staff. The project will result in less than significant impacts to the environment after mitigation. Project implementation may result in significant impacts from construction noise and vibration. However, mitigation measures will reduce any potential effects on the environment to less than significant. These measures include, but are not limited to, pre-drilling foundation pile holes, locating construction equipment as far as possible from vibration-sensitive receptors, identifying and limiting the use of heavy equipment, and using equipment that would create less vibration in proximity to buildings. As a result, an MND was completed for this project.

Attachment 3

[04]-05-Sb-135, PM 0.00/0.73
Resolution E-25-38

The attached resolution proposes to approve for future consideration of funding the following project for which a Negative Declaration (ND) has been completed:

- Los Alamos Connected Community Project: SR 135 in Santa Barbara County. Construct pedestrian and bicycle infrastructure to connect residential areas to the downtown community corridor and to Olga Reed Elementary School from south of the US 101 and SR 135 separation to PM 0.73. (PPNO 3111A)

The project is located on SR 135 from PM 0.0 to 0.73, in Santa Barbara County. The Department proposes to construct pedestrian and bicycle infrastructure to connect residential areas to the

"Improving lives and communities through transportation."

downtown community corridor and to Olga Reed Elementary School from south of the US 101 and SR 135 separation to PM 0.73. Specifically, the project includes construction of 4,800 feet of new sidewalk, 6,215 feet of buffered bike lanes, 575 feet of reconstructed sidewalk, 42 ADA-compliant ramps, 17 traffic-calming curb extensions, and 23 marked crosswalks. The project is currently programmed in the 2022 Active Transportation Program (ATP). The total programmed amount, which includes Right of Way (Capital) and Construction (Capital) is \$8,475,000 in ATP. Construction will begin in 2026-27. The scope, as described for the preferred alternative, is consistent with the project scope as programmed by the Commission in the 2022 ATP.

A copy of the ND has been provided to Commission staff. The project will result in less than significant impacts to the environment. As a result, an ND was completed for this project.

Attachment 4

Attachment 1

CALIFORNIA TRANSPORTATION COMMISSION

Resolution for Future Consideration of Funding

01-Men-1, PM 75.47/84.10

Resolution E-25-35

- 1.1 **WHEREAS**, the California Department of Transportation (Department) has completed a Mitigated Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
 - Westport Culverts Project: SR 1 in Mendocino County. Improve drainage systems and reduce erosion. (EA 0K170)
- 1.2 **WHEREAS**, the Department has certified that a Mitigated Negative Declaration has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3 **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Mitigated Negative Declaration; and
- 1.4 **WHEREAS**, the project will not have a significant effect on the environment;
- 2.1 **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for consideration of funding.

Attachment 1

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: Westport Culverts Project

2024080750	Gillian Levy	(707) 498-4071
State Clearinghouse Number	Lead Agency Contact Person	Area Code/Telephone

Project Location (include county): State Route (SR) 1 between Post Miles (PM) 75.47 and 84.10, in Mendocino County.

Project Description: Improve drainage systems and reduce erosion. Specifically, the project would replace five culvert systems.

This is to advise that the California Transportation Commission has approved the above described project on

(☐ Lead Agency/ ☒ Responsible Agency)

June 26-27, 2025, and has made the following determinations regarding the above described project:

1. The project (☐ will/ ☒ will not) have a significant effect on the environment.
2. ☐ An Environmental Impact Report was prepared for this project pursuant to the provisions of CEQA.
☒ A Mitigated Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (☒ were/ ☐ were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (☒ was / ☐ was not) adopted for this project.
5. A Statement of Overriding Considerations (☐ was / ☒ was not) adopted for this project.
6. Findings (☐ were/ ☒ were not) made pursuant to the provisions of CEQA.

The above identified document with comments and responses and record of project approval is available to the General Public at: Caltrans District 1, 1656 Union Street, Eureka, CA 95501.

TANISHA TAYLOR

Signature (Public Agency)

Date

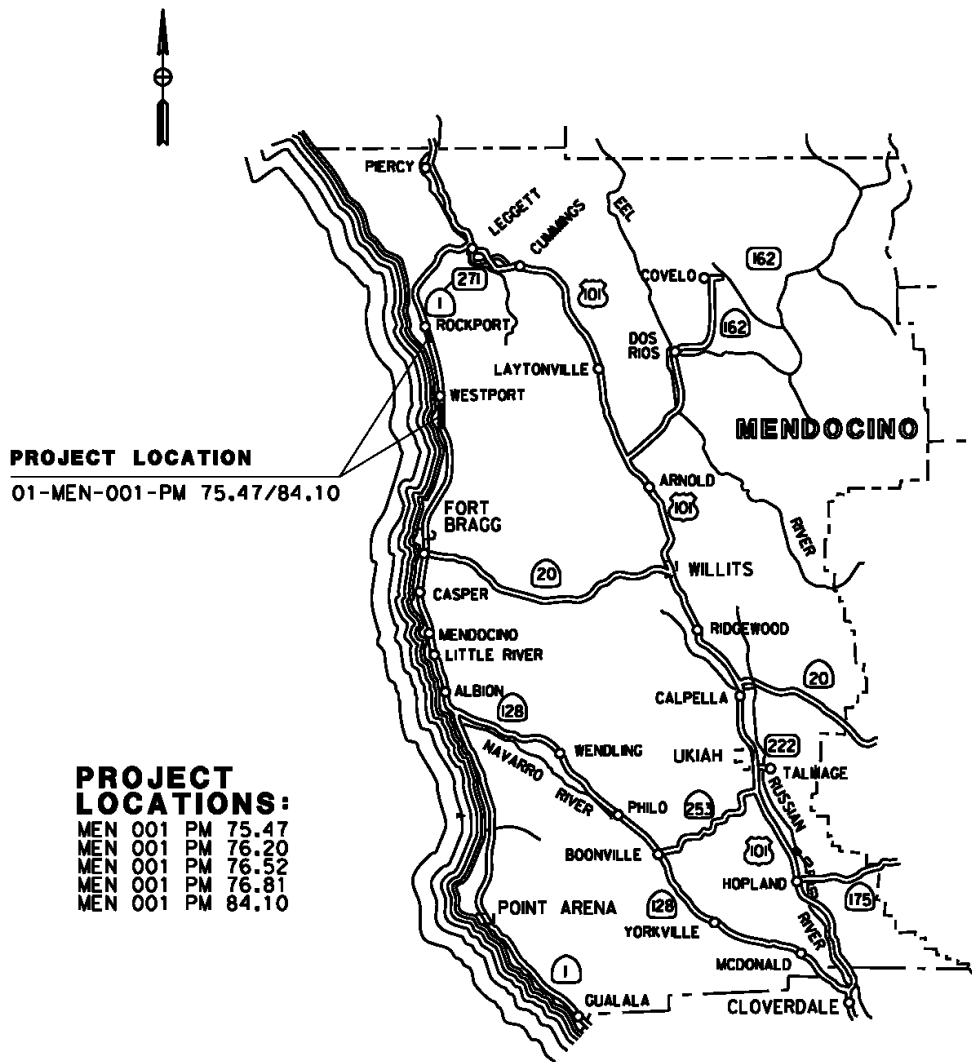
Executive Director

California Transportation Commission

Title

Date received for filing at OPR:

VICINITY MAP



MEN-001 CULVERT REHAB

01-MEN-001 (75.47 / 84.10)

01-OK170 (0120000037)

No Scale

Attachment 2

CALIFORNIA TRANSPORTATION COMMISSION

Resolution for Future Consideration of Funding

04-Sm-1, PM 17.4/18.2

Resolution E-25-36

- 1.1 **WHEREAS**, the California Department of Transportation (Department) has completed a Mitigated Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- San Gregorio Creek Bridge Project: SR 1 in San Mateo County. Perform a seismic restoration of the San Gregorio Creek Bridge. The alternative selected is replacing the existing bridge with a new bridge featuring pedestrian and bicycle facilities. (PPNO 2021J)
- 1.2 **WHEREAS**, the Department has certified that a Mitigated Negative Declaration has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3 **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Mitigated Negative Declaration; and
- 1.4 **WHEREAS**, the project will not have a significant effect on the environment;
- 2.1 **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for consideration of funding.

Attachment 2

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: San Gregorio Creek Bridge Project

2025020198	Zachary Gifford	(510) 506-1264
State Clearinghouse Number	Lead Agency Contact Person	Area Code/Telephone

Project Location (include county): State Route (SR) 1 between Post Miles (PM) 17.4 and 18.2, in San Mateo County.

Project Description: Perform a seismic restoration of the San Gregorio Creek Bridge. The alternative selected would replace the existing bridge with a new bridge featuring pedestrian and bicycle facilities.

This is to advise that the California Transportation Commission has approved the above described project on

(Lead Agency/ X Responsible Agency)

June 26-27, 2025, and has made the following determinations regarding the above described project:

1. The project (will/ X will not) have a significant effect on the environment.
2. An Environmental Impact Report was prepared for this project pursuant to the provisions of CEQA.
X A Mitigated Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (X were/ were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (X was / was not) adopted for this project.
5. A Statement of Overriding Considerations (was / X was not) adopted for this project.
6. Findings (were/ X were not) made pursuant to the provisions of CEQA.

The above identified document with comments and responses and record of project approval is available to the General Public at: Caltrans District 4, 111 Grand Ave, Oakland, CA 94612.

TANISHA TAYLOR

Signature (Public Agency)

Date

Executive Director

California Transportation Commission

Title

Date received for filing at OPR:



San Gregorio Creek Bridge Project

Attachment 3

CALIFORNIA TRANSPORTATION COMMISSION

Resolution for Future Consideration of Funding

04-Son-101, PM 21.0/21.8

Resolution E-25-37

- 1.1 **WHEREAS**, the California Department of Transportation (Department) has completed a Mitigated Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- Santa Rosa U.S. Highway 101 Bridge and Pedestrian Overcrossing: US 101 in Sonoma County. Construct a 14.5-foot-wide bicycle and pedestrian, Americans with Disabilities Act compliant, Class I shared-use overcrossing spanning US 101 in Santa Rosa. The project would include a 5-foot-wide walking lane and 8-foot-wide bicycle path with possible mode separation provided by a curb, barrier and fencing. (PPNO 2354)
- 1.2 **WHEREAS**, the Department has certified that a Mitigated Negative Declaration has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3 **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Mitigated Negative Declaration; and
- 1.4 **WHEREAS**, the project will not have a significant effect on the environment;
- 2.1 **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for consideration of funding.

Attachment 3

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: Santa Rosa U.S. Highway 101 Bicycle and Pedestrian Overcrossing

2020060455	Liz Nagle	(510) 496-9654
State Clearinghouse Number	Lead Agency Contact Person	Area Code/Telephone

Project Location (include county): United States Highway (US) 101 between Post Miles (PM) 21.0 and 21.8, in Sonoma County.

Project Description: Construct a 14.5-foot-wide bicycle and pedestrian, Americans with Disabilities Act compliant, Class I shared-use overcrossing spanning US 101 in Santa Rosa. The project would include a 5-foot-wide walking lane and 8-foot-wide bicycle path with possible mode separation provided by a curb, barrier and fencing.

This is to advise that the California Transportation Commission has approved the above described project on

(Lead Agency/ X Responsible Agency)

June 26-27, 2025, and has made the following determinations regarding the above described project:

1. The project (will/ X will not) have a significant effect on the environment.
2. An Environmental Impact Report was prepared for this project pursuant to the provisions of CEQA.
X A Mitigated Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (X were/ were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (X was / was not) adopted for this project.
5. A Statement of Overriding Considerations (was / X was not) adopted for this project.
6. Findings (were/ X were not) made pursuant to the provisions of CEQA.

The above identified document with comments and responses and record of project approval is available to the General Public at: Caltrans District 4, 111 Grand Ave, Oakland, CA 94612.

TANISHA TAYLOR

Signature (Public Agency)

Date

Executive Director
California Transportation Commission

Title

Date received for filing at OPR:

Attachment 3



Santa Rosa U.S. Highway 101 Bicycle and Pedestrian Overcrossing

PM 21.0 to 21.8, in Sonoma County

Attachment 4

CALIFORNIA TRANSPORTATION COMMISSION

Resolution for Future Consideration of Funding

05-SB-135, PM 0.00/0.73

Resolution E-25-38

- 1.1 **WHEREAS**, the California Department of Transportation (Department) has completed a Negative Declaration pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
 - Los Alamos Connected Community Project: SR 135 in Santa Barbara County. Construct pedestrian and bicycle infrastructure to connect residential areas to the downtown community corridor and to Olga Reed Elementary School from south of the US 101 and SR 135 separation to PM 0.73. (PPNO 3111A)
- 1.2 **WHEREAS**, the Department has certified that a Negative Declaration has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3 **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Negative Declaration; and
- 1.4 **WHEREAS**, the project will not have a significant effect on the environment;
- 2.1 **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve of the above referenced project to allow for consideration of funding.

Attachment 4

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: Los Alamos Connected Community Project

2024120277	Lucas Marsalek	(805) 458-5408
State Clearinghouse Number	Lead Agency Contact Person	Area Code/Telephone

Project Location (include county): State Route (SR) 135 from postmile (PM) 0.00 to PM 0.73 in Santa Barbara County.

Project Description: Construct pedestrian and bicycle infrastructure to connect residential areas to the downtown community corridor and to Olga Reed Elementary School from south of the US 101 and SR 135 separation to PM 0.73. Specifically, the project includes construction of 4,800 feet of new sidewalk, 6,215 feet of buffered bike lanes, 575 feet of reconstructed sidewalk, 42 ramps compliant with the Americans with Disabilities Act, 17 traffic-calming curb extensions, and 23 marked crosswalks.

This is to advise that the California Transportation Commission has approved the above described project on

(Lead Agency/ X Responsible Agency)

June 26-27, 2025, and has made the following determinations regarding the above described project:

1. The project (will/ X will not) have a significant effect on the environment.
2. An Environmental Impact Report was prepared for this project pursuant to the provisions of CEQA.
X A Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (were/ X were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (was / X was not) adopted for this project.
5. A Statement of Overriding Considerations (was / X was not) adopted for this project.
6. Findings (were/ X were not) made pursuant to the provisions of CEQA.

The above identified document with comments and responses and record of project approval is available to the General Public at: Caltrans District 5, 50 Higuera Street, San Luis Obispo, CA 93401.

TANISHA TAYLOR

Signature (Public Agency)

Date

Executive Director
California Transportation Commission

Title

Date received for filing at OPR:

Attachment 4



Los Alamos Connected Community Project

Signature: *Chris Kuzak*

Email: chris.kuzak@dot.ca.gov

Signature: *Jeremy Ketchum*

Email: jeremy.ketchum@dot.ca.gov

Signature: *Donna Berry*
Donna Berry (Jun 3, 2025 8:11 PDT)

Email: donna.m.berry@dot.ca.gov

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5c.(7), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ALLOCATION AMENDMENT FOR A LOCALLY-ADMINISTERED STATE
TRANSPORTATION IMPROVEMENT PROGRAM PROJECT, OFF THE STATE
HIGHWAY SYSTEM
RESOLUTION FP-24-92, AMENDING RESOLUTION FP-24-12**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for the locally-administered State Transportation Improvement Program (STIP) Kammerer Rd Extension (Connector Segment A) project (PPNO 5844), off the State Highway System, in Sacramento County, to revise the description and outputs?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation amendment for the locally-administered STIP Kammerer Rd Extension (Connector Segment A) project (PPNO 5844), off the State Highway System, in Sacramento County, to revise the description and outputs.

BACKGROUND:

In August 2024, the Commission approved \$7,750,000 for the Right of Way phase of the locally-administered STIP Kammerer Rd Extension (Connector Segment A) project (PPNO 5844), under Resolution FP-24-12. In the same month, the Commission accepted the Mitigated Negative Declaration for the Capital Southeast Connector – A1/A2 Kammerer Road project (PPNO 5844) in Sacramento County and approved the project for future funding consideration. This 4-lane project will replace an existing portion of Kammerer Road with a 4-lane thoroughfare, construct a new 4-lane expressway section to Interstate 5 (I-5), and implement railroad grade separation and interchange improvements. The total length of the 4-lane project is approximately 5.75 miles, and dependent upon funding, initial construction may consist of a 2-lane facility. This project includes Class II bike lanes; however, it does not

include the Class I Path. As funding and traffic conditions warrant, the 4-lane facility will be constructed. Per Section 1.4 project design of the California Environmental Quality Act (CEQA) document:

- Project design features for the project from Bruceville Road to the I-5 interchange include the following:
 - Four through lanes (two lanes in each direction) with turn lanes at intersections
 - Shoulders
 - Median of varying width
 - Signage
 - Multi-use path (adjacent west-bound travel lane)

The Kammerer Rd Extension (Connector Segment A) project is an initial 2-lane phase of the 4-lane Capital Southeast Connector – A1/A2 Kammerer Road project as referenced above. The project description of the Kammerer Rd Extension (Connector Segment A) project included Class II bike lanes but did not include a Class I path even though the Class I path has been environmentally studied and cleared with the above referenced CEQA document. The proposed change is to add the Class I path to the project description of the Kammerer Rd Extension (Connector Segment A) 2-lane project as a Class I path provides:

- A higher level of safety for users compared to Class II bike lanes due to the Class I path's separation from traffic
- Affords more multi-modal options than a Class II bike lane
- Can easily be accommodated within the 2-lane design and right-of-way acquisitions
- Better positions the project for future grant opportunities by including a multi-modal facility with the initial 2-lane project

As such, the requested addition of the Class I path (multiuse path) to the 2-lane project scope has been environmentally studied and cleared with the above-referenced CEQA document.

The proposed changes are reflected in strikethrough and bold in the attached revised vote box. There is no change to the allocation amount.

FINANCIAL RESOLUTION:

Be it Resolved, that the locally-administered STIP Kammerer Rd Extension (Connector Segment A) project (PPNO 5844), is hereby amended to revise the description and outputs, in accordance with the attached revised vote box.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type												
2.5c.(7) Allocation Amendment - Locally-Administered STIP Project off the State Highway System			Resolution FP-24-92 Amending Resolution FP-24-12													
1 \$7,750,000 City of Elk Grove SACOG 03-Sacramento	Kammerer Rd Extension (Connector Segment A). Kammerer Road Extension (Connector Segment) (In Elk Grove, Kammerer Rd. from Bruceville Rd. to Interstate 5/Hood Franklin Interchange: Extend 2 lane road and include Class I path. Modify the I-5/Hood Franklin interchange (signalization and turn lanes at the ramps), construct a grade separation at the UPRR tracks, Class II bike lanes, and signalized intersections at major road crossings.) <table><tr><td><u>Outcome/Outputs</u></td><td><u>Unit</u></td><td><u>Total</u></td></tr><tr><td>Roadway lane mile(s) - new</td><td>Miles</td><td>6</td></tr><tr><td>New roadway lane-mile(s)</td><td>Miles</td><td>5.5</td></tr><tr><td>Pedestrian/Bicycle facilities miles constructed</td><td>Miles</td><td>3</td></tr></table> CEQA - ND, 12/14/2018 NEPA - FONSI, 10/23/2023 Future consideration of funding approved under Resolution E-24-80; August 2024. Time Extension for FY 22-23 R/W expires February 28, 2025. <u>Amend Resolution FP-24-12 to revise the description and outputs.</u> <u>There is no change to the allocation amount.</u>	<u>Outcome/Outputs</u>	<u>Unit</u>	<u>Total</u>	Roadway lane mile(s) - new	Miles	6	New roadway lane-mile(s)	Miles	5.5	Pedestrian/Bicycle facilities miles constructed	Miles	3	03-5844 RIP/22-23 R/W \$7,750,000 0312000019	2023-24 101-0890 FTF 20.30.600.620	\$7,750,000
<u>Outcome/Outputs</u>	<u>Unit</u>	<u>Total</u>														
Roadway lane mile(s) - new	Miles	6														
New roadway lane-mile(s)	Miles	5.5														
Pedestrian/Bicycle facilities miles constructed	Miles	3														

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.2c.(4), Action Item

Prepared By: Jeremy Ketchum, Chief
Division of Environmental Analysis

Subject: **APPROVAL OF PROJECTS FOR FUTURE CONSIDERATION OF FUNDING
RESOLUTIONS E-25-41A, E-25-42A, E-25-43A, E-25-44A, and E-25-45A**

ISSUE:

Should the California Transportation Commission (Commission), as a responsible agency, approve attached resolutions E-25-41A, E-25-42A, E-25-43A, E-25-44A, and E-25-45A?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission, as a responsible agency, approve attached resolutions E-25-41A, E-25-42A, E-25-43A, E-25-44A, and E-25-45A.

BACKGROUND:

[01]-04-Ala-880, PM 30.47/31.61

04-Ala-260, PM R0.78/R1.90

Resolution E-25-41A

The attached resolution proposes to approve for future consideration of funding the following project for which a Final Environmental Impact Report (FEIR) and Addendum have been completed:

- Oakland Alameda Access Project: Interstate 880 (I-880) and State Route (SR) 260 in Alameda County. Improve motorist, pedestrian, and bicycle safety, reduce conflicts between regional and local traffic, and enhance bicycle and pedestrian accessibility and connectivity within the project area. (PPNO 0044D)

The project is located on I-880 from Post Mile (PM) 30.47 to 31.61, and on SR 260 from PM R0.78 to R1.90 within the cities of Oakland and Alameda, in Alameda County. The Department, in partnership with Alameda County Transportation Commission, proposes to improve mobility and accessibility. Specifically, the project will improve motorist, pedestrian, and bicycle safety, reduce conflicts between regional and local traffic, and enhance bicycle and pedestrian accessibility and connectivity within the project area by removing and modifying existing freeway ramps, modifying the connection from the Posey Tube to I-880, constructing Class IV two-way cycle tracks in Oakland, implementing various related "complete streets" improvements, implementing bicycle and pedestrian improvements at the approaches to the Tubes, and opening the Webster Tube's westside walkway to bicyclists and pedestrians. The project is currently programmed in the 2024 State Transportation Improvement Program (STIP). The total programmed amount, which includes Right of Way (Support and Capital) and Construction (Support and Capital) is \$151,917,000 in STIP, Regional Improvement Program (RIP), and Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic). Construction began in Fiscal Year 2024-25. The scope, as described for the preferred alternative, is consistent with the project scope as programmed by the Commission in the 2024 STIP.

Copies of the FEIR and Addendum have been provided to Commission staff. The Commission accepted the FEIR and approved the project for future consideration of funding on May 18, 2022, under Resolution E-22-36. Since the approval of the FEIR, there have been changes to the project and an Addendum was prepared pursuant to the California Environmental Quality Act (CEQA). Specifically, the Addendum addresses minor design modifications, including overhead and roadside signage, minor changes in permanent maintenance easements, retaining wall modifications, Americans with Disabilities Act (ADA)-compliant curb ramps, pavement rehabilitation and associated improvements, driveway access control modifications, raised cycle track, crosswalk and sidewalk modifications, modifications to median and sidewalk landscaping, changes to proposed utility undergrounding and relocations, changes in tree removal and on-street parking, and conversion of a Harrison Street segment from bidirectional traffic to one-way traffic. The modifications expand the project footprint within I-880 right-of-way (ROW) and on some local streets. There is no change to the construction duration. The design modifications expand the project footprint mostly within the I-880 ROW to accommodate signage and on some local streets where proposed ADA-compliant curb ramps, pavement rehabilitation, sidewalks, and other improvements would occur. Resources in the project area that may be impacted by the project include cultural resources. Potential impacts associated with the project can all be mitigated to below significance with the exception of cultural resources, for which a Statement of Overriding Considerations was prepared. In accordance with CEQA Guidelines Section 15162, no Subsequent FEIR is required for the project because new information and changes in circumstances, project description, impacts, and mitigation are not substantial and do not involve new significant impacts or a substantial increase in the severity of previously identified impacts. As a result, the Department completed an Addendum to the FEIR pursuant to CEQA.

The Department has approved this project for construction. This approval and the Addendum will satisfy the environmental requirements for this stage of the planning process.

Attachment 1

"Improving lives and communities through transportation."

[02]-05-Slo-101, PM 16.0/R22.5

Resolution E-25-42A

The attached resolution proposes to approve for future consideration of funding the following project for which an FEIR and Addendum have been completed:

- Pismo Congestion Relief Pilot Project: United States Highway 101 (US 101) in San Luis Obispo County. Widen the inside shoulder of US 101 from San Luis Creek to the railroad overhead in Pismo Beach, to serve as a travel lane strictly during periods of heavy traffic volumes. (PPNO 2653)

The project is located on US 101, from PM 16.0 to R22.5, in San Luis Obispo County. The Department proposes to widen the inside shoulder of US 101 from San Luis Creek to the railroad overhead in Pismo Beach, to serve as a travel lane strictly during periods of heavy traffic volumes. The California Vehicle Code prohibits general purpose travel on the shoulder of state highways. Therefore, the project is being proposed as a pilot project during its initial 7-years of operation, after which the Department will pursue legislative approval to make the part-time travel lane a permanent feature. The project is currently programmed in the 2024 STIP. The total programmed amount, which includes ROW (Support and Capital) and Construction (Support and Capital) is \$81,017,000 in STIP, RIP, and SB 1 Trade Corridor Enhancement Program (TCEP). Construction began in 2024-25. The scope, as described for the preferred alternative, is consistent with the project scope as programmed by the Commission in the 2024 STIP.

Copies of the FEIR and Addendum have been provided to Commission staff. The Commission accepted the FEIR and approved the project for future consideration of funding on March 22, 2023, under Resolution E-23-39. Since the approval of the FEIR, there have been changes to the project and an Addendum was prepared pursuant to CEQA. Specifically, the Addendum addresses design refinements, including the conclusion that a soundwall on the northbound side of US 101 would not meet reasonability criteria, therefore it would not be constructed; decreasing the effective height of the median barrier (resulting in reduced visual/aesthetic impacts); a reduction in the biological Area of Potential Impact; updates to impacted habitat types and corresponding Mitigation and Monitoring Plans; and, inclusion of specific median barrier coloring. Resources in the project area that may be impacted by the project include aesthetics, cultural resources, noise, and air quality. Potential impacts associated with the project can all be mitigated to below significance with the exception of cultural resources, for which a Statement of Overriding Considerations was prepared. In accordance with CEQA Guidelines Section 15162, no Subsequent FEIR is required for the project because new information and changes in circumstances, project description, impacts, and mitigation are not substantial and do not involve new significant impacts or a substantial increase in the severity of previously identified impacts. As a result, the Department completed an Addendum to the FEIR pursuant to CEQA.

The Department has approved this project for construction. This approval and the Addendum will satisfy the environmental requirements for this stage of the planning process.

"Improving lives and communities through transportation."

Attachment 2

[03]-07-LA-10, PM 44.9/48.3

08-SBd-10, PM 0.0/R37.0

Resolution E-25-43A

The attached resolution proposes to approve for future consideration of funding the following project for which an FEIR and Addendum have been completed:

- Interstate 10 Corridor Project: I-10 in Los Angeles and San Bernardino counties. Construct roadway improvements on a portion of I-10. Specifically, add freeway lanes along the 33-mile segment of I-10 between the Los Angeles and San Bernardino County line and Ford Street. (PPNO 0134K and PPNO 1325)

The project is located on I-10 from PM 44.9 to 48.3 in Los Angeles County, and from PM 0.0 to R37.0 in San Bernardino County. The project will construct roadway improvements on a portion of I-10. The sponsoring agency, San Bernardino County Transportation Authority, proposes to add freeway lanes along the 33-mile segment of I-10 between the Los Angeles and San Bernardino County line and Ford Street. The project consists of three Contracts. Contract 1, which was open to traffic in 2024, provided two express lanes (price managed/tolled facilities) in each direction of I-10 from just west of the Los Angeles/San Bernardino County line to I-15. Contract 2 would include construction of one express lane in each direction of I-10 from I-15 to just east of Pepper Avenue (Contract 2). Contract 3 would include construction of one express lane in each direction of I-10 from Pepper Ave to Ford Street and a second express lane in each direction of I-10 from I-15 to California Street. In total, two express lanes would be added to I-10 upon completion of the entire project. The project is currently programmed in the 2024 STIP. The total programmed amount, which includes Construction (Capital) is \$427,708,000 in STIP, RIP, SB 1 TCEP, and SB 1 LPP (Formulaic). Construction will begin in 2025-26. The scope, as described for the preferred alternative, is consistent with the project scope as programmed by the Commission in the 2024 STIP.

Copies of the FEIR and Addendum have been provided to Commission staff. The Commission approved the project for future consideration of funding on October 19, 2017, under Resolution E-17-68. Since the approval of the FEIR, there have been changes to the project and an Addendum was prepared pursuant to CEQA. Specifically, the Addendum addresses several scope revisions, including a refinement that Contract 2 would construct a single express lane in each direction from I-15 to just east of Pepper Avenue, full replacement of the Etiwanda Wash eastbound off-ramp and Etiwanda-San Sevaine Channel eastbound on-ramp, raising of Etiwanda Avenue off-ramp bridge by 1.7 feet, extension of eastbound auxiliary lanes between Cherry Avenue and Citrus Avenue (including signing, striping, and relocation of toll message signs), an increase in construction closure duration at the Valley Boulevard eastbound off-ramp, and Contract 3 would include construction of one express lane in each direction of I-10 from Pepper Ave to Ford Street followed by a second express lane in each direction of I-10 from I-15 to California Street. In total, two express lanes would be added in each direction of I-10 between the Los Angeles/San Bernardino County line and California Street upon completion of the project.

"Improving lives and communities through transportation."

Resources that may be impacted by the project include community impacts, pedestrian and bicycle, noise, environmental justice, traffic, water quality, air quality, hazardous waste, aesthetics, and biological resources. Potential impacts associated with the project can all be mitigated to below significance. In accordance with CEQA Guidelines Section 15162, no Subsequent FEIR is required for the project because new information and changes in circumstances, project description, impacts, and mitigation are not substantial and do not involve new significant impacts or a substantial increase in the severity of previously identified impacts. As a result, the Department completed an Addendum to the FEIR pursuant to CEQA.

The Department has approved this project for construction. This approval and the Addendum will satisfy the environmental requirements for this stage of the planning process.

Attachment 3

[04]-08-Riv-60, PM 28.03/30.42

Resolution E-25-44A

The attached resolution proposes to approve for future consideration of funding the following project for which a Mitigated Negative Declaration (MND) and Addendum have been completed:

- State Route 60/Potrero Boulevard New Interchange Project: SR 60 in Riverside County. Construction of a new 6-lane bridge overcrossing at SR 60 with a temporary connection to Western Knolls Avenue; construction of westbound and eastbound diagonal and loop entry ramps, extended ramp acceleration and deceleration lanes, realignment of Western Knolls Avenue, and removal of Western Knolls Avenue connections to SR 60. (PPNO 1321)

The project is located on SR 60, from PM 28.03 to 30.42, in Riverside County. The project features construction of a new full access interchange and bridge overcrossing on SR 60 for Potrero Boulevard. The project limits begin east of Jack Rabbit Trail and end west of SR 60 and the I-10 junction. Phase 1 includes construction of a new 6-lane bridge overcrossing at SR 60 with a temporary connection to Western Knolls Avenue. Phase 2 includes constructing the westbound and eastbound diagonal and loop entry ramps, extended ramp acceleration and deceleration lanes, realignment of Western Knolls Avenue, and removal of Western Knolls Avenue connections to SR 60. The project is currently programmed in the 2024 STIP. The total programmed amount, which includes Construction (Capital and Support) is \$70,113,000 in SB 1 TCEP. Construction will begin in 2024-25. The scope, as described for the preferred alternative, is consistent with the project scope as programmed by the Commission in the 2024 STIP.

Copies of the MND and Addendum have been provided to Commission staff. The Commission approved the project for future consideration of funding on May 7, 2013, under Resolution E-13-42. Since the approval of the MND, there have been changes to the project and an Addendum was prepared pursuant to CEQA. Specifically, the Addendum addresses scope revisions and design refinements, including removing an auxiliary lane, relocating a gas line,

"Improving lives and communities through transportation."

adding a new roadway taper at the north end of the project limits, and reducing grading impacts at the south end of the project limits. Project impacts would remain less than significant with mitigation. The project changes do not meet the criteria outlined under CEQA Guidelines Section 15162 to prepare a Subsequent MND. As a result, the Department completed an Addendum to the MND pursuant to CEQA.

The Department has approved this project for construction. This approval and the Addendum will satisfy the environmental requirements for this stage of the planning process.

Attachment 4

[05]-12-Ora-5, PM 21.3/R25.8
Resolution E-25-45A

The attached resolution proposes to approve for future consideration of funding the following project for which an MND and Addendum have been completed:

- Interstate 5 (I-5) Improvement Project from Interstate 405 (I-405) to State Route 55 (SR-55): I-5 in Orange County. Widen I-5 between I-405 and SR 55. (PPNO 2743)

The project is located on I-5 from PM 21.3 to R25.8, in Orange County. The Department, in cooperation with the Orange County Transportation Authority, proposes to widen I-5 between I-405 and SR 55 by adding one additional lane in each direction of travel (northbound and southbound). The project is currently programmed in the 2024 STIP. The total programmed amount, which includes Right of Way (Capital) and Construction (Support and Capital) is \$223,685,000 in STIP, RIP, and SB 1 LPP (Formulaic). Construction will begin in 2026-27. The scope, as described for the preferred alternative, is consistent with the project scope as programmed by the Commission in the 2024 STIP.

Copies of the MND and Addenda have been provided to Commission staff. The Commission approved the project (EA 0K670) for future consideration of funding on May 13, 2020, under Resolution E-20-31. Since the approval of the MND, there have been changes to the project and Addenda were prepared pursuant to CEQA. Specifically, EA 0K670 was divided into two segments, and each segment was subsequently combined with portions of the I-5 Multi-Asset Project (original EA 0S050). The Department determined that since portions of the I-5 Multi-Asset Project included improvements to the same segments of I-5, they should be included as part of the design of projects EA 0K671 (segment 1) and EA 0K672 (segment 2). The I-5 Multi-Asset Project was environmentally cleared through a CEQA Categorical Exemption/National Environmental Policy Act Categorical Exclusion Class 1(c) (CE/CE) in October 2022. EA 0K671 (segment 1) was combined with EA 02051 into new EA 0K61U. Similarly, EA 0K672 (segment 2), was combined with EA 0S052 into new EA 0K62U. The Addenda explain the reasoning for combining the projects in more detail, as well as the updated design refinements as a result, which involve shifting the roadway centerline, removing a portion of one noise barrier, and incorporating updates from the I-5 Multi-Asset Project, such as pavement rehabilitation, storm drain restoration, sign replacements, pedestrian improvements, and updated bike lane markings. Project impacts would remain less

than significant with mitigation. The project changes do not meet the criteria outlined under CEQA Guidelines Section 15162 to prepare a Subsequent MND. As a result, the Department completed an Addendum to the MND pursuant to CEQA.

The Department has approved this project for construction. This approval and the Addendum will satisfy the environmental requirements for this stage of the planning process.

Attachment 5

Attachment 1

CALIFORNIA TRANSPORTATION COMMISSION

Resolution for Future Consideration of Funding

04-Ala-880, PM 30.47/31.61

04-Ala-260, PM R0.78/R1.90

Resolution E-25-41A

- 1.1 **WHEREAS**, the California Department of Transportation (Department) has completed a Final Environmental Impact Report and Addendum pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- Oakland Alameda Access Project: I-880 and SR 260 in Alameda County. Improve motorist, pedestrian, and bicycle safety, reduce conflicts between regional and local traffic, and enhance bicycle and pedestrian accessibility and connectivity within the project area. (PPNO 0044D)
- 1.2 **WHEREAS**, the Department has certified that a Final Environmental Impact Report and Addendum have been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3 **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Final Environmental Impact Report and Addendum; and
- 1.4 **WHEREAS**, the project will have a significant effect on the environment;
- 2.1 **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve of the above referenced project to allow for consideration of funding.

Attachment 1

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: Oakland Alameda Access Project

2017092041	Lindsay Vivian	(510) 506-4310
State Clearinghouse Number	Lead Agency Contact Person	Area Code/Telephone

Project Location (include county): Interstate 880 from postmile (PM) 30.47 to 31.61 and State Route (SR) 260 from PM R0.78 to R1.90, in Alameda County.

Project Description: Improve motorist, pedestrian, and bicycle safety, reduce conflicts between regional and local traffic, and enhance bicycle and pedestrian accessibility and connectivity within the project area by removing and modifying existing freeway ramps, modifying the connection from the Posey Tube to I-880, constructing Class IV two-way cycle tracks in Oakland, implementing various "complete streets" improvements, implementing bicycle and pedestrian improvements at the approaches to the Tubes, and opening the Webster Tube's westside walkway to bicyclists and pedestrians.

This is to advise that the California Transportation Commission has approved the above described project on _____ (Lead Agency/ X Responsible Agency) June 26-27, 2025, and has made the following determinations regarding the above described project:

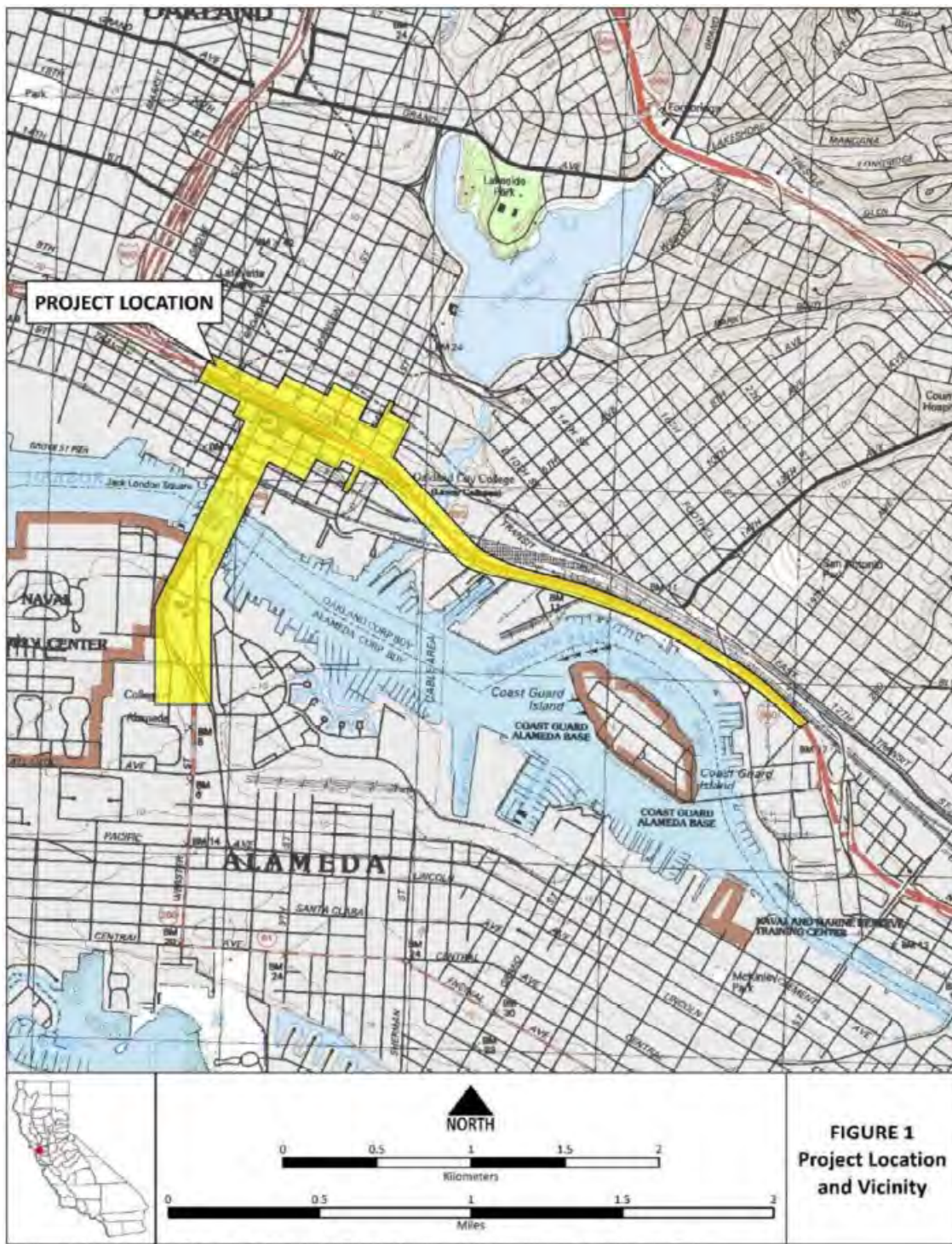
1. The project (X will/ will not) have a significant effect on the environment.
2. X An Environmental Impact Report and Addendum were prepared for this project pursuant to the provisions of CEQA.
 A Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (X were/ were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (X was / was not) adopted for this project.
5. A Statement of Overriding Considerations (X was / was not) adopted for this project.
6. Findings (X were/ were not) made pursuant to the provisions of CEQA.

The above identified document with comments and responses and record of project approval is available to the General Public at: Caltrans District 4, 111 Grand Ave, Oakland, CA 94612.

TANISHA TAYLOR		Executive Director California Transportation Commission
<i>Signature (Public Agency)</i>	<i>Date</i>	<i>Title</i>

Date received for filing at OPR:

Attachment 1



Oakland Alameda Access Project

Alameda I-880 from PM 30.47 to 31.61, Alameda SR 260 from PM R0.78 to R1.90

Attachment 2

CALIFORNIA TRANSPORTATION COMMISSION

Resolution for Future Consideration of Funding

05-Slo-101, PM 16.0/R22.5

Resolution E-25-42A

- 1.1 **WHEREAS**, the California Department of Transportation (Department) has completed a Final Environmental Impact Report and Addendum pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
 - Pismo Congestion Relief Pilot Project: United States Highway 101 (US 101) in San Luis Obispo County. widen the inside shoulder of US 101 from San Luis Creek to the railroad overhead in Pismo Beach, to serve as a travel lane strictly during periods of heavy traffic volumes. (PPNO 2653)
- 1.2 **WHEREAS**, the Department has certified that a Final Environmental Impact Report and Addendum have been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3 **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Final Environmental Impact Report and Addendum; and
- 1.4 **WHEREAS**, the project will have a significant effect on the environment;
- 2.1 **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve of the above referenced project to allow for consideration of funding.

Attachment 2

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: Pismo Congestion Relief Pilot Project

2017091065	Lara Bertaina	(805) 779-0792
State Clearinghouse Number	Lead Agency Contact Person	Area Code/Telephone

Project Location (include county): United States Highway (US) 101 from postmile (PM) 16.0 to R22.5, in San Luis Obispo County.

Project Description: Widen the inside shoulder of US 101 from San Luis Creek to the railroad overhead in Pismo Beach, to serve as a travel lane strictly during periods of heavy traffic volumes.

This is to advise that the California Transportation Commission has approved the above described project on _____ (Lead Agency/ X Responsible Agency) June 26-27, 2025, and has made the following determinations regarding the above described project:

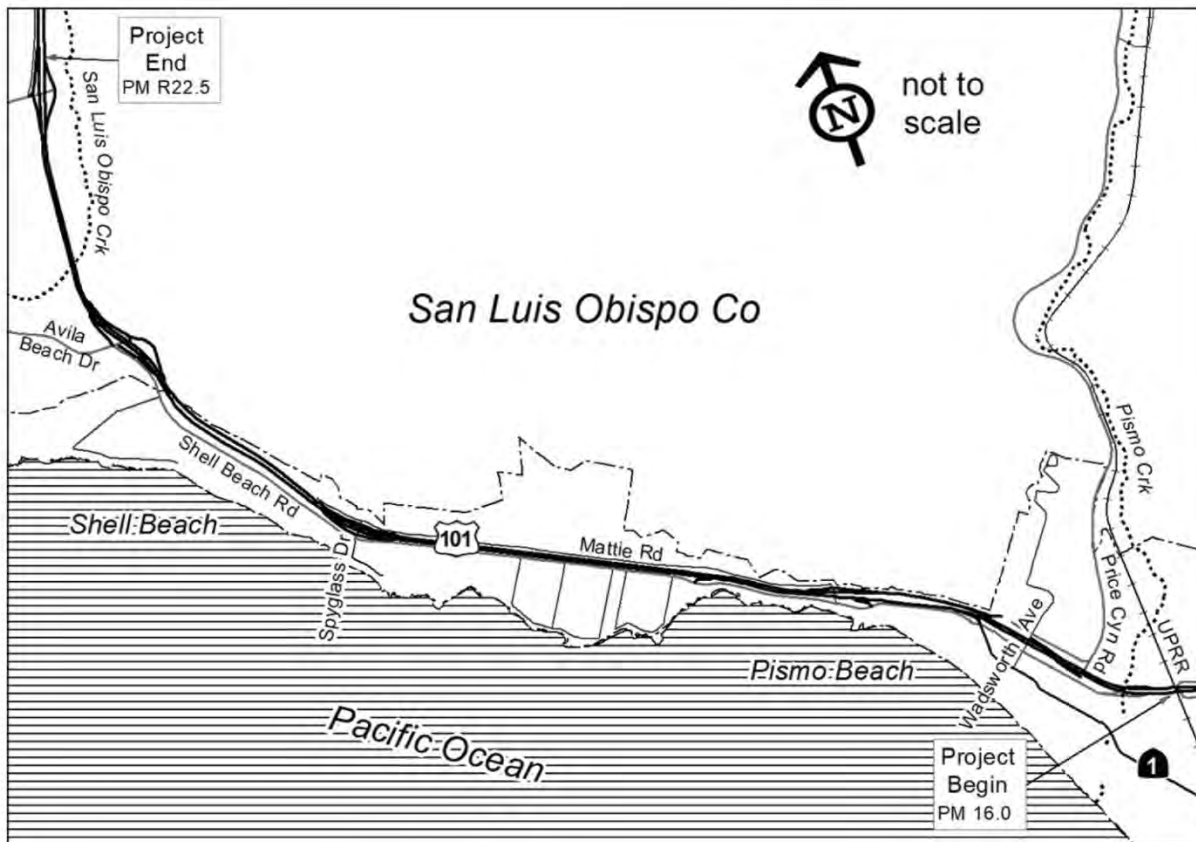
1. The project (X will/ will not) have a significant effect on the environment.
2. X An Environmental Impact Report and Addendum were prepared for this project pursuant to the provisions of CEQA.
 A Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (X were/ were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (X was / was not) adopted for this project.
5. A Statement of Overriding Considerations (X was / was not) adopted for this project.
6. Findings (X were/ were not) made pursuant to the provisions of CEQA.

The above identified document with comments and responses and record of project approval is available to the General Public at: Caltrans District 5, 50 Higuera Street, San Luis Obispo, CA 93401.

TANISHA TAYLOR	Executive Director
Signature (Public Agency)	California Transportation Commission
Date	Title

Date received for filing at OPR:

Attachment 2



Pismo Congestion Relief Pilot Project

Attachment 3

CALIFORNIA TRANSPORTATION COMMISSION

Resolution for Future Consideration of Funding

07-LA-10, PM 44.9/48.3

08-SBd-10, PM 0.0/R37.0

Resolution E-25-43A

- 1.1 **WHEREAS**, the California Department of Transportation (Department) has completed a Final Environmental Impact Report and Addendum pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- Interstate 10 Corridor Project: I-10 in Los Angeles and San Bernardino counties. Construct roadway improvements on a portion of I-10. Specifically, add freeway lanes along the 33-mile segment of I-10 between the Los Angeles and San Bernardino County line and Ford Street. (PPNO 0134K and PPNO 1325)
- 1.2 **WHEREAS**, the Department has certified that a Final Environmental Impact Report and Addendum have been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3 **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Final Environmental Impact Report and Addendum; and
- 1.4 **WHEREAS**, the project will have a significant effect on the environment;
- 2.1 **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve of the above referenced project to allow for consideration of funding.

Attachment 3

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: Interstate 10 Corridor Project

2012101082	Jeanine Porter	(909) 472-1301
State Clearinghouse Number	Lead Agency Contact Person	Area Code/Telephone

Project Location (include county): Interstate 10 (I-10) from postmile (PM) 44.9 to 48.3 in Los Angeles County, and PM 0.0 to R37.0 in San Bernardino County.

Project Description: Construct roadway improvements on a portion of I-10. Specifically, add freeway lanes along the 33-mile segment of I-10 between the Los Angeles and San Bernardino County line and Ford Street.

This is to advise that the California Transportation Commission has approved the above described project on _____ (☐ Lead Agency/ ☒ Responsible Agency) June 26-27, 2025, and has made the following determinations regarding the above described project:

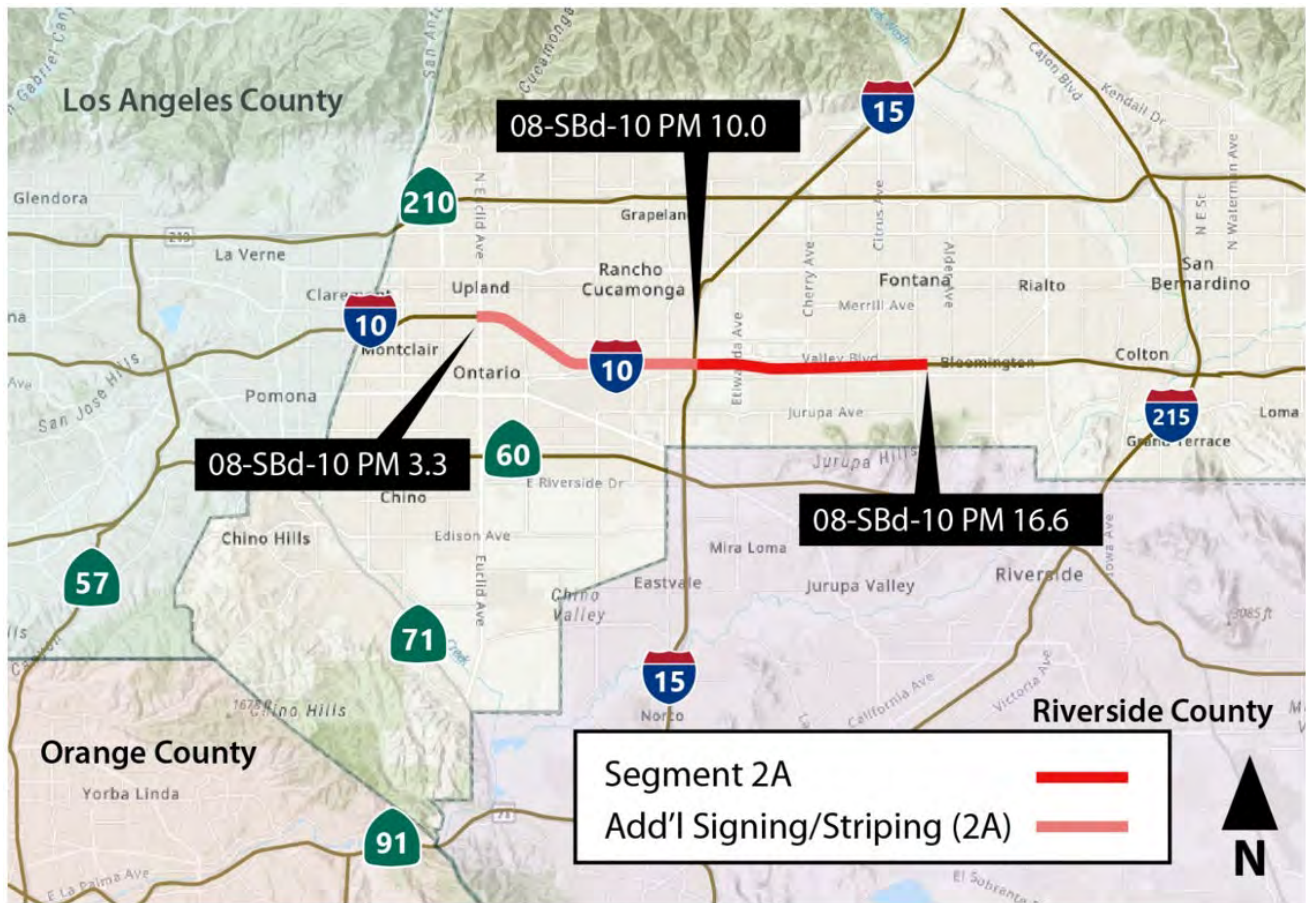
1. The project (☒ will/ ☐ will not) have a significant effect on the environment.
2. ☒ An Environmental Impact Report and Addendum were prepared for this project pursuant to the provisions of CEQA.
☐ A Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (☒ were/ ☐ were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (☒ was / ☐ was not) adopted for this project.
5. A Statement of Overriding Considerations (☒ was / ☐ was not) adopted for this project.
6. Findings (☒ were/ ☐ were not) made pursuant to the provisions of CEQA.

The above identified document with comments and responses and record of project approval is available to the General Public at: Caltrans District 8, 464 W. 4th Street, San Bernardino, CA 92401.

TANISHA TAYLOR	Executive Director
Signature (Public Agency)	California Transportation Commission
Date	Title

Date received for filing at OPR:

Attachment 3



Interstate 10 Corridor Project

Attachment 4

CALIFORNIA TRANSPORTATION COMMISSION

Resolution for Future Consideration of Funding

08-Riv-60, PM 28.03/30.42

Resolution E-25-44A

- 1.1 WHEREAS**, the California Department of Transportation (Department) has completed a Mitigated Negative Declaration and Addendum pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- State Route 60/Potrero Boulevard New Interchange Project: SR 60, in Riverside County. Construction of a new 6-lane bridge overcrossing at SR 60 with a temporary connection to Western Knolls Avenue; construction of westbound and eastbound diagonal and loop entry ramps, extended ramp acceleration and deceleration lanes, realignment of Western Knolls Avenue, and removal of Western Knolls Avenue connections to SR 60. (PPNO 1321)
- 1.2 WHEREAS**, the Department has certified that a Mitigated Negative Declaration and Addendum has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3 WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Mitigated Negative Declaration and Addendum; and
- 1.4 WHEREAS**, the project will not have a significant effect on the environment;
- 2.1 NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for consideration of funding.

Attachment 4

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: State Route 60/Potrero Boulevard New Interchange Project

2012051053	Boniface Udotor	(909) 888-2347
State Clearinghouse Number	Lead Agency Contact Person	Area Code/Telephone

Project Location (include county): State Route (SR) 60 in Riverside County from post mile 28.03 to 30.42.

Project Description: Construction of a new 6-lane bridge overcrossing at SR 60 with a temporary connection to Western Knolls Avenue; construction of westbound and eastbound diagonal and loop entry ramps, extended ramp acceleration and deceleration lanes, realignment of Western Knolls Avenue, and removal of Western Knolls Avenue connections to SR 60.

This is to advise that the California Transportation Commission has approved the above described project on (☐ Lead Agency/ ☒ Responsible Agency)

June 26-27, 2025, and has made the following determinations regarding the above described project:

1. The project (☐ will/ ☒ will not) have a significant effect on the environment.
2. ☐ An Environmental Impact Report was prepared for this project pursuant to the provisions of CEQA.
☒ A Mitigated Negative Declaration and Addendum was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (☒ were/ ☐ were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (☒ was / ☐ was not) adopted for this project.
5. A Statement of Overriding Considerations (☐ was / ☒ was not) adopted for this project.
6. Findings (☐ were/ ☒ were not) made pursuant to the provisions of CEQA.

The above identified document with comments and responses and record of project approval is available to the General Public at: Caltrans District 8, 464 W. 4th Street, San Bernardino, CA 92401.

TANISHA TAYLOR

Signature (Public Agency)

Date

Executive Director

California Transportation Commission

Title

Date received for filing at OPR:

Attachment 4



State Route 60/Potrero Boulevard New Interchange Project

Attachment 5

CALIFORNIA TRANSPORTATION COMMISSION

Resolution for Future Consideration of Funding

12-Ora-5, PM 21.3/R25.8

Resolution E-25-45A

- 1.1 **WHEREAS**, the California Department of Transportation (Department) has completed a Mitigated Negative Declaration and Addendum pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- Interstate 5 (I-5) Improvement Project from Interstate 405 (I-405) to State Route 55 (SR-55): I-5 in Orange County. Widen I-5 between I-405 and SR 55 by adding one additional lane in each direction of travel (northbound and southbound). (PPNO 2743)
- 1.2 **WHEREAS**, the Department has certified that a Mitigated Negative Declaration and Addendum has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3 **WHEREAS**, the California Transportation Commission, as a responsible agency, has considered the information contained in the Mitigated Negative Declaration and Addendum; and
- 1.4 **WHEREAS**, the project will not have a significant effect on the environment;
- 2.1 **NOW, THEREFORE, BE IT RESOLVED** that the California Transportation Commission does hereby approve the above referenced project to allow for consideration of funding.

Attachment 5

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: Interstate 5 (I-5) Improvement Project from Interstate 405 (I-405) to State Route 55 (SR-55)

2018051014	Brian Liu	(657) 328-6135
State Clearinghouse Number	Lead Agency Contact Person	Area Code/Telephone

Project Location (include county): Interstate 5 (I-5) in Orange County from post mile 21.3 to R25.8.

Project Description: Widen I-5 between I-405 and SR 55 by adding one additional lane in each direction of travel (northbound and southbound).

This is to advise that the California Transportation Commission has approved the above described project on (☐ Lead Agency/ ☒ Responsible Agency)

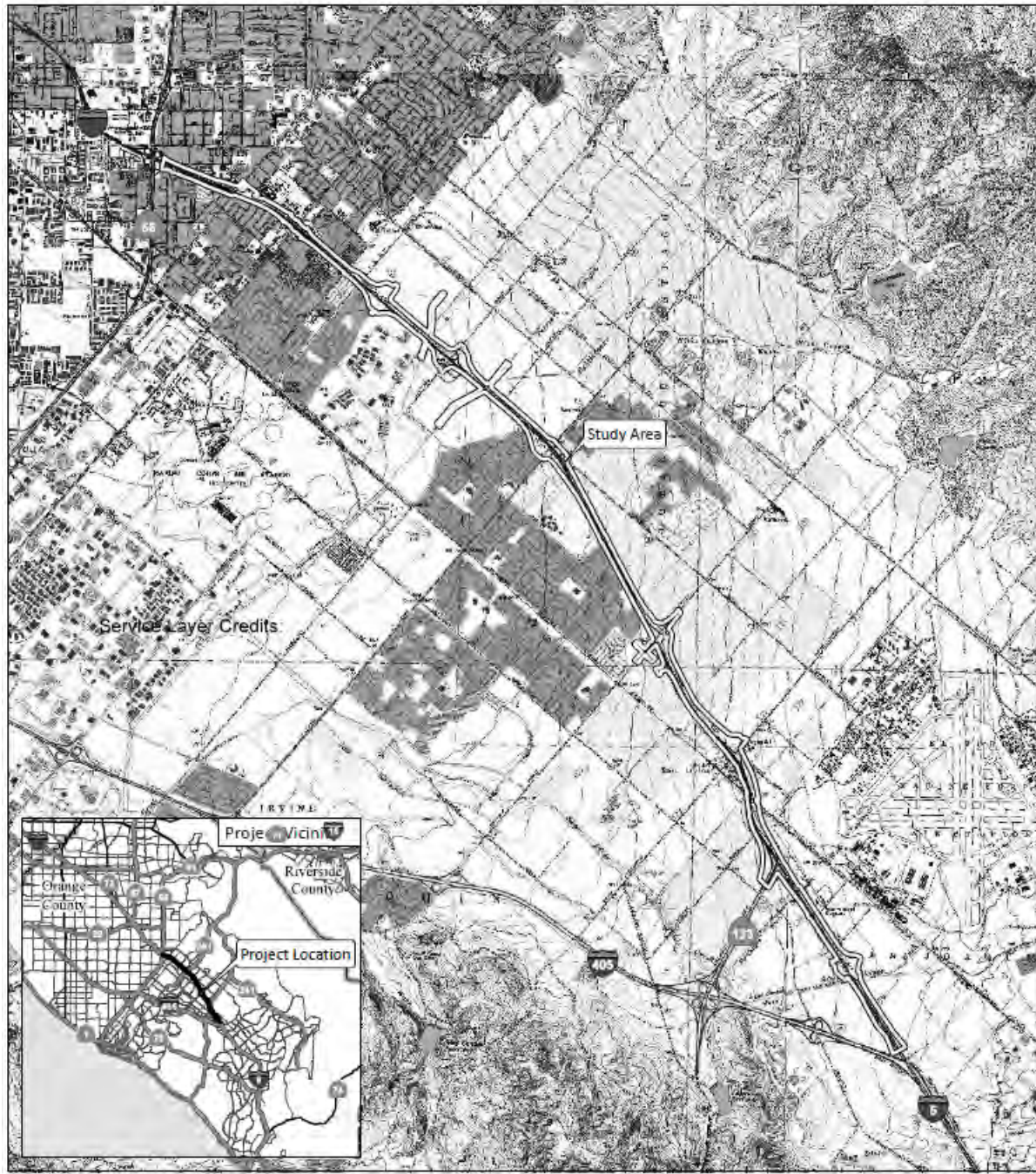
June 26-27, 2025, and has made the following determinations regarding the above described project:

1. The project (☐ will/ ☒ will not) have a significant effect on the environment.
2. ☐ An Environmental Impact Report was prepared for this project pursuant to the provisions of CEQA.
☒ A Mitigated Negative Declaration and Addenda were prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (☒ were/ ☐ were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (☒ was / ☐ was not) adopted for this project.
5. A Statement of Overriding Considerations (☐ was / ☒ was not) adopted for this project.
6. Findings (☐ were/ ☒ were not) made pursuant to the provisions of CEQA.

The above identified document with comments and responses and record of project approval is available to the General Public at: Caltrans District 12, 1750 East 4th Street, Suite 100, Santa Ana, CA 92705.

TANISHA TAYLOR		Executive Director
<i>Signature (Public Agency)</i>	<i>Date</i>	California Transportation Commission
		<i>Title</i>

Date received for filing at OPR:



LEGEND

Study Area

FIGURE I-1

I-5 Improvement Project: I-405 to SR-55

Interstate 5 (I-5) Improvement Project from Interstate 405 (I-405) to State Route 55 (SR-55) from PM 21.3 to R25.8

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.3c., Action Item

Prepared By: Eric Souza, Chief (Acting)
Division of Design

Subject: **RELINQUISHMENT RESOLUTION**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for a relinquishment resolution that will transfer highway facilities no longer needed for the State Highway System to the local agency identified in the summary?

RECOMMENDATION:

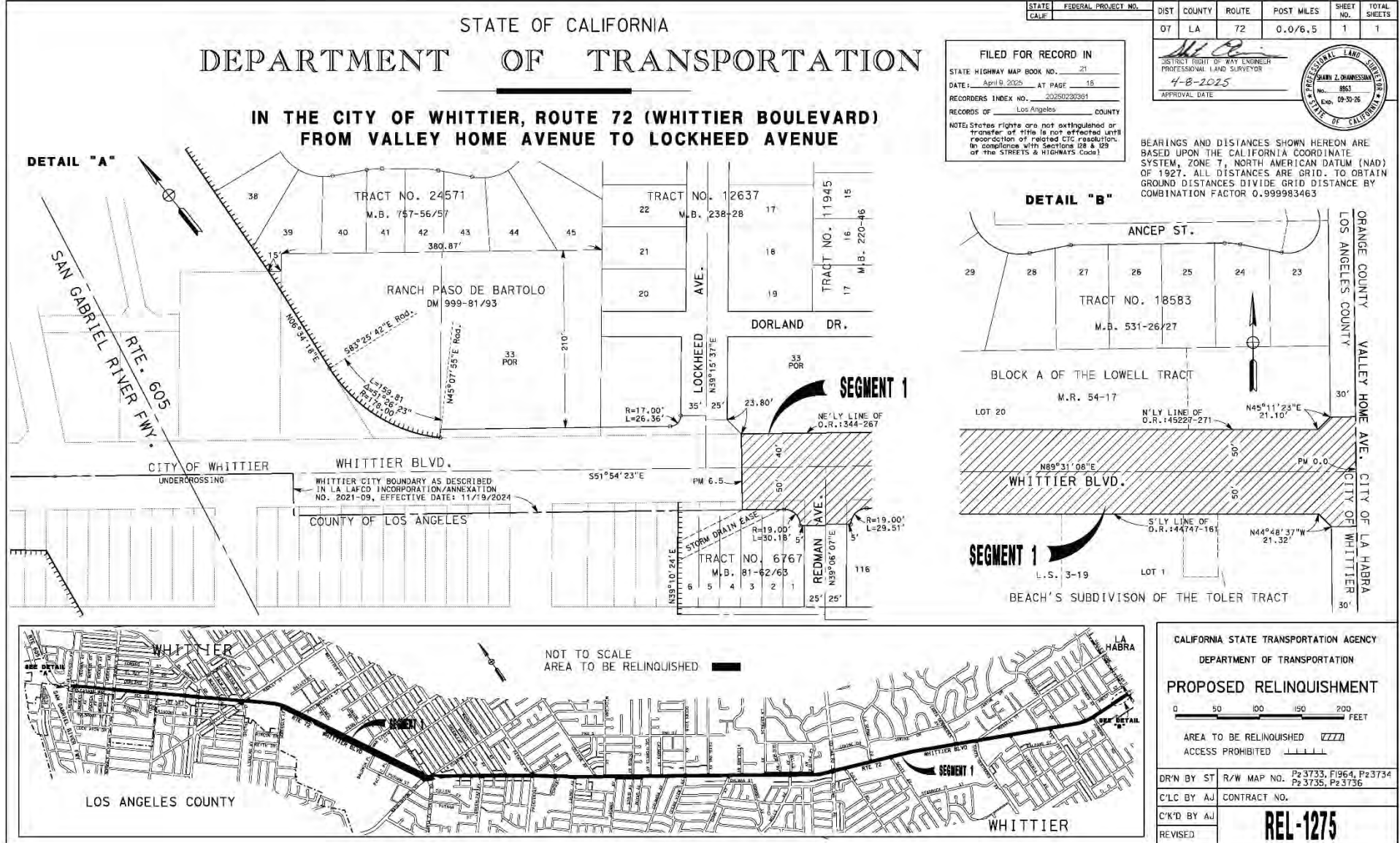
The Department recommends that the Commission approve the relinquishment resolution, summarized below, that will transfer highway facilities no longer needed for the State Highway System to the local agency identified in the summary. It has been determined by the Department that each facility in the specific relinquishment resolution summarized below may be disposed of by relinquishment. Upon the recording of the approved relinquishment resolution in the county where the facilities are located, all rights, title, and interest of the State in and to the facilities to be relinquished will be transferred to the local agency identified in the summary. The facilities are safe and drivable. The local authority has been advised of the pending relinquishment a minimum of 90 days before the Commission meeting pursuant to Section 73 of the Streets and Highways Code. Any exceptions or unusual circumstances are described in the individual summary.

RESOLUTION

Resolution R-4121 – 07-LA-72-PM 0.0/6.5
(Request No. 1275) – 1 Segment

Relinquishes right of way in the city of Whittier along State Route 72 (Whittier Boulevard) from Lockheed Avenue to Valley Home Avenue, under the terms and conditions as stated in the Relinquishment Agreement dated March 27, 2025 and determined to be in the best interest of the State. The relinquishment is authorized by Chapter 636, Statutes of 2019, which amended Section 372 of the Streets and Highways Code. See Attachment 1 for the relinquishment location.

Attachment 1 – Relinquishment Location Map



MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.4b., Action Item

Prepared By: René Fletcher, Chief (Acting)
Division of Right of Way and Land Surveys

Subject: **RESOLUTIONS OF NECESSITY**

ISSUE:

Should the California Transportation Commission (Commission) adopt Resolutions of Necessity (Resolutions) for these parcels, whose Owners are not contesting the declared findings of the California Department of Transportation (Department) under Section 1245.230 of the Code of Civil Procedure?

RECOMMENDATION:

The Department recommends that the Commission adopt Resolutions C-22556 through C-22558 as summarized on the following pages.

BACKGROUND:

Prior to initiating Eminent Domain proceedings to acquire needed right of way for a programmed project, the Commission must first adopt a Resolution stipulating specific findings identified under Section 1245.230 of the Code of Civil Procedure of California (CCP). Moreover, for each of the proposed Resolutions, the property Owners are not contesting the following findings contained in Section 1245.230 of the CCP:

1. The public interest and necessity require the proposed project.
2. The proposed project is planned and located in a manner that will be most compatible with the greatest public good and the least private injury.
3. The property is necessary for the proposed project.
4. An offer to purchase the property in compliance with Government Code Section 7267.2 has been made to the Owner of record.

The only remaining issues with the property Owners are related to compensation.

Discussions have taken place with the Owners, each of whom has been offered the full amount of the Department's appraisal and, where applicable, advised of any relocation assistance benefits to which the Owners may subsequently be entitled. Adoption of the Resolutions will not interrupt the Department's efforts to secure equitable settlements. In accordance with statutory requirements, each Owner has been advised that the Department is requesting a Resolution at this time. Adoption will assist the Department in the continuation of the orderly sequence of events required to meet construction schedules.

C-22556 - WILLIAM A. DRIVER, TRUSTEE OF THE WILLIAM A. DRIVER REVOCABLE TRUST, DATED OCTOBER 5, 2006

03-Sut-113-Post Mile (PM) 0.6 - Parcel 38903-1, 2 - EA 2J9809.

Right of Way Certification (RWC) Date: 12/15/25; Ready to List (RTL) Date: 01/10/26.

Conventional highway – curve realignment near Knights Landing, from north of Yolo County line to south of Leiser Road. Authorizes condemnation of land in fee for a State highway, and a Temporary Construction Easement (TCE) needed to provide space for the State's contractor to construct the improvements. Located in Sutter County at 21536 State Highway 113.

Assessor's Parcel Numbers (APN's) 034-030-001, 034-030-014 & 034-050-001.

The public interest and necessity require the proposed project.

State Route (SR) 113 is a north-south route that extends 38.3 miles from Interstate 80 (I-80), near the City of Davis, to the SR 99/SR 113 junction, 10 miles south of Yuba City. This stretch of SR 113 is outside of the urbanized areas of Davis and Woodland but provides access from the rural communities of Knights Landing and Robbins to I-80 and SR 99, serving a combination of commuter and, primarily, agricultural traffic.

This project was initiated by Traffic Safety due to the high number of collisions along the horizontal roadway curve near the mobile home park in the project limits. In May of 2019, a traffic investigation was conducted after an errant vehicle travelling northbound departed the lane and shoulder, colliding with an occupied residence in the mobile home park. This collision resulted in three fatalities. In 2021, guardrail was constructed along the northbound shoulder as a safety measure to protect the more vulnerable homes within the mobile home park at PM 0.7.

The primary purpose of this project is to improve safety by flattening the curve radius and realigning the horizontal curve away from the mobile home park. Flattening curves is a Federal Highway Administration Proven Safety Countermeasure.

The proposed project is planned or located in the manner that will be most compatible with the greatest public good and least private injury.

The Department completed an Initial Study (IS) for the California Environmental Quality Act (CEQA) and a Categorical Exclusion (CE) for the National Environmental Policy Act (NEPA) on March 13, 2024. The documentation level has been selected based upon Environmental Study Request level studies.

Two alternatives were evaluated including the build and no-build alternative. The Project Development Team analyzed the alternatives to minimize the impact to the environment, reduce constructability issues, and improve safety to the greatest reasonable extent.

The build alternative, the chosen alternative, proposes to flatten the curve radius and realign the horizontal curve away from the mobile home park. Private injury has been minimized by acquiring the smallest footprint needed for construction requirements.

It was determined that the no-build alternative would not meet the purpose and need of the project. If the existing condition remains unaddressed, it is likely to lead to future collisions and pose a risk of injury to individuals.

The property rights to be condemned are necessary for the proposed project.

The owner's parcels are located along the west side of SR 113, approximately 0.7 miles north of the town of Knights Landing. The realignment of the roadway requires the acquisition of additional right of way. The total area of the subject parcel is 223.87 acres, or 9,751,777 square feet. Parcel 38903-1 is 104,413 square feet of fee. Parcel 38903-2 is 113,016 square feet for a TCE.

The fee area of the parcel is necessary for the roadway realignment and the TCE will provide sufficient working room for the improvements. The project improvements at this location cannot be constructed without acquiring the property.

An offer to purchase in accordance with Government Code Section 7267.2 has been made to the owners of record.

A staff appraisal that established just compensation was approved by District 3 Right of Way management on January 3, 2025. On February 7, 2025, the property owner was provided the First Written Offer (FWO) of just compensation. The Owner expressed dissatisfaction with the amount of just compensation and has declined to obtain an independent appraisal.

Negotiations are ongoing and efforts to secure an equitable settlement will continue. Condemnation has been initiated to maintain the orderly sequence of events required to meet construction schedules.

C-22557 - SB Redlands Owner, LLC, a Delaware limited liability company
08-SBd-38-PM 1.46 - Parcel 25920-1, 2, 3 - EA 1K8609.

RWC Date: 11/03/25; RTL Date: 12/04/25. Conventional highway - pavement rehabilitation, upgrading Traffic Management System (TMS) items, upgrading curb ramps and sign panels, installing fiber optic cable, and constructing pedestrian and bicycle infrastructure. Authorizes condemnation of land in fee for a State highway, a temporary easement for construction purposes, and underlying fee. Located in the city of Redlands at the Southeast corner of East Lugonia Avenue and North University Street. APNs 1212-381-01 through 32.

The public interest and necessity require the proposed project.

The project is located on SR 38, in San Bernardino County, in the City of Redlands, from Occidental Drive, PM 1.3, to Crafton Avenue, PM 4.1. The project scope includes repairing distressed pavement areas with digouts, upgrading TMS elements, bicycle and pedestrian improvements, upgrading curb ramps for compliance with the Americans with Disabilities Act (ADA), adding concrete bus pads, minor widening, and upgrading sign panels.

The project area has deficiencies in the pavement condition and the pavement within the project limits is exhibiting distress resulting in poor ride quality. If left uncorrected, the pavement will deteriorate and will need a major roadway rehabilitation. Existing curb ramps need upgrading to meet ADA standards. In addition, TMS elements, such as some traffic signals, cabinets, loop detectors, pedestrian heads, and curb ramps within the project limits, will need to be upgraded to meet current standards.

SR 38 from PM 1.3 to 4.1 has been identified by the Department to require pavement preservation to avoid major roadway rehabilitation. The project proposes to cold plane and overlay existing pavement, upgrade curb ramps to ADA standards, upgrade sign panels and TMS elements. The project will extend the service life of existing pavement, maximize mobility and accessibility for all people and goods in the region, maximize the productivity of our transportation system, and improve the ride quality along this segment of SR 38.

The proposed project is planned or located in the manner that will be most compatible with the greatest public good and least private injury.

A Categorical Exemption (CE) pursuant to CEQA and a CE pursuant to NEPA were completed and approved on January 26, 2024. An Initial Site Assessment for hazardous waste was also completed. Other studies included impacts to air quality, biological and cultural resources, noise, and stormwater.

The build alternative will cold plane and overlay existing pavement at various locations. In addition, curb ramps will also be upgraded to the current standard. New sidewalk, accessible paths, and bus pads will be constructed. Existing sidewalks will be repaired or reconstructed and driveways will be modified. In addition, the proposed improvements include (but are not limited to) upgrading TMS elements, incorporating Class II bicycle route treatments, installing new bike signs, replacing Accessible Pedestrian Signals, incorporating visibility treatment for crosswalks, upgrading sign panels, and minor widening.

To minimize the impact of the project scope on private properties, the project was designed with the least necessary acquisition areas to build the proposed improvements. Non-standard design measures included non-standard lane widths by keeping existing widths or reducing widths to 11 feet at various locations within the route. These measures significantly reduced the private property necessary for the project.

The property rights to be condemned are necessary for the proposed project.

The subject property is located in the city of Redlands at the Southeast corner of East Lugonia Avenue and North University Street, adjacent to SR 38. The property consists of 7.789 acres and is improved with an apartment complex. This project requires 101 square feet of fee from the Owner's property adjacent to the State right of way which is necessary for the construction of a curb ramp to meet current ADA standards. Underlying fee consisting of 34,308 square feet within the adjacent SR 38 is also required to perfect the State's ownership of the highway, along with a TCE measuring 137 square feet which is necessary to allow the contractor a sufficient work area to upgrade the curb ramp located within the project limits to ADA standards. The proposed project improvements cannot be constructed without acquiring a portion of the Owner's property.

An offer to purchase in accordance with Government Code Section 7267.2 has been made to the Owners of record.

A staff appraisal that established just compensation was approved by District 8 Right of Way management on June 3, 2024. On July 12, 2024, the FWO of just compensation was sent via certified mail to the Owner and was acknowledged as received. Negotiations are ongoing, but the Department and the Owner are currently at an impasse due to compensatory issues.

Condemnation has been initiated to maintain the orderly sequence of events required to meet construction schedules.

C-22558 - Pacific/Costanzo-Lewis, a California general partnership

08-SBd-247-PM 77.48 - Parcel 26038-1 - EA 1L0909.

RWC Date: 02/17/26; RTL Date: 03/11/26. Conventional highway - pavement rehabilitation, replace sign panels, add bike lanes and upgrade facilities to ADA standards. Authorizes condemnation of land in fee for a State highway. Located in the city of Barstow at 1380 Barstow Road. APN 0183-201-02.

The public interest and necessity require the proposed project.

The project is located on SR 247, in San Bernardino County, in the City of Barstow, from Stoddard Wells Road (PM 73.2) to Route 15 (PM 78.1). The proposed scope of work includes constructing sidewalks at sidewalk gaps, localized dig-outs, restriping all lanes, replacing sign panels, upgrading traffic signals at Armory Road and Rimrock Road intersections, and five to six-foot shoulder widening to accommodate buffered Class II bike lanes. The bike lanes are proposed in both directions of SR 247, from Veterans Parkway to Armory Road. Other assets include constructing three bus pads, rumble strips, and upgrading the Metal Beam Guardrail (MBGR) to a Midwest Guardrail System (MGS) at the SR 247/I-15 separation near the NB I-15 on-ramp loop entrance. A flashing beacon warning curve sign will also be installed at PM 74.5 and shoulder backing will be placed, as needed, at the drop offs.

The existing pavement is exhibiting signs of distress and deterioration at various locations along SR 247. As indicated in the Pavement Management System (PaveM), there are areas of alligator B cracking, rutting and surface roughness requiring pavement rehabilitation to extend pavement life and improve ride quality.

Existing sidewalks and curb ramps at several intersections do not meet the latest ADA standards. Reconstruction of these curb ramps is necessary to make the following features standard: Curb ramp dimensions, slopes, detectable warning surfaces and pedestrian push buttons. Several locations within the project limits lack complete streets elements as well. To encourage other modes of transportation, it is proposed to construct new sidewalks and bus pads. Bike routes will be provided through the installation of Buffered Class II bike lanes in both directions from Veterans Parkway to Armory Road. This will help bicyclists reach their destination and encourage the use of active transportation.

The proposed project is planned or located in the manner that will be most compatible with the greatest public good and least private injury.

A CE/CE pursuant to CEQA and NEPA was completed and approved on February 14, 2024. An Initial Site Assessment for hazardous waste was also completed. Other studies included impacts to air quality, hazardous waste, biological and cultural resources, noise, visual impacts, and stormwater.

The build alternative will cold plane and overlay existing pavement at various locations. In addition, the proposed improvements include (but are not limited to) replacing sign panels, upgrading traffic signals at Armory Road/Rimrock Road intersection, reconstructing existing curb ramps to comply with current ADA standards, constructing new curb ramps at identified locations, upgrading existing curb and gutter, installing sidewalk to current ADA standards, upgrading MBGR to MGS and installing buffered Class II bike lanes.

"Improving lives and communities through transportation."

Considerations of non-standard features were made to minimize impacts to the subject property; however a standard curb ramp design was required at this location to ensure safe and accessible pedestrian access.

The property rights to be condemned are necessary for the proposed project.

The subject property is located in the city of Barstow at 1380 Barstow Road, adjacent to SR 247. The property consists of 2.12 acres or 92,347 square feet, and is improved with a Rite-Aid store. This project requires 429 square feet of fee from the Owner's property adjacent to the State right of way which is necessary to install an ADA curb ramp. The proposed project improvements cannot be constructed without acquiring a portion of the Owner's property.

An offer to purchase in accordance with Government Code Section 7267.2 has been made to the Owners of record.

A staff appraisal that established just compensation was approved by District 8 Right of Way management on September 23, 2024. On October 16, 2024, the FWO of just compensation was sent via certified mail to the Owner at the address of record and was confirmed as received on October 22, 2024. The offer was also emailed to the Owner per their request on October 18, 2024, and was received the same day. Negotiations ensued for the next few months, however the Owner repeatedly indicated that they needed more time to review the offer. The District Right of Way office repeatedly reached out to the Owner between December 2024 to March 2025, leaving phone messages in an attempt to negotiate, however the Owner has been non-responsive. Efforts to contact the Owner will continue, however the parties are currently at an impasse due to Owner's refusal to communicate.

Condemnation has been initiated to maintain the orderly sequence of events required to meet construction schedules.

Attachments

TRANSPORTATION COMMISSION
RESOLUTION NO.

C-22556

CALIFORNIA TRANSPORTATION COMMISSION
RESOLUTION OF NECESSITY
TO ACQUIRE CERTAIN REAL PROPERTY
OR INTEREST IN REAL PROPERTY BY EMINENT DOMAIN
HIGHWAY 03-Sut-113-PM 0.6 PARCEL 38903-1, 2
OWNER: WILLIAM A. DRIVER, TRUSTEE OF THE WILLIAM A. DRIVER
REVOCABLE TRUST, DATED OCTOBER 5, 2006

Resolved by the California Transportation Commission after notice (and hearing)
pursuant to Code of Civil Procedure Section 1245.235 that it finds and determines and
hereby declares that:

The hereinafter described real property is necessary for State Highway purposes
and is to be acquired by eminent domain pursuant to Streets and Highways Code Section
102; and Code of Civil Procedure Section 1240.510 in that the property being acquired is
for a compatible use;

The public interest and necessity require the proposed public project, namely a State
highway;

The proposed project is planned and located in the manner that will be most
compatible with the greatest public good and the least private injury;

The property sought to be acquired and described by this resolution is necessary for
the public project;

The offer required by Section 7267.2 of the Government Code has been made to the
owner or owners of record; and be it further

RESOLVED by this Commission that the Department of Transportation be and said
Department is hereby authorized and empowered;

APPROVED AS TO FORM AND PROCEDURE

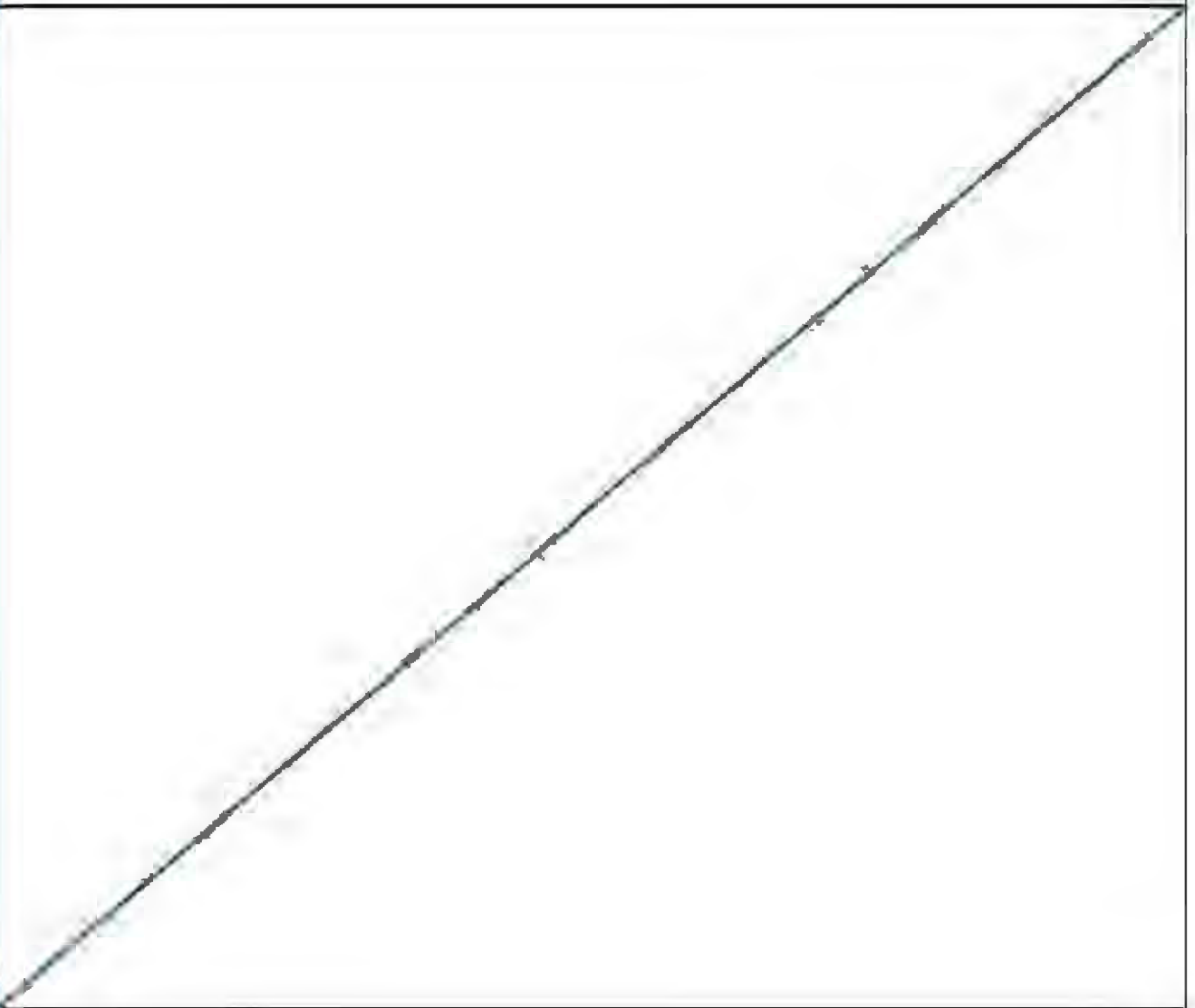
APPROVAL RECOMMENDED

Attorney, Department of Transportation

DIVISION OF RIGHT OF WAY

1 To acquire, in the name of the People of the State of California, in fee simple
2 absolute, unless a lesser estate is hereinafter expressly described, the said hereinafter
3 described real property, or interests in real property, by condemnation proceeding or
4 proceedings in accordance with the provisions of the Streets and Highways Code, Code of
5 Civil Procedure and of the Constitution of California relating to eminent domain;

6 The real property or interests in real property, which the Department of
7 Transportation is by this resolution authorized to acquire, is situated in the County of Sutter,
8 State of California, Highway 03-Sut-113 and described as follows:



Parcel 38903-1

A portion of that real property described in that Grant Deed recorded September 6, 2022, in Document Number 2022-0012161, Official Records of Sutter County, being a portion of Section 11, Township 11 North, Range 2 East, Mount Diablo Base and Meridian, County of Sutter, State of California, lying easterly of the following described line:

COMMENCING at a point on the existing centerline of California State Route 113, said point marking the Southerly terminus of the course described as "N 41° 16' E 1532.14'" as shown on California Department of Transportation Record Map 004323R, on file at the Caltrans District 3 Office,

THENCE (1) leaving said centerline North 38° 07' 34" East, 774.37 feet, to a point on the existing Westerly Right of Way line of California State Route 113, being the point of beginning, said point bears North 84° 21' 35" West, 4981.20 feet from a brass disk set in concrete in a monument well, said brass disk stamped "T11N, R2E 1/4 S12/S13 2019 SCPW", set per Sutter County Corner Record 2020-06.

THENCE (2) Leaving said Westerly Right of Way line North 48° 54' 47" West, 14.33 feet;

THENCE (3) North 41° 25' 33" East, 598.92 feet;

THENCE (4) along a tangent curve to the left, having a radius of 2000.00 feet, a length of 1868.30 feet, an internal angle of 53° 31' 22", and a chord of North 14° 39' 52" East, 1801.10 feet;

THENCE (5) North 12° 05' 49" West, 1039.08 feet,

THENCE (6) North 77° 29' 12" East, 18.83 feet, to a point on said Westerly Right of Way line, said point bears South 9° 44' 06" East, 1603.55 feet from a 3/4" iron pipe set in concrete, as shown on the Record of Survey filed in Book 18 of Record of Surveys, at page 180, Sutter County Official Records, said 3/4" iron pipe bears North 40° 55' 05" West, 7001.99 feet from the abovementioned brass disk set in concrete in a monument well.

The bearings and distances used in this description are on the California Coordinate System of 1983, Epoch 2017.50, Zone 2. Divide distances by 0.999926 to obtain ground distances.

Parcel 38903-2

An easement for temporary construction purposes being a portion of that real property described in that Grant Deed recorded September 6, 2022, in Document Number 2022-0012161, Official Records of Sutter County, being a portion of Section 11, Township 11 North, Range 2 East, Mount Diablo Base and Meridian, County of Sutter, State of California, lying easterly of the following described line:

COMMENCING at a point on the existing centerline of California State Route 113, said point marking the Southerly terminus of the course described as "N 41° 16' E 1532.14'" as shown on California Department of Transportation Record Map 004323R, on file at the Caltrans District 3 Office,

THENCE (1) leaving said centerline North 38° 53' 12" East, 1586.59 feet, to the point of beginning, said point bears North 75° 53' 45" West, 4577.12 feet from a brass disk set in concrete in a monument well, said brass disk stamped "T11N, R2E 1/4 S12/S13 2019 SCPW", set per Sutter County Corner Record 2020-06.

THENCE (2) along a non-tangent curve to the left, having a radius of 1867.31 feet, a length of 280.19 feet, an internal angle of 8° 35' 50", and a chord of North 27° 48' 51" East, 279.93 feet;

THENCE (3) North 64° 14' 38" West, 36.08 feet;

THENCE (4) along a non-tangent curve to the left, having a radius of 1867.31 feet, a length of 1325.78 feet, an internal angle of 40° 40' 47", and a chord of North 7° 57' 30" East, 1298.11 feet;

THENCE (5) North 12° 30' 56" West, 823.88 feet,

THENCE (6) North 77° 29' 12" East, 46.90 feet;

THENCE (7) North 12° 30' 48" West, 230.89 feet;

THENCE (8) North 77° 29' 12" East, 10.92 feet;

THENCE (9) South 12° 05' 49" East, 1039.08 feet;

THENCE (10) along a tangent curve to the right, having a radius of 2000.00 feet, a length of 1654.78 feet, an internal angle of 47° 24' 21", and a chord of South 11° 36' 22" West, 1607.98 feet to the point of beginning.

The bearings and distances used in this description are on the California Coordinate System of 1983, Epoch 2017.50, Zone 2. Divide distances by 0.999926 to obtain ground distances.

The rights to the above-described temporary easement shall cease and terminate no later than May 1, 2028. Said rights may also be terminated prior to stated date by the STATE upon notice to OWNER.

**TRANSPORTATION COMMISSION
RESOLUTION NO.**

C-22557

**CALIFORNIA TRANSPORTATION COMMISSION
RESOLUTION OF NECESSITY
TO ACQUIRE CERTAIN REAL PROPERTY
OR INTEREST IN REAL PROPERTY BY EMINENT DOMAIN
HIGHWAY 08-SBd-38-PM 1.46 PARCEL 25920-1, 2, 3
OWNER: SB Redlands Owner, LLC, a Delaware limited liability company**

Resolved by the California Transportation Commission after notice (and hearing) pursuant to Code of Civil Procedure Section 1245.235 that it finds and determines and hereby declares that:

The hereinafter described real property is necessary for State Highway purposes and is to be acquired by eminent domain pursuant to Streets and Highways Code Section 102 and Code of Civil Procedure Section 1240.510 in that the property being acquired is for a compatible use;

The public interest and necessity require the proposed public project, namely a State highway;

The proposed project is planned and located in the manner that will be most compatible with the greatest public good and the least private injury;

The property sought to be acquired and described by this resolution is necessary for the public project;

The offer required by Section 7267.2 of the Government Code has been made to the owner or owners of record; and be it further

RESOLVED by this Commission that the Department of Transportation be and said Department is hereby authorized and empowered;

APPROVED AS TO FORM AND PROCEDURE

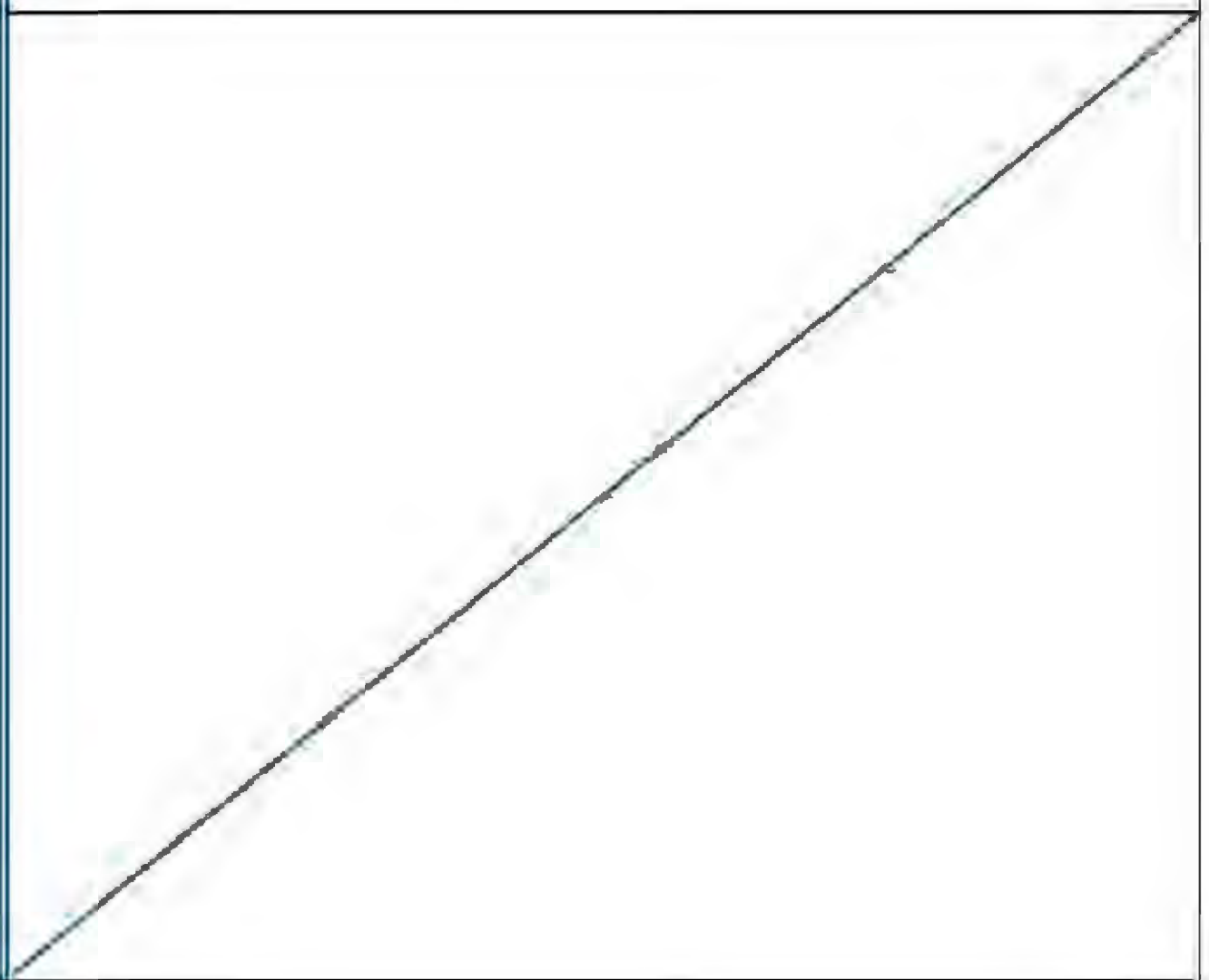
APPROVAL RECOMMENDED

Attorney, Department of Transportation

DIVISION OF RIGHT OF WAY

1 To acquire, in the name of the People of the State of California, in fee simple
2 absolute, unless a lesser estate is hereinafter expressly described, the said hereinafter
3 described real property, or interests in real property, by condemnation proceeding or
4 proceedings in accordance with the provisions of the Streets and Highways Code, Code of
5 Civil Procedure and of the Constitution of California relating to eminent domain;

6 The real property or interests in real property, which the Department of
7 Transportation is by this resolution authorized to acquire, is situated in the County of San
8 Bernardino, State of California, Highway 08-SBd-38 and described as follows:



Legal Description

PARCEL: 25920-1

For state highway purposes, that portion of Common Lot 32 as shown on AMENDING MAP TRACT NO. 10356 in the City of Redlands, County of San Bernardino, State of California, as shown on a map filed in Book 164, Pages 6 through 8 of Maps, in the Office of the County Recorder of said County, lying northerly, northwesterly, and westerly of the following described line:

COMMENCING at the intersection of the centerline of Lugonia Avenue (State Route 38) and the northerly prolongation of the westerly line of Lot 54 of University Tract as shown on map filed in Book 17, Page 36 of Maps, in the office of the County Recorder of said County and also as shown on said AMENDING MAP TRACT NO. 10356; thence along said northerly prolongation and the westerly line of said Lot 54 South 00°05'35" East 72.32 feet; thence North 89°54'25" East 52.00 feet to the westerly line of said Common Lot 32, being the **POINT OF BEGINNING**; thence North 89°54'25" East 3.00 feet to the beginning of a non-tangent curve, concave southeasterly, having a radius of 20.00 feet, to which a radial line bears South 89°54'25" West; thence northeasterly along said curve through a central angle of 89°29'02" an arc distance of 31.23 feet; thence North 00°36'33" West 3.00 feet to northerly line of said Common 32, being the **POINT OF TERMINATION** of this line.

PARCEL: 25920-2

TOGETHER WITH underlying fee interest, if any, contiguous to said Common Lot 32, in and to the adjoining Lugonia Avenue (State Route 38) and University Street, lying within the following described land:

BEGINNING at the intersection of the centerline of Lugonia Avenue (State Route 38) and the northerly prolongation of the westerly line of said Lot 54 as shown

on said AMENDING MAP TRACT NO. 10356; thence along said northerly prolongation and the westerly line of said Lot 54 South 00°05'35" East 72.32 feet; thence North 89°54'25" East 52.00 feet to the westerly line of said Common Lot 32, also being the beginning of a non-tangent curve, concave southeasterly, having a radius of 23.00 feet, to which a radial bears South 89°54'25" West; thence northeasterly along said curve through a central angle of 89°29'02" an arc distance of 35.92 feet to the northerly line of said Common Lot 32; thence along said northerly line North 89°23'27" East 585.72 feet to the northeasterly corner of said Common Lot 32; thence along the northerly prolongation of the easterly line of said Common Lot 32 North 00°02'59" East 50.00 feet to said centerline of Lugonia Avenue (State Route 38); thence along said centerline South 89°23'27" West 660.55 feet to the **POINT OF BEGINNING**.

PARCEL: 25920-3

A temporary easement for construction purposes incidents thereto, in and to that portion of Common Lot 32 as shown on AMENDING MAP TRACT NO. 10356 in the City of Redlands, County of San Bernardino, State of California, as shown on a map filed in Book 164, Pages 6 through 8 of Maps, in the Office of the County Recorder of said County, described as follows:

COMMENCING at the intersection of the centerline of Lugonia Avenue (State Route 38) and the northerly prolongation of the westerly line of Lot 54 of University Tract as shown on map filed in Book 17, Page 36 of Maps, in the office of the County Recorder of said County and also as shown on said AMENDING MAP TRACT NO. 10356; thence along said northerly prolongation and the westerly line of said Lot 54 South 00°05'35" East 72.32 feet; thence North 89°54'25" East 52.00 feet to the westerly line of said Common Lot 32; thence North 89°54'25" East 3.00 feet to the beginning of a non-tangent curve, concave southeasterly, having a radius of 20.00 feet, to which a radial line bears South 89°54'25" West, being the **POINT OF BEGINNING**; thence northeasterly along said curve through a central angle of

89°29'02" an arc distance of 31.23 feet; thence South 00°36'33" East 5.00 feet to the beginning of a non-tangent curve, concave southeasterly, having a radius of 15.00 feet, to which a radial line bears North 0°36'33" West; thence southwesterly along said curve through a central angle of 89°29'02" an arc distance of 23.42 feet; thence South 89°54'25" West 5.00 feet to the **POINT OF BEGINNING**.

Rights to the above-described temporary easement shall cease and terminate on February 3, 2028. The rights may also be terminated prior to the above date by STATE upon notice to OWNER.

The bearings and distances used in the above description are on the California Coordinate System of 1983, Zone 5 (EPOCH 2017.50). Divide the distances used in the above description by 0.99991676 to obtain ground level distances.

TRANSPORTATION COMMISSION
RESOLUTION NO.

C-22558

CALIFORNIA TRANSPORTATION COMMISSION
RESOLUTION OF NECESSITY
TO ACQUIRE CERTAIN REAL PROPERTY
OR INTEREST IN REAL PROPERTY BY EMINENT DOMAIN
HIGHWAY 08-SBd-247-PM 77.48 PARCEL 26038-1
OWNER: Pacific/Costanzo-Lewis, a California general partnership

Resolved by the California Transportation Commission after notice (and hearing)
pursuant to Code of Civil Procedure Section 1245.235 that it finds and determines and
hereby declares that:

The hereinafter described real property is necessary for State Highway purposes
and is to be acquired by eminent domain pursuant to Streets and Highways Code Section
102 and Code of Civil Procedure Section 1240.510 in that the property being acquired is for
a compatible use;

The public interest and necessity require the proposed public project, namely a State
highway;

The proposed project is planned and located in the manner that will be most
compatible with the greatest public good and the least private injury;

The property sought to be acquired and described by this resolution is necessary for
the public project;

The offer required by Section 7267.2 of the Government Code has been made to the
owner or owners of record; and be it further

RESOLVED by this Commission that the Department of Transportation be and said
Department is hereby authorized and empowered;

APPROVED AS TO FORM AND PROCEDURE

APPROVAL RECOMMENDED

Attorney, Department of Transportation

DIVISION OF RIGHT OF WAY

1 To acquire, in the name of the People of the State of California, in fee simple
2 absolute, unless a lesser estate is hereinafter expressly described, the said hereinafter
3 described real property, or interests in real property, by condemnation proceeding or
4 proceedings in accordance with the provisions of the Streets and Highways Code, Code of
5 Civil Procedure and of the Constitution of California relating to eminent domain;

6 The real property or interests in real property, which the Department of
7 Transportation is by this resolution authorized to acquire, is situated in the County of San
8 Bernardino, State of California, Highway 08-SBd-247 and described as follows:

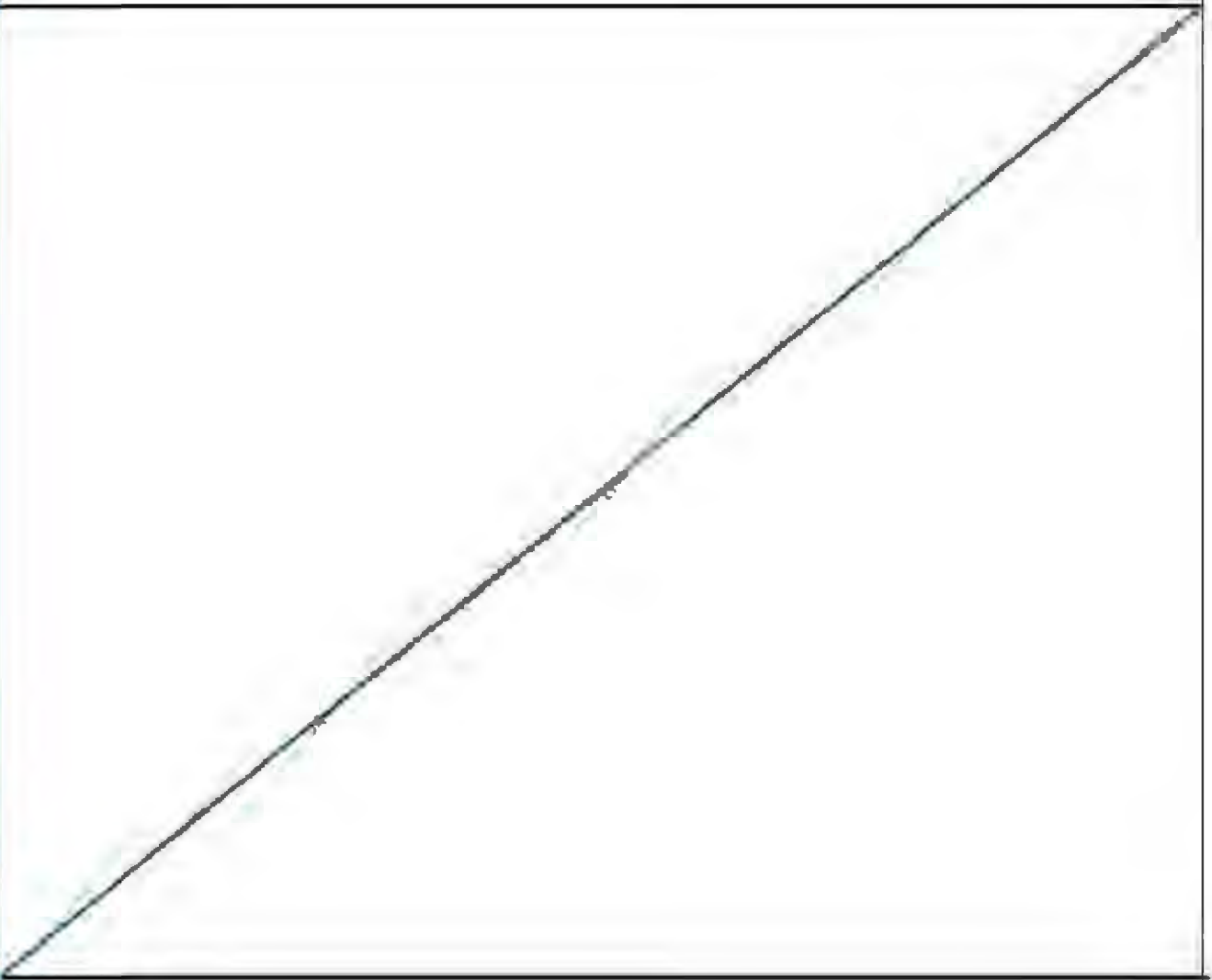


Exhibit "A"

Parcel 26038-1:

For State highway purposes in the City of Barstow, County of San Bernardino, State of California, being a portion of Government Lot 257, in the Southeast Quarter of Section 7, Township 9 North, Range 1 West, San Bernardino Meridian, according to the Official Plat approved November 28, 1952, as described in Grant Deed recorded on August 15, 2006, as Document No. 2006-0554971 of Official Records, in the Office of the County Recorder of said County, lying southeasterly of the following described line:

COMMENCING at the centerline intersection of Barstow Road (State Route 247) and Rimrock Road as shown on Tract Map No. 10916 filed in Book 157 Pages 49 through 50 of Map Book of said County; thence along the centerline of said Barstow Road (State Route 247) North 00°34'16" West 68.10 feet; thence leaving said centerline, South 89°25'44" West 50.00 feet to a point on the westerly right of way line of said Barstow Road (State Route 247), said point being the **POINT OF BEGINNING**; thence leaving said westerly line South 41°59'20" West 23.26 feet to a point on the northerly right of way line of Rimrock Road and said point also being the northerly line of Parcel 2 as described in Right of Way Deed recorded on May 26, 1987, as Document No. 87-175136 of Official Records, in the Office of the County Recorder of said County, said point being the **POINT OF TERMINATION**.

The bearings and distances used in the above description are on the California Coordinate System of 1983, Zone 5 (Epoch 2007.00). Divide the distances used in the above description by 0.99980260 to obtain ground level distances.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.4d., Action Item

Prepared By: René Fletcher, Chief (Acting)
Division of Right of Way and Land Surveys

Subject: **CONVEYANCE OF STATE-OWNED REAL PROPERTY**

ISSUE:

Should the California Transportation Commission (Commission) approve execution of the following Director's Deeds?

RECOMMENDATION:

The California Department of Transportation (Department) recommends the Commission authorize execution of the Director's Deeds summarized below. The conveyance of excess State-owned real property, including exchanges, is pursuant to Section 118 of the Streets and Highways Code (SHC).

The Director's Deeds included in this item involve an estimated current value of \$1,580,140. The State will receive a return of \$1,580,140 from the sale of these properties; corresponding maps are attached.

DIRECTOR'S DEEDS:

01-03-Sac-50 PM R5.20/R5.50
Disposal Units #DD 19353-01-01
(DD 19353-01-02)

Convey to: Sacramento County

Sacramento County
2,270 Square feet (s.f.)
2,630 s.f.
\$2,940 (Appraisal \$2,940)

Direct Sale. Disposal units is being conveyed at fair market value to Sacramento County, and are incapable of independent development due to their irregular shape and sizes. Per the Co-Op Agreement number 03-0419 dated November 9, 2010, and the first amendment to Co-Op dated June 14, 2012, the Department and Sacramento County agreed to construct the soundwalls at this location and Sacramento County agreed to take title in fee to the area behind the soundwalls that is not necessary for highway operations or maintenance. The disposal units are not subject to Department of General Services (DGS) criteria for Executive Orders N-06-19 or N-23-20.

02-05-SLO-101 PM 16.50
Disposal Unit #DD 16-01-01
Convey to: Michael Joseph Connolly
Living Trust Dated May 31, 2019

San Luis Obispo County
0.03 acres
\$42,200 (Appraisal \$42,200)

Direct sale. The disposal unit is part of a decertification requested by the adjacent property owner. The excess parcel is of such size and shape that it is not a developable property on its own. Adjoining owner has the support from local agencies for acquiring this current operating Right of Way to facilitate the construction of affordable housing. The disposal unit does not meet DGS criteria for Executive Orders N-06-19 or N-23-20.

03-06-Mad-99 PM R7.45
Disposal Unit #DD 86528-01-01
Convey to: Riley Survivor's Trust

Madera County
1.25 acres
\$105,000 (Appraisal \$105,000)

Direct sale. Disposal unit is incapable of independent development due to the limitations of developing independent access points. Disposal unit was appraised and offered to the only adjoining owner at fair market value. The disposal unit does not meet DGS criteria for Executive Orders N-06-19 or N-23-20.

04-06-Mad-99 PM R7.45
Disposal Unit #DE 86528-2
Convey to: Pacific Gas and Electric Company

Madera County
5,971 s.f.
\$0 (Appraisal N/A)

Direct conveyance for an easement with no monetary consideration. Conveyance is 100 percent State's obligation pursuant to Utility Agreement number 06-1356.44 dated May 22, 2013, and First Amendment dated May 29, 2014. The disposal unit is not subject to DGS criteria for Executive Orders N-06-19 or N-23-20.

05-06-Mad-99 PM R7.45
Disposal Unit #DE 86528-3
Convey to: Pacific Gas and Electric Company

Madera County
9,018 s.f.
\$0 (Appraisal N/A)

Direct conveyance for an easement with no monetary consideration. Conveyance is 100 percent State's obligation pursuant to Utility Agreement number 06-1356.44 dated May 22, 2013, and First Amendment dated May 29, 2014. Disposal unit is not subject to Executive Orders N-06-19 or N-23-20.

06-07-LA-10 PM 39.2-39.8
Disposal Units #DK 79736-5
(79736-5)
(79738-3)
(79739-2)
Convey to: Southern California Edison Company

Los Angeles County
14,303 s.f.
5,597 s.f.
5,537 s.f.
3,169 s.f.
\$0 (Appraisal N/A)

Direct conveyance for an easement with no monetary consideration. This is a direct conveyance to Southern California Edison (SCE) for no monetary consideration pursuant to Utility Agreement 7-UA-12156 dated December 24, 2013, and by Legal Memorandum dated July 28, 2023. This conveyance was previously approved March 2024 however; the legal description and exhibits had inaccuracies. The State is 100 percent liable for the relocation and replacement of the easements owed to SCE. The disposal unit is not subject to DGS criteria for Executive Orders N-06-19 or N-23-20.

07-07-LA-101 PM 5.1

Disposal Unit #DD 980582-01-01

(DD 980582-01-01)

(DD 980583-01-01)

(DD 980584-01-01)

(DD 980585-01-01)

(DD 980586-01-01)

Convey to: Cordova Associates

Los Angeles County

15,738 s.f.

170 s.f.

2,584 s.f.

4,259 s.f.

1,860 s.f.

6,865 s.f.

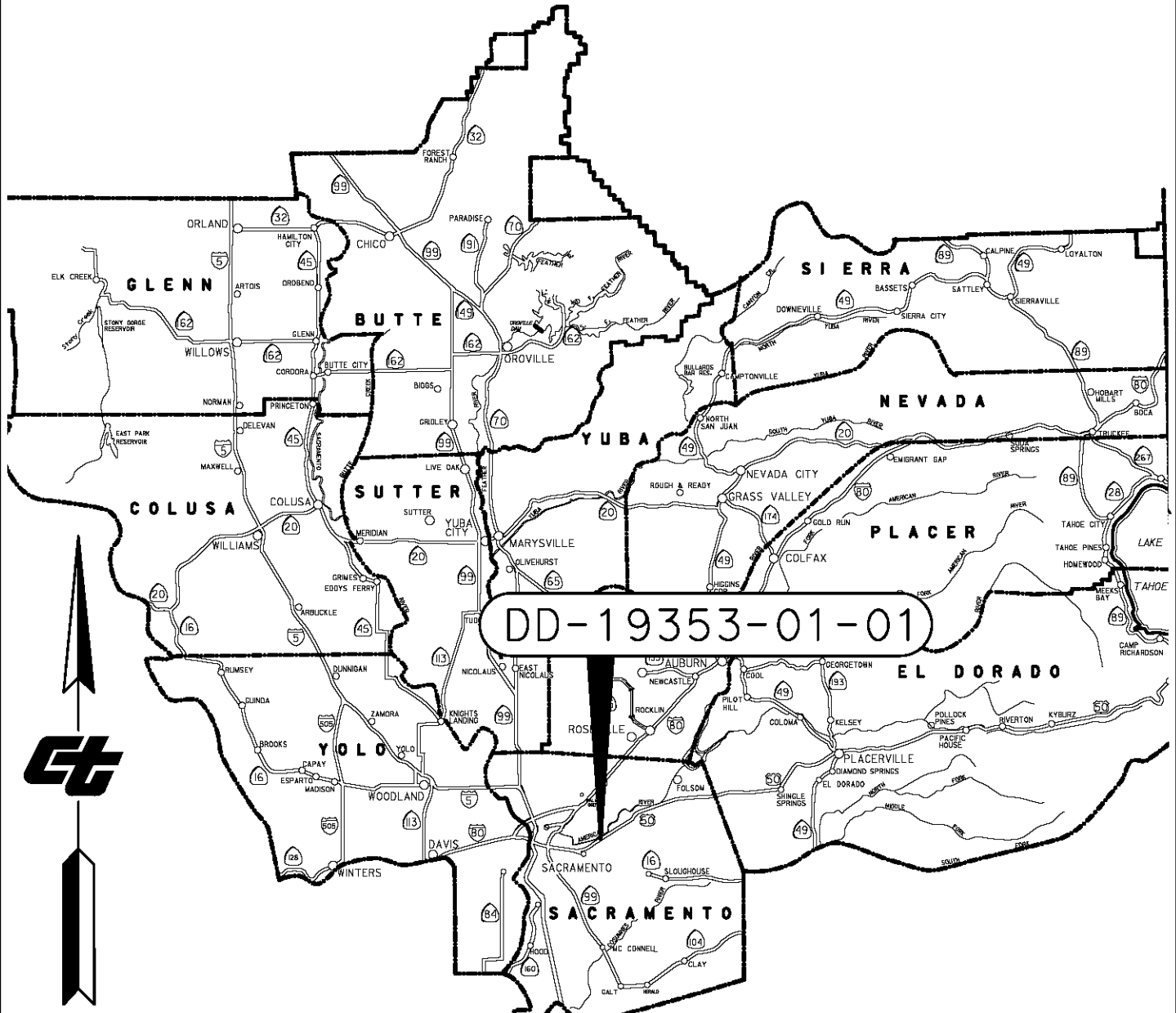
\$1,430,000 (Appraisal\$1,430,000)

Direct sale. This is a direct sale to the only adjoining owner through the decertification process. The sales price represents fair market value. The sloped disposal unit has no access to the adjacent freeway and is at street level. It's incapable of independent development because it's contaminated by active hazardous waste. Per the terms and conditions of the Purchase and Sale Agreement, section 31 and the requestor must remediate the parcel of the current hazardous waste per the agreement within 2 years of approval by the Commission. The disposal is not subject to DGS criteria for Executive Orders N-06-19 or N-23-20.

Attachments: 1A –7B – Parcel Maps

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

LOCATION MAP FOR DIRECTOR'S DEED PARCEL DD-19353-01-01 SACRAMENTO COUNTY, CA



NOTES		LEGEND			STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION RIGHT OF WAY DIRECTOR'S DEED LOCATION MAP DD-19353-01-01 NO SCALE		
Coordinates and bearings are on NAD83 (Epoch 2007) CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.							
DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
A. HESSABI	02/21/2025	03	SAC	50	R5.50	1	7



GLENBROOK UNIT NO. 11
(83 B.M. 9)

PORTIONS OF
T. 8 N. R. 5 E., M.D.M.
SECTION 12
WITHIN THE COUNTY OF
SACRAMENTO
STATE OF CALIFORNIA

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

P.O.B.

P.O.T.



B11 P.O.T. 142+41.75(1.3)
M100 P.O.T. 47+39.36(1.3)

145+00

150+00

155+00

160+00

165+00

P.O.B.

GLENBROOK
UNIT NO. 6
(88 B.M. 07)

PARCEL 5
(83 P.M. 25)

PARCEL 7
(83 P.M. 25)

DD-19353-01-01

PARCEL 9
(83 P.M. 25)

CALTRANS MON 24-486(2,3)
B11 STA: 144+60.98
B11 377.05'RT
M100 STA: 43+08.42
(M100 STA: 43+08.29)(2)
(M100 STA: 43+08.23)(3)
M100 70.53'RT (70.42')(2) (70.37')(3)

AMENDED PARCEL MAP
CALIFORNIA CENTER
(83 P.M. 25)

PARCEL 2
(83 P.M. 25)

CALTRANS MON 24-487(2,3)
M100 STA: 29+04.99 (M100)
STA: 29+04.80 (2,3) 25.99'LT
(25.91')(2,3)

THE PURPOSE OF THIS EXHIBIT IS TO DEFINE THE DIRECTOR'S DEED AREA BETWEEN THE SOUTHWEST FACE OF THE CONSTRUCTED SOUNDWALL-1 AND THE FEE OWNERSHIP LINES TO THE SOUTH AND SOUTHWEST OF SOUNDWALL-1, AND TO DEFINE THE DIRECTOR'S DEED AREA BETWEEN THE NORTH FACE OF THE CONSTRUCTED SOUNDWALL-2 AND FEE OWNERSHIP LINES TO THE NORTHEAST AND NORTH OF SOUNDWALL-2.

PORTIONS OF
T. 8 N. R. 5 E., M.D.M.
SECTION 12
WITHIN THE CITY OF SACRAMENTO
AND COUNTY OF SACRAMENTO
STATE OF CALIFORNIA

NOTES

Coordinates and bearings are on NAD83 (Epoch 2007) CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.

LEGEND

- Access Prohibited
- Access Superseded
- Existing R/W Superseded
- Access Opening (Private)
- Indicates Radial Bearing
- Indicates Found Monument as noted
- Indicates calculated point. (Does not imply monument set)
- Director's Deed Parcel

STATE OF CALIFORNIA
CALIFORNIA STATE TRANSPORTATION AGENCY
DEPARTMENT OF TRANSPORTATION

**RIGHT OF WAY
DIRECTOR'S DEED**

DETAIL MAP

DD-19353-01-01

SCALE: 1" = 500'

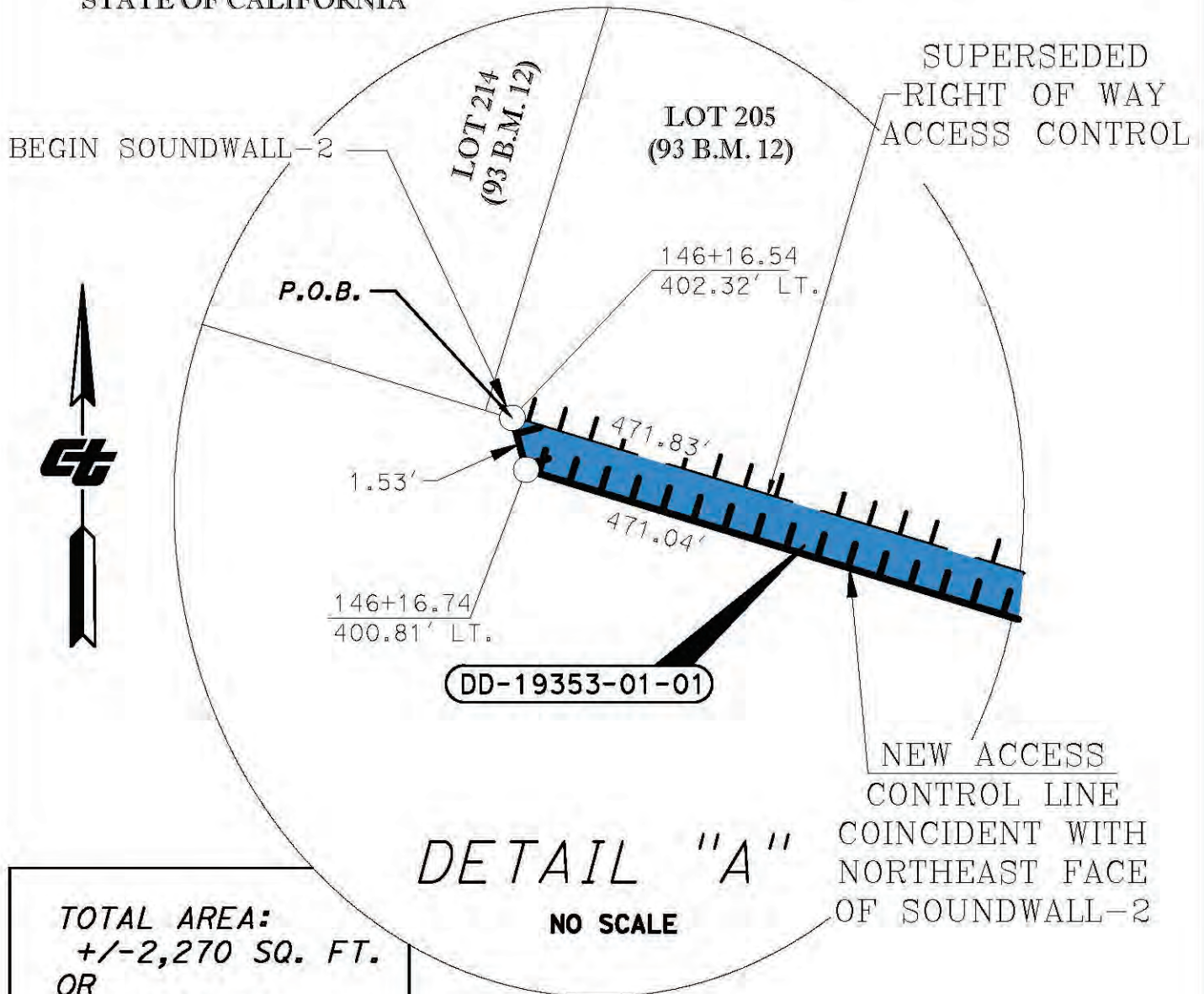
FEET 0 250 500 1000

DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
A. HESSABI	02/21/2025	03	SAC	50	R5.50	2	7

PORTIONS OF
T. 8 N. R. 5 E., M.D.M.
SECTION 12
WITHIN THE COUNTY OF
SACRAMENTO
STATE OF CALIFORNIA

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

NOTE: STATIONING SHOWN FOR SOUNDWALL-2 IS BASED ON THE B11 ALIGNMENT (1)



TOTAL AREA:
+/-2,270 SQ. FT.
OR
+/-0.05 AC.

NOTES		LEGEND		STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION			
Coordinates and bearings are on NAD83 Zone (Epoch 2007) CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.		<div>===== Access Prohibited</div> <div>===== Access Superseded</div> <div>----- Existing R/W Superseded</div> <div>□ Access Opening (Private)</div> <div>(R) Indicates Radial Bearing</div> <div>○ Indicates angle point. (Does not imply monument set)</div> <div>○ Director's Deed Parcel</div>		RIGHT OF WAY DD-19353-01-01 DETAIL MAP DETAIL "A" NO SCALE			
DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
A. HESSABI	02/21/2025	03	SAC	50	R5.50	4	7

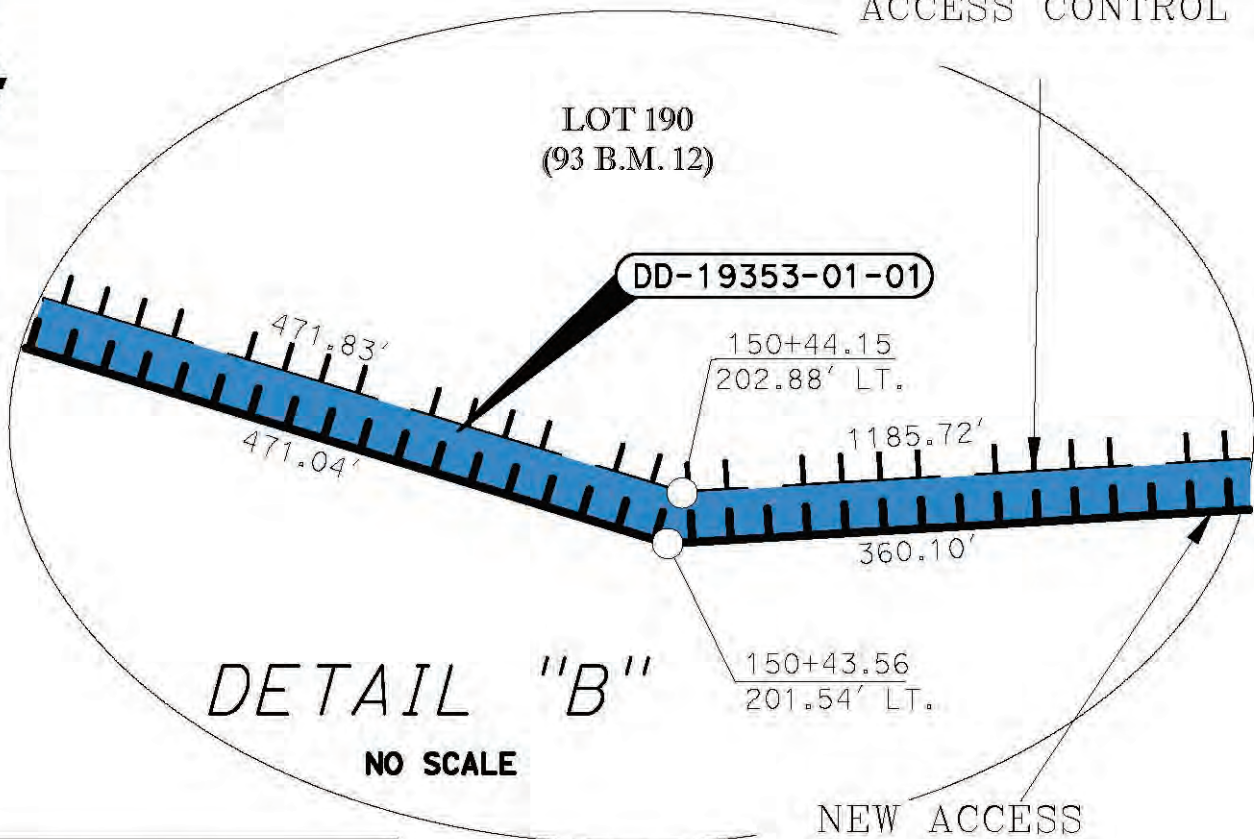
PORTIONS OF
T. 8 N. R. 5 E., M.D.M.
SECTION 12
WITHIN THE COUNTY OF
SACRAMENTO
STATE OF CALIFORNIA

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

NOTE: STATIONING SHOWN FOR
SOUNDWALL-2 IS BASED
ON THE B11 ALIGNMENT (1)










SUPERSEDED
RIGHT OF WAY
ACCESS CONTROL



TOTAL AREA:
+/-2,270 SQ. FT.
OR
+/-0.05 AC.

NEW ACCESS
CONTROL LINE
COINCIDENT WITH
NORTHEAST FACE
OF SOUNDWALL-2

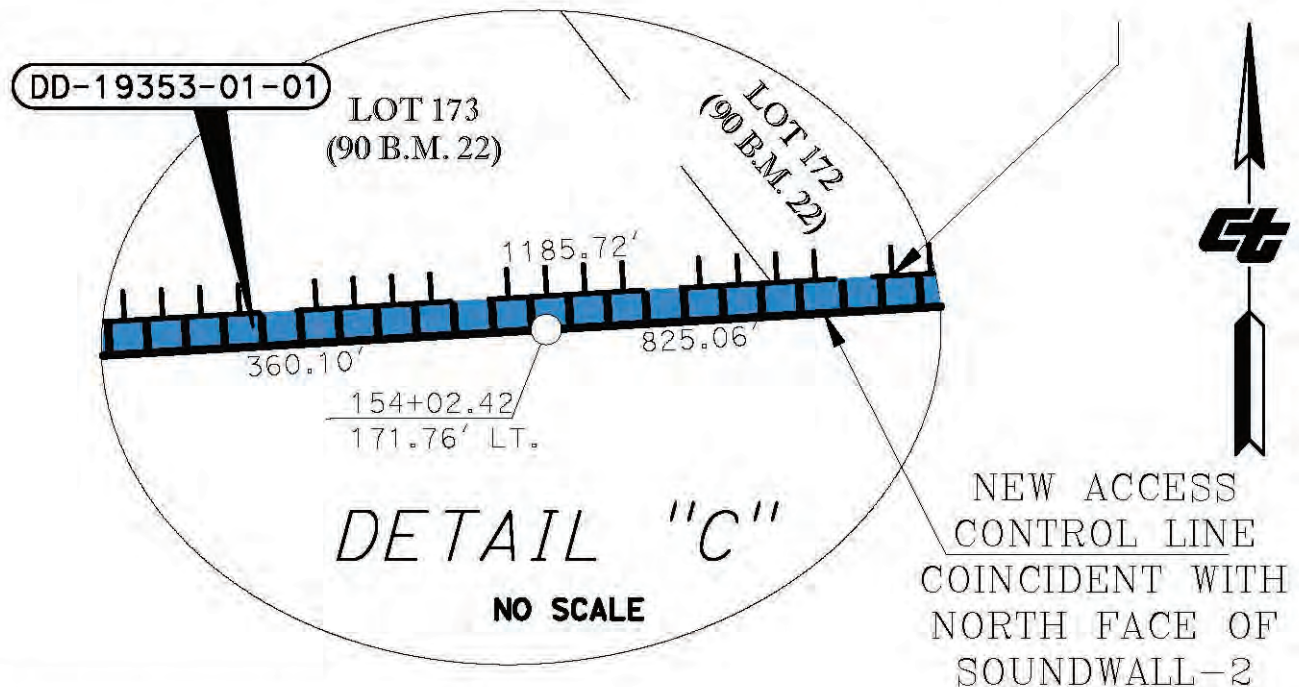
NOTES		LEGEND			STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION RIGHT OF WAY DD-19353-01-01 DETAIL MAP DETAIL "B" NO SCALE		
Coordinates and bearings are on NAD83 (Epoch 2007) CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.		 Access Prohibited  Access Superseded  Existing R/W Superseded  Access Opening (Private)  (R) Indicates Radial Bearing  Indicates angle point. (Does not imply monument set)  Director's Deed Parcel					
DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
A. HESSABI	02/21/2025	03	SAC	50	R5.50	5	7

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

PORTIONS OF
T. 8 N. R. 5 E., M.D.M.
SECTION 12
WITHIN THE COUNTY OF
SACRAMENTO
STATE OF CALIFORNIA

NOTE: STATIONING SHOWN FOR
SOUNDWALL-2 IS BASED
ON THE B11 ALIGNMENT (1)

SUPERSEDED
RIGHT OF WAY
ACCESS CONTROL

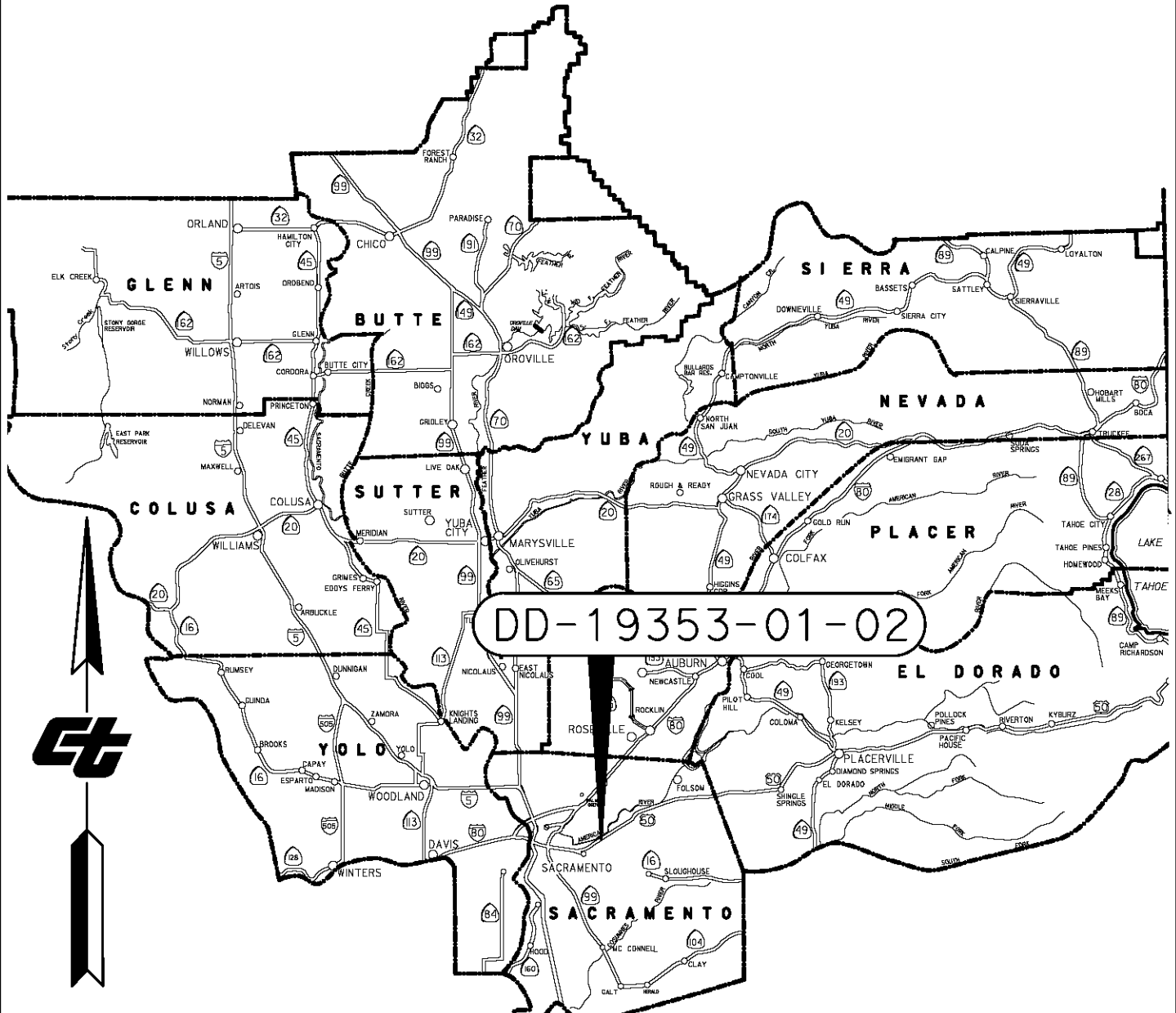


TOTAL AREA:
+/-2,270 SQ. FT.
OR
+/-0.05 AC.

NOTES		LEGEND			STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION		
Coordinates and bearings are on NAD83 (Epoch 2007). CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.		Access Prohibited Access Superseded Existing R/W Superseded Access Opening (Private) (R) Indicates Radial Bearing Indicates angle point. (Does not imply monument set) Director's Deed Parcel			RIGHT OF WAY DD-19353-01-01 DETAIL MAP DETAIL "C" NO SCALE		
DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
A. HESSABI	02/21/2025	03	SAC	50	R5.50	6	7

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

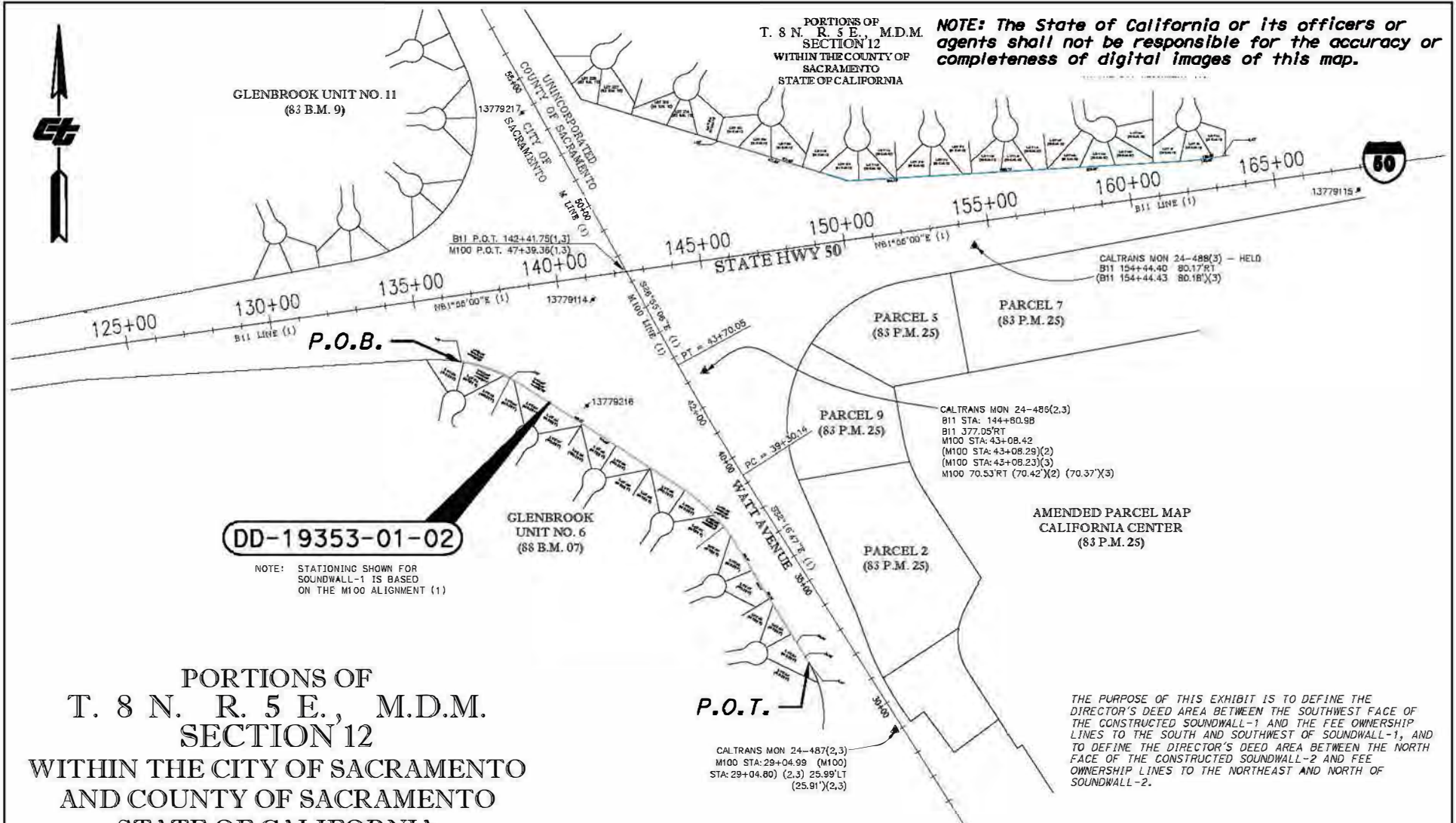
LOCATION MAP FOR DIRECTOR'S DEED PARCEL DD-19353-01-02 SACRAMENTO COUNTY, CA



NOTES		LEGEND			STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION RIGHT OF WAY DIRECTOR'S DEED LOCATION MAP DD-19353-01-02 NO SCALE			
Coordinates and bearings are on NAD83 (Epoch 2007) CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.								
DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS	
A. HESSABI	02/21/2025	03	SAC	50	R5.20	1	6	

PORTIONS OF
T. 8 N. R. 5 E., M.D.M.
SECTION 12
WITHIN THE COUNTY OF
SACRAMENTO
STATE OF CALIFORNIA

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

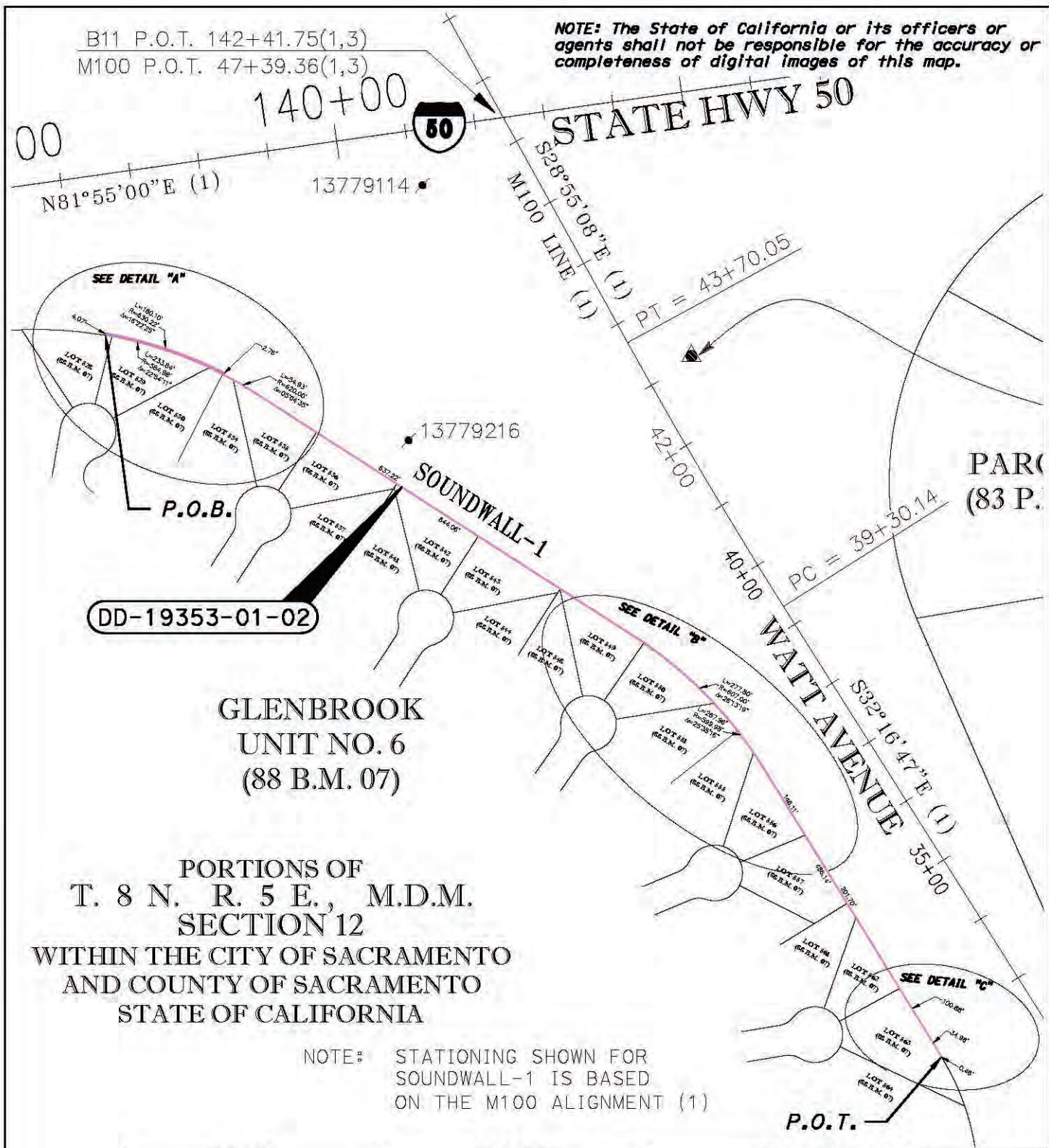


DD-19353-01-02

NOTE: STATIONING SHOWN FOR
SOUNDWALL-1 IS BASED
ON THE M100 ALIGNMENT (1)

PORTIONS OF
T. 8 N. R. 5 E., M.D.M.
SECTION 12
WITHIN THE CITY OF SACRAMENTO
AND COUNTY OF SACRAMENTO
STATE OF CALIFORNIA

NOTES			LEGEND			STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION		
Coordinates and bearings are on NAD83 (Epoch 2007) CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.			<div>Access Prohibited</div> <div>Access Superseded</div> <div>Existing R/W Superseded</div> <div>Access Opening (Private)</div> <div>Indicates Radial Bearing</div> <div>Indicates Found Monument as noted</div> <div>Indicates calculated point. (Does not imply monument set)</div> <div>Director's Deed Parcel</div>			RIGHT OF WAY DIRECTOR'S DEED DETAIL MAP DD-19353-01-02 SCALE: 1" = 500'		
DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS	
A. HESSABI	02/21/2025	03	SAC	50	R5.20	2	6	



NOTES			LEGEND			STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION		
Coordinates and bearings are on NAD83 (Epoch 2007) CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.			Access Prohibited Access Superseded Existing R/W Superseded Access Opening (Private) (R) Indicates Radial Bearing Indicates Found Monument as noted Indicates calculated point. (Does not imply monument set) Director's Deed Parcel			RIGHT OF WAY DIRECTOR'S DEED MAP DD-19353-01-02 DETAIL MAP SCALE: 1" = 200'		
DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS	
A. HESSABI	02/21/2025	03	SAC	50	R5.20	3	6	

DETAIL "A"

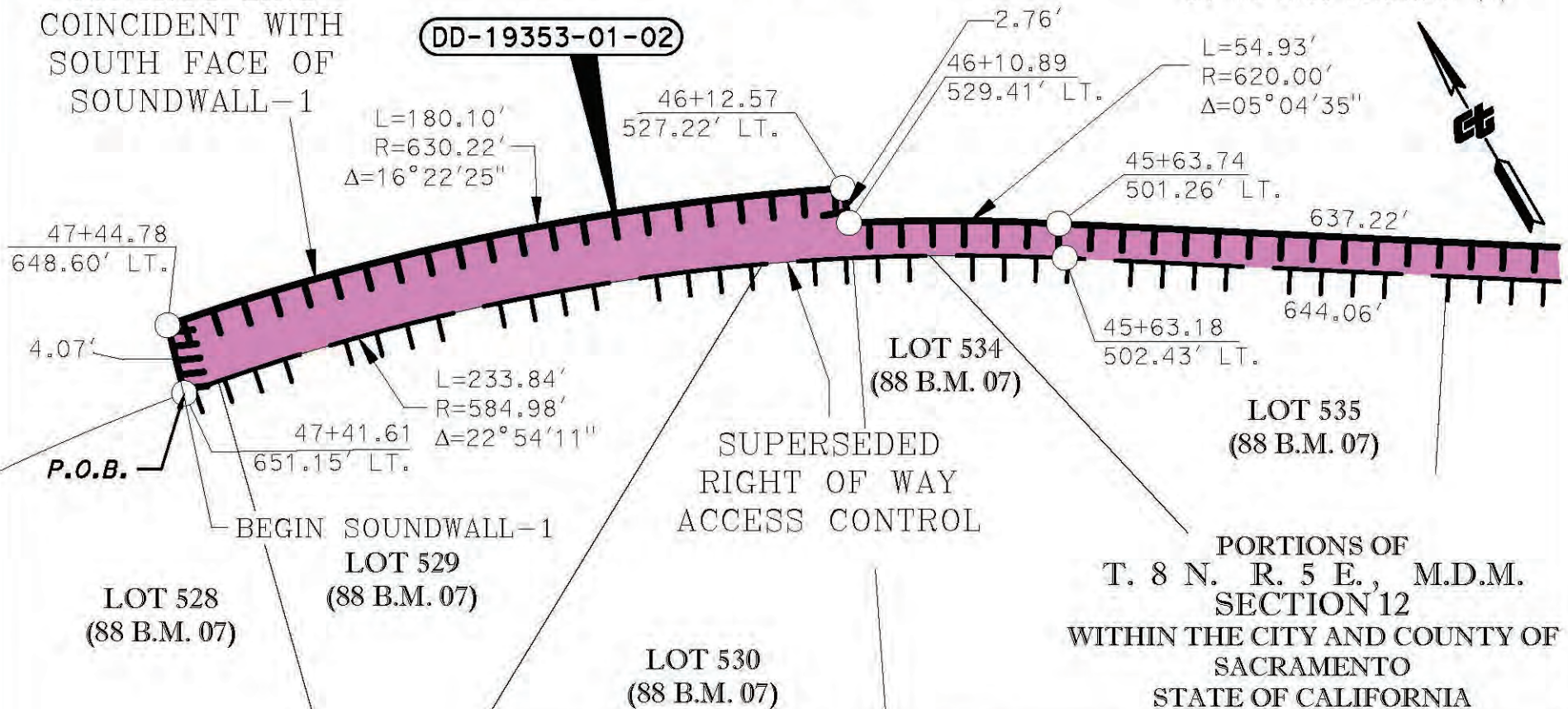
NO SCALE

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

NOTE: STATIONING SHOWN FOR SOUNDWALL-1 IS BASED ON THE M100 ALIGNMENT (1)

NEW ACCESS
CONTROL LINE
COINCIDENT WITH
SOUTH FACE OF
SOUNDWALL-1

DD-19353-01-02



PORTIONS OF
T. 8 N. R. 5 E., M.D.M.
SECTION 12
WITHIN THE CITY AND COUNTY OF
SACRAMENTO
STATE OF CALIFORNIA

TOTAL AREA:
+/-2,630 SQ. FT.
OR
+/-0.06 AC.

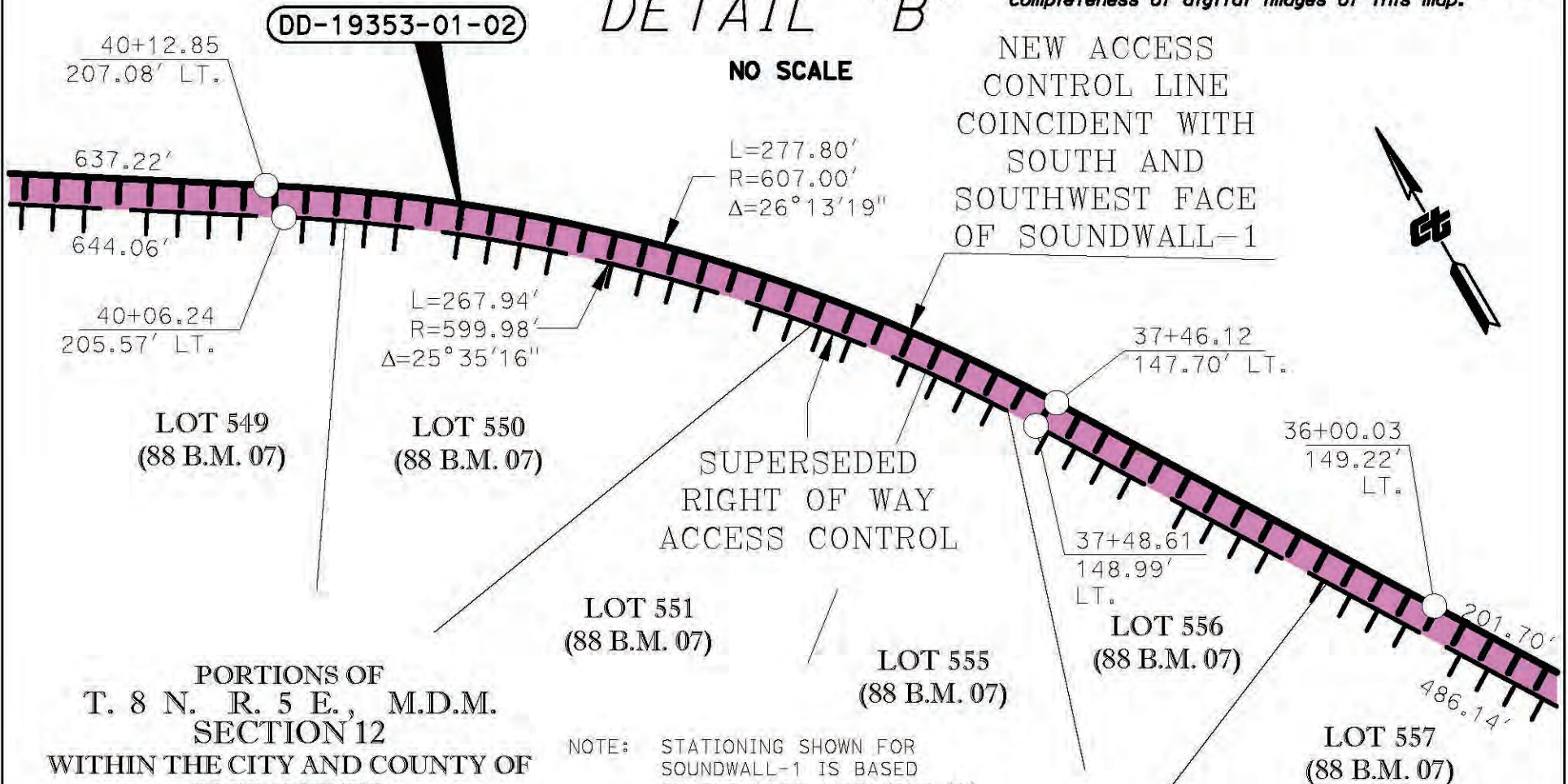
NOTES		LEGEND		STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION RIGHT OF WAY DD-19353-01-02 DETAIL MAP DETAIL "A" NO SCALE			
Coordinates and bearings are on NAD83 (Epoch 2007) CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.		<div> <div>-----</div> Access Prohibited <div>-----</div> Access Superseded <div>-----</div> Existing R/W Superseded <div>□</div> Access Opening (Private) <div>(R)</div> Indicates Radial Bearing <div>○</div> Indicates angle point. (Does not imply monument set) <div>○</div> Director's Deed Parcel </div>					
DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
A. HESSABI	02/21/2025	03	SAC	50	R5.20	4	6

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

DETAIL "B"

NO SCALE

NEW ACCESS
CONTROL LINE
COINCIDENT WITH
SOUTH AND
SOUTHWEST FACE
OF SOUNDWALL-1



PORTIONS OF
T. 8 N. R. 5 E., M.D.M.
SECTION 12
WITHIN THE CITY AND COUNTY OF
SACRAMENTO
STATE OF CALIFORNIA

NOTE: STATIONING SHOWN FOR
SOUNDWALL-1 IS BASED
ON THE M100 ALIGNMENT (1)

TOTAL AREA:
+/-2,630 SQ. FT.
OR
+/-0.06 AC.

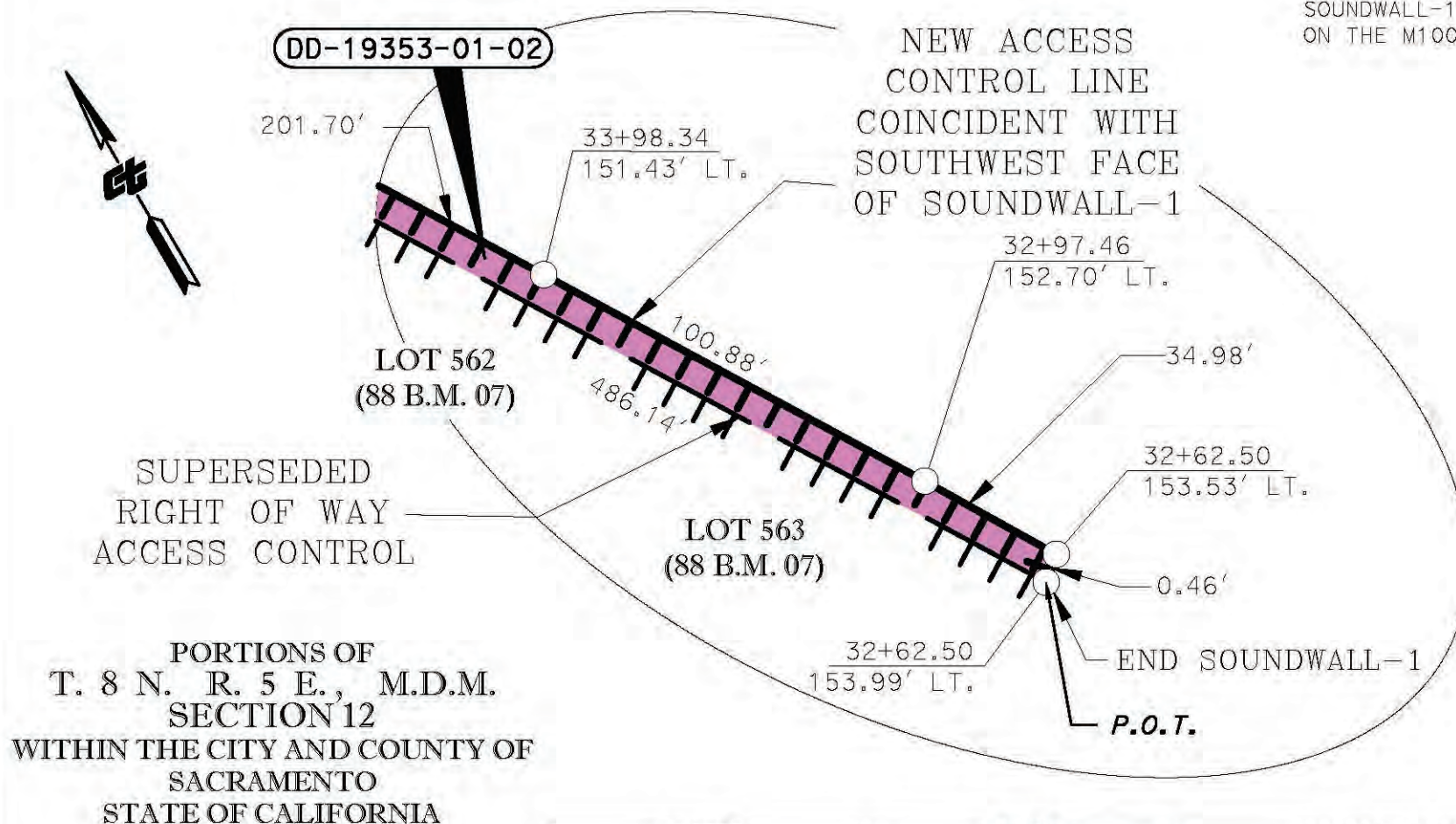
NOTES		LEGEND		STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION RIGHT OF WAY DD-19353-01-02 DETAIL MAP DETAIL "B" NO SCALE			
Coordinates and bearings are on NAD83 (Epoch 2007) CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.		Access Prohibited Access Superseded Existing R/W Superseded Access Opening (Private) (R) Indicates Radial Bearing Indicates angle point. (Does not imply monument set) Director's Deed Parcel					
DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
A. HESSABI	02/21/2025	03	SAC	50	R5.20	5	6

DETAIL "C"

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

NO SCALE

NOTE: STATIONING SHOWN FOR SOUNDWALL-1 IS BASED ON THE M100 ALIGNMENT (1)

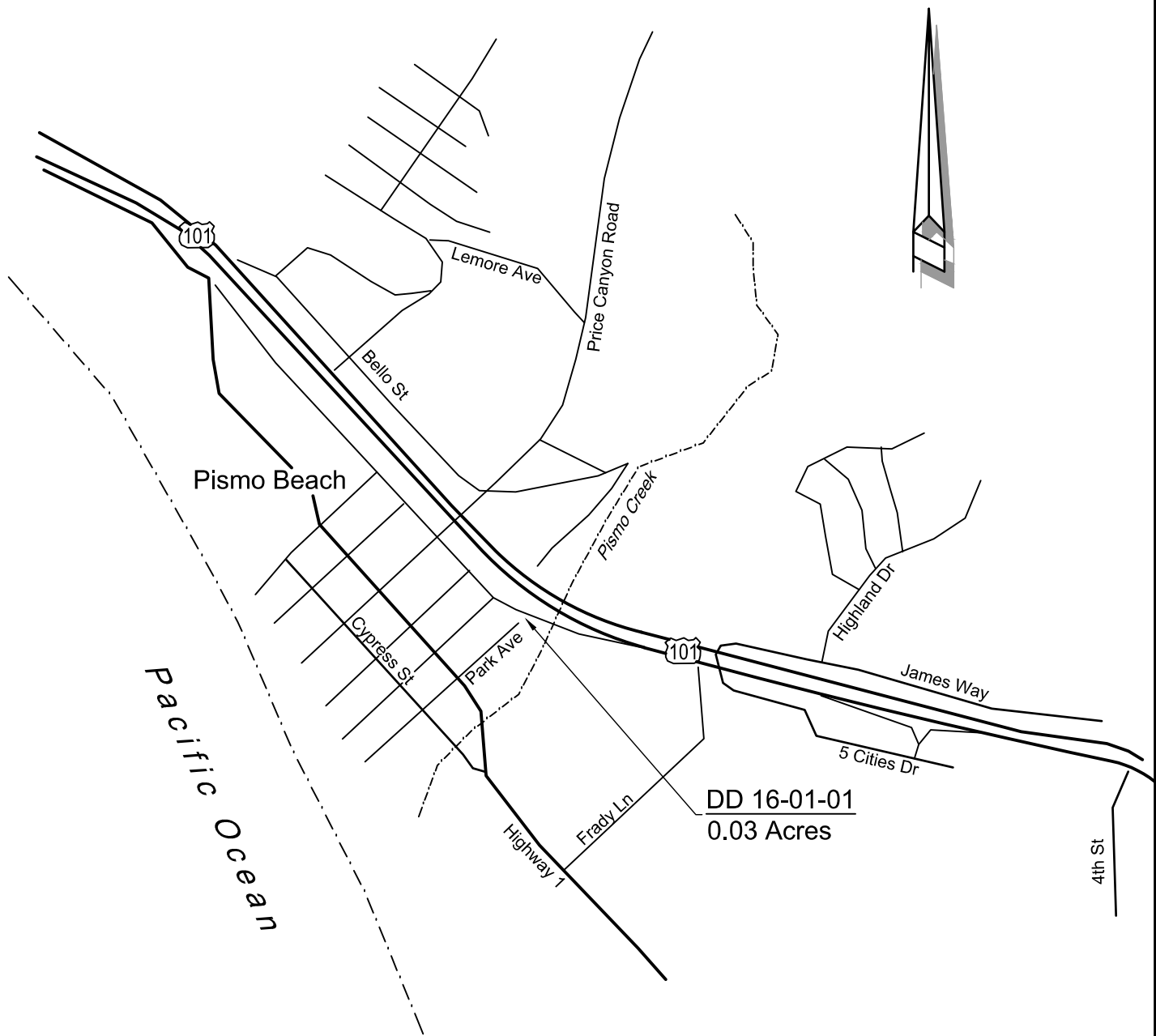


TOTAL AREA:
+/-2,630 SQ. FT.
OR
+/-0.06 AC.

NOTES		LEGEND		STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION RIGHT OF WAY DD-19353-01-02 DETAIL MAP DETAIL "C" NO SCALE			
Coordinates and bearings are on NAD83 (Epoch 2007) CCS Zone 2. Distances and stationing are grid distances. Divide by 0.99993942 to obtain ground distances. All distances are in feet unless otherwise noted.		<div>===== Access Prohibited</div> <div>----- Access Superseded</div> <div>----- Existing R/W Superseded</div> <div>□ Access Opening (Private)</div> <div>(R) Indicates Radial Bearing</div> <div>○ Indicates angle point. (Does not imply monument set)</div> <div>○ Director's Deed Parcel</div>					
DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
A. HESSABI	02/21/2025	03	SAC	50	R5.20	6	6

COUNTY OF SAN LUIS OBISPO

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of ditigal images of this map.

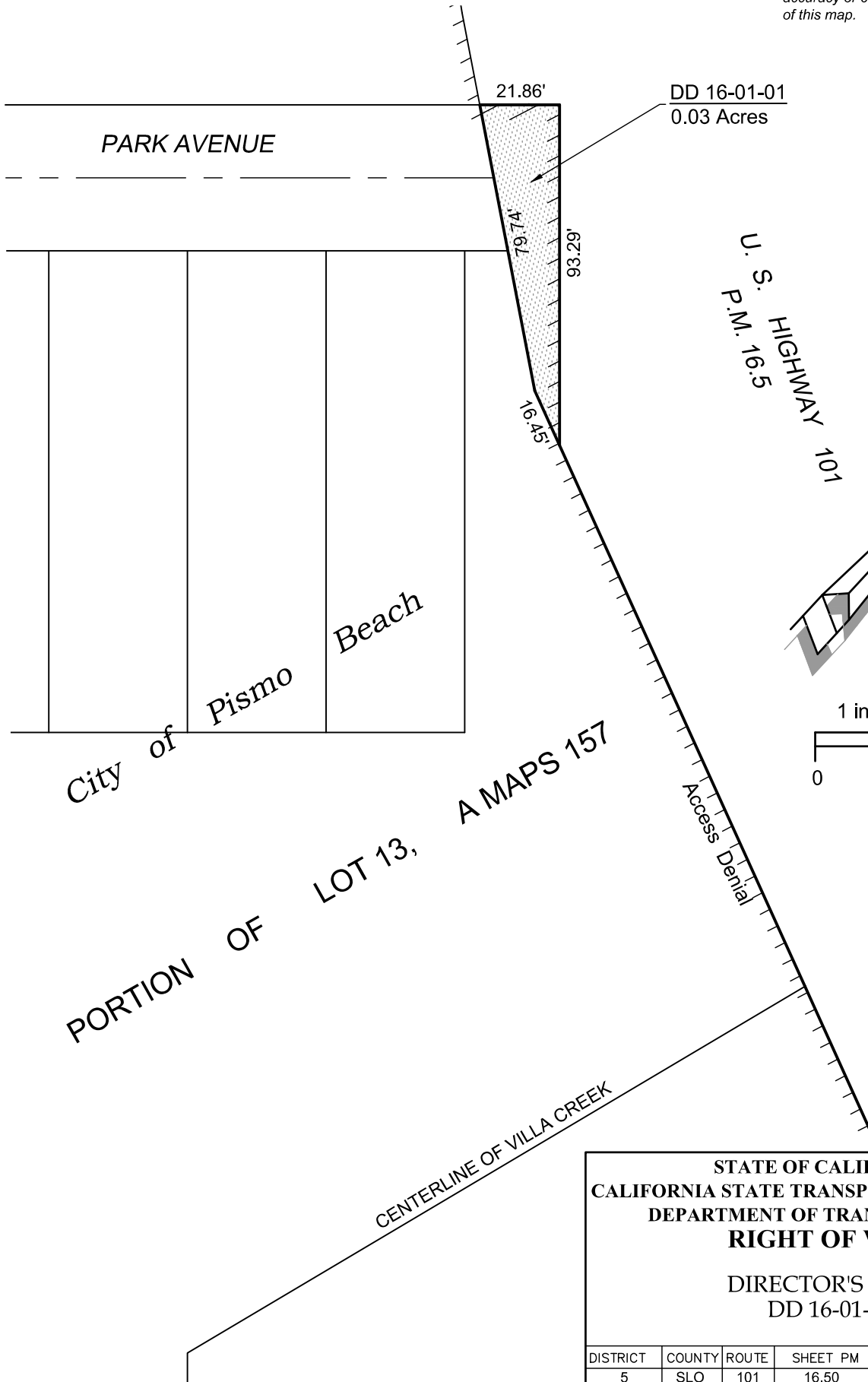


STATE OF CALIFORNIA
CALIFORNIA STATE TRANSPORTATION AGENCY
DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY
DIRECTOR'S DEED
AREA MAP
DD 16-01-01
NO SCALE

DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
5	SLO	101	16.50	1	2

COUNTY OF SAN LUIS OBISPO

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.



PORTION OF LOT 13, A MAPS 157

Access Denial

NOTES
Coordinates and bearings are local.
Distances are ground distances.
All distances are in feet unless otherwise noted.

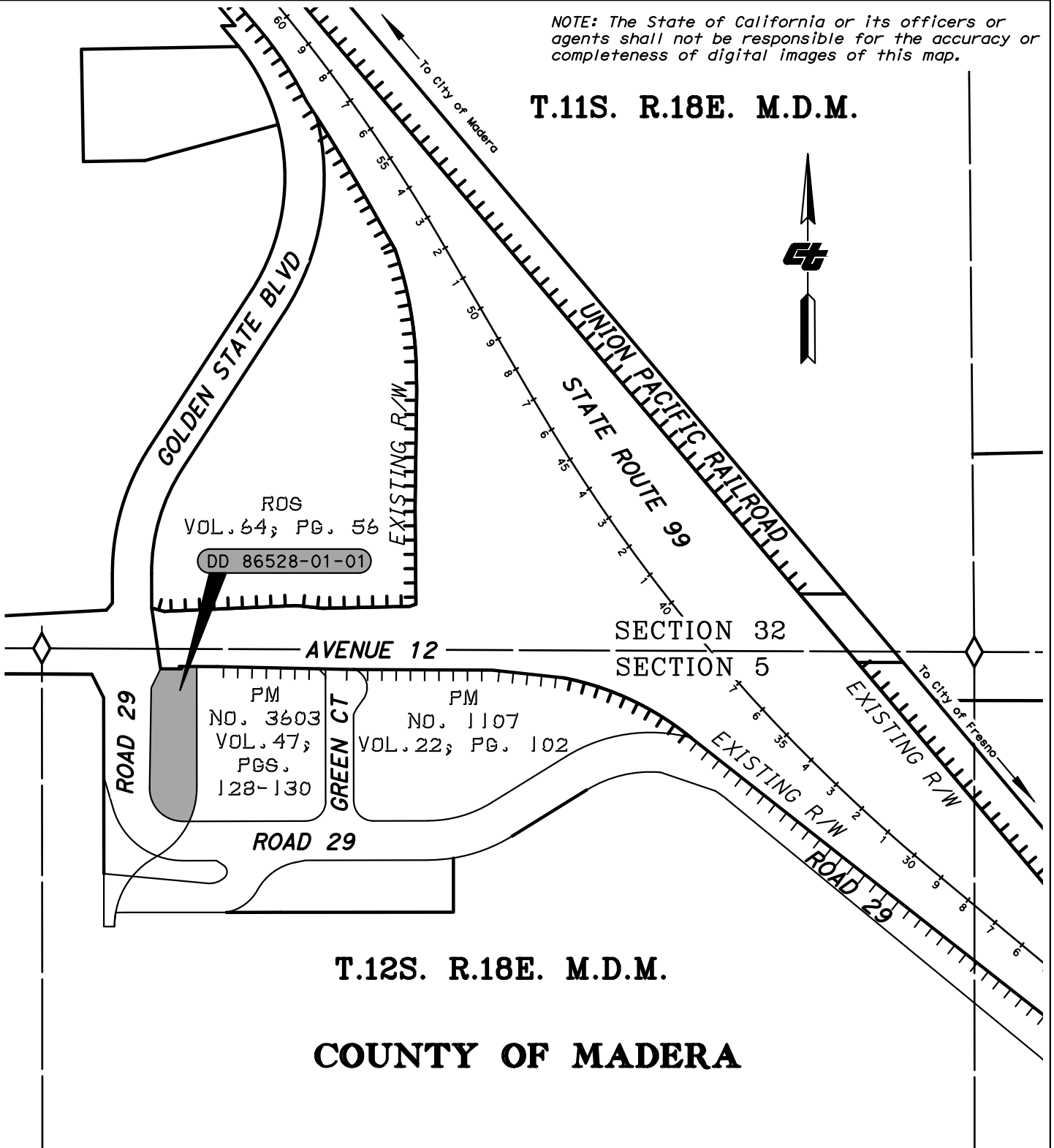
STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION RIGHT OF WAY

DIRECTOR'S DEED
DD 16-01-01

DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
5	SLO	101	16.50	2	2

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

T.11S. R.18E. M.D.M.



T.12S. R.18E. M.D.M.

COUNTY OF MADERA

LEGEND	
	Parcel DD 86528-01-01
	Access Restricted

STATE OF CALIFORNIA
CALIFORNIA STATE TRANSPORTATION AGENCY
DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY
DIRECTOR'S DEED MAP

Exhibit A
DD 86528-01-01

SCALE: 1" = 400'

FEET 0 200 400 800

DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
F. CARDENAS	11/5/2024	06	MAD	99	R7.45	1	2

T.11S. R.18E. M.D.M.
SECTION 32

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

SECTION LINE

AVENUE 12 SW Corner
Sec 33-11/18

N Corner
Sec 5-12/18

T.12S. R.18E. M.D.M.
SECTION 5

PARCEL 3

PM NO. 3603
VOL. 47, PGS.
128-130

LINE DATA TABLE

NO.	DISTANCE
L1	19.62'
L2	60.19'
L3	59.91'
L4	9.91'
L5	50.00'

GREEN COURT

NO SCALE

VACATION FOR
WOODWARD WAY PER
RESOLUTION NO.
2019-031 RECORDED
3/28/2019 IN DOC.
2019-006227

PARCEL 4

PM NO. 3603
VOL. 47, PG. 129

PARCEL 4

PARCEL 3

ROAD 29

ROS
VOL. 64, PG. 56

COUNTY OF MADERA

WOODWARD WAY

NOTES

Coordinates and bearings are on CCS 1983 Zone 03. Distances and stationing are grid distances. Divide by 1.00003181 to obtain ground distances. All distances are in feet unless otherwise noted.

LEGEND

- Parcel DD 86528-01-01 Containing 1.25 Acres
- Access Restricted

STATE OF CALIFORNIA
CALIFORNIA STATE TRANSPORTATION AGENCY
DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY
DIRECTOR'S DEED MAP

Exhibit B

DD 86528-01-01

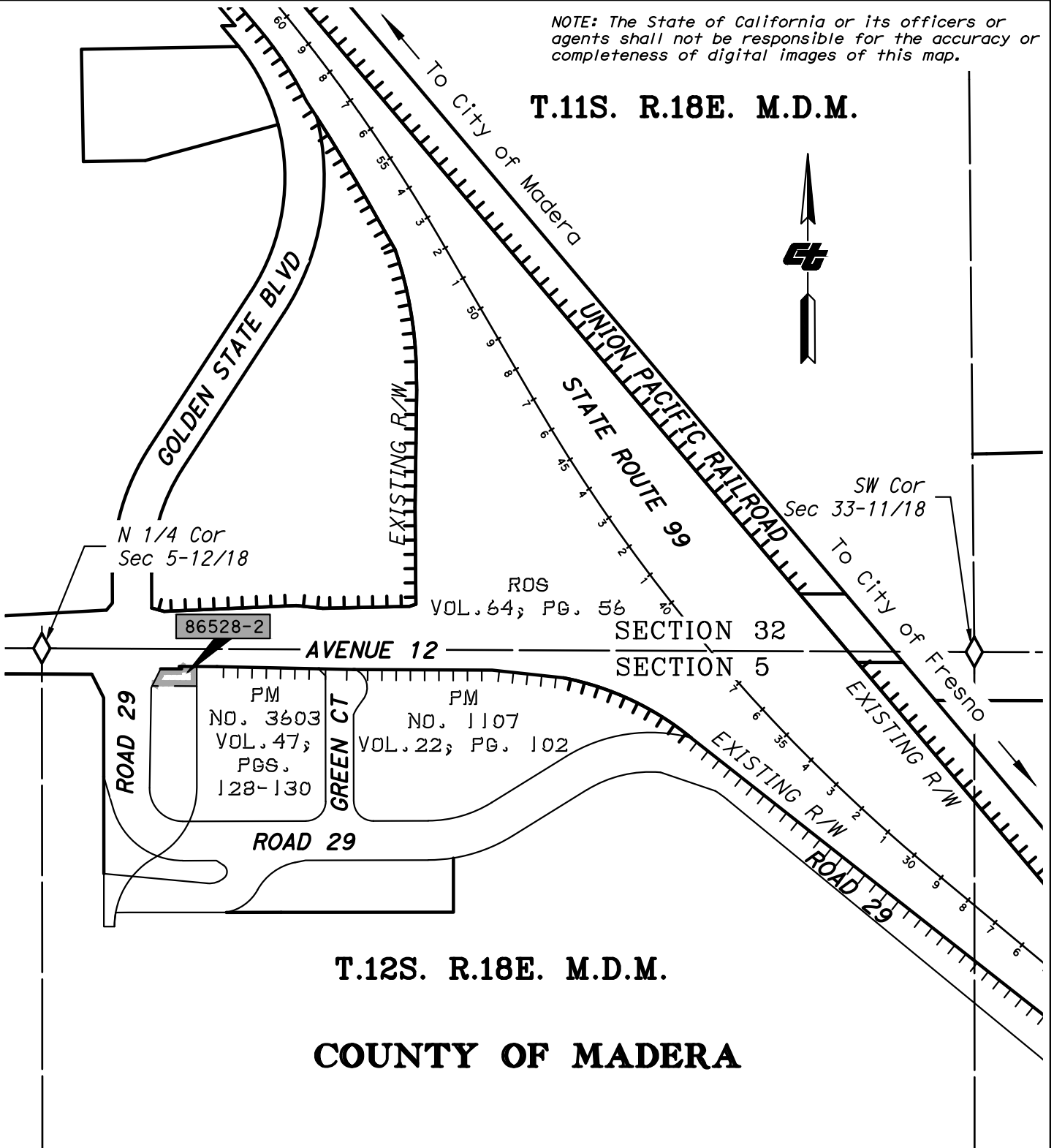
SCALE: 1" = 100'

FEET 0 50 100 200

DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
F. CARDENAS	11/5/2024	06	MAD	99	R7.45	2	2



NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

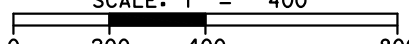
T.11S. R.18E. M.D.M.



T.12S. R.18E. M.D.M.

COUNTY OF MADERA

LEGEND				
				
Parcel DE 86528-2				
				
Access Restricted				

STATE OF CALIFORNIA		
CALIFORNIA STATE TRANSPORTATION AGENCY		
DEPARTMENT OF TRANSPORTATION		
RIGHT OF WAY		
DIRECTOR'S DEED MAP		
Exhibit A		
DE 86528-2		
SCALE: 1" = 400'		
		
FEET	0	200 400 800
DRAFTED BY	DATE	DISTRICT
F. CARDENAS	11/5/2024	06
COUNTY	ROUTE	SHEET PM
MAD	99	R7.45
SHEET NO.	TOTAL SHEETS	
1	2	

T.11S. R.18E. M.D.M.

SECTION 32

2,657.96'

SECTION LINE

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

AVENUE 12

SW Cor
Sec 33-11/18

N 1/4 Cor
Sec 5-12/18

444.47' (TIE)

SEE DETAIL 'A'
EXISTING R/W

86528-2

ROAD 29

ROS
VOL. 64, PG. 56

PARCEL 5

**WOODWARD WAY
(VACATED)**

50' GAS LINE EASEMENT TO
PG&E PER DOC. 2013-005042

5,971 Square Feet ROS
VOL. 64, PG. 56

PARCEL 4

PM NO. 3603
VOL. 47, PGS.
128-130

PARCEL 3

PM NO. 3603
VOL. 47, PGS.
128-130

VACATION FOR
WOODWARD WAY PER
RESOLUTION NO.
2019-031 RECORDED
3/28/2019 IN DOC.
2019-006227

GREEN COURT

T.12S. R.18E. M.D.M.

SECTION 5

COUNTY OF MADERA

ROAD 29

ROS
VOL. 64, PG. 56

DETAIL "A"

444.47'

L5

L6

L4

86528-2

NO SCALE

LINE DATA TABLE

NO.	DISTANCE
L1	56.12'
L2	127.17'
L3	54.56'
L4	59.91'
L5	9.91'
L6	50.00'

NOTES

Coordinates and bearings are on CCS 1983 Zone 3. Distances and stationing are grid distances. Divide by 1.00003181 to obtain ground distances. All distances are in feet unless otherwise noted.

LEGEND



Parcel DE 86528-2
Containing
5,971 Sq. Ft.



Access Restricted

**STATE OF CALIFORNIA
CALIFORNIA STATE TRANSPORTATION AGENCY
DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY
DIRECTOR'S DEED MAP**

**Exhibit B
DE 86528-2**

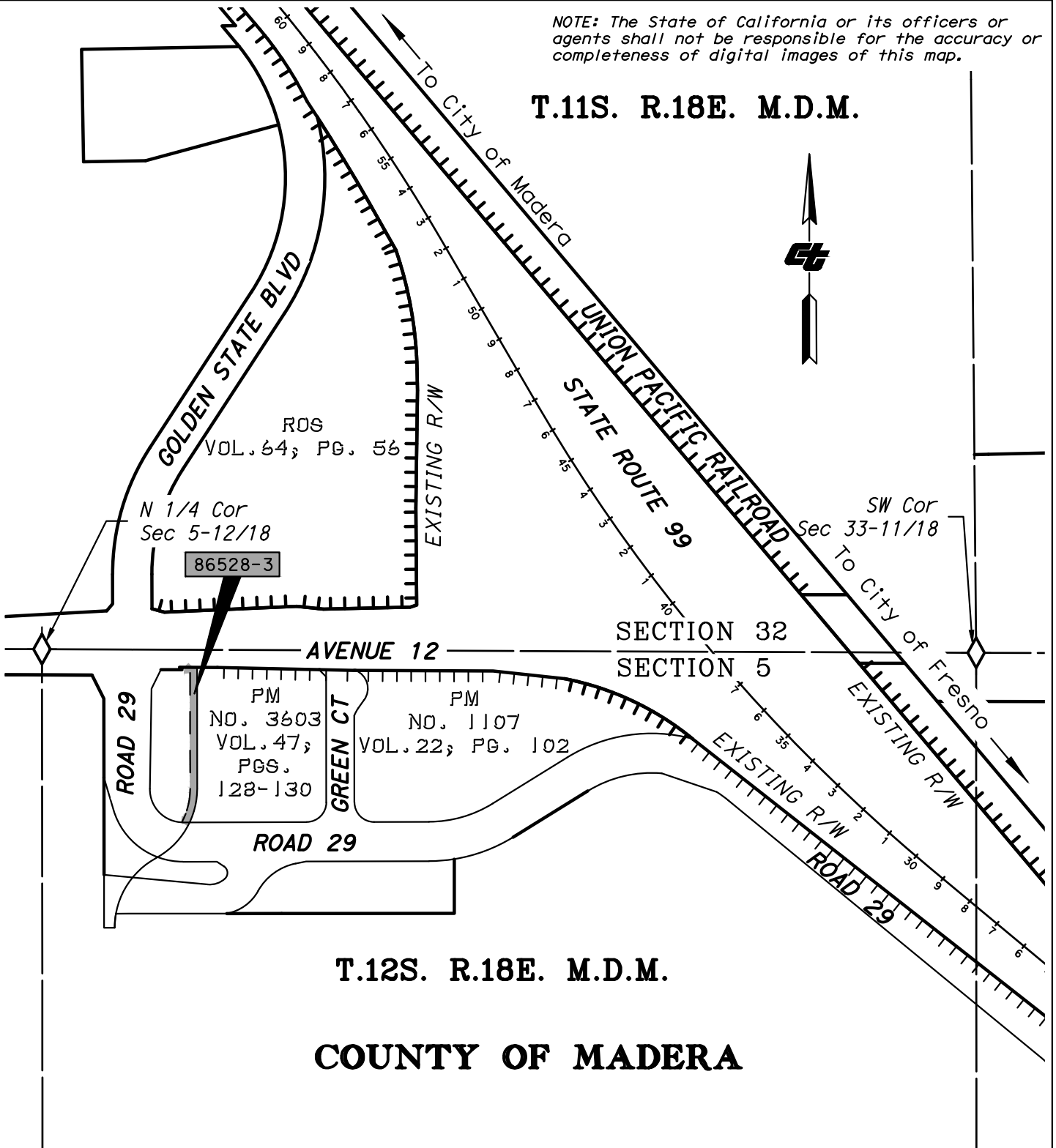
SCALE: 1" = 100'

FEET 0 50 100 200

DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
F. CARDENAS	11/5/2024	06	MAD	99	R7.45	2	2

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

T.11S. R.18E. M.D.M.



T.12S. R.18E. M.D.M.

COUNTY OF MADERA

LEGEND				
Parcel DE 86528-3				
Access Restricted				

STATE OF CALIFORNIA		
CALIFORNIA STATE TRANSPORTATION AGENCY		
DEPARTMENT OF TRANSPORTATION		
RIGHT OF WAY		
DIRECTOR'S DEED MAP		
Exhibit A		
DE 86528-3		
SCALE: 1" = 400'		
FEET	0	200 400 800
SHEET PM	R7.45	
SHEET NO.	1	
TOTAL SHEETS	2	

T.11S. R.18E. M.D.M.
SECTION 32

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

SECTION LINE

AVENUE 12

SW Cor

Sec 33-11/18

2,657.96'
EXISTING R/W

444.47' (TIE)

N 1/4 Cor
Sec 5-12/18

EAST LINE OF 10'
EASEMENT TO PG&E
RECORDED 8/28/1995
IN DOC. 9521267

ROAD 29

PARCEL 5

330.97'

**WOODWARD WAY
(VACATED)**

346.78'

NO SCALE

444.47'

35.85'
22.50'

86528-3

VACATION FOR
WOODWARD WAY PER
RESOLUTION NO.
2019-031 RECORDED
3/28/2019 IN DOC.
2019-006227

86528-3

9,018 Square Feet
PARCEL 4
PM NO. 3603
VOL. 47; PGS.
128-130

PARCEL 3

PM NO. 3603
VOL. 47; PGS.
128-130

T.12S. R.18E. M.D.M.
SECTION 5

NO SCALE

R=205.00'
L=98.04'
R=225.00'
L=97.36'

ROAD 29

ROS

VOL. 64; PG. 56

86528-3
R=94.00'
L=2.64'

19.62'

ROS
VOL. 64; PG. 56

WOODWARD WAY

COUNTY OF MADERA

NOTES

Coordinates and bearings
are on CCS 1983 Zone 3.
Distances and stationing
are grid distances.
Divide by 1.00003181 to
obtain ground distances.
All distances are in feet
unless otherwise noted.

LEGEND



Parcel DE 86528-3
Containing
9,018 Sq. Ft.



Access Restricted

STATE OF CALIFORNIA
CALIFORNIA STATE TRANSPORTATION AGENCY
DEPARTMENT OF TRANSPORTATION
**RIGHT OF WAY
DIRECTOR'S DEED MAP**

Exhibit B

DE 86528-3

SCALE: 1" = 100'

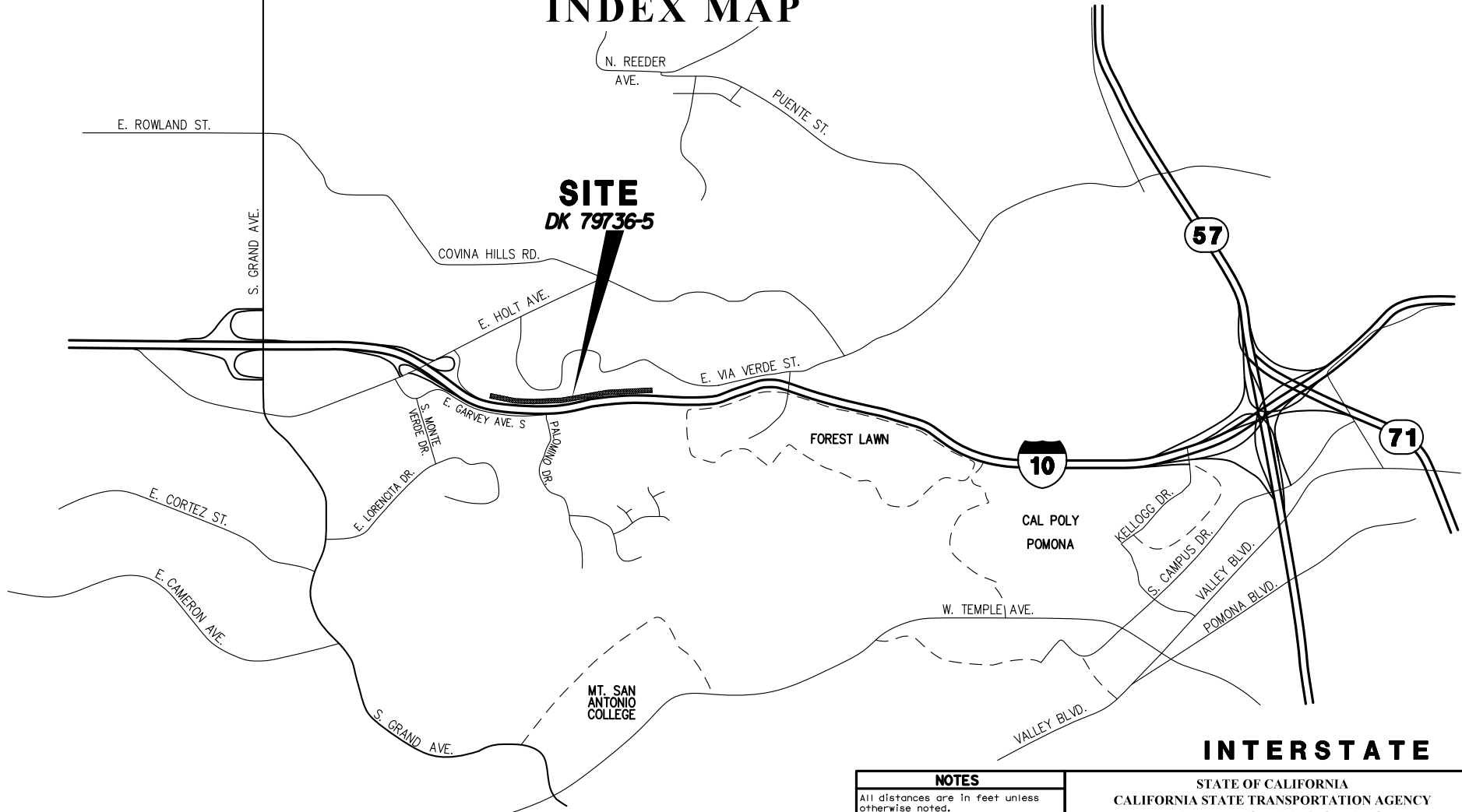
FEET 0 50 100 200

DRAFTED BY	DATE	DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
F. CARDENAS	11/5/2024	06	MAD	99	R7.45	2	2

EXHIBIT "A"

INDEX MAP

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.



CITY OF COVINA
COUNTY OF LOS ANGELES

INTERSTATE

NOTES					
All distances are in feet unless otherwise noted.					
LEGEND					
STATE RETAINS ACCESS RIGHTS					
STATE OF CALIFORNIA CALIFORNIA STATE TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION RIGHT OF WAY DIRECTOR'S DEED DK 79736-5 NOT TO SCALE					
REF.: R/W MAP NO. F-1866A-10, -11 & -12 DATE: 01-29-2025					
DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
07	LA	10	39.2-39.8	1	1

EXHIBIT "B"

PLAT MAP

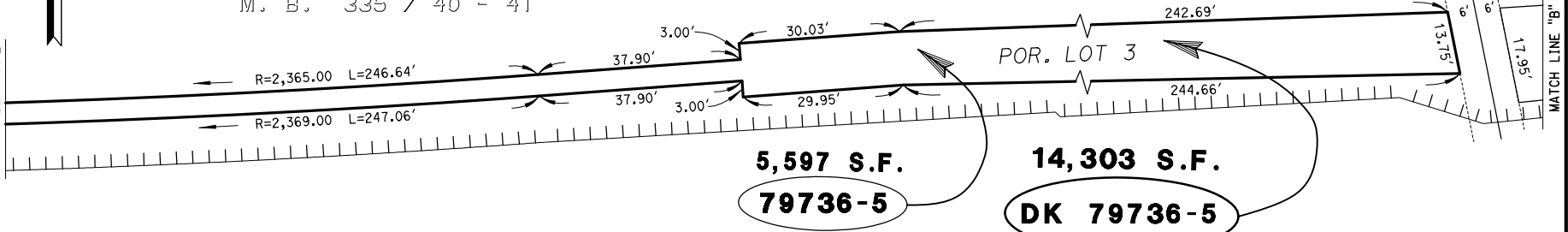
NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

TRACT NO. 15241

M.B. 335 / 40 - 41

LOT 3

MATCH LINE "B"
SEE SHEET NO. 2



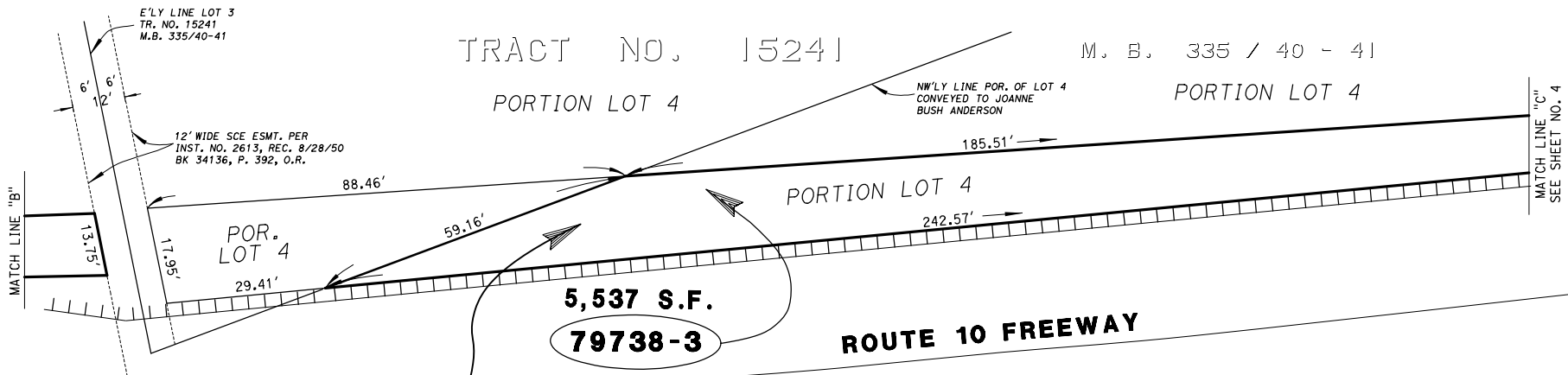
ROUTE 10 FREEWAY

TRACT NO. 15241

M.B. 335 / 40 - 41

PORTION LOT 4

PORTION LOT 4



ROUTE 10 FREEWAY

PORTIONS OF LOTS 3 & 4
OF TRACT NO. 15241
M.B. 335/40-41

PARCEL NO.	AREA
79736-5	5,597 SF
79738-3	5,537 SF
79739-2	3,169 SF
DK 79736-5	14,303 SF

14,303 S.F.
DK 79736-5

CITY OF COVINA
COUNTY OF LOS ANGELES

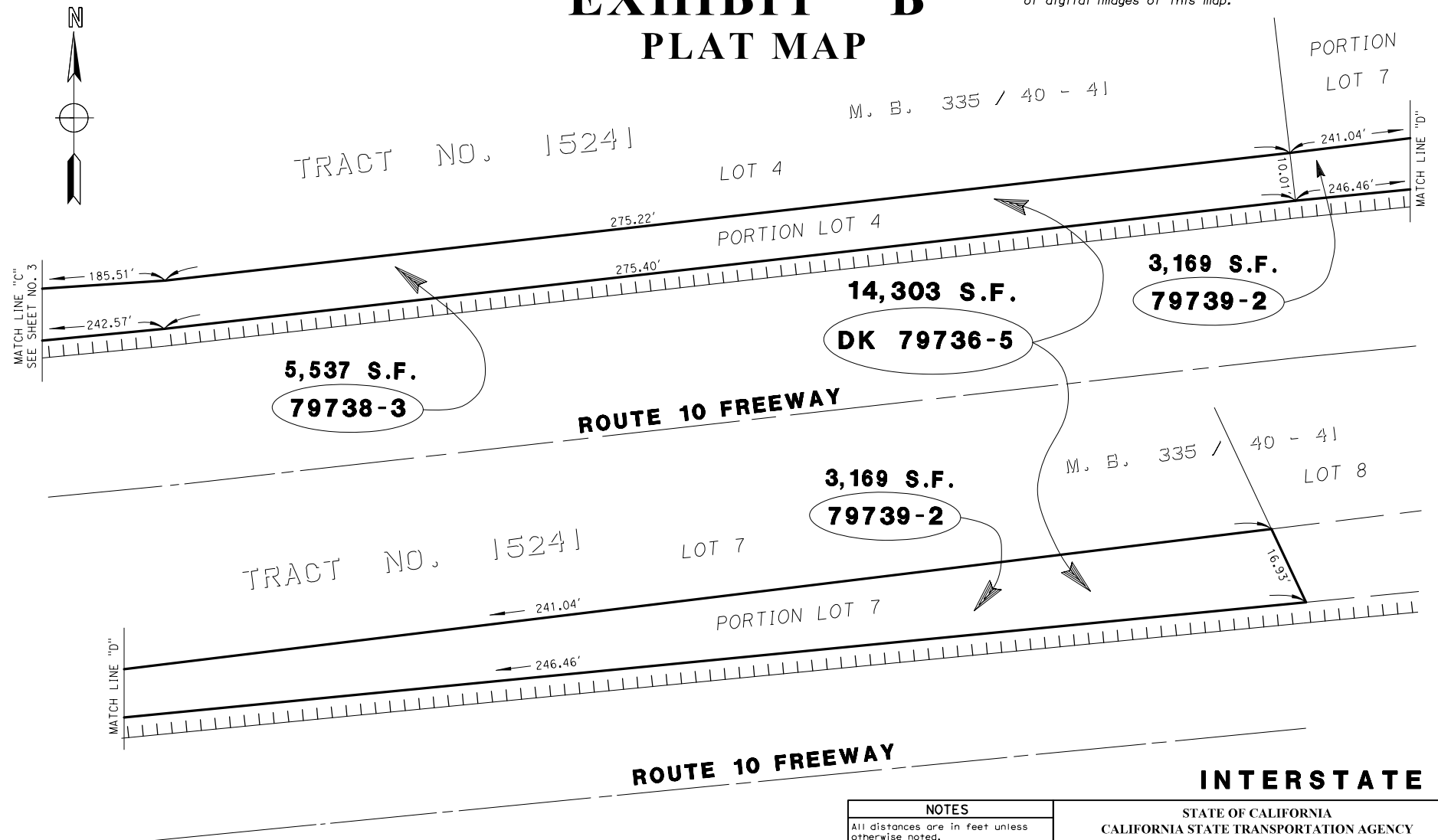
INTERSTATE

<div>NOTES</div> <div>All distances are in feet unless otherwise noted.</div>		<div>STATE OF CALIFORNIA</div> <div>CALIFORNIA STATE TRANSPORTATION AGENCY</div> <div>DEPARTMENT OF TRANSPORTATION</div> <div>RIGHT OF WAY</div> <div>DIRECTOR'S DEED</div> <div>DK 79736-5</div> <div>NOT TO SCALE</div>					
<div>LEGEND</div> <div> STATE RETAINS ACCESS RIGHTS</div>							
REF.: R/W MAP NO. F-1866A-10, -11 & -12 DATE: 01-29-2025							
DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS		
07	LA	10	39.2-39.8	3	4		

EXHIBIT "B"

PLAT MAP

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.



PORTIONS OF LOTS 4 & 7
OF TRACT NO. 15241
M.B. 335/40-41

PARCEL NO.	AREA
79736-5	5,597 SF
79738-3	5,537 SF
79739-2	3,169 SF
DK 79736-5	14,303 SF

CITY OF COVINA
COUNTY OF LOS ANGELES

INTERSTATE

STATE OF CALIFORNIA
CALIFORNIA STATE TRANSPORTATION AGENCY
DEPARTMENT OF TRANSPORTATION

**RIGHT OF WAY
DIRECTOR'S DEED
DK 79736-5**

NOT TO SCALE

NOTES
All distances are in feet unless otherwise noted.

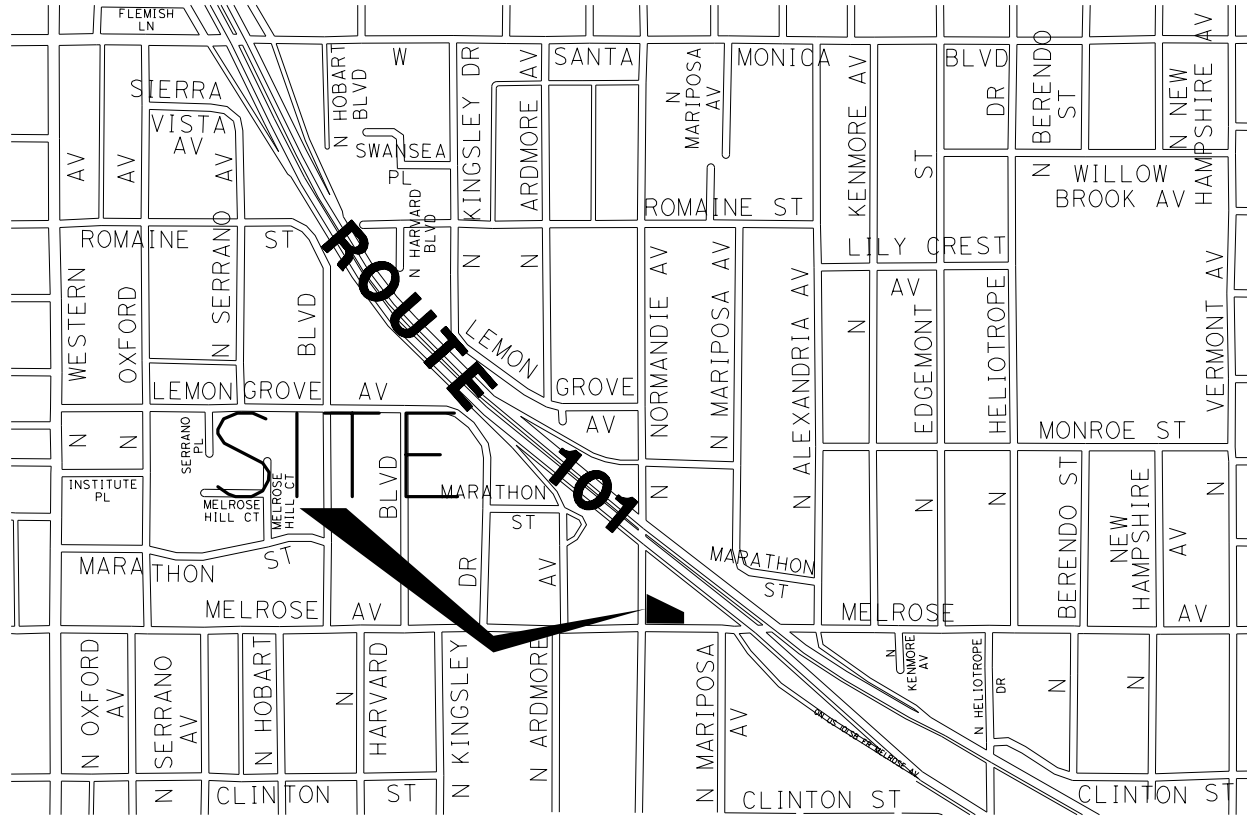
LEGEND
STATE RETAINS ACCESS RIGHTS

REF.: R/W MAP NO. F-1866A-10, -11 & -12 DATE: 01-29-2025					
DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
07	LA	10	39.2-39.8	4	4

EXHIBIT "A"

INDEX MAP

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.



CITY OF LOS ANGELES
COUNTY OF LOS ANGELES

**FOR CTC ONLY, NOT PART
OF THE DIRECTOR'S DEED**

<div>NOTES</div> <div>All distances are in meters unless otherwise noted.</div> <div>The bearings and distances in the hereinabove described lines are on the California Coordinate System of 1983 (1991.35), Zone 5. Divide by a combination factor of 1.00002871 to obtain ground level distances.</div>		<div>STATE OF CALIFORNIA</div> <div>CALIFORNIA STATE TRANSPORTATION AGENCY</div> <div>DEPARTMENT OF TRANSPORTATION</div> <div>RIGHT OF WAY</div> <div>DIRECTOR'S DEED</div> <div>DD 980582-01-01</div> <div>NOT TO SCALE</div>				
<div>LEGEND</div>		REF.: R/W MAP NO. F1377-5 EA: 0R00D2 DATE: 10-21-2024				
DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS	
07	LA	101	5.1	1	2	

EXHIBIT "B"

PLAT MAP

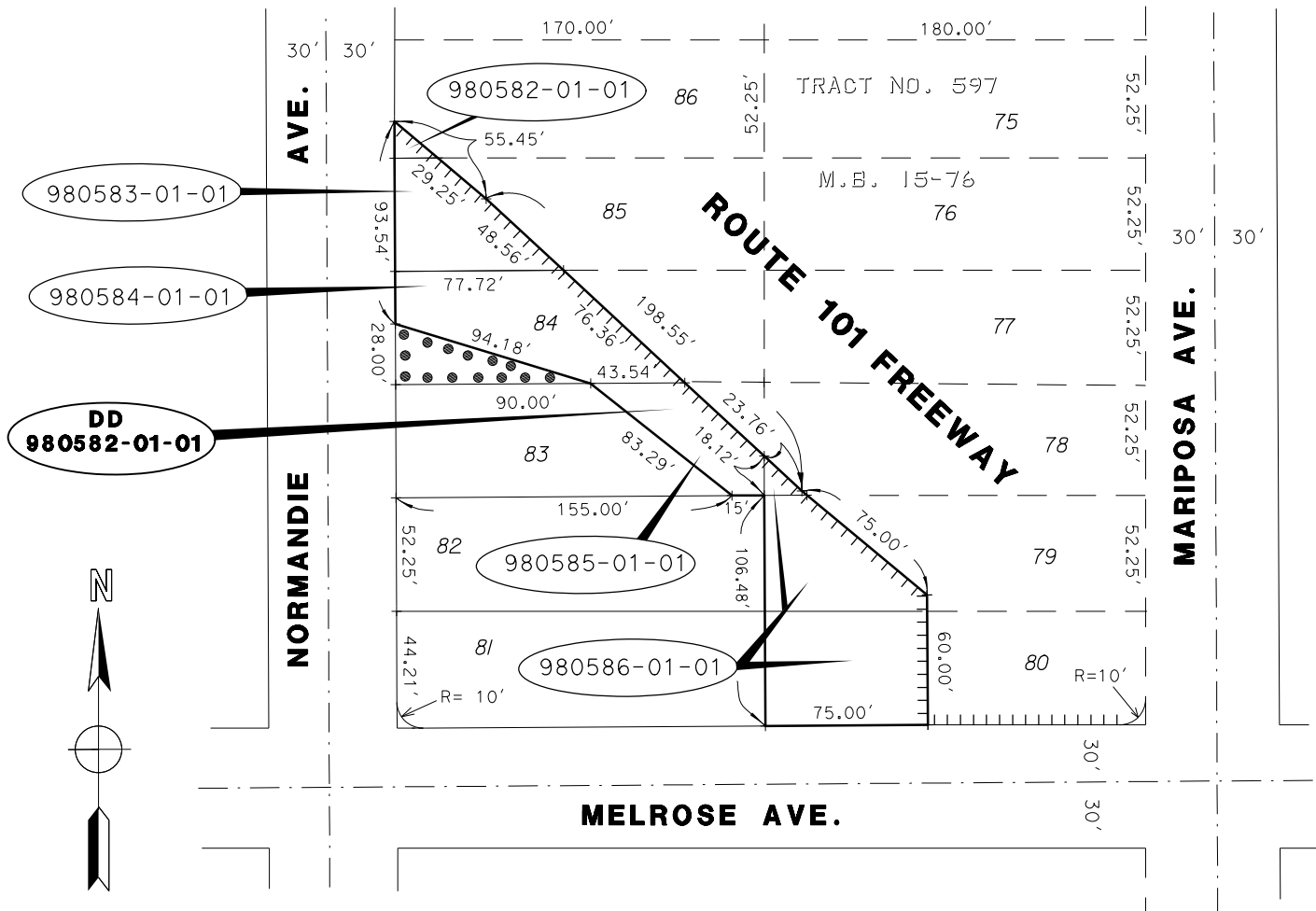
CITY OF LOS ANGELES
COUNTY OF LOS ANGELES

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

PARCEL NO.	AREA
980582-01-01 (274-01-01)	170 S.F.
980583-01-01 (275-01-01)	2,584 S.F.
980584-01-01 (276-01-01)	4,259 S.F.
980585-01-01 (277-01-01)	1,860 S.F.
980586-01-01 (249-01-01)	6,865 S.F.

DD 980582-01-01 15,738 S.F.

PORTIONS OF LOTS 78, 79, 80, 83,
84, 85 AND 86 OF TRACT NO. 597,
M.B. 15-76, OF MAPS



D-276 RECORDED 9-29-1952,
BOOK 39953, PAGE 110 OF OR.

Note: Complete and accurate utility data
should be verified by the leasee.

**FOR CTC ONLY, NOT PART
OF THE DIRECTOR'S DEED**

NOTES
All distances are in feet unless otherwise noted.
The bearings and distances in the hereinabove described lines are on the California Coordinate System of 1983 (1991.35), Zone 5. Divide by a combination factor of 1.00002871 to obtain ground level distances.
LEGEND
STATE RETAINS ACCESS RIGHTS

STATE OF CALIFORNIA
CALIFORNIA STATE TRANSPORTATION AGENCY
DEPARTMENT OF TRANSPORTATION
**RIGHT OF WAY
DIRECTOR'S DEED
DD 980582-01-01**

NOT TO SCALE

REF.: R/W MAP NO. F1377-5 EA: 0R00D2 DATE: 10-21-2024					
DISTRICT	COUNTY	ROUTE	SHEET PM	SHEET NO.	TOTAL SHEETS
07	LA	101	5.1	2	2

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(13), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **ALLOCATION AMENDMENT FOR A LOCALLY-ADMINISTERED
MULTI-FUNDED COORDINATED BORDER INFRASTRUCTURE PROGRAM
AND SENATE BILL 1 TRADE CORRIDOR ENHANCEMENT PROGRAM
PROJECT, ON THE STATE HIGHWAY SYSTEM
RESOLUTION FA-24-106, AMENDING RESOLUTION FA-23-107**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for the locally-administered multi-funded Coordinated Border Infrastructure Program (CBI) and Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) East Otay Mesa Land Port of Entry - Segment 3 project (PPNO 0999C), on the State Highway System, in San Diego County, to adjust the CBI funding line and revise the outputs?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation amendment for the locally-administered multi-funded CBI and SB 1 TCEP East Otay Mesa Land Port of Entry - Segment 3 project (PPNO 0999C), on the State Highway System, in San Diego County, to adjust the CBI funding line and revise the outputs.

BACKGROUND:

In May 2024, the Commission approved supplemental funds in the amount of \$15,000,000 in CBI Federal funds for the Plans, Specifications, and Estimate phase of this locally-administered multi-funded CBI and SB 1 TCEP East Otay Mesa Land Port of Entry - Segment 3 project (PPNO 0999C), under Resolution FA-23-107. At the time of the supplemental funds request, the CBI Federal funds were assigned to the incorrect program code. Therefore, the Department requests that the Commission approve this allocation amendment to move the CBI Federal funds to the correct program code. Additionally, this project will only provide the design work for the East Otay Mesa Land Port of Entry project

(PPNO 0999C). The construction work has been moved and will be delivered under the East Otay Mesa Land Port of Entry – Construction project (PPNO 0999I). Therefore, the outputs need to be revised.

The proposed changes are reflected in strikethrough and bold in the attached revised vote box. There is no change to the allocation amount.

FINANCIAL RESOLUTION:

Be it Resolved, that the locally-administered multi-funded CBI and SB 1 TCEP East Otay Mesa Land Port of Entry - Segment 3 project (PPNO 0999C), is hereby amended adjust the CBI Federal funding and revise the outputs, in accordance with the attached revised vote box.

Attachment

PPNO
Program
Funding Year
Item #
Fund Type
Program Codes
Project ID
Adv Phase
EA

Project Title	Location	Project Description

	State	State
State	Federal	Federal
Federal	Additional	Revised
Current Amount by Fund Type	Amount by Fund Type	Amount by Fund Type

1	East Otay Mesa Land Port of Entry - Segment 3. Near San Diego on Route 11 at 1.9 miles east of Sanyo Avenue Undercrossing (Mexico border). Construct Port of Entry at the border with Mexico.				11-0999C CBI/2022-23 PS&E 301-0890 FTF 20.20.400.300	\$10,641,000	\$15,000,000	\$25,641,000 \$10,641,000
San Diego Association of Governments	<u>Outputs</u>	<u>Unit</u>	<u>Total</u>					
<u>SANDAG</u>	Port improvement(s)	Each	4 0.01	CBI/2022-23				
San Diego	Border crossing improvement(s)	Each	4	PS&E				
11-SD-11				301-0890 FTF	\$7,300,000	\$15,000,000		\$22,300,000
2.7/2.7	Total revised amount \$38,841,000.			20.20.400.301				
	Supplemental funds are needed to complete PS&E.			TCEP-R/2021-22 PS&E				
	CEQA - EIR, 03/29/2012			301-3291	\$5,900,000			\$5,900,000
	NEPA - EIS, 03/29/2012			20.20.723.200 1100020521				
	Future consideration of funding approved under Resolution E-12-35; June 2020.			4PSEL 05634				
	This project will only provide the design work for the East Otay Mesa Land Port of Entry project (PPNO 11-0999C). The construction work has been moved and will be delivered under the East Otay Mesa Land Port of Entry - Construction project (PPNO 11-0999I).							
	<u>Amend Resolution FA-23-107 to move the \$15,000,000 in CBI Federal funding to the correct program code 20.20.400.301 and revise the outputs. There is no change to the allocation amount.</u>							

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(14)/2.5v.(1), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **ALLOCATION AMENDMENT FOR A STATE-ADMINISTERED MULTI-FUNDED
STATE TRANSPORTATION IMPROVEMENT PROGRAM AND PROPOSITION 1B
STATE ROUTE 99 PROJECT, ON THE STATE HIGHWAY SYSTEM
RESOLUTION FA-24-107, AMENDING RESOLUTION FA-24-48**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for the State-Administered multi-funded State Transportation Improvement Program (STIP) and Proposition 1B (Prop 1B) State Route 99 (SR 99) Delano to Pixley 6-Lane with Pavement Rehabilitation Project (PPNO 7072), on the State Highway System, in Kern and Tulare counties, to revise the Regional Transportation Planning Agency (RTPA), add Tulare County information, the county shares note, and revise the Regional Improvement Program (RIP) and Interregional Improvement Program (IIP) split amount?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation amendment for the State-Administered multi-funded STIP and Prop 1B SR 99 Delano to Pixley 6-Lane with Pavement Rehabilitation Project (PPNO 7072), on the State Highway System, in Kern and Tulare counties, to revise the RTPA, add Tulare County information, the county shares note, and revise the RIP and IIP split amount.

BACKGROUND:

In January 2025, the Commission approved supplemental funds in the amount of \$12,314,000 for the State-Administered multi-funded STIP and Prop 1B SR 99 Delano to Pixley 6-Lane with Pavement Rehabilitation Project (PPNO 7072), under Resolution FA-24-48. At the time of approval, the RTPA was incorrectly reflected, the County and Postmiles only reflected Kern County information, and the county shares note was inadvertently omitted. Additionally, the cost-sharing percentages between the RIP and the

IIP were not aligned with the original distributions as initially programmed. Therefore, the Department is requesting this allocation amendment to revise the RTPA from Kern Council of Governments to Tulare County Association of Governments, add Tulare County information, the county shares note, and revise the RIP and IIP split amount.

The proposed changes are reflected in strikethrough and bold in the attached revised vote box. There is no change to the allocation amount.

FINANCIAL RESOLUTION:

Be it Resolved, that the State-Administered multi-funded STIP and Prop 1B SR 99 Delano to Pixley 6-Lane with Pavement Rehabilitation Project (PPNO 7072), is hereby amended to revise the RTPA, add Tulare County information, the county shares note, and revise the RIP and IIP split amount, in accordance with the attached revised vote box.

Attachment

PPNO	Program	Funding Year	Item #	Fund Type	Program Codes	Project ID	Adv Phase	EA
------	---------	--------------	--------	-----------	---------------	------------	-----------	----

[illegible]

	State	State
State	Federal	Federal
Federal	Additional	Revised
Current Amount by Fund Type	Amount by Fund Type	Amount by Fund Type

Resolution FA-24-107
Amending Resolution FA-24-48

06-7072		
RIP/2022-23		
CONST		
301-0042 SHA	\$710,000	\$710,000
301-0890 FTF	\$5,472,000	\$5,472,000
20.20.075.600		
IIP/2022-23		
CONST		
301-0042 SHA	\$849,000	\$849,000
301-0890 FTF	\$6,551,000	\$6,551,000
20.20.025.700		
SR 99/2023-24		
R/W		
304-6072 SR99	\$3,000,000	\$3,000,000
20.20.722.000		
SR 99/2023-24		
CONST		
304-6072 SR99	\$27,900,000	\$27,900,000
20.20.722.000		
COVID-RIP(X)		
CON ENG		
001-0042 SHA	\$972,000	\$972,000
20.10.075.600		
RIP/2024-25		
CONST		
301-0042 SHA	\$858,000	\$858,000
	\$656,000	\$656,000
301-0890 FTF	\$6,618,000	\$6,618,000
20.20.075.600	\$5,062,000	\$5,062,000
IIP/2024-25		
CONST		
301-0042 SHA	\$555,000	\$555,000
	\$757,000	\$757,000
301-0890 FTF	\$4,283,000	\$4,283,000
20.20.025.700	\$5,839,000	\$5,839,000
0621000142		
4,9,3		
0W791		

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5s.(6), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: ALLOCATION AMENDMENT FOR A STATE-ADMINISTERED MULTI-FUNDED STATE TRANSPORTATION IMPROVEMENT PROGRAM, SENATE BILL 1 LOCAL PARTNERSHIP PROGRAM (COMPETITIVE AND FORMULAIC) PROJECT, ON THE STATE HIGHWAY SYSTEM RESOLUTION FP-24-93, AMENDING RESOLUTION FP-24-46 RESOLUTION LPP-A-2425-28, AMENDING RESOLUTION LPP-A-2425-14

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for the State-Administered multi-funded State Transportation Improvement Program (STIP), Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Competitive and Formulaic) Oakland Alameda Access Project (PPNO 0044D), on the State Highway System, in Alameda County, to revise the post miles?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation amendment for the State-Administered multi-funded STIP and SB 1 LPP (Competitive and Formulaic) Oakland Alameda Access Project (PPNO 0044D), on the State Highway System, in Alameda County, to revise the post miles.

BACKGROUND:

In December 2024, Commission approved \$67,625,000 for the Construction phase of the State-Administered multi-funded STIP and SB 1, LPP (Competitive and Formulaic) Oakland Alameda Access Project (PPNO 0044D), under Resolutions FP-24-46 and LPP-A-2425-14. Although the original request included the revised project post miles, it was inadvertently omitted at the time of allocation. Therefore, the Department requests that the Commission approve this allocation amendment, to revise the post miles as originally intended.

The proposed change is reflected in bold in the attached revised vote box. There is no change to the allocation amount. There is a concurrent contract award time extension request on this month's Commission agenda.

FINANCIAL RESOLUTION:

Be it resolved, that the State-Administered multi-funded STIP, SB 1 LPP (Competitive and Formulaic) Oakland Alameda Access Project (PPNO 0044D), is hereby amended to revise the post miles, in accordance with the attached revised vote box.

Attachment

Project #	Allocation Amount	Recipient	County	Dist-Co-Rte	Postmile	Project Title	Location	Project Description	PPNO	Program/Year	Phase	Prgm'd Amount	Project ID	Budget Year	Item #	Fund Type	Program Code	Amount by	Fund Type	
2.5s.(6) Allocation Amendment - Multi-Funded STIP and Senate Bill 1 LPP (Formulaic and Competitive) Project on the State Highway System																			Resolution FP-24-93 Amending Resolution FP-24-46 Resolution LPP-A-2425-28 Amending Resolution LPP-A-2425-14	
1	\$67,625,000	Department of Transportation	Alameda	04-Ala-260, 880	R0.78/R1.90, R30.47/R31.64	Oakland Alameda Access Project. Project is located on Interstate 880 and State Route 260 between 5th Avenue Overhead and 5th and 6th Street Viaduct within the Cites of Oakland and Alameda in Alameda County. The project proposes to improve access along I-880 and in and around the Webster and Posey Tubes (SR-260 tunnels under the Oakland Estuary [Tubes]), downtown Oakland, and the City of Alameda. Proposed improvements also include implementation of follow up landscaping work to restore removed vegetation required by the highway improvements.			04-0044D	RIP/24-25	CONST	\$18,000,000	CON ENG	2010-075.600				\$2,065,000		
												\$17,344,000		2024-25				\$15,935,000		
													LPP-C/24-25	301-0042 SHA				\$1,990,000		
													CONST	301-0890 FTF				\$15,354,000		
												\$25,000,000		2020.075.600				\$17,344,000		
						<u>Outcome/Outputs</u>														
						<u>Unit</u>														
						<u>Total</u>														
						Traffic monitoring detection station(s)														
						Each														
						Miles														
						Bicycle lane mile(s)														
						Miles														
						Roadway lane mile(s) - new														
						Miles														
						New bridge(s)/tunnel(s)														
						SQFT														
						110,523														
						CEQA - EIR, 08/20/2021; Re-validation 10/07/2024														
						NEPA - FONSI, 08/20/2021; Re-validation 10/07/2024														
						Future consideration of funding approved under Resolution E-22-36; May 2022.														
						Concurrent addendum for the previously approved future consideration of funding under Resolution E-25-41A; June 2025.														
						Right of Way Certification: 10/14/2024														
						Contribution from other sources: \$51,375,000.														

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6s.(5), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ALLOCATION AMENDMENT FOR A LOCALLY-ADMINISTERED SENATE BILL 1
LOCAL PARTNERSHIP PROGRAM (FORMULAIC) TRANSIT PROJECT
RESOLUTION LPP-A-2425-29, AMENDING RESOLUTION LPP-A-2122-18**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for the locally-administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) SMART Rail Operations Equipment Expansion (Phase 3) Transit project (PPNO 2318J), in various counties, to deallocate \$216,000 from the Construction (CON) phase to reflect project savings?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation amendment for the locally-administered SB 1 LPP (Formulaic) SMART Rail Operations Equipment Expansion (Phase 3) Transit project (PPNO 2318J), in various counties, to deallocate \$216,000 from the CON phase to reflect project savings.

BACKGROUND:

In March of 2022, the Commission approved \$679,000 for the CON phase of the SB 1 LPP (Formulaic) SMART Rail Operations Equipment Expansion (Phase 3) Transit project (PPNO 2318J), under Resolution LPP-A-2122-18. Construction has been completed and the local agency is requesting that \$216,000 be deallocated to reflect project savings.

The proposed changes are reflected in the strikethrough and bold in the attached revised vote box.

FINANCIAL RESOLUTION:

Be it Resolved, that the SB 1 LPP (Formulaic) SMART Rail Operations Equipment Expansion (Phase 3) Transit project (PPNO 2318J), is hereby amended to deallocate \$216,000 from the CON phase to reflect project savings, in accordance with the attached revised vote box.

Attachment

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5w.(4) - 2.5w.(11), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ALLOCATION AMENDMENT FOR LOCALLY-ADMINISTERED ACTIVE
TRANSPORTATION PROGRAM PROJECTS, OFF THE STATE HIGHWAY
SYSTEM**

ISSUE:

Should the California Transportation Commission (Commission) approve an a allocation amendment for eight locally-administered Active Transportation Program (ATP) projects, off the State Highway System, in Orange County, to split the original allocations between two Budget Authority fiscal years?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation amendment for eight locally-administered ATP projects, off the State Highway System, in Orange County, to split the original allocations between two Budget Authority fiscal years.

BACKGROUND:

The City of Santa Ana (City) conducted an audit that found indirect costs are owed to the City by the Department. Currently, the Department cannot pay back the total reimbursement amount of \$171,000 owed to the City via standard invoicing due to the lapsed budget authority.

The City has not expended all of the allocated funds for the project phases for the locally-administered ATP projects. To allow the City to liquidate the locally-administered ATP project allocations, current Budget Authority is needed in order to process invoices. Therefore, the Department requests the Commission approve this allocation amendment to split the Budget Authority for the eight locally-administered ATP projects, as shown in the attachment.

The changes are reflected in strikethrough and bold in the attached revised vote boxes. There is no change to the original allocation amounts.

Resolution FATP-2425-21, Amending:

<u>ATP Project</u>	<u>PPNO</u>	<u>Allocation Month/Year</u>	<u>Resolution</u>	<u>Allocated Amount</u>
City of Santa Ana West Willits Street Protected Bicycle Lanes (2.5w.(4))	12-2189	December 2018	FATP-1819-06	\$30,000
Safe Routes to Schools - Davis Elementary ADA Compliance (2.5w.(5))	12-2190	December 2018	FATP-1819-06	\$20,000
City of Santa Ana - West Willits Street Protected Bicycle Lanes (2.5w.(6))	12-2189	June 2019	FATP-1819-14	\$465,000
Safe Routes to School - Davis Elementary ADA Compliance (2.5w.(7))	12-2190	June 2019	FATP-1819-14	\$900,000
Kennedy Elementary and Villa Fundamental Intermediate SRTS (2.5w.(8))	12-2194	December 2019	FATP-1920-07	\$23,000
Fremont Elementary and Spurgeon Intermediate SRTS (2.5w.(9))	12-2195	December 2019	FATP-1920-07	\$84,000
McFadden Avenue Protected Bike Lane and Bicycle Boulevard Project (2.5w.(10))	12-2198	December 2019	FATP-1920-07	\$102,000
Standard Avenue Protected Bike Lane and Protected Intersection Project (2.5w.(11))	12-2199	December 2019	FATP-1920-07	\$122,000

FINANCIAL RESOLUTION:

Be it Resolved, that the Commission does hereby amend the eight locally-administered ATP projects, to split the original allocations between two Budget Authority fiscal years, in accordance with the attached revised vote boxes.

Attachments

2.5 Highway Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Amount by
RTPA/CTC		Advantage Phase	Program Code	Fund Type
District-County				
2.5w.(4) Allocation Amendment - ATP Project			Resolution FATP-2425-21	
			Amending Resolution FATP-1819-06	
1	City of Santa Ana West Willits Street Protected Bicycle Lanes. This	12-2189	2017-18	
\$30,000	project will construct median and parking protected bicycle lanes along West	ATP/18-19	108-3290 RMRA	\$30,000
	Willits Street from Fairview Street to Raitt Street with a bicycle-only signal	PA&ED	20.30.720.100	\$23,944
City of Santa	phase at the intersection of Willits Street and Sullivan Street.	\$30,000		
Ana		1219000022	2024-25	
OCTA	MPO	S	108-3290 RMRA	\$6,056
12-Orange			20.30.720.100	
<u>Outcome/Output:</u> The project outcome will provide safety enhancements in a disadvantaged community identified as a high pedestrian/bicyclist collision corridor.				
SB 1 Augmentation				
<u>Amend Resolution FATP-1819-06 to split the funding into two Budget Authority fiscal years as follows: \$23,944 in Budget Year 2017-18 and \$6,056 in Budget Year 2024-25. There is no change to the original allocation amount.</u>				

2.5 Highway Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Amount by
RTPA/CTC		Advantage Phase	Program Code	Fund Type
District-County				
2.5w.(5) Allocation Amendment - ATP Project			Resolution FATP-2425-21	
			Amending Resolution FATP-1819-06	
1	Safe Routes to Schools - Davis Elementary ADA Compliance. The	12-2190	2017-18	
\$20,000	project will construct bulb-outs at crossings adjacent to Davis Elementary	ATP/18-19	108-3290 RMRA	\$20,000
City of Santa Ana OCTA 12-Orange	School, reconstruct ADA-compliant curb ramps and sections of sidewalk and	PA&ED	20.30.720.100	\$7,040
	driveways along the safe routes to Davis Elementary School.	\$20,000		
	MPO	1219000021	2024-25	
		S	108-3290 RMRA	\$12,960
			20.30.720.100	
	<u>Outcome/Output:</u> The project outcome will encourage students to walk and ride their bicycles to and from school. The upgrades will allow active modes to access surrounding destinations.			
	SB 1 Augmentation			
	<u>Amend Resolution FATP-1819-06 to split the funding into two Budget Authority fiscal years as follows: \$7,040 in Budget Year 2017-18 and \$12,960 in Budget Year 2024-25. There is no change to the original allocation amount.</u>			

2.5 Highway Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Fund Type
RTPA/CTC		Advantage Phase	Program Code	
District-County				
2.5w.(6) Allocation Amendment - ATP Project			Resolution FATP-2425-21	
			Amending Resolution FATP-1819-14	
1	City of Santa Ana - West Willits Street Protected Bicycle Lanes. This	12-2189	2017-18	
\$465,000	project will construct median and parking protected bicycle lanes along West	ATP/18-19	108-3290 RMRA	\$465,000
	Willits Street from Fairveiw Street to Raitt Street with a bicycle-only signal	PS&E	20.30.720.100	\$285,141
City of Santa	phase at the intersection of Willits Street and Sullivan Street.	\$465,000		
Ana		1219000022	2024-25	
<u>OCTA</u>		S	108-3290 RMRA	\$179,859
12-Orange	MPO		20.30.720.100	
<u>Outcome/Output:</u> The project outcome will provide safety enhancements in a				
disadvantaged community identified as a high pedestrian and bicyclist				
collision corridor.				
CEQA - CE, 03/27/2019				
NEPA - CE, 04/29/2019				
SB 1 Augmentation				
<u>Amend Resolution FATP-1819-14 to split the funding into two Budget</u>				
<u>Authority fiscal years as follows: \$285,141 in Budget Year 2017-18 and</u>				
<u>\$179,859 in Budget Year 2024-25. There is no change to the original</u>				
<u>allocation amount.</u>				

Project #	PPNO			
Allocation Amount	Program/Year		Budget Year	
Recipient	Phase		Item #	
RTPA/CTC	Prgm'd Amount	Project Title	Fund Type	Amount by
District-County	Project ID	Location	Program Code	Fund Type
	Advantage Phase	Project Description		

2.5w.(7)		Allocation Amendment - ATP Project		Resolution FATP-2425-21 Amending Resolution FATP-1819-14	
1	Safe Routes to School - Davis Elementary ADA Compliance. The project will construct bulb-outs at crossings adjacent to Davis Elementary School, reconstruct ADA-compliant curb ramps and sections of sidewalk and driveways along the safe routes to Davis Elementary School.	12-2190	2017-18		
\$900,000		ATP/18-19	108-3290 RMRA	\$900,000	
		PS&E	20.30.720.100	\$764,238	
City of Santa Ana		\$900,000			
<u>OCTA</u>	MPO	1219000021	2024-25		
12-Orange		S	108-3290 RMRA		\$135,762
	<u>Outcome/Output:</u> The project outcome will encourage students to walk and ride their bicycles to and from school. The upgrades will allow active modes to access surrounding destinations.		20.30.720.100		
	CEQA - CE, 04/16/2019				
	NEPA - CE, 05/02/2019				
	SB 1 Augmentation				
	<u>Amend Resolution FATP-1819-14 to split the funding into two Budget Authority fiscal years as follows: \$764,238 in Budget Year 2017-18 and \$135,762 in Budget Year 2024-25. There is no change to the original allocation amount.</u>				

Project #	PPNO			
Allocation Amount	Program/Year	Budget Year		
Recipient	Phase	Item #		
RTPA/CTC	Prgm'd Amount	Fund Type		Amount by
District-County	Project ID	Program Code		Fund Type

2.5w.(8)		Allocation Amendment - ATP Project		Resolution FATP-2425-21 Amending Resolution FATP-1920-07	
1	Kennedy Elementary and Villa Fundamental Intermediate SRTS.	12-2194	2018-19		
\$23,000	Pedestrian and bicyclist traffic safety improvements for Kennedy Elementary	ATP/19-20	108-3290 RMRA	\$23,000	
	and Villa Fundamental Intermediate routes to school. Work includes	PA&ED	20.30.720.100	\$11,316	
City of Santa	bulbouts, sidewalk, curb ramps and bikeways.	\$23,000			
Ana		1220000019			
<u>OCTA</u>	Statewide		2024-25		
12-Orange			108-3290 RMRA		\$11,684
			20.30.720.100		
	<u>Outcome/Output:</u> The project outcome will provide safety to students while walking or riding their bicycles to and from school, sidewalk and ramp upgrades allow easier access to surrounding areas and facilities for physically challenged.				
	SB 1 Augmentation				
	<u>Amend Resolution FATP-1920-07 to split the funding into two Budget Authority fiscal years as follows: \$11,316 in Budget Year 2018-19 and \$11,684 in Budget Year 2024-25. There is no change to the original allocation amount.</u>				

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(9) Allocation Amendment - ATP Project			Resolution FATP-2425-21 Amending Resolution FATP-1920-07	
1 \$84,000	Fremont Elementary and Spurgeon Intermediate SRTS. In the City of Santa Ana, the safe routes to school for Fremont Elementary and Spurgeon Intermediate. Pedestrian and bike traffic safety improvements for Fremont Elementary and Spurgeon Intermediate safe routes to school. Work includes bulbouts, sidewalk, curb ramp and bikeways.	12-2195 ATP/19-20 PA&ED \$84,000 1220000017	2018-19 108-3290 RMRA 20.30.720.100	\$84,000 \$26,858
City of Santa Ana OCTA 12-Orange	Statewide		2024-25 108-3290 RMRA 20.30.720.100	\$57,142
<u>Outcome/Output:</u> The project output will produce 2,383 linear feet (LF) of new sidewalk, 10,824 LF of Class III bikeway and a road diet with 5,280 LF of Class II bikeway.				
SB 1 Augmentation				
<u>Amend Resolution FATP-1920-07 to split the funding into two Budget Authority fiscal years as follows: \$26,858 in Budget Year 2018-19 and \$57,142 in Budget Year 2024-25. There is no change to the original allocation amount.</u>				

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(10) Allocation Amendment - ATP Project			Resolution FATP-2425-21 Amending Resolution FATP-1920-07	
1 \$102,000 City of Santa Ana OCTA 12-Orange	McFadden Avenue Protected Bike Lane and Bicycle Boulevard Project. McFadden Avenue from Harbor Boulevard to Grand Avenue in the City of Santa Ana. McFadden Avenue Class IV protected bike lane and Class III bicycle boulevard from Harbor Boulevard to Grand Avenue in the City of Santa Ana. MPO	12-2198 ATP/19-20 PA&ED \$102,000 1220000018	2018-19 108-3290 RMRA 20.30.720.100 2024-25 108-3290 RMRA 20.30.720.100	\$102,000 \$69,665 \$32,335
<u>Outcome/Output:</u> The project output will provide a safe and low stress mobility option, decrease car trips, vehicle miles traveled, improves air quality, reduces greenhouse gas emissions and promotes healthy lifestyles.				
SB 1 Augmentation				
<u>Amend Resolution FATP-1920-07 to split the funding into two Budget Authority fiscal years as follows: \$69,665 in Budget Year 2018-19 and \$32,335 in Budget Year 2024-25. There is no change to the original allocation amount.</u>				

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(11) Allocation Amendment - ATP Project			Resolution FATP-2425-21 Amending Resolution FATP-1920-07	
1 \$122,000 City of Santa Ana OCTA 12-Orange	Standard Avenue Protected Bike Lane and Protected Intersection Project. Standard Avenue from 3rd Street to Warner Avenue in the City of Santa Ana. Standard Avenue Class IV protected bike lane, Class II buffered bike lane and Class III from 3rd Street to Warner Avenue and protected intersection project at McFadden Avenue and Standard Avenue in the City of Santa Ana. MPO <u>Outcome/Output:</u> The project output will create 1,700 linear feet (LF) of Class III bikeway; 4,000 LF of Class II bikeway; 5,900 LF of Class IV bikeway; 11,800 LF of new sidewalk and 9,900 LF of road diet. SB 1 Augmentation <u>Amend Resolution FATP-1920-07 to split the funding into two Budget Authority fiscal years as follows: \$69,023 in Budget Year 2018-19 and \$52,977 in Budget Year 2024-25. There is no change to the original allocation amount.</u>	12-2199 ATP/19-20 PA&ED \$122,000 1220000016	2018-19 108-3290 RMRA 20.30.720.100 2024-25 108-3290 RMRA 20.30.720.100	\$122,000 \$69,023 \$52,977

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5w.(12) – 2.5w.(24), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ALLOCATION AMENDMENT FOR LOCALLY-ADMINISTERED ACTIVE
TRANSPORTATION PROGRAM PROJECTS, OFF THE STATE HIGHWAY
SYSTEM, FROM CYCLES 3A, 4, 5 AND 6**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for 13 locally-administered Active Transportation Program (ATP) projects, off the State Highway System, to rescind a combined total of \$9,372,000 from ATP Cycles 3A, 4, 5, and 6 due to these projects being canceled by the agency?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation amendment for 13 locally-administered ATP projects, off the State Highway System, to rescind a combined total of \$9,372,000 from ATP Cycles 3A, 4, 5, and 6 due to these projects being canceled by the agency.

The deallocated funds from the 13 locally-administered ATP projects will now be available for reprogramming in future ATP cycles. A breakdown of the specific project phases for each cycle is shown on the following page.

Cycle	Total Funds Deallocated	Funding Available for Future ATP Cycle
Cycle 3A	\$7,682,000	\$7,682,000
Cycle 4	\$98,000	\$98,000
Cycle 5	\$1,382,000	\$1,382,000
Cycle 6	\$210,000	\$210,000
Total	\$9,372,000	\$9,372,000

RESCISSIONS CYCLE 3A - Resolution FATP-2425-22, Amending:

<u>ATP Project</u>	<u>PPNO</u>	<u>Allocation Month/Year</u>	<u>Resolution</u>	<u>Current Project Allocation</u>	<u>Amount to be Rescinded</u>	<u>Revised Allocation Amount</u>
Alondra Active Transportation Improvement Project (2.5w.(12))	07-5426	June 2018	FATP-1718-19	\$12,000	\$12,000	\$0
City of Santa Ana First Street Pedestrian Improvements (2.5w.(13))	12-2188	December 2018	FATP-1819-06	\$10,000	\$10,000	\$0
Puente Creek Bikeway (2.5w.(14))	07-5428A	June 2019	FATP-1819-14	\$400,000	\$400,000	\$0
City of Santa Ana - First Street Pedestrian Improvements (2.5w.(15))	12-2188	June 2019	FATP-1819-14	\$530,000	\$530,000	\$0
Alondra Active Transportation Improvement Project (2.5w.(16))	07-5426	January 2021	FATP-2021-10	\$138,000	\$138,000	\$0
City of Santa Ana - First Street Pedestrian Improvements (2.5w.(17))	12-2188	May 2022	FATP-2122-23	\$4,032,000	\$4,032,000	\$0
Puente Creek Bikeway (2.5w.(18))	07-5428A	March 2023	FATP-2223-23	\$2,360,000	\$2,360,000	\$0
Puente Creek Bikeway (2.5w.(19))	07-5428B	March 2023	FATP-2223-23	\$200,000	\$200,000	\$0
COMBINED CYCLE 3A TOTAL					\$7,682,000	

RESCISSIONS CYCLE 4 - Resolution FATP-2425-23, Amending:

<u>ATP Project</u>	<u>PPNO</u>	<u>Allocation Month/Year</u>	<u>Resolution</u>	<u>Current Project Allocation</u>	<u>Amount to be Rescinded</u>	<u>Revised Allocation Amount</u>
Oxnard Boulevard Bikeway Gap Closure (2.5w.(20))	07-5667	June 2020	FATP-1920-19	\$98,000	\$98,000	\$0
COMBINED CYCLE 4 TOTAL					\$98,000	

RESCISSIONS CYCLE 5 - Resolution FATP-2425-24, Amending:

<u>ATP Project</u>	<u>PPNO</u>	<u>Allocation Month/Year</u>	<u>Resolution</u>	<u>Current Project Allocation</u>	<u>Amount to be Rescinded</u>	<u>Revised Allocation Amount</u>
Southeast Eastvale SRTS Equitable Access Project (2.5w.(21))	08-1281	August 2021	FATP-2122-01	\$150,000	\$150,000	\$0
Cottonwood Active Transportation Trunk Line Express (CATTLE) Network (2.5w.(22))	02-2607A	January 2022	FATP-2122-13	\$1,197,000	\$1,197,000	\$0
City of Perris Bike and Pedestrian Network Project (2.5w.(23))	08-1273B	March 2024	FATP-2324-28	\$35,000	\$35,000	\$0
COMBINED CYCLE 5 TOTAL					\$1,382,000	

RESCISSIONS CYCLE 6 - Resolution FATP-2425-25, Amending:

<u>ATP Project</u>	<u>PPNO</u>	<u>Allocation Month/Year</u>	<u>Resolution</u>	<u>Current Project Allocation</u>	<u>Amount to be Rescinded</u>	<u>Revised Allocation Amount</u>
Chester Avenue between 4th Street to Brundage Lane (2.5w.(24))	06-7083	June 2023	FATP-2223-31	\$210,000	\$210,000	\$0
COMBINED CYCLE 6 TOTAL					\$210,000	

BACKGROUND:

The Department requests that the Commission approve this amendment to rescind a combined total of \$9,372,000 from ATP Cycles 3A, 4, 5, and 6 from the 13 locally-administered ATP projects, by the amounts shown in the tables above, due to these projects being canceled by the agency.

FINANCIAL RESOLUTION

Be it Resolved, that the Commission does hereby amend the 13 locally-administered ATP projects, to rescind a combined total of \$9,372,000 from ATP Cycles 3A, 4, 5, and 6 due to these projects being canceled by the agency, in accordance with the attached revised vote boxes.

Attachments

2.5 Highway Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Program Code
RTPA/CTC		Advantage Phase	Program Code	Fund Type
District-County				
2.5w.(12) Allocation Amendment - ATP Project			Resolution FATP-2425-22	
			Amending Resolution FATP-1718-19	
1	Alondra Active Transportation Improvement Project. Alondra Boulevard.	07-5426	2017-18	
\$12,000	between Studebaker Road and Pioneer Boulevard just north of Cerritos	ATP/17-18	108-3290 RMRA	\$12,000
\$0	College. Alondra Boulevard is a four-lane road that acts as a main	PA&ED	20.30.720.100	\$0
City of Norwalk <u>LACMTA</u> 07-Los Angeles	thoroughfare for Cerritos College (student enrollment nearly 24,000), and the	\$12,000		
	Norwalk La Mirada Adult School (with Design and construct 12,000 LF of	\$0		
	Class II bicycle lanes and improve 2,000 LF of sidewalk on Alondra	0718000304		
	Boulevard. This project is part of a long-range project identified in the	S		
	Gateway Cities 2014 Strategic Transportation Plan to create over 14 miles of			
	bike lanes.			
	Statewide			
	<u>Outcome/Output:</u> The resultant of this project is to decrease pedestrian			
	incidents and encourage the use of safe and separate route to school and			
	reducing pedestrian related incidents.			
	SB1 Augmentation			
	Concurrent ATP Programming Amendments under Resolutions G-25-48			
	and G-25-49; June 2025.			
	<u>Amend Resolution FATP-1718-19 to rescind the full amount of \$12,000</u>			
	<u>in ATP PA&ED due to cancellation of the project.</u>			

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Advantage Phase	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(13) Allocation Amendment - ATP Project		Resolution FATP-2425-22 Amending Resolution FATP-1819-06		
1 \$10,000 \$0 City of Santa Ana OCTA 12-Orange	City of Santa Ana First Street Pedestrian Improvements. The project will widen the existing sidewalks by three feet, narrow the vehicle lanes, construct ADA improvements on sidewalks and wheel chair ramps, provide high visibility marked crosswalks, and add a signal-controlled pedestrian crossing on First Street from Flower Street to Grand Avenue, Statewide <u>Outcome/Output:</u> The project outcome will provide safety enhancements in a disadvantaged community and will reduce pedestrian/bicyclist collisions. SB 1 Augmentation Concurrent ATP Programming Amendments under Resolutions G-25-48 and G-25-49; June 2025. <u>Amend Resolution FATP-1819-06 to rescind the full amount of \$10,000 in ATP PA&ED due to cancellation of the project.</u>	12-2188 ATP/18-19 PA&ED \$10,000 \$0 1219000020 S	2017-18 108-3290 RMRA 20.30.720.100	\$10,000 \$0

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Advantage Phase	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(14)	Allocation Amendment - ATP Project		Resolution FATP-2425-22 Amending Resolution FATP-1819-14	
1 \$400,000 \$0	Puente Creek Bikeway. Cities of La Puente and Industry and unincorporated Valinda community. Design and construct 1.86 miles of Class I bike path along Puente Creek and 0.37 miles of enhanced Class III bike route along surface streets.	07-5428A ATP/18-19 PS&E \$400,000 \$0 0719000133 S	2017-18 108-3290 RMRA 20.30.720.100	\$400,000 \$0
Los Angeles County LACMTA 07-Los Angeles	Statewide <u>Outcome/Output:</u> Puente Creek Bikeway and San Jose Creek Bike Path will connect area residents to the regional San Gabriel River Bike Path and expand mobility options for San gabriel Valley residents to walk, bike, or connect to transit for daily commuting. CEQA - EIR, 05/07/2013 NEPA - CE, 01/22/2019 Future consideration of funding approved under Resolution E-13-33; May 2013. SB 1 Augmentation Concurrent ATP Programming Amendments under Resolutions G-25-48 and G-25-49; June 2025. <u>Amend Resolution FATP-1819-14 to rescind the full amount of \$400,000 in ATP PS&E due to cancellation of the project.</u>			

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Advantage Phase	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(15)	Allocation Amendment - ATP Project		Resolution FATP-2425-22 Amending Resolution FATP-1819-14	
1 \$530,000 \$0 City of Santa Ana OCTA 12-Orange	City of Santa Ana - First Street Pedestrian Improvements. In the City of Santa Ana, First Street between Flower Street and Grand Avenue. Widen the existing sidewalks by three feet, narrow the vehicle lanes, constructed ADA improvements on sidewalks and wheel chair ramps, and provide high visibility marked crosswalks, and add a signal controlled pedestrian crossing along First Street. <u>Outcome/Output:</u> Provide safety enhancements in a disadvantaged community and reduce pedestrian and bicyclist collisions. CEQA - CE, 03/27/2019 NEPA - CE, 04/29/2019 SB 1 Augmentation Concurrent ATP Programming Amendments under Resolutions G-25-48 and G-25-49; June 2025. <u>Amend Resolution FATP-1819-14 to rescind the full amount of \$530,000 in ATP PS&E due to cancellation of the project.</u>	12-2188 ATP/18-19 PS&E \$530,000 \$0 1219000020 S	2017-18 108-3290 RMRA 20.30.720.100	\$530,000 \$0

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(16) Allocation Amendment - ATP Project			Resolution FATP-2425-22 Amending Resolution FATP-2021-10	
1 \$138,000 \$0	Alondra Active Transportation Improvement Project. Alondra Boulevard between Studebaker Road and Pioneer Boulevard just north of Cerritos College. Alondra Boulevard. is a four-lane road that acts as a main thoroughfare for Cerritos College student enrollment nearly 24,000, and the Norwalk La Mirada Adult School with over 18,000 students. Design and construct 12,000 linear feet (LF) of Class II bicycle lanes and improve 2,000 LF of sidewalk on Alondra Boulevard. This project is part of a long-range project identified in the Gateway Cities 2014 Strategic Transportation Plan to create over 14 miles of bike lanes along this corridor.	07-5426 ATP/18-19 PS&E \$138,000 \$0 0718000304	2019-20 108-3290 RMRA 20.30.720.100	\$138,000 \$0
Norwalk, City of <u>LACMTA</u> 07-Los Angeles	<p>Statewide</p> <p><u>Outcome/Output:</u> The 2,000 feet of reconstructed/enhanced sidewalks and 12,000 feet of Class II bike lanes will encourage the use of safe and separate routes to school and reducing pedestrian related incidents.</p> <p>CEQA - NOE, 04/01/2020</p> <p>Time Extension for FY 18-19 PS&E expires February 28, 2021.</p> <p>SB 1 Augmentation</p> <p>Concurrent ATP Programming Amendments under Resolutions G-25-48 and G-25-49; June 2025.</p> <p><u>Amend Resolution FATP-2021-10 to rescind the full amount of \$138,000 in ATP PS&E due to cancellation of the project.</u></p>			

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Advantage Phase	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(17)	Allocation Amendment - ATP Project	Resolution FATP-2425-22 Amending Resolution FATP-2122-23		
1 \$4,032,000 \$0 City of Santa Ana OCTA 12-Orange	City of Santa Ana - First Street Pedestrian Improvements. In the City of Santa Ana, First Street between Flower Street and Grand Avenue. Widen the existing sidewalks by three feet, narrow the vehicle lanes, constructed ADA improvements on sidewalks and wheel chair ramps, and provide high visibility marked crosswalks, and add a signal controlled pedestrian crossing along First Street. <u>Outcome/Output:</u> Reconstruct 11,700 feet sidewalk and 48 curb ramps. Install one traffic signal and enhance 42 crosswalks. CEQA - CE, 03/21/2022 NEPA - CE, 03/21/2022 Right of Way Certification: 3/25/2022 Time extension for FY 19-20 CON expires on June 30, 2022 Concurrent ATP Programming Amendments under Resolutions G-25-48 and G-25-49; June 2025. <u>Amend Resolution FATP-2122-23 to rescind the full amount of \$4,032,000 in ATP CON due to cancellation of the project.</u>	12-2188 ATP/19-20 CONST \$4,032,000 \$0 1219000020	2020-21 108-0890 FTF 20.30.720.100	\$4,032,000 \$0

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Advantage Phase	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(18) Allocation Amendment - ATP Project		Resolution FATP-2425-22 Amending Resolution FATP-2223-23		
1 \$2,360,000 \$0 County of Los Angeles LACMTA 07-Los Angeles	Puente Creek Bikeway. Cities of La Puente and Industry, and the unincorporated Valinda Community. The project would include improvements to the existing sidewalk and bicycle infrastructure along Franklin Street, which currently serves Sierra Vista Elementary School. Improvements include installing sidewalks, curbs, gutters, ADA-compliant ramps, crossing markings, high visibility crosswalks, bike sharrows and enhancements to existing sidewalks, driveways, curbs, and gutters. Statewide <u>Outcome/Output:</u> Install 1.84 miles of Class I bike path along Puente Creek and 0.37 miles of Enhanced Class III bike route along surface streets. The remaining 131 feet of Class I bike path will be delivered in Phase II. CEQA - EIR, 05/07/2013 NEPA - CE, 04/18/2022 Right of Way Certification: 10/19/2022 Future consideration of funding approved under Resolution E-13-33; May 2013. Time extension for FY 20-21 CONST expires on February 28, 2023 PPNO 5428A is the infrastructure component to PPNO 5428B. Commission staff recommended approval of a minor scope change on 03/06/2023 to deliver 131 feet of Class I bike path in Phase II using local funds. Concurrent ATP Programming Amendments under Resolutions G-25-48 and G-25-49; June 2025. <u>Amend Resolution FATP-2223-23 to rescind the full amount of \$2,360,000 in ATP CON due to cancellation of the project.</u>	07-5428A ATP/20-21 CONST \$2,360,000 \$0 0719000133	2022-23 108-0890 FTF 20.30.720.100	\$2,360,000 \$0

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Advantage Phase	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(19) Allocation Amendment - ATP Project			Resolution FATP-2425-22 Amending Resolution FATP-2223-23	
1 \$200,000 \$0	Puente Creek Bikeway. The non-infrastructure portion of the project includes bicycle and pedestrian safety education and encouragement training workshops and rodeos to students at 3 elementary, 1 middle, and 1 high school located near the proposed bikeway.	07-5428B ATP/20-21 CONST \$200,000 \$0 0722000185	2022-23 108-0890 FTF 20.30.720.100	\$200,000 \$0
County of Los Angeles LACMTA 07-Los Angeles	Statewide <u>Outcome/Output:</u> Bicycle and pedestrian lesson plans for 5 schools, volunteer training, bicycle and pedestrian safety workshops, a before and after survey, and final report. CEQA - EIR, 05/07/2013 NEPA - CE, 04/18/2022 PPNO 5428B is the Non-Infrastructure component to PPNO 5428A Future consideration of funding approved under Resolution E-13-33; May 2013. Time Extension for FY 20-21 CON-NI expires on February 28, 2023 Concurrent ATP Programming Amendments under Resolutions G-25-48 and G-25-49; June 2025. <u>Amend Resolution FATP-2223-23 to rescind the full amount of \$200,000 in ATP CON due to cancellation of the project.</u>			

2.5 Highway Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Fund Type
RTPA/CTC		Advantage Phase	Program Code	
District-County				
2.5w.(20) Allocation Amendment - ATP Project			Resolution FATP-2425-23	
			Amending Resolution FATP-1920-19	
1	Oxnard Boulevard Bikeway Gap Closure. Class I Bikeway parallel to	07-5667	2018-19	
\$98,000	Oxnard Boulevard from Camino Del Sol to Cooper Road in the City of	ATP/19-20	108-3290 RMRA	\$98,000
\$0	Oxnard. Class I bikeway gap closure and ADA improvements alongside	PS&E	20.30.720.100	\$0
	Oxnard Boulevard between Gonzales Road and Camino Del Sol.	\$98,000		
City of Oxnard		\$0		
VCTC	MPO	0720000226		
07-Ventura				
<u>Outcome/Output:</u> Construct 1,400 feet of Class I bike lanes and 2 ADA ramps.				
CEQA - NOE-CE, 03/24/2020				
SB 1 Augmentation				
Concurrent ATP Programming Amendments under Resolutions G-25-48 and G-25-49; June 2025.				
<u>Amend Resolution FATP-1920-19 to rescind the full amount of \$98,000 in ATP PS&E due to cancellation of the project.</u>				

2.5 Highway Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Amount by
RTPA/CTC		Advantage Phase	Program Code	Fund Type
District-County				
2.5w.(21) Allocation Amendment - ATP Project			Resolution FATP-2425-24	
			Amending Resolution FATP-2122-01	
1	Southeast Eastvale SRTS Equitable Access Project. In Eastvale along	08-1281	2020-21	
\$150,000	Orange Street from Sumner Avenue to Scholar Way; Schleisman Road &	ATP/21-22	108-3290 RMRA	\$150,000
\$0	Scholar Way; along Class I path crossings at Copper Sky, Estrella Street,	PS&E	20.30.720.100	\$0
City of Eastvale	Schleisman Road, and Stratus Street. Construct one lane mile of Class II	\$150,000		
	bikeway, a pedestrian signal with bulb-outs & pedestrian refuge island, three	\$0		
RCTC	additional crossing improvements for existing Class I path, 4 bulb-outs.	0821000147		
08-Riverside	MPO			
<u>Outcome/Output:</u> Complete network gap of Class II bike lane along Orange				
Street, adding pedestrian signal, refuge island, and enhanced crossing at				
intersections.				
CEQA - CE, 12/09/2020				
SB 1 Augmentation				
Concurrent ATP Programming Amendments under Resolutions G-25-48				
and G-25-49; June 2025.				
<u>Amend Resolution FATP-2122-01 to rescind the full amount of \$150,000</u>				
<u>in ATP PS&E due to cancellation of the project.</u>				

2.5 Highway Financial Matters

Project # Allocation Amount Recipient RTPA/CTC District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Advantage Phase	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(22) Allocation Amendment - ATP Project			Resolution FATP-2425-24 Amending Resolution FATP-2122-13	
1 \$1,197,000 \$0	Cottonwood Active Transportation Trunk Line Express (CATTLE) Network. In the unincorporated community of Cottonwood, develop a complete, low-stress bicycle/pedestrian network connecting residents to key Cottonwood attractions and community resources.	02-2607A ATP/21-22 PA&ED \$1,197,000 \$0 0221000163	2020-21 108-0890 FTF 20.30.720.100	\$1,197,000 \$0
Shasta County SRTA 02-Shasta	Small Urban and Rural <u>Outcome/Output:</u> Construct 7,740 feet of Class I multi-use path, 6,470 feet of sidewalks, 25 curb ramps PPNO 2607A is the infrastructure component to PPNO 2607B Concurrent ATP Programming Amendments under Resolutions G-25-48 and G-25-49; June 2025. <u>Amend Resolution FATP-2122-13 to rescind the full amount of \$1,197,000 in ATP PA&ED due to cancellation of the project.</u>			

2.5 Highway Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Amount by
RTPA/CTC		Advantage Phase	Program Code	Fund Type
District-County				
2.5w.(23) Allocation Amendment - ATP Project			Resolution FATP-2425-24	
			Amending Resolution FATP-2324-28	
1	City of Perris Bike and Pedestrian Network Project. In the City of Perris,	08-1273B	2022-23	
\$35,000	On Redlands Avenue between Placentia Avenue and Tahoe Street, and on	ATP/21-22	108-3290 RMRA	\$35,000
\$0	Citrus Avenue between Redlands Avenue and Perris Boulevard. Extensive	CONST	20.30.720.100	\$0
City of Perris	public outreach campaign and educational program to inform pedestrians	\$35,000		
RCTC	and bicyclists on project features.	\$0		
08-Riverside		0822000102		
Statewide				
<u>Outcome/Output:</u> Create educational campaign materials, conduct 3 school				
events, and 2 community event. Collect photos, participant list, and				
participant surveys during events.				
CEQA - CE, 09/07/2021				
Right of Way Certification: 01/12/2024				
Time Extension for FY 21-22 CONST expires				
February 29, 2024.				
PPNO 1273B is the non-infrastructure component to				
PPNO 1273A.				
Concurrent ATP Programming Amendments under Resolutions G-25-48				
and G-25-49; June 2025.				
<u>Amend Resolution FATP-2324-28 to rescind the full amount of \$35,000</u>				
<u>in ATP CON due to cancellation of the project.</u>				

2.5 Highway Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Amount by
RTPA/CTC		Advantage Phase	Program Code	Fund Type
District-County				
2.5w.(24) Allocation Amendment - ATP Project			Resolution FATP-2425-25	
			Amending Resolution FATP-2223-31	
1	Chester Avenue between 4th Street to Brundage Lane. On Chester	06-7083	2021-22	
\$210,000	Avenue between 4th Street and Brundage Lane. Construct curb extensions,	ATP/21-22	102-0001 GF	\$210,000
\$0	high-visibility crosswalks, advanced stop/turn line markings, leading	CONST	20.30.720.100	\$0
	pedestrian intervals at signalized intersections, bike lanes and center	\$210,000		
City of	medians.	\$0		
Bakersfield		0622000183		
KCOG	MPO			
06-Kern				
<u>Outcome/Output:</u> Construction of center medians, continental crosswalks,				
and bike lanes with additional pavement markings will increase pedestrian				
visibility and will provide added safety for pedestrians and bicyclists along				
this corridor.				
CEQA - CE, 03/10/2023				
Right of Way Certification: 03/13/2023				
Time extension for FY 21-22 CONST expires on 06/30/2023				
Concurrent ATP Programming Amendments under Resolutions G-25-48				
and G-25-49; June 2025.				
<u>Amend Resolution FATP-2223-31 to rescind the full amount of \$210,000</u>				
<u>in ATP CON due to cancellation of the project.</u>				

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6g.(4), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ALLOCATION AMENDMENT FOR A TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM COMPONENT**
RESOLUTION TIRCP-2425-68, AMENDING RESOLUTION TIRCP-2223-36

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for the Transit and Intercity Rail Capital Program (TIRCP) (2022:11) Fresno County Rural Transit Agency Resiliency Hub (Resiliency Hub Structure with Inductive Charging) component (PPNO CP089), in Fresno County, to rescind the full amount of \$30,000 from the Project Approval and Environmental Document (PA&ED) phase due to a third-party contract not being awarded within the Timely Use of Funds (TUF) deadline?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation amendment for the TIRCP (2022:11) Fresno County Rural Transit Agency Resiliency Hub (Resiliency Hub Structure with Inductive Charging) component (PPNO CP089), in Fresno County, to rescind the full amount of \$30,000 from the PA&ED phase due to a third-party contract not being awarded within the TUF deadline.

BACKGROUND:

In December 2022, the Commission approved \$30,000 for the PA&ED phase of the TIRCP (2022:11) Fresno County Rural Transit Agency Resiliency Hub (Resiliency Hub Structure with Inductive Charging) component (PPNO CP089), under Resolution TIRCP-2223-36. After allocation, the agency experienced unforeseen delays related to acquiring the property for the resiliency hub project. The original site was not affordable or available to buy, which prompted the agency to explore alternate locations. Due to the Fresno County Rural Transit Agency's inability to secure a project location, a third-party contract for the environmental work could not

be awarded within the TUF deadline. The agency is actively working to purchase a suitable location but requires additional time. As a result, the PA&ED funds will be reprogrammed to a future fiscal year.

The proposed changes are reflected in strikethrough and bold in the attached revised vote box.

FINANCIAL RESOLUTION:

Be it resolved, that the TIRCP (2022:11) Fresno County Rural Transit Agency Resiliency Hub (Resiliency Hub Structure with Inductive Charging) component (PPNO CP089), is hereby amended to rescind the full amount of \$30,000 from the PA&ED phase, in accordance with the attached revised vote box.

Attachment

2.6 Mass Transportation Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Program Code
RTPA/CTC		Advantage Phase		
District-County		EA		
2.6g.(4) Allocation Amendment - Transit and Intercity Rail Capital Program			Resolution TIRCP-2425-68 Amending Resolution TIRCP-2223-36	
1	(2022:11) Fresno County Rural Transit Agency Resiliency Hub	06-CP089	2021-22	
\$30,000	(Resiliency Hub Structure with Inductive Charging). Fresno County Rural	TIRCP/22-23	301-0046R GGRF	\$18,000
\$0	Transit Agency will work with an environmental consultant to obtain project	PA&ED	30.20.301.100	\$0
	approval and environmental clearance for the Resiliency Hub.	\$18,000		
Fresno County	<u>Outcome/Output:</u> Reduction of greenhouse gas emissions by constructing a	\$0	2021-22	
Rural Transit	Resiliency Hub with vehicle and bus electric charging stations to expand	TIRCP/22-23	101-0046 PTA	\$12,000
Agency	Fresno County Rural Transit Agency's zero emission vehicle (ZEV) fleet.	PA&ED	30.10.030.200	\$0
<u>FCOG</u>	Increased ridership and service through improvements to ZEV supporting	\$12,000		
06-Fresno	infrastructure. Provides direct benefits to priority populations within Fresno	\$0		
	County and its rural populations.	0023000106		
		S		
	<u>Amend Resolution TIRCP-2223-36 to rescind the full amount of \$30,000</u>	T500GA		
	<u>from the PA&ED phase due to a third-party contract not being awarded</u>			
	<u>within the Timely Use of Funds deadline.</u>			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6g.(6), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ALLOCATION AMENDMENT FOR A TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM COMPONENT**
RESOLUTION TIRCP-2425-70S, AMENDING RESOLUTION TIRCP-2122-30S

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for the Transit and Intercity Rail Capital Program (TIRCP) (2018:27) Southern California Optimized Rail Expansion (SCORE) (El Monte Siding and Station Improvements) component (PPNO CP033Y), in Los Angeles County, to rescind the full amount of \$525,000 from the Right of Way (RW) phase due to not being able to expend the funds within the Timely Use of Funds (TUF) deadline?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation amendment for the TIRCP (2018:27) Southern California Optimized Rail Expansion (SCORE) (El Monte Siding and Station Improvements) component (PPNO CP033Y), in Los Angeles County, to rescind the full amount of \$525,000 from the RW phase due to not being able to expend the funds within the TUF deadline.

BACKGROUND:

In May 2022, the Commission approved \$525,000 in supplemental funding for the RW phase of the TIRCP (2018:27) Southern California Optimized Rail Expansion (SCORE) (El Monte Siding and Station Improvements) component (PPNO CP033Y), under Resolution TIRCP-2122-30S. The responsible agency is unable to expend the funds by the TUF deadline.

The Southern California Regional Rail Authority (SCRRA) budgeted the TIRCP awarded funding for the RW phase to two elements of the component, the station improvements, and

siding extension. The supplemental funds allocated were budgeted for the station improvements element. At the time of allocation, the RW needs had been determined, but since then, the RW needs have been under reevaluation due to a change in the design because of new railroad industry standards and the addition of a new community development adjacent to the station. The SCRRA is working with component stakeholders to determine the best approach in addressing the RW needs for the station improvements. However, since the design is on hold and the RW needs have not been determined, the funding cannot be expended by the TUF deadline of June 30, 2026. Therefore, the Department is requesting an allocation amendment to rescind the full amount of \$525,000 from the RW phase, to be allocated at a future Commission meeting when the RW needs have been determined.

The proposed changes are reflected in strikethrough and bold in the attached revised vote box.

FINANCIAL RESOLUTION:

Be it resolved, that the TIRCP (2018:27) Southern California Optimized Rail Expansion (SCORE) (EI Monte Siding and Station Improvements) component (PPNO CP033Y), is hereby amended to rescind the full amount of \$525,000 from the RW phase, in accordance with the attached revised vote box.

Attachment

2.6 Mass Transportation Financial Matters

Project #	Allocation Amount	Recipient	Project Title	PPNO	Budget Year	Amount by
RTPA/CTC	District-County	Location	Project Description	Program/Year Phase Prgm'd Amount Project ID Advantage Phase EA	Item # Fund Type Program Code	Fund Type
2.6g.(6) Allocation Amendment - Transit and Intercity Rail Capital Program				Resolution TIRCP-2425-70S Amending Resolution TIRCP-2122-30S		
1	\$525,000 \$0	Southern California Regional Rail Authority LACMTA 07-Los Angeles	(2018:27) Southern California Optimized Rail Expansion (SCORE) (EI Monte Siding and Station Improvements). Design for siding extension at El Monte Station that will extend from Control Point Hondo to the new Control Point Watson and will be 0.91 miles long, including relocating the station entrance to the east end of the platform. <u>Outcome/Output:</u> Increased ridership through improved frequency, performance, and expanded service. Improved infrastructure, reduced greenhouse gas emissions, improved safety, and improved regional system integration through improved local and regional connectivity. Completion will provide 30-minute bi-directional headways between Marengo Siding and El Monte. CEQA - SE, 03/16/2021 Current allocation includes the funding split: \$235,000 \$0 GGRF and \$290,000 \$0 SB1. A prior allocation for \$518,000 was approved for this component under Resolution TIRCP-2021-26; May 2021. Upon approval of this request the total allocation amount for the R/W phase of this component will be \$1,043,000 \$518,000. Supplemental funds in the amount of \$525,000 are needed to complete R/W for the El Monte Siding and Station Improvements component. The additional TIRCP funding of \$525,000 will be redirected from the original overall award amount itself and does not represent additional funding from any past or future funding cycles. <u>Amend Resolution TIRCP-2122-30S to rescind the full amount of \$525,000 from the RW phase due to not being able to expend the funds within the Timely Use of Funds deadline.</u>	07-CP033Y TIRCP/21-22 R/W \$235,000 \$0 TIRCP/21-22 R/W \$290,000 \$0 0022000246 S R386GY	2020-21 301-0046R GGRF 30.20.301.100 2020-21 101-0046 PTA 30.10.030.200	\$235,000 \$0 \$290,000 \$0

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.12, Action

Prepared By: Ahmed Ghonim
Assistant Deputy Director

Published Date: June 13, 2025

Subject: Approval of Two State Highway Operation and Protection Program Baseline Agreements, Resolution SHOPP-P-2425-07B

Recommendation:

Staff recommends that the California Transportation Commission (Commission) approve the following two State Highway Operation and Protection Program (SHOPP) Baseline Agreements and establish these agreements as the basis for project delivery and monitoring:

Caltrans District	Expenditure Authorization Number (EA)	County	Route	Project Description (With embedded link to agreement)
02	0J810	Shasta and Siskiyou	5	The project is located along Route 5 in Shasta and Siskiyou counties and is being delivered under the Capital Preventive Maintenance program. The project is to rehabilitate 45.4 lane miles of pavement and enhance roadway safety elements. The scope of work includes rehabilitating the pavement, upgrading the median barrier and guardrail to the latest standards, and replacing two intelligent transportation systems. Additionally, the project will rehabilitate the bridge decks of two structures and construct a wildlife crossing with associated wildlife management fencing.

Caltrans District	Expenditure Authorization Number (EA)	County	Route	Project Description (With embedded link to agreement)
04	OW140	San Francisco	80	The project is located along Route 80 in the City and County of San Francisco. The San Francisco–Oakland Bay Bridge relies on a fender system to safeguard its piers from vessel collisions. These systems are designed to absorb and deflect the energy of an impact, minimizing the risk of structural damage. Federal regulations require that the fender system remain fully functional. The existing system at Piers W3 to W6 has significantly deteriorated due to age and environmental exposure, making continued maintenance increasingly expensive. The purpose of this project is to replace the existing system with a new fender system to ensure continued protection of the bridge piers.

Issue:

In accordance with the Commission’s Senate Bill (SB) 1 Accountability and Transparency Guidelines, the California Department of Transportation (Caltrans) must submit SHOPP Baseline Agreements to the Commission for approval in order to establish these agreements as the basis for project delivery and monitoring.

Commission staff has reviewed these Baseline Agreements and determined the project’s expected benefits, delivery schedule, project cost, and funding plan is consistent with the project the Commission approved at the time of programming and the requirements set forth in the SHOPP Guidelines.

Background:

The Commission adopted the SB 1 Accountability and Transparency Guidelines at its March 21, 2018 meeting and adopted revised Guidelines at its March 23, 2023 meeting. The Guidelines direct agencies to provide executed Baseline Agreements that set forth the agreed-upon expected benefits, delivery schedule, project cost, and funding plan. The Baseline Agreement provides a benchmark for comparison to the current status of a project for subsequent reporting purposes. The Baseline Agreement must be signed by the Caltrans Director and District Director, the Commission's Executive Director, the project applicant, and the Implementing Agency.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6g.(3), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ALLOCATION AMENDMENT FOR TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM COMPONENTS**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for three Transit and Intercity Rail Capital Program (TIRCP) components (2022:01) ATN FAST (Family of Advanced Solutions for Transit): Revolutionizing Transit for a Global Audience components, in Orange County, to rescind a combined total amount of \$13,776,000 from the Construction (CON) phase, due to third-party contracts not being awarded within the Timely Use of Funds (TUF) deadline?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation amendment for three TIRCP (2022:01) ATN FAST (Family of Advanced Solutions for Transit): Revolutionizing Transit for a Global Audience components, in Orange County, to rescind a combined total amount of \$13,776,000 from the CON phase due to third-party contracts not being awarded within the TUF deadline as follows:

Project Component	PPNO	Resolution	Amending Resolution	Current Project Allocation	Rescinded Amount	Revised Allocation Amount
Purchase 7 (10 passenger) ZEV vans for EVE: New Airport Connection & Night Owl) (2.6g.(3a))	CP086	TIRCP-2425-65	TIRCP-2223-82	\$877,000	\$877,000	\$0
Purchase 10 (15 passenger) ZEV Microtransit for FRAN) (2.6g.(3b))	CP086A	TIRCP-2425-66	TIRCP-2223-82	\$313,000	\$313,000	\$0
Purchase of 15 - 40' Zero-Emission Buses for ART Fleet Conversion) (2.6g.(3c))	CP086C	TIRCP-2425-67	TIRCP-2223-82	\$12,586,000	\$12,586,000	\$0
Total:				\$13,776,000	\$13,776,000	\$0

BACKGROUND:

In June 2023, the Commission approved a total of \$13,776,000 for the CON phase of three ATN FAST (Family of Advanced Solutions for Transit): Revolutionizing Transit for a Global Audience components, under Resolution TIRCP-2223-82. A total of \$877,000 was approved for the Purchase 7 (10 passenger) ZEV vans for EVE: New Airport Connection & Night Owl component (PPNO CP086); \$313,000 for the Purchase 10 (15 passenger) ZEV Microtransit for FRAN component (PPNO CP086A); and \$12,586,000 for the Purchase of 15 - 40' Zero-Emission Buses for ART Fleet Conversion component (PPNO CP086C).

Anaheim Transportation Network was unable to award third-party contracts as potential vehicle manufacturers began experiencing financial difficulties, with some eventually filing for bankruptcy, some exiting the United States market, and some stopping vehicle production. In addition, vehicle offerings lacked required range to maintain the daily service requirement. Therefore, a combined total rescission of \$13,776,000 in previously allocated CON phase funds from the three components is needed to allow the phase to move forward with a revised timeline. There are concurrent allocations on this month's Commission agenda for all three components.

The proposed changes are reflected in strikethrough and bold in the attached revised vote boxes.

FINANCIAL RESOLUTION:

Be it resolved, that three TIRCP (2022:01) ATN FAST (Family of Advanced Solutions for Transit): Revolutionizing Transit for a Global Audience components, are hereby amended to rescind a combined total of \$13,776,000 from the CON phase, in accordance with the attached revised vote boxes.

Attachments

2.6 Mass Transportation Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Fund Type
RTPA/CTC		Advantage Phase	Program Code	
District-County		EA		
2.6g.(3a) Allocation Amendment - Transit and Intercity Rail Capital Program			Resolution TIRCP-2425-65	
			Amending Resolution TIRCP-2223-82	
1	(2022:01) ATN FAST (Family of Advanced Solutions for Transit):	12-CP086	2021-22	
\$877,000	Revolutionizing Transit for a Global Audience (Purchase 7 (10	TIRCP/22-23	301-0046R GGRF	\$482,000
\$0	passenger) ZEV vans for EVE: New Airport Connection & Night Owl).	CONST	30.20.301.100	\$0
	Installation of contactless payment readers and lease of seven new all-	\$482,000		
Anaheim	electric shuttle vans for transit service between John Wayne Airport,	\$0	2021-22	
Transportation	Anaheim Convention Center, and Disneyland Resort in Orange County.	TIRCP/22-23	101-0046 PTA	\$395,000
Network		CONST	30.10.030.200	\$0
<u>OCTA</u>	<u>Outcome/Output:</u> Increased ridership through new and expanded service,	\$395,000		
12-Orange	reduced greenhouse gas emissions, improved integration with other transit	\$0		
	services and access of services for low-income communities and low-income	0023000233		
	households.	S		
	CEQA - CE, 05/21/2023	T493GA		
	NEPA - N/A			
Concurrent Allocation under Resolution TIRCP-2425-74; June 2025.				
<u>Amend Resolution TIRCP-2223-82 to rescind the full amount of</u>				
<u>\$877,000 from the CON phase due to a third-party contract not being</u>				
<u>awarded within the Timely Use of Funds deadline.</u>				

2.6 Mass Transportation Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Fund Type
RTPA/CTC		Advantage Phase	Program Code	
District-County		EA		
2.6g.(3b) Allocation Amendment - Transit and Intercity Rail Capital Program			Resolution TIRCP-2425-66	
			Amending Resolution TIRCP-2223-82	
1	(2022:01) ATN FAST (Family of Advanced Solutions for Transit):	12-CP086A	2021-22	
\$313,000	Revolutionizing Transit for a Global Audience (Purchase 10 (15	TIRCP/22-23	301-0046R GGRF	\$172,000
\$0	passenger) ZEV Microtransit for FRAN). Purchase of ten all-electric 15	CONST	30.20.301.100	\$0
Anaheim	passenger zero-emission vehicles to extend the Free Rides Around the	\$172,000		
Transportation	Neighborhood (FRAN) service to Historic Orange and Chapman University in	\$0	2021-22	
Network	Orange County.		101-0046 PTA	\$141,000
<u>OCTA</u>	<u>Outcome/Output:</u> Increased ridership through new and expanded service,	TIRCP/22-23	30.10.030.200	\$0
12-Orange	reduced greenhouse gas emissions, improved integration with other transit	CONST		
	services and access of services for low-income communities and low-income	\$141,000		
	households.	\$0		
		0023000236		
		S		
	CEQA - CE, 05/21/2023	T493GB		
	NEPA - N/A			
Concurrent Allocation under Resolution TIRCP-2425-74; June 2025.				
<u>Amend Resolution TIRCP-2223-82 to rescind the full amount of</u>				
<u>\$313,000 from the CON phase due to a third-party contract not being</u>				
<u>awarded within the Timely Use of Funds deadline.</u>				

2.6 Mass Transportation Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Fund Type
RTPA/CTC		Advantage Phase	Program Code	
District-County		EA		
2.6g.(3c) Allocation Amendment - Transit and Intercity Rail Capital Program			Resolution TIRCP-2425-67	
			Amending Resolution TIRCP-2223-82	
1	(2022:01) ATN FAST (Family of Advanced Solutions for Transit):	12-CP086C	2022-23	
\$12,586,000	Revolutionizing Transit for a Global Audience (Purchase of 15 - 40'	TIRCP/22-23	301-0046R GGRF	\$6,922,000
\$0	Zero-Emission Buses for ART Fleet Conversion). Purchase fifteen 40-foot	CONST	30.20.301.100	\$0
	zero-emission buses to expand service and complete full transition to an all-	\$6,922,000		
Anaheim	electric fleet in Orange County.	\$0	2021-22	
Transportation	<u>Outcome/Output:</u> Increased ridership through new and expanded service,	TIRCP/22-23	101-0046 PTA	\$5,664,000
Network	reduced greenhouse gas emissions, improved integration with other transit	CONST	30.10.030.200	\$0
<u>OCTA</u>	services and access of services for low-income communities and low-income	\$5,664,000		
12-Orange	households.	\$0		
	CEQA - CE, 05/21/2023	0023000238		
	NEPA - N/A	S		
		T493GC		
Concurrent Allocation under Resolution TIRCP-2425-74; June 2025.				
<u>Amend Resolution TIRCP-2223-82 to rescind the full amount of</u>				
<u>\$12,586,000 from the CON phase due to a third-party contract not being</u>				
<u>awarded within the Timely Use of Funds deadline.</u>				

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6g.(5), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ALLOCATION AMENDMENT FOR A TRANSIT AND INTERCITY RAIL
CAPITAL PROGRAM COMPONENT
RESOLUTION TIRCP-2425-69, AMENDING RESOLUTION TIRCP-2324-58**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for the Transit and Intercity Rail Capital Program (TIRCP) (2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure) (2022:17) San Francisco Zero Emissions High-Frequency Ferry Network (Shoreside Charging Infrastructure) component (PPNO CP075A), in San Francisco County, to revise the project title and deallocate \$12,381,000 from the Construction (CON) phase due to the component being split into three separate components?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation amendment for the TIRCP (2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure) (2022:17) San Francisco Zero Emissions High-Frequency Ferry Network (Shoreside Charging Infrastructure) component (PPNO CP075A), in San Francisco County, to revise the project title and deallocate \$12,381,000 from the Construction (CON) phase due to the component being split into three separate components.

BACKGROUND:

In May 2024, the Commission allocated \$14,581,000 for the Construction (CON) phase of the (2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure) (2022:17) San Francisco Zero Emissions High-Frequency Ferry Network (Shoreside Charging Infrastructure) component (PPNO CP075A). In December 2024, the Commission approved a 7-month time extension to award the period of contract award. The Expansion of WETA Ferry

Service and San Francisco Zero Emissions High-Frequency Network project will modify standard floats to house batteries that will allow vessels to charge while they are docked. This is necessary to support the planned acquisition of the all-electric vessels awarded through TIRCP Cycle 4 and Cycle 5, in May 2024.

The San Francisco Bay Area Water Emergency Transportation Authority (WETA) received bids for the Shoreside Charging Infrastructure component that were approximately double the original engineering estimates. The original plan was to award the component under a single contract and work concurrently at three separate locations. However, to bring the costs down, minimize the need for additional funding, and ensure the successful delivery of the system as originally planned, WETA requested to split the component into three separate components. The current Shoreside Charging Infrastructure component will focus on the Treasure Island Electrification. The remaining two components will be allocated at a future meeting as necessary to construct the Shoreside Charging Infrastructure at the Downtown San Francisco Terminal and the Mission Bay Terminal. There is a related time extension amendment for the period of contract award on this month's Commission agenda.

The proposed changes are reflected in strikethrough and bold in the attached revised vote box.

FINANCIAL RESOLUTION:

Be it Resolved, that the TIRCP (2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure) (2022:17) San Francisco Zero Emissions High-Frequency Ferry Network (Shoreside Charging Infrastructure) component (PPNO CP075A), is hereby amended to revise the project title and deallocate \$12,381,000 from the CON phase due to the component being split into three separate components, in accordance with the attached revised vote box.

Attachment

2.6 Mass Transportation Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Fund Type
RTPA/CTC		Advantage Phase	Program Code	
District-County		EA		
2.6g.(5) Allocation Amendment - Transit and Intercity Rail Capital Program			Resolution TIRCP-2425-69 Amending Resolution TIRCP-2324-58	
1	(2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure <u>Treasure Island Electrification</u>) (2022:17) San Francisco Zero Emissions High-Frequency Ferry Network (Shoreside Charging Infrastructure). The infrastructure will modify standard floats to house batteries that will allow vessels to charge while vessels are docked.	04-CP075A TIRCP/23-24 CONST	2023-24 301-0046R GGRF 30.20.301.100	\$0,321,000 \$1,100,000
\$14,581,000 \$2,200,000		\$0,321,000 \$1,100,000		
San Francisco Bay Area Water Emergency Transportation Authority MTC 04-San Francisco	<u>Outcome/Output:</u> Reduction of greenhouse gas emissions and vehicle miles traveled by providing the supporting infrastructure for the all-electric vessels to operate. CEQA - NOE, 10/31/2022 Right of Way Certification: N/A Cycle 4 (2020:17) funding split is: GGRF \$2,143,000 \$1,100,000 and PTA \$2,142,000 \$1,100,000 . Cycle 5 (2022:17) funding split is: GGRF \$0,178,000 and PTA \$4,118,000 . <u>Amend Resolution TIRCP-2324-58 to revise the project title and deallocate \$12.381.000 due to the component being split into three separate components.</u>	TIRCP/23-24 CONST \$6,260,000 \$1,100,000 0024000157 S T519GB	2023-24 101-0046 PTA 30.10.030.200	\$6,260,000 \$1,100,000

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6g.(7), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ALLOCATION AMENDMENT FOR A TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM COMPONENT**
RESOLUTION TIRCP-2425-71, AMENDING RESOLUTION TIRCP-2425-14

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation amendment for the Transit and Intercity Rail Capital Program (TIRCP) (2018:26) SMART Larkspur to Windsor Corridor (Windsor to Healdsburg) component (PPNO CP041A), in Sonoma County, to revise the component name?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation amendment for the TIRCP (2018:26) SMART Larkspur to Windsor Corridor (Windsor to Healdsburg) component (PPNO CP041A), in Sonoma County, to revise the component name.

BACKGROUND:

In August of 2024, the Commission allocated \$1,450,000 for the Plans, Specifications, and Estimate phase of the TIRCP (2018:26) SMART Larkspur to Windsor Corridor (Windsor to Healdsburg) component (PPNO CP041A), under Resolution TIRCP-2425-14. At the time of allocation, there were no other developments expanding on this project. Sonoma-Marin Area Rail Transit (SMART), requests a correction to the component name to match their internal tracking methods relating to this project. There is a concurrent allocation on this month's Commission agenda for the Construction phase.

The proposed change is reflected in strikethrough and bold in the attached revised vote box. There is no change to the allocation amount.

FINANCIAL RESOLUTION:

Be it Resolved, that the (2018:26) SMART Larkspur to Windsor Corridor (Windsor to Healdsburg) component (PPNO CP041A), is hereby amended to revise the component name, in accordance with the attached revised vote box.

Attachment

2.6 Mass Transportation Financial Matters

Project #	Project Title	PPNO Program/Year Phase	Budget Year	Amount by
Allocation Amount	Location	Prgm'd Amount	Item #	Fund Type
Recipient	Project Description	Project ID	Fund Type	Fund Type
RTPA/CTC		Advantage Phase	Program Code	
District-County		EA		
2.6g.(7) Allocation Amendment - Transit and Intercity Rail Capital Program			Resolution TIRCP-2425-71	
			Amending Resolution TIRCP-2425-14	
1	(2018:26) SMART Larkspur to Windsor Corridor (Windsor to Healdsburg North Extension). Complete progressive design-build documents and procurement of a progressive design-build Project delivery team for the completion of the SMART Windsor to Healdsburg Rail and Pathway Extension.	04-CP041A	2023-24	
\$1,450,000		TIRCP/24-25	301-0046R GF	\$471,000
Sonoma-Marin		PS&E	30.20.301.100	
Area Rail Transit		\$471,000		
<u>MTC</u>			2023-24	
04-Sonoma	<u>Outcome/Output:</u> Increased ridership, reduction in greenhouse gas emissions and improved safety.	TIRCP/24-25	101-0046 PTA	\$979,000
		PS&E	30.10.030.200	
		\$979,000		
		0024000228		
		S		
	CEQA - FEIR, 7/17/2009	R390GC		
	Future consideration of funding approved under Resolution E-09-56; July 2009.			
	This project will be delivered using the progressive design-build method.			
	In January 2023, this project was awarded \$34,338,000 in Cycle 6 General Fund Augmentation funding.			
	Concurrent Allocation under Resolution TIRCP-2425-74; June 2025.			
	<u>Amend Resolution TIRCP-2425-14 to revise the component name. There is no change to the allocation amount.</u>			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(15), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **TIME EXTENSION AMENDMENT FOR A STATE HIGHWAY OPERATION AND PROTECTION PROGRAM PROJECT
WAIVER 25-150, AMENDING WAIVER 25-52**

ISSUE:

Should the California Transportation Commission (Commission) amend the previously approved project completion time extension for the State Highway Operation and Protection Program (SHOPP) Facilities project (PPNO 3005R), to revise the county from Los Angeles to San Bernardino?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission amend the previously approved project completion time extension for the SHOPP Facilities project (PPNO 3005R), to revise the county from Los Angeles to San Bernardino.

BACKGROUND:

In May 2025, the Commission approved a time extension to extend the period of project completion for the Construction phase of the SHOPP Facilities project (PPNO 3005R), under Waiver 25-52. At the time of approval, the county was incorrectly reflected as Los Angeles instead of San Bernardino, therefore the Department requests that the Commission approve this time extension amendment to reflect the County correctly.

RESOLUTION:

Be it Resolved, that the Commission does hereby amend the previously approved project completion time extension for the SHOPP Facilities project (PPNO 3005R), to revise the county from Los Angeles to San Bernardino.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 4.11, Action Item

Prepared By: Tarek Tabshouri, Chief
Division of Aeronautics

Subject: **APPROVAL OF THE AERONAUTICS' CAPITAL IMPROVEMENT PLAN
2025-2034**

ISSUE:

Should the California Transportation Commission (Commission) approve the attached California Department of Transportation (Department), Division of Aeronautics' (Aeronautics) Capital Improvement Plan (CIP) that was presented as an informational item at the May 2025 Commission meeting?

RECOMMENDATION:

The Department recommends that the Commission approve the attached Aeronautics' CIP that was presented as an informational item at the May 2025 Commission meeting.

BACKGROUND:

The CIP is a 10-year, fiscally unconstrained listing of capital and planning projects at California's publicly owned, public-use airports. Every two years, Aeronautics reaches out to local sponsors (including cities, counties, and airport districts) to submit projects for inclusion in the CIP, as required by California Public Utilities Code sections 21702-21706. Inclusion in the CIP is a requirement for grant eligibility for the Aeronautics' Acquisition and Development (A&D) and Airport Improvement Program Matching Grants Programs.

The Aeronautics' A&D grants are selected from the CIP using the Commission-approved Priority List in the order of safety, capacity, and security. Additional factors, such as safety inspection reports and local sponsor input, are used as well.

Attachment:
Aeronautics' Capital Improvement Plan 2025-2034

CALTRANS DIVISION OF AERONAUTICS CAPITAL IMPROVEMENT PLAN 2025–2034

The Capital Improvement Plan (CIP) is a ten-year, fiscally unconstrained listing of capital and planning projects submitted to the California Department of Transportation (Caltrans). These projects are predominantly based on airport master plans or other comparable long-range planning documents. The CIP is compiled biennially (every two years) in accordance with the California Public Utilities Code (PUC) and is presented to the California Transportation Commission (Commission) for review, comment, and approval.

Not all projects listed in the CIP will be programmed or funded. Projects may be funded by various combinations of federal, State, and local sources or may be entirely Locally Funded Projects (LFP).

California Aid to Airports Program

The approved CIP also serves as the planning document that allows Caltrans to program grants for the California Aid to Airports Program (CAAP). The CAAP provides financial assistance to local sponsors in order to establish, maintain, and improve the statewide system of airports. The CAAP includes two grant programs: the Acquisition and Development program and the Airport Improvement Program State match program.

Airport Improvement Program

The Airport Improvement Program (AIP), administered by the Federal Aviation Administration (FAA), provides grants to public agencies—and in some cases, to private owners and entities—for the planning and development of public-use airports that are included in the National Plan of Integrated Airport Systems (NPIAS). The NPIAS identifies nearly 3,300 existing and proposed airports nationwide that are significant to national air transportation and thus eligible to receive federal grants.

An AIP grant constitutes 90 to 95 percent of a project cost. The FAA requires that the local sponsor receiving the grant provide a 5 to 10 percent match. Depending on sponsor eligibility (including participation in the CIP) and available funds, the State may contribute up to 5 percent of the federal grant amount to the local sponsor to assist in meeting their 10 percent requirement.

Acquisition and Development Program

The Acquisition and Development (A&D) Program grants are provided by Caltrans for eligible projects in the CIP that are for General Aviation capital improvement and planning purposes. An A&D grant constitutes 90 percent of a project cost. The remaining 10 percent is matched by the local sponsor.

Every even-numbered year, Caltrans prepares, and the Commission approves, the Aeronautics Program, a two-year list of grant projects from the CIP for which funding is available. Projects are selected for the Aeronautics Program based on eligibility and ranking. The priority ranking

list is used to rank projects. The ranking is based on project category and project description. Project categories listed in priority are: safety, capacity, and security. Other selection criteria may be used as well, such as input from the Caltrans Office of Airports and the sponsor.

Airport Land Use Compatibility Plans

A&D grants are also provided to local sponsors to prepare or update Airport Land Use Compatibility Plans (ALUCP). ALUCPs are prepared by County Airport Land Use Commissions as required by the PUC and contain land use measures that minimize the public's exposure to safety hazards within two-miles around public-use airports. Protecting people and property on the ground from the potential consequences of near-airport aircraft accidents is a fundamental land use compatibility planning objective.

The Division of Aeronautics recommends a comprehensive review and update of an ALUCP at least every five years. Consistent funding for ALUCPs is vital for the protection of the California air transportation system and those communities surrounding the airports. The Commission has historically set 25 percent of the A&D Grant Program to help fund the preparation of ALUCPs.

Ground Access Projects

Ground access projects include improvements to off-airport roadways, highways, public transit systems, passenger shuttle systems, parking lots, and other transportation-related modes and facilities. Enhancements to these facilities provide more convenient and predictable access for passengers, employees, air cargo traffic, and general aviation users. This CIP contains a listing of planned ground access projects; however, these projects are not eligible for federal AIP funding or funding through the State CAAP.

Data Summary

The total need submitted for the CIP is as follows:

10-year Project Needs	Airport CIP Participation	FAA	State	Local
Commercial Airports	21 out of 26 (81%)	\$4,096,708,142	\$ 0	\$3,269,077,433
General Aviation Airports	170 out of 183 (93%)	\$2,032,138,775	\$142,265,777	\$ 146,503,901
Total		\$6,128,846,917	\$142,265,777	\$3,415,581,334

The following tables provide data summaries, airport project submittals, and abbreviations and acronyms. Tables 1-3 summarize data over the upcoming 4-year period (2026-2029) to identify airport short and mid-term needs.

1. Table 1: 4-Year Summary of Needs by County, Airport, and Service Level (2026-2029)
2. Table 2: 4-Year Summary of Needs by Caltrans District (2026 – 2029)
3. Table 3: 4-Year Summary of Needs by Program and Service Level (2026 – 2029)
4. Table 4: 10-Year CIP List of Projects (2025-2034)
5. Table 5: Abbreviations and Acronyms

Table 1: 2026 - 2029 (4-Year Summary of Needs by County, Airport, and Service Level)

County	Airport	Service Level	FAA	State	Local
Alameda	Hayward Executive Airport	Reliever	\$18,223,075	\$911,154	\$1,113,632
	Livermore Municipal Airport	Reliever	\$16,875,000	\$843,750	\$1,031,250
Alameda Total			\$35,098,075	\$1,754,904	\$2,144,882
Amador	Westover Field Amador County Airport	General Aviation	\$4,288,500	\$214,425	\$262,075
Amador Total			\$4,288,500	\$214,425	\$262,075
Butte	Chico Municipal Airport	General Aviation	\$32,382,375	\$1,619,119	\$1,978,923
	Oroville Municipal Airport	General Aviation	\$5,603,130	\$280,157	\$342,414
Butte Total			\$37,985,505	\$1,899,275	\$2,321,336
Calaveras	Calaveras Co. - Maury Rasmussen Airport	General Aviation	\$7,873,200	\$393,660	\$481,140
Calaveras Total			\$7,873,200	\$393,660	\$481,140
Colusa	Colusa County Airport	General Aviation	\$1,029,600	\$51,480	\$62,920
Colusa Total			\$1,029,600	\$51,480	\$62,920
Contra Costa	Buchanan Field Airport	Commercial Service Primary	\$13,599,413	\$0	\$1,511,046
	Byron Airport	Reliever	\$3,676,950	\$183,848	\$3,611,703
Contra Costa Total			\$17,276,363	\$183,848	\$5,122,748
Del Norte	Andy McBeth Airport	Not a NPIAS Facility	\$0	\$468,000	\$52,000
	Jack McNamara Field Airport	Commercial Service Non-Primary	\$10,917,000	\$0	\$1,213,000
	Ward Field Airport	Not a NPIAS Facility	\$0	\$153,000	\$17,000
Del Norte Total			\$10,917,000	\$621,000	\$1,282,000
El Dorado	Cameron Park Airpark	General Aviation	\$787,500	\$39,375	\$48,125
	Georgetown Airport	General Aviation	\$65,700	\$3,285	\$4,015
	Lake Tahoe Airport	General Aviation	\$10,245,000	\$7,802,250	\$1,436,083
	Placerville Airport	General Aviation	\$2,337,134	\$116,857	\$142,825
El Dorado Total			\$13,435,334	\$7,961,767	\$1,631,048
Fresno	Firebaugh Airport	General Aviation	\$1,147,500	\$57,375	\$70,125
	Fresno Chandler Executive Airport	Reliever	\$12,015,000	\$600,750	\$734,250
	Fresno Yosemite International Airport	Commercial Service Primary	\$210,375,000	\$0	\$23,375,000
	New Coalinga Municipal Airport	General Aviation	\$892,500	\$44,625	\$54,542
	Reedley Municipal Airport	General Aviation	\$1,912,500	\$95,625	\$116,875
Fresno Total			\$226,342,500	\$798,375	\$24,350,792
Glenn	Haigh Field Airport	General Aviation	\$1,651,928	\$82,596	\$100,951
Glenn Total			\$1,651,928	\$82,596	\$100,951

County	Airport	Service Level	FAA	State	Local
Humboldt	California Redwood Coast (Arcata)-Humboldt County Airport	Commercial Service Primary	\$22,027,500	\$0	\$2,447,500
	Garberville Airport	General Aviation	\$1,777,500	\$88,875	\$108,625
	Murray Field Airport	General Aviation	\$2,197,500	\$109,875	\$134,292
	Rohnerville Airport	General Aviation	\$3,147,728	\$157,386	\$192,361
	Samoa Field Airport	Not a NPIAS Facility	\$0	\$3,007,800	\$334,200
	Shelter Cove Airport	General Aviation	\$2,655,000	\$1,230,750	\$284,250
Humboldt Total			\$31,805,229	\$4,594,686	\$3,501,228
Imperial	Brawley Municipal Airport	General Aviation	\$6,690,363	\$334,518	\$408,856
	Calexico International Airport	General Aviation	\$3,090,333	\$154,517	\$188,854
	Cliff Hatfield Memorial Airport	Not a NPIAS Facility	\$0	\$817,722	\$90,858
	Imperial County Airport	Commercial Service Non-Primary	\$8,091,000	\$0	\$899,000
Imperial Total			\$17,871,696	\$1,306,757	\$1,587,567
Inyo	Bishop Airport	General Aviation	\$21,510,000	\$1,143,000	\$1,322,000
	Independence Airport	General Aviation	\$0	\$137,700	\$15,300
	Lone Pine Airport	General Aviation	\$1,745,000	\$154,750	\$114,139
	Shoshone Airport	Not a NPIAS Facility	\$0	\$931,500	\$103,500
Inyo Total			\$23,255,000	\$2,366,950	\$1,554,939
Kern	Bakersfield Municipal Airport	General Aviation	\$1,108,337	\$55,417	\$67,732
	California City Municipal Airport	General Aviation	\$937,800	\$46,890	\$57,310
	Inyokern Airport	General Aviation	\$19,150,002	\$957,500	\$1,321,878
	Kern Valley Airport	General Aviation	\$5,820,631	\$291,032	\$355,705
	Meadows Field Airport	Commercial Service Primary	\$17,096,723	\$0	\$1,899,636
	Mojave Air and Space Port	General Aviation	\$6,462,000	\$323,100	\$394,900
	Shafter Airport - Minter Field	General Aviation	\$7,911,000	\$395,550	\$483,450
	Taft Airport	General Aviation	\$1,125,710	\$56,286	\$68,793
	Tehachapi Municipal Airport	General Aviation	\$2,656,800	\$132,840	\$162,360
	Wasco-Kern Airport	General Aviation	\$937,500	\$46,874	\$31,415
Kern Total			\$63,206,504	\$2,305,488	\$4,843,179
Kings	Hanford Municipal Airport	General Aviation	\$3,135,600	\$156,780	\$191,620
Kings Total			\$3,135,600	\$156,780	\$191,620

County	Airport	Service Level	FAA	State	Local
Lake	Lampson Field Airport	General Aviation	\$2,745,000	\$137,250	\$167,750
Lake Total			\$2,745,000	\$137,250	\$167,750
Lassen	Susanville Municipal Airport	General Aviation	\$3,096,000	\$154,800	\$189,200
Lassen Total			\$3,096,000	\$154,800	\$189,200
Los Angeles	Bob Hope Airport	Commercial Service Primary	\$97,020,000	\$0	\$10,780,000
	Brackett Field Airport	Reliever	\$34,605,000	\$1,730,250	\$2,114,750
	Compton-Woodley Airport	Reliever	\$6,210,000	\$310,500	\$379,500
	General William J Fox Airport	General Aviation	\$16,155,000	\$807,750	\$987,250
	Jack Northrop Field-Hawthorne Municipal Airport	Reliever	\$5,660,100	\$283,005	\$345,895
	Long Beach Airport - Daugherty Field	Commercial Service Primary	\$55,265,233	\$0	\$6,140,581
	Los Angeles International Airport	Commercial Service Primary	\$168,570,000	\$0	\$2,172,684,000
	San Gabriel Valley (El Monte) Airport	Reliever	\$19,458,000	\$1,242,900	\$1,219,100
	Van Nuys Airport	Reliever	\$52,506,000	\$2,625,300	\$3,208,700
	Whiteman Airport	Reliever	\$10,421,620	\$775,858	\$651,122
Los Angeles Total			\$465,870,953	\$7,775,563	\$2,198,510,898
Madera	Chowchilla Airport	General Aviation	\$623,840	\$31,192	\$38,124
	Madera Municipal Airport	General Aviation	\$5,694,277	\$284,714	\$347,984
Madera Total			\$6,318,116	\$315,906	\$386,107
Marin	Gnoss Field Airport	Reliever	\$14,058,000	\$702,900	\$859,100
Marin Total			\$14,058,000	\$702,900	\$859,100
Mariposa	Mariposa - Yosemite Airport	General Aviation	\$4,041,000	\$202,050	\$246,950
Mariposa Total			\$4,041,000	\$202,050	\$246,950
Mendocino	Boonville Airport	General Aviation	\$274,500	\$13,725	\$16,775
	Little River Airport	General Aviation	\$4,618,482	\$230,924	\$282,241
	Round Valley Airport	General Aviation	\$1,251,000	\$62,550	\$76,450
	Ukiah Municipal Airport	General Aviation	\$4,549,500	\$227,475	\$278,025
	Willits Municipal Airport	General Aviation	\$2,197,500	\$109,875	\$134,292
Mendocino Total			\$12,890,983	\$644,549	\$787,782
Merced	Castle Airport	General Aviation	\$4,414,500	\$220,725	\$269,775
	Gustine Airport	General Aviation	\$751,500	\$37,575	\$45,925
	Los Banos Municipal Airport	General Aviation	\$3,582,000	\$179,100	\$218,900
	Turlock Municipal Airport	General Aviation	\$7,605,631	\$380,282	\$464,789
Merced Total			\$16,353,631	\$817,682	\$999,389

County	Airport	Service Level	FAA	State	Local
Modoc	Adin Airport	Not a NPIAS Facility	\$0	\$247,500	\$27,500
	Alturas Municipal Airport	General Aviation	\$4,870,800	\$243,540	\$297,660
	Cedarville Airport	General Aviation	\$856,800	\$42,840	\$52,360
	Fort Bidwell Airport	Not a NPIAS Facility	\$0	\$72,000	\$8,000
	Tulelake Airport	General Aviation	\$1,911,600	\$95,580	\$116,820
Modoc Total			\$7,639,200	\$701,460	\$502,340
Mono	Mammoth Yosemite Airport	Commercial Service Primary	\$36,692,718	\$0	\$4,076,969
Mono Total			\$36,692,718	\$0	\$4,076,969
Monterey	Marina Municipal Airport	General Aviation	\$1,116,000	\$55,800	\$68,200
	Mesa Del Rey Airport	General Aviation	\$2,494,800	\$124,740	\$152,460
	Monterey Regional Airport	Commercial Service Primary	\$98,334,507	\$0	\$10,926,056
	Salinas Municipal Airport	General Aviation	\$16,016,305	\$800,815	\$978,774
Monterey Total			\$117,961,612	\$981,355	\$12,125,490
Napa	Napa County Airport	Reliever	\$5,400,000	\$270,000	\$330,000
Napa Total			\$5,400,000	\$270,000	\$330,000
Nevada	Nevada County Airport	General Aviation	\$2,622,600	\$131,130	\$160,270
	Truckee-Tahoe Airport	General Aviation	\$12,766,500	\$638,325	\$780,175
Nevada Total			\$15,389,100	\$769,455	\$940,445
Orange	Fullerton Municipal Airport	Reliever	\$189,000	\$9,450	\$11,550
	John Wayne Airport, Orange Co.	Commercial Service Primary	\$87,403,986	\$0	\$300,568,933
Orange Total			\$87,592,986	\$9,450	\$300,580,483
Placer	Auburn Municipal Airport	General Aviation	\$36,758,931	\$2,017,947	\$2,266,379
	Blue Canyon Airport	Not a NPIAS Facility	\$0	\$171,000	\$19,000
	Lincoln Regional Airport - Karl Harder Field	Reliever	\$10,911,600	\$545,580	\$666,820
Placer Total			\$47,670,531	\$2,734,527	\$2,952,199
Plumas	Gansner Airport	General Aviation	\$2,677,500	\$133,875	\$163,625
	Nervino Airport	General Aviation	\$2,632,275	\$131,614	\$160,861
	Rogers Field Airport	General Aviation	\$2,418,300	\$120,915	\$147,785
Plumas Total			\$7,728,075	\$386,404	\$472,271

County	Airport	Service Level	FAA	State	Local
Riverside	Blythe Airport	General Aviation	\$16,372,100	\$818,605	\$1,000,517
	Chiriaco Summit Airport	Not a NPIAS Facility	\$0	\$500,000	\$55,556
	Corona Municipal Airport	General Aviation	\$517,500	\$25,875	\$31,625
	French Valley Airport	General Aviation	\$16,794,000	\$839,700	\$9,026,300
	Hemet-Ryan Airport	General Aviation	\$21,375,488	\$1,068,774	\$1,306,280
	Jacqueline Cochran Regional Airport	General Aviation	\$31,455,000	\$1,572,750	\$1,922,250
	March ARB and Inland Port Airport	Reliever	\$12,060,000	\$603,000	\$737,000
	Riverside Municipal Airport	Reliever	\$10,647,000	\$532,350	\$650,650
Riverside Total			\$109,221,088	\$5,961,054	\$14,730,178
Sacramento	Sacramento Executive Airport	Reliever	\$18,360,000	\$918,000	\$1,122,000
	Sacramento International Airport	Commercial Service Primary	\$281,475,000	\$0	\$31,275,000
	Sacramento Mather Airport	Reliever	\$13,500,000	\$675,000	\$825,000
Sacramento Total			\$313,335,000	\$1,593,000	\$33,222,000
San Benito	Hollister Municipal Airport	General Aviation	\$16,423,133	\$821,157	\$1,003,636
San Benito Total			\$16,423,133	\$821,157	\$1,003,636
San Bernardino	Apple Valley Airport	General Aviation	\$16,592,700	\$883,635	\$1,019,998
	Baker Airport	Not a NPIAS Facility	\$0	\$756,000	\$84,000
	Barstow-Daggett Airport	General Aviation	\$8,410,500	\$735,525	\$548,975
	Big Bear City Airport	General Aviation	\$9,551,499	\$477,575	\$583,703
	Chino Airport	Reliever	\$17,856,000	\$1,207,800	\$1,126,200
	Needles Airport	General Aviation	\$0	\$1,485,000	\$165,000
	Ontario International Airport	Commercial Service Primary	\$103,047,212	\$0	\$24,818,791
	Redlands Municipal Airport	General Aviation	\$1,004,400	\$50,220	\$61,380
	San Bernardino International Airport	Reliever	\$36,069,750	\$1,803,488	\$2,204,263
	Twentynine Palms Airport	General Aviation	\$9,447,158	\$742,358	\$607,326
San Bernardino Total			\$201,979,219	\$8,141,600	\$31,219,636

County	Airport	Service Level	FAA	State	Local
San Diego	Bob Maxwell Memorial Airfield Airport	General Aviation	\$5,328,000	\$266,400	\$325,600
	Brown Field Airport	Reliever	\$15,845,850	\$792,293	\$968,358
	Gillespie Field Airport	Reliever	\$8,100,000	\$405,000	\$495,000
	McClellan-Palomar Airport	General Aviation	\$34,290,000	\$1,714,500	\$2,095,500
	Montgomery-Gibbs Executive Airport	Reliever	\$10,740,196	\$537,010	\$656,345
	Ramona Airport	Reliever	\$7,650,000	\$382,500	\$467,500
	San Diego International Airport	Commercial Service Primary	\$321,559,593	\$0	\$222,344,660
San Diego Total			\$403,513,639	\$4,097,702	\$227,352,963
San Joaquin	Tracy Municipal Airport	General Aviation	\$2,777,481	\$138,874	\$169,735
San Joaquin Total			\$2,777,481	\$138,874	\$169,735
San Luis Obispo	Oceano County Airport	General Aviation	\$1,305,000	\$65,250	\$79,750
	Paso Robles Municipal Airport	General Aviation	\$4,950,000	\$247,500	\$302,500
	San Luis Obispo County Regional Airport	Commercial Service Primary	\$42,293,520	\$0	\$4,699,280
San Luis Obispo Total			\$48,548,520	\$312,750	\$5,081,530
San Mateo	Half Moon Bay Airport	Reliever	\$1,669,500	\$83,475	\$102,025
	San Carlos Airport	Reliever	\$1,360,800	\$68,040	\$83,160
	San Francisco International Airport	Commercial Service Primary	\$409,185,000	\$0	\$45,465,000
San Mateo Total			\$412,215,300	\$151,515	\$45,650,185
Santa Barbara	Santa Barbara Municipal Airport	Commercial Service Primary	\$61,312,500	\$0	\$8,312,500
	Santa Maria Public Airport	Commercial Service Primary	\$27,990,000	\$0	\$3,110,000
	Santa Ynez Airport	General Aviation	\$4,361,200	\$218,060	\$266,518
Santa Barbara Total			\$93,663,700	\$218,060	\$11,689,018
Santa Clara	Palo Alto Airport	Reliever	\$8,145,000	\$407,250	\$497,750
	Reid Hillview Airport	Reliever	\$5,568,264	\$278,413	\$340,283
	San Jose International Airport, Norman Y. Mineta	Commercial Service Primary	\$76,564,350	\$0	\$8,507,150
	San Martin Airport	Reliever	\$4,316,763	\$215,838	\$263,802
Santa Clara Total			\$94,594,377	\$901,501	\$9,608,985
Santa Cruz	Watsonville Municipal Airport	General Aviation	\$5,218,200	\$260,910	\$318,890
Santa Cruz Total			\$5,218,200	\$260,910	\$318,890
Shasta	Benton Field Airport	General Aviation	\$7,464,351	\$373,218	\$456,155
	Fall River Mills Airport	General Aviation	\$838,451	\$41,923	\$51,239
	Redding Municipal Airport	Commercial Service Primary	\$46,400,563	\$0	\$5,155,618
Shasta Total			\$54,703,365	\$415,140	\$5,663,012

County	Airport	Service Level	FAA	State	Local
Sierra	Sierraville Dearwater Airport	Not a NPIAS Facility	\$0	\$1,264,500	\$140,500
Sierra Total			\$0	\$1,264,500	\$140,500
Siskiyou	Dunsmuir Muni-Mott Airport	General Aviation	\$4,530,001	\$226,500	\$276,833
	Montague-Yreka Airport - Rohrer Field	Not a NPIAS Facility	\$0	\$297,000	\$33,000
	Scott Valley Airport - Bud Davis Field	General Aviation	\$1,414,105	\$70,705	\$86,418
	Siskiyou County Airport	General Aviation	\$15,876,000	\$793,800	\$970,200
	Weed Airport	General Aviation	\$4,809,000	\$240,450	\$293,883
Siskiyou Total			\$26,629,106	\$1,628,455	\$1,660,334
Solano	Nut Tree Airport	General Aviation	\$10,419,957	\$520,998	\$636,775
	Rio Vista Municipal Airport	General Aviation	\$3,003,750	\$150,188	\$183,563
Solano Total			\$13,423,707	\$671,185	\$820,338
Sonoma	Cloverdale Municipal Airport	General Aviation	\$1,786,500	\$89,325	\$109,175
	Healdsburg Municipal Airport	General Aviation	\$3,567,600	\$178,380	\$218,020
	Petaluma Municipal Airport	Reliever	\$2,141,100	\$107,055	\$130,845
Sonoma Total			\$7,495,200	\$374,760	\$458,040
Stanislaus	Modesto City - County Airport - Harry Sham Field	General Aviation	\$3,330,000	\$166,500	\$203,500
	Oakdale Municipal Airport	General Aviation	\$1,771,200	\$88,560	\$108,240
Stanislaus Total			\$5,101,200	\$255,060	\$311,740
Tehama	Corning Municipal Airport	General Aviation	\$1,417,500	\$70,875	\$86,625
	Red Bluff Municipal Airport	General Aviation	\$3,594,300	\$179,715	\$219,652
Tehama Total			\$5,011,800	\$250,590	\$306,277
Trinity	Hayfork Airport	General Aviation	\$9,000	\$450	\$550
	Hyampom Airport	Not a NPIAS Facility	\$0	\$9,000	\$10,000
	Lonnie Pool Field-Weaverville	General Aviation	\$18,000	\$900	\$1,100
	Ruth Airport	General Aviation	\$207,990	\$53,600	\$17,511
	Trinity Center James E. Swett Airport	General Aviation	\$4,401,000	\$220,050	\$268,950
Trinity Total			\$4,635,990	\$284,000	\$298,111
Tulare	Mefford Field Airport	General Aviation	\$15,354,000	\$767,700	\$938,300
	Porterville Municipal Airport	General Aviation	\$9,756,000	\$487,800	\$596,200
	Visalia Municipal Airport	General Aviation	\$8,106,273	\$405,314	\$495,383
	Woodlake Airport	General Aviation	\$10,756,350	\$537,818	\$657,333
Tulare Total			\$43,972,623	\$2,198,631	\$2,687,216

County	Airport	Service Level	FAA	State	Local
Tuolumne	Columbia Airport	General Aviation	\$5,492,900	\$274,645	\$335,677
	Pine Mountain Lake Airport	General Aviation	\$6,120,000	\$306,000	\$374,000
Tuolumne Total			\$11,612,900	\$580,645	\$709,677
Ventura	Camarillo Airport	Reliever	\$70,697,583	\$3,894,879	\$4,360,408
	Oxnard Airport	General Aviation	\$4,201,875	\$210,094	\$256,781
Ventura Total			\$74,899,458	\$4,104,973	\$4,617,189
Yolo	University Airport	General Aviation	\$1,732,298	\$86,615	\$105,863
	Yolo County-Davis Woodland Winters Airport	General Aviation	\$7,825,410	\$391,271	\$478,220
Yolo Total			\$9,557,708	\$477,885	\$584,082
Yuba	Yuba County Airport	General Aviation	\$18,135,000	\$906,750	\$1,108,250
Yuba Total			\$18,135,000	\$906,750	\$1,108,250
Grand Total			\$3,323,287,651	\$77,306,039	\$2,972,977,320

Table 2: 2026-2029 (4-Year Summary of Needs by Caltrans District)

Caltrans District	FAA	State	Local
1	\$58,358,211	\$5,997,486	\$5,738,760
2	\$109,443,536	\$3,820,849	\$9,091,544
3	\$458,189,705	\$17,741,235	\$43,063,732
4	\$599,561,022	\$5,010,613	\$64,994,278
5	\$281,815,165	\$2,594,232	\$30,218,564
6	\$342,975,343	\$5,775,180	\$32,458,914
7	\$540,770,411	\$11,880,536	\$2,203,128,088
8	\$311,200,306	\$14,102,654	\$45,949,813
9	\$59,947,718	\$2,366,950	\$5,631,908
10	\$52,047,912	\$2,602,396	\$3,180,706
11	\$421,385,335	\$5,404,459	\$228,940,530
12	\$87,592,986	\$9,450	\$300,580,483
Grand Total	\$3,323,287,651	\$77,306,039	\$2,972,977,320

Table 3: 2026-2029 (4-Year Summary by Program and Service Level)

Program and Service Level	Program Year	FAA	State	Local
A&D (General Aviation)	2026	\$0	\$2,407,500	\$267,500
	2027	\$0	\$1,845,000	\$205,000
	2028	\$0	\$7,274,700	\$808,300
	2029	\$0	\$695,700	\$77,300
A&D (General Aviation) Total		\$0	\$12,222,900	\$1,358,100
A&D (Non-NPIAS)	2026	\$0	\$1,521,522	\$169,058
	2027	\$0	\$1,679,000	\$195,556
	2028	\$0	\$1,683,000	\$187,000
	2029	\$0	\$3,811,500	\$423,500
A&D (Non-NPIAS) Total		\$0	\$8,695,022	\$975,114
AIP (Commercial)	2026	\$804,393,948	\$0	\$207,971,160
	2027	\$620,450,923	\$0	\$90,469,750
	2028	\$413,284,237	\$0	\$125,618,383
	2029	\$357,091,710	\$0	\$93,799,418
AIP (Commercial) Total		\$2,195,220,818	\$0	\$517,858,711
AIP (General Aviation)	2026	\$277,704,509	\$13,885,225	\$16,970,831
	2027	\$267,396,656	\$13,372,232	\$16,312,630
	2028	\$324,454,502	\$16,218,345	\$19,832,155
	2029	\$258,511,167	\$12,912,315	\$15,780,170
AIP (General Aviation) Total		\$1,128,066,833	\$56,388,118	\$68,895,786
Locally Funded Project	2026	\$0	\$0	\$1,141,830,309
	2027	\$0	\$0	\$1,091,102,640
	2028	\$0	\$0	\$114,259,220
	2029	\$0	\$0	\$36,697,440
Locally Funded Project Total		\$0	\$0	\$2,383,889,609
Grand Total		\$3,323,287,651	\$77,306,039	\$2,972,977,320

Table 4: 10-Year CIP List of Projects (2025-2034)

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Adin Airport	Engineering, Design, Construction Perimeter Fence	A&D	2025	\$ -	\$ 292,500	\$ 32,500	\$ 325,000
Adin Airport	Engineering, Design and Slurry Seal all Surfaces	A&D	2028	\$ -	\$ 247,500	\$ 27,500	\$ 275,000
Alpine County Airport	Crack Seal, Slurry Seal, and Restripe Runway and Taxiway	A&D	2025	\$ -	\$ 225,000	\$ 25,000	\$ 250,000
Alpine County Airport	AC Overlay and Restripe Runway	A&D	2033	\$ -	\$ 1,395,000	\$ 155,000	\$ 1,550,000
Alturas Municipal Airport	Construct: Displace Runway 31 Threshold 200 feet and Associate	AIP	2025	\$ 221,400	\$ 11,070	\$ 13,530	\$ 246,000
Alturas Municipal Airport	Remove Existing FAA VASI and Install New FAA 4-box PAPI	AIP	2025	\$ 495,900	\$ 24,795	\$ 30,305	\$ 551,000
Alturas Municipal Airport	FAA Reimbursable Agreement - PAPI Resident Engineering	AIP	2025	\$ 122,760	\$ 6,138	\$ 7,502	\$ 136,400
Alturas Municipal Airport	Rehabilitate and Widen Runway 13-31 - Design	AIP	2027	\$ 307,800	\$ 15,390	\$ 18,810	\$ 342,000
Alturas Municipal Airport	Design/Construct - Reseal Joints and Cracks in Airfield Pave	AIP	2028	\$ 331,200	\$ 16,560	\$ 20,240	\$ 368,000
Alturas Municipal Airport	Design/Construct - Reseal Joints and Cracks in Rwy 3-21	AIP	2028	\$ 272,700	\$ 13,635	\$ 16,665	\$ 303,000
Alturas Municipal Airport	Rehabilitate and Widen Runway 13-3	AIP	2029	\$ 3,959,100	\$ 197,955	\$ 241,945	\$ 4,399,000
Alturas Municipal Airport	Acquire New Snow Removal Equipment	AIP	2030	\$ 324,000	\$ 16,200	\$ 19,800	\$ 360,000
Alturas Municipal Airport	Remove Existing Connector Twy and Construct New Twy E	AIP	2030	\$ 438,300	\$ 21,915	\$ 26,785	\$ 487,000
Alturas Municipal Airport	Design/Construct Obstruction Mitigation (Tree Removal)	AIP	2031	\$ 123,300	\$ 6,165	\$ 7,535	\$ 137,000
Alturas Municipal Airport	Environmental Assessment - Extend Taxiways A and B	AIP	2031	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Alturas Municipal Airport	Design: Extend Twy B to Serve Existing Rwy 13-31	AIP	2032	\$ 166,500	\$ 8,325	\$ 10,175	\$ 185,000
Alturas Municipal Airport	ALP Narrative Including Updated ALP Drawings	AIP	2033	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Alturas Municipal Airport	Construct: Extend Twy B to Serve Existing Rwy 13-31	AIP	2033	\$ 1,567,800	\$ 78,390	\$ 95,810	\$ 1,742,000
Alturas Municipal Airport	ALP Update Narrative and Plans	AIP	2034	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Alturas Municipal Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Alturas Municipal Airport	Design: Extend Taxiway A to Serve Existing Runway 3-21	AIP	2034	\$ 107,100	\$ 5,355	\$ 6,545	\$ 119,000
Alturas Municipal Airport	Reconstruct Circle Hangar Taxilane	AIP	2034	\$ 656,100	\$ 32,805	\$ 40,095	\$ 729,000
Andy McBeth Airport	Perimeter Fencing	A&D	2027	\$ -	\$ 67,500	\$ 7,500	\$ 75,000
Andy McBeth Airport	Pavement Study	A&D	2028	\$ -	\$ 18,000	\$ 2,000	\$ 20,000
Andy McBeth Airport	Runway Rehabilitation - Phase 1 (Design)	A&D	2028	\$ -	\$ 67,500	\$ 7,500	\$ 75,000
Andy McBeth Airport	Runway Rehabilitation - Phase 2 (Construction)	A&D	2029	\$ -	\$ 315,000	\$ 35,000	\$ 350,000
Andy McBeth Airport	Obstruction Removal - Phase 1 (Design)	A&D	2032	\$ -	\$ 45,000	\$ 5,000	\$ 50,000
Andy McBeth Airport	Obstruction Removal - Phase 2 (Construction)	A&D	2034	\$ -	\$ 315,000	\$ 35,000	\$ 350,000
Apple Valley Airport	Construct New Detention Basin - Design	AIP	2025	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Apple Valley Airport	New Lighting Vault - Design	AIP	2025	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Apple Valley Airport	New Lighting Vault and Backup Generator	AIP	2026	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
Apple Valley Airport	Rehabilitation of Runway 18-3Construction	AIP	2026	\$ 7,830,000	\$ 391,500	\$ 478,500	\$ 8,700,000
Apple Valley Airport	ALUCP Update	A&D	2026	\$ -	\$ 54,000	\$ 6,000	\$ 60,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Apple Valley Airport	Construct New Detention Basin - Construction□	AIP	2027	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
Apple Valley Airport	Perimeter Fence - Construction	AIP	2028	\$ 3,917,700	\$ 195,885	\$ 239,415	\$ 4,353,000
Apple Valley Airport	Runway 8/26 MIRL, PAPI and Approach Lighting	AIP	2029	\$ 795,000	\$ 39,750	\$ 48,583	\$ 883,333
Apple Valley Airport	Taxiway Rehabilitation	AIP	2032	\$ 3,600,000	\$ 180,000	\$ 220,000	\$ 4,000,000
Apple Valley Airport	General Aviation Ramp Rehabilitation	AIP	2034	\$ 3,600,000	\$ 180,000	\$ 220,000	\$ 4,000,000
Auburn Municipal Airport	Design Taxiway Rehabilitation	AIP	2026	\$ 319,104	\$ 15,955	\$ 19,501	\$ 354,560
Auburn Municipal Airport	Des. /Cons. East End Airfield Access/Run-up Area Improv.	AIP	2026	\$ 297,000	\$ 14,850	\$ 18,150	\$ 330,000
Auburn Municipal Airport	Master Plan CEQA	AIP	2026	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Auburn Municipal Airport	Pavement Rehabilitation- East Tiedown Apron	AIP	2026	\$ 943,827	\$ 47,191	\$ 57,678	\$ 1,048,697
Auburn Municipal Airport	Pavement Rehabilitation Ph 1	AIP	2026	\$ 1,549,558	\$ 77,478	\$ 94,695	\$ 1,721,731
Auburn Municipal Airport	Runway Design	AIP	2026	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Auburn Municipal Airport	Runway Rehabilitation Design	AIP	2026	\$ 491,200	\$ 24,560	\$ 30,018	\$ 545,778
Auburn Municipal Airport	Terminal Apron Reconfigure Design and Apron Rehabilitation	AIP	2026	\$ 1,353,431	\$ 67,672	\$ 82,710	\$ 1,503,812
Auburn Municipal Airport	Wildlife Hazard Assessment	AIP	2026	\$ 40,500	\$ 2,025	\$ 2,475	\$ 45,000
Auburn Municipal Airport	ALUCP Update	A&D	2027	\$ -	\$ 180,000	\$ 20,000	\$ 200,000
Auburn Municipal Airport	Cracksealing Pavement Rehabilitation Plan Phase 2 of 5	AIP	2027	\$ 1,194,081	\$ 59,704	\$ 72,972	\$ 1,326,757
Auburn Municipal Airport	Pavement Rehab - Southeast Hangar Row, West Tiedown	AIP	2027	\$ 399,051	\$ 19,953	\$ 24,386	\$ 443,390
Auburn Municipal Airport	Runway/Taxiway Extension Pre-Design / EA	AIP	2027	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Auburn Municipal Airport	Taxiway A Rehabilitation/Relocation Construct- Phase 1	AIP	2027	\$ 1,855,525	\$ 92,776	\$ 113,393	\$ 2,061,694
Auburn Municipal Airport	Access Road - Denham Property	AIP	2028	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Auburn Municipal Airport	Construct Runway Extension and Rehabilitation	AIP	2028	\$ 8,550,000	\$ 427,500	\$ 522,500	\$ 9,500,000
Auburn Municipal Airport	Construct Taxiway Rehabilitation	AIP	2028	\$ 243,000	\$ 12,150	\$ 14,850	\$ 270,000
Auburn Municipal Airport	Cracksealing Pavement Rehabilitation Plan Phase 3 of 5	AIP	2028	\$ 1,111,317	\$ 55,566	\$ 67,914	\$ 1,234,797
Auburn Municipal Airport	Obstruction Removal in Approach Surfaces for RY 7/25	AIP	2028	\$ 58,500	\$ 2,925	\$ 3,575	\$ 65,000
Auburn Municipal Airport	Pavement Rehab - Taxilanes West of the West Tiedown	AIP	2028	\$ 1,422,737	\$ 71,137	\$ 86,945	\$ 1,580,819
Auburn Municipal Airport	Runway Extension Full Design	AIP	2028	\$ 397,599	\$ 19,880	\$ 24,298	\$ 441,777
Auburn Municipal Airport	Taxiway A Rehabilitation/Relocation Construct- Phase 2	AIP	2028	\$ 428,675	\$ 21,434	\$ 26,197	\$ 476,305
Auburn Municipal Airport	Terminal Apron Reconfigure Construct	AIP	2028	\$ 3,265,438	\$ 163,272	\$ 199,555	\$ 3,628,264
Auburn Municipal Airport	Terminal Design	AIP	2028	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Auburn Municipal Airport	Construct Perimeter Fence Phase 3	AIP	2029	\$ 272,250	\$ 13,613	\$ 16,638	\$ 302,500
Auburn Municipal Airport	Construct Perimeter Fencing Phase 2	AIP	2029	\$ 282,150	\$ 14,108	\$ 17,243	\$ 313,500
Auburn Municipal Airport	Construct Runway 7/25 Resurfacing	AIP	2029	\$ 4,407,487	\$ 220,374	\$ 269,346	\$ 4,897,208
Auburn Municipal Airport	Design Runway 7/25 Resurfacing	AIP	2029	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Auburn Municipal Airport	Terminal Construction	AIP	2029	\$ 5,926,500	\$ 296,325	\$ 362,175	\$ 6,585,000
Auburn Municipal Airport	Design and Construction REIL	AIP	2030	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Auburn Municipal Airport	Fuel Farm Replacement	AIP	2031	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Auburn Municipal Airport	Perimeter Fence Design III	AIP	2031	\$ 112,500	\$ 5,625	\$ 6,875	\$ 125,000
Auburn Municipal Airport	Phase III Perimeter Fence	AIP	2032	\$ 495,000	\$ 24,750	\$ 30,250	\$ 550,000
Baker Airport	ALUCP- Airport Land Use Compatibility Plan Update	A&D	2026	\$ -	\$ 270,000	\$ 30,000	\$ 300,000
Baker Airport	2027-Runway 15-33 Rehabilitation	A&D	2027	\$ -	\$ 486,000	\$ 54,000	\$ 540,000
Baker Airport	Fog Seal Rwy 15-33 and Parking Ramp	A&D	2030	\$ -	\$ 180,000	\$ 20,000	\$ 200,000
Baker Airport	APMS Update	A&D	2032	\$ -	\$ 180,000	\$ 20,000	\$ 200,000
Baker Airport	Runway Rehabilitation and Striping	A&D	2034	\$ -	\$ 498,600	\$ 55,400	\$ 554,000
Bakersfield Municipal Airport	New AWOS System (Construction)	AIP	2026	\$ 658,337	\$ 32,917	\$ 40,232	\$ 731,486
Bakersfield Municipal Airport	Replace Perimeter Fence	AIP	2027	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Bakersfield Municipal Airport	Rehabilitate Main Apron Pavement (Design)	AIP	2029	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Bakersfield Municipal Airport	Rehabilitate Main Apron Pavement (Construction)	AIP	2031	\$ 2,250,000	\$ 112,500	\$ 137,500	\$ 2,500,000
Bakersfield Municipal Airport	SW Hangar TxIn and Access Road Pvmnt. Rehab (Design)	AIP	2033	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Bakersfield Municipal Airport	Southwest Hangar TxIn and Access Road Pvmnt. Rehab	AIP	2034	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Banning Municipal Airport	Sign/Stripe Terminal Lot and Apron and Relocate Light	AIP	2030	\$ 40,500	\$ 2,025	\$ 2,475	\$ 45,000
Banning Municipal Airport	Sign/Stripe Terminal and Apron and Relocate Light Obst.	AIP	2032	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Barstow-Daggett Airport	Acquire Runway and Taxiway Edge Lights	AIP	2025	\$ 420,631	\$ 21,032	\$ 25,705	\$ 467,368
Barstow-Daggett Airport	ALP Update	AIP	2025	\$ 568,421	\$ 28,421	\$ 34,737	\$ 631,579
Barstow-Daggett Airport	Runway 8-26 Rehabilitation (Design)	AIP	2026	\$ 630,000	\$ 31,500	\$ 38,500	\$ 700,000
Barstow-Daggett Airport	ALUCP - Airport Land Use Compatibility Plan Update	A&D	2026	\$ -	\$ 315,000	\$ 35,000	\$ 350,000
Barstow-Daggett Airport	Runway 8-26 Rehabilitation (Construction)	AIP	2028	\$ 7,780,500	\$ 389,025	\$ 475,475	\$ 8,645,000
Barstow-Daggett Airport	Apron Rehabilitation (Design Reimb. and Construction)	AIP	2030	\$ 3,870,000	\$ 193,500	\$ 236,500	\$ 4,300,000
Barstow-Daggett Airport	Twy A, B and C Pavement Preservation (Design and Cons.)	AIP	2032	\$ 3,240,000	\$ 162,000	\$ 198,000	\$ 3,600,000
Benton Field Airport	Reconstruct Eastside T-Hangar Taxilanes and Drainage Design	AIP	2025	\$ 333,000	\$ 16,650	\$ 20,350	\$ 370,000
Benton Field Airport	Reconstruct Eastside T-Hangar Taxilanes and Drainage	AIP	2026	\$ 3,213,000	\$ 160,650	\$ 196,350	\$ 3,570,000
Benton Field Airport	Security Fencing - North RPZ - Design	AIP	2026	\$ 61,426	\$ 3,071	\$ 3,754	\$ 68,251
Benton Field Airport	Underground Powerline in the RPZ - Design	AIP	2026	\$ 114,346	\$ 5,717	\$ 6,988	\$ 127,051
Benton Field Airport	Security Fencing - North RPZ	AIP	2027	\$ 259,875	\$ 12,994	\$ 15,881	\$ 288,750
Benton Field Airport	Underground Powerline in the RPZ	AIP	2027	\$ 378,000	\$ 18,900	\$ 23,100	\$ 420,000
Benton Field Airport	Northeast Apron and Taxilanes Reconstruction	AIP	2028	\$ 309,753	\$ 15,488	\$ 18,929	\$ 344,170
Benton Field Airport	Airport Pavement Management Study	AIP	2028	\$ 47,251	\$ 2,363	\$ 2,888	\$ 52,501
Benton Field Airport	Northeast Apron and Taxilanes Reconstruction	AIP	2029	\$ 2,457,000	\$ 122,850	\$ 150,150	\$ 2,730,000
Benton Field Airport	East Apron Pavement Rehab	AIP	2029	\$ 108,675	\$ 5,434	\$ 6,641	\$ 120,750
Benton Field Airport	Rehabilitate Taxiway A	AIP	2029	\$ 80,325	\$ 4,016	\$ 4,909	\$ 89,250
Benton Field Airport	Westside T-Hangar Taxilane Reconstruction	AIP	2029	\$ 340,200	\$ 17,010	\$ 20,790	\$ 378,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Benton Field Airport	Runway 16 Approach Obstruction Removal	AIP	2029	\$ 94,500	\$ 4,725	\$ 5,775	\$ 105,000
Benton Field Airport	East Apron Pavement Rehabilitation	AIP	2030	\$ 122,850	\$ 6,143	\$ 7,508	\$ 136,500
Benton Field Airport	Rehabilitate Parallel Taxiway A	AIP	2030	\$ 378,000	\$ 18,900	\$ 23,100	\$ 420,000
Benton Field Airport	Westside T-Hangar Taxiway Reconstruction	AIP	2030	\$ 3,600,045	\$ 180,002	\$ 220,003	\$ 4,000,050
Benton Field Airport	Construct T-Hangar Taxilane	AIP	2030	\$ 118,125	\$ 5,906	\$ 7,219	\$ 131,250
Big Bear City Airport	AWOS Replacement (BIL Funding)	AIP	2025	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Big Bear City Airport	Runway 26 Obstruction Survey (BIL Funding)	AIP	2025	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Big Bear City Airport	Runway 8-26 Rehab (Fog Seal/Remarking)(design/Cons.)	AIP	2025	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Big Bear City Airport	Taxiway A - Design (BIL Funding)	AIP	2025	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Big Bear City Airport	Acquire Security Equipment: Perimeter Gates (BIL Funding)	AIP	2026	\$ 127,499	\$ 6,375	\$ 7,792	\$ 141,666
Big Bear City Airport	Acquire Snow Removal Equipment	AIP	2026	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
Big Bear City Airport	Runway 26 Approach Obstruction Removal (BIL Funding)	AIP	2026	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Big Bear City Airport	Acquire Equipment: Sweeper	AIP	2027	\$ 289,000	\$ 14,450	\$ 17,661	\$ 321,111
Big Bear City Airport	Acquire Snow Removal Equipment Phase 2	AIP	2027	\$ 720,000	\$ 36,000	\$ 44,000	\$ 800,000
Big Bear City Airport	Taxiway A: Construction	AIP	2027	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Big Bear City Airport	Terminal Apron Reconstruction: Design	AIP	2027	\$ 495,000	\$ 24,750	\$ 30,250	\$ 550,000
Big Bear City Airport	Terminal Apron: Construction	AIP	2029	\$ 5,400,000	\$ 270,000	\$ 330,000	\$ 6,000,000
Big Bear City Airport	ALP Update Including Narrative Report and AGIS Survey	AIP	2030	\$ 720,000	\$ 36,000	\$ 44,000	\$ 800,000
Big Bear City Airport	Runway 8-26 Rehabilitation	AIP	2032	\$ 4,500,000	\$ 225,000	\$ 275,000	\$ 5,000,000
Big Bear City Airport	Taxiway B Rehabilitation	AIP	2034	\$ 3,600,000	\$ 180,000	\$ 220,000	\$ 4,000,000
Bishop Airport	Runway 12-30 RSA Improvement Project	AIP	2025	\$ 2,635,200	\$ 131,760	\$ 161,040	\$ 2,928,000
Bishop Airport	Airport Perimeter Fence (Des. and Cons.)	AIP	2026	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Bishop Airport	Wildlife Hazard Assessment	AIP	2026	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Bishop Airport	Construct Commercial Service Terminal	AIP	2027	\$ 9,900,000	\$ 495,000	\$ 605,000	\$ 11,000,000
Bishop Airport	Master Plan Update	AIP	2027	\$ 765,000	\$ 38,250	\$ 46,750	\$ 850,000
Bishop Airport	Construct Airline Ramp	AIP	2028	\$ 9,000,000	\$ 450,000	\$ 550,000	\$ 10,000,000
Bishop Airport	Airport Land Use Compatibility Plan	A&D	2029	\$ -	\$ 67,500	\$ 7,500	\$ 75,000
Bishop Airport	Construct Runway 12-30 Blast Pads	AIP	2029	\$ 765,000	\$ 38,250	\$ 46,750	\$ 850,000
Bishop Airport	Runway 8-26 Closure/Conversion	AIP	2030	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Bishop Airport	Relocation of Parallel Twy 'A' (Construction)	AIP	2032	\$ 7,065,000	\$ 353,250	\$ 431,750	\$ 7,850,000
Blue Canyon Airport	Runway Re-Surfacing	A&D	2025	\$ -	\$ 108,000	\$ 12,000	\$ 120,000
Blue Canyon Airport	Blue Canyon Airport Mastication	A&D	2026	\$ -	\$ 90,000	\$ 10,000	\$ 100,000
Blue Canyon Airport	Replace Aircraft Tiedowns	A&D	2026	\$ -	\$ 4,500	\$ 500	\$ 5,000
Blue Canyon Airport	Easement Acquisition	A&D	2028	\$ -	\$ 31,500	\$ 3,500	\$ 35,000
Blue Canyon Airport	Runway Lighting Repair	A&D	2028	\$ -	\$ 45,000	\$ 5,000	\$ 50,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Blythe Airport	Runway 8-26 Rehabilitation, Phase I Design	AIP	2025	\$ 990,000	\$ 49,500	\$ 60,500	\$ 1,100,000
Blythe Airport	Rehab Primary Apron: Lighting Control Access Gate, Cons.	AIP	2026	\$ 858,000	\$ 42,900	\$ 52,433	\$ 953,333
Blythe Airport	Runway 8-26 Rehabilitation, Phase II Construction	AIP	2026	\$ 8,910,000	\$ 445,500	\$ 544,500	\$ 9,900,000
Blythe Airport	Twy A (East) and Twy Connectors Pvmnt. Recons, Ph I Design	AIP	2026	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Blythe Airport	Rehab Rwy and Twy A, B, C, D, E, F, AA - LED Upgrades, Des.	AIP	2027	\$ 224,100	\$ 11,205	\$ 13,695	\$ 249,000
Blythe Airport	Twy A (East) and Twy Connectors Pvmnt. Recons, Ph 2 Cons.	AIP	2027	\$ 3,500,000	\$ 175,000	\$ 213,889	\$ 3,888,889
Blythe Airport	General Aviation Apron Rehabilitation, Phase I Design	AIP	2029	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
Blythe Airport	Rehab Rwy and Twy A, B, C, D, F, AA - Electrical Upgrades	AIP	2029	\$ 1,890,000	\$ 94,500	\$ 115,500	\$ 2,100,000
Blythe Airport	Rehabilitate Runway 8-26 (Fog Seal)	AIP	2032	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Blythe Airport	Rehabilitate Taxiway A (Fog Seal)	AIP	2034	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Bob Hope Airport	Replacement Passenger Terminal	AIP	2025	\$ 999,066,731	\$ -	\$ 111,007,415	\$ 1,110,074,145
Bob Hope Airport	ALP Update/Master Plan	AIP	2026	\$ 1,350,000	\$ -	\$ 150,000	\$ 1,500,000
Bob Hope Airport	ARFF Truck Replacement	AIP	2026	\$ 1,170,000	\$ -	\$ 130,000	\$ 1,300,000
Bob Hope Airport	Existing Terminal Demolition	AIP	2026	\$ 27,000,000	\$ -	\$ 3,000,000	\$ 30,000,000
Bob Hope Airport	Pavement Management Plan	AIP	2026	\$ 1,350,000	\$ -	\$ 150,000	\$ 1,500,000
Bob Hope Airport	Air Operations Area Fencing and Taxiways A/C Extension	AIP	2027	\$ 31,500,000	\$ -	\$ 3,500,000	\$ 35,000,000
Bob Hope Airport	Runway 8/26 Reconstruction	AIP	2028	\$ 27,000,000	\$ -	\$ 3,000,000	\$ 30,000,000
Bob Hope Airport	Runway 15/33 Rehabilitation	AIP	2029	\$ 7,650,000	\$ -	\$ 850,000	\$ 8,500,000
Bob Hope Airport	Vehicle Service Road Rehabilitation	AIP	2030	\$ 9,585,000	\$ -	\$ 1,065,000	\$ 10,650,000
Bob Maxwell Memorial Airfield Airport	East Apron Reconstruction (Design)	AIP	2025	\$ 212,888	\$ 10,644	\$ 13,010	\$ 236,542
Bob Maxwell Memorial Airfield Airport	East Apron Drainage Improvements (Design)	AIP	2025	\$ 17,807	\$ 890	\$ 1,088	\$ 19,786
Bob Maxwell Memorial Airfield Airport	Acquire Airport Sweeper	AIP	2025	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Bob Maxwell Memorial Airfield Airport	Rwy and Vault Building Electrical Improvements (Cons.)	AIP	2026	\$ 1,012,500	\$ 50,625	\$ 61,875	\$ 1,125,000
Bob Maxwell Memorial Airfield Airport	REIL's (Construction)	AIP	2026	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000
Bob Maxwell Memorial Airfield Airport	Connector Twy Recons. and Removal of Twy B (Design)	AIP	2026	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Bob Maxwell Memorial Airfield Airport	Connector Twy Edge Lights and Hold Position Signs (Design)	AIP	2026	\$ 72,000	\$ 3,600	\$ 4,400	\$ 80,000
Bob Maxwell Memorial Airfield Airport	PAPI's (Design)	AIP	2026	\$ 18,000	\$ 900	\$ 1,100	\$ 20,000
Bob Maxwell Memorial Airfield Airport	East Apron Reconstruction and Drainage Improv. (Cons.)	AIP	2027	\$ 2,340,000	\$ 117,000	\$ 143,000	\$ 2,600,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Bob Maxwell Memorial Airfield Airport	Connector Twys Recons. and Removal of Twy B	AIP	2028	\$ 1,170,000	\$ 58,500	\$ 71,500	\$ 1,300,000
Bob Maxwell Memorial Airfield Airport	Connector Twy Edge Lights and Hold Position Signs	AIP	2028	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Bob Maxwell Memorial Airfield Airport	PAPI's (Construction)	AIP	2028	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Bob Maxwell Memorial Airfield Airport	ALP Update and Narrative Report	AIP	2030	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Boonville Airport	Slurry Seal Runway 13/31	AIP	2027	\$ 193,500	\$ 9,675	\$ 11,825	\$ 215,000
Boonville Airport	Design Only-Obstruction Removal/Culvert Extension	AIP	2029	\$ 81,000	\$ 4,050	\$ 4,950	\$ 90,000
Boonville Airport	Obstruction Removal (Dam and Spillway) and New Fence	AIP	2030	\$ 517,500	\$ 25,875	\$ 31,625	\$ 575,000
BORREGO Valley Airport	Rehabilitate Rwy 08-26	AIP	2034	\$ 6,750,000	\$ 337,500	\$ 412,500	\$ 7,500,000
Brackett Field Airport	Recons. Rwy 8L/26R and Cross Twys C, D, E, F, G (Cons.)	AIP	2027	\$ 20,700,000	\$ 1,035,000	\$ 1,265,000	\$ 23,000,000
Brackett Field Airport	Reconstruct North Taxilanes and East Ramp [Design]	AIP	2027	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Brackett Field Airport	Reconstruct North Taxilanes and East Ramp [Construction]	AIP	2029	\$ 13,320,000	\$ 666,000	\$ 814,000	\$ 14,800,000
Brawley Municipal Airport	Design and Construct Crack fill of Airfield Pavements	AIP	2025	\$ 141,750	\$ 7,088	\$ 8,663	\$ 157,500
Brawley Municipal Airport	Design and Construct Overlay of Ken Bemis Drive	AIP	2025	\$ 307,125	\$ 15,356	\$ 18,769	\$ 341,250
Brawley Municipal Airport	Design and Construct Pilot Lounge Modernization Upgrade	AIP	2025	\$ 354,375	\$ 17,719	\$ 21,656	\$ 393,750
Brawley Municipal Airport	Design of Seal and Stripe Airfield Project	AIP	2026	\$ 233,888	\$ 11,694	\$ 14,293	\$ 259,875
Brawley Municipal Airport	Construct Reconstruction of Runway 8/26	AIP	2027	\$ 4,781,700	\$ 239,085	\$ 292,215	\$ 5,313,000
Brawley Municipal Airport	Construct Seal and Stripe Airfield Project	AIP	2028	\$ 1,417,500	\$ 70,875	\$ 86,625	\$ 1,575,000
Brawley Municipal Airport	Design Seal and Stripe Runway and Connecting Taxiways	AIP	2029	\$ 257,276	\$ 12,864	\$ 15,722	\$ 285,862
Brawley Municipal Airport	Construct Seal and Stripe Runway and Connecting Twys	AIP	2030	\$ 1,701,000	\$ 85,050	\$ 103,950	\$ 1,890,000
Brown Field Airport	Twy B Rehab, Twy C Rehab Realign/Pavement Removal Des.	AIP	2025	\$ 335,229	\$ 16,761	\$ 20,486	\$ 372,477
Brown Field Airport	Twy G (East) Design	AIP	2026	\$ 1,751,058	\$ 87,553	\$ 107,009	\$ 1,945,620
Brown Field Airport	Twy B Rehab, Twy C Rehab Realign/Pavement Removal	AIP	2027	\$ 2,223,990	\$ 111,200	\$ 135,911	\$ 2,471,100
Brown Field Airport	Twy G (East) Reconstruction	AIP	2027	\$ 4,085,802	\$ 204,290	\$ 249,688	\$ 4,539,780
Brown Field Airport	Airfield Lighting Upgrade and AD Beacon	AIP	2028	\$ 5,985,000	\$ 299,250	\$ 365,750	\$ 6,650,000
Brown Field Airport	PMMP Survey/Study	AIP	2028	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Brown Field Airport	Wildlife Hazard Management Plan	AIP	2028	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Brown Field Airport	Airfield Signage and Markings Plan	AIP	2029	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Brown Field Airport	Terminal Parking Lot Design	AIP	2029	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Brown Field Airport	Terminal Parking Lot Rehabilitation	AIP	2030	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Buchanan Field Airport	Pavement Maintenance Management Plan (PMMP)	AIP	2025	\$ 202,500	\$ -	\$ 22,500	\$ 225,000
Buchanan Field Airport	Replace 1500 Gallon ARFF Vehicle	AIP	2026	\$ 810,000	\$ -	\$ 90,000	\$ 900,000
Buchanan Field Airport	Airport Master Plan and Enviro. Review; Including RSAT	AIP	2027	\$ 990,000	\$ -	\$ 110,000	\$ 1,100,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Buchanan Field Airport	Construct RIM Hotspot Mitigation for the Runway 32R Run-Up A	AIP	2027	\$ 1,318,749	\$ -	\$ 146,528	\$ 1,465,277
Buchanan Field Airport	Pavement Rehab of Twy Golf and Rwy 01L Run-up - Des.	AIP	2027	\$ 226,310	\$ -	\$ 25,146	\$ 251,455
Buchanan Field Airport	Design Taxiway Alpha Repairs	AIP	2027	\$ 652,500	\$ -	\$ 72,500	\$ 725,000
Buchanan Field Airport	Asphalt Pvmt. Overlay of Twy G and Rwy 01L Run-Up Area	AIP	2028	\$ 1,357,854	\$ -	\$ 150,873	\$ 1,508,727
Buchanan Field Airport	Rehabilitate Main Ramp (Design and Environmental)	AIP	2028	\$ 270,000	\$ -	\$ 30,000	\$ 300,000
Buchanan Field Airport	Slurry Seal Rwy 01L-19R	AIP	2028	\$ 315,000	\$ -	\$ 35,000	\$ 350,000
Buchanan Field Airport	Slurry Seal Rwy 14L-32R	AIP	2028	\$ 270,000	\$ -	\$ 30,000	\$ 300,000
Buchanan Field Airport	Taxiway A and Run-up Areas Asphalt Overlay	AIP	2028	\$ 2,700,000	\$ -	\$ 300,000	\$ 3,000,000
Buchanan Field Airport	Taxiway G Reconstruction and Pavement Replacement	AIP	2028	\$ 1,786,500	\$ -	\$ 198,500	\$ 1,985,000
Buchanan Field Airport	Air Traffic Control Tower Siting Study	AIP	2029	\$ 675,000	\$ -	\$ 75,000	\$ 750,000
Buchanan Field Airport	Design Replacement of Twy Juliet Pavement	AIP	2029	\$ 472,500	\$ -	\$ 52,500	\$ 525,000
Buchanan Field Airport	Design Taxiway Lighting Upgrades	AIP	2029	\$ 675,000	\$ -	\$ 75,000	\$ 750,000
Buchanan Field Airport	Main Ramp Pavement Overlay and Reconstruction	AIP	2029	\$ 1,080,000	\$ -	\$ 120,000	\$ 1,200,000
Buchanan Field Airport	Airfield Electrical System Replacement	AIP	2030	\$ 3,150,000	\$ -	\$ 350,000	\$ 3,500,000
Buchanan Field Airport	Design and Engineer RIM Hotspot Mitigation - Runway 32	AIP	2030	\$ 219,792	\$ -	\$ 24,421	\$ 244,213
Buchanan Field Airport	EA for Runway 32R Run-Up Area	AIP	2030	\$ 225,000	\$ -	\$ 25,000	\$ 250,000
Buchanan Field Airport	Runway 01L/19R Runway Safety Overruns	AIP	2030	\$ 450,000	\$ -	\$ 50,000	\$ 500,000
Buchanan Field Airport	Runway 01L-19R Safety Overrun Improvements (Cons.)	AIP	2031	\$ 114,300	\$ -	\$ 12,700	\$ 127,000
Buchanan Field Airport	Design Repair of Taxiway Golf Pavement	AIP	2032	\$ 180,000	\$ -	\$ 20,000	\$ 200,000
Buchanan Field Airport	Enviro. Analysis and Design for Replacement ATCT	AIP	2032	\$ 2,250,000	\$ -	\$ 250,000	\$ 2,500,000
Buchanan Field Airport	Replace Airfield Signs and Electrical Systems	AIP	2032	\$ 2,250,000	\$ -	\$ 250,000	\$ 2,500,000
Buchanan Field Airport	Pavement Rehab and Marking Removal/Replace	AIP	2033	\$ 3,600,000	\$ -	\$ 400,000	\$ 4,000,000
Buchanan Field Airport	FED Part 150 Update	AIP	2034	\$ 436,500	\$ -	\$ 48,500	\$ 485,000
Buchanan Field Airport	Noise Monitoring and Flight Track System	AIP	2034	\$ 1,215,000	\$ -	\$ 135,000	\$ 1,350,000
Buchanan Field Airport	Pavement Surface Treatment Runways 14R-32L and 01R/19L	AIP	2034	\$ 3,420,000	\$ -	\$ 380,000	\$ 3,800,000
Byron Airport	Pavement Maintenance Mgt. Plan (PMMP)	AIP	2025	\$ 63,000	\$ 3,150	\$ 3,850	\$ 70,000
Byron Airport	Wildlife Hazard Site Visit	AIP	2026	\$ 63,000	\$ 3,150	\$ 3,850	\$ 70,000
Byron Airport	Wildlife Hazard Assessment Improvements Environmental	AIP	2027	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Byron Airport	Wildlife Hazard Management Plan	AIP	2027	\$ 126,000	\$ 6,300	\$ 7,700	\$ 140,000
Byron Airport	Install safety perimeter fencing upgrades	LFP	2028	\$ -	\$ -	\$ 1,187,000	\$ 1,187,000
Byron Airport	Taxiway Edge Lighting and Electrical Upgrades (Design)	AIP	2028	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Byron Airport	Taxiway Edge Lighting (Des., Engineering and Environmental)	AIP	2028	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Byron Airport	Wildlife Hazard Assessment Improvements	AIP	2028	\$ 1,068,750	\$ 53,438	\$ 65,313	\$ 1,187,500
Byron Airport	Land Purchase RSA Rwy 05-23	LFP	2029	\$ -	\$ -	\$ 2,200,000	\$ 2,200,000
Byron Airport	Runway 12/30 Extension 25% Design and Environmental	AIP	2029	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Byron Airport	Taxiway Alpha Edge Lighting and Electrical Upgrades	AIP	2029	\$ 1,384,200	\$ 69,210	\$ 84,590	\$ 1,538,000
Byron Airport	Runway 12/30 Design, Engineering and Environmental	AIP	2030	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Byron Airport	Airport Master Plan and ALP with Narrative Study	AIP	2031	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Byron Airport	ALUCP: Airport Land Use Compatibility Plan Update	A&D	2032	\$ -	\$ 180,000	\$ 20,000	\$ 200,000
Byron Airport	Environmental Review for the Purchase of Three Parcels	LFP	2032	\$ -	\$ -	\$ 275,000	\$ 275,000
Byron Airport	Rwy 12-30 Extension (Construction)	AIP	2033	\$ 4,050,000	\$ 202,500	\$ 247,500	\$ 4,500,000
Byron Airport	Slurry Seal Rwy 5-23	AIP	2034	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Calaveras Co. - Maury Rasmussen Airport	PAPI Construction- Reimbursable Agreement	AIP	2026	\$ 286,200	\$ 14,310	\$ 17,490	\$ 318,000
Calaveras Co. - Maury Rasmussen Airport	PAPI Design	AIP	2026	\$ 112,500	\$ 5,625	\$ 6,875	\$ 125,000
Calaveras Co. - Maury Rasmussen Airport	Runway Pavement Reconstruction Design	AIP	2026	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Calaveras Co. - Maury Rasmussen Airport	AWOS Upgrade	AIP	2027	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Calaveras Co. - Maury Rasmussen Airport	PAPI Construction	AIP	2028	\$ 1,125,000	\$ 56,250	\$ 68,750	\$ 1,250,000
Calaveras Co. - Maury Rasmussen Airport	Runway Pavement Recons. and Safety Area Grading Cons.	AIP	2028	\$ 5,332,500	\$ 266,625	\$ 325,875	\$ 5,925,000
Calaveras Co. - Maury Rasmussen Airport	Design Parallel Taxiway A, Connectors and Apron Connectors	AIP	2029	\$ 333,000	\$ 16,650	\$ 20,350	\$ 370,000
Calaveras Co. - Maury Rasmussen Airport	Prepare Airfield Pavement Mgmt. System Plan	AIP	2029	\$ 54,000	\$ 2,700	\$ 3,300	\$ 60,000
Calaveras Co. - Maury Rasmussen Airport	Construct Parallel Taxiway A and Connectors Pvmnt. Recons.	AIP	2031	\$ 3,501,000	\$ 175,050	\$ 213,950	\$ 3,890,000
Calaveras Co. - Maury Rasmussen Airport	Apron Pavement Rehabilitation Design	AIP	2032	\$ 129,600	\$ 6,480	\$ 7,920	\$ 144,000
Calaveras Co. - Maury Rasmussen Airport	Apron Pavement Rehabilitation Construction	AIP	2034	\$ 774,000	\$ 38,700	\$ 47,300	\$ 860,000
Calexico International Airport	Runway Pavement Seal Design - BIL	AIP	2025	\$ 74,700	\$ 3,735	\$ 4,565	\$ 83,000
Calexico International Airport	Design Runway Pavement Seal Construction	AIP	2026	\$ 546,300	\$ 27,315	\$ 33,385	\$ 607,000
Calexico International Airport	Design Southeast Apron Pavement Rehabilitation	AIP	2026	\$ 138,073	\$ 6,904	\$ 8,438	\$ 153,414
Calexico International Airport	Construct SE Apron Pavement Rehab	AIP	2027	\$ 462,221	\$ 23,111	\$ 28,247	\$ 513,579
Calexico International Airport	Repairs to Terminal Building	AIP	2027	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Calexico International Airport	North Hangar Area Pavement Reconstruction Design	AIP	2028	\$ 151,879	\$ 7,594	\$ 9,281	\$ 168,754
Calexico International Airport	North Hangar Area Pavement Construction	AIP	2029	\$ 1,656,860	\$ 82,843	\$ 101,253	\$ 1,840,956
California City Municipal Airport	Runway 6-24 Crack Fill and seal	AIP	2025	\$ 276,628	\$ 13,831	\$ 16,905	\$ 307,364

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
California City Municipal Airport	Airport Fueling Station Upgrades Ph-1	AIP	2027	\$ 219,600	\$ 10,980	\$ 13,420	\$ 244,000
California City Municipal Airport	Airport Fueling Station Upgrades Ph-2	AIP	2028	\$ 718,200	\$ 35,910	\$ 43,890	\$ 798,000
California City Municipal Airport	Airport Lighting Electrical Upgrades Ph-1	AIP	2030	\$ 178,200	\$ 8,910	\$ 10,890	\$ 198,000
California City Municipal Airport	Airport Lighting Electrical Upgrades Ph-2	AIP	2031	\$ 1,170,770	\$ 58,539	\$ 71,547	\$ 1,300,856
California Redwood Coast (Arcata)-Humboldt County Airport	Acquire ARFF Vehicle	AIP	2025	\$ 1,170,000	\$ -	\$ 130,000	\$ 1,300,000
California Redwood Coast (Arcata)-Humboldt County Airport	Reconstruct/Relocate Twy A (Cons.)	AIP	2026	\$ 7,740,000	\$ -	\$ 860,000	\$ 8,600,000
California Redwood Coast (Arcata)-Humboldt County Airport	Reconstruct/Relocate Twy A (Reimb. Agreement)	AIP	2026	\$ 112,500	\$ -	\$ 12,500	\$ 125,000
California Redwood Coast (Arcata)-Humboldt County Airport	Rehab/Reconfigure Terminal Aircraft Parking Apron (Cons.)	AIP	2026	\$ 3,600,000	\$ -	\$ 400,000	\$ 4,000,000
California Redwood Coast (Arcata)-Humboldt County Airport	Rehab/Reconfigure Terminal Aircraft Parking Apron Ph1 Cons.	AIP	2027	\$ 2,058,331	\$ -	\$ 228,703	\$ 2,287,034
California Redwood Coast (Arcata)-Humboldt County Airport	Approach Lighting Improvements - Design	AIP	2028	\$ 225,000	\$ -	\$ 25,000	\$ 250,000
California Redwood Coast (Arcata)-Humboldt County Airport	Rehab General Aviation Aircraft Parking Apron	AIP	2028	\$ 2,250,000	\$ -	\$ 250,000	\$ 2,500,000
California Redwood Coast (Arcata)-Humboldt County Airport	Rehab/Reconfigure Terminal Aircraft Parking Apron Ph2 Cons.	AIP	2028	\$ 2,441,669	\$ -	\$ 271,297	\$ 2,712,966
California Redwood Coast (Arcata)-Humboldt County Airport	Approach Lighting Improvements - Construct	AIP	2029	\$ 3,600,000	\$ -	\$ 400,000	\$ 4,000,000
California Redwood Coast (Arcata)-Humboldt County Airport	Rehab General Aviation Aircraft Parking Apron (Design)	AIP	2030	\$ 225,000	\$ -	\$ 25,000	\$ 250,000
California Redwood Coast (Arcata)-Humboldt County Airport	Rehab General Aviation Aircraft Parking Apron (Cons.)	AIP	2031	\$ 315,000	\$ -	\$ 35,000	\$ 350,000
California Redwood Coast (Arcata)-Humboldt County Airport	Terminal Building Improvements	AIP	2032	\$ 36,000,000	\$ -	\$ 4,000,000	\$ 40,000,000
California Redwood Coast (Arcata)-Humboldt County Airport	Rehab Rwy 14-32	AIP	2034	\$ 315,000	\$ -	\$ 35,000	\$ 350,000
Camarillo Airport	Runway 8-26 Reconstruction	AIP	2026	\$ 40,161,941	\$ 2,008,097	\$ 2,454,341	\$ 44,624,379
Camarillo Airport	Update Airport Comprehensive Land Use Plan (ACLUP)	A&D	2026	\$ -	\$ 360,000	\$ 40,000	\$ 400,000
Camarillo Airport	Final Design for Taxiway Connectors Recons. (RIM at Twy A)	AIP	2027	\$ 1,830,218	\$ 91,511	\$ 111,847	\$ 2,033,576
Camarillo Airport	Taxiway Connectors Reconstruction (RIM at Twy A)	AIP	2028	\$ 28,705,424	\$ 1,435,271	\$ 1,754,220	\$ 31,894,915
Camarillo Airport	Design for PCC Rehab Main Apron and Key Hangar Area	AIP	2030	\$ 186,659	\$ 9,333	\$ 11,407	\$ 207,399
Camarillo Airport	Design for PCC Rehabilitation Taxiways F and G1	AIP	2030	\$ 186,659	\$ 9,333	\$ 11,407	\$ 207,399
Camarillo Airport	PCC Rehabilitation Main Apron and Key Hangar Area	AIP	2032	\$ 1,244,395	\$ 62,220	\$ 76,046	\$ 1,382,661
Camarillo Airport	PCC Rehabilitation Twys F, G1	AIP	2032	\$ 1,244,395	\$ 62,220	\$ 76,046	\$ 1,382,661

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Camarillo Airport	Design for Rehab Twys G, G2 and G3 and Northeast Txln	AIP	2033	\$ 500,052	\$ 25,003	\$ 30,559	\$ 555,613
Camarillo Airport	Design for Rehabilitate Central Apron (Construction)	AIP	2034	\$ 349,896	\$ 17,495	\$ 21,383	\$ 388,773
Cameron Park Airpark	Airport Layout Plan Update	AIP	2026	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Cameron Park Airpark	Pavement Maintenance Management Plan	AIP	2027	\$ 112,500	\$ 5,625	\$ 6,875	\$ 125,000
Cameron Park Airpark	Crack Seal All Pavements	AIP	2028	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Cameron Park Airpark	ALUCP Update	AIP	2029	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Cameron Park Airpark	Engineering Evaluation of Slope	AIP	2033	\$ 33,750	\$ 1,688	\$ 2,063	\$ 37,500
Cameron Park Airpark	Slurry Seal Apron and Tiedown Areas	AIP	2033	\$ 118,800	\$ 5,940	\$ 7,260	\$ 132,000
Cameron Park Airpark	Underground Tank Removal	AIP	2033	\$ 54,000	\$ 2,700	\$ 3,300	\$ 60,000
Cameron Park Airpark	Engineering of Covered Parking with Solar	AIP	2034	\$ 49,500	\$ 2,475	\$ 3,025	\$ 55,000
Cameron Park Airpark	Hangar Door Replacement	AIP	2034	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Cameron Park Airpark	Slurry Seal Taxiway and Runway	AIP	2034	\$ 183,600	\$ 9,180	\$ 11,220	\$ 204,000
Castle Airport	Install Flush Mounted Edge Lights (Design)	AIP	2025	\$ 99,000	\$ 4,950	\$ 6,050	\$ 110,000
Castle Airport	Install New Beacon (Design)	AIP	2025	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Castle Airport	AWOS III Installation (Design)	AIP	2025	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000
Castle Airport	Tower Equipment (TVS, ATIS, Voice Recorder)	AIP	2025	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Castle Airport	Install Flush Mounted Edge Lights (Construction)	AIP	2026	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Castle Airport	Install New Beacon (Construction)	AIP	2026	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Castle Airport	AWOS III Installation (Construction)	AIP	2026	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Castle Airport	Replace Runway Edge Lights (Design)	AIP	2027	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Castle Airport	Replace Taxiway Edge Lights (Design)	AIP	2027	\$ 117,000	\$ 5,850	\$ 7,150	\$ 130,000
Castle Airport	Guidance Sign Replacement (Design)	AIP	2027	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Castle Airport	ILS and Approach Lighting (Feasibility Study)	AIP	2027	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000
Castle Airport	Guidance Sign Replacement (Construction)	AIP	2028	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Castle Airport	Replace Runway Edge Lights (Construction)	AIP	2029	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Castle Airport	Replace Taxiway Edge Lights (Construction)	AIP	2029	\$ 1,620,000	\$ 81,000	\$ 99,000	\$ 1,800,000
Castle Airport	ILS and Approach Lighting (Design)	AIP	2030	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000
Cedarville Airport	Design - Rehabilitate Runway Lighting	AIP	2026	\$ 25,200	\$ 1,260	\$ 1,540	\$ 28,000
Cedarville Airport	Design - Reseal Joints - Taxiway A, B and Apron	AIP	2026	\$ 9,000	\$ 450	\$ 550	\$ 10,000
Cedarville Airport	Design - Reseal Joints Runway 2-20 (50' x 4,415')	AIP	2026	\$ 32,400	\$ 1,620	\$ 1,980	\$ 36,000
Cedarville Airport	Construct - Rehabilitate Runway Lighting	AIP	2027	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Cedarville Airport	Construct Reseal Joints Runway 2-20 (50' x 4,415')	AIP	2027	\$ 382,500	\$ 19,125	\$ 23,375	\$ 425,000
Cedarville Airport	Construction Reseal Joints - Taxiway A, B and Apron	AIP	2027	\$ 92,700	\$ 4,635	\$ 5,665	\$ 103,000
Cedarville Airport	ALP Update Narrative and Plans	AIP	2032	\$ 2,700	\$ 135	\$ 165	\$ 3,000
Cedarville Airport	ALUCP Update	AIP	2033	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Cedarville Airport	Pavement Management Maintenance Plan	AIP	2034	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Chico Municipal Airport	New AWOS Modular System AWOS 3000 (Equipment)	AIP	2025	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Chico Municipal Airport	Rehabilitate Runway 13R-31L (60' x 3000') Crack Fill and Strip	AIP	2025	\$ 251,285	\$ 12,564	\$ 15,356	\$ 279,205
Chico Municipal Airport	Rehabilitate South Apron Crack Fill, and Restripe	AIP	2025	\$ 205,110	\$ 10,256	\$ 12,535	\$ 227,900
Chico Municipal Airport	Rehabilitate Taxiway R Crack Fill, and Restripe	AIP	2025	\$ 33,390	\$ 1,670	\$ 2,041	\$ 37,100
Chico Municipal Airport	Rehab Airline Cargo Apron - Crack fill, restripe (Des. /Cons.)	AIP	2026	\$ 382,500	\$ 19,125	\$ 23,375	\$ 425,000
Chico Municipal Airport	Rehab Runway 13L-31R (150' x 6,724') (Forecast Study)	AIP	2026	\$ 31,500	\$ 1,575	\$ 1,925	\$ 35,000
Chico Municipal Airport	Terminal Expansion/Modernization - Temporary Modular	AIP	2026	\$ 382,500	\$ 19,125	\$ 23,375	\$ 425,000
Chico Municipal Airport	Terminal Expansion/Modernization (EA)	AIP	2026	\$ 202,500	\$ 10,125	\$ 12,375	\$ 225,000
Chico Municipal Airport	Airport Improvements (ARFF)	AIP	2027	\$ 1,260,000	\$ 63,000	\$ 77,000	\$ 1,400,000
Chico Municipal Airport	Rehab Runway 13L-31R (150' x 6,724') (Design)	AIP	2027	\$ 1,048,500	\$ 52,425	\$ 64,075	\$ 1,165,000
Chico Municipal Airport	Rehab Runway 13L-31R (150' x 6,724') (Reimb. Agreement)	AIP	2027	\$ 112,500	\$ 5,625	\$ 6,875	\$ 125,000
Chico Municipal Airport	Terminal Expansion/Modernization (Design)	AIP	2028	\$ 739,100	\$ 36,955	\$ 45,167	\$ 821,222
Chico Municipal Airport	Reconstruct Aircraft Parking Apron Phase IV and V (Design)	AIP	2029	\$ 787,500	\$ 39,375	\$ 48,125	\$ 875,000
Chico Municipal Airport	Rehabilitate Runway 13L-31R (150' x 6,724') (Cons.)	AIP	2029	\$ 22,485,776	\$ 1,124,289	\$ 1,374,131	\$ 24,984,195
Chico Municipal Airport	Terminal Expansion/modernization (Cons.)	AIP	2029	\$ 4,950,000	\$ 247,500	\$ 302,500	\$ 5,500,000
Chico Municipal Airport	Master Plan and Aeronautical Survey	AIP	2030	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Chico Municipal Airport	Reconstruct Aircraft Parking Apron Phase IV and V (Cons.)	AIP	2030	\$ 9,897,750	\$ 494,888	\$ 604,863	\$ 10,997,500
Chico Municipal Airport	EA - Runway 13R-31L Expansion	AIP	2031	\$ 162,000	\$ 8,100	\$ 9,900	\$ 180,000
Chico Municipal Airport	EA - Terminal and Parking Lot Expansion	AIP	2031	\$ 122,850	\$ 6,143	\$ 7,508	\$ 136,500
Chico Municipal Airport	Rehab Apron A1b, A3b, A4a, and Hangar Taxilanes	AIP	2031	\$ 889,321	\$ 44,466	\$ 54,347	\$ 988,134
Chico Municipal Airport	Replace Rotating Beacon (Design)	AIP	2031	\$ 61,425	\$ 3,071	\$ 3,754	\$ 68,250
Chico Municipal Airport	Car Parking Lot Expansion (Design)	AIP	2032	\$ 140,400	\$ 7,020	\$ 8,580	\$ 156,000
Chico Municipal Airport	Replace Rotating Beacon (Construction)	AIP	2032	\$ 145,436	\$ 7,272	\$ 8,888	\$ 161,595
Chico Municipal Airport	Car Parking Lot Expansion (Construction)	AIP	2033	\$ 1,221,300	\$ 61,065	\$ 74,635	\$ 1,357,000
Chico Municipal Airport	EA - Land Acquisition	AIP	2033	\$ 216,000	\$ 10,800	\$ 13,200	\$ 240,000
Chico Municipal Airport	Reconstruct Runway 13R-31L Extension (75' x 6,000') (Design)	AIP	2033	\$ 711,000	\$ 35,550	\$ 43,450	\$ 790,000
Chico Municipal Airport	West Side Access Road	AIP	2033	\$ 4,136,400	\$ 206,820	\$ 252,780	\$ 4,596,000
Chico Municipal Airport	West Side Utility Development (Power, Water, Sewer)	AIP	2033	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Chico Municipal Airport	EA - Runway 13L-31R Extension	AIP	2034	\$ 162,000	\$ 8,100	\$ 9,900	\$ 180,000
Chico Municipal Airport	Reconstruct Runway 13R-31L Extension (Construction)	AIP	2034	\$ 3,150,000	\$ 157,500	\$ 192,500	\$ 3,500,000
Chino Airport	Reconstruct Southeast Ramp	AIP	2025	\$ 2,691,473	\$ 134,574	\$ 164,479	\$ 2,990,526
Chino Airport	Relocate Taxiway K and L - Design	AIP	2025	\$ 990,000	\$ 49,500	\$ 60,500	\$ 1,100,000
Chino Airport	ALUCP- Airport Land Use Compatibility Plan Update	A&D	2026	\$ -	\$ 315,000	\$ 35,000	\$ 350,000
Chino Airport	Rehabilitate Taxiway J	AIP	2027	\$ 846,000	\$ 42,300	\$ 51,700	\$ 940,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Chino Airport	Relocate Taxiways K and L - Construction	AIP	2027	\$ 7,020,000	\$ 351,000	\$ 429,000	\$ 7,800,000
Chino Airport	Runway 8L-26R Rehabilitation	AIP	2028	\$ 4,500,000	\$ 225,000	\$ 275,000	\$ 5,000,000
Chino Airport	General Aviation Apron Rehabilitation Phase Design	AIP	2029	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Chino Airport	Relocate Taxiway L, Extend Taxiway H - Construction	AIP	2029	\$ 5,220,000	\$ 261,000	\$ 319,000	\$ 5,800,000
Chino Airport	Runway 8R-26L Surface Treatment	AIP	2030	\$ 3,465,000	\$ 173,250	\$ 211,750	\$ 3,850,000
Chino Airport	General Aviation Apron Rehabilitation Phase 1 - Const.	AIP	2031	\$ 3,240,000	\$ 162,000	\$ 198,000	\$ 3,600,000
Chino Airport	General Aviation Apron Rehabilitation	AIP	2031	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Chino Airport	Safety Area Improvements, Removal of Wildlife Attractant	AIP	2031	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Chino Airport	General Aviation Apron Rehabilitation Phase Cons.	AIP	2033	\$ 5,080,500	\$ 254,025	\$ 310,475	\$ 5,645,000
Chino Airport	Extension of Runway 26R and Connector Taxiways - Enviro.	AIP	2034	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Chiriaco Summit Airport	Rehabilitate Runway 6-24	A&D	2027	\$ -	\$ 500,000	\$ 55,556	\$ 555,555
Chiriaco Summit Airport	Rehabilitate Primary Apron	A&D	2030	\$ -	\$ 500,000	\$ 55,556	\$ 555,555
Chiriaco Summit Airport	Rehabilitate Taxiway Pavement	A&D	2033	\$ -	\$ 500,000	\$ 55,556	\$ 555,555
Chiriaco Summit Airport	Rehabilitate Runway 6-24 (Fog Seal)	A&D	2034	\$ -	\$ 270,000	\$ 30,000	\$ 300,000
Chowchilla Airport	Reconstruct Runway 12-30 and Airfield Electrical - Cons.	AIP	2025	\$ 5,908,500	\$ 295,425	\$ 361,075	\$ 6,565,000
Chowchilla Airport	FAA Reimbursable Agreement - PAPI Flight Check	AIP	2025	\$ 121,500	\$ 6,075	\$ 7,425	\$ 135,000
Chowchilla Airport	Install Taxiway Retroreflective Markers - Des. / Cons.	AIP	2026	\$ 89,100	\$ 4,455	\$ 5,445	\$ 99,000
Chowchilla Airport	Pavement Maintenance Management Plan (PMMP)	AIP	2026	\$ 117,000	\$ 5,850	\$ 7,150	\$ 130,000
Chowchilla Airport	Land Acquisition - RPZ - 6.34 acres	AIP	2027	\$ 201,600	\$ 10,080	\$ 12,320	\$ 224,000
Chowchilla Airport	Environmental Assessment - East Side Development	AIP	2028	\$ 216,140	\$ 10,807	\$ 13,209	\$ 240,155
Chowchilla Airport	Land Acquisition - East Side Development	AIP	2030	\$ 630,000	\$ 31,500	\$ 38,500	\$ 700,000
Chowchilla Airport	Design/Construct: Perimeter Fence	AIP	2031	\$ 498,600	\$ 24,930	\$ 30,470	\$ 554,000
Chowchilla Airport	Relocate and Extend Taxiway B, Seg. Circle with Wind cone	AIP	2031	\$ 189,000	\$ 9,450	\$ 11,550	\$ 210,000
Chowchilla Airport	Airport Layout Plan Narrative including ALP Updated Plans	AIP	2032	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Chowchilla Airport	Relocate and Extend Taxiway - Construction	AIP	2032	\$ 1,563,300	\$ 78,165	\$ 95,535	\$ 1,737,000
Chowchilla Airport	Design: Northeast Side Access Road Phase 1	AIP	2033	\$ 144,000	\$ 7,200	\$ 8,800	\$ 160,000
Chowchilla Airport	Design: Northeast Side Hangar Taxilanes - Phase 1	AIP	2033	\$ 306,000	\$ 15,300	\$ 18,700	\$ 340,000
Chowchilla Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Chowchilla Airport	Construct: Northeast Side Access Road Phase 1	AIP	2034	\$ 972,000	\$ 48,600	\$ 59,400	\$ 1,080,000
Chowchilla Airport	Environmental Assessment - East Side Development	AIP	2034	\$ 216,140	\$ 10,807	\$ 13,209	\$ 240,155
Cliff Hatfield Memorial Airport	Airport Runway Maintenance	A&D	2025	\$ -	\$ 175,500	\$ 19,500	\$ 195,000
Cliff Hatfield Memorial Airport	Remediation of Solid Waste	A&D	2026	\$ -	\$ 85,500	\$ 9,500	\$ 95,000
Cliff Hatfield Memorial Airport	Security Fencing Project	A&D	2026	\$ -	\$ 91,800	\$ 10,200	\$ 102,000
Cliff Hatfield Memorial Airport	Taxiway Asphalt Overlay and Striping	A&D	2026	\$ -	\$ 473,922	\$ 52,658	\$ 526,580
Cliff Hatfield Memorial Airport	Airplane tie down project	A&D	2029	\$ -	\$ 166,500	\$ 18,500	\$ 185,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Cliff Hatfield Memorial Airport	Municipal Airport Hanger Relocation	A&D	2030	\$ -	\$ 157,500	\$ 17,500	\$ 175,000
Cliff Hatfield Memorial Airport	Airport Subdivision	A&D	2031	\$ -	\$ 5,490,000	\$ 610,000	\$ 6,100,000
Cloverdale Municipal Airport	Infield Area Drainage Improvements (Environmental Study)	AIP	2025	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Cloverdale Municipal Airport	Infield Area Drainage Improvements (Design)	AIP	2026	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Cloverdale Municipal Airport	PAPI for Rwy 32 (Replacement) and REIL's (Construction)	AIP	2026	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Cloverdale Municipal Airport	In Field Area Drainage Improvements (Construction)	AIP	2027	\$ 427,500	\$ 21,375	\$ 26,125	\$ 475,000
Cloverdale Municipal Airport	Wildlife Hazard Assessment	AIP	2027	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Cloverdale Municipal Airport	Apron Pavement Reconstruction and Lighting (Design)	AIP	2028	\$ 198,000	\$ 9,900	\$ 12,100	\$ 220,000
Cloverdale Municipal Airport	Perimeter Fencing and Card Access Gate (Design)	AIP	2028	\$ 171,000	\$ 8,550	\$ 10,450	\$ 190,000
Cloverdale Municipal Airport	Apron Pavement Reconstruction (Construction)	AIP	2029	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Cloverdale Municipal Airport	Apron Lighting (Construction)	AIP	2030	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Cloverdale Municipal Airport	Perimeter Fencing and Card Access Gate (Construction)	AIP	2031	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Cloverdale Municipal Airport	AWOS (Design)	AIP	2031	\$ 126,000	\$ 6,300	\$ 7,700	\$ 140,000
Cloverdale Municipal Airport	AWOS (Construction)	AIP	2032	\$ 288,000	\$ 14,400	\$ 17,600	\$ 320,000
Cloverdale Municipal Airport	Pave Access within Airport Property (Design)	AIP	2032	\$ 72,000	\$ 3,600	\$ 4,400	\$ 80,000
Cloverdale Municipal Airport	Pave Access within Airport Property (Construction)	AIP	2033	\$ 337,500	\$ 16,875	\$ 20,625	\$ 375,000
Cloverdale Municipal Airport	Segmented Circle and Lighted Wind Cone (Design)	AIP	2034	\$ 81,000	\$ 4,050	\$ 4,950	\$ 90,000
Columbia Airport	ALP Master Plan Update with AGIS	AIP	2025	\$ 495,000	\$ 24,750	\$ 30,250	\$ 550,000
Columbia Airport	AWOS Replacement	AIP	2025	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Columbia Airport	Pavement Management Plan	AIP	2026	\$ 81,000	\$ 4,050	\$ 4,950	\$ 90,000
Columbia Airport	Rwy 17/35 Rehab/Rehab Lighting/VASI to PAPI	AIP	2028	\$ 1,980,000	\$ 99,000	\$ 121,000	\$ 2,200,000
Columbia Airport	Rehab17/35/ Rehab Lighting, VASI to PAPI - Phase I	AIP	2029	\$ 3,431,900	\$ 171,595	\$ 209,727	\$ 3,813,222
Columbia Airport	Rehab Rwy 17-35, Lighting and VASI to PAPI - Phase II	AIP	2030	\$ 3,294,116	\$ 164,706	\$ 201,307	\$ 3,660,129
Columbia Airport	Taxiway A Rehabilitation Design	AIP	2031	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000
Columbia Airport	Taxiway A Rehabilitation	AIP	2032	\$ 4,050,000	\$ 202,500	\$ 247,500	\$ 4,500,000
Columbia Airport	Parking Apron Rehabilitation Construction - Phase 1	AIP	2033	\$ 927,000	\$ 46,350	\$ 56,650	\$ 1,030,000
Columbia Airport	Parking Apron Rehabilitation Construction - Phase 2	AIP	2034	\$ 927,000	\$ 46,350	\$ 56,650	\$ 1,030,000
Colusa County Airport	Pre-Design: Perimeter Fence (300 Ln. Ft.)	AIP	2025	\$ 82,800	\$ 4,140	\$ 5,060	\$ 92,000
Colusa County Airport	Design - AWOS III	AIP	2026	\$ 49,500	\$ 2,475	\$ 3,025	\$ 55,000
Colusa County Airport	Design - Crack Repair and Seal Cracks	AIP	2026	\$ 22,500	\$ 1,125	\$ 1,375	\$ 25,000
Colusa County Airport	Design/Construct: Perimeter Fence (300 Ln. Ft.)	AIP	2026	\$ 164,700	\$ 8,235	\$ 10,065	\$ 183,000
Colusa County Airport	Construct - AWOS III	AIP	2027	\$ 415,800	\$ 20,790	\$ 25,410	\$ 462,000
Colusa County Airport	Construct - Crack Repair and Seal Cracks	AIP	2027	\$ 162,900	\$ 8,145	\$ 9,955	\$ 181,000
Colusa County Airport	Design - Crack Repair, Seal Cracks, and Seal Coat Taxiway A	AIP	2027	\$ 23,400	\$ 1,170	\$ 1,430	\$ 26,000
Colusa County Airport	Construct - Crack Repair, Seal Cracks, and Seal Coat Twy	AIP	2028	\$ 190,800	\$ 9,540	\$ 11,660	\$ 212,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Colusa County Airport	Design - Crack Repair and Seal Cracks - Runway 14-32	AIP	2030	\$ 25,200	\$ 1,260	\$ 1,540	\$ 28,000
Colusa County Airport	Design: Crack Repair and Seal Cracks - Hangar Taxilanes 1-4	AIP	2031	\$ 25,200	\$ 1,260	\$ 1,540	\$ 28,000
Colusa County Airport	Design: Crack Repair and Seal Cracks - Runway 14-32	AIP	2031	\$ 149,400	\$ 7,470	\$ 9,130	\$ 166,000
Colusa County Airport	ALP Update Narrative and Plans	AIP	2032	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Colusa County Airport	Construct Crack Repair and Seal - Hangar Taxilanes 1-4	AIP	2032	\$ 97,200	\$ 4,860	\$ 5,940	\$ 108,000
Colusa County Airport	Design: Recon Access Road	AIP	2033	\$ 54,900	\$ 2,745	\$ 3,355	\$ 61,000
Colusa County Airport	Design: Recon FBO Parking Lot	AIP	2033	\$ 52,200	\$ 2,610	\$ 3,190	\$ 58,000
Colusa County Airport	Design: Reconstruct General Aviation Apron	AIP	2033	\$ 389,700	\$ 19,485	\$ 23,815	\$ 433,000
Colusa County Airport	Pavement Maintenance Management Plan (PMMP)	AIP	2033	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Colusa County Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Colusa County Airport	Construct: Recon General Aviation Apron Phase 1	AIP	2034	\$ 2,789,100	\$ 139,455	\$ 170,445	\$ 3,099,000
Colusa County Airport	Construct: Reconstruct Access Road	AIP	2034	\$ 280,800	\$ 14,040	\$ 17,160	\$ 312,000
Colusa County Airport	Construct: Reconstruct FBO Parking Lot	AIP	2034	\$ 252,900	\$ 12,645	\$ 15,455	\$ 281,000
Compton-Woodley Airport	Recons Taxiway B and Rehab Associated Taxiways - Des.	AIP	2025	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
Compton-Woodley Airport	Terminal Building Project [Construction]	LFP	2025	\$ -	\$ -	\$ 5,400,000	\$ 5,400,000
Compton-Woodley Airport	Recon Taxiway B and Rehab Associated Taxiways [Cons]	AIP	2027	\$ 6,210,000	\$ 310,500	\$ 379,500	\$ 6,900,000
Corning Municipal Airport	Rehab North Apron (Cons.)	AIP	2025	\$ 107,168	\$ 5,358	\$ 6,549	\$ 119,075
Corning Municipal Airport	Rehab Runway 17/35 (Cons.)	AIP	2025	\$ 87,230	\$ 4,361	\$ 5,331	\$ 96,922
Corning Municipal Airport	Rehab Rwy 17/35, North, Apron, Twy A and Associated C	AIP	2025	\$ 40,320	\$ 2,016	\$ 2,464	\$ 44,800
Corning Municipal Airport	Rehab Runway 35 PAPI (Cons.)	AIP	2025	\$ 211,536	\$ 10,577	\$ 12,927	\$ 235,040
Corning Municipal Airport	Rehab Runway 35 PAPI (Design)	AIP	2025	\$ 44,051	\$ 2,203	\$ 2,692	\$ 48,945
Corning Municipal Airport	Rehab Runway 35 PAPI (Flight Check)	AIP	2025	\$ 26,821	\$ 1,341	\$ 1,639	\$ 29,801
Corning Municipal Airport	Rehab Taxiway A and Associated Connector Taxiways	AIP	2025	\$ 54,830	\$ 2,741	\$ 3,351	\$ 60,922
Corning Municipal Airport	RSA Improvements (Design)	AIP	2025	\$ 147,447	\$ 7,372	\$ 9,011	\$ 163,830
Corning Municipal Airport	RSA Improvements (Cons.)	AIP	2026	\$ 855,000	\$ 42,750	\$ 52,250	\$ 950,000
Corning Municipal Airport	Rehab Airfield Lighting System (Design)	AIP	2027	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000
Corning Municipal Airport	Rehab Airfield Lighting System (Bid and Construct)	AIP	2028	\$ 495,000	\$ 24,750	\$ 30,250	\$ 550,000
Corning Municipal Airport	Install Self-Serve 100LL Fuel Station 6000 Gal Option	AIP	2030	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Corning Municipal Airport	Reconstruct South Apron (Design/Construction)	AIP	2031	\$ 893,250	\$ 44,663	\$ 54,588	\$ 992,500
Corning Municipal Airport	Rehab Runway 17/35 (Design/Construction)	AIP	2032	\$ 131,220	\$ 6,561	\$ 8,019	\$ 145,800
Corning Municipal Airport	Install AWOS-III	AIP	2033	\$ 463,500	\$ 23,175	\$ 28,325	\$ 515,000
Corning Municipal Airport	Airport Master Plan Update	AIP	2034	\$ 600,000	\$ 30,000	\$ 36,667	\$ 666,667
Corona Municipal Airport	Construct Public Restroom	AIP	2025	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Corona Municipal Airport	Upgrade City Tie-Down Area for Transient Planes	AIP	2026	\$ 112,500	\$ 5,625	\$ 6,875	\$ 125,000
Corona Municipal Airport	Install New PAPI System	AIP	2027	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Corona Municipal Airport	Airport Master Plan	AIP	2028	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Corona Municipal Airport	Runway/Taxiway Surface Treatment	AIP	2030	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Corona Municipal Airport	Airport Lighting Upgrades to LED	AIP	2031	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Corona Municipal Airport	Obstruction Reduction	AIP	2033	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Corona Municipal Airport	Security Upgrades	AIP	2034	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Delano Municipal Airport	Airport Parking Lot Rehabilitation Project	AIP	2033	\$ 220,000	\$ 11,000	\$ 13,444	\$ 244,444
Delano Municipal Airport	Application of Rejuvenation and Stripe on Runway	AIP	2033	\$ 148,500	\$ 7,425	\$ 9,075	\$ 165,000
Delano Municipal Airport	Resurface Parking Lot	AIP	2033	\$ 149,400	\$ 7,470	\$ 9,130	\$ 166,000
Delano Municipal Airport	Perimeter Access Gates	AIP	2034	\$ 112,500	\$ 5,625	\$ 6,875	\$ 125,000
Delano Municipal Airport	Realign Parallel Taxiway	AIP	2034	\$ 301,500	\$ 15,075	\$ 18,425	\$ 335,000
Dinsmore Airport	Rehab Rwy 9-27 and Improve RSA	AIP	2033	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
Dunsmuir Muni-Mott Airport	North End Taxiway Alpha Alignment Reconstruction (Ph C)	AIP	2025	\$ 1,035,000	\$ 51,750	\$ 63,250	\$ 1,150,000
Dunsmuir Muni-Mott Airport	Southend Taxiway Alpha Alignment Reconstruction (Ph D)	AIP	2026	\$ 423,000	\$ 21,150	\$ 25,850	\$ 470,000
Dunsmuir Muni-Mott Airport	Southend Twy A Alignment Recons. (Phase 2) - Cons.	AIP	2027	\$ 3,807,000	\$ 190,350	\$ 232,650	\$ 4,230,000
Dunsmuir Muni-Mott Airport	Segmented Circle with Windsock	AIP	2028	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Dunsmuir Muni-Mott Airport	Refurbish Beacon	AIP	2029	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Dunsmuir Muni-Mott Airport	Snow Removal Equipment	AIP	2030	\$ 550,000	\$ 27,500	\$ 33,611	\$ 611,111
Dunsmuir Muni-Mott Airport	Design Taxiway Extensions North and South End	AIP	2033	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Dunsmuir Muni-Mott Airport	Expand Apron and Aircraft Parking	AIP	2033	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Dunsmuir Muni-Mott Airport	Construct Additional Taxiways North/South End	AIP	2034	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Fall River Mills Airport	Taxiway A Rehabilitation - Construction	AIP	2025	\$ 434,602	\$ 21,730	\$ 26,559	\$ 482,891
Fall River Mills Airport	South Aircraft Apron Rehabilitation - Construction	AIP	2025	\$ 179,848	\$ 8,992	\$ 10,991	\$ 199,831
Fall River Mills Airport	Nested T-Hangar Taxilanes Rehabilitation - Construction	AIP	2025	\$ 129,134	\$ 6,457	\$ 7,892	\$ 143,483
Fall River Mills Airport	Airfield Pavement Management System	AIP	2028	\$ 99,225	\$ 4,961	\$ 6,064	\$ 110,250
Fall River Mills Airport	Runway 2-20 Improvements - Construction	AIP	2029	\$ 616,187	\$ 30,809	\$ 37,656	\$ 684,653
Fall River Mills Airport	Runway 2-20 Connectors Improvements - Design	AIP	2029	\$ 68,465	\$ 3,423	\$ 4,184	\$ 76,073
Fall River Mills Airport	Runway 2-20 PAPI - Design	AIP	2029	\$ 54,574	\$ 2,729	\$ 3,335	\$ 60,638
Fall River Mills Airport	Runway 2-20 Improvements - Construction	AIP	2031	\$ 4,336,133	\$ 216,807	\$ 264,986	\$ 4,817,925
Fall River Mills Airport	Runway 2-20 Connectors Improvements - Construction	AIP	2031	\$ 481,241	\$ 24,062	\$ 29,409	\$ 534,713
Fall River Mills Airport	Runway 2-20 PAPI - Construction	AIP	2031	\$ 248,063	\$ 12,403	\$ 15,159	\$ 275,625
Fall River Mills Airport	ALP Update with AGIS	AIP	2032	\$ 334,913	\$ 16,746	\$ 20,467	\$ 372,125
Fall River Mills Airport	Taxiway A Improvements - Design	AIP	2033	\$ 356,317	\$ 17,816	\$ 21,775	\$ 395,908
Fall River Mills Airport	Taxiway A Improvements - Construction	AIP	2034	\$ 2,250,000	\$ 112,500	\$ 137,500	\$ 2,500,000
Fallbrook Community Airpark	Rehabilitate Rwy 18/36	AIP	2025	\$ 2,160,000	\$ 108,000	\$ 132,000	\$ 2,400,000
Firebaugh Airport	ALP Narrative Report	AIP	2025	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Firebaugh Airport	Paving Airport Access Road	AIP	2026	\$ 252,000	\$ 12,600	\$ 15,400	\$ 280,000
Firebaugh Airport	Replace Segmented Circle	AIP	2026	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Firebaugh Airport	RSA Obstruction Removal (Design)	AIP	2028	\$ 117,000	\$ 5,850	\$ 7,150	\$ 130,000
Firebaugh Airport	RSA Obstruction Removal (Construction)	AIP	2029	\$ 598,500	\$ 29,925	\$ 36,575	\$ 665,000
Firebaugh Airport	Additional Aircraft Apron and Fuel Island (Design)	AIP	2031	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Firebaugh Airport	Additional Aircraft Apron and Fuel Island (Construction)	AIP	2033	\$ 522,000	\$ 26,100	\$ 31,900	\$ 580,000
Firebaugh Airport	Install Medium Intensity Taxiway Lighting	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Fort Bidwell Airport	Engineering, Design and Add New Gravel for Runway	A&D	2026	\$ -	\$ 72,000	\$ 8,000	\$ 80,000
French Valley Airport	Air Traffic Control Tower, Phase I Design	AIP	2025	\$ 1,440,000	\$ 72,000	\$ 88,000	\$ 1,600,000
French Valley Airport	Apron Pavement Rehab (Middle Apron) Phase II Construct	AIP	2025	\$ 4,275,000	\$ 213,750	\$ 261,250	\$ 4,750,000
French Valley Airport	Fuel Farm Improvements	AIP	2025	\$ 2,160,000	\$ 108,000	\$ 132,000	\$ 2,400,000
French Valley Airport	Air Traffic Control Tower, Phase II Construction	AIP	2026	\$ 14,400,000	\$ 720,000	\$ 880,000	\$ 16,000,000
French Valley Airport	Apron Pavement Rehab (North Apron), Phase III Cons.	AIP	2026	\$ 981,000	\$ 49,050	\$ 59,950	\$ 1,090,000
French Valley Airport	Apron Pavement Rehab (South Apron), Phase IV Design	AIP	2026	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
French Valley Airport	Apron Pavement Rehab (South Apron), Phase V - Cons.	AIP	2027	\$ 378,000	\$ 18,900	\$ 23,100	\$ 420,000
French Valley Airport	Rehabilitate Taxiway A, Phase I Design	AIP	2028	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
French Valley Airport	Rehabilitate Taxiway A, Phase II Construction	LFP	2029	\$ -	\$ -	\$ 8,000,000	\$ 8,000,000
French Valley Airport	Runway 18-36 Rehabilitation	AIP	2031	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
French Valley Airport	North Apron Rehabilitation	AIP	2034	\$ 4,500,000	\$ 225,000	\$ 275,000	\$ 5,000,000
Fresno Chandler Executive Airport	Security Upgrade Ph 2	AIP	2025	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Fresno Chandler Executive Airport	Rehabilitate Runway 12-30 (Planning, Enviro., Design)	AIP	2026	\$ 630,000	\$ 31,500	\$ 38,500	\$ 700,000
Fresno Chandler Executive Airport	Hangar Removal and Replacement	AIP	2027	\$ 4,500,000	\$ 225,000	\$ 275,000	\$ 5,000,000
Fresno Chandler Executive Airport	Rehabilitate Runway 12-30 (Cons.Ph 1)	AIP	2027	\$ 3,150,000	\$ 157,500	\$ 192,500	\$ 3,500,000
Fresno Chandler Executive Airport	Rehabilitate Runway 12-30 (Cons.Ph 2)	AIP	2028	\$ 3,150,000	\$ 157,500	\$ 192,500	\$ 3,500,000
Fresno Chandler Executive Airport	Master Plan Update	AIP	2029	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Fresno Chandler Executive Airport	Pavement Management and Maintenance Plan	AIP	2029	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Fresno Yosemite International Airport	Acquire ARFF Vehicle	AIP	2025	\$ 1,440,000	\$ -	\$ 160,000	\$ 1,600,000
Fresno Yosemite International Airport	East Terminal Expansion (Planning and Design)	AIP	2025	\$ 14,040,000	\$ -	\$ 1,560,000	\$ 15,600,000
Fresno Yosemite International Airport	Part 150 Noise Mitigation Program	AIP	2025	\$ 3,150,000	\$ -	\$ 350,000	\$ 3,500,000
Fresno Yosemite International Airport	Reconstruct Runway 11L-29R PCC (Cons.Ph 1)	AIP	2025	\$ 24,750,000	\$ -	\$ 2,750,000	\$ 27,500,000
Fresno Yosemite International Airport	Reconstruct Runway 11L-29R PCC (Planning and Design)	AIP	2025	\$ 1,800,000	\$ -	\$ 200,000	\$ 2,000,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Fresno Yosemite International Airport	Taxiway B Rehab (Cons.)	AIP	2025	\$ 2,970,000	\$ -	\$ 330,000	\$ 3,300,000
Fresno Yosemite International Airport	Taxiway B Rehab (Planning and Design)	AIP	2025	\$ 1,530,000	\$ -	\$ 170,000	\$ 1,700,000
Fresno Yosemite International Airport	ATCT Replacement (Cons. Ph 1)	AIP	2026	\$ 40,500,000	\$ -	\$ 4,500,000	\$ 45,000,000
Fresno Yosemite International Airport	ATCT Replacement (Planning and Design)	AIP	2026	\$ 5,670,000	\$ -	\$ 630,000	\$ 6,300,000
Fresno Yosemite International Airport	East Terminal Expansion (Cons. Ph 1)	AIP	2026	\$ 13,500,000	\$ -	\$ 1,500,000	\$ 15,000,000
Fresno Yosemite International Airport	Part 150 Noise Mitigation Program	AIP	2026	\$ 3,150,000	\$ -	\$ 350,000	\$ 3,500,000
Fresno Yosemite International Airport	Reconstruct Runway 11L-29R PCC (Cons. Ph 2)	AIP	2026	\$ 24,750,000	\$ -	\$ 2,750,000	\$ 27,500,000
Fresno Yosemite International Airport	East Terminal Expansion (Cons.Ph 2)	AIP	2027	\$ 63,000,000	\$ -	\$ 7,000,000	\$ 70,000,000
Fresno Yosemite International Airport	Part 150 Noise Mitigation Program	AIP	2027	\$ 3,150,000	\$ -	\$ 350,000	\$ 3,500,000
Fresno Yosemite International Airport	Pavement Management Update	AIP	2027	\$ 135,000	\$ -	\$ 15,000	\$ 150,000
Fresno Yosemite International Airport	Reconstruct Runway 11L-29R PCC (Cons. Ph 3)	AIP	2027	\$ 24,750,000	\$ -	\$ 2,750,000	\$ 27,500,000
Fresno Yosemite International Airport	ARFF Station (Planning and Design)	AIP	2028	\$ 1,620,000	\$ -	\$ 180,000	\$ 1,800,000
Fresno Yosemite International Airport	Master Plan Update	AIP	2028	\$ 1,350,000	\$ -	\$ 150,000	\$ 1,500,000
Fresno Yosemite International Airport	Part 150 Noise Mitigation Program	AIP	2028	\$ 3,150,000	\$ -	\$ 350,000	\$ 3,500,000
Fresno Yosemite International Airport	RTR Relocation (Planning and Design)	AIP	2028	\$ 4,500,000	\$ -	\$ 500,000	\$ 5,000,000
Fresno Yosemite International Airport	ARFF Station (Cons.)	AIP	2029	\$ 18,000,000	\$ -	\$ 2,000,000	\$ 20,000,000
Fresno Yosemite International Airport	Part 150 Noise Mitigation Program	AIP	2029	\$ 3,150,000	\$ -	\$ 350,000	\$ 3,500,000
Fresno Yosemite International Airport	Part 150 Noise Mitigation Program	AIP	2030	\$ 3,150,000	\$ -	\$ 350,000	\$ 3,500,000
Fresno Yosemite International Airport	Part 150 Noise Mitigation Program	AIP	2031	\$ 3,150,000	\$ -	\$ 350,000	\$ 3,500,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Fresno Yosemite International Airport	Part 150 Noise Mitigation Program	AIP	2032	\$ 3,150,000	\$ -	\$ 350,000	\$ 3,500,000
Fresno Yosemite International Airport	Part 150 Noise Mitigation Program	AIP	2033	\$ 3,150,000	\$ -	\$ 350,000	\$ 3,500,000
Fullerton Municipal Airport	Rehabilitate South Aircraft Parking Ramp - Design	AIP	2029	\$ 189,000	\$ 9,450	\$ 11,550	\$ 210,000
Fullerton Municipal Airport	Rehabilitate South Ramp	AIP	2030	\$ 1,485,000	\$ 74,250	\$ 90,750	\$ 1,650,000
Gansner Airport	Construct - New Beacon Tower and Beacon	AIP	2025	\$ 268,983	\$ 13,449	\$ 16,438	\$ 298,870
Gansner Airport	Design - Perimeter Fencing - 11,500 In. ft.	AIP	2025	\$ 88,200	\$ 4,410	\$ 5,390	\$ 98,000
Gansner Airport	Design: Snow Removal Equipment Building	AIP	2026	\$ 103,500	\$ 5,175	\$ 6,325	\$ 115,000
Gansner Airport	Construct: Perimeter Fencing	AIP	2027	\$ 907,200	\$ 45,360	\$ 55,440	\$ 1,008,000
Gansner Airport	Construct: Snow Removal Equipment Building	AIP	2027	\$ 695,700	\$ 34,785	\$ 42,515	\$ 773,000
Gansner Airport	Reseal Airfield Pavement Joints in Taxiway A and Apron	AIP	2028	\$ 72,900	\$ 3,645	\$ 4,455	\$ 81,000
Gansner Airport	Reseal Airfield Pavement Joints Twy A and Slurry Seal Apron	AIP	2029	\$ 898,200	\$ 44,910	\$ 54,890	\$ 998,000
Gansner Airport	Design: Crack Repair and Seal Cracks	AIP	2030	\$ 27,000	\$ 1,350	\$ 1,650	\$ 30,000
Gansner Airport	ALP Narrative including Updated ALP Drawings	AIP	2032	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Gansner Airport	ALUCP Update	AIP	2033	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Gansner Airport	Pavement Management Maintenance Program (PMMP)	AIP	2034	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Garberville Airport	RSA Improvements - Replace Culvert (Design and Cons.)	AIP	2025	\$ 207,000	\$ 10,350	\$ 12,650	\$ 230,000
Garberville Airport	Reconstruct South Apron (Design)	AIP	2028	\$ 202,500	\$ 10,125	\$ 12,375	\$ 225,000
Garberville Airport	Reconstruct South Apron (Ph 1) (Cons.)	AIP	2029	\$ 1,575,000	\$ 78,750	\$ 96,250	\$ 1,750,000
Garberville Airport	Reconstruct South Apron (Cons.)	AIP	2030	\$ 1,575,000	\$ 78,750	\$ 96,250	\$ 1,750,000
Garberville Airport	Rehab Rwy 18-36, Taxiways and Apron	AIP	2033	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000
General William J Fox Airport	Reconstruct South Txlns and Aircraft Parking Ramp/Apron	AIP	2025	\$ 495,000	\$ 24,750	\$ 30,250	\$ 550,000
General William J Fox Airport	Rehab South Txlns and Aircraft Parking Ramp/Apron (Des.)	AIP	2027	\$ 423,000	\$ 21,150	\$ 25,850	\$ 470,000
General William J Fox Airport	Recons. South Txlns and Aircraft Parking Ramp/Apron - Cons.	AIP	2028	\$ 9,495,000	\$ 474,750	\$ 580,250	\$ 10,550,000
General William J Fox Airport	Rehab South Txlns F & E and Aircraft Parking Ramp/Apron C	AIP	2029	\$ 6,120,000	\$ 306,000	\$ 374,000	\$ 6,800,000
General William J Fox Airport	Rehabilitate Taxiway A and Associated Taxilanes [Design]	AIP	2029	\$ 117,000	\$ 5,850	\$ 7,150	\$ 130,000
General William J Fox Airport	Rehab Taxiway A and Associated Taxilanes - Cons	AIP	2031	\$ 1,368,000	\$ 68,400	\$ 83,600	\$ 1,520,000
Georgetown Airport	Construct: Replace Airport Beacon and Beacon Tower	AIP	2025	\$ 301,707	\$ 15,085	\$ 18,438	\$ 335,230
Georgetown Airport	Design: Install New AWOS III, AWOS Obstruction Removal	AIP	2028	\$ 65,700	\$ 3,285	\$ 4,015	\$ 73,000
Georgetown Airport	Construct: AWOS III, Wind cone, AWOS Obstruct Removal	AIP	2030	\$ 551,700	\$ 27,585	\$ 33,715	\$ 613,000
Georgetown Airport	Design Crack Seal Pavements	AIP	2032	\$ 54,000	\$ 2,700	\$ 3,300	\$ 60,000
Georgetown Airport	Pavement Maintenance Management Program (PMMP)	AIP	2032	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Georgetown Airport	ALUCP Update	AIP	2033	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Georgetown Airport	Construct Crack Seal Pavements	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Georgetown Airport	Airport Layout Plan Updated Narrative and Plans	AIP	2034	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Gillespie Field Airport	Runway 17/35 Rehabilitation and RSA/ROFA Improvement	AIP	2026	\$ 5,400,000	\$ 270,000	\$ 330,000	\$ 6,000,000
Gillespie Field Airport	Rehabilitate Runway 09R/27L	AIP	2029	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
Gillespie Field Airport	Electrical Vault Relocation and Airfield Sign/Lighting Improv.	AIP	2031	\$ 4,500,000	\$ 225,000	\$ 275,000	\$ 5,000,000
Gillespie Field Airport	Taxiway C Extension - Drainage Improvements (Phase I of III)	AIP	2031	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
Gillespie Field Airport	Rehabilitate Taxiway Delta	AIP	2032	\$ 10,800,000	\$ 540,000	\$ 660,000	\$ 12,000,000
Gillespie Field Airport	Taxiway C Extension - Drainage Improvement (Phase II of III)	AIP	2032	\$ 9,000,000	\$ 450,000	\$ 550,000	\$ 10,000,000
Gillespie Field Airport	Taxiway C Extension (Phase III of III)	AIP	2033	\$ 9,000,000	\$ 450,000	\$ 550,000	\$ 10,000,000
Gillespie Field Airport	Rehabilitate Taxiway A and B	AIP	2034	\$ 5,400,000	\$ 270,000	\$ 330,000	\$ 6,000,000
Gillespie Field Airport	Rehabilitate Taxiway C	AIP	2034	\$ 6,750,000	\$ 337,500	\$ 412,500	\$ 7,500,000
Gnoss Field Airport	APMP	AIP	2025	\$ 72,000	\$ 3,600	\$ 4,400	\$ 80,000
Gnoss Field Airport	Enviro. Mitigation Plan Development and Permit Application	AIP	2025	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Gnoss Field Airport	Runway 13-31 Seal Coat - Design	AIP	2027	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Gnoss Field Airport	Levee Relocation Rwy 13-31 Shift (Design)	AIP	2028	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Gnoss Field Airport	Parallel Taxiway Extension and Shift (Design)	AIP	2028	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Gnoss Field Airport	Runway 13-31 Seal Coat - Construction	AIP	2028	\$ 423,000	\$ 21,150	\$ 25,850	\$ 470,000
Gnoss Field Airport	Runway 13-31 Extension and Shift (design)	AIP	2028	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Gnoss Field Airport	Runway and Taxiway Airfield Lighting Improvements (Design)	AIP	2028	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Gnoss Field Airport	Enviro. Mitigation and Permit Application for Levee Reloc.	AIP	2029	\$ 6,480,000	\$ 324,000	\$ 396,000	\$ 7,200,000
Gnoss Field Airport	Levee Relocation for Runway 13-31 Extension and shift	AIP	2029	\$ 6,300,000	\$ 315,000	\$ 385,000	\$ 7,000,000
Gnoss Field Airport	Parallel Taxiway Extension and Shift (Construction)	AIP	2030	\$ 1,513,800	\$ 75,690	\$ 92,510	\$ 1,682,000
Gnoss Field Airport	Runway 13-31 Extension and Shift (Construction)	AIP	2030	\$ 1,976,130	\$ 98,807	\$ 120,764	\$ 2,195,700
Gnoss Field Airport	Runway and Taxiway Airfield Lighting Improvements	AIP	2030	\$ 1,467,900	\$ 73,395	\$ 89,705	\$ 1,631,000
Gnoss Field Airport	Master Plan Update	AIP	2031	\$ 1,080,000	\$ 54,000	\$ 66,000	\$ 1,200,000
Gustine Airport	Replace Fuel Pump and Card Reader	AIP	2025	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Gustine Airport	Runway Rehabilitation - Design and Construction	AIP	2025	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Gustine Airport	ALP Narrative Report	AIP	2026	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Gustine Airport	Hangar Replacement	AIP	2027	\$ 211,500	\$ 10,575	\$ 12,925	\$ 235,000
Gustine Airport	Runway Lights Replacement	AIP	2029	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Gustine Airport	Extend Parallel Taxiway - Design	AIP	2031	\$ 202,500	\$ 10,125	\$ 12,375	\$ 225,000
Gustine Airport	Extend Parallel Taxiway - Construction	AIP	2033	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Haigh Field Airport	Taxiway C and D Connectors Construction	AIP	2025	\$ 1,610,100	\$ 80,505	\$ 98,395	\$ 1,789,000
Haigh Field Airport	Reconstruct Apron (Design)	AIP	2026	\$ 252,000	\$ 12,600	\$ 15,400	\$ 280,000
Haigh Field Airport	Reconstruct Apron (Construction)	AIP	2028	\$ 1,399,928	\$ 69,996	\$ 85,551	\$ 1,555,475
Half Moon Bay Airport	Crack Seal/Seal Coat Taxiways - Design	AIP	2025	\$ 135,879	\$ 6,794	\$ 8,304	\$ 150,977

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Half Moon Bay Airport	Crack Seal/Seal Coat Taxiways Construction	AIP	2026	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Half Moon Bay Airport	Crack Seal/Seal Coat Transient Ramp - Design	AIP	2027	\$ 40,500	\$ 2,025	\$ 2,475	\$ 45,000
Half Moon Bay Airport	Crack Seal/Seal Coat Taxiways (Ph 2) - Construction	AIP	2027	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Half Moon Bay Airport	Runway Edge Lights and Signage - Design	AIP	2028	\$ 99,000	\$ 4,950	\$ 6,050	\$ 110,000
Half Moon Bay Airport	Runway Edge Lights and Signage - Construction	AIP	2029	\$ 630,000	\$ 31,500	\$ 38,500	\$ 700,000
Half Moon Bay Airport	Crack Seal/Seal Coat Transient Ramp - Construction	AIP	2031	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Half Moon Bay Airport	Taxiway Edge Lights and Signage - Design	AIP	2032	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Half Moon Bay Airport	Taxiway Edge Lights and Signage - Construction	AIP	2033	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Half Moon Bay Airport	Terminal Narrative Report with ALP Drawing Update	AIP	2034	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Hanford Municipal Airport	Construct New Fuel Island and Tanks	AIP	2025	\$ 1,486,800	\$ 74,340	\$ 90,860	\$ 1,652,000
Hanford Municipal Airport	Reconstruct South Transient Ramp - Design Phase I - Des.	AIP	2025	\$ 142,200	\$ 7,110	\$ 8,690	\$ 158,000
Hanford Municipal Airport	Design New Fuel Island and Tanks	AIP	2025	\$ 220,500	\$ 11,025	\$ 13,475	\$ 245,000
Hanford Municipal Airport	Construct Fuel Island and Tanks Reimbursement	AIP	2026	\$ 144,000	\$ 7,200	\$ 8,800	\$ 160,000
Hanford Municipal Airport	Reconstruct South Transient Ramp - Phase 1	AIP	2026	\$ 687,600	\$ 34,380	\$ 42,020	\$ 764,000
Hanford Municipal Airport	Design - Reconstruct South Transient Ramp - Phase 2	AIP	2027	\$ 63,000	\$ 3,150	\$ 3,850	\$ 70,000
Hanford Municipal Airport	Reconstruct South Transient Ramp - Phase 2	AIP	2028	\$ 2,111,400	\$ 105,570	\$ 129,030	\$ 2,346,000
Hanford Municipal Airport	Des. - Relocate Taxiway A North, Remark Runway 14 Aligned	AIP	2029	\$ 129,600	\$ 6,480	\$ 7,920	\$ 144,000
Hanford Municipal Airport	Relocate Twy A North, Remark Rwy 14 Aligned Twy as Blast Pad	AIP	2030	\$ 712,800	\$ 35,640	\$ 43,560	\$ 792,000
Hanford Municipal Airport	ALUCP Update	AIP	2032	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Hanford Municipal Airport	Pavement Maintenance Management Program (PMMP)	AIP	2033	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Hanford Municipal Airport	ALP Update Narrative and Plans	AIP	2034	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Hayfork Airport	Pvmt. Repairs and Maintenance - Crack Sealing/Patching	AIP	2026	\$ 9,000	\$ 450	\$ 550	\$ 10,000
Hayward Executive Airport	Design Reimbursement and Agreement of Twy A1 and Z	AIP	2025	\$ 151,059	\$ 7,553	\$ 9,231	\$ 167,843
Hayward Executive Airport	Taxiway A1 and Taxiway Z Reconstruction Project	AIP	2025	\$ 3,134,610	\$ 156,731	\$ 191,560	\$ 3,482,900
Hayward Executive Airport	Taxiway Z (Add Alternative Reconstruction Project)	AIP	2025	\$ 2,381,490	\$ 119,075	\$ 145,536	\$ 2,646,100
Hayward Executive Airport	Air Traffic Control Tower Relocation (Des. and Cons.)	AIP	2026	\$ 9,000,000	\$ 450,000	\$ 550,000	\$ 10,000,000
Hayward Executive Airport	ATCT Relocation Sighting Study and Enviro.	AIP	2026	\$ 1,102,500	\$ 55,125	\$ 67,375	\$ 1,225,000
Hayward Executive Airport	Taxiway Z (Add. Alternative Reconstruction Project)	AIP	2026	\$ 2,381,490	\$ 119,075	\$ 145,536	\$ 2,646,100
Hayward Executive Airport	Taxiway Z Reconstruction	AIP	2028	\$ 5,208,085	\$ 260,404	\$ 318,272	\$ 5,786,761
Hayward Executive Airport	Taxiway A West Rehab Design	AIP	2029	\$ 531,000	\$ 26,550	\$ 32,450	\$ 590,000
Hayward Executive Airport	Sulphur Creek Safety Area Improvements - Construction	AIP	2030	\$ 6,750,000	\$ 337,500	\$ 412,500	\$ 7,500,000
Hayward Executive Airport	Airfield Electrical Design	AIP	2031	\$ 756,000	\$ 37,800	\$ 46,200	\$ 840,000
Hayward Executive Airport	Runway Safety Area Improvements Design	AIP	2031	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Hayward Executive Airport	Airfield Lighting Upgrade Construction	AIP	2032	\$ 8,244,000	\$ 412,200	\$ 503,800	\$ 9,160,000
Hayward Executive Airport	Runway Safety Area EA and Conceptual Design 10%	AIP	2032	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Hayward Executive Airport	Runway Safety Area Improvements- Construction	AIP	2034	\$ 12,487,500	\$ 624,375	\$ 763,125	\$ 13,875,000
Healdsburg Municipal Airport	Rehabilitate Taxiway A (North), A1, and Associate - Cons.	AIP	2025	\$ 509,400	\$ 25,470	\$ 31,130	\$ 566,000
Healdsburg Municipal Airport	Design - Runway 13-31 Pavement Reconstruction	AIP	2025	\$ 174,600	\$ 8,730	\$ 10,670	\$ 194,000
Healdsburg Municipal Airport	Rehabilitate Hangar Row C (West) - Des.	AIP	2025	\$ 45,900	\$ 2,295	\$ 2,805	\$ 51,000
Healdsburg Municipal Airport	Rehab Twy A (North) and A1 (Remove and Replace) - Des.	AIP	2025	\$ 56,700	\$ 2,835	\$ 3,465	\$ 63,000
Healdsburg Municipal Airport	Rehabilitate Hangar Row C (West) - Cons.	AIP	2026	\$ 281,700	\$ 14,085	\$ 17,215	\$ 313,000
Healdsburg Municipal Airport	Rwy 13-31 Pavement Recons. Ans Remove Twy A3 - Cons.	AIP	2027	\$ 2,557,800	\$ 127,890	\$ 156,310	\$ 2,842,000
Healdsburg Municipal Airport	Twy Reconfiguration (Cons. of Twy A2) and Rehab Twy Lighting	AIP	2028	\$ 637,200	\$ 31,860	\$ 38,940	\$ 708,000
Healdsburg Municipal Airport	Design - Reconstruct North Apron	AIP	2029	\$ 90,900	\$ 4,545	\$ 5,555	\$ 101,000
Healdsburg Municipal Airport	Construct - Reconstruct North Apron Reconstruction	AIP	2030	\$ 982,800	\$ 49,140	\$ 60,060	\$ 1,092,000
Healdsburg Municipal Airport	ALP Narrative with Updated Airport Layout Plan	AIP	2032	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Healdsburg Municipal Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Hemet-Ryan Airport	Fuel Farm Improvements	AIP	2025	\$ 2,160,000	\$ 108,000	\$ 132,000	\$ 2,400,000
Hemet-Ryan Airport	Rehabilitate Runway 5-23, Phase I Design	AIP	2026	\$ 720,000	\$ 36,000	\$ 44,000	\$ 800,000
Hemet-Ryan Airport	Rwy 5-23 and Twy A,B,C, and D Electrical/Signage Phase I Des.	AIP	2026	\$ 720,000	\$ 36,000	\$ 44,000	\$ 800,000
Hemet-Ryan Airport	Land Acquisition for RPZ	AIP	2027	\$ 1,258,276	\$ 62,914	\$ 76,895	\$ 1,398,084
Hemet-Ryan Airport	Rehabilitate Runway 5-23, Phase II Construction	AIP	2027	\$ 3,917,212	\$ 195,861	\$ 239,385	\$ 4,352,458
Hemet-Ryan Airport	Rehabilitate Taxiway A, Phase I Design	AIP	2027	\$ 495,000	\$ 24,750	\$ 30,250	\$ 550,000
Hemet-Ryan Airport	Rwy 5-23 and Twy A, B, C, D, Electrical/Signage Ph II Cons.	AIP	2027	\$ 4,500,000	\$ 225,000	\$ 275,000	\$ 5,000,000
Hemet-Ryan Airport	Rehabilitate Primary Apron, Phase III Construction	AIP	2028	\$ 5,085,000	\$ 254,250	\$ 310,750	\$ 5,650,000
Hemet-Ryan Airport	Rehabilitate Taxiway A, Phase II Construction	AIP	2028	\$ 3,780,000	\$ 189,000	\$ 231,000	\$ 4,200,000
Hemet-Ryan Airport	Rehabilitate Taxiways C and D Phase I Design	AIP	2029	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Hemet-Ryan Airport	Rwy 5-23 Rehab	AIP	2031	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Hemet-Ryan Airport	Rehabilitate Taxiway A	AIP	2033	\$ 1,575,000	\$ 78,750	\$ 96,250	\$ 1,750,000
Hemet-Ryan Airport	Rehabilitate Primary Apron	AIP	2034	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
Herlong Airport	Airport Land Use Compatibility Plan-Update County Wide	A&D	2025	\$ -	\$ 247,500	\$ 27,500	\$ 275,000
Herlong Airport	Pavement Maintenance and Remarking; Rwy and Twy	A&D	2025	\$ -	\$ 148,500	\$ 16,500	\$ 165,000
Hollister Municipal Airport	Airport Operation Area Perimeter Fence Relocation	AIP	2025	\$ 132,688	\$ 6,634	\$ 8,109	\$ 147,431
Hollister Municipal Airport	Remove In-Line Taxiway Proceeding Rwy Design	AIP	2025	\$ 247,050	\$ 12,353	\$ 15,098	\$ 274,500
Hollister Municipal Airport	Taxiway A Reconstruction- Design	AIP	2025	\$ 752,293	\$ 37,615	\$ 45,973	\$ 835,881
Hollister Municipal Airport	Airport Layout Plan Update	AIP	2026	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000
Hollister Municipal Airport	Convert In-Line Taxiway To Runway Design	AIP	2026	\$ 85,410	\$ 4,271	\$ 5,220	\$ 94,900
Hollister Municipal Airport	Purchase of 2 EV Truck and Charger	AIP	2026	\$ 148,973	\$ 7,449	\$ 9,104	\$ 165,526
Hollister Municipal Airport	Taxiway A Reconstruction - Construction Phase 1	AIP	2026	\$ 8,748,000	\$ 437,400	\$ 534,600	\$ 9,720,000
Hollister Municipal Airport	APMS	AIP	2027	\$ 151,200	\$ 7,560	\$ 9,240	\$ 168,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Hollister Municipal Airport	Convert In-Line Taxiway To Runway- Construction	AIP	2027	\$ 269,550	\$ 13,478	\$ 16,473	\$ 299,500
Hollister Municipal Airport	Taxiway A Reconstruction- Construction Phase 2	AIP	2027	\$ 5,940,000	\$ 297,000	\$ 363,000	\$ 6,600,000
Hollister Municipal Airport	Replace Rwy 13/31 PAPI's	AIP	2028	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Hollister Municipal Airport	Taxilanes-Keenan Complex - Designs	AIP	2029	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000
Hollister Municipal Airport	Taxilanes - Keenan Complex - Construction	AIP	2030	\$ 3,690,000	\$ 184,500	\$ 225,500	\$ 4,100,000
Hollister Municipal Airport	Reconstruct Southwest Hangar Taxilanes - Design	AIP	2031	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Hollister Municipal Airport	Reconstruct Southwest Hangar Taxilanes- Construction	AIP	2032	\$ 5,184,000	\$ 259,200	\$ 316,800	\$ 5,760,000
Hollister Municipal Airport	Rehabilitate Taxiway C- Design	AIP	2033	\$ 810,000	\$ 40,500	\$ 49,500	\$ 900,000
Hollister Municipal Airport	Rehabilitate Taxiway C - Construction	AIP	2034	\$ 10,620,000	\$ 531,000	\$ 649,000	\$ 11,800,000
Hyampom Airport	Pavement Repairs and Maintenance - Crack Sealing/Patching	A&D	2027	\$ -	\$ 9,000	\$ 1,000	\$ 10,000
Imperial County Airport	Runway 14-32 and Connector Taxiways Rehabilitation (Des.)	AIP	2025	\$ 297,000	\$ -	\$ 33,000	\$ 330,000
Imperial County Airport	Runway 14-32 and Connector Twys Rehab (Cons.)	AIP	2026	\$ 3,060,000	\$ -	\$ 340,000	\$ 3,400,000
Imperial County Airport	Taxiway A Rehabilitation (Design)	AIP	2026	\$ 90,000	\$ -	\$ 10,000	\$ 100,000
Imperial County Airport	Terminal Parking Lot Reconstruction (Construction)	AIP	2026	\$ 1,521,000	\$ -	\$ 169,000	\$ 1,690,000
Imperial County Airport	Taxiway A Rehabilitation (Construction)	AIP	2027	\$ 450,000	\$ -	\$ 50,000	\$ 500,000
Imperial County Airport	Runway 8/26 Rehabilitation (Design)	AIP	2027	\$ 180,000	\$ -	\$ 20,000	\$ 200,000
Imperial County Airport	Runway 8/26 Rehabilitation (Construction)	AIP	2028	\$ 2,520,000	\$ -	\$ 280,000	\$ 2,800,000
Imperial County Airport	Airport Pavement Management Study (APMS)	AIP	2029	\$ 270,000	\$ -	\$ 30,000	\$ 300,000
Independence Airport	Runway 14-32 Rehabilitation	AIP	2025	\$ 1,665,000	\$ 83,250	\$ 101,750	\$ 1,850,000
Independence Airport	Terminal Area Fencing and Access Gate (Design/Constr)	A&D	2028	\$ -	\$ 70,200	\$ 7,800	\$ 78,000
Independence Airport	Airport Land Use Compatibility Plan	A&D	2029	\$ -	\$ 67,500	\$ 7,500	\$ 75,000
Inyokern Airport	Airfield Pavement Marking Upgrades	AIP	2025	\$ 162,000	\$ 8,100	\$ 9,900	\$ 180,000
Inyokern Airport	Fire Protection Improvements	AIP	2025	\$ 514,422	\$ 25,721	\$ 31,437	\$ 571,580
Inyokern Airport	Fire Protection Improvements	AIP	2025	\$ 145,446	\$ 7,272	\$ 8,888	\$ 161,607
Inyokern Airport	Purchase crack fill material	AIP	2025	\$ 47,790	\$ 2,390	\$ 2,921	\$ 53,100
Inyokern Airport	Fire Protection Improvements (Construction Reimb.) - BIL	LFP	2026	\$ -	\$ -	\$ 151,600	\$ 151,600
Inyokern Airport	Reconfigure and Rehab Taxiway A (Ph. 1)	AIP	2027	\$ 2,500,002	\$ 125,000	\$ 152,778	\$ 2,777,780
Inyokern Airport	Reconstruct Runway 15-33	AIP	2029	\$ 16,650,000	\$ 832,500	\$ 1,017,500	\$ 18,500,000
Inyokern Airport	Reconfigure and Rehabilitate Taxiway A (Ph. 2)	AIP	2031	\$ 3,546,000	\$ 177,300	\$ 216,700	\$ 3,940,000
Jack McNamara Field Airport	Beach Front Fence Replacement	AIP	2025	\$ 180,000	\$ -	\$ 20,000	\$ 200,000
Jack McNamara Field Airport	Runway 12/30 Rehab and Lighting Upgrade - Phase 1 (Des.)	AIP	2026	\$ 585,000	\$ -	\$ 65,000	\$ 650,000
Jack McNamara Field Airport	Rwy 12-30 Rehab and Lighting Upgrade (Phase 2)	AIP	2027	\$ 7,200,000	\$ -	\$ 800,000	\$ 8,000,000
Jack McNamara Field Airport	Reloc. and Replacement of Primary Wind Direction Indicator	AIP	2028	\$ 58,500	\$ -	\$ 6,500	\$ 65,000
Jack McNamara Field Airport	Replace Rotating Beacon	AIP	2028	\$ 85,500	\$ -	\$ 9,500	\$ 95,000
Jack McNamara Field Airport	Twy A and B Redesign and Rehabilitation (Phase Design)	AIP	2028	\$ 288,000	\$ -	\$ 32,000	\$ 320,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Jack McNamara Field Airport	ALP Master Plan	AIP	2029	\$ 450,000	\$ -	\$ 50,000	\$ 500,000
Jack McNamara Field Airport	Twy A and B Redesign and Rehabilitation	AIP	2029	\$ 2,250,000	\$ -	\$ 250,000	\$ 2,500,000
Jack Northrop Field-Hawthorne Municipal Airport	Runway and Connector Taxiways Slurry Seal	AIP	2025	\$ 1,020,960	\$ 51,048	\$ 62,392	\$ 1,134,400
Jack Northrop Field-Hawthorne Municipal Airport	Slurry Seal of Taxiway S and Taxiway N	AIP	2025	\$ 895,140	\$ 44,757	\$ 54,703	\$ 994,600
Jack Northrop Field-Hawthorne Municipal Airport	Replacement / Upgrade Rwy Lighting and Obstruction lights	AIP	2027	\$ 2,480,940	\$ 124,047	\$ 151,613	\$ 2,756,600
Jack Northrop Field-Hawthorne Municipal Airport	Twy Lighting and Signage and Beacon Replacement	AIP	2027	\$ 2,158,200	\$ 107,910	\$ 131,890	\$ 2,398,000
Jack Northrop Field-Hawthorne Municipal Airport	Runway Seal Coat	AIP	2029	\$ 1,020,960	\$ 51,048	\$ 62,392	\$ 1,134,400
Jack Northrop Field-Hawthorne Municipal Airport	Taxiway Seal Coat and Blast Fence Replacement	AIP	2031	\$ 532,980	\$ 26,649	\$ 32,571	\$ 592,200
Jack Northrop Field-Hawthorne Municipal Airport	Remove and Replace Existing Chain Link Perimeter Fence	AIP	2033	\$ 1,170,000	\$ 58,500	\$ 71,500	\$ 1,300,000
Jacqueline Cochran Regional Airport	Taxiway A (Relocation), Phase I Design	AIP	2025	\$ 833,000	\$ 41,650	\$ 50,906	\$ 925,556
Jacqueline Cochran Regional Airport	Air Traffic Control Tower, Phase I Design	AIP	2026	\$ 2,160,000	\$ 108,000	\$ 132,000	\$ 2,400,000
Jacqueline Cochran Regional Airport	Air Traffic Control Tower, Phase II Construction	AIP	2026	\$ 18,000,000	\$ 900,000	\$ 1,100,000	\$ 20,000,000
Jacqueline Cochran Regional Airport	New PCC Apron Expansion: Phase I Design	AIP	2026	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
Jacqueline Cochran Regional Airport	Taxiway A (Relocation), Phase II Construction	AIP	2027	\$ 6,390,000	\$ 319,500	\$ 390,500	\$ 7,100,000
Jacqueline Cochran Regional Airport	New PCC Apron Expansion: Phase II Construction	AIP	2028	\$ 4,140,000	\$ 207,000	\$ 253,000	\$ 4,600,000
Jacqueline Cochran Regional Airport	Taxiway F Electrical and Lighting Rehab - Phase I Design	AIP	2029	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Jacqueline Cochran Regional Airport	Acquire Land for Approaches (approx. 9.5 acres)	AIP	2030	\$ 324,000	\$ 16,200	\$ 19,800	\$ 360,000
Jacqueline Cochran Regional Airport	Taxiway F Electrical/Lighting Rehabilitation - Construction	AIP	2030	\$ 1,099,800	\$ 54,990	\$ 67,210	\$ 1,222,000
Jacqueline Cochran Regional Airport	Taxiway A Rehabilitation	AIP	2031	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
Jacqueline Cochran Regional Airport	Rwy 17-35 Rehabilitation	AIP	2033	\$ 11,700,000	\$ 585,000	\$ 715,000	\$ 13,000,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Jacqueline Cochran Regional Airport	Twy F Rehabilitation	AIP	2034	\$ 10,800,000	\$ 540,000	\$ 660,000	\$ 12,000,000
John Wayne Airport, Orange Co.	Airfield Pavement Marking Improvements	LFP	2025	\$ -	\$ -	\$ 3,050,000	\$ 3,050,000
John Wayne Airport, Orange Co.	Airport Power Generation and Distribution Upgrades - Phase I	AIP	2025	\$ 47,635,418	\$ -	\$ 11,908,854	\$ 59,544,272
John Wayne Airport, Orange Co.	Comm. Ramp Ground Service Equipment EV Upgrades - Ph II	LFP	2025	\$ -	\$ -	\$ 3,265,560	\$ 3,265,560
John Wayne Airport, Orange Co.	Zero Emission Vehicles	AIP	2025	\$ 4,798,906	\$ -	\$ 1,155,811	\$ 5,954,717
John Wayne Airport, Orange Co.	ARFF Stormwater and Materials Storage Improvements	AIP	2026	\$ 1,635,977	\$ -	\$ 394,023	\$ 2,030,000
John Wayne Airport, Orange Co.	Common Use Passenger Processing System (CUPPS)	LFP	2026	\$ -	\$ -	\$ 10,000,000	\$ 10,000,000
John Wayne Airport, Orange Co.	Facility Accessibility Improvements - Phase II	LFP	2026	\$ -	\$ -	\$ 2,489,000	\$ 2,489,000
John Wayne Airport, Orange Co.	Facility Security Improvements	LFP	2026	\$ -	\$ -	\$ 33,513,709	\$ 33,513,709
John Wayne Airport, Orange Co.	Main Street Parking Lot Improv. EV Charging Implementation	LFP	2026	\$ -	\$ -	\$ 3,835,000	\$ 3,835,000
John Wayne Airport, Orange Co.	Perimeter Fence Security Enhancement - Phase III	LFP	2026	\$ -	\$ -	\$ 10,347,000	\$ 10,347,000
John Wayne Airport, Orange Co.	Airfield Runway and Infield Rehab	AIP	2027	\$ 26,782,878	\$ -	\$ 6,450,622	\$ 33,233,500
John Wayne Airport, Orange Co.	Airfield Stormwater Improvement - Phase I	AIP	2027	\$ 13,253,026	\$ -	\$ 5,566,974	\$ 18,820,000
John Wayne Airport, Orange Co.	Vertical Conveyance Systems Improvements - Phase II	LFP	2027	\$ -	\$ -	\$ 17,142,640	\$ 17,142,640
John Wayne Airport, Orange Co.	Airfield Stormwater Improvements - Phase II	AIP	2028	\$ 6,288,438	\$ -	\$ 2,889,562	\$ 9,178,000
John Wayne Airport, Orange Co.	Airport Power Generation and Distribution Upgrades - Phase II	AIP	2028	\$ 12,686,176	\$ -	\$ 57,606,234	\$ 70,292,410
John Wayne Airport, Orange Co.	Terminal A and B Baggage System Improvements	LFP	2028	\$ -	\$ -	\$ 113,072,220	\$ 113,072,220
John Wayne Airport, Orange Co.	Terminal Apron Improvements	AIP	2028	\$ 26,757,491	\$ -	\$ 10,764,509	\$ 37,522,000
John Wayne Airport, Orange Co.	Vertical Conveyance Systems Improvements - Phase IV	LFP	2029	\$ -	\$ -	\$ 26,497,440	\$ 26,497,440
Kern Valley Airport	Crack Fill, Seal Coat and Restripe Transient Parking Ramp	AIP	2025	\$ 216,000	\$ 10,800	\$ 13,200	\$ 240,000
Kern Valley Airport	Slurry Seal, Crack Fill and Restripe Runway	AIP	2025	\$ 324,000	\$ 16,200	\$ 19,800	\$ 360,000
Kern Valley Airport	Runway 17-35 Widening (Design)	AIP	2026	\$ 420,631	\$ 21,032	\$ 25,705	\$ 467,368
Kern Valley Airport	Taxiway A Realignment and RSA Widening (Design)	AIP	2026	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Kern Valley Airport	Taxiway A Realignment and RSA Widening (Construction)	AIP	2028	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
Kern Valley Airport	Runway 17-35 Widening (Construction)	AIP	2029	\$ 2,250,000	\$ 112,500	\$ 137,500	\$ 2,500,000
Lake Tahoe Airport	Rehabilitate Runway 18/36 (Crack Seal/Seal Coat/Paint)	AIP	2025	\$ 355,263	\$ 17,763	\$ 21,711	\$ 394,737
Lake Tahoe Airport	Acquire SRE (Truck with Plow)	AIP	2026	\$ 300,000	\$ 15,000	\$ 18,333	\$ 333,333
Lake Tahoe Airport	Cons. New Connector Twys A-2, A-3 and H (Design)	A&D	2027	\$ -	\$ 225,000	\$ 25,000	\$ 250,000
Lake Tahoe Airport	Reconstruct Connector Taxiways A-1 and A4 - A6	A&D	2027	\$ -	\$ 297,000	\$ 33,000	\$ 330,000
Lake Tahoe Airport	Rehabilitate Parallel Taxiway A and Electrical	A&D	2027	\$ -	\$ 468,000	\$ 52,000	\$ 520,000
Lake Tahoe Airport	Construct New Connector Twys A-2, A-3 and H	A&D	2028	\$ -	\$ 2,700,000	\$ 300,000	\$ 3,000,000
Lake Tahoe Airport	Reconstruct Twys A-1, A-4, and Electrical Infrastructure	A&D	2028	\$ -	\$ 3,600,000	\$ 400,000	\$ 4,000,000
Lake Tahoe Airport	Rehabilitate Parallel Taxiway A and Electrical Infrastructure	AIP	2028	\$ 9,180,000	\$ 459,000	\$ 561,000	\$ 10,200,000
Lake Tahoe Airport	Rehabilitate Runway 18/36 and Electrical Infrastructure	AIP	2029	\$ 765,000	\$ 38,250	\$ 46,750	\$ 850,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Lake Tahoe Airport	Rehabilitate Runway 18-36 (Construction)	AIP	2030	\$ 13,320,000	\$ 666,000	\$ 814,000	\$ 14,800,000
Lake Tahoe Airport	Improve Airport Drainage (Engineering, Design, Construction)	AIP	2032	\$ 1,150,000	\$ 57,500	\$ 70,278	\$ 1,277,778
Lake Tahoe Airport	Install Weather Reporting Equipment (AWOS III)	AIP	2033	\$ 150,000	\$ 200,000	\$ 38,889	\$ 388,889
Lampson Field Airport	Install Security Fencing (Wildlife Hazard Assessment)	AIP	2025	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Lampson Field Airport	Phase 2 Apron (Supplemental Design)(West)	AIP	2025	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Lampson Field Airport	Phase 1 Apron (Cons.)(East)	AIP	2026	\$ 1,125,000	\$ 56,250	\$ 68,750	\$ 1,250,000
Lampson Field Airport	Phase 2 Apron (Cons.)(West)	AIP	2029	\$ 1,620,000	\$ 81,000	\$ 99,000	\$ 1,800,000
Lampson Field Airport	Install Security Fencing (Design)	AIP	2031	\$ 150,300	\$ 7,515	\$ 9,185	\$ 167,000
Lampson Field Airport	Install Security Fencing (Cons.)	AIP	2032	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Lampson Field Airport	Runway and Taxiway Rehabilitation (Design)	AIP	2033	\$ 300,000	\$ 15,000	\$ 18,333	\$ 333,333
Lincoln Regional Airport - Karl Harder Field	Rwy 15-33 Reconstruction including Runway Lighting - Des.	AIP	2025	\$ 363,600	\$ 18,180	\$ 22,220	\$ 404,000
Lincoln Regional Airport - Karl Harder Field	Crack Seal/Seal Coat Aircraft Parking Apron - Des.	AIP	2025	\$ 74,700	\$ 3,735	\$ 4,565	\$ 83,000
Lincoln Regional Airport - Karl Harder Field	Runway 15-33 FAA Service Road Realignment - Des.	AIP	2025	\$ 21,600	\$ 1,080	\$ 1,320	\$ 24,000
Lincoln Regional Airport - Karl Harder Field	Rwy 15-33 RSA, Including RSA/OFA Grading, ROFA - Des.	AIP	2025	\$ 42,300	\$ 2,115	\$ 2,585	\$ 47,000
Lincoln Regional Airport - Karl Harder Field	Reimbursable Agreement MALSR Light Bar Adjustments	AIP	2025	\$ 76,500	\$ 3,825	\$ 4,675	\$ 85,000
Lincoln Regional Airport - Karl Harder Field	Construct - Crack Seal Aircraft Parking Apron	AIP	2026	\$ 274,500	\$ 13,725	\$ 16,775	\$ 305,000
Lincoln Regional Airport - Karl Harder Field	Construct Runway 15-33 FAA Service Road Realignment	AIP	2028	\$ 114,300	\$ 5,715	\$ 6,985	\$ 127,000
Lincoln Regional Airport - Karl Harder Field	Construct Runway 15-33 Recons. Including Runway Light	AIP	2028	\$ 9,716,400	\$ 485,820	\$ 593,780	\$ 10,796,000
Lincoln Regional Airport - Karl Harder Field	Construct Runway 15-33 Recons. Including Runway Light	AIP	2028	\$ 286,200	\$ 14,310	\$ 17,490	\$ 318,000
Lincoln Regional Airport - Karl Harder Field	Construct Rwy 15-33 RSA, Including RSA/OFA Grading, ROFA	AIP	2028	\$ 340,200	\$ 17,010	\$ 20,790	\$ 378,000
Lincoln Regional Airport - Karl Harder Field	Reimbursable Agreement, MALSR Light Bar Adjustments	AIP	2028	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Lincoln Regional Airport - Karl Harder Field	Design - Recons. Twys A and K ; Replace Twy Lights	AIP	2030	\$ 79,200	\$ 3,960	\$ 4,840	\$ 88,000
Lincoln Regional Airport - Karl Harder Field	Design: Recon Twys (Twy K) Phase 1	AIP	2030	\$ 69,300	\$ 3,465	\$ 4,235	\$ 77,000
Lincoln Regional Airport - Karl Harder Field	Design: Recon Twys Airfield Lighting Twy K Phase 1	AIP	2030	\$ 19,800	\$ 990	\$ 1,210	\$ 22,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Lincoln Regional Airport - Karl Harder Field	Recons. Taxiways and Airfield Lighting Taxiway A	AIP	2030	\$ 29,700	\$ 1,485	\$ 1,815	\$ 33,000
Lincoln Regional Airport - Karl Harder Field	Construct: Recons. Twy K Phase 2	AIP	2031	\$ 701,760	\$ 35,088	\$ 42,885	\$ 779,733
Lincoln Regional Airport - Karl Harder Field	Construct: Recons. Twys Twy A Phase 2	AIP	2031	\$ 1,033,440	\$ 51,672	\$ 63,155	\$ 1,148,267
Lincoln Regional Airport - Karl Harder Field	Construct: Recons. Twys Airfield Lighting Twy A Phase 2	AIP	2031	\$ 131,040	\$ 6,552	\$ 8,008	\$ 145,600
Lincoln Regional Airport - Karl Harder Field	Construct: Recons. Twys Airfield Lighting Twy K	AIP	2031	\$ 87,360	\$ 4,368	\$ 5,339	\$ 97,067
Lincoln Regional Airport - Karl Harder Field	Reimbursable Agreement - Coordination with FAA Facilities	AIP	2031	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Lincoln Regional Airport - Karl Harder Field	ALUCP Update	AIP	2032	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Lincoln Regional Airport - Karl Harder Field	ALP Update Narrative and Plans	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Lincoln Regional Airport - Karl Harder Field	PMMP Update	AIP	2034	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Little River Airport	Aircraft Parking Apron Pavement Reconstruction - Cons.	AIP	2025	\$ 2,184,300	\$ 109,215	\$ 133,485	\$ 2,427,000
Little River Airport	Twy A Pavement Reconstruction - Design and Rebid	AIP	2026	\$ 107,052	\$ 5,353	\$ 6,542	\$ 118,947
Little River Airport	Taxiway A Pavement Reconstruction - Bid and Construction	AIP	2027	\$ 4,140,000	\$ 207,000	\$ 253,000	\$ 4,600,000
Little River Airport	Runway and Runway Shoulder Rehab - Design	AIP	2029	\$ 371,430	\$ 18,572	\$ 22,699	\$ 412,700
Little River Airport	Runway and Runway Shoulder Rehab - Construction	AIP	2030	\$ 4,101,121	\$ 205,056	\$ 250,624	\$ 4,556,801
Livermore Municipal Airport	Northside Pavement Rehabilitation (Design)	AIP	2025	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Livermore Municipal Airport	Master Plan ALP Narrative Update with AGIS	AIP	2025	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Livermore Municipal Airport	Airport Geometry Improvements (Hot Spots 1-Cons.	AIP	2026	\$ 9,900,000	\$ 495,000	\$ 605,000	\$ 11,000,000
Livermore Municipal Airport	Northside Pavement Rehabilitation (Construct Phase 1)	AIP	2027	\$ 3,600,000	\$ 180,000	\$ 220,000	\$ 4,000,000
Livermore Municipal Airport	Northside Pavement Rehabilitation (Cons. Phase 2)	AIP	2028	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
Livermore Municipal Airport	Southside Pavement Rehabilitation (Design)	AIP	2029	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Livermore Municipal Airport	Airport Sustainability and Facilities Management Plan	AIP	2029	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Livermore Municipal Airport	Southside Pavement Rehabilitation (Construction)	AIP	2030	\$ 3,600,000	\$ 180,000	\$ 220,000	\$ 4,000,000
Livermore Municipal Airport	Perimeter Fencing and Security Upgrades Design	AIP	2030	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Livermore Municipal Airport	Perimeter Fencing and Security Upgrades Construction	AIP	2031	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Lompoc Airport	Rehabilitate City Hangars	AIP	2025	\$ 594,360	\$ 29,718	\$ 36,322	\$ 660,400
Lone Pine Airport	Airport Lighting and Signs (Phase 2 Construct)	AIP	2026	\$ 440,000	\$ 22,000	\$ 26,889	\$ 488,889
Lone Pine Airport	Taxiway B Rehabilitation	AIP	2026	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Lone Pine Airport	Airport Apron/Hanger Areas Pavement Rehab	AIP	2028	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Lone Pine Airport	Airport Land Use Compatibility Plan	A&D	2029	\$ -	\$ 67,500	\$ 7,500	\$ 75,000
Lone Pine Airport	Airfield Pavement Sealcoat and Markings	AIP	2034	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Long Beach Airport - Daugherty Field	AGS Phase 5 -Taxiway F Realignment and Reconstruction	AIP	2026	\$ 23,263,695	\$ -	\$ 2,584,855	\$ 25,848,550
Long Beach Airport - Daugherty Field	Safety and Security Checkpoint Improvements	AIP	2026	\$ 4,680,000	\$ -	\$ 520,000	\$ 5,200,000
Long Beach Airport - Daugherty Field	Apron High Mast Light Replacement	AIP	2027	\$ 5,850,000	\$ -	\$ 650,000	\$ 6,500,000
Long Beach Airport - Daugherty Field	Apron Reconstruction (Pad 1 and Taxiway K Rehab)	AIP	2028	\$ 21,471,538	\$ -	\$ 2,385,726	\$ 23,857,264
Long Beach Airport - Daugherty Field	Runway 12-30 Rehabilitation	AIP	2031	\$ 33,032,025	\$ -	\$ 3,670,225	\$ 36,702,250
Long Beach Airport - Daugherty Field	Taxiway D (Future Taxiway M) Reconstruction (South of Twy F)	AIP	2034	\$ 20,052,630	\$ -	\$ 2,228,070	\$ 22,280,700
Lonnie Pool Field-Weaverville	PAPI Installation	AIP	2025	\$ 31,500	\$ 1,575	\$ 1,925	\$ 35,000
Lonnie Pool Field-Weaverville	Pavement Repairs and Maintenance - Crack Sealing/Patching	AIP	2026	\$ 9,000	\$ 450	\$ 550	\$ 10,000
Lonnie Pool Field-Weaverville	Pavement Repairs and Maintenance - Crack Sealing/Patching	AIP	2029	\$ 9,000	\$ 450	\$ 550	\$ 10,000
Los Angeles International Airport	LAX Terminal Roadways Program (Progressive Design-Build)	LFP	2025	\$ -	\$ -	\$ 43,750,000	\$ 43,750,000
Los Angeles International Airport	Residential Sound Insulation Program Year 6	LFP	2025	\$ -	\$ -	\$ 28,500,000	\$ 28,500,000
Los Angeles International Airport	Taxiway C Extension	AIP	2025	\$ 53,106,300	\$ -	\$ 5,900,700	\$ 59,007,000
Los Angeles International Airport	VALE Charging Infrastructure - eGSE Chargers and PCA Units	AIP	2025	\$ 7,816,500	\$ -	\$ 868,500	\$ 8,685,000
Los Angeles International Airport	Zero Emissions Vehicle - Phase 2 Procurement	LFP	2025	\$ -	\$ -	\$ 13,255,200	\$ 13,255,200
Los Angeles International Airport	ARFF Station	LFP	2026	\$ -	\$ -	\$ 100,000,000	\$ 100,000,000
Los Angeles International Airport	Terminal Roadways Program (Progressive Design-Build)	LFP	2026	\$ -	\$ -	\$ 738,490,000	\$ 738,490,000
Los Angeles International Airport	Reconstruct Taxiway A and Taxiway F (Phase 1)	AIP	2026	\$ 61,650,000	\$ -	\$ 6,850,000	\$ 68,500,000
Los Angeles International Airport	Rehabilitate Parking Structure	LFP	2026	\$ -	\$ -	\$ 70,000,000	\$ 70,000,000
Los Angeles International Airport	RSI Program Year 7	LFP	2026	\$ -	\$ -	\$ 21,504,000	\$ 21,504,000
Los Angeles International Airport	Terminal HVAC Modernization	LFP	2026	\$ -	\$ -	\$ 150,000,000	\$ 150,000,000
Los Angeles International Airport	Terminal Roadways Program (Progressive Design-Build)	LFP	2027	\$ -	\$ -	\$ 1,073,960,000	\$ 1,073,960,000
Los Angeles International Airport	Reconstruct Taxiway E and Taxiway Z	AIP	2027	\$ 22,770,000	\$ -	\$ 2,530,000	\$ 25,300,000
Los Angeles International Airport	Reconstruct Taxiway A and Taxiway F (Phase 2)	AIP	2028	\$ 65,970,000	\$ -	\$ 7,330,000	\$ 73,300,000
Los Angeles International Airport	Reconstruct Taxiway Y	AIP	2029	\$ 18,180,000	\$ -	\$ 2,020,000	\$ 20,200,000
Los Angeles International Airport	Reconstruct Runway 6L-24R	AIP	2030	\$ 204,570,000	\$ -	\$ 22,730,000	\$ 227,300,000
Los Banos Municipal Airport	Central and South Apron, Runway 14-32, and Taxiway Overlay	AIP	2025	\$ 198,000	\$ 9,900	\$ 12,100	\$ 220,000
Los Banos Municipal Airport	Runway 12-32 Overlay - Construction	AIP	2026	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Los Banos Municipal Airport	South Apron Pavement Rehab and Marking - Cons.	AIP	2026	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Los Banos Municipal Airport	Central Apron Rehab and Marking - Construction	AIP	2027	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
Los Banos Municipal Airport	Parallel Taxiway Overlay - Construction	AIP	2027	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Los Banos Municipal Airport	Internal Access Road Extension - Design and Construction	AIP	2028	\$ 567,000	\$ 28,350	\$ 34,650	\$ 630,000
Los Banos Municipal Airport	New Hangar Taxiways - Design and Construction	AIP	2029	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Los Banos Municipal Airport	T-Hangars (12 Units) - Design and Construction	AIP	2030	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
Los Banos Municipal Airport	Sealing and Marking Runways - Design	AIP	2031	\$ 72,000	\$ 3,600	\$ 4,400	\$ 80,000
Los Banos Municipal Airport	Sealing and Marking Runways - Construction	AIP	2032	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Los Banos Municipal Airport	Parallel Taxiway Slurry and Marking - Design	AIP	2033	\$ 54,000	\$ 2,700	\$ 3,300	\$ 60,000
Los Banos Municipal Airport	Parallel Taxiway Slurry and Marking - Construction	AIP	2034	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Madera Municipal Airport	ALP Update Narrative and Plans	AIP	2025	\$ 327,060	\$ 16,353	\$ 19,987	\$ 363,400
Madera Municipal Airport	Design/Construct: Rehabilitate Apron A2 and Taxilane	AIP	2025	\$ 1,490,420	\$ 74,521	\$ 91,081	\$ 1,656,022
Madera Municipal Airport	F-150 Lightning Acquisition	AIP	2026	\$ 85,500	\$ 4,275	\$ 5,225	\$ 95,000
Madera Municipal Airport	Reimbursement: Rehabilitate Apron A2 and Taxilane	AIP	2026	\$ 278,527	\$ 13,926	\$ 17,021	\$ 309,474
Madera Municipal Airport	Design - Reconstruct Runway 12-30 (75' x 4,550') and Rehab	AIP	2027	\$ 126,000	\$ 6,300	\$ 7,700	\$ 140,000
Madera Municipal Airport	Reconstruct Rwy 12-30 and Rehab Rwy Lighting	AIP	2028	\$ 5,114,700	\$ 255,735	\$ 312,565	\$ 5,683,000
Madera Municipal Airport	Reimb. Agreement for Flight Check - PAPI, REIL, MALSF	AIP	2028	\$ 27,000	\$ 1,350	\$ 1,650	\$ 30,000
Madera Municipal Airport	Taxiways P and B Rehabilitation - Design	AIP	2029	\$ 62,550	\$ 3,128	\$ 3,823	\$ 69,500
Madera Municipal Airport	Const. - Taxiways P (Phase 1, and B Rehabilitation	AIP	2030	\$ 669,600	\$ 33,480	\$ 40,920	\$ 744,000
Madera Municipal Airport	Design: Rehabilitate Taxiways P (Phase 2), C (North) and D	AIP	2031	\$ 62,550	\$ 3,128	\$ 3,823	\$ 69,500
Madera Municipal Airport	Rehabilitate Taxiways P, C (North) and D - Construction	AIP	2032	\$ 669,600	\$ 33,480	\$ 40,920	\$ 744,000
Madera Municipal Airport	Pavement Maintenance Management Program Update	AIP	2032	\$ 99,000	\$ 4,950	\$ 6,050	\$ 110,000
Madera Municipal Airport	Design/Construct: Tee Hangar Development	AIP	2033	\$ 963,900	\$ 48,195	\$ 58,905	\$ 1,071,000
Madera Municipal Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Madera Municipal Airport	Extend Commercial Hangar Development Area Phase III - Des.	AIP	2034	\$ 215,100	\$ 10,755	\$ 13,145	\$ 239,000
Mammoth Yosemite Airport	Construct Airfield Access Road (24' x 330')	AIP	2025	\$ 1,305,000	\$ -	\$ 145,000	\$ 1,450,000
Mammoth Yosemite Airport	Design/Construct: Relocate Twy A	AIP	2025	\$ 876,600	\$ -	\$ 97,400	\$ 974,000
Mammoth Yosemite Airport	Reconstruct Tee Hangar Taxilanes - Des.	AIP	2025	\$ 206,100	\$ -	\$ 22,900	\$ 229,000
Mammoth Yosemite Airport	Multipurpose Building - Phase II - Building - Cons.	AIP	2026	\$ 5,996,723	\$ -	\$ 666,303	\$ 6,663,025
Mammoth Yosemite Airport	Reconstruct Tee Hangar Taxilanes	AIP	2026	\$ 2,503,774	\$ -	\$ 278,197	\$ 2,781,971
Mammoth Yosemite Airport	Equipment Acquisition - Replace ARFF Vehicle	AIP	2026	\$ 1,188,000	\$ -	\$ 132,000	\$ 1,320,000
Mammoth Yosemite Airport	Construction: Wildlife Security Fence (23,000 Ft.)	AIP	2027	\$ 4,395,762	\$ -	\$ 488,418	\$ 4,884,180
Mammoth Yosemite Airport	MAGVAR Change Rwy Numbers to 10-28 and Signage	AIP	2027	\$ 210,600	\$ -	\$ 23,400	\$ 234,000
Mammoth Yosemite Airport	Construct 15-ft. Twy A Shoulder	AIP	2028	\$ 5,274,000	\$ -	\$ 586,000	\$ 5,860,000
Mammoth Yosemite Airport	Design/Construct: Helicopter Parking Apron	AIP	2028	\$ 7,051,623	\$ -	\$ 783,514	\$ 7,835,137
Mammoth Yosemite Airport	Design/Construct General Aviation Apron North Expansion	AIP	2029	\$ 4,857,399	\$ -	\$ 539,711	\$ 5,397,110

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Mammoth Yosemite Airport	Design/Construct - Replace Doe Ridge Obstruction Lights	AIP	2029	\$ 222,300	\$ -	\$ 24,700	\$ 247,000
Mammoth Yosemite Airport	Design/Construct - Runway RSA and OFA Grading	AIP	2029	\$ 4,992,538	\$ -	\$ 554,726	\$ 5,547,264
Mammoth Yosemite Airport	Design/Construct: Helicopter Parking Apron	AIP	2030	\$ 2,521,094	\$ -	\$ 280,122	\$ 2,801,215
Mammoth Yosemite Airport	ALP Narrative Including ALP Updated Plans and Study	AIP	2033	\$ 270,000	\$ -	\$ 30,000	\$ 300,000
Mammoth Yosemite Airport	Pavement Maintenance Management Plan	AIP	2033	\$ 108,000	\$ -	\$ 12,000	\$ 120,000
Mammoth Yosemite Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ -	\$ 25,000	\$ 250,000
March ARB and Inland Port Airport	Design and Construct AP-5 Crack Seal Project	AIP	2025	\$ 206,100	\$ 10,305	\$ 12,595	\$ 229,000
March ARB and Inland Port Airport	Construct Apron Reconstruction (Phases 14 and 15 from PMP)	AIP	2025	\$ 5,820,300	\$ 291,015	\$ 355,685	\$ 6,467,000
March ARB and Inland Port Airport	Construct Apron Reconstruction (Phase 12 from PMP)	AIP	2026	\$ 2,655,000	\$ 132,750	\$ 162,250	\$ 2,950,000
March ARB and Inland Port Airport	Construct Apron Reconstruction (Phase 13 from PMP)	AIP	2027	\$ 2,655,000	\$ 132,750	\$ 162,250	\$ 2,950,000
March ARB and Inland Port Airport	Design Apron Reconstruction (Phases 1-2, 6-11 from PMP)	AIP	2028	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
March ARB and Inland Port Airport	Construct Apron Reconstruction (Phases 1 and 2 from PMP)	AIP	2029	\$ 5,850,000	\$ 292,500	\$ 357,500	\$ 6,500,000
March ARB and Inland Port Airport	Construct Apron Reconstruction (Phases 6-8 from PMP)	AIP	2030	\$ 7,020,000	\$ 351,000	\$ 429,000	\$ 7,800,000
Marina Municipal Airport	Acquire Vacuum Sweeper - BIL	AIP	2025	\$ 150,030	\$ 7,502	\$ 9,169	\$ 166,700
Marina Municipal Airport	Rehab Taxilane (Former Twy A) (Cons.)	AIP	2025	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
Marina Municipal Airport	Airfield Electrical Upgrades - BIL	AIP	2026	\$ 486,000	\$ 24,300	\$ 29,700	\$ 540,000
Marina Municipal Airport	Master Plan Update	AIP	2026	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Marina Municipal Airport	PMMP	AIP	2027	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Marina Municipal Airport	Hangar Complex Taxilanes (Design)	AIP	2030	\$ 171,000	\$ 8,550	\$ 10,450	\$ 190,000
Marina Municipal Airport	Hangar Complex Taxilanes (Cons.)	AIP	2031	\$ 1,890,000	\$ 94,500	\$ 115,500	\$ 2,100,000
Marina Municipal Airport	Construct Helicopter Parking	AIP	2032	\$ 3,150,000	\$ 157,500	\$ 192,500	\$ 3,500,000
Marina Municipal Airport	Perimeter Fence Upgrades (Cons. and Reimb. Design)	AIP	2033	\$ 324,000	\$ 16,200	\$ 19,800	\$ 360,000
Marina Municipal Airport	Rehabilitate Runway 11-29 (design)	AIP	2034	\$ 333,000	\$ 16,650	\$ 20,350	\$ 370,000
Mariposa - Yosemite Airport	Pavement Maintenance Management Plan Geotechnical	AIP	2025	\$ 151,200	\$ 7,560	\$ 9,240	\$ 168,000
Mariposa - Yosemite Airport	Pavement Maintenance Management Plan	AIP	2025	\$ 144,000	\$ 7,200	\$ 8,800	\$ 160,000
Mariposa - Yosemite Airport	Rehabilitate Runway 8-26 (design)	AIP	2026	\$ 234,000	\$ 11,700	\$ 14,300	\$ 260,000
Mariposa - Yosemite Airport	Remove and Replace AWOS (Cons. and Reimburse Des.)	AIP	2027	\$ 297,000	\$ 14,850	\$ 18,150	\$ 330,000
Mariposa - Yosemite Airport	Rehabilitate Runway 8-26 (Cons.)	AIP	2028	\$ 3,510,000	\$ 175,500	\$ 214,500	\$ 3,900,000
Mariposa - Yosemite Airport	Rehabilitate Parallel and Connecting Taxiways (Design)	AIP	2030	\$ 171,000	\$ 8,550	\$ 10,450	\$ 190,000
Mariposa - Yosemite Airport	Rehabilitate Parallel and Connecting Taxiways (Cons.)	AIP	2031	\$ 2,790,000	\$ 139,500	\$ 170,500	\$ 3,100,000
Mariposa - Yosemite Airport	Perimeter Fence Upgrades (Design)	AIP	2032	\$ 171,000	\$ 8,550	\$ 10,450	\$ 190,000
Mariposa - Yosemite Airport	Perimeter Fence Upgrades (Cons.)	AIP	2033	\$ 882,000	\$ 44,100	\$ 53,900	\$ 980,000
Mariposa - Yosemite Airport	Apron and Helipad Rehabilitation (Design)	AIP	2034	\$ 207,000	\$ 10,350	\$ 12,650	\$ 230,000
McClellan-Palomar Airport	EMAS to Improve Rwy 24	AIP	2027	\$ 22,500,000	\$ 1,125,000	\$ 1,375,000	\$ 25,000,000
McClellan-Palomar Airport	Rehabilitate Taxiway Alpha	AIP	2028	\$ 9,900,000	\$ 495,000	\$ 605,000	\$ 11,000,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
McClellan-Palomar Airport	Aircraft Rescue and Fire Fighting Building (Phase I of II)	AIP	2029	\$ 1,890,000	\$ 94,500	\$ 115,500	\$ 2,100,000
McClellan-Palomar Airport	Aircraft Rescue and Fire Fighting Building (Phase II of II)	AIP	2031	\$ 9,000,000	\$ 450,000	\$ 550,000	\$ 10,000,000
McClellan-Palomar Airport	Rehabilitate Runway 06/24	AIP	2032	\$ 6,300,000	\$ 315,000	\$ 385,000	\$ 7,000,000
McClellan-Palomar Airport	Install Runway and Taxiway Lighting and Signage	AIP	2034	\$ 3,600,000	\$ 180,000	\$ 220,000	\$ 4,000,000
Meadows Field Airport	Extend Terminal Long-Term Parking North	LFP	2025	\$ -	\$ -	\$ 1,400,000	\$ 1,400,000
Meadows Field Airport	Preliminary Analysis and Enviro. Document for Rwy 12R-30L	AIP	2025	\$ 380,000	\$ -	\$ 20,000	\$ 400,000
Meadows Field Airport	Terminal Parking Lot Security Fencing and CCTV	AIP	2025	\$ 657,000	\$ -	\$ 73,000	\$ 730,000
Meadows Field Airport	Acquire ARFF Vehicle	AIP	2026	\$ 1,170,000	\$ -	\$ 130,000	\$ 1,300,000
Meadows Field Airport	Existing Inbound Baggage System (Des. and Cons.)	AIP	2026	\$ 806,723	\$ -	\$ 89,636	\$ 896,359
Meadows Field Airport	Runway 12R-30L Rehab and Twy B Widening (Design)	AIP	2026	\$ 900,000	\$ -	\$ 100,000	\$ 1,000,000
Meadows Field Airport	Rwy 12R-30L Rehabilitation and Twy. B Widening	AIP	2028	\$ 11,520,000	\$ -	\$ 1,280,000	\$ 12,800,000
Meadows Field Airport	Upgrade Airport Operation Area Perimeter Security Fence	AIP	2029	\$ 2,700,000	\$ -	\$ 300,000	\$ 3,000,000
Mefford Field Airport	Airport Pavement Management System (APMS) Update	AIP	2025	\$ 128,835	\$ 6,442	\$ 7,873	\$ 143,150
Mefford Field Airport	Design Runway 13-31 Electrical Upgrade	AIP	2025	\$ 119,700	\$ 5,985	\$ 7,315	\$ 133,000
Mefford Field Airport	Rehab Twy, Twy geometry modifications (cons.)	AIP	2025	\$ 4,410,000	\$ 220,500	\$ 269,500	\$ 4,900,000
Mefford Field Airport	Construct Runway 13-31 Electrical Upgrade	AIP	2026	\$ 1,620,000	\$ 81,000	\$ 99,000	\$ 1,800,000
Mefford Field Airport	Construct two Helipads and Access Gate Improv. (Cons.)	AIP	2026	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Mefford Field Airport	Design Runway 13-31 Reconstruction	AIP	2027	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000
Mefford Field Airport	Rehab South End Taxilane and Area Paving (Design)	AIP	2028	\$ 189,000	\$ 9,450	\$ 11,550	\$ 210,000
Mefford Field Airport	Reconstruct Runway 13-31	AIP	2029	\$ 10,800,000	\$ 540,000	\$ 660,000	\$ 12,000,000
Mefford Field Airport	Rehab South End Taxilane and Area Paving (Cons.)	AIP	2029	\$ 1,665,000	\$ 83,250	\$ 101,750	\$ 1,850,000
Mefford Field Airport	Rehabilitate Runway 13-31 (design)	AIP	2031	\$ 387,000	\$ 19,350	\$ 23,650	\$ 430,000
Mefford Field Airport	Rehabilitate Runway 13-31 (Cons.)	AIP	2032	\$ 5,130,000	\$ 256,500	\$ 313,500	\$ 5,700,000
Mesa Del Rey Airport	PAPI and REIL Runway 11 Electrical Improvements (Cons.)	AIP	2025	\$ 845,773	\$ 42,289	\$ 51,686	\$ 939,748
Mesa Del Rey Airport	Airport Pavement Management Plan	AIP	2026	\$ 100,800	\$ 5,040	\$ 6,160	\$ 112,000
Mesa Del Rey Airport	Rehabilitation of Runway (Design)	AIP	2027	\$ 252,000	\$ 12,600	\$ 15,400	\$ 280,000
Mesa Del Rey Airport	Rehabilitation of Runway (Construction)	AIP	2028	\$ 1,890,000	\$ 94,500	\$ 115,500	\$ 2,100,000
Mesa Del Rey Airport	Rehabilitation of Taxiway B and Apron (Design)	AIP	2029	\$ 252,000	\$ 12,600	\$ 15,400	\$ 280,000
Mesa Del Rey Airport	Rehabilitation of Taxiway B and Apron (Construction)	AIP	2030	\$ 895,500	\$ 44,775	\$ 54,725	\$ 995,000
Mesa Del Rey Airport	ALUCP Update	A&D	2031	\$ -	\$ 138,600	\$ 15,400	\$ 154,000
Mesa Del Rey Airport	AWOS (Design)	AIP	2031	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Mesa Del Rey Airport	AWOS (Construction)	AIP	2032	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Mesa Del Rey Airport	Wildlife Hazard Environmental Assessment	AIP	2033	\$ 117,000	\$ 5,850	\$ 7,150	\$ 130,000
Mesa Del Rey Airport	Airport Service Road; Acquire Clear Zone	A&D	2034	\$ -	\$ 135,000	\$ 15,000	\$ 150,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Modesto City - County Airport - Harry Sham Field	ALCMS and CCR Upgrades Construction	AIP	2025	\$ 476,100	\$ 23,805	\$ 29,095	\$ 529,000
Modesto City - County Airport - Harry Sham Field	Equipment-Sweeper	A&D	2025	\$ -	\$ 225,000	\$ 25,000	\$ 250,000
Modesto City - County Airport - Harry Sham Field	Design and Rehabilitate Taxiway C	AIP	2026	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Modesto City - County Airport - Harry Sham Field	Reconstruct Taxiway C	AIP	2027	\$ 1,485,000	\$ 74,250	\$ 90,750	\$ 1,650,000
Modesto City - County Airport - Harry Sham Field	Design and Rehabilitate Taxiway E	AIP	2028	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Modesto City - County Airport - Harry Sham Field	Reconstruction Taxiway E	AIP	2029	\$ 1,395,000	\$ 69,750	\$ 85,250	\$ 1,550,000
Modesto City - County Airport - Harry Sham Field	Narrow Taxiway A ,A1, A2, A4, A5 ,A6	AIP	2030	\$ 1,080,000	\$ 54,000	\$ 66,000	\$ 1,200,000
Modesto City - County Airport - Harry Sham Field	Design Taxiway B4 and Taxiway D Rehabilitation	AIP	2031	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Modesto City - County Airport - Harry Sham Field	Construct Taxiway B4 and Rehabilitate Taxiway D	AIP	2032	\$ 990,000	\$ 49,500	\$ 60,500	\$ 1,100,000
Modesto City - County Airport - Harry Sham Field	Narrow Runway 10L/28R from 150' to 100'	AIP	2033	\$ 1,620,000	\$ 81,000	\$ 99,000	\$ 1,800,000
Modesto City - County Airport - Harry Sham Field	Replace VASI Runway 10L with 4-Light PAPI	AIP	2034	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Mojave Air And Space Port	Rehabilitate Taxiway A, Phase 1	AIP	2025	\$ 3,150,000	\$ 157,500	\$ 192,500	\$ 3,500,000
Mojave Air And Space Port	Perform APMS Survey	AIP	2026	\$ 162,000	\$ 8,100	\$ 9,900	\$ 180,000
Mojave Air And Space Port	Rehabilitate Taxiway A, Phase 2	AIP	2028	\$ 6,300,000	\$ 315,000	\$ 385,000	\$ 7,000,000
Mojave Air And Space Port	Rehab Rwy 12-30 Pavement, Electrical, and Surface Improv.	AIP	2030	\$ 8,794,800	\$ 439,740	\$ 537,460	\$ 9,772,000
Mojave Air And Space Port	Rehabilitate Taxiway A, Phase 2 and Remove Taxiway D	AIP	2031	\$ 3,690,000	\$ 184,500	\$ 225,500	\$ 4,100,000
Mojave Air And Space Port	Runway 12-30 Lighting Improvements	AIP	2032	\$ 4,050,000	\$ 202,500	\$ 247,500	\$ 4,500,000
Montague-Yreka Airport - Rohrer Field	Resurface Taxiways and Ramps	A&D	2025	\$ -	\$ 499,500	\$ 55,500	\$ 555,000
Montague-Yreka Airport - Rohrer Field	Automated Weather Observing System (AWOS) (New)	A&D	2027	\$ -	\$ 72,000	\$ 8,000	\$ 80,000
Montague-Yreka Airport - Rohrer Field	Rohrer Field Entrance Road Rehabilitation Project	A&D	2029	\$ -	\$ 225,000	\$ 25,000	\$ 250,000
Montague-Yreka Airport - Rohrer Field	PAPI Lights Conversion to LED Lights	A&D	2030	\$ -	\$ 16,200	\$ 1,800	\$ 18,000
Montague-Yreka Airport - Rohrer Field	New Helipad	A&D	2032	\$ -	\$ 90,000	\$ 10,000	\$ 100,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Montague-Yreka Airport - Rohrer Field	Rohrer Field Parking Apron Resurface Project	A&D	2034	\$ -	\$ 630,242	\$ 70,027	\$ 700,269
Monterey Regional Airport	ATP Passenger Boarding Bridges	AIP	2026	\$ 6,750,000	\$ -	\$ 750,000	\$ 7,500,000
Monterey Regional Airport	Terminal Relocation Cable FAA Reimbursable	AIP	2026	\$ 256,500	\$ -	\$ 28,500	\$ 285,000
Monterey Regional Airport	Taxiway A Shift Construction	AIP	2026	\$ 5,058,778	\$ -	\$ 562,086	\$ 5,620,864
Monterey Regional Airport	Terminal Building Construction	AIP	2026	\$ 79,519,229	\$ -	\$ 8,835,470	\$ 88,354,699
Monterey Regional Airport	Taxiway A Relocation Phase E-1 Storm water Improvement	AIP	2027	\$ 2,250,000	\$ -	\$ 250,000	\$ 2,500,000
Monterey Regional Airport	Runway Safety Area Rehab Phase 2	AIP	2028	\$ 2,250,000	\$ -	\$ 250,000	\$ 2,500,000
Monterey Regional Airport	Runway Safety Area Infield Rehab Phase 3	AIP	2029	\$ 2,250,000	\$ -	\$ 250,000	\$ 2,500,000
Monterey Regional Airport	Extend Taxiway B and K/L, Reconfigure for GA Rwy 10L-28R	AIP	2030	\$ 2,460,150	\$ -	\$ 273,350	\$ 2,733,500
Monterey Regional Airport	ARFF Airfield Service Road	AIP	2033	\$ 630,000	\$ -	\$ 70,000	\$ 700,000
Monterey Regional Airport	ARFF Construction	AIP	2033	\$ 4,590,000	\$ -	\$ 510,000	\$ 5,100,000
Monterey Regional Airport	Terminal Building Design	AIP	2033	\$ 4,140,000	\$ -	\$ 460,000	\$ 4,600,000
Monterey Regional Airport	Commercial Apron Construction Phase 2	AIP	2034	\$ 19,710,000	\$ -	\$ 2,190,000	\$ 21,900,000
Monterey Regional Airport	Demolition of ARFF Southside	AIP	2034	\$ 720,000	\$ -	\$ 80,000	\$ 800,000
Montgomery-Gibbs Executive Airport	Design of Twy B, F, H, D, C Rehab and Twy E Demolition	AIP	2025	\$ 2,216,618	\$ 110,831	\$ 135,460	\$ 2,462,909
Montgomery-Gibbs Executive Airport	Rwy 10R/28L Design	AIP	2025	\$ 802,084	\$ 40,104	\$ 49,016	\$ 891,204
Montgomery-Gibbs Executive Airport	Terminal Apron Rehab Design	AIP	2026	\$ 840,914	\$ 42,046	\$ 51,389	\$ 934,349
Montgomery-Gibbs Executive Airport	Cons. of Twy B, F, H, D, C Rehab and Twy E Demolition	AIP	2027	\$ 2,582,811	\$ 129,141	\$ 157,838	\$ 2,869,790
Montgomery-Gibbs Executive Airport	Rwy 10R/28L Rehabilitation	AIP	2027	\$ 1,871,528	\$ 93,576	\$ 114,371	\$ 2,079,476
Montgomery-Gibbs Executive Airport	PMMP Study/Survey	AIP	2028	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Montgomery-Gibbs Executive Airport	Terminal Apron Rehab Reconstruction	AIP	2028	\$ 1,962,133	\$ 98,107	\$ 119,908	\$ 2,180,148
Montgomery-Gibbs Executive Airport	Airfield Signage and Markings Plan	AIP	2029	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Montgomery-Gibbs Executive Airport	Twys H/A/J Rehab/Rwys 28L and 28R Runup	AIP	2029	\$ 2,582,811	\$ 129,141	\$ 157,838	\$ 2,869,790
Montgomery-Gibbs Executive Airport	Twys H/A/J Rehab/Rwys 28L and 28R Runup Construction	AIP	2031	\$ 6,026,558	\$ 301,328	\$ 368,290	\$ 6,696,176
Murray Field Airport	Install Runway 12-30 Vertical Guidance System PAPI-(Cons.)	AIP	2025	\$ 531,000	\$ 26,550	\$ 32,450	\$ 590,000
Murray Field Airport	Rehab Rwy 12/30 and Twy A (Design and Const)	AIP	2025	\$ 707,769	\$ 35,388	\$ 43,253	\$ 786,410

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Murray Field Airport	Major Rehab/Replace Hangars (Design and Construction)	AIP	2026	\$ 832,500	\$ 41,625	\$ 50,875	\$ 925,000
Murray Field Airport	Remove Obstructions	AIP	2026	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Murray Field Airport	Rehab Runway Lighting System (Final Design)	AIP	2027	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Murray Field Airport	Rehab Runway Lighting System (Cons.)	AIP	2028	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Murray Field Airport	Install Perimeter Fencing	AIP	2029	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Napa County Airport	Reconstruct GA Parking Apron (Design)	AIP	2025	\$ 423,000	\$ 21,150	\$ 25,850	\$ 470,000
Napa County Airport	Runway 6-24 PCC Joint Rehab (Design and Construction)	AIP	2025	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
Napa County Airport	Reconstruct GA Parking Apron (Construction)	AIP	2026	\$ 4,320,000	\$ 216,000	\$ 264,000	\$ 4,800,000
Napa County Airport	Airfield Electrical Equipment Replacement (Construction)	AIP	2028	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Napa County Airport	ALP Update and Narrative	AIP	2029	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Napa County Airport	PMMP Update	AIP	2029	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Napa County Airport	Reconstruct Taxiway C (Construction)	AIP	2030	\$ 6,525,000	\$ 326,250	\$ 398,750	\$ 7,250,000
Napa County Airport	Reconstruct Taxiway C (Design)	AIP	2030	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Napa County Airport	EA for Rwy 1L-19R Safety Area Improvements	AIP	2031	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Napa County Airport	Rehabilitate Taxiway E (Construction)	AIP	2032	\$ 5,175,000	\$ 258,750	\$ 316,250	\$ 5,750,000
Napa County Airport	Rehabilitate Taxiway E (Design)	AIP	2032	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Napa County Airport	Reconstruct Taxiway H Section 2 (Construction)	AIP	2033	\$ 2,330,100	\$ 116,505	\$ 142,395	\$ 2,589,000
Napa County Airport	Reconstruct Taxiway H Section 2 (Design)	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Napa County Airport	Taxiway J, J1, and J2 Seal Coat (Construction)	AIP	2034	\$ 435,600	\$ 21,780	\$ 26,620	\$ 484,000
Napa County Airport	Taxiway J, J1, and J2 Seal Coat (Design)	AIP	2034	\$ 61,200	\$ 3,060	\$ 3,740	\$ 68,000
Needles Airport	ALUCP Update	A&D	2026	\$ -	\$ 315,000	\$ 35,000	\$ 350,000
Needles Airport	Runway/Taxiway Rehabilitation and Striping	A&D	2026	\$ -	\$ 450,000	\$ 50,000	\$ 500,000
Needles Airport	Airport Electrical Upgrades Ph I Design/Environmental	A&D	2028	\$ -	\$ 270,000	\$ 30,000	\$ 300,000
Needles Airport	Airport Electrical Upgrades Phase II Construction	A&D	2029	\$ -	\$ 450,000	\$ 50,000	\$ 500,000
Needles Airport	Airport Electrical Upgrades Phase III Construction	A&D	2030	\$ -	\$ 450,000	\$ 50,000	\$ 500,000
Needles Airport	Airport Electrical Upgrades Phase IV Construction	A&D	2031	\$ -	\$ 450,000	\$ 50,000	\$ 500,000
Needles Airport	Safety Area Improvements - Design	A&D	2033	\$ -	\$ 135,000	\$ 15,000	\$ 150,000
Needles Airport	Safety Area Improvements - Construction	A&D	2034	\$ -	\$ 585,000	\$ 65,000	\$ 650,000
Nervino Airport	Reseal Joints in Pavements- Taxiways, Apron and Txln - Des.	AIP	2025	\$ 36,000	\$ 1,800	\$ 2,200	\$ 40,000
Nervino Airport	Reseal Joints in Pavements- Twys, Apron and Txln - Cons.	AIP	2026	\$ 330,075	\$ 16,504	\$ 20,171	\$ 366,750
Nervino Airport	Rehab Twy A and Aprons 2 and 3	AIP	2027	\$ 180,900	\$ 9,045	\$ 11,055	\$ 201,000
Nervino Airport	Rehab Twy A and Aprons 2 and 3	AIP	2028	\$ 1,990,800	\$ 99,540	\$ 121,660	\$ 2,212,000
Nervino Airport	Rehab Surface Pavement: Hangar Taxilane 9 and Access R	AIP	2029	\$ 85,500	\$ 4,275	\$ 5,225	\$ 95,000
Nervino Airport	Relocate Existing Beacon on New Tip Tower	AIP	2029	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Nervino Airport	Construct - Rehab Surface Pavement Section - Hangar 9	AIP	2030	\$ 625,500	\$ 31,275	\$ 38,225	\$ 695,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Nervino Airport	Construct: Relocate Existing Beacon on New Tip-Down Tower	AIP	2030	\$ 274,500	\$ 13,725	\$ 16,775	\$ 305,000
Nervino Airport	Design: Rehab Surface Pavement Rwy 8-26	AIP	2031	\$ 372,600	\$ 18,630	\$ 22,770	\$ 414,000
Nervino Airport	Construct: Rehab Surface Pavement - Rwy 8-26	AIP	2032	\$ 4,987,800	\$ 249,390	\$ 304,810	\$ 5,542,000
Nervino Airport	ALUCP Update	AIP	2033	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Nervino Airport	ALP Update Narrative and Plans	AIP	2034	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Nervino Airport	Pavement Maintenance Management Plan (PMMP)	AIP	2034	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Nevada County Airport	Reconstruct Ramp 4 Access Road Phase 2(1,650 sq.) - Cons.	AIP	2025	\$ 55,800	\$ 2,790	\$ 3,410	\$ 62,000
Nevada County Airport	Crack Repair Ramp 4 Taxilanes Phase 2 - Cons.	AIP	2025	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Nevada County Airport	Reconstruct Ramp 4 Taxilanes Phase 2 - Cons.	AIP	2025	\$ 498,600	\$ 24,930	\$ 30,470	\$ 554,000
Nevada County Airport	Replace Runway Lights (LED), Wire, Transformer - Des.	AIP	2025	\$ 28,800	\$ 1,440	\$ 1,760	\$ 32,000
Nevada County Airport	Replace Runway Lights (LED), Wire, Transformer - Cons.	AIP	2026	\$ 322,200	\$ 16,110	\$ 19,690	\$ 358,000
Nevada County Airport	Rehab (860 sy) and Expand (360 sy) Old Helipad - Des.	AIP	2026	\$ 58,500	\$ 2,925	\$ 3,575	\$ 65,000
Nevada County Airport	Construct - Rehab and Expand Old Helicopter Parking	AIP	2027	\$ 252,900	\$ 12,645	\$ 15,455	\$ 281,000
Nevada County Airport	Design - Crack Seal and Seal Coat - Runway 7-25	AIP	2027	\$ 54,000	\$ 2,700	\$ 3,300	\$ 60,000
Nevada County Airport	Design - Crack Seal and Seal Coat - Taxiways B, C, and D	AIP	2027	\$ 8,100	\$ 405	\$ 495	\$ 9,000
Nevada County Airport	Design - Reconstruct Taxiway A (East End) Phase 1	AIP	2027	\$ 77,400	\$ 3,870	\$ 4,730	\$ 86,000
Nevada County Airport	Replace Twy Lights, Wire, Transformers, and New Regulator	AIP	2027	\$ 36,000	\$ 1,800	\$ 2,200	\$ 40,000
Nevada County Airport	Reconstruct Taxiway A (East End)	AIP	2028	\$ 733,500	\$ 36,675	\$ 44,825	\$ 815,000
Nevada County Airport	Replace Twy Lights, Wire, Transformers, and New Regulator	AIP	2028	\$ 432,900	\$ 21,645	\$ 26,455	\$ 481,000
Nevada County Airport	Construct: Crack Seal and Seal Coat - Runway 7-25	AIP	2029	\$ 587,700	\$ 29,385	\$ 35,915	\$ 653,000
Nevada County Airport	Construct: Crack Seal and Seal Coat - Taxiways B, C, and D	AIP	2029	\$ 59,400	\$ 2,970	\$ 3,630	\$ 66,000
Nevada County Airport	Design: Rehabilitate Taxiway A - Surface Replacement	AIP	2030	\$ 72,900	\$ 3,645	\$ 4,455	\$ 81,000
Nevada County Airport	Construct: Rehab Twy A - Surface Replacement Phase 1	AIP	2031	\$ 537,300	\$ 26,865	\$ 32,835	\$ 597,000
Nevada County Airport	ALP Update Narrative and Plans	AIP	2032	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Nevada County Airport	AWOS Replacement/Relocation Phase 1	AIP	2032	\$ 47,700	\$ 2,385	\$ 2,915	\$ 53,000
Nevada County Airport	Design: Runways 7 and 25 REIL Install Phase 1	AIP	2032	\$ 33,300	\$ 1,665	\$ 2,035	\$ 37,000
Nevada County Airport	Design: Runways 7-25 PAPI Installation Ph. 1	AIP	2032	\$ 53,100	\$ 2,655	\$ 3,245	\$ 59,000
Nevada County Airport	Construct - AWOS Replacement/Relocation Phase 2	AIP	2033	\$ 445,500	\$ 22,275	\$ 27,225	\$ 495,000
Nevada County Airport	Construct: Runway 7-25 REIL Install Phase 2	AIP	2033	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Nevada County Airport	Construct: Runways 7-25 PAPI Install Phase 2	AIP	2033	\$ 299,700	\$ 14,985	\$ 18,315	\$ 333,000
Nevada County Airport	Pavement Maintenance Management Plan (PMMP)	AIP	2033	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Nevada County Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
New Coalinga Municipal Airport	Rehabilitate Crosswind Rwy 1-19 (Construction)	AIP	2025	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000
New Coalinga Municipal Airport	Rehabilitate Runway 12-30	AIP	2026	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
New Coalinga Municipal Airport	Airport Layout Plan Update with Narrative Report	AIP	2027	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
New Coalinga Municipal Airport	Perimeter Access Control Gates	AIP	2028	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000
New Coalinga Municipal Airport	Airfield Signs and Airfield Paint Markings	AIP	2029	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
New Coalinga Municipal Airport	Apron Pavement Recons. (Transient, Hangar and Fuel Apron)	AIP	2030	\$ 198,000	\$ 9,900	\$ 12,100	\$ 220,000
New Coalinga Municipal Airport	Airfield Parking Lot Resurface	AIP	2031	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
New Coalinga Municipal Airport	Construct Airfield Signs and Airfield Paint Markings	AIP	2032	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
New Coalinga Municipal Airport	Crackseal, Rwy, Twys and Apron, Including Drainage	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
New Coalinga Municipal Airport	Replace Existing Runway Lights with LEDs	AIP	2034	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
New Jerusalem Airport	Master Plan	A&D	2025	\$ -	\$ 270,000	\$ 30,000	\$ 300,000
New Jerusalem Airport	PMMP	A&D	2025	\$ -	\$ 67,500	\$ 7,500	\$ 75,000
Nut Tree Airport	Rwy, Twy, and Wind Cone Electrical Improvements (Design)	AIP	2025	\$ 382,500	\$ 19,125	\$ 23,375	\$ 425,000
Nut Tree Airport	DBE Program Update FY 2025/2026/2027	AIP	2025	\$ 22,500	\$ 1,125	\$ 1,375	\$ 25,000
Nut Tree Airport	Airport Pavement Management Program (APMP) Update	AIP	2025	\$ 81,742	\$ 4,087	\$ 4,995	\$ 90,824
Nut Tree Airport	NE TxIn and Hangar TxIn Recons. Ph2B (Construction)	AIP	2026	\$ 4,178,457	\$ 208,923	\$ 255,350	\$ 4,642,730
Nut Tree Airport	Rwy, Twy and Wind Cone Electrical Improvements (Cons.)	AIP	2027	\$ 5,516,100	\$ 275,805	\$ 337,095	\$ 6,129,000
Nut Tree Airport	REILs, PAPIs, Apron Lighting and Rotating Beacon Improv.	AIP	2029	\$ 270,900	\$ 13,545	\$ 16,555	\$ 301,000
Nut Tree Airport	Replace ASOS with AWOS III and Relocate (Des. and Cons.)	AIP	2029	\$ 454,500	\$ 22,725	\$ 27,775	\$ 505,000
Nut Tree Airport	REILs, PAPIs, Apron Lighting and Rotating Beacon Improv.	AIP	2030	\$ 2,034,000	\$ 101,700	\$ 124,300	\$ 2,260,000
Oakdale Municipal Airport	Runway 10-28 Safety Area Improvements (Design)	AIP	2025	\$ 139,500	\$ 6,975	\$ 8,525	\$ 155,000
Oakdale Municipal Airport	Runway and Taxiway Lighting Improvements (Design)	AIP	2026	\$ 81,000	\$ 4,050	\$ 4,950	\$ 90,000
Oakdale Municipal Airport	Runway 10-28 Safety Area Improvements (Construction)	AIP	2026	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Oakdale Municipal Airport	Drainage Improvements (Environmental/Design)	AIP	2026	\$ 202,500	\$ 10,125	\$ 12,375	\$ 225,000
Oakdale Municipal Airport	Pavement Preservation Phase III - VI	AIP	2027	\$ 58,500	\$ 2,925	\$ 3,575	\$ 65,000
Oakdale Municipal Airport	Drainage Improvements (Construction)	AIP	2027	\$ 216,000	\$ 10,800	\$ 13,200	\$ 240,000
Oakdale Municipal Airport	Pavement Preservation, Phase III (Construction)	AIP	2028	\$ 758,700	\$ 37,935	\$ 46,365	\$ 843,000
Oakdale Municipal Airport	Runway and Taxiway Lighting Improvements (Construction)	AIP	2028	\$ 162,000	\$ 8,100	\$ 9,900	\$ 180,000
Oakdale Municipal Airport	Taxiway Pavement Preservation (Design)	AIP	2029	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000
Oakdale Municipal Airport	Taxiway Pavement Preservation (Construction)	AIP	2030	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Oakdale Municipal Airport	Pavement Preservation, Phase IV (Construction)	AIP	2030	\$ 891,000	\$ 44,550	\$ 54,450	\$ 990,000
Oakdale Municipal Airport	Pavement Preservation, Phase V (Construction)	AIP	2030	\$ 720,000	\$ 36,000	\$ 44,000	\$ 800,000
Oakdale Municipal Airport	Pavement Preservation, Phase VI (Construction)	AIP	2030	\$ 792,000	\$ 39,600	\$ 48,400	\$ 880,000
Oceano County Airport	Pavement Preservation for Apron - Design and Construct	AIP	2025	\$ 297,000	\$ 14,850	\$ 18,150	\$ 330,000
Oceano County Airport	Pavement Preservation for Runway 11-29 Des. and Cons.	AIP	2025	\$ 297,000	\$ 14,850	\$ 18,150	\$ 330,000
Oceano County Airport	Pavement Preservation for Taxiway - Design and Construct	AIP	2025	\$ 243,000	\$ 12,150	\$ 14,850	\$ 270,000
Oceano County Airport	Install Electrical Vault and Rwy 11-29 Lighting Improv.	AIP	2026	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Oceano County Airport	Install Miscellaneous NAVAIDs (Seg. Circle/Wind Cone)	AIP	2028	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Ontario International Airport	Reconstruct Taxiway S East of Cucamonga Channel	AIP	2025	\$ 31,350,185	\$ -	\$ 7,550,653	\$ 38,900,838
Ontario International Airport	Holdroom Enhancements	AIP	2026	\$ 5,140,002	\$ -	\$ 1,237,963	\$ 6,377,965
Ontario International Airport	Reconstruct Twy F South of Twy S Within Movement Area (D)	AIP	2026	\$ 219,789	\$ -	\$ 52,936	\$ 272,725
Ontario International Airport	Rehab Rwy 8L-26R, Twy N and Connector Twys East of Twy U	AIP	2026	\$ 37,462,733	\$ -	\$ 9,022,852	\$ 46,485,585
Ontario International Airport	Construct Taxiway E, Phase 1	AIP	2027	\$ 14,623,460	\$ -	\$ 3,522,042	\$ 18,145,502
Ontario International Airport	Reconstruct Twy F South of Twy S Within Movement Area (C)	AIP	2027	\$ 1,897,789	\$ -	\$ 457,080	\$ 2,354,869
Ontario International Airport	Restroom Modernization	AIP	2027	\$ 7,426,430	\$ -	\$ 1,788,646	\$ 9,215,076
Ontario International Airport	Construct Twy E, Phase 2, and Rehab Select Panels on Twy F	AIP	2028	\$ 17,305,893	\$ -	\$ 4,168,102	\$ 21,473,995
Ontario International Airport	Rehab Select Panels on Terminals 2, 3, and 4 Aprons	AIP	2028	\$ 637,179	\$ -	\$ 153,464	\$ 790,643
Ontario International Airport	Rehab Twy U Connector and Standardize Fillets	AIP	2028	\$ 443,077	\$ -	\$ 106,715	\$ 549,792
Ontario International Airport	Terminal Infrastructure Renewal	AIP	2028	\$ 6,102,421	\$ -	\$ 1,469,760	\$ 7,572,181
Ontario International Airport	Construct Taxiway S5 (Construction)	AIP	2029	\$ 11,788,439	\$ -	\$ 2,839,231	\$ 14,627,670
Oroville Municipal Airport	Crack Seal South Hangar Taxiway, Taxilane, North - Cons.	AIP	2025	\$ 171,014	\$ 8,551	\$ 10,451	\$ 190,015
Oroville Municipal Airport	Remarking Runway 13-31 - Cons.	AIP	2025	\$ 84,600	\$ 4,230	\$ 5,170	\$ 94,000
Oroville Municipal Airport	Crack Seal Taxiway L and North Apron - Des. and Cons.	AIP	2025	\$ 277,182	\$ 13,859	\$ 16,939	\$ 307,980
Oroville Municipal Airport	Design: Rehab Rwy 2-20 and Rwy Lighting System	AIP	2027	\$ 477,900	\$ 23,895	\$ 29,205	\$ 531,000
Oroville Municipal Airport	Construct Rehabilitate Rwy 2-20	AIP	2029	\$ 5,125,230	\$ 256,262	\$ 313,209	\$ 5,694,700
Oroville Municipal Airport	Construct: Rehab Lighting, Cross Taxiways, and Blast Pad	AIP	2030	\$ 1,483,470	\$ 74,174	\$ 90,657	\$ 1,648,300
Oroville Municipal Airport	Design/Construct: Crack Repair and Seal Cracks	AIP	2031	\$ 1,087,200	\$ 54,360	\$ 66,440	\$ 1,208,000
Oroville Municipal Airport	ALUCP Update	AIP	2032	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Oroville Municipal Airport	Design: Realign Twy S	AIP	2032	\$ 68,850	\$ 3,443	\$ 4,208	\$ 76,500
Oroville Municipal Airport	Pavement Maintenance Management Plan (PMMP)	AIP	2032	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Oroville Municipal Airport	Construct: Realign Twy S	AIP	2033	\$ 666,900	\$ 33,345	\$ 40,755	\$ 741,000
Oroville Municipal Airport	ALP Updated Narrative and Plans with Aero Study	AIP	2034	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Oxnard Airport	Reconstruct Taxiway F (Companion Grant)	AIP	2025	\$ 3,339,000	\$ 166,950	\$ 204,050	\$ 3,710,000
Oxnard Airport	Rehabilitate ATCT (Sponsor to Req ATP or FCT BIL Grant)	AIP	2025	\$ 1,935,000	\$ 96,750	\$ 118,250	\$ 2,150,000
Oxnard Airport	Reconstruction of Terminal Apron and ARFF Apron - Design	AIP	2027	\$ 489,375	\$ 24,469	\$ 29,906	\$ 543,750
Oxnard Airport	Rehab Central Apron, Executive Hangar and Transient Apron	AIP	2028	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Oxnard Airport	Recons. of Terminal Apron PCC/AC and ARFF Apron (Cons.)	AIP	2029	\$ 3,262,500	\$ 163,125	\$ 199,375	\$ 3,625,000
Oxnard Airport	Land Acquisitions - RPZ Property Purchases	AIP	2030	\$ 1,440,000	\$ 72,000	\$ 88,000	\$ 1,600,000
Oxnard Airport	Rehab Central and Transient Apron, Executive Hangar Area	AIP	2030	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Oxnard Airport	Rehab Perimeter and Terminal Loop Roads, and Parking	AIP	2031	\$ 118,362	\$ 5,918	\$ 7,233	\$ 131,513
Oxnard Airport	Land Acquisitions - OFA Property Purchases	AIP	2032	\$ 6,552,050	\$ 327,602	\$ 400,403	\$ 7,280,055
Oxnard Airport	Rehab Perim. and Terminal Loop Roads, Parkings	AIP	2033	\$ 789,078	\$ 39,454	\$ 48,221	\$ 876,753
Oxnard Airport	Design for Rehabilitate West Hangar Apron Area	AIP	2034	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Palo Alto Airport	Airport Land Use Compatibility Plan	A&D	2025	\$ -	\$ 315,000	\$ 35,000	\$ 350,000
Palo Alto Airport	AWOS III (Construction)	AIP	2025	\$ 310,500	\$ 15,525	\$ 18,975	\$ 345,000
Palo Alto Airport	Airfield Solar Array - Construction	AIP	2025	\$ 3,816,000	\$ 190,800	\$ 233,200	\$ 4,240,000
Palo Alto Airport	PAPI Replacement (Construction)	AIP	2025	\$ 69,300	\$ 3,465	\$ 4,235	\$ 77,000
Palo Alto Airport	Airfield Electrical Improvements (Design)	AIP	2025	\$ 382,500	\$ 19,125	\$ 23,375	\$ 425,000
Palo Alto Airport	Airfield Electrical Improvements (Construction)	AIP	2026	\$ 3,577,500	\$ 178,875	\$ 218,625	\$ 3,975,000
Palo Alto Airport	Rwy and Twy Recons. and Drainage Improvements - Design	AIP	2027	\$ 517,500	\$ 25,875	\$ 31,625	\$ 575,000
Palo Alto Airport	Airport Beacon Refurbishment	AIP	2027	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Palo Alto Airport	Rwy and Twy Recons. and Drainage Improvements - Cons.	AIP	2028	\$ 3,600,000	\$ 180,000	\$ 220,000	\$ 4,000,000
Palo Alto Airport	EA for Rwy and Twy Recon and Drainage Improvement	AIP	2030	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Palo Alto Airport	Reconstruction Access/Service Road (Construction)☐	AIP	2030	\$ 2,160,000	\$ 108,000	\$ 132,000	\$ 2,400,000
Paso Robles Municipal Airport	Rehabilitate Taxiway A and Holding Bay	AIP	2025	\$ 4,050,000	\$ 202,500	\$ 247,500	\$ 4,500,000
Paso Robles Municipal Airport	Rehabilitate Terminal Ramp and North Ramp	AIP	2027	\$ 4,950,000	\$ 247,500	\$ 302,500	\$ 5,500,000
Paso Robles Municipal Airport	Construct Taxilane	AIP	2031	\$ 1,215,000	\$ 60,750	\$ 74,250	\$ 1,350,000
Paso Robles Municipal Airport	Geometry Upgrades Twy D at Rwy 1-19, and Apron Expansion	AIP	2031	\$ 6,300,000	\$ 315,000	\$ 385,000	\$ 7,000,000
Paso Robles Municipal Airport	Rehabilitate Runway 13-31	AIP	2032	\$ 2,970,000	\$ 148,500	\$ 181,500	\$ 3,300,000
Paso Robles Municipal Airport	Rehabilitate Runway 1-19	AIP	2033	\$ 5,220,000	\$ 261,000	\$ 319,000	\$ 5,800,000
Paso Robles Municipal Airport	Geometry Upgrades Twy F at Rwy 1 and Holding Bay	AIP	2034	\$ 4,950,000	\$ 247,500	\$ 302,500	\$ 5,500,000
Paso Robles Municipal Airport	Security Fence Upgrades	AIP	2034	\$ 1,170,000	\$ 58,500	\$ 71,500	\$ 1,300,000
Petaluma Municipal Airport	Taxiway A Rehab - Overlay (Construction) - Base Bid	AIP	2025	\$ 1,174,297	\$ 58,715	\$ 71,763	\$ 1,304,774
Petaluma Municipal Airport	Taxiway A Rehab - Overlay (Cons.) - Bid Alternative	AIP	2025	\$ 1,393,925	\$ 69,696	\$ 85,184	\$ 1,548,805
Petaluma Municipal Airport	DBE Program Update FY2025/2026/2027	AIP	2025	\$ 27,000	\$ 1,350	\$ 1,650	\$ 30,000
Petaluma Municipal Airport	South Taxilane B Reconstruction (Construction) - Base Bid	AIP	2025	\$ 973,193	\$ 48,660	\$ 59,473	\$ 1,081,326
Petaluma Municipal Airport	South Txln B Reconstruction (Construction) - Bid Alternative	AIP	2025	\$ 686,861	\$ 34,343	\$ 41,975	\$ 763,179
Petaluma Municipal Airport	Runway 11-29 Seal Coat (Design)	AIP	2026	\$ 118,800	\$ 5,940	\$ 7,260	\$ 132,000
Petaluma Municipal Airport	North, South, and Executive Hangar Seal Coat (Design)	AIP	2026	\$ 118,800	\$ 5,940	\$ 7,260	\$ 132,000
Petaluma Municipal Airport	Runway 11-29 Seal Coat (Construction)	AIP	2027	\$ 718,200	\$ 35,910	\$ 43,890	\$ 798,000
Petaluma Municipal Airport	North, South, and Executive Hangar Seal Coat (Cons.)	AIP	2027	\$ 817,200	\$ 40,860	\$ 49,940	\$ 908,000
Petaluma Municipal Airport	Apron C Reconstruction (Design)	AIP	2028	\$ 368,100	\$ 18,405	\$ 22,495	\$ 409,000
Petaluma Municipal Airport	Apron C Reconstruction (Construction)	AIP	2030	\$ 2,772,000	\$ 138,600	\$ 169,400	\$ 3,080,000
Pine Mountain Lake Airport	AWOS Construction	AIP	2025	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Pine Mountain Lake Airport	AWOS Design	AIP	2025	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Pine Mountain Lake Airport	Runway 09/27 Rehab Design	AIP	2026	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Pine Mountain Lake Airport	Runway 09/27 Rehab Construction	AIP	2027	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
Pine Mountain Lake Airport	Taxiway A Rehab Design	AIP	2027	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Pine Mountain Lake Airport	Taxiway A Rehab Construction	AIP	2028	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
Pine Mountain Lake Airport	Vehicle Access Roads and Drainage Design	AIP	2029	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Pine Mountain Lake Airport	Vehicle Access Roads and Drainage Construction	AIP	2030	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Pine Mountain Lake Airport	Parking Apron Rehab Design	AIP	2031	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Pine Mountain Lake Airport	Parking Apron Rehab Construction	AIP	2032	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Pine Mountain Lake Airport	Design of Rehabilitation of AC Ramp and Helicopter Parking	AIP	2033	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Pine Mountain Lake Airport	Rehabilitation of AC Ramp and Helicopter Parking	AIP	2034	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Placerville Airport	Hangar Site Feasibility/Pre-Design Study - Des.	AIP	2025	\$ 57,335	\$ 2,867	\$ 3,504	\$ 63,705
Placerville Airport	Construct New Hangar - Structure and Site Work - Des.	AIP	2025	\$ 207,900	\$ 10,395	\$ 12,705	\$ 231,000
Placerville Airport	Install New Airport Beacon and Tower - Des.	AIP	2025	\$ 70,200	\$ 3,510	\$ 4,290	\$ 78,000
Placerville Airport	Construct New Hangar - Structure and Site Work	AIP	2026	\$ 974,084	\$ 48,704	\$ 59,527	\$ 1,082,316
Placerville Airport	Install New AWOS on Existing Tower	AIP	2026	\$ 247,500	\$ 12,375	\$ 15,125	\$ 275,000
Placerville Airport	Crack Seal and Remark Pavements	AIP	2026	\$ 52,200	\$ 2,610	\$ 3,190	\$ 58,000
Placerville Airport	Construct: Install New Airport Beacon and Tower	AIP	2027	\$ 338,400	\$ 16,920	\$ 20,680	\$ 376,000
Placerville Airport	Design: Install New PAPI Runway 5, Remove Existing VASI	AIP	2027	\$ 40,500	\$ 2,025	\$ 2,475	\$ 45,000
Placerville Airport	Construct Crack Seal and Remark Pavements	AIP	2028	\$ 684,450	\$ 34,223	\$ 41,828	\$ 760,500
Placerville Airport	Install New PAPI Runway 5, Remove Existing VASI	AIP	2030	\$ 126,900	\$ 6,345	\$ 7,755	\$ 141,000
Placerville Airport	Reimbursable Agreement PAPI Flight Check	AIP	2030	\$ 22,500	\$ 1,125	\$ 1,375	\$ 25,000
Placerville Airport	Airport Layout Plan Updated Narrative and Plans	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Placerville Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Placerville Airport	Pavement Maintenance Management Program (PMMP)	AIP	2034	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Porterville Municipal Airport	Airfield electrical improvements (design)	AIP	2025	\$ 171,000	\$ 8,550	\$ 10,450	\$ 190,000
Porterville Municipal Airport	Airfield Electrical Improvements (Cons.)	AIP	2026	\$ 639,000	\$ 31,950	\$ 39,050	\$ 710,000
Porterville Municipal Airport	Runway Rehabilitation (Design)	AIP	2026	\$ 657,000	\$ 32,850	\$ 40,150	\$ 730,000
Porterville Municipal Airport	Runway Rehabilitation (Cons.)	AIP	2028	\$ 8,460,000	\$ 423,000	\$ 517,000	\$ 9,400,000
Porterville Municipal Airport	ALP Update with Narrative	AIP	2030	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Porterville Municipal Airport	EA: Shift Runway 12-30	AIP	2031	\$ 317,970	\$ 15,899	\$ 19,432	\$ 353,300
Porterville Municipal Airport	Taxilane Rehabilitation (Design)	AIP	2031	\$ 144,000	\$ 7,200	\$ 8,800	\$ 160,000
Porterville Municipal Airport	EA - Continuing Funding	AIP	2032	\$ 159,030	\$ 7,952	\$ 9,719	\$ 176,700
Porterville Municipal Airport	Taxilane Rehabilitation (Cons.)	AIP	2032	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
Porterville Municipal Airport	EA - Continuing funding	AIP	2033	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Porterville Municipal Airport	Runway and Parallel Taxiway Shift (Design)	AIP	2033	\$ 378,000	\$ 18,900	\$ 23,100	\$ 420,000
Porterville Municipal Airport	Rehabilitate Airport Fuel Facility (Design)	AIP	2034	\$ 297,000	\$ 14,850	\$ 18,150	\$ 330,000
Porterville Municipal Airport	Runway and Parallel Taxiway Shift to North (Cons.)	AIP	2034	\$ 5,715,000	\$ 285,750	\$ 349,250	\$ 6,350,000
Ramona Airport	Rehabilitate Runway 09/27	AIP	2027	\$ 7,650,000	\$ 382,500	\$ 467,500	\$ 8,500,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Ramona Airport	Rehabilitate Taxiway Alpha	AIP	2030	\$ 5,400,000	\$ 270,000	\$ 330,000	\$ 6,000,000
Ravendale Airport	Pavement Maintenance and Remarking; Rwy and Twy	A&D	2025	\$ -	\$ 142,200	\$ 15,800	\$ 158,000
Red Bluff Municipal Airport	Expand Apron, Including 2 Helicopter Parking Pad	AIP	2025	\$ 121,500	\$ 6,075	\$ 7,425	\$ 135,000
Red Bluff Municipal Airport	Install Perimeter Fencing and Acquire Security Equipment	AIP	2026	\$ 100,800	\$ 5,040	\$ 6,160	\$ 112,000
Red Bluff Municipal Airport	Terminal Building Improvements (Design and Construct)	AIP	2026	\$ 765,000	\$ 38,250	\$ 46,750	\$ 850,000
Red Bluff Municipal Airport	Update Airport Layout Plan with Narrative Report	AIP	2026	\$ 382,500	\$ 19,125	\$ 23,375	\$ 425,000
Red Bluff Municipal Airport	Update Pavement Maintenance Management Plan (PMMP)	AIP	2026	\$ 112,500	\$ 5,625	\$ 6,875	\$ 125,000
Red Bluff Municipal Airport	Expand Apron, including 2 Helicopter Parking Pad	AIP	2027	\$ 1,215,000	\$ 60,750	\$ 74,250	\$ 1,350,000
Red Bluff Municipal Airport	Rehab Runway 15/33 (100'x5,431') (Pavement Maint) (Design)	AIP	2028	\$ 76,500	\$ 3,825	\$ 4,675	\$ 85,000
Red Bluff Municipal Airport	Runway 15/33 Reconstruction (EA)	AIP	2028	\$ 198,000	\$ 9,900	\$ 12,100	\$ 220,000
Red Bluff Municipal Airport	Improve Runway Safety Area - Drainage Improvements	AIP	2029	\$ 202,500	\$ 10,125	\$ 12,375	\$ 225,000
Red Bluff Municipal Airport	Rehab Airfield Lighting System (Design)	AIP	2029	\$ 76,500	\$ 3,825	\$ 4,675	\$ 85,000
Red Bluff Municipal Airport	Rehab Runway 15/33 (Cons.)	AIP	2029	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Red Bluff Municipal Airport	Runway 15/33 Reconstruction (Design)	AIP	2029	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Red Bluff Municipal Airport	Rehab Airfield Lighting System (Cons.)	AIP	2030	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Red Bluff Municipal Airport	Runway 15/33 Reconstruction(Cons.)	AIP	2030	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
Red Bluff Municipal Airport	Rehab Apron (Design)	AIP	2031	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Red Bluff Municipal Airport	Rehab Apron (Constr)	AIP	2032	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Red Bluff Municipal Airport	Rehab Taxiways (Design)	AIP	2033	\$ 54,000	\$ 2,700	\$ 3,300	\$ 60,000
Red Bluff Municipal Airport	Rehab Taxiways (Cons.)	AIP	2034	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000
Redding Municipal Airport	Runway 16-34 Rehab and Taxiway Connectors, MAGVAR (Cons.)	AIP	2025	\$ 27,900,000	\$ -	\$ 3,100,000	\$ 31,000,000
Redding Municipal Airport	Runway 16-34 Safety Area Improvements - Phase 1 Design	AIP	2026	\$ 1,032,508	\$ -	\$ 114,723	\$ 1,147,231
Redding Municipal Airport	Taxiway D/Future Taxiway A and Taxiway Connectors - Design	AIP	2026	\$ 1,800,000	\$ -	\$ 200,000	\$ 2,000,000
Redding Municipal Airport	Master Drainage Study	AIP	2026	\$ 526,680	\$ -	\$ 58,520	\$ 585,200
Redding Municipal Airport	Runway 16-34 Safety Area Improvements	AIP	2027	\$ 12,285,000	\$ -	\$ 1,365,000	\$ 13,650,000
Redding Municipal Airport	Taxiway D / Future Taxiway A and Taxiway Connectors	AIP	2028	\$ 24,750,000	\$ -	\$ 2,750,000	\$ 27,500,000
Redding Municipal Airport	Passenger Terminal Expansion - Phase I	AIP	2028	\$ 5,400,000	\$ -	\$ 600,000	\$ 6,000,000
Redding Municipal Airport	WHA and WHMP	AIP	2028	\$ 157,500	\$ -	\$ 17,500	\$ 175,000
Redding Municipal Airport	Acquire Snow Removal Equipment	AIP	2029	\$ 283,500	\$ -	\$ 31,500	\$ 315,000
Redding Municipal Airport	Airfield Lighting Improvements Study	AIP	2029	\$ 165,375	\$ -	\$ 18,375	\$ 183,750
Redding Municipal Airport	Passenger Terminal Expansion - Phase II	AIP	2030	\$ 36,000,000	\$ -	\$ 4,000,000	\$ 40,000,000
Redding Municipal Airport	Security/Perimeter Fence and Access	AIP	2030	\$ 4,252,500	\$ -	\$ 472,500	\$ 4,725,000
Redding Municipal Airport	ARFF Vehicle Replacement	AIP	2030	\$ 1,417,500	\$ -	\$ 157,500	\$ 1,575,000
Redlands Municipal Airport	Fuel Farm Design	AIP	2026	\$ 79,200	\$ 3,960	\$ 4,840	\$ 88,000
Redlands Municipal Airport	Wash Rack Construction	AIP	2026	\$ 594,000	\$ 29,700	\$ 36,300	\$ 660,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Redlands Municipal Airport	Wash Rack Design	AIP	2026	\$ 79,200	\$ 3,960	\$ 4,840	\$ 88,000
Redlands Municipal Airport	Taxiway Rehabilitation	AIP	2028	\$ 117,000	\$ 5,850	\$ 7,150	\$ 130,000
Redlands Municipal Airport	Land Development	AIP	2029	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Redlands Municipal Airport	Pavement Construction	AIP	2031	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Reedley Municipal Airport	Design Installation of Runway 16-34 End Lights	AIP	2025	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Reedley Municipal Airport	Design Above Ground Fuel Tanks	AIP	2025	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Reedley Municipal Airport	Construct Above Ground Fuel Tanks	AIP	2026	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
Reedley Municipal Airport	Construct Installation of Runway 16-34 End Lights	AIP	2027	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Reedley Municipal Airport	Replace Runway 16-34 Edge Lights with LED's	AIP	2028	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Reedley Municipal Airport	ALP Narrative Report and Obstruction Survey	AIP	2029	\$ 337,500	\$ 16,875	\$ 20,625	\$ 375,000
Reedley Municipal Airport	Design Taxiway A Pavement Preservation	AIP	2030	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000
Reedley Municipal Airport	Construct Taxiway A Pavement Preservation	AIP	2031	\$ 166,500	\$ 8,325	\$ 10,175	\$ 185,000
Reedley Municipal Airport	Design Apron Pavement Reconstruction	AIP	2032	\$ 486,000	\$ 24,300	\$ 29,700	\$ 540,000
Reedley Municipal Airport	Construct Apron Pavement Recons. (Transient Apron) Phase 1	AIP	2033	\$ 972,000	\$ 48,600	\$ 59,400	\$ 1,080,000
Reedley Municipal Airport	Construct Apron Pavement Recon (Fuel Apron) Phase 2	AIP	2034	\$ 720,000	\$ 36,000	\$ 44,000	\$ 800,000
Reid Hillview Airport	Design and Construct - Taxilane TLA-001 Rehab	AIP	2026	\$ 1,044,576	\$ 52,229	\$ 63,835	\$ 1,160,640
Reid Hillview Airport	Design and Construct - Taxilane TLA-001 (Twy Q) Rehab	AIP	2027	\$ 1,507,896	\$ 75,395	\$ 92,149	\$ 1,675,440
Reid Hillview Airport	Design and Construct - Taxilane TLA-001 (R) Rehab	AIP	2028	\$ 1,507,896	\$ 75,395	\$ 92,149	\$ 1,675,440
Reid Hillview Airport	Design and Construct - Taxilane TLA-001 (S) Rehab	AIP	2029	\$ 1,507,896	\$ 75,395	\$ 92,149	\$ 1,675,440
Reid Hillview Airport	Design and Construct - Taxilane Rehab	AIP	2030	\$ 1,507,896	\$ 75,395	\$ 92,149	\$ 1,675,440
Reid Hillview Airport	APMS Update	AIP	2032	\$ 68,747	\$ 3,437	\$ 4,201	\$ 76,385
Reid Hillview Airport	ALP Update with Narrative and AGIS	AIP	2033	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Rio Vista Municipal Airport	Airport Pavement Management System (APMS) Update	AIP	2025	\$ 150,750	\$ 7,538	\$ 9,213	\$ 167,500
Rio Vista Municipal Airport	Construction Ph I - Runway 7-25 Reconstruction	AIP	2025	\$ 2,130,300	\$ 106,515	\$ 130,185	\$ 2,367,000
Rio Vista Municipal Airport	Runway 15-33 Reconstruction Design and Construction	AIP	2026	\$ 409,050	\$ 20,453	\$ 24,998	\$ 454,500
Rio Vista Municipal Airport	Terminal Improvements	AIP	2026	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Rio Vista Municipal Airport	Runway 7-25 Extension CEQA and NEPA	AIP	2027	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Rio Vista Municipal Airport	Construct Row of Tee Hangars	AIP	2028	\$ 1,874,700	\$ 93,735	\$ 114,565	\$ 2,083,000
Rio Vista Municipal Airport	Relocate Helipad and Helicopter Parking Area	AIP	2030	\$ 54,000	\$ 2,700	\$ 3,300	\$ 60,000
Rio Vista Municipal Airport	Design: Generator, Repave Parking Lot, Jet A Fuel	AIP	2031	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000
Rio Vista Municipal Airport	Install Jet A Fuel System	AIP	2032	\$ 161,100	\$ 8,055	\$ 9,845	\$ 179,000
Rio Vista Municipal Airport	Install Corporate Hangars Near Building and West Apron	AIP	2033	\$ 1,376,100	\$ 68,805	\$ 84,095	\$ 1,529,000
Riverside Municipal Airport	Airport Pavement Management System (APMS) Study	AIP	2025	\$ 99,000	\$ 4,950	\$ 6,050	\$ 110,000
Riverside Municipal Airport	Runway 9/27 Drainage Improvements, Phase I Design	AIP	2025	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Riverside Municipal Airport	Runway 9/27 Asphalt Reconstruction, Phase 1 Design	AIP	2026	\$ 1,557,000	\$ 77,850	\$ 95,150	\$ 1,730,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Riverside Municipal Airport	Runway 9/27 Drainage Improvements, Phase 2 Construction	AIP	2026	\$ 1,593,000	\$ 79,650	\$ 97,350	\$ 1,770,000
Riverside Municipal Airport	Runway 16/34 Asphalt Reconstruction, Phase 1 Design	AIP	2028	\$ 297,000	\$ 14,850	\$ 18,150	\$ 330,000
Riverside Municipal Airport	Runway 9/27 Asphalt Reconstruction Phase 2 Construction	AIP	2028	\$ 5,400,000	\$ 270,000	\$ 330,000	\$ 6,000,000
Riverside Municipal Airport	Taxiway B and J Asphalt Reconstruction - Phase 1 Design	AIP	2029	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Riverside Municipal Airport	Airfield Electrical Vault Upgrades, Phase 1 Design	AIP	2030	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Riverside Municipal Airport	Runway 16/34 Asphalt Reconstruction, Phase 2 Construction	AIP	2030	\$ 3,276,000	\$ 163,800	\$ 200,200	\$ 3,640,000
Riverside Municipal Airport	Airfield Electrical Vault Upgrades, Phase 2 Construction	AIP	2031	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
Riverside Municipal Airport	Taxiway B and J Asphalt Reconstruction, Phase 2 Construction	AIP	2031	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
Riverside Municipal Airport	Aircraft Ramp Reconstruction, Phase 1 Design	AIP	2032	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Riverside Municipal Airport	Aircraft Ramp Reconstruction, Phase 2a Construction	AIP	2033	\$ 4,500,000	\$ 225,000	\$ 275,000	\$ 5,000,000
Riverside Municipal Airport	Aircraft Ramp Reconstruction, Phase 2b Construction	AIP	2034	\$ 4,500,000	\$ 225,000	\$ 275,000	\$ 5,000,000
Rogers Field Airport	Design/Construct: Expand Existing Fuel Storage Tanks	AIP	2025	\$ 751,500	\$ 37,575	\$ 45,925	\$ 835,000
Rogers Field Airport	Reseal Joints in Twy A Pavements - Des.	AIP	2025	\$ 33,300	\$ 1,665	\$ 2,035	\$ 37,000
Rogers Field Airport	Construct - Reseal Joints in Taxiway A Pavements	AIP	2026	\$ 207,900	\$ 10,395	\$ 12,705	\$ 231,000
Rogers Field Airport	Design - Extend Taxiway A, Relocate Threshold Rwy 16	AIP	2027	\$ 198,000	\$ 9,900	\$ 12,100	\$ 220,000
Rogers Field Airport	Extend Taxiway A, Relocate Rwy 16 Threshold	AIP	2029	\$ 1,897,200	\$ 94,860	\$ 115,940	\$ 2,108,000
Rogers Field Airport	Design - New Security and Wildlife Fence	AIP	2029	\$ 115,200	\$ 5,760	\$ 7,040	\$ 128,000
Rogers Field Airport	Construct - New Security and Wildlife Fence	AIP	2030	\$ 1,423,800	\$ 71,190	\$ 87,010	\$ 1,582,000
Rogers Field Airport	Des./Construct: Saw and Seal New Joints and Tie Down Apron	AIP	2031	\$ 181,800	\$ 9,090	\$ 11,110	\$ 202,000
Rogers Field Airport	Pavement Maintenance Management Plan (PMMP)	AIP	2032	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Rogers Field Airport	ALP Update Narrative and Plans	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Rogers Field Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Rohnerville Airport	Rehab Runway 11/29 (100'x4,005') (Design)	AIP	2025	\$ 359,276	\$ 17,964	\$ 21,956	\$ 399,195
Rohnerville Airport	Rehab Runway Lighting System (Final Design)	AIP	2025	\$ 169,052	\$ 8,453	\$ 10,331	\$ 187,835
Rohnerville Airport	Rehab Runway 11/29 (100'x4,005') (Cons.)	AIP	2026	\$ 2,655,000	\$ 132,750	\$ 162,250	\$ 2,950,000
Rohnerville Airport	Rehab Runway Lighting System (Cons.)	AIP	2027	\$ 342,728	\$ 17,136	\$ 20,944	\$ 380,809
Rohnerville Airport	Rehab Taxiways A, A2, A3, and B (Design)	AIP	2029	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Rohnerville Airport	Crack Seal/Surface Treatment/Remark Rwy 11-29	AIP	2031	\$ 300,000	\$ 15,000	\$ 18,333	\$ 333,333
Rohnerville Airport	Rehab Taxiways A, A2, A3, and B (Cons.)	AIP	2031	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Round Valley Airport	Runway 10-28 Lighting Upgrades - Design	AIP	2026	\$ 63,000	\$ 3,150	\$ 3,850	\$ 70,000
Round Valley Airport	Runway 10-28 Lighting Upgrades - Construction	AIP	2027	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000
Round Valley Airport	Pavement Preservation - Design	AIP	2028	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Round Valley Airport	Pavement Preservation - Cons.	AIP	2029	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Round Valley Airport	Airport Pavement Management Plan and ALP	AIP	2030	\$ 199,500	\$ 9,975	\$ 12,192	\$ 221,667
Round Valley Airport	Rwy 10 RSA Obstruction Removal Grading and Twy A Extension	AIP	2031	\$ 208,350	\$ 10,418	\$ 12,733	\$ 231,500

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Round Valley Airport	Rwy 10 RSA Obst. Removal, Grading and Twy A Ext. - Cons.	AIP	2032	\$ 1,575,000	\$ 78,750	\$ 96,250	\$ 1,750,000
Ruth Airport	Airport Layout Plan	AIP	2027	\$ 198,990	\$ 9,950	\$ 12,161	\$ 221,100
Ruth Airport	Pavement Repairs and Maintenance - Crack Sealing/Patching	AIP	2027	\$ 9,000	\$ 450	\$ 550	\$ 10,000
Ruth Airport	Pavement Maintenance Management Plan	A&D	2029	\$ -	\$ 43,200	\$ 4,800	\$ 48,000
Sacramento Executive Airport	Rwy 2/20 Rehabilitate Runway, and RSA Improvements	AIP	2027	\$ 12,240,000	\$ 612,000	\$ 748,000	\$ 13,600,000
Sacramento Executive Airport	New Connector Taxiway at Runway 20, Threshold	AIP	2027	\$ 3,150,000	\$ 157,500	\$ 192,500	\$ 3,500,000
Sacramento Executive Airport	Rehabilitation of Taxiways M, N and W	AIP	2028	\$ 1,485,000	\$ 74,250	\$ 90,750	\$ 1,650,000
Sacramento Executive Airport	Rehabilitation Twys E and H, including Twy Realignment	AIP	2029	\$ 1,485,000	\$ 74,250	\$ 90,750	\$ 1,650,000
Sacramento Executive Airport	Rehabilitate Taxiways C and D, Including Twy Realignment	AIP	2030	\$ 1,485,000	\$ 74,250	\$ 90,750	\$ 1,650,000
Sacramento International Airport	Concourse B Expansion	AIP	2026	\$ 225,000,000	\$ -	\$ 25,000,000	\$ 250,000,000
Sacramento International Airport	Rehab South Portion of Twy A	AIP	2026	\$ 9,000,000	\$ -	\$ 1,000,000	\$ 10,000,000
Sacramento International Airport	Taxiway Reconfiguration in location of P, G1 and G2	AIP	2027	\$ 9,000,000	\$ -	\$ 1,000,000	\$ 10,000,000
Sacramento International Airport	West Airfield Apron Construction	AIP	2028	\$ 9,675,000	\$ -	\$ 1,075,000	\$ 10,750,000
Sacramento International Airport	Reconstruct/Realign Taxiway A System (A11 and A5)	AIP	2029	\$ 28,800,000	\$ -	\$ 3,200,000	\$ 32,000,000
Sacramento Mather Airport	Rehabilitate Taxiway D and D1	AIP	2028	\$ 4,500,000	\$ 225,000	\$ 275,000	\$ 5,000,000
Sacramento Mather Airport	Rehabilitate General Aviation Apron	AIP	2029	\$ 9,000,000	\$ 450,000	\$ 550,000	\$ 10,000,000
Salinas Municipal Airport	Additional Services Taxilane Pavement Rehab Program	AIP	2025	\$ 98,173	\$ 4,909	\$ 5,999	\$ 109,081
Salinas Municipal Airport	ALUCP	A&D	2025	\$ -	\$ 337,500	\$ 37,500	\$ 375,000
Salinas Municipal Airport	Runway Extension Feasibility Study	AIP	2025	\$ 218,295	\$ 10,915	\$ 13,340	\$ 242,550
Salinas Municipal Airport	Taxilane Pavement Rehabilitation Hangar L	AIP	2026	\$ 1,261,305	\$ 63,065	\$ 77,080	\$ 1,401,450
Salinas Municipal Airport	Terminal Improvements Construction	AIP	2026	\$ 7,087,500	\$ 354,375	\$ 433,125	\$ 7,875,000
Salinas Municipal Airport	Construct Taxilane Pavement Rehab Hangars O, Q, S, T	AIP	2027	\$ 3,244,271	\$ 162,214	\$ 198,261	\$ 3,604,746
Salinas Municipal Airport	Design and Construct Taxiway B Pavement Rehabilitation	AIP	2027	\$ 326,025	\$ 16,301	\$ 19,924	\$ 362,250
Salinas Municipal Airport	Airfield Signage and Runway Lighting Upgrades	AIP	2028	\$ 74,419	\$ 3,721	\$ 4,548	\$ 82,688
Salinas Municipal Airport	Perimeter Road Pavement Rehabilitation	AIP	2028	\$ 141,750	\$ 7,088	\$ 8,663	\$ 157,500
Salinas Municipal Airport	Rwy 13-31 Lighting Upgrades	AIP	2028	\$ 178,605	\$ 8,930	\$ 10,915	\$ 198,450
Salinas Municipal Airport	Rwy 8-26 Lighting Upgrades	AIP	2028	\$ 124,031	\$ 6,202	\$ 7,580	\$ 137,812
Salinas Municipal Airport	EA - Rwy RSA Enhancements Safety Environmental	AIP	2028	\$ 382,725	\$ 19,136	\$ 23,389	\$ 425,250
Salinas Municipal Airport	PMMP Update	AIP	2028	\$ 189,000	\$ 9,450	\$ 11,550	\$ 210,000
Salinas Municipal Airport	ALP Update	AIP	2029	\$ 283,500	\$ 14,175	\$ 17,325	\$ 315,000
Salinas Municipal Airport	Construction - Airfield Signage Upgrades	AIP	2029	\$ 992,250	\$ 49,613	\$ 60,638	\$ 1,102,500
Salinas Municipal Airport	Construction - Rwy 13-31 Lighting Upgrades	AIP	2029	\$ 992,250	\$ 49,613	\$ 60,638	\$ 1,102,500
Salinas Municipal Airport	Construction - Rwy 8-26 Lighting Upgrades	AIP	2029	\$ 738,674	\$ 36,934	\$ 45,141	\$ 820,749
Salinas Municipal Airport	Construction - Enhance Runway Safety (Rwy 13-31)	AIP	2030	\$ 7,278,153	\$ 363,908	\$ 444,776	\$ 8,086,837
Salinas Municipal Airport	Construction - Perimeter Road Pavement Rehabilitation	AIP	2030	\$ 496,125	\$ 24,806	\$ 30,319	\$ 551,250

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Salinas Municipal Airport	NEPA - East Side Taxiway System, Access, and Hangars	AIP	2030	\$ 638,820	\$ 31,941	\$ 39,039	\$ 709,800
Salinas Municipal Airport	PMMP Update	AIP	2030	\$ 212,625	\$ 10,631	\$ 12,994	\$ 236,250
Salinas Municipal Airport	Design - Enhance Runway Safety (Rwy 13-31 RSA Standards)	AIP	2031	\$ 395,483	\$ 19,774	\$ 24,168	\$ 439,425
Salinas Municipal Airport	Design - Enhance Rwy 8-26 RSA Shift Rwy 8-26	AIP	2031	\$ 1,371,431	\$ 68,572	\$ 83,810	\$ 1,523,812
Salinas Municipal Airport	Design - East Side Hangar Area	AIP	2032	\$ 381,780	\$ 19,089	\$ 23,331	\$ 424,200
Salinas Municipal Airport	Design - East Side Hangar Area Site Prep	AIP	2032	\$ 258,977	\$ 12,949	\$ 15,826	\$ 287,752
Salinas Municipal Airport	Design - East Side Taxiways and Access	LFP	2032	\$ -	\$ -	\$ 609,000	\$ 609,000
Salinas Municipal Airport	Construction - East Side Site Prep P1	AIP	2033	\$ 1,416,083	\$ 70,804	\$ 86,538	\$ 1,573,425
Salinas Municipal Airport	Construction - Enhance Rwy 8-26 RSA Shift Rwy 8-26	AIP	2033	\$ 12,757,500	\$ 637,875	\$ 779,625	\$ 14,175,000
Salinas Municipal Airport	Construction - East Side Site Preparation P2	AIP	2034	\$ 1,463,285	\$ 73,164	\$ 89,423	\$ 1,625,872
Samoa Field Airport	Removal/Pruning Willow Stand	A&D	2026	\$ -	\$ 37,800	\$ 4,200	\$ 42,000
Samoa Field Airport	Design Ten T Hangars	A&D	2028	\$ -	\$ 270,000	\$ 30,000	\$ 300,000
Samoa Field Airport	Construct 10 T Hangars	A&D	2029	\$ -	\$ 2,700,000	\$ 300,000	\$ 3,000,000
Samoa Field Airport	Construct Security Fencing	A&D	2030	\$ -	\$ 139,500	\$ 15,500	\$ 155,000
Samoa Field Airport	Install Runway Lights	A&D	2030	\$ -	\$ 495,000	\$ 55,000	\$ 550,000
San Bernardino International Airport	Pavement Management Plan Update	AIP	2025	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
San Bernardino International Airport	Reconstruct Portion of Taxiway A	AIP	2025	\$ 2,880,000	\$ 144,000	\$ 176,000	\$ 3,200,000
San Bernardino International Airport	Slurry Seal and Restripe Airfield	AIP	2025	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
San Bernardino International Airport	ARFF Response Vehicle	AIP	2026	\$ 990,000	\$ 49,500	\$ 60,500	\$ 1,100,000
San Bernardino International Airport	Construct Heliport	AIP	2026	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
San Bernardino International Airport	Reconstruct Aircraft Ramp	AIP	2026	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
San Bernardino International Airport	Runway 24 Stopway Reconstruction Project	AIP	2026	\$ 4,320,000	\$ 216,000	\$ 264,000	\$ 4,800,000
San Bernardino International Airport	Taxiway C Reconstruction Project	AIP	2027	\$ 10,158,750	\$ 507,938	\$ 620,813	\$ 11,287,500
San Bernardino International Airport	Apron Rehabilitation Project	AIP	2028	\$ 3,240,000	\$ 162,000	\$ 198,000	\$ 3,600,000
San Bernardino International Airport	Master Plan	AIP	2028	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
San Bernardino International Airport	Taxiway B Reconstruction Project	AIP	2028	\$ 9,261,000	\$ 463,050	\$ 565,950	\$ 10,290,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
San Bernardino International Airport	EIS for Second Runway	AIP	2029	\$ 2,250,000	\$ 112,500	\$ 137,500	\$ 2,500,000
San Bernardino International Airport	Construct Taxiway to Corporate Hangars	AIP	2030	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
San Bernardino International Airport	EA for Extension of Runway 6/24 and Taxiway ""A""	AIP	2031	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
San Bernardino International Airport	General Aviation Runway and Connecting Taxiways	AIP	2032	\$ 46,800,000	\$ 2,340,000	\$ 2,860,000	\$ 52,000,000
San Carlos Airport	Twys Reconfiguration and Electrical Improvements (Cons.)	AIP	2025	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
San Carlos Airport	Transient Parking Ramp Rehab (Design and Construction)	AIP	2025	\$ 648,000	\$ 32,400	\$ 39,600	\$ 720,000
San Carlos Airport	Hotel Aircraft Ramp and Taxilane Rehab (Design)	AIP	2026	\$ 76,500	\$ 3,825	\$ 4,675	\$ 85,000
San Carlos Airport	North Helicopter Ramp Expansion (Design)	AIP	2027	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000
San Carlos Airport	Hotel Aircraft Ramp and Taxilane Rehab (Construction)	AIP	2027	\$ 676,800	\$ 33,840	\$ 41,360	\$ 752,000
San Carlos Airport	North Helicopter Ramp Expansion (Construction)	AIP	2028	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
San Carlos Airport	Taxilane and Ramp Rehabilitation (Design)	AIP	2031	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
San Carlos Airport	Taxilane and Ramp Rehabilitation (Construction)	AIP	2032	\$ 630,000	\$ 31,500	\$ 38,500	\$ 700,000
San Carlos Airport	Airport Entrance Road Rehabilitation (Design)	AIP	2033	\$ 157,500	\$ 7,875	\$ 9,625	\$ 175,000
San Carlos Airport	Airport Entrance Road Rehabilitation (Construction)	AIP	2034	\$ 247,500	\$ 12,375	\$ 15,125	\$ 275,000
San Diego International Airport	Airport Development Plan Airside Improv. - Phase 4/4A	AIP	2025	\$ 32,806,498	\$ -	\$ 17,198,151	\$ 50,004,649
San Diego International Airport	Electrical Infrastructure Improvements	AIP	2025	\$ 763,483	\$ -	\$ 2,636,517	\$ 3,400,000
San Diego International Airport	Residential Sound Attenuation	AIP	2025	\$ 15,000,000	\$ -	\$ 3,750,000	\$ 18,750,000
San Diego International Airport	Sidewalk and Crosswalk Improvements	AIP	2025	\$ 3,656,190	\$ -	\$ 1,218,730	\$ 4,874,920
San Diego International Airport	Terminal Building Structural Systems-Phase 1B	AIP	2025	\$ 12,097,251	\$ -	\$ 31,902,749	\$ 44,000,000
San Diego International Airport	Airside Development Plan Airside Improvements-Phase 5	AIP	2026	\$ 10,000,000	\$ -	\$ 3,333,334	\$ 13,333,334
San Diego International Airport	Part 150 Noise Exposure Map Update	AIP	2026	\$ 375,000	\$ -	\$ 125,000	\$ 500,000
San Diego International Airport	Replace ARFF Station	AIP	2026	\$ 22,500,000	\$ -	\$ 7,500,000	\$ 30,000,000
San Diego International Airport	Replace ARFF Vehicles	AIP	2026	\$ 2,250,000	\$ -	\$ 750,000	\$ 3,000,000
San Diego International Airport	Residential Sound Attenuation	AIP	2026	\$ 15,000,000	\$ -	\$ 3,750,000	\$ 18,750,000
San Diego International Airport	Restroom Upgrades T2E	AIP	2026	\$ 5,250,000	\$ -	\$ 1,750,000	\$ 7,000,000
San Diego International Airport	T2E Connector	AIP	2026	\$ 72,806,498	\$ -	\$ 107,193,502	\$ 180,000,000
San Diego International Airport	ZEV - Acquire RCC Buses Phase 1	AIP	2026	\$ 3,000,000	\$ -	\$ 3,000,000	\$ 6,000,000
San Diego International Airport	Airside Development Plan Airside Improvements-Phase 6	AIP	2027	\$ 10,000,000	\$ -	\$ 3,333,334	\$ 13,333,334
San Diego International Airport	Reconstruct/Relocate Taxiways B7, B8, and D	AIP	2027	\$ 17,471,059	\$ -	\$ 5,823,687	\$ 23,294,746
San Diego International Airport	Residential Sound Attenuation	AIP	2027	\$ 15,000,000	\$ -	\$ 3,750,000	\$ 18,750,000
San Diego International Airport	ZEV - Acquire RCC Buses Phase 2	AIP	2027	\$ 3,000,000	\$ -	\$ 3,000,000	\$ 6,000,000
San Diego International Airport	Airside Development Plan Airside Improvements-Phase 7	AIP	2028	\$ 10,000,000	\$ -	\$ 3,333,334	\$ 13,333,334

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
San Diego International Airport	Central Utility Plant Electrification	AIP	2028	\$ 11,707,831	\$ -	\$ 3,902,611	\$ 15,610,442
San Diego International Airport	Reconstruct/Relocate Taxiways B9 and B10	AIP	2028	\$ 6,330,546	\$ -	\$ 2,110,183	\$ 8,440,729
San Diego International Airport	Residential Sound Attenuation	AIP	2028	\$ 12,000,000	\$ -	\$ 3,000,000	\$ 15,000,000
San Diego International Airport	ZEV - Acquire Transit Buses - Phase 1	AIP	2028	\$ 2,750,000	\$ -	\$ 2,750,000	\$ 5,500,000
San Diego International Airport	Airside Development Plan Airside Improvements-Phase 8	AIP	2029	\$ 10,000,000	\$ -	\$ 3,333,334	\$ 13,333,334
San Diego International Airport	Construct Northside Apron Improvements	AIP	2029	\$ 26,043,659	\$ -	\$ 8,681,341	\$ 34,725,000
San Diego International Airport	Residential Sound Attenuation	AIP	2029	\$ 12,000,000	\$ -	\$ 3,000,000	\$ 15,000,000
San Diego International Airport	Energy Sustainability Improvements	AIP	2029	\$ 54,075,000	\$ -	\$ 48,925,000	\$ 103,000,000
San Diego International Airport	Airside Development Plan Airside Improvements - Phase 9	AIP	2030	\$ 15,000,000	\$ -	\$ 5,000,000	\$ 20,000,000
San Diego International Airport	Reconstruct Taxiway F	AIP	2030	\$ 12,375,000	\$ -	\$ 4,125,000	\$ 16,500,000
San Diego International Airport	Residential Sound Attenuation	AIP	2030	\$ 12,000,000	\$ -	\$ 3,000,000	\$ 15,000,000
San Diego International Airport	ZEV - Acquire Transit Buses - Phase 2	AIP	2030	\$ 2,750,000	\$ -	\$ 2,750,000	\$ 5,500,000
San Diego International Airport	Airside Development Plan Airside Improvements - Phase 10	AIP	2031	\$ 15,000,000	\$ -	\$ 5,000,000	\$ 20,000,000
San Francisco International Airport	Electric Bus Procurement	AIP	2025	\$ 7,110,000	\$ -	\$ 790,000	\$ 7,900,000
San Francisco International Airport	Noise Insulation Program Pre-1993 Expansion	AIP	2025	\$ 10,800,000	\$ -	\$ 1,200,000	\$ 12,000,000
San Francisco International Airport	Electric Bus Procurement	AIP	2026	\$ 7,110,000	\$ -	\$ 790,000	\$ 7,900,000
San Francisco International Airport	eGSE Chargers Phase 2 (Boarding Area C)	AIP	2026	\$ 450,000	\$ -	\$ 50,000	\$ 500,000
San Francisco International Airport	eGSE Chargers, Phase 3 (Boarding Area A)	AIP	2026	\$ 450,000	\$ -	\$ 50,000	\$ 500,000
San Francisco International Airport	eGSE Chargers Phase 1 (Boarding Area A)	AIP	2027	\$ 9,900,000	\$ -	\$ 1,100,000	\$ 11,000,000
San Francisco International Airport	Noise Insulation Program 2023-2027	AIP	2027	\$ 10,800,000	\$ -	\$ 1,200,000	\$ 12,000,000
San Francisco International Airport	Runway 1R-19L Rehabilitation	AIP	2027	\$ 135,000,000	\$ -	\$ 15,000,000	\$ 150,000,000
San Francisco International Airport	Taxiway E and J Realignment	AIP	2027	\$ 60,750,000	\$ -	\$ 6,750,000	\$ 67,500,000
San Francisco International Airport	Taxiway Z Reconstruction Phase I	AIP	2027	\$ 23,625,000	\$ -	\$ 2,625,000	\$ 26,250,000
San Francisco International Airport	Airport Beacon Replacement	AIP	2028	\$ 10,800,000	\$ -	\$ 1,200,000	\$ 12,000,000
San Francisco International Airport	Noise Insulation Program 2024-2028	AIP	2028	\$ 9,000,000	\$ -	\$ 1,000,000	\$ 10,000,000
San Francisco International Airport	Taxiways A and B Rehabilitation - Phase 2	AIP	2028	\$ 45,000,000	\$ -	\$ 5,000,000	\$ 50,000,000
San Francisco International Airport	Taxiway C Reconstruction	AIP	2029	\$ 55,800,000	\$ -	\$ 6,200,000	\$ 62,000,000
San Francisco International Airport	Taxiway F and N Reconstruction	AIP	2029	\$ 40,500,000	\$ -	\$ 4,500,000	\$ 45,000,000
San Gabriel Valley (El Monte) Airport	Pavement Repairs and Maintenance - Crack Sealing/Patching	AIP	2025	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
San Gabriel Valley (El Monte) Airport	Airport Land Use Compatibility Plan [ALUCP]	A&D	2026	\$ -	\$ 270,000	\$ 30,000	\$ 300,000
San Gabriel Valley (El Monte) Airport	Runway and Taxiway Reconstruction - [Construction]	AIP	2026	\$ 17,100,000	\$ 855,000	\$ 1,045,000	\$ 19,000,000
San Gabriel Valley (El Monte) Airport	Rehabilitation of South Apron [Design]	AIP	2027	\$ 153,000	\$ 7,650	\$ 9,350	\$ 170,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
San Gabriel Valley (El Monte) Airport	Replace Perimeter Fencing	AIP	2028	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
San Gabriel Valley (El Monte) Airport	Rehabilitation of South Apron - Construction	AIP	2029	\$ 1,980,000	\$ 99,000	\$ 121,000	\$ 2,200,000
San Jose International Airport, Norman Y. Mineta	Airfield Pavement Condition Study	AIP	2025	\$ 360,000	\$ -	\$ 40,000	\$ 400,000
San Jose International Airport, Norman Y. Mineta	Terminal B Expansion Ramp Ph4 and Ph5	AIP	2025	\$ 18,036,000	\$ -	\$ 2,004,000	\$ 20,040,000
San Jose International Airport, Norman Y. Mineta	Terminal B Expansion Ramp Ph6	AIP	2025	\$ 9,558,000	\$ -	\$ 1,062,000	\$ 10,620,000
San Jose International Airport, Norman Y. Mineta	Airside High Mast Lighting	AIP	2026	\$ 1,530,000	\$ -	\$ 170,000	\$ 1,700,000
San Jose International Airport, Norman Y. Mineta	Taxiway V Phase 3A	AIP	2026	\$ 14,805,720	\$ -	\$ 1,645,080	\$ 16,450,800
San Jose International Airport, Norman Y. Mineta	Zero Emissions Buses	AIP	2026	\$ 4,950,000	\$ -	\$ 550,000	\$ 5,500,000
San Jose International Airport, Norman Y. Mineta	Aircraft Rescue and Fire Fighting Apparatus	AIP	2027	\$ 2,700,000	\$ -	\$ 300,000	\$ 3,000,000
San Jose International Airport, Norman Y. Mineta	Electric Vehicle Chargers	AIP	2027	\$ 1,350,000	\$ -	\$ 150,000	\$ 1,500,000
San Jose International Airport, Norman Y. Mineta	Pavement Management Plan	AIP	2027	\$ 360,000	\$ -	\$ 40,000	\$ 400,000
San Jose International Airport, Norman Y. Mineta	Perimeter Fence Line Upgrades	AIP	2027	\$ 1,800,000	\$ -	\$ 200,000	\$ 2,000,000
San Jose International Airport, Norman Y. Mineta	Southwest Quadrant Apron Replacement	AIP	2027	\$ 853,200	\$ -	\$ 94,800	\$ 948,000
San Jose International Airport, Norman Y. Mineta	Taxiway V Phase 3B	AIP	2027	\$ 11,873,430	\$ -	\$ 1,319,270	\$ 13,192,700
San Jose International Airport, Norman Y. Mineta	Terminal A Exit Lanes	AIP	2027	\$ 2,466,000	\$ -	\$ 274,000	\$ 2,740,000
San Jose International Airport, Norman Y. Mineta	Aircraft Rescue and Fire Fighting Apparatus	AIP	2028	\$ 225,000	\$ -	\$ 25,000	\$ 250,000
San Jose International Airport, Norman Y. Mineta	Taxiway B Pavement Painting	AIP	2028	\$ 6,345,000	\$ -	\$ 705,000	\$ 7,050,000
San Jose International Airport, Norman Y. Mineta	Taxiway F Closure	AIP	2028	\$ 2,880,000	\$ -	\$ 320,000	\$ 3,200,000
San Jose International Airport, Norman Y. Mineta	Taxiway H Closure	AIP	2028	\$ 2,587,500	\$ -	\$ 287,500	\$ 2,875,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
San Jose International Airport, Norman Y. Mineta	Taxiway L Pavement Painting	AIP	2028	\$ 6,345,000	\$ -	\$ 705,000	\$ 7,050,000
San Jose International Airport, Norman Y. Mineta	Blast Pad Improvements	AIP	2029	\$ 6,480,000	\$ -	\$ 720,000	\$ 7,200,000
San Jose International Airport, Norman Y. Mineta	Design for Runway 12R-30L Hold Bar Relocations	AIP	2029	\$ 1,530,000	\$ -	\$ 170,000	\$ 1,700,000
San Jose International Airport, Norman Y. Mineta	Pavement Management Plan	AIP	2029	\$ 360,000	\$ -	\$ 40,000	\$ 400,000
San Jose International Airport, Norman Y. Mineta	Runway 12R-30L Hold Bar Relocations	AIP	2029	\$ 6,966,000	\$ -	\$ 774,000	\$ 7,740,000
San Jose International Airport, Norman Y. Mineta	Taxiway Designation Study	AIP	2029	\$ 157,500	\$ -	\$ 17,500	\$ 175,000
San Jose International Airport, Norman Y. Mineta	Construction: ADG-III taxi lane west of Taxiway V	AIP	2030	\$ 6,480,000	\$ -	\$ 720,000	\$ 7,200,000
San Jose International Airport, Norman Y. Mineta	Design: ADG-III Taxilane West of Taxiway V	AIP	2030	\$ 1,980,000	\$ -	\$ 220,000	\$ 2,200,000
San Jose International Airport, Norman Y. Mineta	Design: Realign Taxiways B, D, H, L Between Taxiways Y and Z	AIP	2030	\$ 1,665,000	\$ -	\$ 185,000	\$ 1,850,000
San Jose International Airport, Norman Y. Mineta	Realign Taxiways B, D, H, and L Between Taxiways Y and Z	AIP	2030	\$ 6,210,000	\$ -	\$ 690,000	\$ 6,900,000
San Luis Obispo County Regional Airport	Construct Taxiway B3 (C South) to GA Apron	AIP	2025	\$ 2,880,000	\$ -	\$ 320,000	\$ 3,200,000
San Luis Obispo County Regional Airport	PFAS/F3 Transition/AFFF Disposal	AIP	2025	\$ 315,000	\$ -	\$ 35,000	\$ 350,000
San Luis Obispo County Regional Airport	Reimbursable Agreement - Conduit Work for Taxiway B3	AIP	2025	\$ 6,546	\$ -	\$ 727	\$ 7,273
San Luis Obispo County Regional Airport	Reimbursable Agreement - Taxiways A and B3 Phasing Cons.	AIP	2025	\$ 67,500	\$ -	\$ 7,500	\$ 75,000
San Luis Obispo County Regional Airport	Taxiway A Mill and Overlay (Zones 1, 2 and Design and Construct	AIP	2025	\$ 8,775,000	\$ -	\$ 975,000	\$ 9,750,000
San Luis Obispo County Regional Airport	Taxiway Connectors Redesignation - Design and Construct	AIP	2025	\$ 675,000	\$ -	\$ 75,000	\$ 750,000
San Luis Obispo County Regional Airport	Environmental Assessment - Taxiway A Zone 2 Shift Enabling	AIP	2026	\$ 1,170,000	\$ -	\$ 130,000	\$ 1,300,000
San Luis Obispo County Regional Airport	Outbound Baggage Handling System Expansion	AIP	2026	\$ 6,865,020	\$ -	\$ 762,780	\$ 7,627,800
San Luis Obispo County Regional Airport	Wildlife Hazard Assessment Wildlife Management Plan	AIP	2026	\$ 157,500	\$ -	\$ 17,500	\$ 175,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
San Luis Obispo County Regional Airport	Demolish Old Terminal and Other - Phase I	AIP	2027	\$ 2,934,000	\$ -	\$ 326,000	\$ 3,260,000
San Luis Obispo County Regional Airport	Replace/Install Electrical Vault - Design and Construct	AIP	2027	\$ 2,880,000	\$ -	\$ 320,000	\$ 3,200,000
San Luis Obispo County Regional Airport	Building Acquisition	AIP	2028	\$ 1,620,000	\$ -	\$ 180,000	\$ 1,800,000
San Luis Obispo County Regional Airport	PAPI Implementation on Runway 11-2 Design and Construct	AIP	2028	\$ 1,287,000	\$ -	\$ 143,000	\$ 1,430,000
San Luis Obispo County Regional Airport	Runway 11-29 Sealcoat Design and Cons.	AIP	2028	\$ 2,880,000	\$ -	\$ 320,000	\$ 3,200,000
San Luis Obispo County Regional Airport	Land Acquisition for Mini Storage Property	AIP	2029	\$ 22,500,000	\$ -	\$ 2,500,000	\$ 25,000,000
San Luis Obispo County Regional Airport	Remove Airport Restaurant (Enabling Safety Project)	AIP	2030	\$ 693,000	\$ -	\$ 77,000	\$ 770,000
San Luis Obispo County Regional Airport	Taxiway A Realignment (Zone - Design)	AIP	2030	\$ 26,460,000	\$ -	\$ 2,940,000	\$ 29,400,000
San Martin Airport	Environmental and Design - Access Road	AIP	2025	\$ 98,307	\$ 4,915	\$ 6,008	\$ 109,230
San Martin Airport	Environmental and Design - North Taxiway X Realignment	AIP	2025	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
San Martin Airport	Environmental and Des. and Cons. - AWOS Replacement	AIP	2025	\$ 445,410	\$ 22,271	\$ 27,220	\$ 494,900
San Martin Airport	Construction - Access Road	AIP	2026	\$ 663,775	\$ 33,189	\$ 40,564	\$ 737,528
San Martin Airport	Construction - North Taxiway X Realignment	AIP	2026	\$ 1,669,536	\$ 83,477	\$ 102,027	\$ 1,855,040
San Martin Airport	Environmental and Design - Lighting and Signage Repair	AIP	2027	\$ 564,300	\$ 28,215	\$ 34,485	\$ 627,000
San Martin Airport	APMS Update	AIP	2028	\$ 69,152	\$ 3,458	\$ 4,226	\$ 76,835
San Martin Airport	Construction - Lighting and Signage Repair Base Bid	AIP	2029	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
San Martin Airport	Construction - Lighting and Signage Repair	AIP	2030	\$ 6,277,356	\$ 313,868	\$ 383,616	\$ 6,974,840
San Martin Airport	Master Plan Update (Including AGIS and CEQA)	AIP	2030	\$ 751,500	\$ 37,575	\$ 45,925	\$ 835,000
San Martin Airport	Design - North Parking Ramp Rehabilitation	AIP	2031	\$ 137,340	\$ 6,867	\$ 8,393	\$ 152,600
San Martin Airport	Construction - N Parking Ramp (APB-00 Rehabilitation)	AIP	2032	\$ 2,005,164	\$ 100,258	\$ 122,538	\$ 2,227,960
San Martin Airport	Design - N Parking Ramp (Rehabilitation)	AIP	2033	\$ 99,900	\$ 4,995	\$ 6,105	\$ 111,000
San Martin Airport	Construction - N Parking Ramp (Rehabilitation)	AIP	2034	\$ 1,458,540	\$ 72,927	\$ 89,133	\$ 1,620,600
San Martin Airport	Design and Construct Pavement Reconstruction	AIP	2034	\$ 494,208	\$ 24,710	\$ 30,202	\$ 549,120
San Martin Airport	Design and Construct Perimeter Fencing	AIP	2034	\$ 3,097,224	\$ 154,861	\$ 189,275	\$ 3,441,360
San Martin Airport	Rotating Beacon Replacement	AIP	2034	\$ 204,984	\$ 10,249	\$ 12,527	\$ 227,760
Santa Barbara Municipal Airport	Building 257 Apron and Hangar 4 Apron Rehab	AIP	2025	\$ 247,500	\$ -	\$ 27,500	\$ 275,000
Santa Barbara Municipal Airport	Taxiway E, A3, A5 Rehabilitation	AIP	2025	\$ 4,905,000	\$ -	\$ 545,000	\$ 5,450,000
Santa Barbara Municipal Airport	Terminal Addition	AIP	2025	\$ 4,776,827	\$ -	\$ 530,759	\$ 5,307,585
Santa Barbara Municipal Airport	ARFF Vehicle Replacement	LFP	2026	\$ -	\$ -	\$ 1,500,000	\$ 1,500,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Santa Barbara Municipal Airport	Northwest Apron Repairs	AIP	2026	\$ 900,000	\$ -	\$ 100,000	\$ 1,000,000
Santa Barbara Municipal Airport	Taxiway A, B, C Rehabilitation	AIP	2026	\$ 702,000	\$ -	\$ 78,000	\$ 780,000
Santa Barbara Municipal Airport	Taxiway B Extension	AIP	2026	\$ 3,702,600	\$ -	\$ 411,400	\$ 4,114,000
Santa Barbara Municipal Airport	Taxiway B Extension	AIP	2027	\$ 11,642,400	\$ -	\$ 1,293,600	\$ 12,936,000
Santa Barbara Municipal Airport	Taxiway B, C Rehabilitation	AIP	2027	\$ 2,700,000	\$ -	\$ 300,000	\$ 3,000,000
Santa Barbara Municipal Airport	Terminal Addition	AIP	2027	\$ 36,000,000	\$ -	\$ 4,000,000	\$ 40,000,000
Santa Barbara Municipal Airport	South Terminal Apron Rehabilitation	AIP	2028	\$ 3,078,000	\$ -	\$ 342,000	\$ 3,420,000
Santa Barbara Municipal Airport	Taxiway A Rehabilitation	AIP	2028	\$ 2,520,000	\$ -	\$ 280,000	\$ 2,800,000
Santa Barbara Municipal Airport	Terminal Curbside Improvements	AIP	2029	\$ 67,500	\$ -	\$ 7,500	\$ 75,000
Santa Barbara Municipal Airport	Runway 15L-33R Closure	AIP	2030	\$ 190,800	\$ -	\$ 21,200	\$ 212,000
Santa Maria Public Airport	Rehabilitate Runway 12-30 (Construct and Reimburse Design)	AIP	2025	\$ 10,179,000	\$ -	\$ 1,131,000	\$ 11,310,000
Santa Maria Public Airport	Rehab segment Twy A, Remove select Twys (Const.)	AIP	2026	\$ 8,100,000	\$ -	\$ 900,000	\$ 9,000,000
Santa Maria Public Airport	Rehabilitate Taxiways A, A6, S, T, and U (Cons.)	AIP	2026	\$ 10,800,000	\$ -	\$ 1,200,000	\$ 12,000,000
Santa Maria Public Airport	Rehab Twy A, A2-A6, Include Geometry upgrades (design)	AIP	2027	\$ 990,000	\$ -	\$ 110,000	\$ 1,100,000
Santa Maria Public Airport	Rehab Twy A, A2-A6, Include Geometry Upgrades (Cons.)	AIP	2029	\$ 8,100,000	\$ -	\$ 900,000	\$ 9,000,000
Santa Maria Public Airport	Extend Taxiway B from Taxiway E to Taxiway B7 (Design)	AIP	2031	\$ 576,000	\$ -	\$ 64,000	\$ 640,000
Santa Maria Public Airport	Rehab Terminal Apron, Ph 3 (construct and Reimburse Design)	AIP	2031	\$ 4,050,000	\$ -	\$ 450,000	\$ 4,500,000
Santa Maria Public Airport	Extend Taxiway B from Taxiway E to Taxiway B7 (Cons.)	AIP	2032	\$ 9,000,000	\$ -	\$ 1,000,000	\$ 10,000,000
Santa Maria Public Airport	Rehabilitate Main Hangar Apron (Design)	AIP	2033	\$ 405,000	\$ -	\$ 45,000	\$ 450,000
Santa Maria Public Airport	Rehabilitate Main Hangar Apron (Construction)	AIP	2034	\$ 5,760,000	\$ -	\$ 640,000	\$ 6,400,000
Santa Ynez Airport	Reseal Aprons and Taxilanes (Cons.) - BIL	AIP	2025	\$ 657,000	\$ 32,850	\$ 40,150	\$ 730,000
Santa Ynez Airport	Rehab Runway 8-26: Crackfill, Overlay, Markings (Design)	AIP	2025	\$ 166,500	\$ 8,325	\$ 10,175	\$ 185,000
Santa Ynez Airport	Reseal Taxiways: Crackfill, Slurry, Markings (Design)	AIP	2025	\$ 81,000	\$ 4,050	\$ 4,950	\$ 90,000
Santa Ynez Airport	Reseal Aprons and Taxilanes	AIP	2026	\$ 149,200	\$ 7,460	\$ 9,118	\$ 165,778
Santa Ynez Airport	Rehab Runway 8-26: Crackfill, Overlay, Markings (Cons.)	AIP	2026	\$ 2,340,000	\$ 117,000	\$ 143,000	\$ 2,600,000
Santa Ynez Airport	Reseal Taxiways: Crackfill, Slurry, Markings (Cons.)	AIP	2026	\$ 558,000	\$ 27,900	\$ 34,100	\$ 620,000
Santa Ynez Airport	Perimeter Fence Improvements (Design)	AIP	2028	\$ 144,000	\$ 7,200	\$ 8,800	\$ 160,000
Santa Ynez Airport	Perimeter Fence Improvements (Cons.)	AIP	2029	\$ 1,170,000	\$ 58,500	\$ 71,500	\$ 1,300,000
Santa Ynez Airport	Pole-Mounted Apron Lights (Construct and Reimburse Design)	AIP	2030	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Santa Ynez Airport	Rehabilitate Access Road and Parking (Design)	AIP	2031	\$ 162,000	\$ 8,100	\$ 9,900	\$ 180,000
Santa Ynez Airport	Rehabilitate Access Road and Parking (Cons.)	AIP	2032	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Santa Ynez Airport	Fuel facility Upgrades (Construct and Reimburse Design)	AIP	2034	\$ 1,980,000	\$ 99,000	\$ 121,000	\$ 2,200,000
Scott Valley Airport - Bud Davis Field	ALP Update with Aeronautical Survey	AIP	2025	\$ 301,263	\$ 15,063	\$ 18,411	\$ 334,737

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Scott Valley Airport - Bud Davis Field	Rehab Taxiway Turnarounds	AIP	2027	\$ 142,105	\$ 7,105	\$ 8,684	\$ 157,895
Scott Valley Airport - Bud Davis Field	Taxiway Turnarounds (Phase Construction Phase)	AIP	2028	\$ 1,272,000	\$ 63,600	\$ 77,733	\$ 1,413,333
Sequoia Field Airport	Runway 13-31 and Exits A, C, and E Rehabilitation (Cons.)	AIP	2025	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Sequoia Field Airport	AVGAS Fuel Island (Design)	AIP	2030	\$ 27,000	\$ 1,350	\$ 1,650	\$ 30,000
Sequoia Field Airport	12000 Gal. AVGAS Fuel Island (Construction)	AIP	2031	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Sequoia Field Airport	16 Unit Nested T-Hangar Building (Design)	AIP	2032	\$ 81,000	\$ 4,050	\$ 4,950	\$ 90,000
Sequoia Field Airport	16 Unit Nested T-Hangar Buildings (Construction)	AIP	2033	\$ 720,000	\$ 36,000	\$ 44,000	\$ 800,000
Sequoia Field Airport	Airport Layout Plan Update with ArcGIS Survey	AIP	2034	\$ 171,000	\$ 8,550	\$ 10,450	\$ 190,000
Sequoia Field Airport	Airport Master Plan	AIP	2034	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Sequoia Field Airport	Apron and Taxilane Rehabilitation (Construction)	AIP	2034	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Sequoia Field Airport	Apron and Taxilane Rehabilitation (Design)	AIP	2034	\$ 36,000	\$ 1,800	\$ 2,200	\$ 40,000
Sequoia Field Airport	Parallel Taxiway Rehabilitation (Construction)	AIP	2034	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Sequoia Field Airport	Parallel Taxiway Rehabilitation (Design)	AIP	2034	\$ 18,000	\$ 900	\$ 1,100	\$ 20,000
Shafter Airport - Minter Field	Eng. Concept Budget Report and Enviro. - Rehab of Rwy 17-35	AIP	2025	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Shafter Airport - Minter Field	Design of Rehabilitation/Reconstruction of Runway 17-35	AIP	2026	\$ 445,500	\$ 22,275	\$ 27,225	\$ 495,000
Shafter Airport - Minter Field	Construction of Rehabilitation/Reconstruction of Rwy 17-35	AIP	2028	\$ 7,087,500	\$ 354,375	\$ 433,125	\$ 7,875,000
Shafter Airport - Minter Field	EA for Property Acquisition for Rwy 12-30 Extension	AIP	2029	\$ 378,000	\$ 18,900	\$ 23,100	\$ 420,000
Shafter Airport - Minter Field	Design of Rehabilitation of GA Apron North	AIP	2030	\$ 347,288	\$ 17,364	\$ 21,223	\$ 385,875
Shafter Airport - Minter Field	Construction of Rehabilitation of GA Apron North	AIP	2032	\$ 3,722,355	\$ 186,118	\$ 227,477	\$ 4,135,950
Shelter Cove Airport	Airport Layout Plan	A&D	2026	\$ -	\$ 58,500	\$ 6,500	\$ 65,000
Shelter Cove Airport	Hangers Feasibility Study	A&D	2027	\$ -	\$ 45,000	\$ 5,000	\$ 50,000
Shelter Cove Airport	Improve Drainage Planning South and Central Areas	A&D	2027	\$ -	\$ 315,000	\$ 35,000	\$ 350,000
Shelter Cove Airport	Runway and Taxiway Markings	AIP	2027	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Shelter Cove Airport	Runway Overlay	AIP	2027	\$ 1,575,000	\$ 78,750	\$ 96,250	\$ 1,750,000
Shelter Cove Airport	Tiedown Area Paving Planning South and North	A&D	2027	\$ -	\$ 45,000	\$ 5,000	\$ 50,000
Shelter Cove Airport	Improve Drainage Construction Central Area	A&D	2028	\$ -	\$ 225,000	\$ 25,000	\$ 250,000
Shelter Cove Airport	Improve Drainage Construction South Area	A&D	2028	\$ -	\$ 247,500	\$ 27,500	\$ 275,000
Shelter Cove Airport	Outside Public Restroom	A&D	2028	\$ -	\$ 72,000	\$ 8,000	\$ 80,000
Shelter Cove Airport	Pilots Lounge	A&D	2028	\$ -	\$ 90,000	\$ 10,000	\$ 100,000
Shelter Cove Airport	South Tiedown Area Paving Construction	AIP	2028	\$ 675,000	\$ 33,750	\$ 41,250	\$ 750,000
Shelter Cove Airport	North Tiedown Area Paving Construction	AIP	2029	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Shelter Cove Airport	Taxiway Realignment Planning	A&D	2030	\$ -	\$ 270,000	\$ 30,000	\$ 300,000
Shelter Cove Airport	Taxiway Realignment Construction	AIP	2031	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Shelter Cove Airport	Ten Space Pilot's Parking Lot Planning and Design	A&D	2032	\$ -	\$ 36,000	\$ 4,000	\$ 40,000
Shelter Cove Airport	Ten Space Pilot's Parking Lot Construction	A&D	2033	\$ -	\$ 180,000	\$ 20,000	\$ 200,000
Shoshone Airport	Replace Runway Lighting Control System	A&D	2026	\$ -	\$ 31,500	\$ 3,500	\$ 35,000
Shoshone Airport	Rehabilitate Rwy 15-33	A&D	2028	\$ -	\$ 900,000	\$ 100,000	\$ 1,000,000
Shoshone Airport	Replace Segmented Circle	A&D	2030	\$ -	\$ 29,700	\$ 3,300	\$ 33,000
Sierraville Dearwater Airport	Widen Runway to 60 feet	A&D	2025	\$ -	\$ 450,000	\$ 50,000	\$ 500,000
Sierraville Dearwater Airport	Construct Turn Around Rwy 3	A&D	2026	\$ -	\$ 49,500	\$ 5,500	\$ 55,000
Sierraville Dearwater Airport	New Parallel Taxiway-One Half Length	A&D	2026	\$ -	\$ 315,000	\$ 35,000	\$ 350,000
Sierraville Dearwater Airport	Land Acquisition for Aviation Easement	A&D	2027	\$ -	\$ 180,000	\$ 20,000	\$ 200,000
Sierraville Dearwater Airport	New Parallel Taxiway-One Half Length	A&D	2027	\$ -	\$ 297,000	\$ 33,000	\$ 330,000
Sierraville Dearwater Airport	ALP/Master Plan	A&D	2028	\$ -	\$ 18,000	\$ 2,000	\$ 20,000
Sierraville Dearwater Airport	Reconstruct apron	A&D	2029	\$ -	\$ 405,000	\$ 45,000	\$ 450,000
Sierraville Dearwater Airport	Runway Reconstruction	A&D	2030	\$ -	\$ 1,066,500	\$ 118,500	\$ 1,185,000
Siskiyou County Airport	Reconstruct Runway 17-35 (Phase 1)	AIP	2027	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
Siskiyou County Airport	Runway 17-35 Full Depth Replacement (Phase 2)	AIP	2028	\$ 7,263,000	\$ 363,150	\$ 443,850	\$ 8,070,000
Siskiyou County Airport	Runway 17-35 Full Depth Replacement	AIP	2029	\$ 7,263,000	\$ 363,150	\$ 443,850	\$ 8,070,000
Siskiyou County Airport	LVP Approach with Obstruction Survey, MAGVAR, REILs	AIP	2030	\$ 405,000	\$ 20,250	\$ 24,750	\$ 450,000
Siskiyou County Airport	Runway 17-35 Full Depth Replacement	AIP	2030	\$ 7,263,000	\$ 363,150	\$ 443,850	\$ 8,070,000
Siskiyou County Airport	Runway 17-35 Full Depth Replacement	AIP	2030	\$ 7,263,000	\$ 363,150	\$ 443,850	\$ 8,070,000
Siskiyou County Airport	Utilities (Water, Sewer, Power)	AIP	2030	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Southard Field Airport	Design and Relocate Beacon and Reconstruct Seg. Circle	A&D	2025	\$ -	\$ 129,600	\$ 14,400	\$ 144,000
Southard Field Airport	Pavement Maintenance and Remarking: Rwy, Twy & Tiedown	A&D	2025	\$ -	\$ 149,400	\$ 16,600	\$ 166,000
Spaulding Airport	Design and Relocate Beacon and Reconstruct Seg. Circle	A&D	2025	\$ -	\$ 129,600	\$ 14,400	\$ 144,000
Spaulding Airport	Pavement Maintenance and Remarking: Rwy, Twy & Tiedown	A&D	2025	\$ -	\$ 148,500	\$ 16,500	\$ 165,000
Susanville Municipal Airport	South Apron Reconstruction (Re-Package)	AIP	2025	\$ 10,800	\$ 540	\$ 660	\$ 12,000
Susanville Municipal Airport	Taxiway C Reconstruction (Re-Package)	AIP	2025	\$ 10,800	\$ 540	\$ 660	\$ 12,000
Susanville Municipal Airport	Taxiway D Reconstruction (Design)	AIP	2025	\$ 88,200	\$ 4,410	\$ 5,390	\$ 98,000
Susanville Municipal Airport	South Apron Reconstruction (Construction)	AIP	2026	\$ 990,000	\$ 49,500	\$ 60,500	\$ 1,100,000
Susanville Municipal Airport	Taxiway C Reconstruction (Construction)	AIP	2026	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Susanville Municipal Airport	Taxiway D Reconstruction (Construction)	AIP	2026	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Susanville Municipal Airport	Runway 11-29 Sealcoat (Design)	AIP	2026	\$ 67,500	\$ 3,375	\$ 4,125	\$ 75,000
Susanville Municipal Airport	Perimeter Fence (WHA/WHMP)	AIP	2026	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Susanville Municipal Airport	Runway 11-29 Sealcoat (Construction)	AIP	2027	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Susanville Municipal Airport	Perimeter Fence (Design)	AIP	2028	\$ 85,500	\$ 4,275	\$ 5,225	\$ 95,000
Susanville Municipal Airport	Perimeter Fence (Construction)	AIP	2029	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Taft Airport	Slurry Seal, Crack Fill, and Stripe Runway	AIP	2025	\$ 482,400	\$ 24,120	\$ 29,480	\$ 536,000
Taft Airport	Conduct Master Plan on Proposed New Site	AIP	2026	\$ 428,210	\$ 21,411	\$ 26,168	\$ 475,789
Taft Airport	EA for New Airport Location	AIP	2027	\$ 202,500	\$ 10,125	\$ 12,375	\$ 225,000
Taft Airport	Design New Airport Location	AIP	2028	\$ 495,000	\$ 24,750	\$ 30,250	\$ 550,000
Tehachapi Municipal Airport	Land Acquisition (0.3 Acres)	AIP	2025	\$ 351,000	\$ 17,550	\$ 21,450	\$ 390,000
Tehachapi Municipal Airport	Reconstruct Southwest Diagonal Taxilane (Cons. and Reimb.)	AIP	2025	\$ 1,116,000	\$ 55,800	\$ 68,200	\$ 1,240,000
Tehachapi Municipal Airport	Pavement Rehabilitation, Various Locations	AIP	2026	\$ 136,800	\$ 6,840	\$ 8,360	\$ 152,000
Tehachapi Municipal Airport	Rehabilitate Runway 11-29	AIP	2028	\$ 2,520,000	\$ 126,000	\$ 154,000	\$ 2,800,000
Tehachapi Municipal Airport	Remove and Replace Above Ground Fuel Facility	AIP	2030	\$ 1,530,000	\$ 76,500	\$ 93,500	\$ 1,700,000
Tehachapi Municipal Airport	New Airport Master Plan and Airport Layout Plan	AIP	2031	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Tehachapi Municipal Airport	Northside Parallel and Connecting Taxiways	AIP	2033	\$ 2,520,000	\$ 126,000	\$ 154,000	\$ 2,800,000
Tehachapi Municipal Airport	Airport Fencing and Access Control upgrades	AIP	2034	\$ 747,000	\$ 37,350	\$ 45,650	\$ 830,000
Tracy Municipal Airport	Design - Install Backup Generator	AIP	2025	\$ 71,100	\$ 3,555	\$ 4,345	\$ 79,000
Tracy Municipal Airport	Design/Construct - Replace Airfield Guidance Sign Panels	AIP	2025	\$ 243,900	\$ 12,195	\$ 14,905	\$ 271,000
Tracy Municipal Airport	Reimbursable Agreement - Flight Check 4 REILs	AIP	2025	\$ 24,300	\$ 1,215	\$ 1,485	\$ 27,000
Tracy Municipal Airport	Construct - Install Backup Generator	AIP	2026	\$ 344,700	\$ 17,235	\$ 21,065	\$ 383,000
Tracy Municipal Airport	Design/Construct - Drainage Improvements	AIP	2026	\$ 398,700	\$ 19,935	\$ 24,365	\$ 443,000
Tracy Municipal Airport	ROFA and BRL Land Acquisition and Relocate Fence	AIP	2026	\$ 72,000	\$ 3,600	\$ 4,400	\$ 80,000
Tracy Municipal Airport	Terminal Planning Study	AIP	2026	\$ 140,400	\$ 7,020	\$ 8,580	\$ 156,000
Tracy Municipal Airport	Design - New Terminal Building (3,060 sq. ft.) with Parking	AIP	2027	\$ 341,100	\$ 17,055	\$ 20,845	\$ 379,000
Tracy Municipal Airport	Design/Construct - Perimeter Fencing	AIP	2027	\$ 246,600	\$ 12,330	\$ 15,070	\$ 274,000
Tracy Municipal Airport	Reconstruct Tee Hangar Taxilanes Hangar H	AIP	2028	\$ 179,181	\$ 8,959	\$ 10,950	\$ 199,090
Tracy Municipal Airport	Relocate Twy D, Reconfigure Apron Txln/Twy A Intersection	AIP	2029	\$ 1,054,800	\$ 52,740	\$ 64,460	\$ 1,172,000
Tracy Municipal Airport	Construct - New Terminal Building with Parking	AIP	2030	\$ 2,160,000	\$ 108,000	\$ 132,000	\$ 2,400,000
Tracy Municipal Airport	ALP Update Narrative and Plans	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Tracy Municipal Airport	ALUCP Update	AIP	2034	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Tracy Municipal Airport	Pavement Maintenance Management Plan (PMMP)	AIP	2034	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Trinity Center James E. Swett Airport	Pavement Maintenance Management Plan	AIP	2025	\$ 153,948	\$ 7,697	\$ 9,408	\$ 171,053
Trinity Center James E. Swett Airport	Runway 14-32 Reconstruction (Design)	AIP	2026	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Trinity Center James E. Swett Airport	Runway 14-32 Reconstruction (Construction)	AIP	2027	\$ 3,636,000	\$ 181,800	\$ 222,200	\$ 4,040,000
Trinity Center James E. Swett Airport	ALP Update with Narrative Report	AIP	2029	\$ 315,000	\$ 15,750	\$ 19,250	\$ 350,000
Truckee-Tahoe Airport	Design/Construct: Reconstruct Apron A2	AIP	2025	\$ 4,365,450	\$ 218,273	\$ 266,778	\$ 4,850,500

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Truckee-Tahoe Airport	Ford F-150 Lighting (ZEV Program)	AIP	2026	\$ 76,500	\$ 3,825	\$ 4,675	\$ 85,000
Truckee-Tahoe Airport	Design - Reconstruct Runway 11-29 (East)	AIP	2027	\$ 351,000	\$ 17,550	\$ 21,450	\$ 390,000
Truckee-Tahoe Airport	Widen and Extend Runway 2-20, Including FAA VASI-PAPI	AIP	2028	\$ 7,443,900	\$ 372,195	\$ 454,905	\$ 8,271,000
Truckee-Tahoe Airport	Construct - Reconstruct Runway 11-29 (East)	AIP	2029	\$ 4,895,100	\$ 244,755	\$ 299,145	\$ 5,439,000
Truckee-Tahoe Airport	Construct New Aircraft Control Tower	AIP	2030	\$ 13,500,000	\$ 675,000	\$ 825,000	\$ 15,000,000
Truckee-Tahoe Airport	Pavement Maintenance Management Plan (PMMP)	AIP	2030	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Truckee-Tahoe Airport	Design/Construct - Remove/Replace Apron A3	AIP	2031	\$ 1,569,600	\$ 78,480	\$ 95,920	\$ 1,744,000
Truckee-Tahoe Airport	Design/Construct - Remove/Replace Hangar Txlns	AIP	2031	\$ 2,096,100	\$ 104,805	\$ 128,095	\$ 2,329,000
Truckee-Tahoe Airport	Design/Construct: Reconstruct Apron A1	AIP	2031	\$ 4,266,000	\$ 213,300	\$ 260,700	\$ 4,740,000
Truckee-Tahoe Airport	Design/Construct - Relocate Twy G and Extend Associate	AIP	2032	\$ 6,920,100	\$ 346,005	\$ 422,895	\$ 7,689,000
Truckee-Tahoe Airport	Airport Layout Plan Narrative with Updated ALP	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Truckee-Tahoe Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Tulelake Airport	Crack Seal and Slurry Seal Runway, Taxiway, Apron - Des.	AIP	2025	\$ 78,300	\$ 3,915	\$ 4,785	\$ 87,000
Tulelake Airport	Construct Crack Seal and Slurry Seal Runway, Taxiway, Apron	AIP	2026	\$ 589,050	\$ 29,453	\$ 35,998	\$ 654,500
Tulelake Airport	Des. / Cons.: Construction of 8-foot Habitat/Security Fence	AIP	2027	\$ 1,322,550	\$ 66,128	\$ 80,823	\$ 1,469,500
Tulelake Airport	ALUCP Update	AIP	2032	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Tulelake Airport	ALP Update Narrative and Plans	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Tulelake Airport	Pavement Management Maintenance Plan (PMMP)	AIP	2034	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Turlock Municipal Airport	Environmental Assessment for New Runway 12-30	AIP	2025	\$ 250,200	\$ 12,510	\$ 15,290	\$ 278,000
Turlock Municipal Airport	Update Airport Master Plan	AIP	2025	\$ 180,000	\$ 9,000	\$ 11,000	\$ 200,000
Turlock Municipal Airport	Widen Runway, RSA and Infrastructure Improvements	AIP	2025	\$ 2,777,249	\$ 138,862	\$ 169,721	\$ 3,085,832
Turlock Municipal Airport	Widening Runway 12-30	AIP	2025	\$ 2,160,000	\$ 108,000	\$ 132,000	\$ 2,400,000
Turlock Municipal Airport	Runway Relocation Project	AIP	2026	\$ 2,038,500	\$ 101,925	\$ 124,575	\$ 2,265,000
Turlock Municipal Airport	Apron A1, Wind Indicator and Segmented Circle - Design	AIP	2026	\$ 172,500	\$ 8,625	\$ 10,542	\$ 191,667
Turlock Municipal Airport	Apron A1, Wind Indicator and Segmented Circle Cons.	AIP	2026	\$ 672,750	\$ 33,638	\$ 41,113	\$ 747,500
Turlock Municipal Airport	Extend Parallel Twy A Cons. 3 New Rwy/Twy Connectors (Des.)	AIP	2026	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Turlock Municipal Airport	Install MIRL, MITL and Navaids	AIP	2026	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Turlock Municipal Airport	Perimeter Fencing	AIP	2026	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Turlock Municipal Airport	Rehab Apron A1; Relocate Wind Indicator and Seg. Circle	AIP	2026	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Turlock Municipal Airport	Slurry Seal RW, Twy and Apron	AIP	2026	\$ 27,000	\$ 1,350	\$ 1,650	\$ 30,000
Turlock Municipal Airport	Widen Runway, RSA and Infrastructure Improvements Des.	AIP	2026	\$ 140,618	\$ 7,031	\$ 8,593	\$ 156,242
Turlock Municipal Airport	Widen Rwy, Grading, RSA and Infrastructure Improvements	AIP	2026	\$ 2,415,000	\$ 120,750	\$ 147,583	\$ 2,683,333
Turlock Municipal Airport	Rehab Apron A1; Relocate Wind Indicator and Seg. Circle	AIP	2027	\$ 585,000	\$ 29,250	\$ 35,750	\$ 650,000
Turlock Municipal Airport	Extend Parallel Taxi A ; 3 New Runway/Taxiway - Design	AIP	2028	\$ 163,013	\$ 8,151	\$ 9,962	\$ 181,125
Turlock Municipal Airport	Extend Parallel Twy A; Construct 3 New Rwy/Twy connectors	AIP	2029	\$ 776,250	\$ 38,813	\$ 47,438	\$ 862,500

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Twentynine Palms Airport	Safety Area Improvements - Design	AIP	2025	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Twentynine Palms Airport	Runway 8-26 Rehabilitation (Design)	AIP	2026	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Twentynine Palms Airport	ALUCP Update	A&D	2026	\$ -	\$ 270,000	\$ 30,000	\$ 300,000
Twentynine Palms Airport	Safety Area Improvements - Construction	AIP	2026	\$ 1,257,158	\$ 62,858	\$ 76,826	\$ 1,396,842
Twentynine Palms Airport	Runway 8-26 Rehabilitation (Construction)	AIP	2027	\$ 4,950,000	\$ 247,500	\$ 302,500	\$ 5,500,000
Twentynine Palms Airport	Runway 17-35 Rehabilitation (Design)	AIP	2028	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Twentynine Palms Airport	Runway 17-35 Rehabilitation (Construction)	AIP	2029	\$ 2,520,000	\$ 126,000	\$ 154,000	\$ 2,800,000
Twentynine Palms Airport	West Apron Reconstruction (Design and Construct)	AIP	2030	\$ 4,050,000	\$ 202,500	\$ 247,500	\$ 4,500,000
Twentynine Palms Airport	Airfield Electrical Upgrades, Ph I, Design/Enviro.	AIP	2031	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Twentynine Palms Airport	Airfield Electrical Upgrades Ph Construction	AIP	2032	\$ 1,350,000	\$ 67,500	\$ 82,500	\$ 1,500,000
Twentynine Palms Airport	Narrative ALP Update	AIP	2032	\$ 171,000	\$ 8,550	\$ 10,450	\$ 190,000
Twentynine Palms Airport	Safety Signs and Markings Update	A&D	2034	\$ -	\$ 270,000	\$ 30,000	\$ 300,000
Ukiah Municipal Airport	Master Plan	AIP	2026	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Ukiah Municipal Airport	Pavement Rehabilitation of Taxiways A, A3, A Design	AIP	2027	\$ 283,500	\$ 14,175	\$ 17,325	\$ 315,000
Ukiah Municipal Airport	New Taxiway A4 (Taxiway D Demolition) - Design	AIP	2027	\$ 243,000	\$ 12,150	\$ 14,850	\$ 270,000
Ukiah Municipal Airport	Pavement Rehabilitation of Twy A, A3, A5 - Construction	AIP	2029	\$ 2,020,500	\$ 101,025	\$ 123,475	\$ 2,245,000
Ukiah Municipal Airport	New Taxiway A4 (Taxiway D Demolition) - Construction	AIP	2029	\$ 1,552,500	\$ 77,625	\$ 94,875	\$ 1,725,000
University Airport	Construct: AWOS III P/T	AIP	2025	\$ 367,200	\$ 18,360	\$ 22,440	\$ 408,000
University Airport	Design: Recons. West Apron - Phases 1 and 2	AIP	2026	\$ 171,000	\$ 8,550	\$ 10,450	\$ 190,000
University Airport	Construct: Reconstruction of the West Apron Phases 1 and 2	AIP	2027	\$ 1,322,798	\$ 66,140	\$ 80,838	\$ 1,469,775
University Airport	Design - Rehabilitate Parallel Taxiway, Construct New Exit	AIP	2029	\$ 238,500	\$ 11,925	\$ 14,575	\$ 265,000
University Airport	Rehab Parallel Twy, Construct New Exit Twy, Twy Lighting	AIP	2030	\$ 2,173,500	\$ 108,675	\$ 132,825	\$ 2,415,000
University Airport	Design: Reconstruct West Apron Phase 3	AIP	2032	\$ 169,200	\$ 8,460	\$ 10,340	\$ 188,000
University Airport	ALP Update Narrative and Plans	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
University Airport	Construct: Reconstruct West Apron - Phase 3	AIP	2033	\$ 1,911,191	\$ 95,560	\$ 116,795	\$ 2,123,546
University Airport	Pavement Maintenance Management Plan (PMMP)	AIP	2033	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
University Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Van Nuys Airport	Decarbonization Study	AIP	2025	\$ 1,500,000	\$ 75,000	\$ 91,667	\$ 1,666,667
Van Nuys Airport	Taxiway Phase 2 Construction (AIP)	AIP	2025	\$ 23,301,427	\$ 1,165,071	\$ 1,423,976	\$ 25,890,474
Van Nuys Airport	Taxiway Phase 2 Construction (BIL AIG - FY22-FY25)	AIP	2025	\$ 2,979,473	\$ 148,974	\$ 182,079	\$ 3,310,526
Van Nuys Airport	Design - Slurry Coat Runways 16R-34L and 16L-34R; Recons.	AIP	2026	\$ 2,520,000	\$ 126,000	\$ 154,000	\$ 2,800,000
Van Nuys Airport	Slurry Seal Taxiways A and B	AIP	2027	\$ 6,516,000	\$ 325,800	\$ 398,200	\$ 7,240,000
Van Nuys Airport	Reconstruct Taxiways C and R - PCC Sections	AIP	2028	\$ 11,070,000	\$ 553,500	\$ 676,500	\$ 12,300,000
Van Nuys Airport	Slurry Coat Runways 16R-34L and 16L-34R (Construction)	AIP	2028	\$ 17,100,000	\$ 855,000	\$ 1,045,000	\$ 19,000,000
Van Nuys Airport	Reconstruct Taxilane A3 and Rehab Taxilane A2 (Cons.)	AIP	2029	\$ 15,300,000	\$ 765,000	\$ 935,000	\$ 17,000,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Van Nuys Airport	Reconstruct Taxilane B1 and B3, Phase 1 (Design)	AIP	2030	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Visalia Municipal Airport	Reconstruct Taxilane A (50' x 1,314') - Des.	AIP	2025	\$ 129,600	\$ 6,480	\$ 7,920	\$ 144,000
Visalia Municipal Airport	Architectural Design - Recons. General Aviation Terminal	AIP	2026	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Visalia Municipal Airport	Reconstruct Taxilane A (35' x1,314') - Construction	AIP	2026	\$ 1,098,828	\$ 54,941	\$ 67,151	\$ 1,220,920
Visalia Municipal Airport	Reconstruct Tie Down Apron A3 (182,000 sq. ft.) - Design	AIP	2026	\$ 350,100	\$ 17,505	\$ 21,395	\$ 389,000
Visalia Municipal Airport	Construct: Reconstruct Tie Down Apron A3	AIP	2027	\$ 3,001,545	\$ 150,077	\$ 183,428	\$ 3,335,050
Visalia Municipal Airport	Reconstruct General Aviation Terminal	AIP	2028	\$ 2,165,400	\$ 108,270	\$ 132,330	\$ 2,406,000
Visalia Municipal Airport	Reconstruct Hangar H1 Taxilanes Rows 3, 4, and 7 and H2	AIP	2028	\$ 152,100	\$ 7,605	\$ 9,295	\$ 169,000
Visalia Municipal Airport	Construct - Reconstruct Hangar H1 Txlns Rows 3, 4, and 7	AIP	2029	\$ 1,068,300	\$ 53,415	\$ 65,285	\$ 1,187,000
Visalia Municipal Airport	New MidField Apron Development (Remove/Relocate)	AIP	2030	\$ 216,000	\$ 10,800	\$ 13,200	\$ 240,000
Visalia Municipal Airport	New MidField Apron Development (Remove/Relocate) - Cons	AIP	2031	\$ 1,440,000	\$ 72,000	\$ 88,000	\$ 1,600,000
Visalia Municipal Airport	Design: Remove and Replace AC on West Cargo Apron	AIP	2032	\$ 459,000	\$ 22,950	\$ 28,050	\$ 510,000
Visalia Municipal Airport	ALP Update Narrative and Plans	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Visalia Municipal Airport	Remove and Replace West Cargo Apron and Expand Twy	AIP	2033	\$ 5,350,500	\$ 267,525	\$ 326,975	\$ 5,945,000
Visalia Municipal Airport	ALUCP Update	AIP	2034	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Visalia Municipal Airport	Pavement Maintenance Management Program (PMMP)	AIP	2034	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Ward Field Airport	Perimeter Fencing	A&D	2027	\$ -	\$ 67,500	\$ 7,500	\$ 75,000
Ward Field Airport	Pavement Study	A&D	2028	\$ -	\$ 18,000	\$ 2,000	\$ 20,000
Ward Field Airport	Runway Rehabilitation - Phase 1 (Design)	A&D	2028	\$ -	\$ 67,500	\$ 7,500	\$ 75,000
Ward Field Airport	Runway Rehabilitation - Phase 2 (Construction)	A&D	2030	\$ -	\$ 315,000	\$ 35,000	\$ 350,000
Ward Field Airport	Obstruction Removal - Phase 1 (Design)	A&D	2032	\$ -	\$ 45,000	\$ 5,000	\$ 50,000
Ward Field Airport	Obstruction Removal - Phase 2 (Construction)	A&D	2034	\$ -	\$ 315,000	\$ 35,000	\$ 350,000
Wasco-Kern Airport	ALP Update	AIP	2026	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Wasco-Kern Airport	Design and Install Security and CCTV System	AIP	2026	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Wasco-Kern Airport	Parking Ramps/HazMat Containment Repairs and Restripe	AIP	2027	\$ 218,500	\$ 10,925	\$ 575	\$ 230,000
Wasco-Kern Airport	Replace Rwy Light Regulator, Building and Partial Wiring	AIP	2027	\$ 181,250	\$ 9,062	\$ 477	\$ 190,789
Wasco-Kern Airport	Update Segmented Circe and Windssock Pole/Lighting	AIP	2027	\$ 42,750	\$ 2,137	\$ 113	\$ 45,000
Wasco-Kern Airport	Design Runway Rehabilitation Project	AIP	2028	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Wasco-Kern Airport	Construction of Runway Rehab Project	AIP	2030	\$ 810,000	\$ 40,500	\$ 49,500	\$ 900,000
Watsonville Municipal Airport	Reconstruct Permanent Tie-down Apron (Area 7) - Des.	AIP	2025	\$ 184,500	\$ 9,225	\$ 11,275	\$ 205,000
Watsonville Municipal Airport	Pollution Control System Design and Electrical Construction	AIP	2025	\$ 114,525	\$ 5,726	\$ 6,999	\$ 127,250
Watsonville Municipal Airport	Design- Lease Hold Taxi Lane Connector	AIP	2026	\$ 70,200	\$ 3,510	\$ 4,290	\$ 78,000
Watsonville Municipal Airport	Reconstruct Permanent Tie-Down Apron (Area A7) - Cons.	AIP	2027	\$ 1,749,525	\$ 87,476	\$ 106,915	\$ 1,943,917
Watsonville Municipal Airport	Reconstruct Permanent Tie-Down Apron (Area A7) - Cons.	AIP	2027	\$ 473,475	\$ 23,674	\$ 28,935	\$ 526,083
Watsonville Municipal Airport	Construct Lease Hold Taxi Lane Connector	AIP	2028	\$ 365,400	\$ 18,270	\$ 22,330	\$ 406,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Watsonville Municipal Airport	Reconstruct South Side Tee Hangar Taxilanes - Des.	AIP	2028	\$ 187,200	\$ 9,360	\$ 11,440	\$ 208,000
Watsonville Municipal Airport	Rehab Drainage Serving Existing East G.A. Apron	AIP	2028	\$ 184,500	\$ 9,225	\$ 11,275	\$ 205,000
Watsonville Municipal Airport	Construction- Reconstruct South Side Tee Hangar Taxilanes	AIP	2029	\$ 2,187,900	\$ 109,395	\$ 133,705	\$ 2,431,000
Watsonville Municipal Airport	Rehab Drainage Serving East GA Apron and Hangars	AIP	2030	\$ 1,930,950	\$ 96,548	\$ 118,003	\$ 2,145,500
Watsonville Municipal Airport	PMMP Update	AIP	2032	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Watsonville Municipal Airport	ALP Update with Narrative	AIP	2033	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000
Watsonville Municipal Airport	Service Road to on Field	AIP	2034	\$ 99,000	\$ 4,950	\$ 6,050	\$ 110,000
Weed Airport	Construction AWOS - Design and Construction	AIP	2025	\$ 298,421	\$ 14,921	\$ 18,237	\$ 331,579
Weed Airport	Taxiway/Apron Rehabilitation (Phase Construction Year 2)	AIP	2026	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
Weed Airport	ALP with Narrative Aeronautical Survey	AIP	2027	\$ 489,000	\$ 24,450	\$ 29,883	\$ 543,333
Weed Airport	Airfield Electrical-REIL and PAPI, Lighting Vault	AIP	2028	\$ 1,080,000	\$ 54,000	\$ 66,000	\$ 1,200,000
Weed Airport	Airfield Electrical - Taxiway Lighting (Des. and Cons.)	AIP	2029	\$ 540,000	\$ 27,000	\$ 33,000	\$ 600,000
Weed Airport	Airfield Electrical - Apron Lighting	AIP	2030	\$ 225,000	\$ 11,250	\$ 13,750	\$ 250,000
Westover Field Amador County Airport	Install Perimeter Fencing (Design)	AIP	2025	\$ 157,500	\$ 7,875	\$ 9,625	\$ 175,000
Westover Field Amador County Airport	Install Perimeter Fencing (8,850 LF) (Construction)	AIP	2026	\$ 1,035,720	\$ 51,786	\$ 63,294	\$ 1,150,800
Westover Field Amador County Airport	Install Perimeter Fencing (3,450 LF) (Construction)	AIP	2026	\$ 404,280	\$ 20,214	\$ 24,706	\$ 449,200
Westover Field Amador County Airport	Reconstruct Runway 1/19 (Design)	AIP	2028	\$ 148,500	\$ 7,425	\$ 9,075	\$ 165,000
Westover Field Amador County Airport	Reconstruct Runway 1/19 (Construction)	AIP	2029	\$ 2,700,000	\$ 135,000	\$ 165,000	\$ 3,000,000
Westover Field Amador County Airport	Reconstruct Taxiway A (Design)	AIP	2030	\$ 130,500	\$ 6,525	\$ 7,975	\$ 145,000
Westover Field Amador County Airport	Reconstruct Taxiway A (Construction)	AIP	2031	\$ 1,800,000	\$ 90,000	\$ 110,000	\$ 2,000,000
Westover Field Amador County Airport	Airport Drainage Improvements Study	AIP	2032	\$ 112,500	\$ 5,625	\$ 6,875	\$ 125,000
Westover Field Amador County Airport	Airport Drainage Improvements (NEPA/EA)	AIP	2033	\$ 202,500	\$ 10,125	\$ 12,375	\$ 225,000
Westover Field Amador County Airport	Airport Drainage Improvements (Design)	AIP	2034	\$ 135,000	\$ 6,750	\$ 8,250	\$ 150,000
Whiteman Airport	Enhance Blast Protection and Stripe Vehicle Zipper Lane	AIP	2025	\$ 450,000	\$ 22,500	\$ 27,500	\$ 500,000
Whiteman Airport	Relocate Runway Thresholds and Approach Markings	AIP	2025	\$ 720,000	\$ 36,000	\$ 44,000	\$ 800,000
Whiteman Airport	Pavement Repairs and Maintenance - Crack Sealing/Patching	AIP	2026	\$ 45,000	\$ 2,250	\$ 2,750	\$ 50,000
Whiteman Airport	Reconfigure Airport Entrance Road (Design)	AIP	2026	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Whiteman Airport	Rehab Rwy and Twy A and Construct Rwy 30 Hold (Cons.)	AIP	2026	\$ 3,600,000	\$ 180,000	\$ 220,000	\$ 4,000,000
Whiteman Airport	Rehabilitate Runway 12-30 and Taxiways [Design]	AIP	2026	\$ 243,000	\$ 12,150	\$ 14,850	\$ 270,000
Whiteman Airport	Airport Land Use Compatibility Plan (ALUCP)	A&D	2027	\$ -	\$ 270,000	\$ 30,000	\$ 300,000
Whiteman Airport	Recons. Wash Rack and Apron Area Pavements (Design)	AIP	2027	\$ 54,000	\$ 2,700	\$ 3,300	\$ 60,000
Whiteman Airport	Survey Under Ground Utilities - Develop Utility Map	AIP	2027	\$ 432,000	\$ 24,000	\$ 24,000	\$ 480,000
Whiteman Airport	Apron Rehab Phase 1 [Design]	AIP	2028	\$ 531,000	\$ 26,550	\$ 32,450	\$ 590,000
Whiteman Airport	Exit Taxiways (MP)	AIP	2028	\$ 687,600	\$ 30,000	\$ 46,400	\$ 764,000
Whiteman Airport	Rehabilitate Runway 12-30 and Taxiways [Construction]	AIP	2028	\$ 2,745,000	\$ 137,250	\$ 167,750	\$ 3,050,000
Whiteman Airport	Acquire 10.8 Acres in Avigation Easements	AIP	2029	\$ 364,500	\$ 18,225	\$ 22,275	\$ 405,000
Whiteman Airport	Apron Rehab Phase 2 [Design]	AIP	2029	\$ 199,800	\$ 9,990	\$ 12,210	\$ 222,000
Whiteman Airport	Construct Non-Airworthy Aircraft Parking Area	AIP	2029	\$ 529,720	\$ 13,243	\$ 14,637	\$ 557,600
Whiteman Airport	Reon Wash Rack and Apron Area Pavements (Construction)	AIP	2029	\$ 630,000	\$ 31,500	\$ 38,500	\$ 700,000
Whiteman Airport	Apron Rehab Phase 1 [Construction]	AIP	2030	\$ 8,895,330	\$ 444,767	\$ 543,604	\$ 9,883,700
Whiteman Airport	Apron Rehab Phase 2 [Construction]	AIP	2031	\$ 3,346,560	\$ 167,328	\$ 204,512	\$ 3,718,400
Willits Municipal Airport	Conduct PMMP Study with Geotechnical Report	AIP	2025	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Willits Municipal Airport	Conduct Wildlife Hazard Assessment	AIP	2025	\$ 108,000	\$ 5,400	\$ 6,600	\$ 120,000
Willits Municipal Airport	Tree Removal (Study: Survey, Biological, Environmental)	AIP	2025	\$ 112,500	\$ 5,625	\$ 6,875	\$ 125,000
Willits Municipal Airport	Install Terminal Area/Wildlife Fencing (Design)	AIP	2026	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Willits Municipal Airport	Reconstruct North Apron/Rehabilitate South Apron (Design)	AIP	2026	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Willits Municipal Airport	Tree Removal (Design and Cons.)	AIP	2026	\$ 247,500	\$ 12,375	\$ 15,125	\$ 275,000
Willits Municipal Airport	Install Terminal Area/Wildlife Fencing (Cons.)	AIP	2027	\$ 297,000	\$ 14,850	\$ 18,150	\$ 330,000
Willits Municipal Airport	Reconstruct North Apron/Rehabilitate South Apron (Cons.)	AIP	2027	\$ 900,000	\$ 45,000	\$ 55,000	\$ 1,000,000
Willits Municipal Airport	Rehabilitate Taxiway A and Remaining Pavements (Design)	AIP	2028	\$ 121,500	\$ 6,075	\$ 7,425	\$ 135,000
Willits Municipal Airport	Rehabilitate Taxiway A and Remaining Pavements (Cons.)	AIP	2029	\$ 391,500	\$ 19,575	\$ 23,925	\$ 435,000
Willits Municipal Airport	Construct GA Terminal Building (Design and Bidding)	AIP	2030	\$ 112,500	\$ 5,625	\$ 6,875	\$ 125,000
Willits Municipal Airport	Update ALP	AIP	2030	\$ 150,000	\$ 7,500	\$ 9,167	\$ 166,667
Willits Municipal Airport	Construct GA Terminal Building (Cons.)	AIP	2031	\$ 440,500	\$ 22,025	\$ 26,919	\$ 489,444
Willits Municipal Airport	Hangar Development (Design)	AIP	2032	\$ 90,000	\$ 4,500	\$ 5,500	\$ 100,000
Willits Municipal Airport	Segmented Circle (Design)	AIP	2034	\$ 40,500	\$ 2,025	\$ 2,475	\$ 45,000
Woodlake Airport	ALP Amendment and AGIS Survey - BIL	AIP	2025	\$ 157,500	\$ 7,875	\$ 9,625	\$ 175,000
Woodlake Airport	Reconstruct and widen Rwy 8-26 and obstruction lights (des.)	AIP	2025	\$ 139,500	\$ 6,975	\$ 8,525	\$ 155,000
Woodlake Airport	Reconstruct and widen Rwy 8-26 and obstruction lights (cons.)	AIP	2026	\$ 3,094,200	\$ 154,710	\$ 189,090	\$ 3,438,000
Woodlake Airport	Relocate/ Reconfigure Twys, Hangars, Apron	AIP	2027	\$ 4,050,000	\$ 202,500	\$ 247,500	\$ 4,500,000
Woodlake Airport	Airspace Easements for Approach Protection (9.7 acres)	AIP	2028	\$ 126,000	\$ 6,300	\$ 7,700	\$ 140,000
Woodlake Airport	Remove Obstructions (Trees)	AIP	2028	\$ 49,500	\$ 2,475	\$ 3,025	\$ 55,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Woodlake Airport	Security enhancements - Fence and Gate Improvement	AIP	2028	\$ 52,650	\$ 2,633	\$ 3,218	\$ 58,500
Woodlake Airport	Misc. NAVAIDs, Relocate Hangars and Apron Ph 2	AIP	2029	\$ 3,384,000	\$ 169,200	\$ 206,800	\$ 3,760,000
Woodlake Airport	Transient Apron (Design)	AIP	2031	\$ 243,000	\$ 12,150	\$ 14,850	\$ 270,000
Woodlake Airport	Transient Apron (Cons.)	AIP	2032	\$ 3,240,000	\$ 162,000	\$ 198,000	\$ 3,600,000
Yolo County-Davis Woodland Winters Airport	Runway 16-34 Rehabilitation/Pavement Preservation(Design)	AIP	2025	\$ 161,640	\$ 8,082	\$ 9,878	\$ 179,600
Yolo County-Davis Woodland Winters Airport	Prepare Airfield Pavement Management System (APMS) Plan	AIP	2025	\$ 88,200	\$ 4,410	\$ 5,390	\$ 98,000
Yolo County-Davis Woodland Winters Airport	Runway 16-34 Rehabilitation (Construction)	AIP	2026	\$ 1,570,950	\$ 78,548	\$ 96,003	\$ 1,745,500
Yolo County-Davis Woodland Winters Airport	Taxiways A and B Pavement Preservation (Design)	AIP	2027	\$ 111,240	\$ 5,562	\$ 6,798	\$ 123,600
Yolo County-Davis Woodland Winters Airport	Taxiways A and B Safety Area Grading (Design)	AIP	2027	\$ 183,150	\$ 9,158	\$ 11,193	\$ 203,500
Yolo County-Davis Woodland Winters Airport	Taxiway Lighting Improvements (Design)	AIP	2027	\$ 283,050	\$ 14,153	\$ 17,298	\$ 314,500
Yolo County-Davis Woodland Winters Airport	Airport Layout Plan Update and AGIS Survey	AIP	2028	\$ 343,800	\$ 17,190	\$ 21,010	\$ 382,000
Yolo County-Davis Woodland Winters Airport	Taxiways A and B Pavement Preservation (Construction)	AIP	2029	\$ 666,630	\$ 33,332	\$ 40,739	\$ 740,700
Yolo County-Davis Woodland Winters Airport	Taxiways A and B Safety Area Grading (Construction)	AIP	2029	\$ 1,182,060	\$ 59,103	\$ 72,237	\$ 1,313,400
Yolo County-Davis Woodland Winters Airport	Taxiway Lighting Improvements (Construction)	AIP	2029	\$ 3,484,530	\$ 174,227	\$ 212,944	\$ 3,871,700
Yolo County-Davis Woodland Winters Airport	Aircraft Parking Apron Reconstruction (Design)	AIP	2030	\$ 262,350	\$ 13,118	\$ 16,033	\$ 291,500
Yolo County-Davis Woodland Winters Airport	Aircraft Parking Apron Reconstruction (Construction)	AIP	2031	\$ 2,067,570	\$ 103,379	\$ 126,352	\$ 2,297,300
Yolo County-Davis Woodland Winters Airport	Replace and Relocate AWOS to West Side of Airfield (Design)	AIP	2032	\$ 151,200	\$ 7,560	\$ 9,240	\$ 168,000
Yolo County-Davis Woodland Winters Airport	Replace and Relocate AWOS to West Side of Airfield (Cons.)	AIP	2033	\$ 326,070	\$ 16,304	\$ 19,927	\$ 362,300
Yuba County Airport	Rotating Beacon Improvements (Design)	AIP	2025	\$ 121,500	\$ 6,075	\$ 7,425	\$ 135,000
Yuba County Airport	Procurement of Airfield Equipment for RSA Protection	AIP	2025	\$ 157,500	\$ 7,875	\$ 9,625	\$ 175,000
Yuba County Airport	Rotating Beacon Improvements (Construction)	AIP	2026	\$ 360,000	\$ 18,000	\$ 22,000	\$ 400,000
Yuba County Airport	Runway 14-32 AC Pavement Improvements (Design)	AIP	2026	\$ 855,000	\$ 42,750	\$ 52,250	\$ 950,000
Yuba County Airport	Runway 14-32 Pavement Improvements (Construction)	AIP	2028	\$ 16,650,000	\$ 832,500	\$ 1,017,500	\$ 18,500,000
Yuba County Airport	Airport Layout Plan (ALP) Update	AIP	2029	\$ 270,000	\$ 13,500	\$ 16,500	\$ 300,000

Facility Name	Project Name	Project Type	Program Year	FAA Grant Amount	State Grant Amount	Local Matching Amount	Total Project Cost
Yuba County Airport	Twy A, Connectors, Based Tie-Down Apron Pvmnt. Preserve	AIP	2030	\$ 148,500	\$ 7,425	\$ 9,075	\$ 165,000
Yuba County Airport	Twy A, Connectors, Based Tie-Down Apron Pvmnt. Preserve	AIP	2031	\$ 1,201,500	\$ 60,075	\$ 73,425	\$ 1,335,000

Table 5: Abbreviations and Acronyms

A

A&D	Acquisition & Development
AC	Asphalt Concrete
AGIS	Airport Geographic Information System
AGS	Advanced Guidance System
AIG	Airport Infrastructure Grant
AIP	Airport Improvement Program
ALP	Airport Layout Plan
ALUCP	Airport Land Use Compatibility Plan
APMS	Airport Pavement Management System
ARFF	Aircraft Rescue and Fire Fighting
ASOS	Automated Surface Observing System
ATCT	Airport Traffic Control Tower
ATP	Airport Terminal Program
AVGAS	Aviation gasoline
AWOS	Automated Weather Observing System

B

BIL	Bipartisan Infrastructure Law
BRL	Building Restriction Line

C

CEQA	California Environmental Quality Act
Comm.	Commercial
Cons.	Construction

D

DBE	Disadvantaged Business Enterprise
Des.	Design

E

EA	Environmental Assessment
eGSE	Electrical Ground Support Equipment
EIR	Environmental Impact Statement
EMAS	Engineered Materials Arresting Systems
Eng.	Engineering
Enviro.	Environmental
EV	Electric Vehicle
Ext.	Extension

F

FAA	Federal Aviation Administration
FBO	Fixed-Base Operator
FCT	Federal Aviation Administration Contract Tower
FED	Federal

G

GA	General Aviation
GSE	Ground Support Equipment

H

HVAC	Heating, Ventilation and Air Conditioning Systems
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I

ILS Instrument Landing System
Improv. Improvement

L

LAX Los Angeles International Airport
LED Light Emitting Diode
LFP Locally Funded Project
LVP Low Visibility Procedures

M

MAGVAR Magnetic Variation
MALSR Medium Intensity Approach Lighting System with
Runway Alignment Indicator Lights
MITL Medium Intensity Taxiway Lights
MIRL Medium Intensity Runway Lights
Misc. Miscellaneous

N

NAVAID Navigational Aids
NE Northeast
NEPA National Environmental Policy Act

O

Obst. Obstruction
OFA Object Free Area

P

P/T Present Weather and Thunderstorm
PAPI Precision Approach Path Indicators
PCC Portland Cement Concrete
Perim. Perimeter
PFAS Perfluoroalkyl and Polyfluoroalkyl Substances
Ph Phase
PMMP Pavement Maintenance Management Program
PMP Pavement Management Program
Pvmt. Pavement

R

RCC Replacement Cost Coverage
Recons. Reconstruction
Rehab Rehabilitation
REIL Runway End Identifier Lights
Reimb. Reimbursement
Reloc. Relocation
RIM Runway Incursion Mitigation
ROFA Runway Object Free Area
RPZ Runway Protection Zones
RSA Runway Safety Area
RSI Residential Sound Insulation
Rwy Runway

S

SE	Southeast
Seg.	Segmented
sq.	Square
SRE	Snow Removal Equipment
SW	Southwest

I

Twy	Taxiway
Txln	Taxilane

V

VALE	Voluntary Airport Low Emission
VASI	Visual Approach Slope Indicator

W

WHA	Wildlife Hazard Assessment
WHMP	Wildlife Hazard Management Plan

Z

ZEV	Zero Emissions Vehicle
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MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 4.22, Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **2023 ACTIVE TRANSPORTATION PROGRAM PROJECT AMENDMENT –
CITY OF CUDAHY – SALT LAKE AVENUE PEDESTRIAN ACCESSIBILITY
PROJECT
RESOLUTION G-25-46, AMENDING RESOLUTION G-23-49**

ISSUE:

Should the California Transportation Commission (Commission) amend the City of Cudahy – Salt Lake Avenue Pedestrian Accessibility Project (PPNO 6171), in Los Angeles County, currently programmed in the 2023 Active Transportation Program (ATP), to approve a funding distribution change to allow the City of Cudahy (City) to move \$675,000 currently programmed for the Right of Way (RW) phase in Fiscal Year 2025-26 to the Construction (CON) phase in 2026-27?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve the City's amendment for the City of Cudahy – Salt Lake Avenue Pedestrian Accessibility Project (PPNO 6171), in Los Angeles County, currently programmed in the 2023 ATP, to approve a funding distribution change to allow the City to move \$675,000 currently programmed for the RW phase in 2025-26 to the CON phase in 2026-27.

BACKGROUND:

On June 28, 2023, the Commission approved the 2023 Metropolitan Planning Organization (MPO) component of the ATP, which included \$7,125,000 in 2023 ATP funding for the City of Cudahy – Salt Lake Avenue Pedestrian Accessibility Project (PPNO 6171), under Resolution G-23-49. The total programmed amount for the City of Cudahy – Salt Lake Avenue Pedestrian Accessibility Project (PPNO 6171) has not changed.

The 2023 ATP Guidelines, adopted by the Commission on March 16, 2022, state that funding distribution changes will be considered for ATP projects, to allow agencies to move funds between programmed phases, provided that moving funds between phases does not increase the total funding amount programmed. The proposed amendment for the City of Cudahy – Salt Lake Avenue Pedestrian Accessibility Project (PPNO 6171) would move funds as outlined in the chart below and does not program additional funding for the project. The City will fund the RW phase.

Phase	PA&ED	PS&E	RW	CON	CON-NI
Program Fiscal Year	2023-24	2024-25	2025-26	2026-27	N/A
Original Amount	\$275,000	\$632,000	\$675,000	\$5,543,000	N/A
Change	\$0	\$0	-\$675,000	\$675,000	N/A
Proposed Amount	\$275,000	\$632,000	\$0	\$6,218,000	N/A

RESOLUTION G-25-46:

Be it Resolved, that the Commission does hereby amend the 2023 MPO component of the ATP to approve a funding distribution change by moving \$675,000 currently programmed for the RW phase in 2025-26 to the CON phase in 2026-27 for the City of Cudahy – Salt Lake Avenue Pedestrian Accessibility Project (PPNO 6171). All other provisions stipulated in Resolution G-23-49 remain in effect.

Be it further Resolved, that the funding is hereby amended to reflect the funding distribution changes for the City of Cudahy – Salt Lake Avenue Pedestrian Accessibility Project (PPNO 6171).

Be it further Resolved, that all other provisions stipulated in Resolution G-23-49 remain in effect.

Attachments

Exhibit 25-G: Request for Funding Distribution Change (Local ATP Projects)

To: Steve Novotny
District Local Assistance Engineer
Caltrans, Office of Local Assistance
100 S. Main St, #12.040
Los Angeles, CA 90012-3712

Date: 04/28/2025

PPNO: 6171

Federal PROJECT #: ATPL-5369(017)

ATP ID #: ATP 7-Cudahy, City of-I

Project Name (Per CTC programming): Salt Lake Avenue Pedestrian Accessibility Project

Implementing Agency: City of Cudahy

Approved Project Description (As submitted in Application): 7,110 feet of widened sidewalk along N-S city jurisdiction of Salt Lake Avenue. Installation of pedestrian lighting, signage, curb extension, and ADA compliant wheelchair ramps

Approved Project Limits (As submitted in Application): Various locations along the N-S city jurisdiction of east side of Salt Lake Avenue

Dear Steve Novotny:

Consistent with the California Transportation Commission's (CTC) *Final Adopted 2021 ATP Guidelines*, Section 28. **Amendment Requests**, adopted on March 25, 2020, we request that Caltrans consider and the CTC approve the proposed Project Amendment Request as documented below:

A. Is this request being made in the same state fiscal year in the funds have been programmed?

☐ Yes

☒ No

If "Yes" then your request is not eligible.

B. Have any of the request funds been allocated?

☐ Yes

☒ No

If "Yes" then your request is not eligible for a Fund Distribution Change request. For funds that have been allocated, review CTC Final Adopted ATP Guidelines, "Project Cost Savings".

C. Does this request propose to move funds out of Construction?

☐ Yes

☒ No

If "Yes" then your request is not eligible.

D. Has this project already received a Funding Distribution Change Request?

☐ Yes

☒ No

If "Yes" then your request is not eligible.

E. Project Funding Chart

Phase/Funding	PA&ED	PS&E	ROW	CON	CON-NI
Program FY*	23/24	24/25	25/26	26/27	
Original \$	\$275,000	\$632,000	\$675,000	\$5,543,000	
Proposed \$	No Change	No Change	\$0	\$6,218,000	
Change (\$)	No Change	No Change	-\$675,000	+\$675,000	

*FY- State fiscal year (i.e. July 1, 2019 through June 30, 2020 is shown as 19/20)

Other Elements (per CTC's Amendment Request Guidance):**1. The reason for the proposed Funding Distribution Change:**

Request to move all programmed funds from ROW phase to CON phase. All of the proposed work is within the public ROW and ROW phase does not require the amount of funds programmed. The ROW funds will be better utilized in the CON phase.

2. The impact the proposed change would have on the overall cost of the Project. (The project must remain fully funded.)

The overall cost of the project is fully funded with ATP funds totaling \$7,125,000. The project delivery phases are PA&ED with \$275,000 currently allocated, PS&E programmed for \$632,000 for FY24/25, ROW programmed for \$675,000 for FY25/26, and CON programmed for \$5,543,000 for FY26/27. We are concurrently requesting at the June CTC meeting an allocation time extension request for the PS&E funds to be extended to June 30, 2026. The PS&E allocation time extension will not impact the overall cost of the project.

This request is to move \$675,000 from the ROW phase to the CON phase. The CON phase revised total will be \$6,218,000. The project will remain fully funded.

3. Discussion of whether the Funding Distribution Change will affect the benefit of the project as described in the project application:

The funding distribution change moving ROW funds to the CON phase will not affect the benefit of the project as described in the project application.

4. For projects programmed in the Metropolitan Planning Organization (MPO) component, evidence of MPO approval and the MPO rationale for their approval.**Caltrans supports this Request for Funding Distribution Change:**

- ☐ Yes << For Caltrans HQ only >>
☐ No

Required revisions to the Project's Description and/or Limits:

The proposed Project Amendments documented above will require the following changes to the Project's Description and/or Limits: *N/A*

Proposed changes to the Project Description: _____

Proposed changes to the Project Limits: _____

For Federally Funded Projects: N/A

Proposed changes to the FTIP/FSTIP Description: _____

Proposed changes to the FTIP/FSTIP Limits: _____

Project Delivery Status:

The following is a side-by-side comparison of the original project schedule and the current project schedule. The explanations for each milestone date change is listed below:

Anticipated CTC Allocation Dates (at the time of Application Submittal for funding):

PA&ED: 23/24 PS&E: 24/25 R/W: 25/26 CON: 26/27 CON-NI:

Actual/New Anticipated CTC Allocation Dates (at the time of this request):

PA&ED: 23/24 PS&E: 24/25 R/W: CON: 26/27 CON-NI:

Explanation for milestone changes:

PA&ED: N/A

PS&E: N/A

R/W: Removing R/W funding

CON: NA

CON-NI: N/A

Local Agency Certification:

This Request for Funding Distribution Change has been prepared in accordance with the *Procedures for Administering Local Projects in the Active Transportation Program (ATP)*. I certify that the information provided in the document is accurate and correct. I understand that if the required information has not been provided this form will be returned and the request may be delayed. Please advise us as soon as the scope change has been approved. You may direct any questions to **Alfonso Noyola at (323)-773-5143**

Signature:  Title: City Manager Date: 04/28/2025

Agency/Commission: City of Cudahy

Attachments:

(1) Revised PPR (Required)

(2) Maps and Plans (required if the Project Description and/or Limits are changing due to this request)

Distribution:

(1) Original -DLAE

(2) Copy – Division of Local Assistance, Headquarters ATP Program Manager

(3) Copy – MPO/RTPA/County Transportation Commission

PROJECT PROGRAMMING REQUEST

LAPG -25I (Revised 6 Feb 2024 v1.02)

General Instructions

Amendment (Existing Project) Yes					Date:	5/5/25	
District	EA	Project ID		PPNO	MPO ID		
07		0724000056		6171	ATP		
County	Route/Corridor	PM Bk	PM Ahd	Nominating Agency			
LA	Salt Lake Avenue			Cudahy, City of			
				MPO	Element		
				SCAG	Local Assistance		
Project Manager/Contact		Phone		E-mail Address			
Alfonso Noyola/City Manager		(323) 773-5143		anoyola@cityofcudahyca.gov			
Project Title							
Salt Lake Avenue Pedestrian Accessibility Project							
Location (Project Limits), Description (Scope of Work)							
Various locations along the N-S city jurisdiction of east side of Salt Lake Avenue.							
Component							
PA&ED		Cudahy, City of					
PS&E		Cudahy, City of					
Right of Way		Cudahy, City of					
Construction		Cudahy, City of					
Legislative Districts							
Assembly:	63	Senate:	33	Congressional:	40		
Project Benefits							
Pedestrian accessibility and compliance throughout Salt Lake Avenue. Street widening project would activate Salt Lake Avenue by turning it into a pedestrian route while also mitigating pedestrian injuries and fatalities. Salt Lake Avenue would connect the western N-S boundaries of the City and would increase pedestrian activity to Teresa Hughes Elementary School, Lugo Park, and two proposed WSAB transit stations.							
Purpose and Need							
Currently, Salt Lake Avenue is too narrow. This makes the street inaccessible to pedestrian activity. This is seen by the low pedestrian volume as well as pedestrian fatalities and injuries that has occurred on this street. The purpose of this project is to activate Salt Lake Avenue as a pedestrian corridor to safely access the City's school and future amenities.							
Category		Outputs			Unit	Total	
Active Transportation		Sidewalk miles			LF	7100	
ADA Improvements		Repair/upgrade curb ramp			EA	20	
Other					LF	7100	
NHS Improvements	No	Roadway Class	NA		Reversible Lane analysis	No	
Inc. Sustainable Communities Strategy Goals		Yes		Reduces Greenhouse Gas Emissions		Yes	
Project Milestone					Existing	Proposed	
Project Study Report Approved							
Begin Environmental (PA&ED) Phase						09/01/23	
Circulate Draft Environmental Document					Document Type	06/01/25	
Draft Project Report						09/01/25	
End Environmental Phase (PA&ED Milestone)						12/01/25	
Begin Design (PS&E) Phase						01/01/26	
End Design Phase (Ready to List for Advertisement Milestone)						06/01/27	
Begin Right of Way Phase							
End Right of Way Phase (Right of Way Certification Milestone)							
Begin Construction Phase (Contract Award Milestone)						09/01/27	
End Construction Phase (Construction Contract Acceptance Milestone)						09/01/28	
Begin Closeout Phase						10/01/28	
End Closeout Phase (Closeout Report)						03/01/29	

PROJECT PROGRAMMING REQUEST

LAPG -25I (Revised 6 Feb 2024 v1.02)

Date: 5/5/25

Additional Information

Fund No. 6:									Program Code
Existing Funding (\$1,000s)									
Component	Prior	24-25	25-26	26-27	27-28	28-29	29-30+	Total	Funding Agency
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL									
Proposed Funding (\$1,000s)									Notes
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL									

PROJECT PROGRAMMING REQUEST

LAPG -25I (Revised 6 Feb 2024 v1.02)

Complete this page for amendments only**Date:** 5/5/25

District	County	Route	EA	Project ID	PPNO	
07	LA	Salt Lake Avenue		0724000056	6171	

SECTION 1 - All Projects**Project Background**

Project with PA&ED, PS&E, ROW, and CON phases programmed. Fully funded through ATP funds. \$275,000 currently allocated for PA&ED phase, PS&E programmed for \$632,000 for FY24/25, ROW programmed for \$675,000 for FY25/26, and CON programmed for \$5,543,000 for FY26/27. We are concurrently requesting at the June CTC meeting an allocation time extension request for the PS&E funds to be extended to June 30, 2026. The PS&E allocation time extension will not impact the overall cost of the project.

Programming Change Requested

Request to move all programmed funds from ROW phase to CON phase.

Reason for Proposed Change

This request is to move \$675,000 from the ROW phase to the CON phase. The CON phase revised total will be \$6,218,000. All of the proposed work is within the public ROW and ROW phase does not require the amount of funds programmed. The ROW funds will be better utilized in the CON phase. The project will remain fully funded. The funding distribution change moving ROW funds to the CON phase will not affect the benefit of the project as described in the project application.

If proposed change will delay one or more components, clearly explain 1) reason the delay, 2) cost increase related to the delay, and 3) how cost increase will be funded

Other Significant Information**SECTION 2 - For SB1 Projects Only**

Project Amendment Request (Please follow the individual SB1 program guidelines for specific criteria)

SECTION 3 - All Projects**Approvals**

I hereby certify that the above information is complete and accurate and all approvals have been obtained for the processing of this amendment request.*

Name (Print or Type)	Signature	Title	Date

Attachments

- 1) Concurrence from Metropolitan Planning Organization and/or Regional Transportation Planning Agency
- 2) Project Location Map



SOUTHERN CALIFORNIA
ASSOCIATION OF GOVERNMENTS
900 Wilshire Blvd., Ste. 1700
Los Angeles, CA 90017
T: (213) 236-1800
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May 5, 2025

Ms. Anja Aulenbacher
Associate Deputy Director
California Transportation Commission
1120 N Street, MS 52
Sacramento, CA 95814

**RE: SCAG Support for the City of Cudahy's Funding Distribution Change
Request – Salt Lake Avenue Pedestrian Accessibility Project**

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Tim Sandoval, Pomona

Dear Ms. Aulenbacher:

On behalf of the Southern California Association of Governments, I write in support of the City of Cudahy's request for a funding distribution change for the Salt Lake Avenue Pedestrian Accessibility Project, which was awarded through the MPO Component of ATP Cycle 6.

The City of Cudahy proposed widening the sidewalk and installing pedestrian lighting, signage, curb extensions, and ADA-compliant wheelchair ramps along Salt Lake Avenue. The city is requesting to move the funds programmed for the right-of-way phase to the construction phase because the proposed project is within the public right-of-way. Therefore, the right-of-way phase does not require the programmed amount (\$675,000). The funding distribution change will maintain the overall project funding (\$7,125,000).

With a funding distribution change, the project is expected to continue supporting SCAG's Active Transportation component of Connect SoCal 2024, the Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS), which encourages the development of Complete Streets that comprise a safe, multimodal network with flexible use of public rights-of-way for people of all ages and abilities using a variety of modes. Increasing non-motorized transportation infrastructure will help improve safety and enhance public health in our region.

Thank you for your continued partnership with this project. If you have questions, please feel free to contact Rachel Om, Senior Regional Planner, at (213) 630-1550 or via email at om@scag.ca.gov.

Sincerely,

Kome Ajise
Executive Director

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.32, Action

Prepared By: Jaeden Gales
Staff Services Manager I

Published Date: June 13, 2025

Subject: Approval of the 2021 Active Transportation Program Baseline Agreement Amendment for the Bob Jones Trail Gap Closure Project, Resolution ATP-P-2425-15B

Recommendation:

Staff recommends the California Transportation Commission (Commission) approve an amendment to the 2021 Active Transportation Program Baseline Agreement for the Bob Jones Trail Gap Closure Project, submitted in accordance with the Commission's Senate Bill (SB) 1 Accountability and Transparency Guidelines. The Baseline Agreement amendment reflects Commission actions for the segmentation and the amended scope, which was approved at the Commission meeting on March 20-21, 2025.

PPNO	County	Implementing Agency	Project Title (with embedded link to agreement)
1P510	San Luis Obispo	County of San Luis Obispo	Bob Jones Trail Gap Closure

Issue:

The SB 1 Accountability and Transparency Guidelines require the Commission's approval of a Baseline Agreement amendment if a project receives additional SB 1 program funds, if responsible parties change, or at the Commission's discretion. The Baseline Agreement amendment for the Bob Jones Trail Gap Closure Project incorporates the following updates approved at the March 2025 Commission meeting and will be the basis for project delivery and monitoring.

- Project segmenting
 - Segment 1 will construct the first phase of this project along the approved project alignment between the existing Octagon Barn Trailhead and Cloverridge Lane. Segment 1 is funded with \$15,953,000 in Active Transportation Program funds. There is no change to the approved project scope for Segment 1.

- Segment 2 will construct phase two of the project between Clover Ridge Lane and the existing Ontario Road Trailhead, thereby completing the full project alignment and officially “closing the gap” in the Bob Jones Trail. Segment 2 is funded with State Transportation Improvement Program funds as well as a combination of local and federal funds. Segment 2 includes an approved scope change.
- Scope change
 - The County’s approved scope change realigns Segment 2 of the multi-use trail. The design for the new alignment will traverse into state owned right-of-way and follow the west side of San Luis Creek, eliminating the need for two proposed bridges across the creek. The new alignment will result in a 0.2-mile reduction in length.

Commission staff reviewed the Baseline Agreement amendment and determined that the expected benefits, project scope, delivery schedule, project cost, and funding plan are consistent with the project amendments approved by the Commission on March 20-21, 2025.

Background:

The Commission adopted the SB 1 Accountability and Transparency Guidelines at its March 21, 2018, meeting and revised Guidelines at its March 23, 2023, meeting. The Guidelines direct agencies to provide executed Baseline Agreements that set forth the agreed-upon expected benefits, delivery schedule, project cost, and funding plan. The Baseline Agreement provides a benchmark for comparison to the status of a project for subsequent reporting purposes. The Baseline Agreement must be signed by the California Department of Transportation Director and District Director, the Commission’s Executive Director, the project applicant, and the implementing agency.

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.31, Action

Prepared By: Laurie Waters
Deputy Director

Published Date: June 13, 2025

Subject: Adoption of the 2025 Interim Timely Use of Funds Policy, Southern California Fires, Resolution G-25-51

Recommendation:

Staff recommends that the California Transportation Commission (Commission) adopt the 2025 Interim Timely Use of Funds Policy, Southern California Fires, as defined in Attachment A, applicable to the Active Transportation Program, Local Partnership Program, Solutions for Congested Corridors Program, and Trade Corridor Enhancement Program, to address impacts to project delivery because of the September 2024 and January 2025 Southern California Fires. The policy's provisions will remain in effect through completion for all projects that have received approved extensions under this Interim Policy. For example, if a pre-construction phase is impacted and uses this policy, any later delivery phase will be eligible to use the provisions under this policy.

Issue:

The 2025 Interim Timely Use of Funds Policy addresses project delivery delays caused by the September 2024 and January 2025 Line, Bridge, Airport, Palisades, Eaton, Hurst, Lidia, Sunset, Woodley, and Hughes Fires, for the counties of San Bernardino, Orange, Riverside, San Bernardino, Los Angeles, and Ventura Counties, for which State of Emergencies were issued. There remain many unknowns regarding the time necessary to deliver projects within the fire impacted areas. Where necessary, this policy allows agencies to request exceptions on a case-by-case basis. In addition, Commission staff will review this policy quarterly while impact assessments and recovery efforts are ongoing. This Interim Timely Use of Funds Policy applies to the Active Transportation Program, Local Partnership Program, Solutions for Congested Corridors Program, and Trade Corridor Enhancement Program under the Commission's purview.

Background:

All program guidelines under the Commission's purview have provisions regarding the timely use of funds. These provisions aim to promote accountability and transparency in the efficient investment of public funds.

The proposed 2025 Interim Timely Use of Funds Policy will extend the deadlines for each project delivery milestone. It will also amend previously approved time extensions and allow additional time for agencies to deliver (complete) each project milestone.

On May 22, 2025, Commission staff held a webinar to discuss the proposed 2025 Interim Timely Use of Funds Policy with transportation stakeholders in the affected counties of San Bernardino, Orange, Riverside, Los Angeles, and Ventura.

Attachments:

- Attachment A: 2025 Interim Timely Use of Funds Policy – Resolution G-25-51

**Interim Timely Use of Funds Policy
2024 and 2025 Southern California Fires
Resolution G-25-51**

In September 2024, Governor Newsom proclaimed a State of Emergency in San Bernardino, Orange, and Riverside Counties due to the Line, Bridge, and Airport Fires. In January 2025, Governor Newsom proclaimed a State of Emergency in Los Angeles and Ventura Counties due to the Palisades Fire and windstorm conditions that ignited multiple subsequent fires, including the Eaton, Hurst, Lidia, Sunset, Woodley, and Hughes Fires. These fires devastated communities across Southern California, collectively burning over 165,000 acres, destroying or damaging thousands of structures, and killing at least 27 people. Initial estimates place these disasters among the most destructive in California's history.

The California Transportation Commission (Commission) recognizes that the impacts of these disasters extend to the timely delivery of transportation projects. In response, the Commission is enacting an **Interim Timely Use of Funds Policy (Interim Policy)** applicable to San Bernardino, Orange, Riverside, Los Angeles, and Ventura Counties projects. The Commission may amend the Interim Policy after conducting at least one workgroup meeting.

Policy Scope

This Interim Policy applies only to projects programmed in the following Commission programs:

- Active Transportation Program
- Local Partnership Program
- Solutions for Congested Corridors Program
- Trade Corridor Enhancement Program

This Interim Policy does not apply to projects in the State Transportation Improvement Program (STIP) because the deadlines for that program are outlined in state law. STIP projects must receive an allocation or an allocation time extension by June 30 of the fiscal year in which the project is programmed, as specified in Government Code section 14529.8. Time extension requests for STIP projects may not exceed 20 months.

Policy Provisions

In addition to the specific provisions detailed in this policy, Commission staff will review individual project concerns that require customization and timely use of funds exceptions on a case-by-case basis. All time extension requests under this Interim Policy must be submitted directly to the California Department of Transportation (Caltrans) for processing, in accordance with the relevant program guidelines. The allocation and timely use of funds requirements outlined in the Commission program guidelines above apply to requests made under this Interim Policy.

To qualify under this Interim Policy, agencies must demonstrate that project delays are directly attributable to the Line, Bridge, Airport, Palisades, Eaton, Hurst, Lidia, Sunset, Woodley, and Hughes Fires. Impacts may include site inaccessibility, wildfire damage, labor or materials disruptions, right of way or design delays attributable to utility replacement or permit approvals, and the implementing agency should be as specific as possible with the reasons for the delay.

Effective immediately:

1. The deadline to request an allocation for projects impacted by the emergencies and programmed in fiscal year 2024-25 is extended to the December 2025 Commission meeting.
2. For projects receiving a construction allocation, the period to award a construction contract is extended from 6 months to 12 months, applicable through the duration of this Interim Policy. This extension is retroactive and includes allocations approved at the January, March, May, and June 2025 Commission meetings.
3. The maximum allowable time extension for projects programmed in the Local Partnership Program, the Solutions for Congested Corridors Program and the Trade Corridor Enhancement Program is increased to 20 months for the following types of extensions:
 - Allocation
 - Project Expenditure (pre-construction)
 - Contract Award (construction)
 - Project Completion (construction)
4. The maximum allowable time extension for projects programmed in the Active Transportation Program is increased to 24 months for the following types of extensions:
 - Allocation
 - Project Expenditure (pre-construction)
 - Contract Award (construction)
 - Project Completion (construction)
5. A one-time time extension amendment will be considered for projects with previously approved time extensions for projects programmed in the Local Partnership Program, the Solutions for Congested Corridors Program, and the Trade Corridor Enhancement Program, allowing the total extension period to be increased up to 20 months.
6. A one-time time extension amendment will be considered for projects with

previously approved time extensions in the Active Transportation Program, allowing the total extension period to be increased up to 24 months.

Policy Expiration

The policy's provisions will remain in effect through project completion (CCA) for all projects that have received approved extensions under this Interim Policy. For example, if a pre-construction phase is impacted and uses this policy, any later phase of delivery will be eligible to use the provisions under this policy. Beginning October 27, 2025, staff will evaluate the continued need for this policy on a quarterly basis.

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.10, Information

Prepared By: Sheila Ennes
Assistant Deputy Director

Published Date: June 13, 2025

Subject: Assembly Bill 744 – Draft Transportation Data Program Proposal

Summary:

California Transportation Commission (Commission) staff will present the Draft Assembly Bill (AB) 744 Proposal (Proposal) to Commissioners. This Proposal, referred to as the Transportation Data Program, outlines a framework to support statewide collaboration on transportation data, modeling, and analytic tools that improve decision-making, interagency coordination, and alignment with state transportation, housing, and climate goals. It responds to requirements established in AB 744 (Carrillo, Chapter 872, Statutes of 2023), which directs the Commission to assess existing transportation data and modeling practices and recommend strategies to improve access to high-quality data, modeling, and analytic software tools that support planning, decision-making, and interagency coordination.

The draft Proposal includes the following components:

1. An assessment of current data and modeling tools used by state, regional, and local transportation agencies.
2. The identification of common challenges and data gaps reported by agencies.
3. Pertinent findings from stakeholder engagement, including surveys, meetings, and public workshops.
4. A summary of potential procurement and funding strategies to expand access to data tools.
5. Two draft implementation options, as well as a draft third hybrid implementation option, are outlined in the paragraph below.
6. Recommendations for improving interagency coordination and technical capacity.
7. Recommendations to support the development and implementation of a statewide transportation data program in alignment with AB 744.

The three draft implementation options offer varying pathways to expand access to transportation data and analytical tools. **Option A**, the Agency-Led Procurement model, would

provide competitive grants directly to agencies, allowing them to select and procure tools that best meet their local needs. **Option B**, the Centralized Access model, would involve a state agency procuring licenses and tools on behalf of agencies and distributing access centrally, ensuring consistency and economies of scale. **Option C**, the Hybrid Implementation model, combines both approaches by offering competitive funding to agencies while also leveraging centralized procurement to reduce costs, streamline access, and support underserved jurisdictions.

Each section of the Proposal was informed by robust engagement with regional, local, and state transportation agencies, as well as other key state agencies with responsibilities related to housing, climate, and infrastructure. Over the past year, Commission staff conducted three statewide surveys designed to gather both quantitative and qualitative insights on current data practices, tool adoption, funding barriers, and interagency coordination needs. In addition, the Commission convened multiple virtual stakeholder meetings and hosted two public workshops to facilitate open dialogue, share findings, and collect feedback on potential implementation strategies. These engagement activities were intentionally designed to reflect the geographic and organizational diversity of California's transportation landscape, ensuring participation from both well-resourced and resource-constrained agencies.

Input from a broad range of public agencies helped ensure the Proposal reflects the diverse capacities and needs across jurisdictions. Feedback from transportation and housing-related state-level partners further supported alignment with interagency objectives related to data equity, climate resilience, and the integration of land use and transportation planning. Lastly, feedback from data vendors informed Proposal sections related to procurement, solicitation, and contract implementation.

The draft Proposal will be published on the Commission's website on June 13, 2025, initiating a 30-day public comment period. We encourage stakeholders to review the options and share which alternative they prefer. Comments may be submitted through July 13, 2025, and should be directed to Sheila Ennes at Sheila.Ennes@catc.ca.gov. Commission staff plan to host an additional public workshop in July 2025, where considered revisions, based on Commissioner and stakeholder feedback, will be presented. The final Proposal will be presented for approval at the Commission's August 2025 meeting.

Background:

AB 744 (Carrillo, Chapter 541, Statutes of 2023), directs the California Transportation Commission (Commission) to submit a data procurement proposal to the California State Legislature. This proposal must outline a process for either procuring transportation data platforms and granting access to data and tools to state, local, and regional agencies, or a process for allocating funds directly to state, local, and regional agencies for the procurement of their own transportation data and tools. The data and tools will support the state's sustainable transportation, congestion management, affordable housing, efficient land use, air quality, economic, and climate change strategies and goals.

Attachments:

- Attachment A: AB 744 Draft Data Procurement Proposal

JUNE

2025



CALIFORNIA
TRANSPORTATION
COMMISSION

DRAFT PROPOSAL

Assembly Bill 744

Prepared by:

Sheila Ennes

Transportation Data Program

Advancing Data-Driven Transportation
Planning in California



catc.ca.gov



AB744@catc.ca.gov

Executive Summary

Assembly Bill 744 (AB 744), enacted in 2023, directs the California Transportation Commission (Commission) to develop a proposal to procure and implement advanced data, modeling, and analytic software tools to support California's sustainable transportation and climate objectives. This initiative is designed to enhance data-driven decision-making and policy development, as well as support public agencies in delivering transportation infrastructure that better aligns with the state's environmental, economic, and housing priorities.

The bill mandates the integration of these tools to address key concerns, including:

- Congestion Management – Reducing traffic congestion and improving mobility through data-driven solutions.
- Affordable Housing & Efficient Land Use – Supporting planning efforts that balance transportation infrastructure with sustainable urban development.
- Air Quality Improvement – Leveraging analytics to monitor and reduce transportation-related emissions.
- Economic Development – Facilitating infrastructure investments that enhance economic growth and job creation.
- Climate Goal Advancement – Implementing the state's climate goals of reducing greenhouse gas emissions.

Proposal Development

This comprehensive proposal will be delivered to the Legislature once adopted by the Commission. This proposal outlines the Commission's efforts to fulfill the requirements of AB 744 through stakeholder engagement, needs assessments, and program development. Key activities in developing this proposal included surveys, public workshops, and one-on-one meetings. A list of participants involved in these activities is provided in Appendix A.

Key Findings

- Data Gaps and Inconsistencies: Agency capacity to implement advanced data analytic software and tools varies widely across the state, with some agencies actively using cutting-edge platforms, and others lacking capacity or resources to implement such tools.
- Workforce and Procurement Barriers: Many agencies lack staff expertise, face budget constraints, and report complex procurement processes as barriers to adoption.
- Equity in Access: Smaller and rural agencies often rely on consultants and cannot afford high-cost tools or licenses, underscoring the need for statewide support.
- Stakeholder Support for Centralization: Agencies expressed interest in collaborative procurement, shared services, and centralized data platforms to promote efficiency and consistency.

Implementation Options

In this proposal, the Commission presents two options, as well as a third hybrid option, for statewide implementation:

- **Agency-led Procurement through a Competitive Grant Program:** A state agency would administer a competitive grant program to allow agencies to select and procure tools based on local and regional needs, with optional consortium applications to promote cost-sharing.
- **Centralized Procurement:** A state agency, in coordination with the Department of General Services, procures licenses for selected tools and distributes access to agencies statewide.
- **Hybrid Model:** Combines direct funding to agencies with optional access to centrally procured tools through master contracts, enabling flexibility and cost efficiency.

Recommendation

To meet the goals of AB 744, the Commission recommends advancing a flexible funding and procurement strategy that ensures:

- Equitable access to high-quality data tools,
- Streamlined procurement and training support,
- Regional coordination and peer learning opportunities.

Each of the three proposed implementation options support the strategic goals of AB 744 by enhancing access to advanced analytical tools, promoting data-based decision-making, and advancing equitable outcomes. While all three options present viable pathways for improving data-driven planning and reducing barriers for resource poor agencies, the third, hybrid approach integrates the advantages of the other two options and has gained stakeholder support due to its flexibility.

By leveraging advanced technology, AB 744 positions California as a leader in sustainable and data-driven transportation planning. This proposal serves as a roadmap for responsible and effective execution, ensuring that the state meets its long-term mobility, safety, equity, economic, and environmental goals.

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I. Introduction and Background

California has long been at the forefront of integrating sustainable transportation planning with environmental and economic policy. As the state continues to address the challenges of climate change, urban growth, and transportation efficiency, advanced data analytics and transportation modeling tools have become essential for informed decision-making within every public transportation agency.

To guide environmentally responsible growth and reduce greenhouse gas (GHG) emissions, California's transportation and environmental policies have evolved. Executive Order N-19-19 (2019) required state transportation agencies to align transportation investments with state climate goals, taking measurable steps toward reducing vehicle miles traveled (VMT) and advancing sustainable mobility options. Building on this directive, the Climate Action Plan for Transportation Infrastructure (CAPTI) established a comprehensive framework to ensure that transportation investments advance equity, sustainability, and resilience. Together, these policies highlight the growing need for data-driven solutions that align transportation improvements with the state's broader sustainability and climate goals.

Despite existing policies and planning efforts, California has experienced gaps in data integration and analysis when addressing transportation and climate challenges. Traditional forecasting models often fail to capture real-time data, limiting the ability of agencies to make proactive, evidence-based decisions, such as traffic operations management. Additionally, regional disparities in access to advanced modeling tools may contribute to inconsistent transportation planning across the state.

AB 744 was introduced in 2023 to address these gaps by directing the Commission to develop a proposal to procure state-of-the-art data, modeling, and analytic software tools or provide a process for direct allocation of funding to agencies for data procurement. These tools will enhance transportation efficiency, congestion reduction, and sustainable land-use planning while supporting broader climate action goals.

AB 744 emphasizes collaboration among state, regional, and local agencies, ensuring that transportation planners across California have equitable access to advanced analytical resources. In compliance with the legislation, this proposal outlines the key actions required to implement its directives, meet statutory obligations, and advance the state's broader transportation and climate objectives. To effectively carry out these initiatives, the following steps have been pursued:

- Assess Existing Tools
Evaluate current data, modeling, and analytic software tools used by state, regional, and local agencies. Additional details are provided in Appendix B.
- Identify Data Needs
Determine the types of data sources, desired data outputs, and modeling parameters necessary for consistent data usage.
- Identify Path Forward
Develop a process for procuring these tools and granting access to them, which may include:
 - developing a process to procure data, modeling, and analytic software tools and a process to grant access to the data procured directly; or
 - developing a process to provide direct allocation of funding to agencies for data procurement; or
 - both.

This initiative aligns with the state's broader transportation and climate strategies, ensuring that California remains a leader in sustainable infrastructure development. The proposal outlined in this document provides a roadmap for implementing AB 744 efficiently, ensuring that data-driven decision-making remains at the core of California's transportation future.

II. Outreach and Engagement Activities

The Commission conducted a series of targeted engagement efforts designed to gather input, identify challenges, and align cross-agency coordination efforts. These engagement efforts included surveys, virtual meetings, and public workshops that involved state, regional, and local agencies, as well as subject matter experts within the transportation industry. The Commission designed engagement activities to ensure diverse perspectives were represented, to identify resource and data gaps, and to promote transparency throughout the implementation process. Details on participating agencies and engagement activities are included in Appendix A.

Meetings

The Commission hosted a series of virtual meetings with transportation planning agencies, transportation and housing-related state agencies, and other stakeholders. These sessions served as open forums to discuss challenges related to data procurement, identify common goals, and explore opportunities for shared tools or services. Meeting feedback helped shape the direction of survey questions.

Surveys

The Commission conducted three surveys to collect structured feedback from agencies at different levels of state and local government:

- Survey 1 (December 2023): Distributed to Metropolitan Planning Organizations to identify existing data tools, modeling capabilities, and estimated spending on analytics platforms.
- Survey 2 (February 2025): Distributed to Regional Transportation Planning Agencies and members of the Rural Counties Task Force, this survey focused on agencies' use of data tools for project evaluation, performance measurement, and policy alignment.
- Survey 3 (March 2025): Distributed to state agency partners to assess state-level engagement, tool usage, and interagency coordination needs.

Workshops

The Commission held two formal workshops to present findings, facilitate dialogue, and gather additional input:

- Workshop 1 (December 17, 2024): Introduced AB 744 requirements and initiated discussion on data tool challenges and statutory milestones. This foundational session helped align participants on key objectives.
- Workshop 2 (May 6, 2025): Highlighted survey findings and meeting feedback, focused on collaborative solution development, identified resource needs, and validated proposed implementation strategies.

III. Stakeholder Engagement Results: Agency Practices and Challenges

This section summarizes the key findings that emerged from the Commission's engagement efforts, including surveys, virtual meetings, and public workshops conducted between December 2023 and May 2025. Input was gathered from state, regional, and local agencies, with the goal of identifying current practices, challenges, and opportunities related to data, modeling, and interagency coordination in support of AB 744. The following analysis reflects patterns, gaps, and strategic insights drawn from both quantitative survey results and qualitative feedback shared through stakeholder engagement.

Direct engagement from Metropolitan Planning Organizations was limited during the first survey. The Commission incorporated supplementary data from Caltrans, including information from an inventory of reporting data points, modeling software, and other tools used by Metropolitan Planning Organizations that Caltrans compiled in response to AB 744. This information was used to better understand the role of Metropolitan Planning Organizations and fill gaps where direct engagement data was limited.

A. Agency Familiarity and Engagement with AB 744

A primary goal of the Commission was to explore the level of agency familiarity and engagement in AB 744. The information gathered highlights a varying level of awareness and involvement.

Awareness Levels

- Among Regional Transportation Planning Agencies and the Rural Counties Task Force, 75% were "somewhat familiar" with AB 744, and 25% were "very familiar".
- By contrast, 50% of responding state agencies reported no familiarity with AB 744.

Commission Engagement

- 62.5% of Regional Transportation Planning Agencies had been involved in discussions or collaborations with the Commission.
- None of the state agencies surveyed indicated engagement occurred with the Commission.

To increase awareness and foster collaboration, we implemented a targeted outreach strategy focused on key stakeholders within the transportation planning community. We initiated direct communications with Metropolitan Planning Organizations and Regional Transportation Planning Agencies to ensure they were informed about the project's objectives, progress, and opportunities for involvement. In parallel, we formally requested input from the Housing and Transportation Subcommittee, which includes subject matter experts from a diverse group of agencies specializing in transportation planning, housing policy, environmental justice, and data analysis. This group includes key state agencies such as Housing and Community Development (HCD), California Air Resources Board (CARB), California State Transportation Agency (CalSTA), California Department of Transportation (Caltrans), Office of Land Use and Climate Innovation (LCI), Strategic Growth Council (SGC), Department of Finance (DOF), and Business Consumer Services and Housing Agency (BCSH). Their expertise and insights were instrumental in shaping the project's direction and ensuring alignment with statewide priorities.

B. Use of Data and Modeling Tools

Engagement with agencies highlighted a growing reliance on data and modeling tools to inform decision-making and improve operational efficiency (see Appendix B for details). According to stakeholders, agencies are utilizing advanced data analytics and modeling software to guide planning, prioritize investments, and forecast future needs. However, responses also indicate that agencies face challenges in fully leveraging these tools, such as limited access to data, insufficient training, and resource constraints. Despite these challenges, there is recognition of the value these tools provide in optimizing transportation strategies and planning for long-term sustainability.

Most Utilized Tools Across All Agencies (see Appendix B for details)

- GIS Tools (e.g., ESRI/ArcGIS, QGIS) emerged as the most widely used across Metropolitan Planning Organizations, Regional Transportation Planning Agencies, and state agencies.
- Replica, INRIX, and StreetLight were popular among Metropolitan Planning Organizations for mobility analytics.
- Traffic Simulation Software and Data Analytics Platforms were mentioned frequently by Regional Transportation Planning Agencies.

How Agencies Utilize These Tools

- Planning (most common among all groups)
- Grant Applications
- Public Engagement
- Statewide Policy Analysis (especially at the state level)
- Compliance Evaluation with climate and housing mandates

Specialized Tools & Analytical Platforms (see Appendix B for details)

- Metropolitan Planning Organizations rely more on travel demand modeling tools like Bentley Cube, TransCAD, and EMME.
- State agencies use tools such as Google Earth Engine, R Studio, PostGIS, and FME for environmental analysis and spatial data integration.

C. Identified Workforce Challenges and Barriers

Throughout the Commission's engagement efforts, stakeholders expressed several challenges facing the transportation industry, including regulatory hurdles and financial constraints. A common theme was there are significant gaps in skill levels, as well as a lack of resources and agency staffing. Addressing these barriers and skill gaps is essential to ensuring the workforce is prepared for future needs.

Workforce Challenges

- Limited staff capacity and technical/programming expertise were the most cited challenges across Regional Transportation Planning Agencies and Metropolitan Planning Organizations.
- State agencies also cited procurement hurdles and data standardization issues (i.e., Department of General Services mechanisms).
- Regulatory and compliance knowledge gaps were mentioned, particularly among Regional Transportation Planning Agencies.

Other Barriers

- Cost and procurement limitations, particularly for smaller agencies.
- State agencies noted interagency coordination and knowledge sharing could be strengthened.
- A lack of real-time operational use of tools, as many agencies remain focused on long-term planning and grant preparation.

D. Estimated Annual Spending

Agencies provided insight into their estimated annual spending patterns, providing context for their capacity to implement the goals of AB 744. While it appears that many agencies continue to allocate substantial resources to long-term planning and grant preparation, only a portion of their budget is directed toward the enhancement of data systems and modeling capability. Responses revealed variations in spending priorities, with some agencies facing financial constraints that limit their ability to carry out AB 744's requirements for data-driven transportation and housing planning.

Metropolitan Planning Organizations

- Annual average per agency (among 16 surveyed agencies): \$414,735.
- 50% of responding agencies reported using both modeling and data tools, suggesting an emerging best practice.

Regional Transportation Planning Agencies and Rural Counties Task Force

- Average annual budget: \$207,857.
- Median budget: \$200,000.
- Budgets ranged from \$10,000 to \$650,000.

State Agencies

- No budget data reported.
- Only one agency reported a new procurement (ArcGIS and ParcelQuest in 2023), indicating limited investment in new tools since AB 744's enactment. Note, significant investment in data occurred pre-AB 744.

IV. Analysis of Gaps and Challenges

Engagement with stakeholders identified key elements related to the use, accessibility, and future needs of data and modeling tools across California's transportation planning agencies. Please see the findings below, summarized by category.

A. Inconsistencies in Data and Modeling Tools

- Adoption of Tools Vary Across Agencies – While GIS mapping is widely used, adoption of more advanced tools like data analytics platforms, simulation software, and real-time traffic operations tools remains inconsistent, especially among smaller or rural agencies.
- Inconsistent Technical Skills and Lack of Staff – Stakeholders noted that they are unable to effectively use data tools due to a lack of technical, programming, and analytical skills, along with limited staff resources. As a result, the ability to conduct complex analysis or apply modeling in decision-making is limited.
- Tools Primarily Used to Support Transportation Planning – Most agencies reported using tools for transportation planning, grant writing, and public engagement. Fewer agencies apply tools

to traffic operations or real-time analytics. Agencies expressed interest in enhanced onboarding or implementation-phase training to help bridge this gap.

B. Funding Challenges

- **Wide Budget Variability** – Reported annual budgets ranged from \$10,000 to \$650,000, with a median of \$200,000. Smaller agencies often cannot sustain high-cost tools. These disparities were emphasized by multiple agencies, including those in the Rural Counties Task Force, who noted rural agencies' reliance on consultants due to lack of internal resources.
- **Cost Barriers for Smaller Agencies** – Agencies with limited budgets often struggle to justify or sustain purchases of expensive platforms. Purchasing or maintaining data tools is often infeasible without external funding or cost-sharing.
- **Calls for Clearer Procurement Guidance** – Agencies cited the need for structured support, simplified procurement processes, and technical assistance to navigate licensing and contracting for tools and platforms.

C. Need for Centralized Procurement and Access

- **Interest in Collaborative Purchasing Models** – Some agencies have successfully implemented group purchasing and cost-sharing strategies, which help reduce costs and improve access, especially for smaller agencies.
- **Desire for Statewide Data Sharing** – A centralized, standardized data sharing platform was identified as a priority to help reduce redundancy, streamline workflows, and provide all agencies with equitable access to high-quality datasets and technical assistance. However, concerns were raised that such systems can become fragmented or underutilized without careful coordination and consistent structure.
- **Calls for Clearer Procurement Guidance** – Agencies cited the need for structured support, simplified procurement processes, and technical assistance to navigate licensing and contracting for tools and platforms. Some agencies expressed interest in hybrid models that balance competitive funding with a baseline allocation to ensure equitable access.

D. Other Key Insights

- **Need for Technical Training** – Agencies emphasized the value of state-supported training programs to build internal capacity and bridge skill gaps related to data management and modeling. Stakeholders suggested that onboarding support at the time of rollout, such as implementation phase training workshops, would improve the adoption and effectiveness of new tools. To support this need, vendors will be expected to provide onboarding and ongoing technical assistance as part of tool implementation, ensuring agency staff are prepared to use and sustain the new systems effectively.
- **Peer Learning and Workshops Are Valuable** – Stakeholders appreciate interactive forums, such as workshops, that promote peer-to-peer sharing of best practices and help identify scalable solutions.
- **Real-Time Tools Underutilized** – Despite interest in expanding analytical capabilities, real-time data tools remain underutilized, particularly among smaller agencies. Stakeholders expressed concern that resource constraints and staff limitations limit the exploration of more advanced applications such as traffic operations or on-demand forecasting.
- **Interest in Rural Resource Sharing Models** – Rural stakeholders expressed strong interest in forming consortiums or shared-service models, potentially through the Rural Counties Task

Force, to collectively access and manage tools, data, and training resources. This was identified as a key strategy to address regional inequities in technical capacity and procurement power.

- Importance of Clear Funding Guidelines and Minimum Allocations – Agencies requested more transparency around how funding alternatives might be structured and stressed the importance of establishing minimum funding thresholds to ensure baseline access for all regions.

V. Proposal Options

The following section outlines implementation options designed to address the key findings and opportunities identified in this proposal. These options are informed by best practices, stakeholder input, and practical experience, and are intended to support the successful implementation of the proposal's objectives. Each option is aligned with the overarching goals of the initiative and includes actionable steps to facilitate measurable outcomes and sustained impact. In particular, the options reflect the priorities and requirements outlined in AB 744, ensuring compliance while advancing efforts to improve program transparency, strengthen local and state partnerships, and enhance the overall effectiveness of transportation planning and projects.

A. Agency-Led Procurement Option: Competitive Grant Program for Individual Agency Access

A state agency could administer a competitive grant program, awarding funds allowing agencies to procure data and modeling tools independently. To further enhance collaboration and cost-effectiveness, the agency could also explore the establishment of a mechanism that allows multiple agencies to apply jointly as a consortium, fostering cost-sharing and more equitable access to advanced tools.

Benefits

- This is the most time-efficient option in terms of program implementation.
- Agencies would have the discretion to choose tools and software that best fit their specific transportation planning needs and goals.
- The program could target agencies with the most data needs, closing the technology gap for agencies that serve historically underserved communities and small and rural agencies that lack access to advanced data tools.
- Funds could potentially be used for training and professional development around the tools that the agencies need and plan to use.
- A one-time grant would help local agencies test potential tools for future investment and develop new technical capacity.
- The program could be structured to allow regional or joint applications, allowing agencies to implement group purchasing and cost-sharing.
- A consortium approach could promote broader knowledge sharing and collaborative tool evaluation, allowing neighboring or similar agencies to benefit from pooled experience.

Challenges

- A competitive program means that some agencies may be unsuccessful in obtaining funds.
- Some agencies may have limited resources to develop a funding application.
- A grant program does not create a centralized data system. Agencies will still be using different tools and data sources as the program would not standardize specific tools.

- Most tools are subscription based. Once the funding is exhausted, agencies will need to find alternative sources of funding to continue using the platform.
- Technical assistance and training would be necessary for less experienced agencies.
- Joint or consortium applications could introduce administrative complexities, including the need to designate lead agencies, coordinate deliverables, and manage shared responsibilities across jurisdictions.

B. Centralized Access Option: Statewide Procurement and License Distribution

A state agency could purchase software licenses and grant access to licenses to state, regional, and local agencies. The California Department of General Services, as a primary control agency for multiple agencies and departments within the state, oversees the authorization and procurement process for software and other goods and services. The process involves several steps and guidelines designed to ensure transparency, competition, and compliance with state laws and regulations.

Benefits

- Allows for more centralized access to data tools and software, creating equal conditions for agencies across California, especially for small agencies and agencies with fewer resources.
- Local agencies would not have to manage procurement or contracts, reducing administrative burden.
- Could be most cost effective in the long run due to the state's purchasing power.
- Removes the cost barrier to accessing advanced data tools for local agencies
- Using the same tools would foster more collaboration across agencies, including peer learning and best practices.

Challenges

- Anticipated high upfront cost for statewide procurement model.
- This option will involve an intensive procurement process in collaboration with the Department of General Services and would likely take significantly longer to roll out compared to a grant program.
- Some agencies might want more flexibility in the tools they use, rather than using a fixed set of resources selected by the state.
- With 482 incorporated cities, 58 counties, 18 Metropolitan Planning Organizations, and 43 Regional Transportation Planning Agencies in California, the designated state agency would need to purchase many licenses (or unlimited) and determine how to distribute the licenses.
- Not vendor neutral – the state would be committing to specific tools and services.
- Technical assistance and training would be necessary for less experienced agencies.

C. Hybrid Implementation Option: Competitive Funding with Centralized Procurement Support

A state agency could implement a hybrid approach that combines the strategic benefits of a competitive funding program with the efficiency and equity of centralized procurement. Under

this model, the state agency would allocate funds to eligible agencies through a structured, competitive process, allowing agencies the flexibility to procure data and modeling tools that best meet their specific needs.

The designated state agency could also coordinate with the Department of General Services to establish master contracts or software license agreements that participating agencies could leverage. This would streamline procurement, reduce administrative burdens, and ensure compliance with state procurement requirements. Agencies would retain the option to utilize these centralized agreements or pursue individual purchases, depending on what best suits their technical and operational contexts.

This hybrid approach balances local autonomy with state-level coordination and purchasing power, making data and modeling tools more accessible while preserving flexibility and efficiency across jurisdictions. To further enhance collaboration and cost-effectiveness, the designated state agency could also explore the establishment of a mechanism that allows multiple agencies to apply jointly as a consortium, fostering collaboration, cost-sharing, and more equitable access to advanced tools.

Benefits

- Agencies will have the choice to either procure tools independently using awarded funds or opt into centrally procured tools, depending on their capacity and needs.
- Master contracts through the Department of General Services can be leveraged, which will reduce administrative burden and speed up procurement.
- Shared licenses and centralized purchasing can reduce costs and promote efficient use of public funds.
- Allows agencies to apply jointly, enabling small or resource limited jurisdictions to collaborate, share costs, and improve access to advanced tools.
- Smaller and underserved communities can benefit from group procurement opportunities and technical support, helping close the technology gaps.

Challenges

- Administering both individual funding and centralized procurement structures may increase program complexity and require clear guidance and coordination mechanisms.
- This option could still result in varied tool adoption across agencies, limiting data standardization.
- Managing Department of General Services master agreements while concurrently overseeing a grant program may require additional staff capacity and administrative oversight.
- Agencies forming consortiums will need to designate leads, share deliverables, and coordinate implementation.
- Less experienced agencies may require ongoing support to navigate both the application and procurement processes, especially when participating in consortium models.

VI. Conclusion and Next Steps

The findings from stakeholder engagement revealed persistent gaps in data accessibility, funding support, and the standardization of tools and practices across the state. To address these challenges in alignment with the goals of AB 744, the Commission recommends advancing policy and funding strategies that supports a hybrid of the first two proposed options (competitive grants and centralized procurement) based on the approach that best meets state, regional, and local needs.

Each of the three proposed implementation options would meet the strategic goals of AB 744 by enhancing access to advanced analytical tools, promoting data-based decision-making, and promoting equitable outcomes. While each standalone approach offers distinct advantages on its own, the hybrid approach provides viable pathways to improving data-driven planning and reducing barriers for underserved agencies.

It is recommended that the State consider:

- Allocating funding to support statewide access to transportation data, modeling programs, and analytical tools, regardless of the implementation pathway selected.
- Authorizing the implementation of flexible procurement and modeling frameworks, with standards and support structures tailored to the adopted approach.
- Facilitating continued stakeholder engagement to refine and adapt the selected implementation strategy and ensure a successful rollout.

The draft proposal will be published on the Commission's website on June 13, 2025, initiating a 30-day public comment period. We encourage stakeholders to review the options and share which alternative they prefer. Comments may be submitted through July 13, 2025, and should be directed to Sheila Ennes at Sheila.Ennes@catc.ca.gov. Commission staff plan to host an additional public workshop in July 2025, where considered revisions, based on Commissioner and stakeholder feedback, will be presented. The final proposal will be presented for approval at the Commission's August 2025 meeting.

This draft proposal reflects a commitment to transforming California's transportation system into a sustainable, equitable, and technology-forward network. With appropriate funding and support, California can enhance transportation planning and decision-making across all jurisdictions.

The Commission welcomes the opportunity to collaborate further and provide technical assistance as needed to support implementation.

VII. Contact Information

For further inquiries, please contact:

Sheila Ennes
Transportation Data Program Manager
California Transportation Commission
Sheila.Ennes@catc.ca.gov or AB744@catc.ca.gov
(916) 832-3878

VIII. Resources

Assembly Bill 744

Climate Action Plan for Transportation Infrastructure (CAPTI)

Executive Order N-19-19 (2019)

IX. Appendix

Appendix A – Stakeholder Engagement Summary

A.1 Overview

This appendix provides details of stakeholder engagement efforts conducted by the Commission as part of the AB 744 implementation process. These efforts included conducting virtual meetings, distributing surveys, and facilitating public workshops to ensure inclusive and informed public participation.

A.2 Summary of Engagement Activities

Engagement Type	Date(s)	Purpose	Participants	Key Outcomes
Survey 1	December 2023	Identify data tools, modeling capabilities, and spending	Metropolitan Planning Organizations	Established baseline of data tool usage and funding gaps
Survey 2	February 2025	Understand use cases, tool performance, and agency needs	Regional Transportation Planning Agencies and Rural Counties Task Force	Collected value-based feedback and tool application insights
Survey 3	March 2025	Assess state-level collaboration and tool gaps	Transportation and housing-related state agencies	Identified awareness gaps and cross-agency coordination needs
Virtual Meetings	Ongoing (2024–2025)	Share updates, discuss challenges, gather feedback	Metropolitan Planning Organizations, Regional Transportation Planning Agencies, State Agencies, and other stakeholders	Helped refine survey focus and validated planning assumptions
Workshop 1	December 17, 2024	Introduce AB 744 and align on statutory requirements	State, Regional, and Local Agencies, Vendors, Assembly Members	Developed a shared understanding of legislative intent
Workshop 2	May 6, 2025	Share findings and discuss implementation strategies	State, Regional, and Local Agencies, Vendors, Assembly Members	Shared key findings and identified next-step priorities

A.3 Respondents by Engagement Type

Engagement Type	Responding Agency	Agency Type
Survey 1	Fresno Council of Governments Kern County of Governments Riverside County Transportation Commission San Diego Association of Governments San Luis Obispo Council of Governments Siskiyou County Local Transportation Commission Tehama County Transportation Commission	Metropolitan Transportation Organizations
Survey 2	Association of Monterey Bay Area Governments Butte County Association of Governments Del Norte Local Transportation Commission El Dorado County Transportation Commission Fresno Council of Governments Glenn County Transportation Commission Madera County Transportation Commission Merced County Association of Governments Sacramento Area Council of Governments San Bernardino County Transportation Authority San Francisco County Transportation Authority San Luis Obispo Council of Governments Santa Barbara County Association of Governments Shasta Regional Transportation Agency Siskiyou County Local Transportation Commission Stanislaus Council of Governments	Regional Transportation Planning Agencies, Rural Counties Task Force, Self-Help Agencies, Congestion Management Agencies
Survey 3	Housing and Community Development (HCD) Department of Finance (DOF) Strategic Growth Council (SGC) Office of Land Use and Climate Innovation (OPR)	State Agencies
Virtual Meetings	Glenn County Transportation Commission Humboldt County Association of Governments Los Angeles County Metropolitan Transportation Authority Madera County Association of Governments Metropolitan Transportation Commission Orange County Transportation Association Riverside County Transportation Commission San Luis Obispo Council of Governments San Mateo County Transportation Authority Santa Barbara County Council of Governments	Metropolitan Transportation Organizations, Regional Transportation Planning Agencies, Self-Help Agencies, Congestion Management Agencies

Engagement Type	Responding Agency	Agency Type
Virtual Meetings (Continued)	Shasta Regional Transportation Agency Sonoma County Transportation Authority Southern California Association of Governments	
Workshop 1	Attended by 80 members of the public.	Transportation Agencies, Assembly and Senate Members, Vendors, Consultants, Caltrans Staff, and CTC Staff
Workshop 2	Attended by 58 members of the public.	Transportation Agencies, Assembly and Senate Members, Vendors, Consultants, Caltrans Staff, and CTC Staff

Appendix B – Data and Modeling Tools Identified

B.1 Overview

This appendix provides details of data and modeling tools reported as being utilized based on recent stakeholder surveys conducted during the AB 744 implementation process. The engagement process revealed a diverse range of analytical platforms, including GIS-based mapping tools, travel demand models, and scenario planning software. It is important to note that these lists are not exhaustive and do not include all vendors or tools available in the market.

B.2 Data Tools Identified Through Stakeholder Input

Data Tools	Key Features
Replica	Urban planning, economic development, policy making
ArcGIS	Mapping, spatial analysis, urban planning
INRIX	Congestion analysis, travel times, predictive analytics
StreetLight	Transportation planning, traffic impact studies, corridor analysis
StreetSaver	Infrastructure maintenance, budget planning
NPMRDS / National Performance Management Research Data Set	Primarily used for federal and state reporting. But agencies often export NPMRDS data and use other tools (ArcGIS, Tableau, Python, R) to analyze travel time patterns.
Teralytics	Mobility behavior insights, transportation demand analysis
Placer.ai	Retail site selection, tourism analysis
RITIS / Regional Integrated Transportation Information System	Traffic management, emergency response, real-time operations
PeMS / Performance Measurement System	Traffic monitoring, congestion tracking
LOCUS	Market analysis, real estate strategy

B.3 Modeling Tools Identified Through Stakeholder Input

Modeling Tools	Key Features
Bentley Cube	Urban and regional travel demand forecasting, land use planning, network optimization
Caliper TransCAD	Network planning, travel demand modeling, freight modeling

Modeling Tools (Continued)	Key Features
Equilibre Multimodal Multimodal Equilibrium (EMME)	Traffic assignment, transit modeling, multimodal analysis

DRAFT

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.8, Information

Prepared By: Cayla McDonell
Associate Deputy Director

Published Date: June 13, 2025

Subject: Vehicle Weight Safety Study Task Force Update (Assembly Bill 251)

Summary:

California Transportation Commission (Commission) staff will present an update on the first meeting of the Vehicle Weight Safety Study Task Force (Task Force), held on June 13. During the kick-off meeting, Commission staff introduced the legislative requirements of the study and provided an overview of the Task Force process and timeline.

The Task Force is expected to meet through 2025, with staff presenting a summary of the Task Force's findings in late 2025.

Background:

Government Code Section 14527.3 requires the Commission to convene a Task Force to study the relationship between vehicle weight and injuries to vulnerable road users (such as pedestrians and cyclists) and degradation to roads, and to study the costs and benefits of imposing a passenger vehicle weight fee that considers vehicle weight ([Assembly Bill 251](#), Ward, Chapter 320, Statutes of 2023). The Task Force is required to consist of state agencies, local transportation agencies, safety advocates, and representatives from the automobile industry. The Commission approved membership of the Task Force in December 2024.

Section 14527.3 requires the Task Force to prepare a report summarizing its findings on the following topics:

- 1) An analysis of the relationship between passenger vehicle weight and vulnerable road user injuries and fatalities.
- 2) An analysis of the relationship between passenger vehicle weight and degradation of road infrastructure.

- 3) A discussion of how a passenger vehicle weight fee may change driver behavior.
- 4) A discussion of how any revenues generated by the imposition of a passenger vehicle weight fee could be directed to enhance road infrastructure that increases safety for pedestrians, bicyclists, and other vulnerable road users.
- 5) An analysis of the equity considerations relating to different population groups in the State, including persons of various demographic groups, persons residing in various regions of the State, persons with low incomes, and persons using a vehicle for commercial use versus personal use, and any appropriate adjustments for these considerations.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 4.25, Information Item

Prepared By: Michael Johnson,
State Asset Management Engineer

Subject: **TRANSPORTATION ASSET MANAGEMENT PLAN – 2025 ANNUAL
PERFORMANCE BENCHMARK REPORT**

SUMMARY:

The California Department of Transportation (Department) will present to the California Transportation Commission (Commission) its 2025 Performance Benchmark Report (Report) on the condition of highway assets.

BACKGROUND:

The California Transportation Asset Management Plan (TAMP) and Senate Bill 1 (SB1) established performance targets for the State Highway Operation and Protection Program (SHOPP) primary asset classes (pavement, bridges, culverts and traffic management system elements). To measure progress toward meeting the defined performance targets, the Commission adopted an addendum to SHOPP Guidelines in October of 2017. The addendum called on the Department to develop annual benchmarks (future condition projections) to measure progress made for each of the four primary asset classes.

The benchmark projection analysis includes uncertainty factors associated with assumptions made relative to the size of the inventory, condition, deterioration rates and expected project accomplishments. These factors were incorporated into the analysis using a Monte Carlo simulation that develops a range of potential outcomes given these uncertainties. These potential outcomes are represented graphically using a shaded range on each side of projection line shown in the lower trend line set of three charts for each asset. The Department will update the benchmark projections every year to reflect the annual project accomplishments and future uncertainties and will adjust the work plan where necessary to meet the SB 1 performance outcomes.

SB1 included two additional performance metrics related to pavement and bridges; Level of Service for pavement and a number of bridges fixed. These two are calculated using a different methodology given the nature of the measure. Both of these measures are also included in the Annual Benchmark Report.

Attachment

2025 Performance Benchmark Report

2024/25 Performance Benchmark Report

June 2025



California Department of
Transportation

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2024/25 Performance Benchmark Report

Prepared by the California Department of Transportation
June 1, 2025

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Introduction

This report presents Caltrans' efforts to date in reducing deferred maintenance through an assessment of current and projected performance accomplishments through 2027. In compliance with Federal and State requirements, Caltrans has prepared an assessment of progress against annual benchmarks associated with the four primary asset classes (pavement, bridge, transportation management systems, and drainage) for the 10-year period spanning 2018 through 2027.

The *California Transportation Asset Management Plan (TAMP)*¹ and Senate Bill 1 (SB1) each established 10-year performance targets for the State Highway Operation and Protection Program (SHOPP) primary asset classes. The California Transportation Commission (Commission) *SHOPP Guidelines*² require annual reporting to measure

¹ California Transportation Asset Management Plan, <https://dot.ca.gov/programs/asset-management/california-transportation-asset-management-plan>

² California Transportation Commission SHOPP Guidelines, May 2022, <https://catc.ca.gov/-/media/ctc-media/documents/programs/shopp/guidelines/2022-shopp-guidelines-a11y.pdf>

progress and develop projections toward meeting the defined performance targets for each of the four primary asset classes by 2027.

This report presents conditions to date in addition to updated condition projections relative to the March 2018 Commission adopted benchmarks. The updated progress reflects Commission actions through March 2024, updated condition information where available, accomplishments expected from projects in the *Ten-Year SHOPP Project Book*³, and updated Highway Maintenance projections.

SB1 includes two additional performance objectives. For pavement, the Level of Service (LOS) is reported based on cracking and spalling. For bridges, the number of additional bridges fixed is reported by annual and cumulative count to date. These two metrics will be achieved through the same project accomplishments and maintenance strategies considered in the benchmark analysis for the core assets. The department is committed to reporting progress made toward these specific performance measures so that the Commission can evaluate progress.

The annual benchmarks report also includes a summary of condition assessments for eight supplementary assets (drainage pump plants, highway lighting, office buildings, overhead sign structures, bicycle and pedestrian infrastructure, roadside rest facilities, transportation related facilities, and weigh-in-motion scales).

³ SHOPP Ten-Year Project Book, <https://dot.ca.gov/programs/asset-management/caltrans-project-portal>



Performance Summary

Caltrans continues to make progress in meeting performance targets set forth by SB1 and the TAMP. Since the passage of SB1 in 2017, Caltrans has delivered projects to address 18,969 lane miles of pavement, 720,018 linear feet of culverts⁴, and 7,547 Transportation Management System (TMS) elements, in addition to fixing a total of 2,162 bridges (an additional 1,250 above the historic baseline).

The projections presented in this report indicate that the conditions of the four primary asset classes will continue to show improvement overall. Caltrans is currently meeting and is projected to exceed SB1 condition-based targets by 2027 for pavement, bridges, and culverts, and TMS as summarized in Table 1. The SB1 requirement to fix an additional 500 bridges was met in 2022, and the trend to fix additional bridges well beyond the SB1 requirement is expected to continue. The Pavement Level of Service

⁴ A computational error was identified in the prior year's reported cumulative culvert quantity. However, this did not affect the reported drainage conditions and projections in the prior report. The cumulative total in this report has been corrected to account for the error.

(LOS) is currently exceeding the target and is projected to remain above the target through 2027.

Table 2 presents the status of progress towards achieving 2027 targets established by the Commission and set forth in the 2018 2022 TAMP, coinciding with the SB1 timeframe. Caltrans is on track to meet or exceed TAMP targets by 2027 for pavement, drainage, and TMS. Current projected conditions for bridge indicate that Caltrans will be approaching the poor target within expected uncertainty bounds by 2027. Caltrans will continue to proactively monitor progress.

Table 1 – Progress Towards 2027 SB1 Targets















Asset Class	2027 SB1 Target	Status of Progress
Pavement	98% Good or Fair Condition;	 On Track
	90% level of service (LOS) achieved for maintenance of potholes, spalls, and cracks	 On Track
Bridges	Fix an additional 500 bridges	 On Track/ Target Achieved
Culverts	90% Good or Fair Condition	 On Track
TMS	90% Good Condition	 On Track

Table 2 – Progress Towards TAMP Targets

Asset Class		Good Target	Fair Target	Poor Target	Status of Progress
Pavement	Class 1	60%	39%	1%	 On Track
	Class 2	55%	43%	2%	 On Track
	Class 3	45%	53%	2%	 On Track
Bridges and Tunnels		48.5%	50%	1.5%	 Monitor
Drainage (Culverts)		70%	20%	10%	 On Track
TMS		90%	N/A	10%	 On Track

-  **On Track** – Caltrans is on track to meet performance targets by 2027.
-  **Monitor** – Projected performance falls within uncertainty bounds, or performance metric under revision.
-  **Action Required** – Changes to plans are needed to assure that performance targets are achieved by 2027.

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Pavement Class I

Overview

Pavement Class I is comprised of route segments classified as interstate, other principal arterials, and urban freeways and expressways. It includes Freight Network Tier I and II, and the Strategic Highway Network (STRAHNET) routes. Examples of Class I routes include Sacramento 80, Ventura 101, San Diego 8, Los Angeles 210, and Alameda 580. There are 27,803 assessed lanes miles of pavement on Class I roadways, representing over half of the 50,724 assessed lane miles of pavement on the State Highway System (SHS).

Note, a complete assessment of all 52,016 lane miles of pavement on the SHS⁵ is not possible each cycle, as ongoing construction work with detours and lane deviations,

⁵ Caltrans Facts, June 2024, <https://dot.ca.gov/-/media/dot-media/programs/research-innovation-system-information/documents/caltrans-fact-booklets/caltransfacts2024-a11y.pdf>

emergency closures, and other unplanned disruptions on the system prevent the acquisition of measurements of pavement distresses in certain areas.

Changes in Asset Condition

Pavement conditions change over time because of construction activities, traffic loading, and environmental factors, such as aging and changes in temperature and moisture. Table 3 presents projected conditions of the Pavement Class I asset inventory for 2024 as well as the prior year’s condition from 2023 condition assessment data. Beginning in 2023, the Automated Pavement Condition Survey (APCS) is conducted in odd years only. As such, the conditions of pavement and Level of Service (LOS) presented in this report are based on a projection of 2024 pavement conditions using the 2023 APCS and project-level condition improvements delivered over the past year.



Timing of the Condition Assessment

Reported annual pavement conditions and total lane miles are based on a phased data collection effort through the Automated Pavement Condition Survey (APCS) over an 11-month period, typically between January and November of the reporting year. Projects under construction will not be reflected in the condition assessment.

Conditions are presented in percentages of good, fair, and poor, by lane miles corresponding to conditions at the end of calendar year. Definitions of these condition states can be found in Appendix A.

Table 3 – Pavement Class I Condition Summary

	Condition	2027 Target	2023 Year End	2024 Year End (Projection)	Change in Condition
	Good	60.0%	61.6%	63.4%	1.8%↑
	Fair	39.0%	37.0%	35.9%	-1.1%↓
	Poor	1.0%	1.4%	0.7%	-0.7%↓

Projected and Assessed Conditions

Pavement Class I benchmarks are presented in Figure 1 through Figure 3. These charts show projected year-end good, fair, and poor conditions as percentages of total lane miles from 2018 through 2027. The assessed conditions are presented in the charts with solid fill symbols. Unfilled symbols represent future projected conditions based on expected completion of projects.

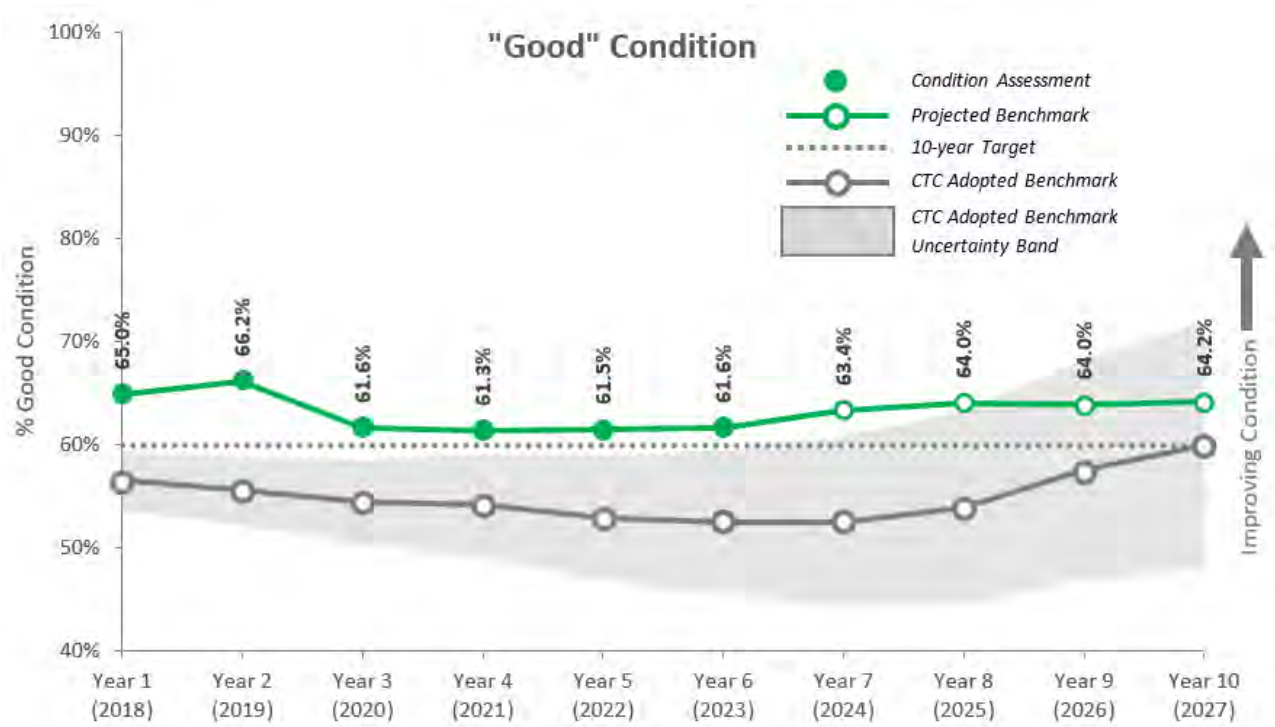


Figure 1 - Pavement Class I, Good

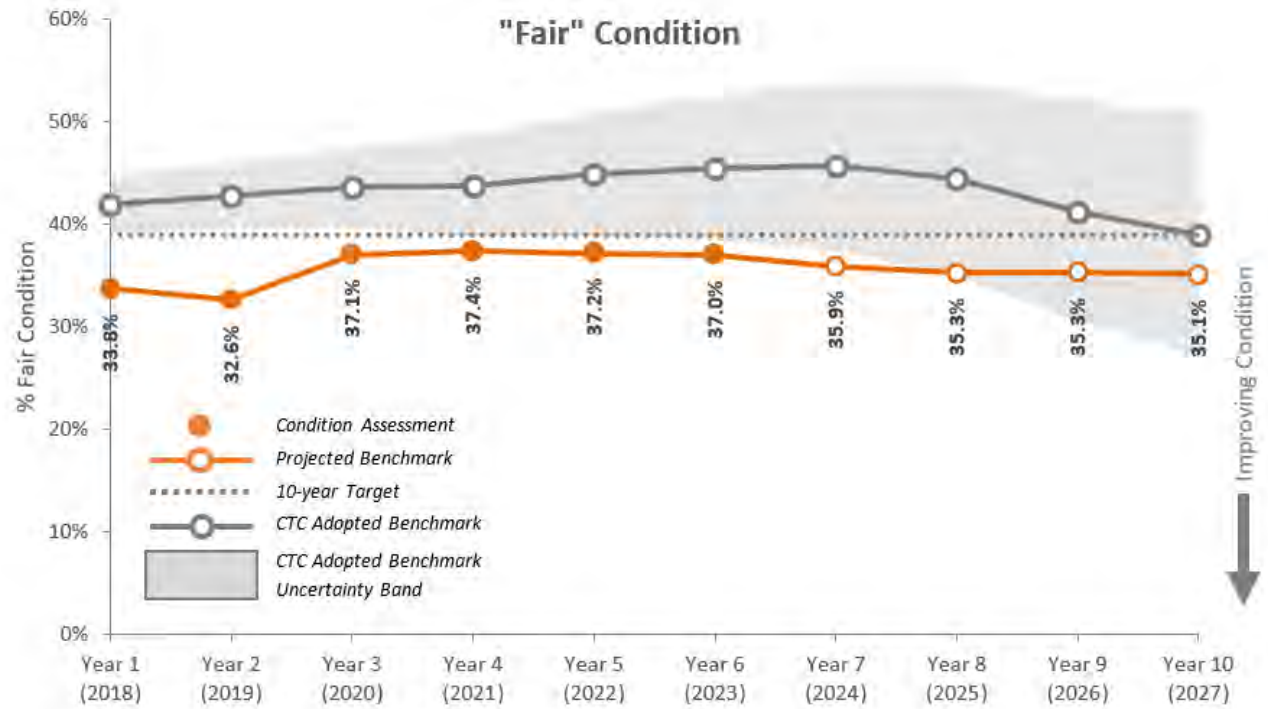


Figure 2 - Pavement Class I, Fair

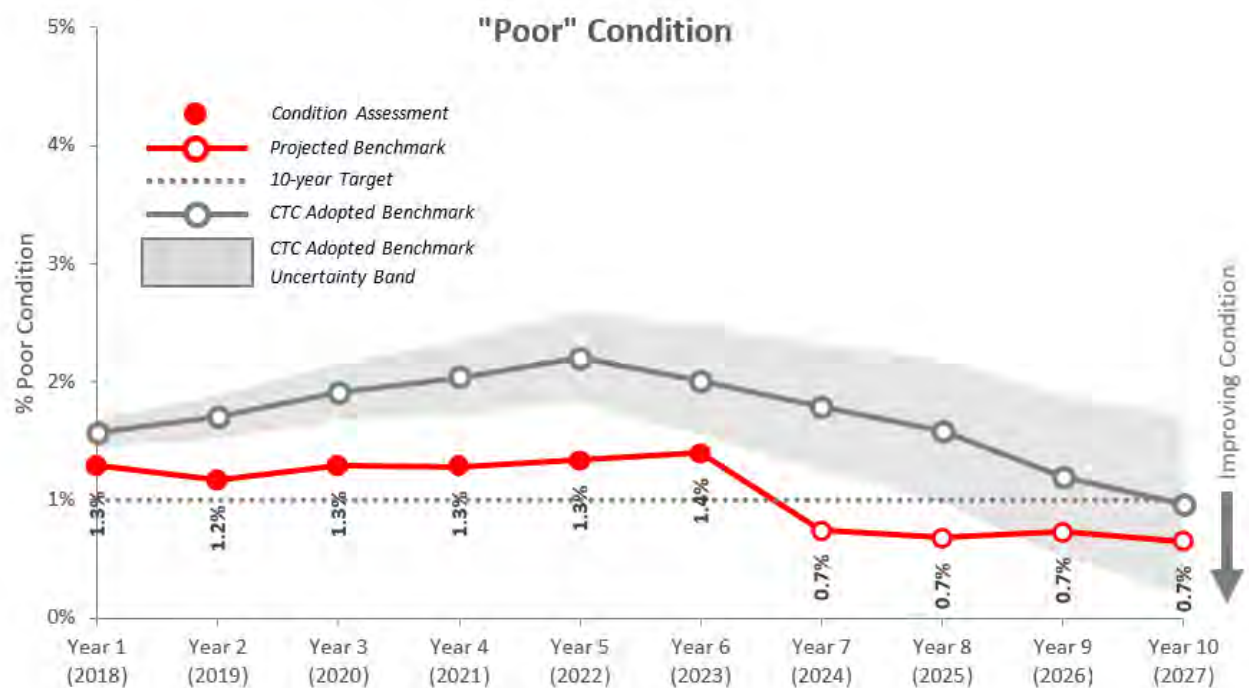


Figure 3 - Pavement Class I, Poor

Benchmark Observations

The 2023 APCS indicates that all SB1 targets are currently being met with over 98% of Pavement Class I lane miles in good or fair condition. Looking ahead, continued improvement in good condition is anticipated with a decrease in poor condition lane miles, meeting both SB1 and TAMP targets in 2027. The delivery of pavement projects supported by the initiation of SB1 funding has been a major contributing factor to these condition improvements.

Pavement Class II

Overview

Pavement Class II is comprised of route segments classified as non-interstate National Highway System and Interregional Road System (IRRS). It includes Freight Network Tier III. Examples of Class II routes include Mendocino 20, Napa 29, Monterey 1, Riverside 74, and Orange 73. There are 16,262 assessed lanes miles of pavement on Class II roadways, representing approximately one-third of the 50,724 assessed lane miles of pavement on the State Highway System (SHS).

Changes in Asset Condition

Pavement conditions change over time because of construction activities, traffic loading, and environmental factors, such as aging and changes in temperature and moisture. Table 4 presents projected conditions of the Pavement Class II asset inventory for 2024 as well as the prior year's condition from 2023 condition assessment data. Beginning in 2023, the Automated Pavement Condition Survey (APCS) is conducted in odd years only. As such, the conditions of pavement and Level of Service (LOS) presented in this report are based on a projection of 2024 pavement conditions using the 2023 APCS and project-level condition improvements delivered over the past year.

Conditions are presented in percentages of good, fair, and poor, by lane miles corresponding to conditions at the end of calendar year. Definitions of these condition states can be found in Appendix A.



Timing of the Condition Assessment

Reported annual pavement conditions and total lane miles are based on a phased data collection effort through the Automated Pavement Condition Survey (APCS) over an 11-month period, typically between January and November of the reporting year. Projects under construction will not be reflected in the condition assessment.

Table 4 - Pavement Class II Condition Summary

	Condition	2027 Target	2023 Year End	2024 Year End (Projection)	Change in Condition
	Good	55.0%	43.9%	46.1%	2.2%↑
	Fair	43.0%	54.7%	53.1%	-1.6%↓
	Poor	2.0%	1.5%	0.9%	-0.6%↓

Projected and Assessed Conditions

Pavement Class II benchmarks are presented in Figure 4 through Figure 6. These charts show projected year-end good, fair, and poor conditions as percentages of total lane miles from 2018 through 2027. The assessed conditions are presented in the charts with solid fill symbols. Unfilled symbols represent future projected conditions based on expected completion of projects.

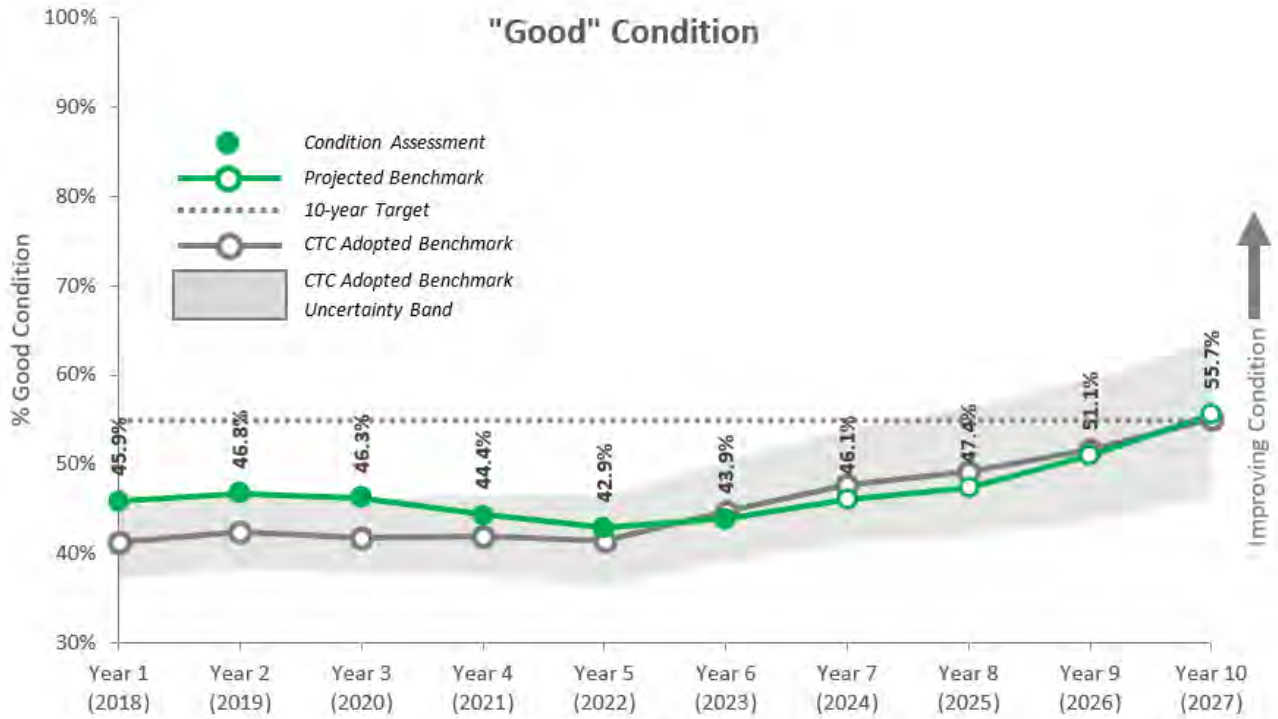


Figure 4 - Pavement Class II, Good

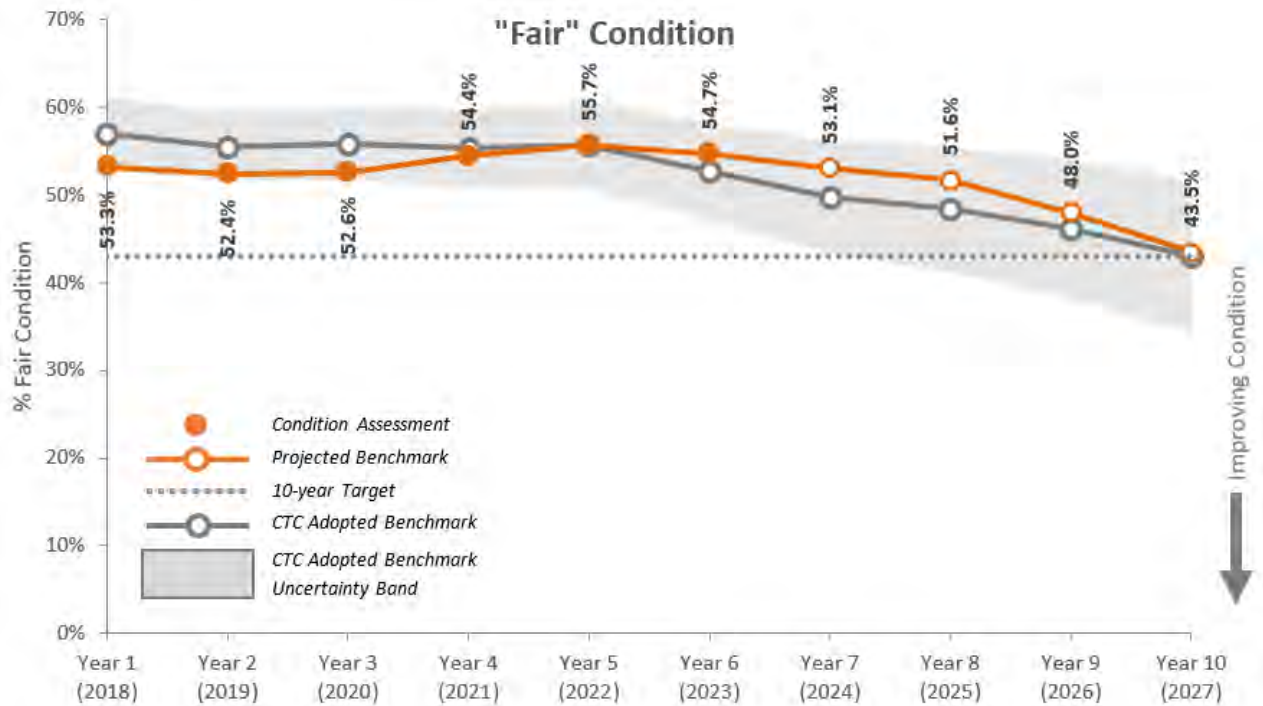


Figure 5 - Pavement Class II, Fair

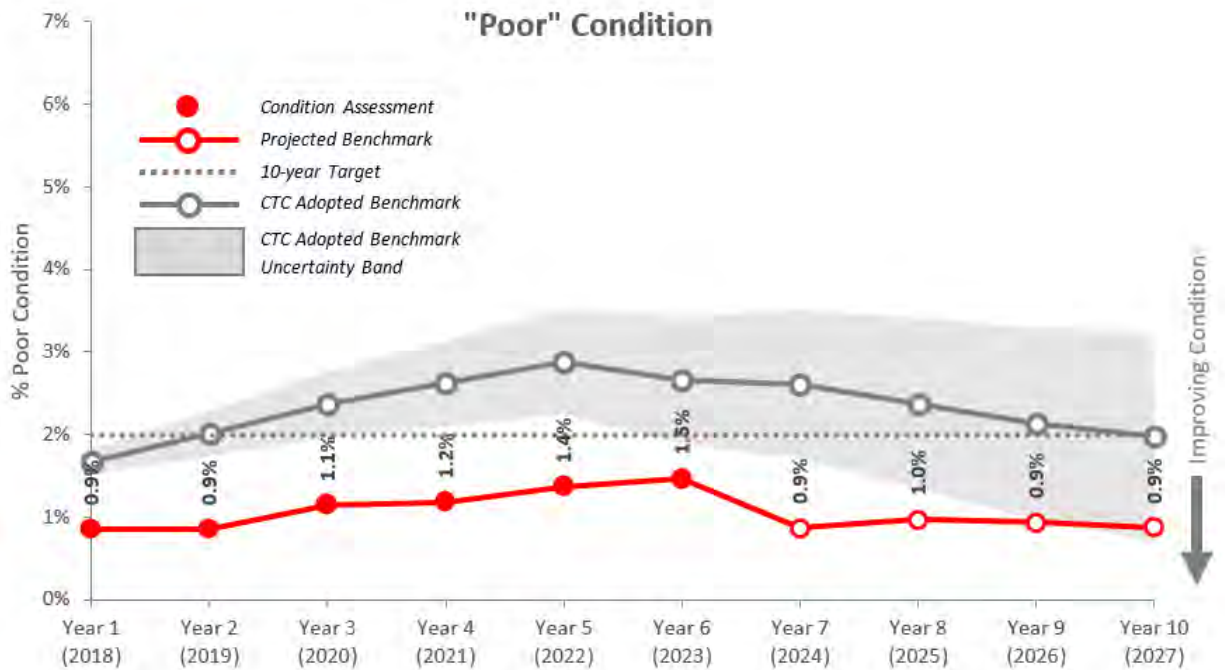


Figure 6 - Pavement Class II, Poor

Benchmark Observations

The 2023 APCS indicates that all SB1 targets are currently being met with over 98% of Pavement Class II lane miles in good or fair condition. Continued improvements are anticipated in future years with decreasing fair and poor lane miles and corresponding increase in good lane miles, meeting both SB1 and TAMP targets in 2027. The delivery of pavement projects supported by the initiation of SB1 funding has been a major contributing factor to these condition improvements.

Pavement Class III

Overview

Pavement Class III is comprised of all other routes not included in Classes I and II. Examples of Class III routes: are Trinity 3, Humboldt 36, San Luis Obispo 58, and Mono 167. There are 6,659 assessed lanes miles of pavement on Class III roadways, representing approximately 13% of the 50,724 assessed lane miles of pavement on the State Highway System (SHS).

Changes in Asset Condition

Pavement conditions change over time because of construction activities, traffic loading, and environmental factors, such as aging and changes in temperature and moisture. Table 5 presents projected conditions of the Pavement Class III asset inventory for 2024 as well as the prior year's condition from 2023 condition assessment data. Beginning in 2023, the Automated Pavement Condition Survey (APCS) is conducted in odd years only. As such, the conditions of pavement and Level of Service (LOS) presented in this report are based on a projection of 2024 pavement conditions using the 2023 APCS and project-level condition improvements delivered over the past year.



Timing of the Condition Assessment

Reported annual pavement conditions and total lane miles are based on a phased data collection effort through the Automated Pavement Condition Survey (APCS) over an 11-month period, typically between January and November of the reporting year. Projects under construction will not be reflected in the condition assessment.

Conditions are presented in percentages of good, fair, and poor, by lane miles corresponding to conditions at the end of calendar year. Definitions of these condition states can be found in Appendix A.

Table 5 - Pavement Class III Condition Summary

	Condition	2027 Target	2023 Year End	2024 Year End (Projection)	Change in Condition
	Good	45.0%	42.1%	45.7%	3.6%↑
	Fair	53.0%	56.5%	53.6%	-2.9%↓
	Poor	2.0%	1.4%	0.7%	-0.7%↓

Projected and Assessed Conditions

Pavement Class III benchmarks are presented in Figure 7 through Figure 9. These charts show projected year-end good, fair, and poor conditions as percentages of total lane miles from 2018 through 2027. The assessed conditions are presented in the charts with solid fill symbols. Unfilled symbols represent future projected conditions based on expected completion of projects.



Figure 7 - Pavement Class III, Good

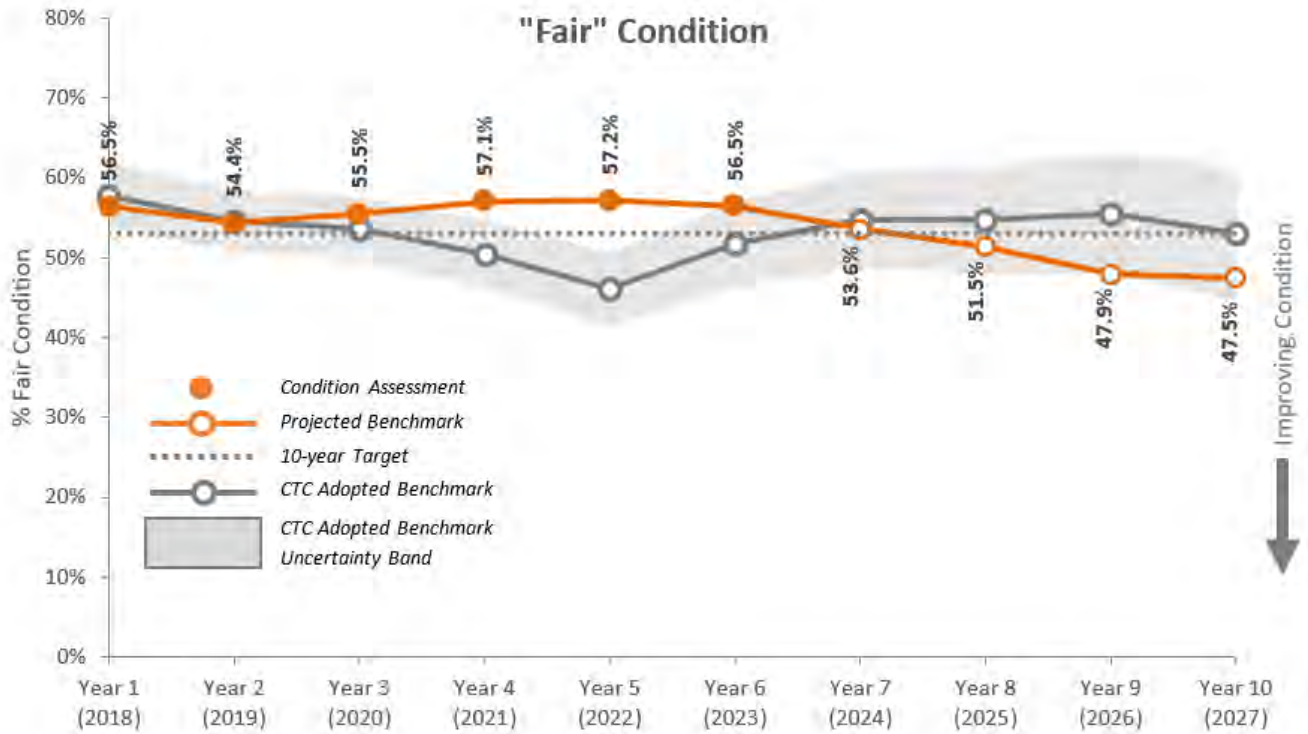


Figure 8 - Pavement Class III, Fair

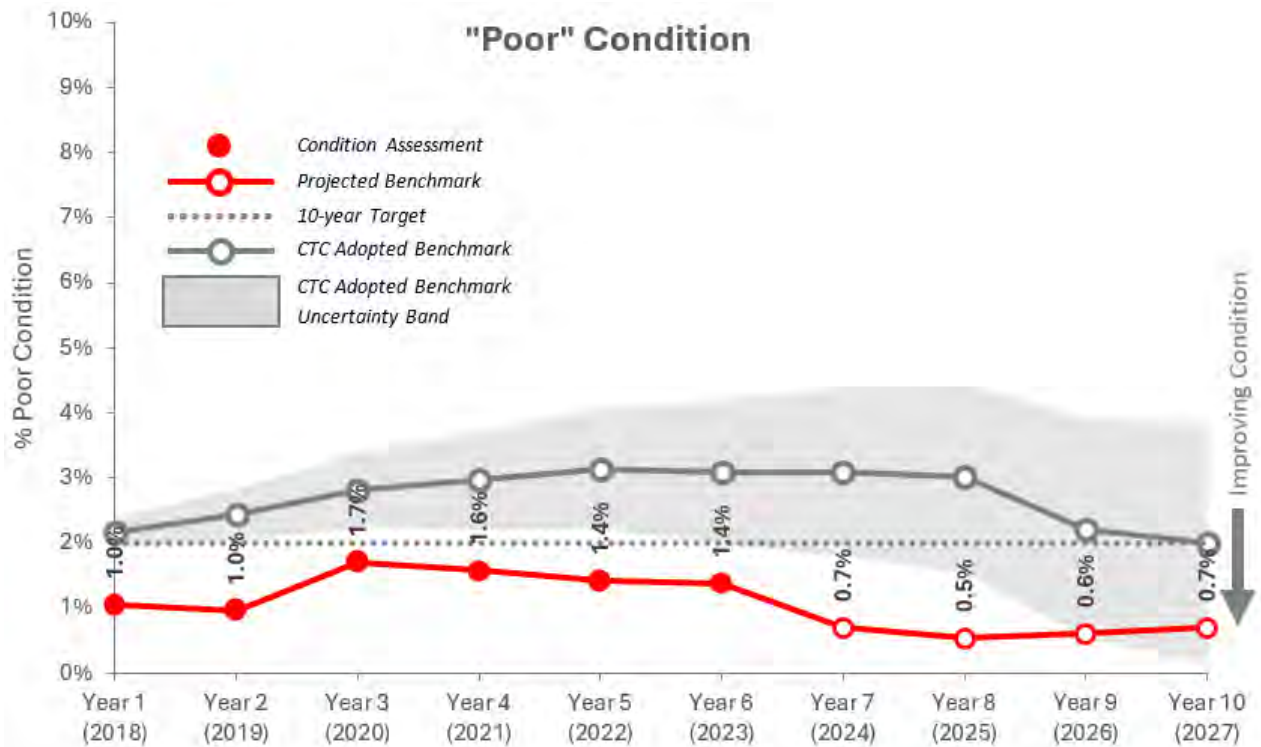


Figure 9 - Pavement Class III, Poor

Benchmark Observations

The 2023 APCS indicates that all SB1 targets are currently being met with over 98% of Pavement Class III lane miles in good or fair condition. Continued improvements are anticipated in future years with decreasing fair and poor lane miles and corresponding increase in good lane miles, meeting both SB1 and TAMP targets in 2027. The delivery of pavement projects supported by the initiation of SB1 funding has been a major contributing factor to these condition improvements.

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Bridge and Tunnel Health

Overview

Caltrans is responsible for the maintenance of 13,185 bridges on the State Highway System (SHS), totaling over 250 million square feet of bridge deck area. These bridges are on average 50 years old and at the point that typically results in increased maintenance needs. Caltrans also maintains 60 tunnels totaling approximately 5 million square feet of liner area.

Changes in Asset Condition

Under requirements established through the federal Moving Ahead for Progress in the 21st Century (MAP-21) Act, the performance measure for bridge health is based on the condition relative to the total deck area, while tunnel health is based on the total structure’s liner area. Both structure types are rated as good, fair, or poor condition.



Timing of the Condition Assessment

The reported annual bridge and tunnel health conditions are based on data collected over a multi-year inspection cycle. Most bridges are inspected every 2 years, with some bridges inspected every 4 years.

Table 6 summarizes the condition of the bridge and tunnel asset inventory for the most recent condition assessment and the prior year’s condition assessment for a year-over-year comparison. Conditions are presented in percentages of good, fair, and poor, relative to total bridge deck or tunnel liner area. The conditions presented in these benchmarks are based on a data set consistent with the subset of bridges Caltrans submitted for the National Bridge Inventory (NBI) in March 2025. Definitions of these condition states can be found in Appendix A.

Table 6 – Bridge and Tunnel Health Condition Summary

Condition	2027 Target	2023 Year End	2024 Year End	Change in Condition
Good	48.5%	44.1%	44.5%	0.4%↑
Fair	50.0%	51.1%	51.6%	0.5%↑
Poor	1.5%	4.8%	3.9%	-0.9%↓

Federal bridge inspection standards are utilized to assess good, fair, and poor conditions in all states. These standards establish a range of conditions that components of bridges are evaluated against. Per federal regulations, the overall condition reported for an individual bridge is the lowest of component ratings. A poor rating for a bridge does not mean that the bridge is unsafe for use. Any bridge determined to be unsafe for use would be immediately repaired or closed to traffic regardless of condition ratings.

Further information about federal bridge inspection standards can be found in Section 2.6 of the Commission-adopted TAMP.

Projected and Assessed Conditions

Bridge and Tunnel Health benchmarks are presented in Figure 10 through Figure 12. These charts show projected year-end good, fair, and poor conditions as percentages of total bridge deck and tunnel liner area from 2018 through 2027. The assessed conditions through 2024 are presented in the charts with solid fill symbols. Unfilled symbols represent future projected conditions based on expected completion of projects.

Note that due to the change in the fair target, as approved by the Commission in March 2021⁶, the projected bridge conditions and associated uncertainty bands (represented by the grey shaded zones) were adjusted and are only shown for years 2020 through 2027.

⁶ Recommended Asset Management Target Changes, March 2021, California Transportation Commission, Ref. 430 Action Item, <https://catc.ca.gov/-/media/ctc-media/documents/ctc-meetings/2021/2021-03/24-4-30-a11y.pdf>



Figure 10 – Bridge and Tunnel Health, Good

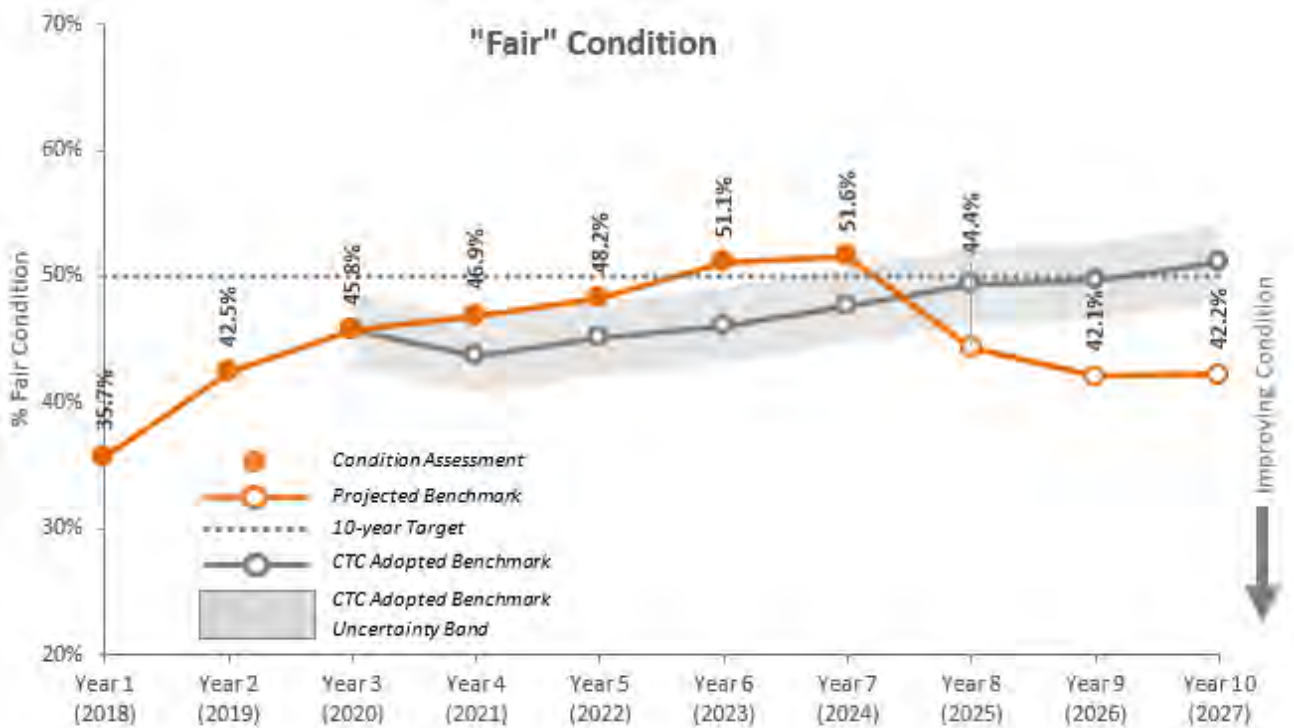


Figure 11 - Bridge and Tunnel Health, Fair



Figure 12 – Bridge and Tunnel Health, Poor

Benchmark Observations

The condition of poor bridges and tunnels has improved over the prior year, and projections indicate future reductions over the next several years. The percentage of fair condition bridges and tunnels is expected to significantly decrease over the next few years with updated inspections on recently completed bridge work being carried out.

The projected improvement trends reflect the surge in bridge focused work from projects programmed in the 2018 SHOPP supported by SB1 funding. As these projects reach completion, condition improvements will be reflected in the biennial bridge inspections. Bridge focused initiatives are also expected to contribute to the condition improvements, including a special program established in 2021 targeting poor condition bridge decks. The additional bridge-specific funding from the federal *Infrastructure Investment and Jobs Act (IIJA)* has resulted in acceleration of work on poor bridges. Several large bridges and tunnels, including the Vincent Thomas Bridge in Los Angeles and the addition of the Caldecott Tunnels in the Bay Area, comprise a significant portion of the current poor gap.

While Caltrans continues to aggressively target poor condition bridge work, current projections indicate that poor bridges will fall just short of the target. Longer project timelines for bridge and tunnel rehabilitation and replacement projects are limiting the degree of reduction in poor bridges and tunnels. As reported last year, Caltrans is closely monitoring progress on bridge work and will continue to pursue the following initiatives:

- Streamlining processes to immediately address bridges that become poor.
- Recommending continuous programming of poor bridges once identified.
- Continuing use of long lead programming, particularly for bridge replacements over waterways.
- Looking at all facets of project planning, design, and reinspection processes to reduce time between the identification of the need and measured condition improvement.



Drainage

Overview

Caltrans provides for the replacement or in-place rehabilitation of culverts and other highway drainage system elements that have lost serviceability because of age, wear, or degradation. Currently, the SHS includes 259,506 inspected and rated culverts totaling over 24.2 million linear feet, that drain rainwater, drainage channels, streams, and rivers away from highways in a controlled manner.

Changes in Asset Condition

The condition assessment of drainage assets is based on a visual inspection of five attributes: waterway adequacy, joints, materials, shape, and culvert alignment. Each attribute is scored, and culvert condition is calculated using a weighted average of attribute scores. Table 7 summarizes the condition of the drainage asset inventory for the most recent condition assessment (April 2025) as well as the prior year’s condition assessment. Conditions are presented in percentages of good, fair, and poor, by linear feet of drainage systems, corresponding to the condition at the end of calendar year. Definitions of these condition states can be found in Appendix A.



Timing of the Condition Assessment

The reported annual drainage asset conditions are determined based on the initial inspection and the expected improvements to the condition state upon completion of the restoration work. This assessment is updated monthly based on the available data.

Table 7 - Drainage Condition Summary

	Condition	2027 Target	2023 Year End	2024 Year End	Change in Condition
	Good	70.0%	74.0%	73.7%	-0.3%↓
	Fair	20.0%	16.5%	16.5%	0.0%↔
	Poor	10.0%	9.5%	9.9%	0.4%↑

Projected and Assessed Conditions

Drainage benchmarks are presented in Figure 13 through Figure 15. These charts show projected year-end good, fair, and poor conditions as percentages of total linear feet from 2018 through 2027. The assessed conditions through 2024 are presented in the charts with solid fill symbols. Unfilled symbols represent future projected conditions based on expected completion of projects.

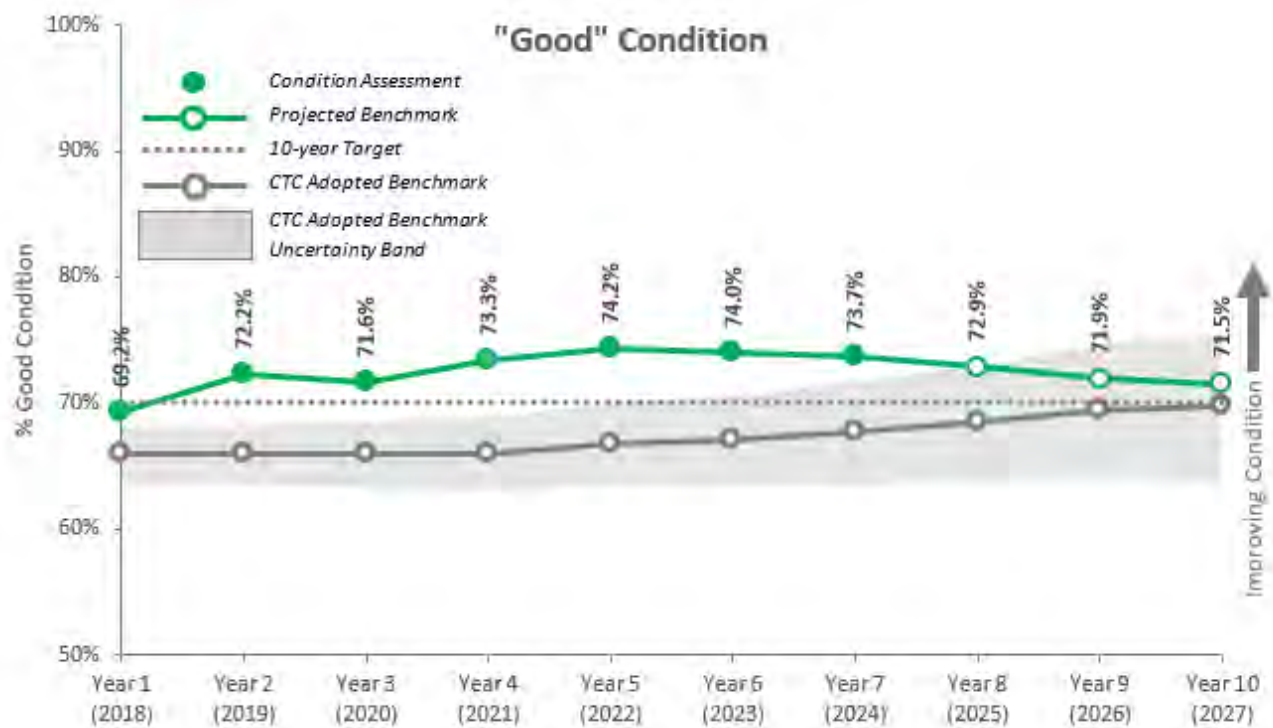


Figure 13 – Drainage, Good

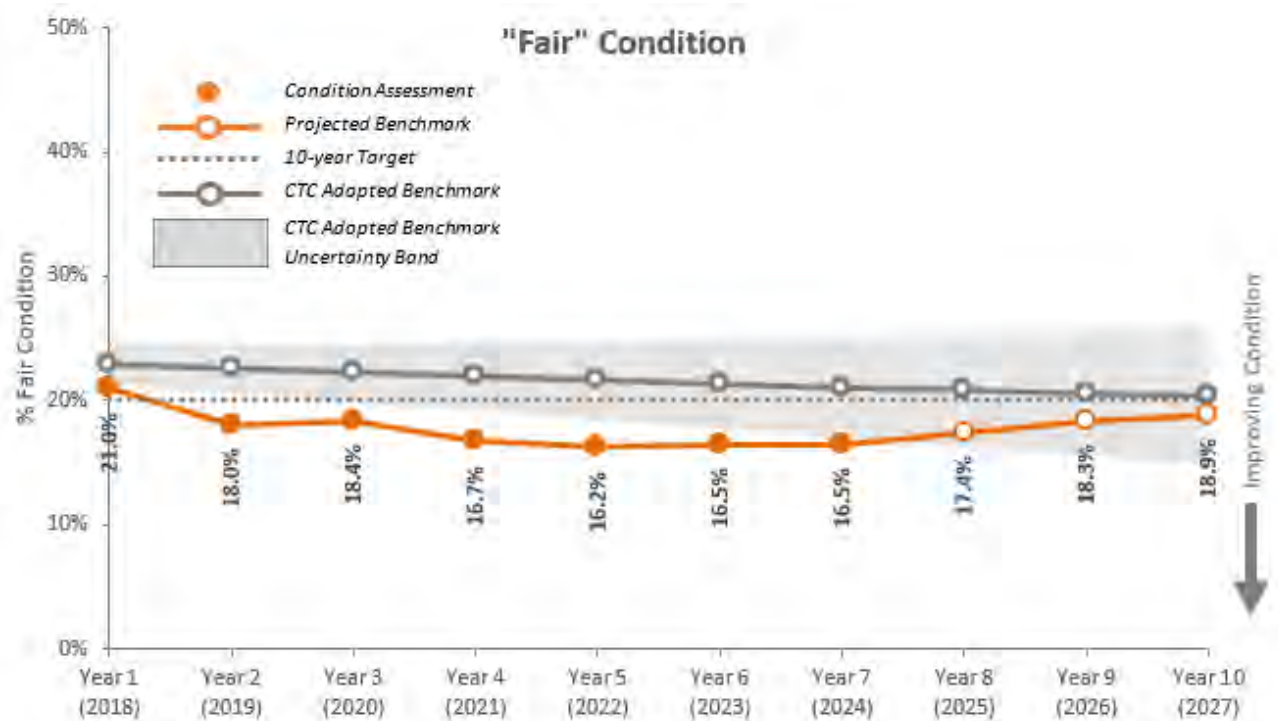


Figure 14 - Drainage, Fair

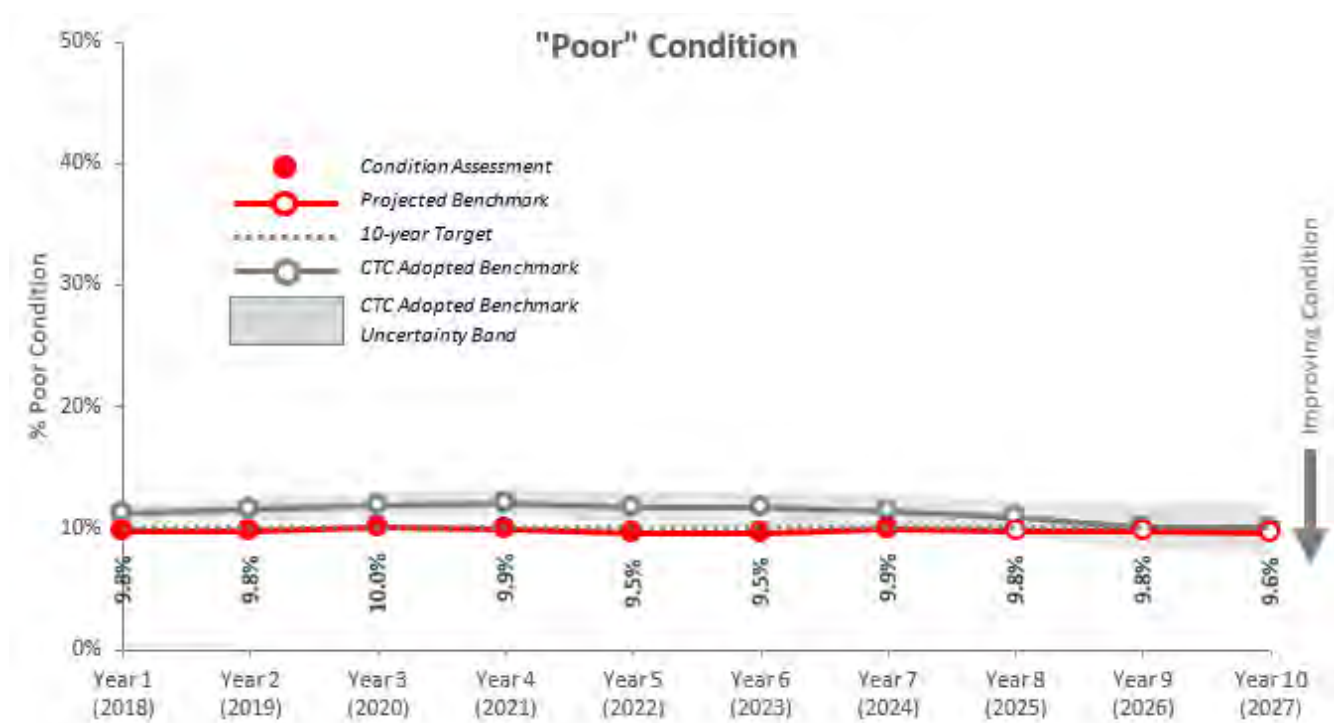


Figure 15 - Drainage, Poor

Benchmark Observations

The year-over-year assessed conditions for drainage continue to remain relatively consistent. The SB1 targets are currently being met with over 90% of drainage in good or fair condition. Projections for poor condition drainage show a gradual decrease over the next several years, while fair condition drainage is projected to gradually increase in future years. However, conditions are projected to continue to remain within both SB1 and TAMP targets through 2027.




Transportation Management Systems

Overview

A Transportation Management System (TMS) is comprised of electrical/electronic TMS units that work together to reduce highway user delay, provide traveler information, and collect information on traffic behavior. There are over 20,000 TMS units on the SHS, comprised of closed-circuit televisions, changeable message signs, traffic monitoring detection stations, highway advisory radios, freeway ramp meters, roadway weather information systems, traffic signals, traffic census stations, and extinguishable message signs.

Changes in Asset Condition

TMS units are categorized as being in either good or poor condition. The condition of a TMS unit is based on the unit being within its expected life cycle and its functional availability. Table 8 summarizes the conditions of the TMS asset inventory for the most recent year’s condition assessment as well as the prior year’s condition assessment. Conditions are presented in percentages of good and poor by TMS units for the most recent condition assessment (March 2025) as well as the prior condition assessments. Definitions of these condition states can be found in Appendix A.



Timing of the Condition Assessment

The reported annual TMS asset conditions are determined based on the age of the TMS asset and an assessment of how the TMS asset is functioning. This assessment is currently being updated quarterly.

Table 8 - Transportation Management Systems Condition Summary

Condition	2027 Target	2023 Year End	2024 Year End	Change in Condition
Good	90.0%	78.0%	78.8%	0.8%↑
Poor	10.0%	22.0%	21.2%	-0.8%↓

Projected and Assessed Conditions

TMS benchmarks are presented in Figure 16 and Figure 17. These charts show projected year-end good and poor conditions as percentages of total TMS units from 2018 through 2027. The assessed conditions through 2024 are presented in the charts with solid fill symbols. Unfilled symbols represent future projected conditions based on expected completion of projects.

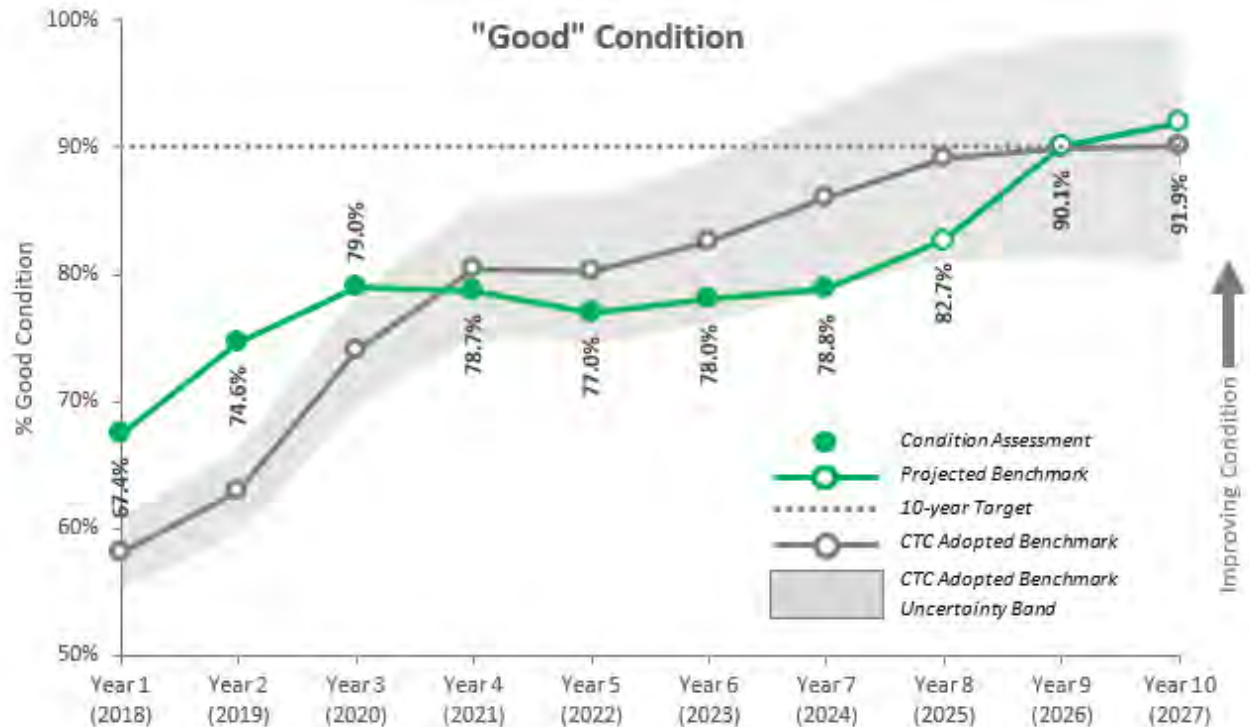


Figure 16 – Transportation Management Systems, Good

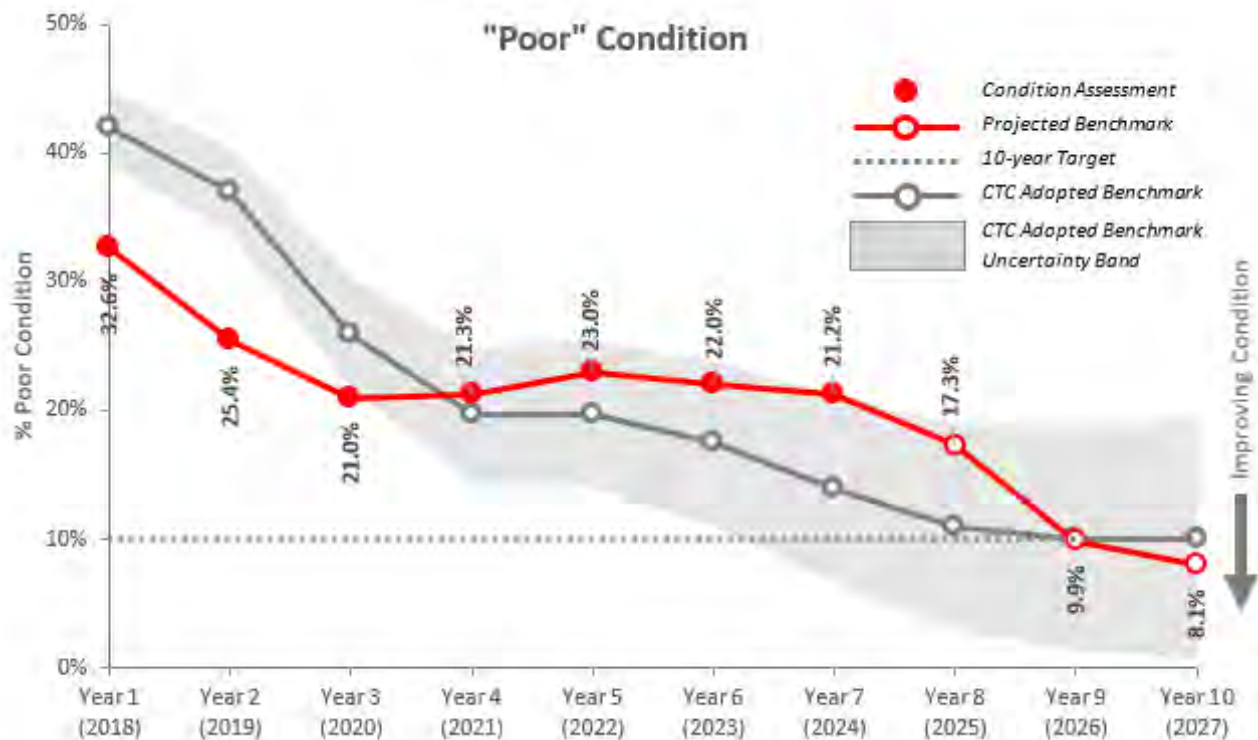


Figure 17 - Transportation Management Systems, Poor

Benchmark Observations

TMS conditions show an overall improvement over the prior year with a decrease in poor and a commensurate increase in good conditions. The projections indicate that TMS conditions are expected to continue to improve in subsequent years through to 2027 meeting SB1 and TAMP targets. Caltrans efforts to replace TMS components through the Highway Maintenance Program projects will be a significant factor in achieving targets.



Fix an Additional 500 Bridges

Overview

SB1 includes a performance requirement to fix not less than an additional 500 bridges over a 10-year period ending in 2027. Projects that improve the condition of the bridge from a lesser condition to a better condition, mitigate seismic or scour vulnerabilities, or address operational limitations are counted towards this goal. Prior to the passage of SB1, Caltrans was fixing an average of 114 bridges per year. For the purpose of counting towards the additional 500 bridges which should be fixed, Caltrans is reporting bridges fixed in excess of the annual baseline of 114 bridges between fiscal years 2017/18 and 2026/27.

Approach for Counting Fixed Bridges

The number of bridges fixed in the current and previous fiscal years is determined from an analysis of bridge project records and an estimate of when the work is effectively complete. For Major SHOPP projects, the *Expected Construction Work Complete (ECWC)* date is used, defined as the date when the primary construction work is largely

complete, the project limits are open to traffic, and benefits are realized by the travelling public. The ECWC date is estimated to be two-thirds the time between the Contract Award (CA) date and the Construction Contract Acceptance (CCA) date. For Highway Maintenance (HM) Program projects, bridges are counted as fixed in the year the contract is awarded, since the primary construction work is typically completed on average within a year of the CA date. Figure 18 presents the timeline for counting when bridges are considered fixed through the Major SHOPP and HM programs.

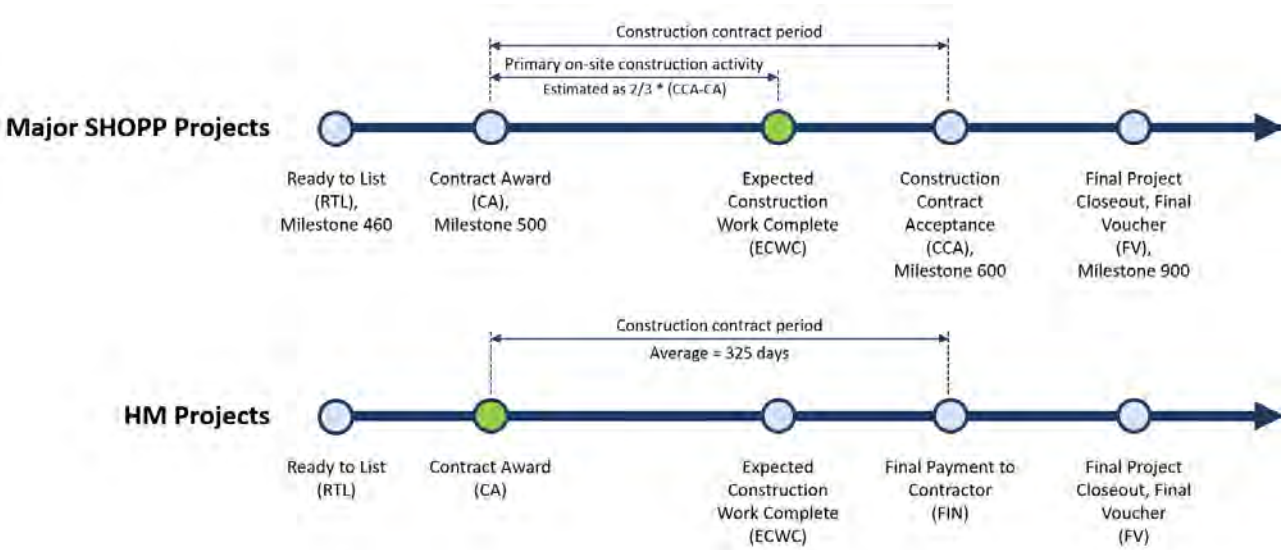


Figure 18 – Timeline for Counting When Bridges are Fixed

The use of different milestone dates between SHOPP and HM was adopted to support timely reporting of accomplishments using the most reliable project milestone data. Construction work on Major SHOPP projects typically span a period of 1 to 3 years, with the bridge portion of the work completed well before the formal closeout of the contract at CCA, especially on complex, multi-asset projects. Caltrans and the California Transportation Commission determined the ECWC date to be the most reasonable indicator of the date when the public would recognize the outcomes of the work.

For HM Program projects, the duration of the construction work period is much shorter than Major SHOPP projects, due to the types of treatments that can be applied in shorter durations. The time between Contract Award (CA) and the Final Payment (FIN)

for these projects on average is 325 days. Given the relatively short construction period, the CA date was used for HM projects. These HM projects will typically have a number of bridges included that have differing dates for when the improvements are experienced by the traveling public. In some cases, the improvements are done lane by lane over a period of multiple nights.

Current and Projected Number of Bridges Fixed

Table 9 presents the number of bridges fixed annually from Fiscal Year (FY) 2017/18 to 2024/25. The data shows that over the course of these past fiscal years, Caltrans has fixed a total of 2,162 bridges. This represents an additional 1,250 bridges fixed relative to the baseline of 114 fixed bridges per year (or 912 bridges over eight years), thereby meeting the SB1 requirement. Table 10 presents the breakdown of the counts of bridges in each fiscal year by the primary type of fix.

Table 9 – Fix an Additional 500 Bridges

Fix Bridges	FY 2017/18	FY 2018/19	FY 2019/20	FY 2020/21	FY 2021/22	FY 2022/23	FY 2023/24	FY 2024/25	Total
Baseline	114	114	114	114	114	114	114	114	912
Additional	84	130	34	242	48	281	242	189	1,250
Total	198	244	148	356	162	395	356	303	2,162

Table 10 – Count of Bridges by Type of Fix

Fix Bridges	FY 2017/18	FY 2018/19	FY 2019/20	FY 2020/21	FY 2021/22	FY 2022/23	FY 2023/24	FY 2024/25	Total
Health	194	234	133	323	142	325	321	267	1,939
Scour	1	6	3	8	8	44	27	16	113
Seismic	2	4	10	17	7	11	6	15	72
Goods Movement	1	0	2	8	5	15	2	5	38
Total	198	244	148	356	162	395	356	303	2,162

Note, the number of bridges reported fixed in prior fiscal years have been reduced by 11 bridges where the work ultimately did not fix the bridge as previously reported.

A 10-year summary of bridges fixed is presented in Figure 19. The chart shows the total number of bridges fixed, or anticipated to be fixed, each fiscal year through FY 2026/27. Bridges fixed through the SHOPP are based on projects defined in the SHOPP Ten-Year Project Book. For bridges fixed through the HM Program, the first seven years were based on projects that were in the approved HM workplan. For HM projects in the remaining years, the minimum of the first two years is used to conservatively estimate the number of bridges fixed in subsequent years, assuming consistent future HM funding.

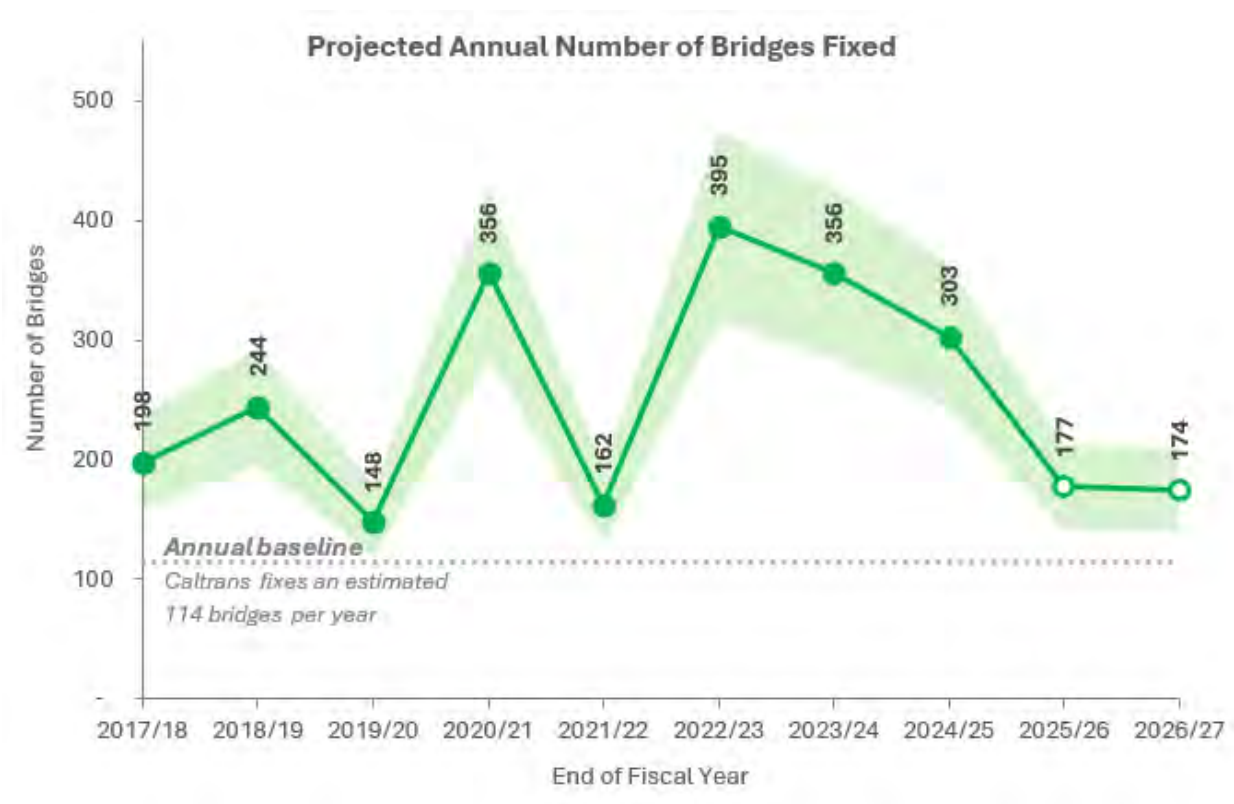
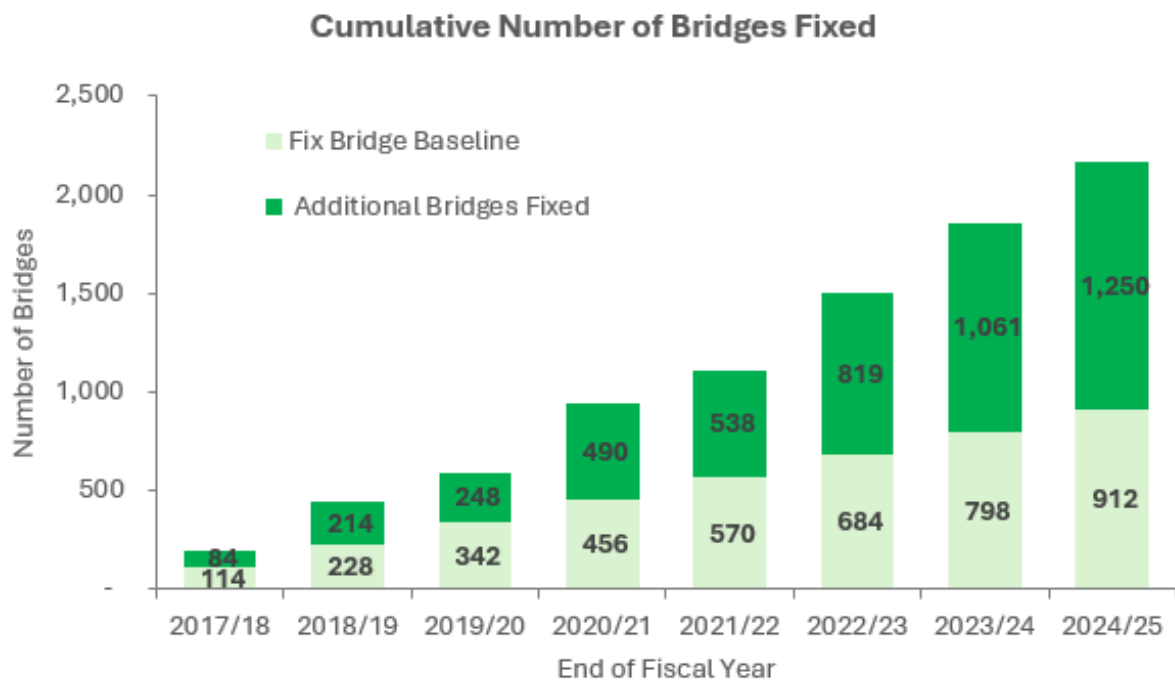


Figure 19 – Projected Number of Bridges Fixed Each Year

The bridges evaluated to be fixed in fiscal years through 2024/25 are presented in the chart with a solid fill symbol. The shaded area in the chart represents an upper and

lower boundary, quantifying two primary uncertainties from assumptions used in the analysis. First, delays in delivery of bridge projects are difficult to predict and could account for a shift of up to 20% of the projected fixed bridges in any given year. Second, programming levels for Highway Maintenance (HM) work and fluctuations in annual HM funding can be a significant source of additional uncertainty.

Caltrans was expected to fix an additional 500 bridges beyond the established baseline of 114 bridges per year. Figure 20 presents the cumulative total number of bridges fixed to date, showing that an additional 1,250 bridges have been fixed since FY 2017/18, thereby meeting SB1 requirements.



**Figure 20 – Cumulative Total Number of Bridges
Fixed to Date Above the Baseline**

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Level of Service (LOS)

Overview

SB 1 includes a performance requirement to achieve a Level of Service (LOS) for pavement cracking, spalls, and potholes of no less than 90 by 2027. LOS is a measure of how well the State Highway System (SHS) is being maintained. In the March 2020 Commission meeting, the Department put forth, and the Commission adopted, an LOS criteria based on data captured in the Automated Pavement Condition Survey (APCS). APCS utilizes state of the art pavement condition assessment technology to capture pavement conditions on 100 percent of the SHS lanes. Pavement surveys are carried out every other year and serve as the basis for statewide pavement condition analysis, reporting, and planning. The technical criteria for determining LOS are detailed in Appendix B.

Pavement Condition Assessment for LOS

The current reported LOS score is carried over from the prior year reported LOS score as presented in Table 11. Beginning in 2023, the Automated Pavement Condition Survey (APCS) is only being conducted in odd years. As such, the conditions of pavement and Level of Service (LOS) do not change in even years of reporting. The current LOS remains above the SB-1 target.

Table 11 – LOS Summary

SB 1 Target	2023 Year End	2024 Year End (Projected)	Change
90	94	94	0.0%↔



Supplementary Asset Classes

Overview

The California Transportation Commission established four primary asset classes and eight supplementary asset classes under the Transportation Asset Management Plan Guidelines in June 2017 with a subsequent revision in 2021. Supplementary Assets include:

- Drainage Pump Plants
- Highway Lighting
- Office Buildings
- Overhead Sign Structures
- Bicycle and Pedestrian Infrastructure
- Roadside Rest Facilities
- Transportation Related Facilities
- Weigh in Motion Scales

Asset Conditions

Figure 21 through Figure 28 presents the trends in supplementary asset conditions, as assessed at the end of each calendar year. Conditions are shown for the 2016 end of calendar year (as reported in the 2017 SHSMP), 2018 (as reported in the 2019 SHSMP), 2020 (as reported in the 2021 SHSMP), 2022 (as reported in the 2023 SHSMP), and 2024 (as reported in the 2025 SHSMP). Condition assessments are not available for the intervening odd numbered years. Fair and poor target conditions are shown in the charts as grey dashed lines.



Figure 21 – Condition of Drainage Pump Plants

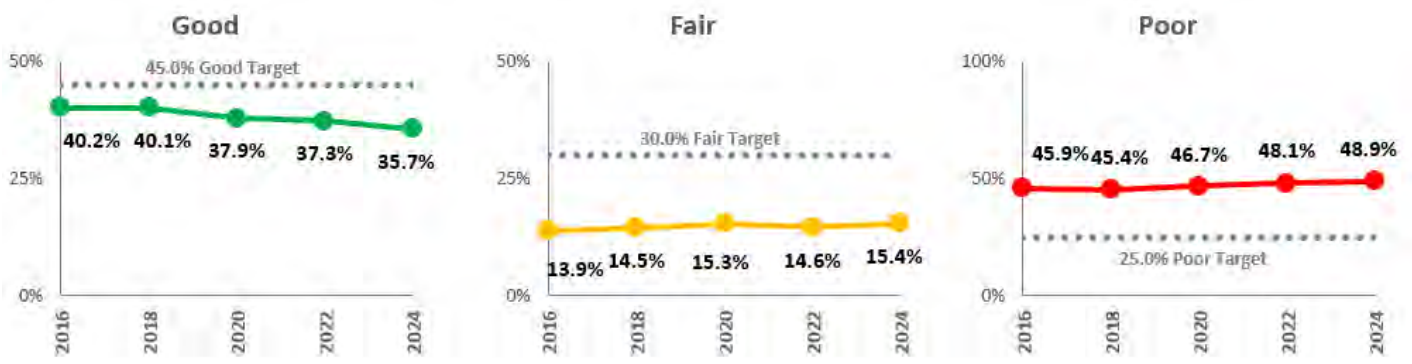


Figure 22 – Condition of Highway Lighting



Figure 23 – Condition of Office Buildings



Figure 24 – Condition of Overhead Sign Structures



Figure 25 – Condition of Bicycle and Pedestrian Infrastructure



Figure 26 – Condition of Roadside Rest Facilities



Figure 27 – Condition of Transportation Related Facilities

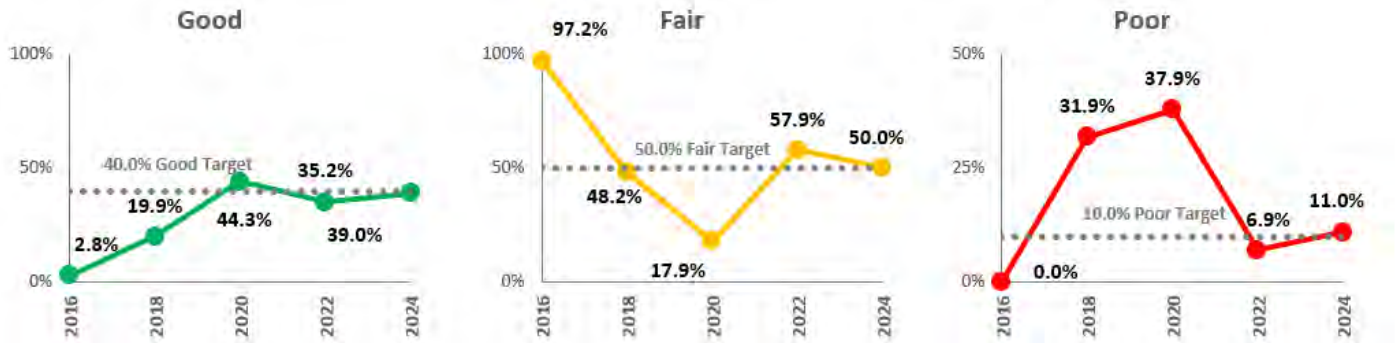


Figure 28 – Condition of Weigh in Motion Scales

Observations

Significant condition improvements were observed in the Drainage Pump Plant and Bicycle and Pedestrian Infrastructure inventories. Office Buildings had a notable shift primarily due age-based condition changes from good to fair for District 7 and 8 Office Buildings. Condition changes in Transportation Related Facilities is attributed to the implementation of the Facility Condition Index (FCI) as the condition metric. The FCI designates building conditions based on inspections and replaces the age-based condition metrics used in the past. For all other supplementary assets, modest plan over plan changes were observed with conditions remaining relatively level.

While funding to fully close performance gaps for supplementary assets has not been available to date, Caltrans continues to make investments towards maintaining and improving conditions.

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Appendix A: Definition of Good, Fair, and Poor Performance Metrics

Performance metric definitions for the four primary asset classes are presented in this section. These definitions are from the 2021 State Highway System Management Plan (SHSMP).

Pavement Class I, II, and III Metrics

Pavement condition is assessed based on the final rule of the Federal MAP-21 performance measures as of January 2017. Cracking, Rutting, and International Roughness Index (IRI) metrics are used to assess the condition of asphalt pavement; while cracking, faulting and IRI metrics are used to assess the condition of jointed plain concrete pavement (JPCP). For each of these metrics, the Federal Highway Administration (FHWA) has established thresholds, as presented in Table 18.

Table 12 – Pavement Performance Metrics

Metrics		Good	Fair	Poor
IRI (inches/mile)		<95	95-170	>170
Cracking (%)	Asphalt	<5	5-20	>20
	Jointed Concrete	<5	5-15	>15
	Continuously Reinforced Concrete	<5	5-10	>10
Rutting (inches)		<0.20	0.20-0.40	>0.40
Faulting (inches)		<0.10	0.10-0.15	>0.15

For each tenth-mile long section, condition is rated good if all three metrics for this section are rated good; poor if two or more metrics are rated poor; and fair, otherwise.

Lane miles in good, fair, and poor condition are tabulated for all sections to determine the overall percentage of pavement in good, fair, and poor condition.

Bridge and Tunnel Health Metrics

Caltrans and local agencies follow FHWA National Bridge Inventory (NBI) and National Tunnel Inspection (NTI) standards for inspecting all California bridges and tunnels. Inventory condition data is based on the most recent Bridge Inspection Reports (bridge and tunnel inspections are typically scheduled every two years) that document condition states of each individual structural element per these federal guidelines. The condition state of appropriate individual elements is then mathematically converted to a condition state (good, fair or poor) of three categories for bridges (deck, superstructure and substructure) and a single condition state for either tunnels or culverts.

Good, fair, and poor NBI ratings for bridge condition span the range from 0-9. A calculated value of 7 or greater is classified as being in good condition; 5 or 6 is classified as being in fair condition; and 4 or less is classified as being in poor condition. A bridge in poor condition is considered structurally deficient (SD) by federal guidelines. Thus, if any major component is classified as being in poor condition, the bridge will be considered SD. Being classified as SD does not imply a bridge is unsafe, just that deficiencies have been identified that require maintenance, rehabilitation, or replacement.

As a bridge is assigned a condition state for the deck, superstructure, and substructure individually, the lowest of the three ratings determines the overall rating of the bridge. Caltrans maintains all data in the Structures Maintenance and Investigations (SM&I) bridge management system databases. Table 14 and Table 15 describe the performance metrics that define the criteria for determining condition for good, fair, and poor Bridge and Tunnel Health.

Table 13 – Bridge Health Performance Metrics

Condition	Criteria
Good	Deck, superstructure, and substructure ratings are all Good, or the culvert rating is Good
Fair	The lowest of the three ratings for deck, superstructure, and substructure is Fair, or the culvert rating is Fair
Poor	The lowest of the three ratings for deck, superstructure, and substructure is Poor, or the culvert rating is Poor

Table 14 – Tunnel Health Performance Metrics

Condition	Criteria
Good	Less than 20% of the elements are classified as deteriorated
Fair	More than 20% of the elements are classified with minor deterioration
Poor	More than 20% of the elements are classified with significant deterioration

Drainage Metrics

The health condition assessment of Drainage Restoration assets is based on a visual inspection of five attributes: waterway adequacy, joints, materials, shape, and culvert alignment. Each attribute is scored, and culvert condition is calculated using a weighted average of attribute scores. Table 16 describes the performance metrics for determining condition for good, fair, and poor Drainage Restoration.

Table 15 – Drainage Performance Metrics

Condition	Criteria
Good	Overall health score between 80 to 100
Fair	Overall health score between 50 to 79
Poor	Overall health score between 0 to 49

Transportation Management System Metrics

TMS units are categorized as being in either good or poor condition. The condition of a TMS unit is based on the unit being within its expected life cycle and its functional availability. Table 17 describes the performance metrics for determining good, fair, and poor Transportation Management Systems.

Table 16 – TMS Performance Metrics

Condition	Criteria
Good	Within expected lifecycle and consistent functional availability
Fair	N/A
Poor	Beyond expected life cycle or is not meeting functional availability because of chronic down time

Appendix B: Criteria for Pavement Level of Service (LOS)

The Department recommends the Commission adoption of the following SB1 pavement LOS criteria that segregates the State Highway System lanes into segments that are each approximately 528 feet long and approximately 12 feet wide. Each segment will be evaluated for cracking and spalls or potholes depending on the material in accordance with the technical criteria below. Each segment will be deemed as passing or failing. The calculation used to determine the pavement LOS is a weighted average of the scores for asphalt and concrete segments.

The SB1 Pavement LOS is calculated using the following formula:

$$SB\ 1\ Pavement\ LOS = (FPS * W_F + RPS * W_R) / Total\ number\ of\ segments$$

where the Flexible Pavement Score (FPS) is defined as:

$$FPS = [Percent\ Passing\ Cracking + Percent\ Passing\ Potholes] / 2$$

the Rigid Pavement Score (RPS) is defined as:

$$RPS = [Percent\ Passing\ Cracking + Percent\ Passing\ Spalling] / 2$$

and the Weighting (W) is defined as:

$$W = Total\ number\ of\ flexible\ or\ rigid\ pavement\ segments$$

The technical criteria for determining if a segment passes or fails is shown in Table 17:

Table 17 – Pavement LOS Criteria

Attribute	Failure Criteria	Description
Cracking	cracking \geq ½ inch	Any single crack at its widest point
Potholes	> 1 each pothole	Any potholes greater than 36 square inches in area
Spalling	\geq 1 sq. ft.	Cumulative area of spalling

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California Department of Transportation
1120 N Street
Sacramento, CA 95814
www.dot.ca.gov

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.29, Action

Prepared By: Rebecca Light
Assistant Deputy Director

Published Date: June 13, 2025

Subject: Adoption of the 2025 State Route 710 Local Alternative Transportation Improvement Program, Resolution G-25-50

Recommendation:

Staff recommends that the California Transportation Commission (Commission) adopt the 2025 State Route (SR) 710 Local Alternative Transportation Improvement Program (LATIP) in accordance with the attached resolution (Attachment A) and recommendations (Attachment B).

Issue:

Commission staff recommends four projects totaling \$1,217,000 in SR-710 Local Alternative Transportation Improvement Program funding, less the \$238,000 distribution to the City of Los Angeles, which the City of Los Angeles has requested for programming in a future SR-710 program adoption due to the large wildfires that occurred in the County of Los Angeles at the beginning of 2025, which reduced the amount of time to conduct robust community outreach for project selection.

The investments made by this program will benefit the communities of Alhambra, La Cañada Flintridge, Pasadena, and South Pasadena by enhancing the safety of bicyclists, pedestrians, and people with disabilities through the improvement or construction of infrastructure. Improvements identified in Attachment B include Americans with Disabilities Act compliant curbs and curb ramps; pedestrian hybrid beacons; storm drain installation or repair; and electrical work relating to traffic signalization and pedestrian push buttons.

Development of Project Recommendations

Prior to the Commission's adoption of the SR-710 LATIP Guidelines in October 2024, the eligible local jurisdictions consulted with the Los Angeles County Metropolitan Transportation Authority (Metro) to determine the best funding distribution method. Metro and city representatives held monthly meetings between August and December 2024 to determine how best to share the SR-710 LATIP funds. Upon agreement on December 3, 2024, the funding

share is based on six factors: population, area, road miles, distance from the SR-710 stub, number of housing properties sold, and value of properties sold. The resulting funding distribution is as follows:

Jurisdiction	Distribution Factor	Amounts
Alhambra	14.7%	\$ 221,000
La Cañada Flintridge	7.7%	\$ 117,000
Pasadena	37.1%	\$ 556,000
South Pasadena	21.6%	\$ 323,000
Zip Code 90032 in the City of Los Angeles	18.9%	\$ 283,000
TOTAL	100%	\$1,500,000

The eligible local jurisdictions have nominated projects according to the agreed-upon funding distribution factor, rounded to the nearest thousand dollars. This rounding left an excess of \$1,000, which was added to La Cañada Flintridge, which had the smallest distribution. The cities selected projects focusing on multimodal needs, regional benefits, and planned projects in their capital improvement programs. The outreach for the nominated projects included existing and planned community outreach efforts such as project public notification, hearings, community meetings, and group sessions to provide feedback.

Background:

The SR-710 LATIP was established following the suspension of the SR-710 Freeway project. Government Code Section 54237.7, added by Senate Bill 416 (Liu, Chapter 468, Statutes of 2013) and later amended by Senate Bill 381 (Portantino, Chapter 362, Statutes of 2021), requires the California Department of Transportation (Caltrans) to deposit the proceeds from the sale of State Route 710 surplus properties into the SR-710 Rehabilitation Account. Caltrans developed the SR-710 Sales Program and adopted regulations in 2016 to allow the resale of surplus properties acquired for the SR-710 project. Once the funds exceed \$1,200,000, less any reimbursements due to the federal government, the remaining funds are transferred to the State Highway Account for allocation by the Commission, through the 710 LATIP Program, to fund projects located in the cities of Pasadena, South Pasadena, Alhambra, La Cañada Flintridge, and the 90032 postal zip code. Eligible projects may include, but are not limited to, sound walls; transit and rail capital improvements; signal synchronization; left turn signals; and major street resurfacing, rehabilitation, and reconstruction.

The statute authorizes the Commission to develop guidelines for programming and allocating the funds in the SR-710 Rehabilitation Account. The statute further states that projects shall be selected and prioritized by the affected communities in consultation with the Los Angeles County Metropolitan Transportation Authority pursuant to the guidelines developed by the Commission. The Los Angeles County Metropolitan Transportation Authority shall submit a proposed program of projects, and the Commission shall have final authority to approve the projects.

Attachments:

- Attachment A: Resolution G-25-50
- Attachment B: 2025 Local Alternative Transportation Improvement Program – Proposed Project Recommendations
- Attachment C: Letter from Metro to California Transportation Commission Executive Director

**CALIFORNIA TRANSPORTATION COMMISSION
ADOPTION OF THE STATE ROUTE 710
LOCAL ALTERNATIVE TRANSPORTATION IMPROVEMENT
PROGRAM**

RESOLUTION G-25-50

- 1.1 **WHEREAS**, Government Code Section 54237.7 authorizes the California Transportation Commission (Commission) to allocate funds resulting from the sale of the State Route 710 surplus properties as specified in Government Code Sections 54235 through 54239.5; and
- 1.2 **WHEREAS**, the Commission adopted the State Route 710 Local Alternative Transportation Improvement Program guidelines on October 17, 2024; and
- 1.3 **WHEREAS**, the guidelines describe the policy, standards, criteria, and procedures for the development, adoption, and management of the State Route 710 Local Alternative Transportation Improvement Program; and
- 1.4 **WHEREAS**, the project recommendations conform to the State Route 710 Local Alternative Transportation Improvement Program guidelines and other statutory requirements for the Local Alternative Transportation Improvement Program; and
- 1.5 **WHEREAS**, the eligible jurisdictions, in consultation with the Los Angeles County Metropolitan Transportation Authority, nominated the projects to the Commission according to the agreed funding distribution; and
- 1.6 **WHEREAS**, the City of Los Angeles has requested to roll their percentage of funding to the Fiscal Year 2025-26 cycle to provide additional time for community engagement;
- 2.1 **NOW, THEREFORE, BE IT RESOLVED**, that the Commission adopts the 2025 State Route 710 Local Alternative Transportation Improvement Program as described in Attachment B of Book Item 4.29; and
- 2.2 **BE IT FURTHER RESOLVED**, that having a project included in the adopted 2025 State Route 710 Local Alternative Transportation Improvement Program is not an authorization to begin work on that project. Contracts may not be awarded, nor may work begin, until the Commission approves an allocation for a project in the adopted program and a Federal Authorization to Proceed has been received for federally funded projects.

LA County Proposed SR-710 2025 LATIP Program

Nominating and Implementing Agency	Project description & Scope of Work	Location	Proposed LATIP Funding & Phase	Total Project Cost	Planned allocation & completion date	Community Engagement Efforts
Alhambra	FY24-25 SR710 LATIP/SB-1 Street Rehabilitation Project The project will include: ADA Curb Ramp Installation (New), ADA Curb Ramp Rehab/Repair(Existing), Bicycle Lane (New), Bicycle Lane Rehab/Repair (Existing), Cost Estimating, Document Preparation, Environmental Study/Documents, Landscape Safety Rehab/Repair(Existing), Pavement Improvement (New), Pavement Rehab/Repair (Existing), Paving and/or Striping Installation (New Safety), Paving and/or Striping Rehab/Repair (Existing Safety), Preliminary Design, Project Scope Development, Sidewalk and/or Crosswalk Installation (New), Sidewalk and/or Crosswalk Rehab/Repair(Existing), Storm Drain and/or Culvert Rehab/Repair (Existing), Storm Drain/Culvert Installation (New)	Project boundaries are Valley Boulevard to the north, Fremont Avenue to the east, the I-10 Freeway to the south and the west City limit to the west.	\$221,000 CON	\$2,335,338 Sources: SB-1: \$2,114,338 LATIP: \$221,000	Allocation - August 2025 Completion- March 2026	Before the City was made aware of the 710 LATIP fund availability the project had already gone through the following public engagement efforts: - Adoption of the 2022 -2027 Street Improvements Plan - Budget Hearings for FY 2023-24; 2024-25 Budget (two-year budget adoption) Leading up to construction, the City will have the following community engagement opportunities to include 710 LATIP as a component of the project funding: - Award of Contract City Council Staff Report - Notification Letters to the Residents/business that will be directly affected by the project construction -Notice of Completion City Council Staff Report
La Canada Flintridge	Mid-Block Pedestrian Crosswalk on Foothill Blvd The project consists of upgrading the existing flashing beacon on Foothill Boulevard at 200 Foothill Boulevard (near Saint Francis High School) to a Pedestrian Hybrid Beacon (HAWK) signal. The general scope includes the removal of the existing yellow flashing beacon poles, installation of new mast arm poles, vehicle heads, pedestrian heads, pedestrian push buttons, video detection, new signal controller cabinet, new electrical service cabinet and other related electrical work. The proposed improvements will upgrade the existing flashing beacon to a traffic signal (HAWK) which will enhance safety for pedestrians and motorists.	Foothill Boulevard at 200 Foothill Boulevard (near Saint Francis High School)	\$117,000 CON	\$527,000 Sources: Local Funds: \$410,000 LATIP: \$117,000	Allocation - August 2025 Completion - Spring 2026	During the design phase of the project, the City has held various public hearings to present the project to the community at many Public Works & Traffic Commission meetings and it was approved by the Commission at its meeting on January 17, 2024.
Los Angeles	The City of Los Angeles has requested to program their allocation in FY 2025-26 to allow for more thorough community engagement.					
Pasadena	Intersection Safety Improvements on Del Mar Blvd and Kinneloa Ave This project provides for installation of a new traffic signal at Kinneloa Avenue and Del Mar Boulevard. The project also provides for new sidewalk and curb ramps along the east side of Kinneloa Avenue between Colorado Boulevard and Del Mar Boulevard. Street lighting upgrades, including new street light poles and design, will also be completed as part of this project. The proposed traffic signal at this location will also enhance the safety of the existing uncontrolled marked crosswalk at Kinneloa Street, connecting the Pasadena Community Urgent Care facility on the south side of Del Mar Boulevard to the neighborhood north of Del Mar Boulevard. Additional concrete enhancements including sidewalk and curb ramps along the segment of Kinneloa Street from Del Mar Boulevard to Colorado Boulevard will also provide increased pedestrian connectivity and safety along this corridor. Street lighting upgrades along the corridor will assist in safety and connectivity	Project is located Del Mar Blvd. at Kinneloa Ave.	\$556,000: \$100,000 - PSE \$456,000 - CON	\$770,000 Sources: Local Funds: \$214,000 LATIP: \$556,000	Allocation - October 2025 Completion - Fall 2027	The new traffic signal is part of a broader project that was included in the City's outreach efforts for the Hydrogen Fueling Station. Community Meetings were held on August 27, 2024, September 19, 2024, and September 30, 2024. Additional outreach is scheduled in May 2025.
South Pasadena	Huntington Drive and Fremont Ave Corridor Improvements The Project scope included proposed improvements along Huntington Drive and Fremont Avenue including pavement rehabilitation, curb ramp improvements for safer pedestrian crossings, high visibility crosswalks and enhanced crossing features, bike facilities, routes, and other cycling and pedestrian improvements.	Project is located along Fremont Avenue and Huntington Drive, along the entirety of both roadways, including connections to the Cities of Pasadena, Alhambra, San Marino, and Los Angeles.	\$323,000 PAED & PSE	\$16,638,645 Sources: MAT Cycle 1: \$6,056,160 Metro MIP: \$10,000,000 Measure M Local Funds: \$112,795 Measure R Local Funds: \$146,690 LATIP: \$323,000	Allocation - October 2025 Completion - FY 2028-29	The goal of this effort was to collaboratively plan and develop a community vision and alternatives through design workshops for the Project corridors. Over 100 people across numerous community groups attended the sessions – which was documented by the local newspaper. The consultant's design direction and recommendations were based on the values, aspirations, and desired roles for the streets as provided by City staff, input from officials, stakeholders, and the public during the charrette and study process.
Total			\$1,217,000*	\$20,270,983		

* LA City distribution amount is \$238,000. Total LATIP funding including Los Angeles would be \$1,500,000



Los Angeles County
Metropolitan Transportation Authority

One Gateway Plaza,
Los Angeles, CA 90012-2952

Stephanie N. Wiggins
Chief Executive Officer
213.922.7599 Tel
213.922.7447 Fax
wiggins@metro.net

April 28, 2025

Ms. Tanisha Taylor
Executive Director
California Transportation Commission
1120 N Street, Suite 2221
Sacramento, CA 95814

Mr. Steve Novotny
Chief, Office of Local Assistance
Caltrans District 7
100 South Main Street
Los Angeles, CA 90012

RE: SR-710 Local Alternative Transportation Improvement Program Nominated Projects

Dear Ms. Taylor and Mr. Novotny:

This Letter is the Los Angeles County Metropolitan Transportation Authority (Metro) request to program the projects listed in Enclosure A to the State Route (SR) 710 Local Alternative Transportation Improvement Program (SR-710 LATIP). These projects fully program the August 2024 SR-710 LATIP estimate of \$1,500,000. We ask that the California Transportation Commission (CTC) approve at its June 2025 meeting this request which has been prepared in accordance with the SR-710 LATIP guidelines. The funds are programmed for Fiscal Year 2024-2025.

The eligible local jurisdictions worked in consultation with Metro to determine the best method for distributing the funding available. Metro and city representatives held monthly meetings between August and December 2024. On December 3, 2024, the cities agreed to share the SR-710 LATIP funds based on six factors: population, area, road miles, distance from the SR-710 stub, number of housing properties sold, and value of properties sold. The eligible local jurisdictions have nominated projects according to the agreed-upon funding distribution.

The cities selected their projects with a focus on multi-modal needs, regional benefits, and planned projects in their capital improvement program. The outreach for the nominated projects included existing and planned community outreach efforts such as project public notification, hearings, community meetings, and group sessions to provide feedback.

The cities provided Metro with a list of projects for nomination. The list of projects nominated under the SR-710 LATIP is enclosed. The City of Los Angeles requested to roll their funding forward to FY 2025-26 to allow additional time for community outreach to inform the identification of the project(s). The other four cities agreed to the City of Los Angeles's request.

Thank you for considering this programming request. If you have any questions, please contact Ms. Patricia Chen at chenp@metro.net.

Sincerely,

E-SIGNED by Mark Yamarone
on 2025-04-28 21:56:57 GMT

Mark Yamarone
Executive Officer
Countywide Planning & Development

Enclosure A: LA County Proposed SR-710 2025 LATIP Program

CC: Laurie Waters, CTC
Rebecca Light, CTC
Dan Murdoch, Caltrans – District 7

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.27, Action

Prepared By: Erika Changizi
Assistant Deputy Director

Published Date: June 13, 2025

Subject: Active Transportation Program Amendment, Resolution G-25-48, Amending Resolutions G-16-32, G-17-04, G-17-29, G-19-12, G-21-30, G-21-46 and G-23-49

Recommendation:

Staff recommends the California Transportation Commission (Commission) approve an amendment to the Active Transportation Program to deprogram 20 projects that have requested cancellation as reflected in Attachment B, returning a total of \$46,069,000 to the Active Transportation Program that will be available for future funding cycles.

Issue:

Twenty projects programmed in Active Transportation Program Cycles 3, 3A, 4, 5, and 6, formally requested withdrawal from the program through submitting letters of cancellation. Commission staff has worked with the California Department of Transportation (Caltrans) to review the requests and find the recommended deprogramming consistent with Active Transportation Program Guidelines. These projects have requested cancellation due to unforeseen challenges ultimately leading to termination of the projects.

The Active Transportation Program maintains a portfolio of over 1,000 programmed projects, funded with approximately \$4.996 billion encompassing seven program cycles. The number of projects requesting cancellation represents less than two percent of all programmed projects and less than 1 percent of all programmed funding.

The Active Transportation Program cancellation process was adopted into the Cycle 6 guidelines to redirect project savings towards future program cycles. This book item captures cancelled projects from Cycles 1 through 6. As a result, a total of \$46,069,000 in funds will be returned to the Active Transportation Program. Book item Reference Number 4.28 recommends utilizing a portion of these deprogrammed funds to fully fund the two partially programmed projects from the 2025 Active Transportation Program, Statewide and Small Urban and Rural components.

Background:

The 20 projects recommended for deprogramming from the Active Transportation Program Cycles 3, 3A, 4, 5, and 6, are currently programmed as follows:

On December 7, 2016, the Commission adopted the 2017 Active Transportation Program (Cycle 3) Statewide and Small Urban and Rural Components (Resolution G-16-32), which included:

- \$73,000 for the City of Corning, First Street Class 2 Bike Lanes project.
- \$1,784,000 for the City of Cudahy, Atlantic Avenue Bicycle and Pedestrian Enhancement project.
- \$6,754,000 for the City of Long Beach, Citywide “8-80” Connections project.
- \$1,078,000 for the City of Hollister, Safe Connections and Complete Streets for West Side of Hollister project.

On March 15, 2017, the Commission adopted the 2017 Active Transportation Program (Cycle 3) Metropolitan Planning Organization Component (Resolution G-17-04), which included:

- \$643,000 for the City of Arvin, Haven Drive Pedestrian and Bicycle Infrastructure Improvements project.
- \$607,000 for the City of Jurupa Valley, Pacific Avenue Safe Routes to School and Road Diet project.
- \$3,031,000 for the City of Long Beach, Atherton Bridge and Campus Connections project.

On October 18, 2017, the Commission adopted the 2017 Active Transportation Program Augmentation (Cycle 3A) Statewide and Small Urban and Rural Components (Resolution G-17-29), which included:

- \$350,000 for the City of Arvin, Franklin Street Pedestrian and Bicycle Infrastructure Improvements project.
- \$963,000 for the City of Norwalk, Alondra Active Transportation Improvement project.
- \$4,572,000 for the City of Santa Ana, First Street Pedestrian Improvements project.
- \$2,960,000 for the Los Angeles County, Puente Creek Bikeway project.
- \$434,000 for the Mono County, Complete Streets: Bridgeport Main Street Revitalization project.

On May 15, 2019, the Commission adopted the 2019 Active Transportation Program (Cycle 4) Metropolitan Planning Organization Component (Resolution G-19-12), which included:

- \$860,000 for the City of Oxnard, Oxnard Boulevard Bikeway Gap Closure project.

On March 24, 2021, the Commission adopted the 2021 Active Transportation Program (Cycle 5) Statewide and Small Urban and Rural Components (Resolution G-21-30) which included:

- \$1,931,000 for the City of Perris, Bike and Pedestrian Network project.
- \$14,273,000 for the Shasta County, Cottonwood Active Transportation Trunk Line Express (CATTLE) Network project.

On June 23, 2021, the Commission adopted the 2021 Active Transportation Program (Cycle 5) Metropolitan Planning Organization Component (Resolution G-21-46), which included:

- \$210,000 for the City of Bakersfield, Chester Avenue Between 4th Street to Brundage Lane project.
- \$1,420,000 for the City of Eastvale, Southeast Eastvale SRTS Equitable Access project.

On June 28, 2023, the Commission adopted the 2023 Active Transportation Program (Cycle 6) Metropolitan Planning Organization Component (Resolution G-23-49), which included:

- \$2,755,000 for the City of Artesia, Pioneer Boulevard Improvements project.
- \$70,000 for the City of Fillmore, Active Transportation Program Needs Assessment project.
- \$1,301,000 for the City of Palm Desert, Palm Desert Bicycle/Low Speed Electric Vehicle Route project.

Resolution G-25-48

Be It Resolved, that the Commission does hereby amend the Active Transportation Program to deprogram 20 projects that have requested cancellation as reflected in Attachment B, returning a total of \$46,069,000 to the Active Transportation Program.

Attachments:

- Attachment A: Summary of Projects Requesting Cancellation
- Attachment B: Amended Active Transportation Program of Projects

California Transportation Commission
Active Transportation Program
Summary of Projects Requesting Cancellation
(1,000's)

Reference Number 4.27
June 26-27, 2025
Attachment A

Agency Name	County	Project Title	Cycle	Component	Total Project Cost	ATP Funding
City of Corning	Tehema	First Street Class 2 Bike Lanes	3	Statewide	\$ 73	\$ 73
City of Cudahy	Los Angeles	Atlantic Avenue Bicycle and Pedestrian Enhancement Project	3	Statewide	\$ 5,068	\$ 1,784
City of Long Beach	Los Angeles	Citywide "8-80" Connections	3	Statewide	\$ 7,987	\$ 6,754
City of Hollister	San Benito	Safe Connections and Complete Streets for West Side of Hollister	3	SUR	\$ 5,315	\$ 1,078
City of Arvin	Kern	Haven Drive Pedestrian and Bicycle Infrastructure Improvements	3	MPO	\$ 643	\$ 643
City of Jurupa Valley	Riverside	Pacific Avenue Safe Route to School and Road Diet Project	3	MPO	\$ 2,638	\$ 607
City of Long Beach	Los Angeles	Atherton Bridge and Campus Connections	3	MPO	\$ 3,091	\$ 3,031
City of Arvin	Kern	Franklin Street Pedestrian and Bicycle Infrastructure Improvements	3A	Statewide	\$ 350	\$ 350
City of Norwalk	Los Angeles	Alondra Active Transportation Improvement Project	3A	Statewide	\$ 973	\$ 963
City of Santa Ana	Orange	City of Santa Ana - First Street Pedestrian Improvements	3A	Statewide	\$ 4,572	\$ 4,572
Los Angeles County	Los Angeles	Puente Creek Bikeway	3A	Statewide	\$ 3,700	\$ 2,960
Mono County	Mono	Mono County Complete Streets: Bridgeport Main Street Revitalization Project	3A	Statewide	\$ 434	\$ 434
City of Oxnard	Ventura	Oxnard Boulevard Bikeway Gap Closure	4	MPO	\$ 860	\$ 860
City of Perris	Riverside	City of Perris Bike and Pedestrian Network Project	5	Statewide	\$ 1,999	\$ 1,931
Shasta County	Shasta	Cottonwood Active Transportation Trunk Line Express (CATTLE) Network	5	SUR	\$ 17,844	\$ 14,273
City of Bakersfield	Kern	Chester Avenue Between 4th Street to Brundage Lane	5	MPO	\$ 791	\$ 210
City of Eastvale	Riverside	Southeast Eastvale SRTS Equitable Access Project	5	MPO	\$ 1,420	\$ 1,420
City of Artesia	Los Angeles	Pioneer Boulevard Improvements Project, City of Artesia	6	MPO	\$ 3,549	\$ 2,755
City of Fillmore	Ventura	City of Fillmore Active Transportation Program Needs Assessment	6	MPO	\$ 70	\$ 70
City of Palm Desert	Riverside	Palm Desert Bicycle/Low Speed Electric Vehicle Route	6	MPO	\$ 3,488	\$ 1,301
					\$ 64,865	\$ 46,069

**2017 Active Transportation Program
Statewide Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-16-32
(\$1,000's)**

Reference Number 4.27
June 26-17, 2025
Attachment B

Co	Applicant	Project Title	DAC	Total Project Cost	Total Fund Request	FY 19-20	FY 20-21	PA&ED	PS&E	ROW	CON	CON-NI	Project Type	Plan	SRTS	SRTS-NI	OTH-NI	REC TR	SOF	Final Score
ALA	Oakland	14th Street: Safe Routes in the City	X	\$ 13,939	\$ 10,578	\$ 1,219	\$ 9,359	\$ -	\$ 1,219	\$ -	\$ 9,359	\$ -	Infrastructure (I)							95.00
ALA	Oakland	Fruitvale Alive Gap Closure Project	X	\$ 8,241	\$ 5,850	\$ 850	\$ 5,000	\$ -	\$ 800	\$ 50	\$ 5,000	\$ -	Infrastructure (I)							90.00
ALA	Alameda	Central Avenue Complete Street Project	X	\$ 12,471	\$ 7,326	\$ 180	\$ 7,146	\$ 180	\$ 300	\$ -	\$ 6,846	\$ -	Infrastructure (I)		X					88.00
BUT	Chico	Esplanade Corridor Safety and Accessibility Improvement Project	X	\$ 7,661	\$ 7,241	\$ 1,005	\$ 6,236	\$ -	\$ 1,005	\$ -	\$ 6,236	\$ -	Infrastructure (I)		X					93.00
BUT	Oroville	Oroville SR 162 Pedestrian/Bicyclist/Disabled Mobility and Safety Improvements	X	\$ 3,951	\$ 3,451	\$ 40	\$ 3,411	\$ -	\$ 40	\$ -	\$ 3,411	\$ -	Infrastructure (I)							92.00
FRE	Fresno	Midtown Fresno Trail: McKinley Avenue Gap Closure	X	\$ 3,519	\$ 1,556	\$ 1,556	\$ -	\$ -	\$ -	\$ -	\$ 1,556	\$ -	Infrastructure (I)							91.00
FRE	Fresno	Midtown Fresno School Area Multimodal Interconnectivity Project	X	\$ 1,401	\$ 1,241	\$ 144	\$ 1,097	\$ 43	\$ 101	\$ -	\$ 1,097	\$ -	Infrastructure (I)		X					90.00
KER	Delano	ATP-3 SRTS Delano Sidewalk Gap Closure	X	\$ 609	\$ 537	\$ 537	\$ -	\$ -	\$ 71	\$ -	\$ 422	\$ 44	Combination (I/NI)		X	X				98.00
KER	Kern County Road Department	Boron/Desert Lake Pedestrian Path	X	\$ 2,319	\$ 1,971	\$ 85	\$ 1,886	\$ 85	\$ 255	\$ 51	\$ 1,580	\$ -	Infrastructure (I)		X					95.00
KER	Kern County Road Department	Rexland Acres Community Sidewalk Project	X	\$ 6,376	\$ 5,640	\$ 1,104	\$ 4,536	\$ 26	\$ 769	\$ 309	\$ 4,536	\$ -	Infrastructure (I)		X					93.00
KER	Delano	Delano ATP3 SRTS Intersection Enhancement and Education Project	X	\$ 669	\$ 589	\$ 589	\$ -	\$ -	\$ 26	\$ -	\$ 519	\$ 44	Combination (I/NI)		X	X				91.00
KER	Kern County Road Department	Rosamond Boulevard Pedestrian Path Project	X	\$ 997	\$ 880	\$ 279	\$ 601	\$ 44	\$ 79	\$ 156	\$ 601	\$ -	Infrastructure (I)		X					90.00
LA	Paramount	West Santa Ana Branch Bikeway Phase 2	X	\$ 4,550	\$ 3,423	\$ 395	\$ 3,028	\$ 56	\$ 339	\$ 7	\$ 3,021	\$ -	Infrastructure (I)							99.00
LA	Cudahy	Atlantic Avenue Bicycle and Pedestrian Enhancement Project	X	\$ 5,068	\$1784 \$0	\$114 \$0	\$1670 \$0	\$20 \$0	\$94 \$0	\$---	\$1670 \$0	\$---	Infrastructure (I)							99.00
LA	San Fernando	City of San Fernando Pacoima Wash Bike/Ped Path, Phase 1	X	\$ 3,543	\$ 973	\$ 973	\$ -	\$ -	\$ -	\$ -	\$ 973	\$ -	Infrastructure (I)					X		98.00
LA	Los Angeles County DPW	Slauson Blue Line Station Intersection Improvements	X	\$ 1,465	\$ 1,465	\$ 60	\$ 1,405	\$ 60	\$ 200	\$ -	\$ 1,205	\$ -	Infrastructure (I)							96.00
LA	Los Angeles DPW Bureau of Engineering	Jefferson Boulevard Complete Street Project	X	\$ 6,336	\$ 5,986	\$ 925	\$ 5,061	\$ -	\$ 925	\$ -	\$ 5,061	\$ -	Infrastructure (I)		X					95.00
LA	South Gate	Garfield Avenue Complete Streets Corridor	X	\$ 826	\$ 660	\$ 116	\$ 544	\$ 32	\$ 84	\$ 8	\$ 536	\$ -	Infrastructure (I)							94.00
LA	Huntington Park	City of Huntington Park – Uncontrolled Crosswalk SRTS Ped Safety Project	X	\$ 1,054	\$ 1,032	\$ 87	\$ 945	\$ 9	\$ 78	\$ -	\$ 945	\$ -	Infrastructure (I)		X					92.00
LA	SCAG	Southern California Disadvantaged Communities Planning Initiative	X	\$ 1,350	\$ 1,150	\$ 1,150	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,150	Plan	X						91.00
LA	Lancaster	2020 Safe Route To School Pedestrian Improvements	X	\$ 7,443	\$ 5,272	\$ 5,272	\$ -	\$ -	\$ -	\$ -	\$ 5,272	\$ -	Infrastructure (I)		X					90.00
LA	Signal Hill	Spring Street Bicycle Lane Gap Closure Project, Signal Hill	X	\$ 2,599	\$ 2,079	\$ 376	\$ 1,703	\$ 44	\$ 332	\$ -	\$ 1,703	\$ -	Infrastructure (I)							90.00
LA	Los Angeles County DPW	Los Nietos Safe Routes to School - Phase II	X	\$ 1,552	\$ 1,452	\$ 47	\$ 1,405	\$ 47	\$ 211	\$ -	\$ 1,194	\$ -	Infrastructure (I)		X					89.00
LA	Long Beach	Citywide "8-80" Connections	X	\$ 7,987	\$6754 \$0	\$6754 \$0	\$---	\$---	\$---	\$---	\$6454 \$0	\$300 \$0	Combination (I/NI)		X	X				88.00
LA	Compton	Compton-Carson Regional Safe Bicycling and Wayfinding Project	X	\$ 1,868	\$ 1,617	\$ 1,617	\$ -	\$ -	\$ -	\$ -	\$ 1,617	\$ -	Infrastructure (I)							88.00
ORA	Santa Ana*	City of Santa Ana - First Street Pedestrian Improvements	X	\$ 4,572	\$ 4,553	\$ 540	\$ 4,013	\$ 10	\$ 530	\$ -	\$ 4,013	\$ -	Infrastructure (I)		X				X	88.00
PLA	Roseville	Dry Creek Greenway Multi-Use Trail Project, Roseville	X	\$ 12,152	\$ 4,500	\$ -	\$ 4,500	\$ -	\$ -	\$ -	\$ 4,318	\$ 182	Combination (I/NI)		X	X		X		89.00
RIV	Moreno Valley	Juan Bautista De Anza Trail Gap Closure	X	\$ 3,149	\$ 2,849	\$ 90	\$ 2,759	\$ 90	\$ 160	\$ 25	\$ 2,574	\$ -	Infrastructure (I)		X					94.00
RIV	La Quinta	La Quinta Village Complete Streets - A Road Diet Project	X	\$ 9,533	\$ 7,313	\$ 7,313	\$ -	\$ -	\$ -	\$ -	\$ 7,313	\$ -	Infrastructure (I)		X					90.00
RIV	Desert Hot Springs	Palm Drive Bicycle and Pedestrian Improvement Project, #2016-02	X	\$ 965	\$ 772	\$ 772	\$ -	\$ -	\$ -	\$ -	\$ 772	\$ -	Infrastructure (I)		X				X	89.50
SAC	Sacramento County	47th Ave Pedestrian and Bicycle Improvements	X	\$ 4,235	\$ 3,009	\$ 3,009	\$ -	\$ -	\$ -	\$ -	\$ 3,009	\$ -	Infrastructure (I)							95.00

**2017 Active Transportation Program
Statewide Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-16-32
(\$1,000's)**

Reference Number 4.27
June 26-17, 2025
Attachment B

Co	Applicant	Project Title	DAC	Total Project Cost	Total Fund Request	FY 19-20	FY 20-21	PA&ED	PS&E	ROW	CON	CON-NI	Project Type	Plan	SRTS	SRTS-NI	OTH-NI	REC TR	SOF	Final Score
SB	Santa Barbara	Eastside Green Lanes & Bike Boulevard Gap Closure	X	\$ 2,763	\$ 2,736	\$ 280	\$ 2,456	\$ 100	\$ 180	\$ 18	\$ 2,438	\$ -	Infrastructure (I)		X				X	90.00
SBD	Yucca Valley	Little League Drive Pedestrian Improvements	X	\$ 779	\$ 622	\$ 68	\$ 554	\$ 4	\$ 64	\$ -	\$ 554	\$ -	Infrastructure (I)							92.00
SCL	Sunnyvale	Sunnyvale SNAIL Neighborhood Active Transportation Connectivity Improvements	X	\$ 6,059	\$ 4,847	\$ 865	\$ 3,982	\$ 72	\$ 780	\$ -	\$ 3,982	\$ 13	Combination (I/NI)		X	X				89.00
SD	SANDAG	Imperial Avenue Bikeway	X	\$ 11,037	\$ 4,450	\$ 4,450	\$ -	\$ -	\$ -	\$ -	\$ 4,450	\$ -	Infrastructure (I)							90.00
SOL	Fairfield	East Tabor/Tolenas Safe Routes to School Gap Closure Project	X	\$ 1,860	\$ 1,700	\$ 88	\$ 1,612	\$ 88	\$ 132	\$ 465	\$ 1,015	\$ -	Infrastructure (I)		X					89.00
SON	Sonoma-Marín Area Rail Transit District	SMART Pathway - Petaluma (Payran to Southpoint)	X	\$ 3,272	\$ 1,461	\$ 1,461	\$ -	\$ -	\$ -	\$ -	\$ 1,461	\$ -	Infrastructure (I)		X					89.00
STA	Modesto	Paradise Road Area Pedestrian and Bicycle Safety Improvements	X	\$ 3,983	\$ 3,943	\$ 388	\$ 3,555	\$ -	\$ 388	\$ -	\$ 3,555	\$ -	Infrastructure (I)							90.00
STA	Ceres	Ceres-Citywide Active Transportation Plan	X	\$ 131	\$ 104	\$ 104	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 104	Plan	X					X	90.00
TEH	Corning	First Street Class 2 Bike Lanes	X	\$ 73	\$ 73	\$ 73	\$ -	\$ 1	\$ 5	\$ -	\$ 67	\$ -	Infrastructure (I)		X				X	93.00
Var	Caltrans	Active Transportation Resource Center		\$ -	\$ 5,058	\$ 3,833	\$ 1,225				\$ 5,058									
YUB	Marysville	Marysville Bicycle and Pedestrian Improvement Project	X	\$ 583	\$ 515	\$ 515	\$ -	\$ -	\$ 63	\$ -	\$ 452	\$ -	Infrastructure (I)						X	92.00
YUB	Yuba County	Eleventh Avenue Pedestrian and Bicyclist Route Improvements	X	\$ 1,701	\$ 1,505	\$ 227	\$ 1,278	\$ 57	\$ 170	\$ -	\$ 1,253	\$ 25	Combination (I/NI)		X	X				92.00
YUB	Yuba County	McGowan Parkway Bicycle Lane and Pedestrian Route Improvements	X	\$ 1,559	\$ 1,246	\$ 216	\$ 1,030	\$ 54	\$ 162	\$ -	\$ 1,000	\$ 30	Combination (I/NI)		X	X			X	88.00
TOTAL				\$ 176,200	\$ 123,152															

Abbreviations, Acronyms, and Initialisms	
CON: Construction Phase	PS&E: Plans, Specifications & Estimate Phase
DAC: Disadvantaged Community	REC TR: Recreational Trails Eligible
NI: Non-Infrastructure	ROW: Right-of-Way Phase
PA&ED: Environmental Phase	SOF: State Only Funds
Plan: Active Transportation Plan	SRTS: Safe Routes to School

**2017 Active Transportation Program
Small Urban and Rural Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-16-32
(\$1,000's)**

Reference Number 4.27
June 26-27, 2025
Attachment B

Co	Applicant	Project Title	DAC	Total Project Cost	Total Fund Request	FY 19-20	FY 20-21	PA&ED	PS&E	ROW	CON	CON-NI	Project Type	Plan	SRTS	SRTS-NI	OTH-NI	REC TR	SOF	Evaluator Score
SCR	Santa Cruz	Rail Trail Segment 8 and 9 Design and Environmental Review	X	\$ 32,934	\$ 3,169	\$ 519	\$ 2,650	\$ 350	\$ 2,650	\$ -	\$ -	\$ 169	Combination (I/NI)							88
SB	Santa Barbara	Westside Bike Boulevard Gap Closure	X	\$ 4,482	\$ 4,437	\$ 355	\$ 4,082	\$ 75	\$ 280	\$ 40	\$ 4,042	\$ -	Infrastructure (I)		X				X	87
SB	Santa Barbara County Association of Governments	Rincon Multi-use Trail	X	\$ 7,828	\$ 6,833	\$ 6,833	\$ -	\$ -	\$ -	\$ -	\$ 6,833	\$ -	Infrastructure (I)							87
SB	Buellton	SR2S Improvements at Intersection of Highway 246 and Sycamore Drive	X	\$ 784	\$ 684	\$ 684	\$ -	\$ -	\$ -	\$ -	\$ 684	\$ -	Infrastructure (I)		X					86
HUM	Humboldt County Public Works	Fortuna & McKinleyville Active Transportation Education Program	X	\$ 595	\$ 595	\$ 595	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 595	Non-Infrastructure (NI)			X			X	86
MON	Monterey County	Via Salinas Valley: An Active Transportation Education Program	X	\$ 1,158	\$ 965	\$ 965	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 965	Non-Infrastructure (NI)			X				85
LAK	Lakeport	Hartley Street Safe Route to School Project - Lakeport, California	X	\$ 1,874	\$ 1,852	\$ 185	\$ 1,667	\$ 30	\$ 155	\$ -	\$ 1,667	\$ -	Infrastructure (I)		X				X	85
SBO	Hollister	Safe Connections and Complete Streets for West Side of Hollister	X	\$ 5,345	\$1078 \$0	\$	\$1078 \$0	\$	\$	\$	\$1078 \$0	\$	Infrastructure (I)		X					84
MEN	Fort Bragg	Fort Bragg Coastal Trail Phase II	X	\$ 1,514	\$ 766	\$ 766	\$ -	\$ -	\$ -	\$ -	\$ 766	\$ -	Infrastructure (I)					X	X	83
SHA	Redding	Bechelli Lane & Loma Vista Active Transportation Corridor Improvements	X	\$ 8,421	\$ 5,954	\$ -	\$ 5,954	\$ -	\$ -	\$ -	\$ 5,954	\$ -	Infrastructure (I)		X					83
TOTAL				\$ 64,905	\$ 25,255															

**2017 Active Transportation Program
Metropolitan Planning Organization Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-17-04
(\$1,000's)**

Reference Number 4.27
June 26-27, 2025
Attachment B

Amendments are shown in bold and strikethrough and highlighted in yellow

MPO	Co	Agency	Project Title	Total Project Cost	Total Fund Request	SOF	DAC	FY 18-19	FY 19-20	FY 20-21	ROW	CON	PA&ED	PS&E	Plan	SRTS	SRTS-NI	OTH-NI
FCOG	FRE	Coalinga	Coalinga SRTS Sidewalk Gap Closure & Ped Improvements	\$ 394	\$ 340		X	\$ -	\$ 38	\$ 302	\$ -	\$ 302	\$ 3	\$ 35		X		
FCOG	FRE	Parlier	Parlier SRTS	\$ 182	\$ 182		X	\$ -	\$ 182	\$ -	\$ -	\$ 164	\$ -	\$ 18		X		
FCOG	FRE	Fresno	Midtown Fresno Trail: Shields Avenue "Last Mile" Gap Closure	\$ 1,498	\$ 611		X	\$ -	\$ 611	\$ -	\$ -	\$ 611	\$ -	\$ -				
FCOG	FRE	Sanger	Sanger Active Transportation Access	\$ 380	\$ 337		X	\$ -	\$ 29	\$ 308	\$ -	\$ 308	\$ -	\$ 29				
FCOG	FRE	Fresno	Intersection Improvements to Increase Active Transportation along Bus Rapid Transit	\$ 1,447	\$ 1,152		X	\$ -	\$ 148	\$ 1,004	\$ -	\$ 1,004	\$ 44	\$ 104				
FCOG	FRE	Fresno PARCS	Fresno Pedestrian and Bike Safety Education Program	\$ 265	\$ 250	X	X	\$ -	\$ 250	\$ -	\$ -	\$ 250	\$ -	\$ -				X
KCOG	KER	Arvin	Haven Drive Pedestrian and Bicycle Project	\$ 643	\$643 \$0	X	X	\$ -	\$643 \$0	\$ -	\$ -	\$643 \$0	\$6 \$0	\$87 \$0		X		
KCOG	KER	Arvin	Varsity Road Pedestrian and Bicycle Project	\$ 833	\$ 833	X	X	\$ -	\$ 119	\$ 714	\$ -	\$ 714	\$ 7	\$ 112		X		
KCOG	KER	Bakersfield	Downtown Bicycle Connectivity Project	\$ 1,367	\$ 1,110		X	\$ -	\$ 1,110	\$ -	\$ -	\$ 1,110	\$ -	\$ -				
MTC	ALA	Alameda Co. PWA	Active Oakland: Comprehensive SRTS Program (NI)	\$ 977	\$ 977	X	X	\$ -	\$ 977	\$ -	\$ -	\$ 977	\$ -	\$ -		X	X	
MTC	ALA	Alameda Co. PWA	Fairview Elementary School SRTS	\$ 3,366	\$ 542		X	\$ -	\$ 542	\$ -	\$ -	\$ -	\$ -	\$ 542		X		
MTC	ALA	Alameda Co. PWA	Somerset Avenue SRTS Corridor	\$ 3,652	\$ 330			\$ -	\$ 330	\$ -	\$ -	\$ -	\$ -	\$ 330		X		
MTC	ALA	Alameda Co. PWA	Lewelling Blvd SRTS (Final Design only)	\$ 3,065	\$ 400		X	\$ -	\$ 400	\$ -	\$ -	\$ -	\$ -	\$ 400		X		
MTC	ALA	Emeryville	Bike/Ped Greenway Safety & Connectivity Improvement Project	\$ 330	\$ 265	X		\$ -	\$ 265	\$ -	\$ -	\$ 265	\$ -	\$ -				
MTC	ALA	Berkeley	SRTS - John Muir Elementary	\$ 336	\$ 270	X		\$ -	\$ 270	\$ -	\$ -	\$ 270	\$ -	\$ -		X		
MTC	CC	Contra Costa Co. PW	Fred Jackson Way First Mile/Last Mile Connection	\$ 4,298	\$ 3,298		X	\$ -	\$ 361	\$ 2,937	\$ 200	\$ 2,937	\$ -	\$ 161		X		
MTC	CC	Contra Costa Co. PW	Pacheco Blvd Sidewalk Gap Closure Phase 3	\$ 1,239	\$ 619	X	X	\$ -	\$ 619	\$ -	\$ -	\$ 619	\$ -	\$ -		X		
MTC	MRN	San Rafael	Francisco Blvd East Avenue Bridge Bike Ped Connectivity	\$ 7,358	\$ 4,025		X	\$ -	\$ 4,025	\$ -	\$ -	\$ 4,025	\$ -	\$ -		X		
MTC	NAP	City of Napa	SR 29 Bike/Ped Undercrossing	\$ 742	\$ 531	X	X	\$ -	\$ 531	\$ -	\$ -	\$ 531	\$ -	\$ -		X		
MTC	SF	SFMTA	Powell Street Safety Project	\$ 9,889	\$ 4,400		X	\$ -	\$ -	\$ 4,400	\$ -	\$ 4,400	\$ -	\$ -				
MTC	SM	Woodside	Woodside Elementary School Student Pathway Phase 3	\$ 745	\$ 528	X		\$ -	\$ 528	\$ -	\$ -	\$ 528	\$ -	\$ -		X		
MTC	SOL	Vallejo	Bay Trail/Vine Trail Gap Closure	\$ 5,128	\$ 4,216		X	\$ -	\$ -	\$ 4,216	\$ -	\$ 4,216	\$ -	\$ -		X		
MTC	SOL	Suisun City	McCoy Creek Trail	\$ 3,000	\$ 1,770		X	\$ -	\$ -	\$ 1,770	\$ -	\$ 1,770	\$ -	\$ -		X		
SACOG	SAC	Sacramento	Broadway Complete Streets Project	\$ 4,414	\$ 2,763		X	\$ -	\$ 353	\$ 2,410	\$ -	\$ 2,410	\$ -	\$ 353				
SACOG	SAC	Sacramento	Northwood School and Transit Access Improvements	\$ 2,541	\$ 1,818		X	\$ -	\$ 350	\$ 1,468	\$ -	\$ 1,468	\$ -	\$ 350		X	X	
SACOG	SAC	Citrus Heights	Citrus Heights Electric Greenway (Class 1 Multi-Use Trail)	\$ 7,015	\$ 350			\$ -	\$ 350	\$ -	\$ -	\$ -	\$ 350	\$ -		X		
SACOG	SAC	Citrus Heights	Mariposa SRTS Project (Phase IV)	\$ 2,719	\$ 2,100	X		\$ -	\$ 278	\$ 1,822	\$ -	\$ 1,822	\$ -	\$ 278		X	X	
SANDAG	SD	National City	El Toyon - Las Palmas Regional Bicycle Boulevard	\$ 1,919	\$ 1,544		X	\$ -	\$ 1,544	\$ -	\$ -	\$ 1,544	\$ -	\$ -				
SANDAG	SD	National City	Euclid Avenue Bicycle and Pedestrian Enhancements	\$ 3,960	\$ 3,335	X	X	\$ -	\$ 3,335	\$ -	\$ -	\$ 3,335	\$ -	\$ -				
SANDAG	SD	Escondido	Juniper Elementary Bike/Pedestrian Improvements & SRTS Outreach	\$ 1,386	\$ 1,336		X	\$ -	\$ 229	\$ 1,107	\$ -	\$ 1,157	\$ 86	\$ 93		X	X	
SANDAG	SD	Escondido	Escondido Creek Trail Bike Path Improvements Project	\$ 1,632	\$ 1,632		X	\$ -	\$ 290	\$ 1,342	\$ -	\$ 1,342	\$ 90	\$ 200				
SANDAG	SD	Carlsbad	Avenida Encinas Coastal Rail Trail and Pedestrian Improvements	\$ 9,095	\$ 1,776			\$ -	\$ 1,776	\$ -	\$ -	\$ 1,776	\$ -	\$ -				

**2017 Active Transportation Program
Metropolitan Planning Organization Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-17-04
(\$1,000's)**

Reference Number 4.27
June 26-27, 2025
Attachment B

MPO	Co	Agency	Project Title	Total Project Cost	Total Fund Request	SOF	DAC	FY 18-19	FY 19-20	FY 20-21	ROW	CON	PA&ED	PS&E	Plan	SRTS	SRTS-NI	OTH-NI
SCAG	IMP	Imperial	Project Ride, Walk, Learn	\$ 224	\$ 224	X	X	\$ -	\$ 224	\$ -	\$ -	\$ 224	\$ -	\$ -			X	
SCAG	IMP	Imperial County	Aten Road Class I Bicycle Path Improvements	\$ 442	\$ 285	X	X	\$ -	\$ 56	\$ 229	\$ -	\$ 229	\$ 16	\$ 40				
SCAG	LA	LADPW	DTLA Arts District Pedestrians & Cyclist Safety Project	\$ 15,000	\$ 14,850		X	\$ -	\$ 2,550	\$ 12,300	\$ -	\$ 12,300	\$ -	\$ 2,550				
SCAG	LA	Long Beach	Atherton Bridge & Campus Connections	\$ 3,091	\$0		X	\$ -	\$0	\$2810	\$ -	\$0	\$ -	\$221		X		
SCAG	LA	El Monte	City of El Monte - Mountain View School District SRTS Program	\$ 583	\$ 583	X	X	\$ -	\$ 583	\$ -	\$ -	\$ 583	\$ -	\$ -			X	
SCAG	LA	Baldwin Park	Maine Avenue/Pacific Avenue Corridor Complete Streets Improvements, Phase II	\$ 1,419	\$ 1,068	X	X	\$ -	\$ 1,068	\$ -	\$ -	\$ 1,068	\$ -	\$ -		X		
SCAG	LA	Vernon	Pacific Blvd./Vernon Ave. Complete Streets Ped and Bike	\$ 1,931	\$ 1,892	X	X	\$ -	\$ 129	\$ 1,763	\$ -	\$ 1,763	\$ 16	\$ 113		X		
SCAG	LA	LA County MTA	Reconnecting Union Station to the Historic Cultural Communities of DTLA	\$ 5,326	\$ 3,157		X	\$ -	\$ 3,157	\$ -	\$ -	\$ 3,157	\$ -	\$ -				
SCAG	LA	Pasadena	Pasadena - PUSD Safe Routes to School Education and Encouragement Program	\$ 514	\$ 462	X	X	\$ -	\$ 462	\$ -	\$ -	\$ 462	\$ -	\$ -			X	
SCAG	LA	Pasadena	Union Street Cycle Track	\$ 6,314	\$ 3,742		X	\$ -	\$ 3,742	\$ -	\$ -	\$ 3,742	\$ -	\$ -				
SCAG	LA	Commerce	City of Commerce Active Transportation & SRTS Plan	\$ 245	\$ 245	X	X	\$ -	\$ 245	\$ -	\$ -	\$ 245	\$ -	\$ -	X			
SCAG	LA	SGVCOG	San Gabriel Valley Greenway Network Implementation Plan	\$ 240	\$ 200	X	X	\$ -	\$ 200	\$ -	\$ -	\$ 200	\$ -	\$ -	X			
SCAG	LA	Office of Exposition Park Management	Exposition Park Active Transportation Plan	\$ 241	\$ 200	X	X	\$ -	\$ 200	\$ -	\$ -	\$ 200	\$ -	\$ -	X			
SCAG	ORA	Santa Ana	City of Santa Ana - West Willits Street Protected Bicycle Lanes	\$ 2,970	\$ 2,970		X	\$ -	\$ 495	\$ 2,475	\$ -	\$ 2,475	\$ 30	\$ 465		X	X	
SCAG	ORA	Santa Ana	City of Santa Ana - SRTS Davis Elementary ADA Compliance	\$ 5,754	\$ 5,754		X	\$ -	\$ 920	\$ 4,834	\$ -	\$ 4,834	\$ 20	\$ 900		X		
SCAG	ORA	Fountain Valley	Fountain Valley Pedestrian Pathway Improvement within School Zones	\$ 296	\$ 226	X	X	\$ -	\$ 226	\$ -	\$ -	\$ 226	\$ -	\$ -				
SCAG	RIV	Riverside Co. DPH	Riverside County Safe Routes to School Program	\$ 1,099	\$ 849	X	X	\$ -	\$ 849	\$ -	\$ -	\$ 849	\$ -	\$ -			X	
SCAG	RIV	Jurupa Valley	Pacific Avenue Safe Routes to School and Road Diet Project	\$ 2,638	\$607	X	X	\$ -	\$164	\$463	\$ -	\$463	\$ -	\$164		X		
SCAG	RIV	Coachella Valley AOG	CV Link - Multi-Modal Transportation Corridor	\$ 99,997	\$ 5,584		X	\$ -	\$ 5,584	\$ -	\$ -	\$ 5,584	\$ -	\$ -				
SCAG	SBD	Highland	City Creek/Alabama Street Bikeways	\$ 3,965	\$ 3,171		X	\$ -	\$ 817	\$ 2,354	\$ 817	\$ 2,354	\$ -	\$ -				
SCAG	SBD	Yucca Valley	Onaga Trail Pedestrian Improvements	\$ 971	\$ 776	X	X	\$ -	\$ 52	\$ 724	\$ -	\$ 724	\$ 4	\$ 48				
SCAG	SBD	Redlands	East Valley Corridor Bike Route Interconnect Project	\$ 2,640	\$ 2,063	X	X	\$ -	\$ 1,863	\$ 200	\$ -	\$ 2,063	\$ -	\$ -				
SCAG	VEN	Ventura	Montalvo SRTS Cypress Point Gap Closure and Complete Streets Project	\$ 1,560	\$ 1,380	X	X	\$ -	\$ 243	\$ 1,137	\$ -	\$ 1,137	\$ 88	\$ 155		X		
SCAG	VEN	Oxnard	City of Oxnard - Etting Road Sidewalk and Bike Lane	\$ 1,239	\$ 991	X	X	\$ -	\$ 120	\$ 871	\$ 40	\$ 871	\$ -	\$ 80		X		
SCAG	VAR	SCAG	SCAG 2017 Active Transportation Safety & Encouragement Campaign	\$ 2,690	\$ 1,784		X	\$ 1,784	\$ -	\$ -	\$ -	\$ 1,784	\$ -	\$ -			X	
SJCOG	SJ	Stockton	Closing Gaps to Schools	\$ 2,295	\$ 287		X		\$ 287	\$ -	\$ -	\$ -	\$ 57	\$ 230		X		
SJCOG	SJ	Stockton	Miner Avenue Complete Streets	\$ 5,347	\$ 670		X		\$ 670	\$ -	\$ -	\$ -	\$ -	\$ 670				
SJCOG	SJ	Stockton	March Lane/EBMUD Bicycle and Pedestrian Path Connectivity Improvements	\$ 3,869	\$ 317		X		\$ 317	\$ -	\$ -	\$ -	\$ 82	\$ 235				
SJCOG	SJ	Lodi	Citywide Bicycle Facilities Detection Improvement Project	\$ 565	\$ 447	X	X		\$ 447	\$ -	\$ -	\$ 447	\$ -	\$ -		X		
SJCOG	SJ	Tracy	Lowell Avenue Sidewalk Construction Project	\$ 328	\$ 232		X		\$ 232	\$ -	\$ -	\$ 232	\$ -	\$ -		X		
SJCOG	SJ	San Joaquin RTD	RTD's Bike and Bus Transportation Project	\$ 272	\$ 152	X	X		\$ 152	\$ -	\$ -	\$ 152	\$ -	\$ -				
StanCOG	STA	Turlock	SRTS ADA Pedestrian Improvements, Various Locations	\$ 1,760	\$ 1,108	X	X	\$ -	\$ 249	\$ 859	\$ -	\$ 859	\$ 125	\$ 124		X	X	
StanCOG	STA	StanCOG	StanCOG Regional Bicyclist and Pedestrian Safety and Education Campaign	\$ 372	\$ 372	X	X	\$ -	\$ 372	\$ -	\$ -	\$ 372	\$ -	\$ -				X

**2017 Active Transportation Program
Metropolitan Planning Organization Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-17-04
(\$1,000's)**

Reference Number 4.27
June 26-27, 2025
Attachment B

MPO	Co	Agency	Project Title	Total Project Cost	Total Fund Request	SOF	DAC	FY 18-19	FY 19-20	FY 20-21	ROW	CON	PA&ED	PS&E	Plan	SRTS	SRTS-NI	OTH-NI
StanCOG	STA	Waterford	City of Waterford SRTS Crosswalks Safety Project	\$ 137	\$ 108	X	X	\$ -	\$ 16	\$ 92	\$ -	\$ 92	\$ 7	\$ 9	X	X	X	
TCAG	TUL	Farmersville	Farmersville SRTS East Walnut Avenue	\$ 827	\$ 520	X	X	\$ -	\$ 94	\$ 426	\$ 6	\$ 426	\$ 25	\$ 63		X		
TCAG	TUL	Tulare County	Woodville Sidewalk Improvements along Road 168	\$ 882	\$ 836		X	\$ -	\$ 80	\$ 756	\$ -	\$ 756	\$ 15	\$ 65		X		
TOTAL				\$ 263,858	\$ 96,864													

**2017 Active Transportation Program Augmentation
Statewide Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-17-29
(\$1,000's)**

Reference Number 4.27
June 26-27, 2025
Attachment B

Application ID	Co	Project Title	DAC	SOF	Total Project Cost	Recommended ATP Funding	FY 17-18	FY 18-19	FY 19-20	FY 20-21	CON	CON NI	PA&ED	PS&E	ROW	Project Type	SRTS	REC TR	Final Score
2017 ATP Funded Projects Requesting Advances																			
7-Paramount-1	LA	West Santa Ana Branch Bikeway Phase 2	X	X	\$ 4,550	\$ 3,423	\$ 345	\$ 3,078	\$ -	\$ -	\$ 3,078	\$ -	\$ -	\$ 345	\$ -	Infrastructure			99.00
4-Oakland-4	ALA	14th Street: Safe Routes in the City	X		\$ 13,939	\$ 10,578	\$ -	\$ 1,235	\$ 9,343	\$ -	\$ 9,343	\$ -	\$ -	\$ 1,235	\$ -	Infrastructure			95.00
9-Kern County Road Department-6	KER	Boron/Desert Lake Pedestrian Path	X	X	\$ 2,319	\$ 1,971	\$ 461	\$ 1,510	\$ -	\$ -	\$ 1,510	\$ -	\$ -	\$ 268	\$ 193	Infrastructure	X		95.00
7-Los Angeles DPW (Bureau of Engineering)-4	LA	Jefferson Boulevard Complete Street	X	X	\$ 6,336	\$ 5,986	\$ 925	\$ 5,061	\$ -	\$ -	\$ 5,061	\$ -	\$ 600	\$ 325	\$ -	Infrastructure	X		95.00
8-Moreno Valley-1	RIV	Juan Bautista De Anza Trail Gap Closure	X	X	\$ 3,149	\$ 2,849	\$ 275	\$ 2,574	\$ -	\$ -	\$ 2,574	\$ -	\$ 90	\$ 160	\$ 25	Infrastructure	X		94.00
6-Kern County Road Department-1	KER	Rexland Acres Community Sidewalk	X	X	\$ 6,376	\$ 5,640	\$ 375	\$ 5,265	\$ -	\$ -	\$ 4,519	\$ -	\$ 25	\$ 746	\$ 350	Infrastructure	X		93.00
3-Yuba County-2	YUB	Eleventh Avenue Pedestrian and Bicyclist Route Improvements	X	X	\$ 1,701	\$ 1,505	\$ 227	\$ 1,278	\$ -	\$ -	\$ 1,253	\$ 25	\$ 57	\$ 170	\$ -	Combination	X		92.00
7-SCAG-4	LA	Southern California Disadvantaged Communities Planning Initiative	X	X	\$ 1,350	\$ 1,150	\$ 1,150	\$ -	\$ -	\$ -	\$ -	\$ 1,150	\$ -	\$ -	\$ -	Plan			91.00
4-Oakland-1	ALA	Fruitvale Alive Gap Closure	X	X	\$ 8,241	\$ 5,850	\$ 850	\$ 5,000	\$ -	\$ -	\$ 5,000	\$ -	\$ -	\$ 800	\$ 50	Infrastructure			90.00
7-Lancaster-2	LA	2020 Safe Route To School Pedestrian Improvements	X	X	\$ 7,443	\$ 5,272	\$ -	\$ 5,272	\$ -	\$ -	\$ 5,272	\$ -	\$ -	\$ -	\$ -	Infrastructure	X		90.00
8-La Quinta-1	RIV	La Quinta Village Complete Streets - A Road Diet	X	X	\$ 9,533	\$ 7,313	\$ -	\$ 7,313	\$ -	\$ -	\$ 7,313	\$ -	\$ -	\$ -	\$ -	Infrastructure	X		90.00
5-Santa Barbara-2	SB	Eastside Green Lanes & Bike Boulevard Gap Closure	X	X	\$ 2,763	\$ 2,736	\$ 73	\$ 207	\$ 18	\$ 2,438	\$ 2,438	\$ -	\$ 73	\$ 207	\$ 18	Infrastructure	X		90.00
8-Desert Hot Springs-1	RIV	Palm Drive Bicycle and Pedestrian Improvement	X	X	\$ 965	\$ 772	\$ -	\$ 772	\$ -	\$ -	\$ 772	\$ -	\$ -	\$ -	\$ -	Infrastructure	X		89.50
4-Sonoma-Marin Area Rail Transit District (SMART)-1	SON	SMART Pathway - Petaluma (Payran to Southpoint)	X	X	\$ 3,272	\$ 1,461	\$ 1,461	\$ -	\$ -	\$ -	\$ 1,461	\$ -	\$ -	\$ -	\$ -	Infrastructure	X		89.00
7-Compton-1	LA	Compton-Carson Regional Safe Bicycling and Wayfinding	X	X	\$ 1,868	\$ 1,617	\$ -	\$ 1,617	\$ -	\$ -	\$ 1,617	\$ -	\$ -	\$ -	\$ -	Infrastructure			88.00
3-Yuba County-1	YUB	McGowan Parkway Bicycle Lane and Pedestrian Route Improvements	X	X	\$ 1,559	\$ 1,246	\$ 216	\$ -	\$ 1,030	\$ -	\$ 1,000	\$ 30	\$ 54	\$ 162	\$ -	Combination	X		88.00
12-Santa Ana-2	ORA	City of Santa Ana – First Street Pedestrian Improvements	X		\$ 4,572	\$ 4,572	\$ -	\$ 540	\$ 4032	\$ -	\$ 4032	\$ -	\$ 40	\$ 530	\$ -	Infrastructure	X		88.00
TOTALS FUNDED PROJECTS REQUESTING ADVANCES					\$ 79,936	\$ 59,369													

New Projects Recommended for Funding																			
11-Imperial Beach-1	SD	Imperial Beach Boulevard Improvement and Safe Routes to Schools	X	X	\$ 5,175	\$ 2,570	\$ 312	\$ 2,258	\$ -	\$ -	\$ 2,193	\$ 65	\$ -	\$ 303	\$ 9	Combination	X		88.00
4-San Francisco Municipal Transportation Agency-5	SF	Geneva Ave Pedestrian and Bicycle Safety Improvement	X	X	\$ 9,987	\$ 2,350	\$ 2,350	\$ -	\$ -	\$ -	\$ 2,350	\$ -	\$ -	\$ -	\$ -	Infrastructure			88.00
3-Davis-1	YOL	Providing Safe Passage: Connecting Montgomery Elementary and Olive Drive	X		\$ 4,425	\$ 3,540	\$ 412	\$ -	\$ 3,128	\$ -	\$ 3,128	\$ -	\$ -	\$ 380	\$ 32	Infrastructure	X		86.50
4-Sunnyvale-1	SCL	Sunnyvale Safe Routes to School Improvements	X		\$ 2,362	\$ 1,889	\$ -	\$ 380	\$ 1,509	\$ -	\$ 1,509	\$ 6	\$ 56	\$ 318	\$ -	Combination	X		86.00
10-Stockton-4	SJ	Miner Avenue Complete Streets	X	X	\$ 5,347	\$ 3,816	\$ -	\$ 3,816	\$ -	\$ -	\$ 3,816	\$ -	\$ -	\$ -	\$ -	Infrastructure			85.00
3-Roseville-2	PLA	Washington Boulevard Bikeway and Pedestrian Pathways	X		\$ 3,982	\$ 2,212	\$ -	\$ -	\$ 2,212	\$ -	\$ 2,030	\$ 182	\$ -	\$ -	\$ -	Combination	X		85.00
8-Riverside County Transportation Department-3	RIV	Cabazon Safe Routes to School Sidewalk Safety Improvements	X	X	\$ 1,120	\$ 1,070	\$ 282	\$ 788	\$ -	\$ -	\$ 788	\$ -	\$ -	\$ 122	\$ 160	Infrastructure	X		85.00
3-Citrus Heights-1	SAC	Citrus Heights Electric Greenway (Class 1 Multi-Use Trail)	X		\$ 7,015	\$ 5,866	\$ -	\$ 311	\$ 5,555	\$ -	\$ 5,525	\$ 30	\$ -	\$ 246	\$ 65	Combination	X	X	84.00
3-El Dorado County-1	ED	El Dorado Trail - Missouri Flat Road to El Dorado	X	X	\$ 4,394	\$ 3,419	\$ 3,419	\$ -	\$ -	\$ -	\$ 3,374	\$ 45	\$ -	\$ -	\$ -	Combination		X	84.00
8-Riverside County Transportation Department-4	RIV	Clark Street SR2S Sidewalk and Intersection Safety Improvements	X	X	\$ 2,945	\$ 684	\$ -	\$ 684	\$ -	\$ -	\$ 684	\$ -	\$ -	\$ -	\$ -	Infrastructure	X		84.00
10-Patterson-1	STA	Ninth Street Improvements	X	X	\$ 907	\$ 907	\$ 75	\$ 832	\$ -	\$ -	\$ 764	\$ -	\$ 75	\$ 68	\$ -	Infrastructure	X		84.00

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(\$1,000's)**

Reference Number 4.27
June 26-27, 2025
Attachment B

Application ID	Co	Project Title	DAC	SOF	Total Project Cost	Recommended ATP Funding	FY 17-18	FY 18-19	FY 19-20	FY 20-21	CON	CON NI	PA&ED	PS&E	ROW	Project Type	SRTS	REC TR	Final Score
6-Tulare County-8	TUL	County of Tulare: Earlimart Sidewalk Improvements	X	X	\$ 1,973	\$ 1,868	\$ 180	\$ 1,688	\$ -	\$ -	\$ 1,688	\$ -	\$ -	\$ 180	\$ -	Combination	X		84.00
4-East Bay Regional Park District-2	ALA	Doolittle Drive Bay Trail, Martin Luther King Jr. Shoreline, Oakland	X	X	\$ 7,950	\$ 4,000	\$ 500	\$ 3,500	\$ -	\$ -	\$ 3,500	\$ -	\$ -	\$ 250	\$ 250	Infrastructure			83.00
3-Placerville-1	ED	Upper Broadway Pedestrian Connection	X	X	\$ 2,206	\$ 1,886	\$ 500	\$ 1,386	\$ -	\$ -	\$ 1,386	\$ -	\$ -	\$ 173	\$ 327	Infrastructure			83.00
8-Indio-1	RIV	Herbert Hoover Elementary Pedestrian Improvements	X		\$ 2,983	\$ 2,983	\$ 100	\$ 240	\$ -	\$ 2,643	\$ 2,638	\$ 5	\$ 100	\$ 240	\$ -	Combination	X		83.00
8-Hemet-1	RIV	Hemet Valley Bikeway Connect	X		\$ 2,288	\$ 2,288	\$ -	\$ 200	\$ 2,088	\$ -	\$ 1,990	\$ 98	\$ -	\$ 200	\$ -	Combination	X		83.00
8-Perris-1	RIV	Perris Valley Storm Drain Channel Trail - Phase 2	X		\$ 3,204	\$ 3,004	\$ 237	\$ 524	\$ -	\$ 2,243	\$ 2,243	\$ -	\$ 237	\$ 287	\$ 237	Infrastructure			83.00
5-Guadalupe-1	SB	Guadalupe Street (Hwy 1) Pedestrian Improvements	X	X	\$ 458	\$ 401	\$ -	\$ 401	\$ -	\$ -	\$ 401	\$ -	\$ -	\$ -	\$ -	Infrastructure			83.00
4-Pittsburg-2	CC	Pittsburg Active Transportation and Safe Routes Plan (WalkBikePittsburg2035)	X	X	\$ 312	\$ 312	\$ 312	\$ -	\$ -	\$ -	\$ 312	\$ -	\$ -	\$ -	\$ -	Plan			82.00
6-Fresno-4	FRE	Downtown Fresno Courthouse Park Interconnectivity Project	X	X	\$ 915	\$ 809	\$ 28	\$ 781	\$ -	\$ -	\$ 715	\$ -	\$ 28	\$ 66	\$ -	Infrastructure			82.00
7-Glendale-1	LA	Glendale Transportation Center 1st/Last Mile Regional Improvements Phase II	X	X	\$ 1,301	\$ 1,101	\$ 172	\$ 929	\$ -	\$ -	\$ 929	\$ -	\$ 43	\$ 129	\$ -	Infrastructure			82.00
7-Palmdale-1	LA	City of Palmdale - Civic Center Complete Streets	X		\$ 2,564	\$ 1,700	\$ 66	\$ 212	\$ -	\$ 1,422	\$ 1,422	\$ -	\$ 66	\$ 212	\$ -	Infrastructure			82.00
7-Rosemead-1	LA	SR2S Sidewalk Gap Closure on Delta Avenue	X		\$ 1,175	\$ 1,100	\$ -	\$ -	\$ 1,100	\$ -	\$ 1,100	\$ -	\$ -	\$ -	\$ -	Infrastructure	X		82.00
7-Santa Monica-2	LA	Active Aging - Safe Routes for Seniors		X	\$ 500	\$ 400	\$ -	\$ 400	\$ -	\$ -	\$ -	\$ 400	\$ -	\$ -	\$ -	Non-Infrastructure			82.00
10-Angels Camp-1	CAL	Route 49 Sidewalk Infill and Bike Lanes, Angels Camp	X	X	\$ 1,985	\$ 1,985	\$ 165	\$ 1,820	\$ -	\$ -	\$ 1,820	\$ -	\$ -	\$ -	\$ 165	Infrastructure	X		81.00
6-Kern County Road Department-2	KER	Virginia Street Pedestrian Path Project	X		\$ 2,456	\$ 2,173	\$ 22	\$ 420	\$ -	\$ 1,731	\$ 1,731	\$ -	\$ 22	\$ 354	\$ 66	Infrastructure	X		81.00
10-Merced County PW-2	MER	Plainsburg Road Complete Street Upgrade	X		\$ 1,688	\$ 1,671	\$ 76	\$ 200	\$ -	\$ 1,395	\$ 1,351	\$ 44	\$ 76	\$ 200	\$ -	Combination	X		81.00
5-Monterey County Resource Management Agency DPW-2	MON	Las Lomas Drive Bicycle Lane & Pedestrian Project	X		\$ 3,168	\$ 2,894	\$ -	\$ 368	\$ -	\$ 2,526	\$ 2,431	\$ -	\$ 98	\$ 270	\$ 95	Infrastructure			81.00
3-Sacramento County-1	SAC	Folsom Boulevard Complete Street Improvements, Phase 1	X		\$ 5,001	\$ 4,180	\$ 532	\$ 176	\$ 3,472	\$ -	\$ 3,472	\$ -	\$ -	\$ 532	\$ 176	Infrastructure			81.00
8-Big Bear Lake-1	SBD	Alpine Pedal Path Rathbun Creek Extension - Big Bear Lake	X	X	\$ 986	\$ 788	\$ -	\$ 788	\$ -	\$ -	\$ 788	\$ -	\$ -	\$ -	\$ -	Infrastructure	X		81.00
8-Ontario-1	SBD	Sultana Elementary and De Anza Middle School Pedestrian Improvements	X		\$ 1,506	\$ 1,278	\$ 178	\$ -	\$ 1,100	\$ -	\$ 1,100	\$ -	\$ -	\$ 136	\$ 42	Infrastructure	X		81.00
6-Tulare County-1	TUL	Allensworth Elementary Sidewalk Improvements	X	X	\$ 313	\$ 260	\$ -	\$ 260	\$ -	\$ -	\$ 260	\$ -	\$ -	\$ -	\$ -	Combination	X		81.00
7-Norwalk-1	LA	Alondra Active Transportation Improvement	X		\$ 973	\$ 963	\$ 12	\$ 138	\$ 0	\$ 13	\$ 13	\$ 0	\$ 12	\$ 138	\$ 0	Infrastructure			80.50
10-Sonora-1	TUO	Red Church Pedestrian and Circulation Improvement	X	X	\$ 815	\$ 722	\$ 76	\$ 646	\$ -	\$ -	\$ 646	\$ -	\$ -	\$ 67	\$ 9	Infrastructure	X		80.50
7-Baldwin Park-3	LA	Walnut Creek-San Gabriel River East Bank Greenway & Neighborhood Connections	X		\$ 2,193	\$ 1,355	\$ -	\$ -	\$ 1,355	\$ -	\$ 573	\$ 782	\$ -	\$ -	\$ -	Combination			80.00
10-Merced County PW-1	MER	Delhi Community Pedestrian and Bike Connectivity Project	X		\$ 1,531	\$ 1,531	\$ 266	\$ -	\$ 1,265	\$ -	\$ 1,221	\$ 44	\$ 88	\$ 178	\$ -	Combination	X		80.00
8-Temecula-1	RIV	Santa Gertrudis Creek Pedestrian and Bicycle Trail Extension and Interconnect	X	X	\$ 4,761	\$ 3,759	\$ 189	\$ 3,570	\$ -	\$ -	\$ 3,570	\$ -	\$ 189	\$ -	\$ -	Combination			80.00
8-Apple Valley-2	SBD	Bear Valley Road Class 1 Bike Path Connector	X	X	\$ 999	\$ 792	\$ 792	\$ -	\$ -	\$ -	\$ 792	\$ -	\$ -	\$ -	\$ -	Infrastructure			80.00
8-Apple Valley-1	SBD	Apple Valley South - Safe Routes to School	X	X	\$ 4,294	\$ 3,470	\$ 333	\$ 3,137	\$ -	\$ -	\$ 3,137	\$ -	\$ -	\$ 333	\$ -	Infrastructure	X		80.00
11-Chula Vista-1	SD	Class 2 Bike Lanes on Broadway in Chula Vista, CA	X	X	\$ 1,466	\$ 851	\$ 104	\$ 747	\$ -	\$ -	\$ 747	\$ -	\$ -	\$ 104	\$ -	Infrastructure			80.00
11-El Cajon-2	SD	Cajon Valley Union School District SRTS Plan (Phase 2)	X	X	\$ 500	\$ 500	\$ 500	\$ -	\$ -	\$ -	\$ -	\$ 500	\$ -	\$ -	\$ -	Plan			80.00
11-Encinitas-1	SD	Santa Fe Drive Corridor Bike and Pedestrian Improvements			\$ 1,142	\$ 996	\$ 41	\$ -	\$ 173	\$ 782	\$ 742	\$ 40	\$ 41	\$ 148	\$ 25	Combination	X		80.00
11-National City-1	SD	National City Safe Routes to School Pedestrian Enhancements	X		\$ 2,028	\$ 1,678	\$ -	\$ -	\$ 1,678	\$ -	\$ 1,678	\$ -	\$ -	\$ -	\$ -	Infrastructure	X		80.00

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Application ID	Co	Project Title	DAC	SOF	Total Project Cost	Recommended ATP Funding	FY 17-18	FY 18-19	FY 19-20	FY 20-21	CON	CON NI	PA&ED	PS&E	ROW	Project Type	SRTS	REC TR	Final Score
2-Redding-3	SHA	West Street Area School Safety Improvements	X		\$ 3,196	\$ 2,538	\$ 400	\$ -	\$ 2,138	\$ -	\$ 2,138	\$ -	\$ 400	\$ -	\$ -	Infrastructure	X		80.00
8-Riverside County DPH-4	RIV	Riverside County SRTS Program, Lake Elsinore	X	X	\$ 625	\$ 500	\$ -	\$ 500	\$ -	\$ -	\$ -	\$ 500	\$ -	\$ -	\$ -	Non-Infrastructure	X		80.00
4-Berkeley-3	ALA	Sacramento Street Complete Streets Improvements	X		\$ 1,814	\$ 1,542	\$ -	\$ 185	\$ 1,357	\$ -	\$ 1,357	\$ -	\$ -	\$ 185	\$ -	Infrastructure			79.00
1-Humboldt County PW-2	HUM	McKinleyville Safe Routes to School Program	X		\$ 612	\$ 612	\$ 25	\$ 52	\$ 535	\$ -	\$ 490	\$ 45	\$ 25	\$ 40	\$ 12	Combination	X		79.00
6-Arvin-1	KER	Franklin Street Pedestrian and Bicycle Infrastructure Improvements	X		\$ 350	\$350 \$0	\$	\$	\$50 \$0	\$300 \$0	\$300 \$0	\$	\$5 \$0	\$45 \$0	\$	Infrastructure	X		-79.00
7-Los Angeles Co Dept. of PW-5	LA	Puente Creek Bikeway	X		\$ 3,700	\$2960 \$0	\$	\$400 \$0	\$	\$2560 \$0	\$2360 \$0	\$200 \$0	\$	\$400 \$0	\$	Combination	X		-79.00
9-Mono County-1	MONO	Mono County Complete Streets- Bridgeport Main Street Revitalization	X	X	\$ 434	\$434 \$0	\$23 \$0	\$43 \$0	\$368 \$0	\$	\$368 \$0	\$	\$23 \$0	\$43 \$0	\$	Infrastructure	X		-79.00
12-Buena Park-1	ORA	Buena Park School District Safe Routes to School (SRTS) Improvements	X		\$ 1,654	\$ 1,644	\$ 50	\$ 166	\$ 1,428	\$ -	\$ 1,428	\$ 91	\$ 50	\$ 75	\$ -	Combination	X		79.00
8-Riverside-1	RIV	La Sierra Neighborhood Sidewalk Improvements	X	X	\$ 1,215	\$ 999	\$ -	\$ -	\$ -	\$ 999	\$ 999	\$ -	\$ -	\$ -	\$ -	Infrastructure			79.00
3-Sacramento-5	SAC	Two Rivers Trail (Phase II)	X	X	\$ 6,361	\$ 3,333	\$ -	\$ 3,333	\$ -	\$ -	\$ 3,333	\$ -	\$ -	\$ -	\$ -	Infrastructure		X	79.00
8-Fontana-1	SBD	Fontana Safe Routes to Schools Pedestrian Improvements	X		\$ 1,918	\$ 1,918	\$ 10	\$ 157	\$ 191	\$ 1,560	\$ 1,556	\$ 4	\$ 10	\$ 157	\$ 191	Combination	X		79.00
TOTALS NEW PROJECTS					\$ 135,172	\$ 94,144													

**2019 Active Transportation Program
Metropolitan Planning Organization Component
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(\$1,000's)**

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MPO	Application ID	County	Project Title	Total Project Cost	ATP Request	FY 19-20	FY 20-21	FY 21-22	FY 22-23	PA&ED	PS&E	ROW	CON	CON NI	SOF	Project Type	DAC	SRTS	State Score	Regional Score or Rank*
FCOG	6-Fresno-1	Fresno	Connecting the Winchell Neighborhood, Butler/8th and Orange/Lowe Signals	\$ 1,251	\$ 1,108	\$ 87	\$ 31	-	\$ 990	\$ 1	\$ 86	\$ 31	\$ 990	-		Infrastructure - S	X	X	86	89.75
FCOG	6-Fresno-4	Fresno	Pathway to Play at Inspiration Park - Gettysburg/Polk Sidewalk/Signal	\$ 865	\$ 616	-	\$ 616	-	-	-	-	-	\$ 616	-		Infrastructure - S	X	X	85	85.25
FCOG	6-Fresno-3	Fresno	Southeast Fresno Cycle Track, First from Tulare to Ventura/Hazelwood	\$ 902	\$ 258	-	\$ 258	-	-	-	-	-	\$ 258	-		Infrastructure - S	X	X	73	83
FCOG	6-Fresno County-1	Fresno	West Park Pedestrian Pathway	\$ 548	\$ 548	-	\$ 98	\$ 450	-	\$ 20	\$ 78	-	\$ 450	-		Infrastructure - S	X	X	77	76.67
FCOG	6-Fresno-7	Fresno	Pathway to Play at Calwa Park - Barton/Florence Sidewalks	\$ 361	\$ 288	\$ 17	\$ 79	-	\$ 192	\$ 1	\$ 16	\$ 79	\$ 192	-		Infrastructure - S	X		66	76
FCOG	6-Fresno-5	Fresno	Robinson Elementary Crossing Improvements	\$ 660	\$ 584	\$ 45	\$ 35	-	\$ 504	\$ 2	\$ 43	\$ 35	\$ 504	-		Infrastructure - S	X	X	81	74
FCOG	6-Fresno-2	Fresno	Mckenzie Trail Crossing Improvements	\$ 422	\$ 374	\$ 32	\$ 342	-	-	\$ 2	\$ 30	-	\$ 342	-		Infrastructure - S	X		73	73.75
FCOG	6-Fresno-6	Fresno	Maple Avenue Cycle Track and Pedestrian Scramble	\$ 182	\$ 111	\$ 111	-	-	-	-	-	-	\$ 111	-		Infrastructure - S	X	X	85	70.5
FCOG	6-Coalinga-1	Fresno	Coalinga Perimeter Multi-Use Trail	\$ 952	\$ 885	\$ 100	\$ 125	-	\$ 660	\$ 100	\$ 65	\$ 60	\$ 660	-		Infrastructure - S	X		69	69.8
KCOG	6-Bakersfield-1	Kern	Friant-Kern Canal Multi-Use Path	\$ 8,200	\$ 4,306	-	-	\$ 4,306	-	-	-	-	\$ 4,306	-		Infrastructure-L	X		87	87
MTC	4-Alameda County TC-2	Alameda	Alameda County School Travel Opportunities Program	\$ 4,178	\$ 3,761	\$ 3,761	-	-	-	-	-	-	-	\$ 3,761	X	Non-Infrastructure	X	X	71	99
MTC	4-San Francisco Municipal Transportation Agency-1	San Francisco	6th Street Pedestrian Safety Project	\$ 19,761	\$ 6,000	\$ 6,000	-	-	-	-	-	-	\$ 6,000	-	X	Infrastructure - L	X		81	92
MTC	4-Sonoma-Marin Area Rail Transit District (SMART)-2	Sonoma	SMART Pathway- Santa Rosa-Rohnert Park and Penngrove Segments	\$ 13,802	\$ 12,574	\$ 1,817	\$ 10,757	-	-	-	-	\$ 1,817	\$ 10,757	-		Infrastructure - L	X	X	80	92
MTC	4-San Jose-1	Santa Clara	Willow-Keyes Complete Streets Improvements	\$ 19,649	\$ 12,926	-	-	-	\$ 12,926	-	-	-	\$ 12,926	-		Infrastructure - L	X	X	82	91
MTC	4-Albany-2	Alameda	Ohlone Greenway Trail Safety Improvements	\$ 665	\$ 410	\$ 410	-	-	-	-	-	-	\$ 410	-	X	Infrastructure - S	X	X	74	88
MTC	4-Alameda County Public Works Department-12	Alameda	Active and Safe Oakland	\$ 999	\$ 999	\$ 999	-	-	-	-	-	-	-	\$ 999	X	Non-Infrastructure	X	X	84	87
SACOG	3-Sacramento-2	Sacramento	Franklin Boulevard Complete Street Phase 2	\$ 12,591	\$ 1,880	\$ 1,880	-	-	-	-	\$ 1,000	\$ 880	-	-		Infrastructure - L	X		88	90
SACOG	3-Sacramento-1	Sacramento	Broadway Complete Streets Project - Phase 2	\$ 4,886	\$ 3,661	\$ 550	\$ 3,111	-	-	-	\$ 550	-	\$ 3,111	-		Infrastructure - M	X		86	84
SACOG	3-Sacramento County-4	Sacramento	Fern Bacon Middle School SRTS Improvements	\$ 990	\$ 872	-	\$ 224	\$ 648	-	-	\$ 83	\$ 141	\$ 598	\$ 50	X	Infrastructure + NI - S		X	85	81
SACOG	3-San Juan Unified School District-1	Sacramento	Three Steps to Safer Routes for Students	\$ 441	\$ 397	\$ 397	-	-	-	-	-	-	-	\$ 397	X	Non-Infrastructure		X	77	78
SACOG	3-Yuba County-2	Yuba	Cedar Lane Elementary SRTS Project	\$ 3,079	\$ 2,729	-	-	\$ 2,729	-	-	-	-	\$ 2,700	\$ 29	X	Infrastructure + NI - M	X	X	65	77
SACOG	3-Woodland-1	Yolo	W. Gibson Road SRTS Project	\$ 5,239	\$ 600	\$ 600	-	-	-	\$ 250	\$ 350	-	-	-	X	Infrastructure - M		X	74.5	77
SACOG	3-Citrus Heights-1	Sacramento	Auburn Boulevard Complete Streets - Phase 2	\$ 17,225	\$ 1,525	-	-	\$ 1,525	-	-	-	-	\$ 1,512	\$ 13		Infrastructure + NI - L		X	69	76
SANDAG	11-San Diego Association of Governments-4	San Diego	GObyBIKE San Diego: Education and Encouragement Start-Up Program	\$ 1,967	\$ 1,381	\$ 1,381	-	-	-	-	-	-	-	\$ 1,381		Non-Infrastructure	X		72	659
SANDAG	11-Escondido-1	San Diego	Escondido Creek Trail Transit Center Bicycle Path Improvements Project	\$ 827	\$ 747	-	\$ 747	-	-	-	-	-	\$ 747	-		Infrastructure - S	X		75	626.4
SANDAG	11-San Diego Association of Governments-1	San Diego	University Bikeway	\$ 16,825	\$ 8,561	-	-	\$ 8,561	-	-	-	-	\$ 8,561	-		Infrastructure - L	X		79	611.52
SANDAG	11-National City-1	San Diego	8th Street and Roosevelt Ave. Active Transportation Corridor, National City	\$ 6,991	\$ 5,185	-	-	-	\$ 5,185	-	-	-	\$ 5,185	-		Infrastructure - M	X		85	604.64
SJCOG	10-San Joaquin County-1	San Joaquin	Oro Avenue & Section Avenue Sidewalk Improvements	\$ 1,439	\$ 1,439	\$ 75	\$ 180	\$ 1,184	-	\$ 75	\$ 180	-	\$ 1,184	-	X	Infrastructure - S	X	X	87	86.8
SJCOG	10-Stockton-4	San Joaquin	SRTS Sidewalk Gap Closure***	\$ 5,982	\$ 2,075	\$ 490	\$ 565	-	\$ 1,020	\$ 490	\$ 452	\$ 113	\$ 1,020	-		Infrastructure - M	X	X	85	85.2
SCAG	11-Imperial County-2	Imperial	Heffernan Avenue from 14th Street to 10th Street	\$ 727	\$ 642	\$ 87	\$ 44	\$ 511	-	\$ 8	\$ 79	\$ 44	\$ 511	-	X	Infrastructure - S	X	X	71	91
SCAG	7-Huntington Park-1	Los Angeles	Huntington Park Bicycle and Pedestrian Safety and Connectivity Project	\$ 4,650	\$ 4,117	\$ 58	-	\$ 288	\$ 3,771	\$ 58	\$ 288	-	\$ 3,771	-	X	Infrastructure - M	X		89	99
SCAG	7-LA Bureau of Street Services (Engineering Division)-8	Los Angeles	Broadway-Manchester Active Transportation Equity Project	\$ 46,600	\$ 24,821	\$ 4,000	-	\$ 1,200	\$ 19,621	\$ 4,000	\$ 1,200	-	\$ 19,621	-		Infrastructure - L	X	X	89	99
SCAG	7-Los Angeles Department of Public Works (Bureau of Engineering)-7	Los Angeles	LA River Greenway, West San Fernando Valley Gap Closure	\$ 51,822	\$ 18,793	\$ 900	-	\$ 17,893	-	-	\$ 500	\$ 400	\$ 17,893	-	X	Infrastructure - L	X		89	99
SCAG	7-LA County Department of Public Works-3	Los Angeles	East LA Active Transportation Education and Encouragement Program	\$ 747	\$ 500	\$ 500	-	-	-	-	-	-	-	\$ 500	X	Non-Infrastructure	X		84	89
SCAG	12-Costa Mesa-1	Orange	Merrimac Way Multipurpose Street, Sidewalk and Bicycle Facility Project	\$ 1,300	\$ 1,105	\$ 1,105	-	-	-	-	-	-	\$ 1,105	-	X	Infrastructure - S	X	X	87	107
SCAG	12-Santa Ana-10	Orange	McFadden Avenue Protected Bike Lane and Bicycle Boulevard Project	\$ 6,999	\$ 6,999	\$ 1,124	\$ 5,875	-	-	\$ 102	\$ 1,022	-	\$ 5,875	-		Infrastructure - M	X		81	101
SCAG	12-Santa Ana-14	Orange	Standard Avenue Protected Bike Lane and Protected Intersection Project	\$ 6,666	\$ 6,666	\$ 1,222	\$ 5,444	-	-	\$ 122	\$ 1,100	-	\$ 5,444	-		Infrastructure - M	X		80.5	99.5
SCAG	12-Orange County Transportation Authority-2	Orange	Safe Travels Education Program (STEP) Campaign	\$ 500	\$ 500	\$ 500	-	-	-	-	-	-	-	\$ 500	X	Non-Infrastructure	X	X	74	94
SCAG	8-City of Palm Desert-1	Riverside	San Pablo Avenue Improvements from Fred Waring to Magnesia Falls	\$ 4,503	\$ 3,222	\$ 3,222	-	-	-	-	-	-	\$ 3,222	-	X	Infrastructure - M	X	X	86	106

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Attachment B

MPO	Application ID	County	Project Title	Total Project Cost	ATP Request	FY 19-20	FY 20-21	FY 21-22	FY 22-23	PA&ED	PS&E	ROW	CON	CON NI	SOF	Project Type	DAC	SRTS	State Score	Regional Score or Rank*	
SCAG	8-Riverside County Department of Public Health (Injury Prevention Services)-2	Riverside	Riverside County SRTS, Corona	\$ 580	\$ 325	-	\$ 325	-	-	-	-	-	-	\$ 325	X	Non-Infrastructure	X	X	86	86	
SCAG	8-Riverside County Transportation Department-2	Riverside	El Toro Road-Dexter Avenue SRTS Sidewalk Project	\$ 2,311	\$ 2,311	\$ 50	\$ 410	-	\$ 1,851	\$ 50	\$ 330	\$ 80	\$ 1,763	\$ 88	X	Infrastructure + NI - M	X	X	77	87	
SCAG	8-Lake Elsinore-3	Riverside	Murrieta Creek Multi-Use Trail - Palomar Trail to Lake Trail	\$ 5,079	\$ 5,079	\$ 365	\$ 350	\$ 460	\$ 3,904	\$ 365	\$ 350	\$ 460	\$ 3,904	-	X	Infrastructure - M			76	86	
SCAG	8-Riverside County Department of Public Health (Injury Prevention Services)-3	Riverside	Riverside County SRTS Program, Desert Hot Springs	\$ 610	\$ 500	-	\$ 500	-	-	-	-	-	-	\$ 500	X	Non-Infrastructure	X	X	87	87	
SCAG	8-Fontana-2	San Bernardino	Fontana SRTS Gap Closure	\$ 1,477	\$ 1,477	\$ 223	\$ 1,254	-	-	\$ 12	\$ 124	\$ 87	\$ 1,254	-	X	Infrastructure - S	X	X	88	108	
SCAG	8-Rialto-3	San Bernardino	Terra Vista Drive Neighborhood SRTS Infrastructure Implementation	\$ 663	\$ 663	\$ 20	\$ 60	\$ 583	-	\$ 20	\$ 60	-	\$ 583	-	X	Infrastructure - S	X	X	87	107	
SCAG	8-Twenty-nine Palms-1	San Bernardino	Twentynine Palms SRTS Infrastructure Implementation Grant	\$ 1,467	\$ 1,467	\$ 153	\$ 51	\$ 1,263	-	\$ 153	\$ 51	-	\$ 1,263	-		Infrastructure - S	X	X	87	107	
SCAG	8-Rialto-1	San Bernardino	Safe Routes for Active Play, Work, and Live Rialto!	\$ 549	\$ 549	\$ 549	-	-	-	-	-	-	-	\$ 549	X	Non-Infrastructure	X	X	86	106	
SCAG	8-Ontario-1	San Bernardino	Pedestrian Improvements around Haynes, Vista Grande and Oaks Schools	\$ 6,998	\$ 5,764	\$ 841	\$ 4,923	-	-	-	-	\$ 841	\$ 4,767	\$ 156	X	Infrastructure + NI - M	X	X	84	104	
SCAG	8-San Bernardino Association of Government-2	San Bernardino	San Bernardino County SRTS Program	\$ 1,053	\$ 500	\$ 500	-	-	-	-	-	-	-	\$ 500	X	Non-Infrastructure	X	X	83	103	
SCAG	SCAG	Various	SCAG 2019 Local Demonstration Initiative	\$ 2,599	\$ 2,599	\$ 2,599	-	-	-	-	-	-	-	\$ 2,599	X	Non-Infrastructure	X		N/A	N/A	
SCAG	7-Ventura-1	Ventura	Active Transportation Mobility Plan	\$ 950	\$ 950	\$ 950	-	-	-	-	-	-	-	\$ 950	X	Plan	X	X	68	88	
SCAG	7-Oxnard-2	Ventura	Oxnard Boulevard Bikeway Gap Closure	\$ 860	\$ 860	\$ 860	\$ 762	-	-	-	\$ 98	-	\$ 762	\$ 0	-	X	Infrastructure - S	X	X	63	83
SCAG	7-Ventura County-1	Ventura	Potrero Road Bike Lane Improvements – Phase 2	\$ 1,515	\$ 1,265	\$ 1,265	-	-	-	-	-	-	\$ 1,265	-	X	Infrastructure - S			68	78	
SCAG	7-Thousand Oaks-1	Ventura	Los Feliz Sidewalk Phase 2	\$ 1,651	\$ 898	-	\$ 898	-	-	-	-	-	\$ 898	-	X	Infrastructure - S	X	X	56	76	
StanCOG	10-Stanislaus County-2	Stanislaus	Bret Harte Elementary Safe Crossing and Active Transportation Connectivity Project	\$ 3,005	\$ 2,402	-	-	-	\$ 2,402	-	-	-	\$ 2,380	\$ 22		Infrastructure +NI-M	X	X	86	86	
StanCOG	10-Patterson-1	Stanislaus	Patterson - Citywide Active Transportation Plan	\$ 99	\$ 99	\$ 99	-	-	-	-	-	-	-	\$ 99	X	Plan	X	X	79	79	
TMPO	3-South Lake Tahoe-1	El Dorado	Lake Tahoe Boulevard Class I Bicycle Trail	\$ 3,025	\$ 744	-	\$ 744	-	-	-	-	-	\$ 744	-		Infrastructure - M	X	X	68	288.25	
TCAG	6-Woodlake-1	Tulare	North Valencia Boulevard SRTS Extension, Gap Improvements	\$ 1,204	\$ 980	\$ 980	-	-	-	-	-	-	\$ 980	-	X	Infrastructure - S	X	X	77	100	
TCAG	6-Tulare County-11	Tulare	County of Tulare: Road 160 Sidewalk Improvements, Ivanhoe	\$ 1,575	\$ 1,288	-	\$ 263	-	\$ 1,025	-	-	\$ 263	\$ 1,025	-		Infrastructure - M	X	X	80	96	
			Totals	\$ 313,663	\$ 174,025																
*Regional scores and ranks are on various scales (and not necessarily out of 100). Individual scoring systems are outlined in each MPO's guidelines.																					

**2021 Active Transportation Program
Statewide Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-21-30
(\$1,000's)**

Reference Number 4.27
June 26-27, 2025
Attachment B

Amendments are shown in bold and strikethrough and highlighted in yellow

Application ID	County	Project Title	Total Project Cost	Recommended Funding	FY 21-22	FY 22-23	FY 23-24	FY 24-25	PA&ED	PS&E	ROW	CON	CON-NI	Project Type	DAC	SRTS	Final Score
Active Transportation Resource Center	Various	Active Transportation Resource Center	\$ 4,000	\$ 4,000	\$ -	\$ -	\$ 2,000	\$ 2,000	\$ -	\$ -	\$ -	\$ -	\$ 4,000	Non-Infrastructure	N/A	N/A	N/A
3-Sacramento, City of-1	Sacramento	Franklin Boulevard Complete Street Project	\$ 16,265	\$ 9,323	\$ -	\$ -	\$ 9,323	\$ -	\$ -	\$ -	\$ -	\$ 9,323	\$ -	Infrastructure - Large	x		99
4-Oakland, City of-1 ^s	Alameda	7th Street Connection Project	\$ 21,037	\$ 14,180	\$ -	\$ -	\$ -	\$ 14,180	\$ -	\$ -	\$ -	\$ 14,180	\$ -	Infrastructure - Large	x		98
6-Huron, City of-1	Fresno	City of Huron Bicyclist and Pedestrian Safety Improvement Project	\$ 1,969	\$ 1,769	\$ 25	\$ 100	\$ 1,644	\$ -	\$ 25	\$ 100	\$ -	\$ 1,644	\$ -	Infrastructure - Small	x	x	98
8-Perris, City of-1	Riverside	City of Perris Bike and Pedestrian Network Project	\$ 1,999	\$1,931 \$0	\$35 \$0	\$1,896 \$0	\$---	\$---	\$---	\$---	\$---	\$1,896 \$0	\$35 \$0	Infrastructure + NI - Small	x		97
8-San Bernardino County-2	San Bernardino	Muscoy Area Safe Routes to School Pedestrian Improvements Project	\$ 2,355	\$ 1,881	\$ 112	\$ 463	\$ -	\$ 1,306	\$ 112	\$ 160	\$ 303	\$ 1,271	\$ 35	Infrastructure + NI - Medium	x	x	97
6-Fresno County-1	Fresno	Biola Community Sidewalks	\$ 1,498	\$ 1,255	\$ -	\$ -	\$ 1,255	\$ -	\$ -	\$ -	\$ -	\$ 1,255	\$ -	Infrastructure - Small	x	x	96
7-Los Angeles, City of-3	Los Angeles	SRTS Carver Middle, Ascot Avenue and Harmony Elementary Schools Project	\$ 6,700	\$ 6,030	\$ 801	\$ -	\$ 290	\$ 4,939	\$ 801	\$ 290	\$ -	\$ 4,939	\$ -	Infrastructure - Medium	x	x	96
7-Los Angeles, City of-5	Los Angeles	SRTS Panorama City Elementary School Project	\$ 6,832	\$ 6,149	\$ 756	\$ -	\$ 329	\$ 5,064	\$ 756	\$ 329	\$ -	\$ 5,064	\$ -	Infrastructure - Medium	x	x	96
3-Sacramento County-2	Sacramento	South Sacramento County Safe Routes to School Project	\$ 1,946	\$ 1,946	\$ 95	\$ 390	\$ 1,461	\$ -	\$ 95	\$ 190	\$ 200	\$ 1,381	\$ 80	Infrastructure + NI - Small	x	x	96
8-Ontario, City of-1	San Bernardino	Vine Ave & B St Bike Boulevard Project	\$ 4,881	\$ 4,392	\$ 45	\$ 468	\$ 3,879	\$ -	\$ 45	\$ 468	\$ -	\$ 3,879	\$ -	Infrastructure - Medium	x		96
5-Watsonville, City of-1 ^s	Santa Cruz	Safer Access to Pajaro Valley High School and Beyond	\$ 15,823	\$ 11,709	\$ 1,168	\$ 10,541	\$ -	\$ -	\$ -	\$ -	\$ 521	\$ 10,541	\$ 647	Infrastructure + NI - Large	x	x	96
7-Long Beach, City of-1	Los Angeles	Downtown Long Beach Walkable Corners	\$ 8,771	\$ 7,893	\$ 768	\$ 450	\$ -	\$ 6,675	\$ 225	\$ 450	\$ -	\$ 6,675	\$ 543	Infrastructure + NI - Large	x		95
7-Maywood, City of-1	Los Angeles	City of Maywood Active Transportation Plan	\$ 263	\$ 263	\$ 263	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 263	Plan	x	x	95
10-Mariposa County-1	Mariposa	Mariposa Elementary School Connectivity Project	\$ 1,900	\$ 1,900	\$ 100	\$ -	\$ 1,800	\$ -	\$ 100	\$ -	\$ -	\$ 1,800	\$ -	Infrastructure - Small	x	x	95
10-Mariposa County-2	Mariposa	Mariposa Creek Parkway	\$ 5,176	\$ 4,415	\$ 200	\$ 1,200	\$ -	\$ 3,015	\$ 200	\$ 450	\$ 750	\$ 3,015	\$ -	Infrastructure - Medium	x		95
8-Riverside County-10	Riverside	Riverside County Safe Routes for All - San Jacinto	\$ 600	\$ 600	\$ -	\$ 600	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 600	Non-Infrastructure	x	x	95
11-San Diego Association of Governments (SANDAG)-1	San Diego	Orange Family Friendly Street Project	\$ 5,660	\$ 4,317	\$ -	\$ 4,317	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 4,317	\$ -	Infrastructure - Medium	x	x	95
5-San Luis Obispo County-1 ^s	San Luis Obispo	San Luis Obispo County-Bob Jones Trail Gap Closure	\$ 23,414	\$ 18,248	\$ 2,295	\$ 15,953	\$ -	\$ -	\$ -	\$ 321	\$ 1,974	\$ 15,953	\$ -	Infrastructure - Large	x		95
2-Redding, City of-2	Shasta	Turtle Bay to Downtown Gap Completion Project	\$ 3,935	\$ 2,665	\$ -	\$ -	\$ 50	\$ 2,615	\$ -	\$ -	\$ 50	\$ 2,462	\$ 153	Infrastructure + NI - Medium	x	x	95
4-Fairfield, City of-1 ^s	Solano	West Texas Street Complete Streets Project	\$ 16,922	\$ 10,903	\$ -	\$ 955	\$ 9,948	\$ -	\$ -	\$ 838	\$ -	\$ 9,948	\$ 117	Infrastructure + NI - Large	x	x	95
3-West Sacramento, City of-2	Yolo	Sycamore Trail (Phase 2) Bicycle/Pedestrian Overcrossing	\$ 11,538	\$ 3,500	\$ 3,500	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 3,500	\$ -	Infrastructure - Large	x		95
6-Fresno, City of-1	Fresno	Kids Crossing: Safe Routes to School in South Fresno	\$ 1,636	\$ 1,636	\$ 141	\$ 14	\$ 1,481	\$ -	\$ 3	\$ 138	\$ 14	\$ 1,441	\$ 40	Infrastructure + NI - Small	x	x	94
1-Arcata, City of-1	Humboldt	Arcata Annie & Mary Trail Connectivity Project	\$ 5,286	\$ 4,220	\$ 67	\$ 495	\$ 3,658	\$ -	\$ 67	\$ 240	\$ 255	\$ 3,658	\$ -	Infrastructure - Medium	x		94
7-South El Monte, City of-1	Los Angeles	South El Monte Safe Routes to School Pedestrian Safety Project	\$ 1,637	\$ 1,637	\$ 10	\$ 130	\$ 1,497	\$ -	\$ 10	\$ 130	\$ -	\$ 1,497	\$ -	Infrastructure - Small	x	x	94
11-Oceanside, City of-1	San Diego	Laurel Elementary Safe Routes to School	\$ 1,535	\$ 1,522	\$ 287	\$ 160	\$ 1,075	\$ -	\$ 160	\$ 160	\$ -	\$ 1,075	\$ 127	Infrastructure + NI - Small	x	x	94
6-Porterville, City of-4	Tulare	Butterfield Stage Corridor (W North Grand Ave to College Ave)	\$ 7,750	\$ 7,100	\$ -	\$ 7,100	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 7,100	\$ -	Infrastructure - Large	x		94

**2021 Active Transportation Program
Statewide Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-21-30
(\$1,000's)**

Reference Number 4.27
June 26-27, 2025
Attachment B

Application ID	County	Project Title	Total Project Cost	Recommended Funding	FY 21-22	FY 22-23	FY 23-24	FY 24-25	PA&ED	PS&E	ROW	CON	CON-NI	Project Type	DAC	SRTS	Final Score
4-Contra Costa County-2	Contra Costa	North Bailey Road Active Transportation Corridor	\$ 6,845	\$ 6,159	\$ 499	\$ -	\$ 5,660	\$ -	\$ 499	\$ -	\$ -	\$ 5,660	\$ -	Infrastructure - Medium	x	x	93
6-Delano, City of-1	Kern	ATP-5 SRTS Intersection Enhancement and NI Work Plan	\$ 1,178	\$ 1,164	\$ -	\$ 140	\$ 1,024	\$ -	\$ -	\$ 140	\$ -	\$ 949	\$ 75	Infrastructure + NI - Small	x	x	93
7-Bell Gardens, City of-1	Los Angeles	Bell Gardens Complete Streets Improvements - Phase 1	\$ 6,999	\$ 6,499	\$ 200	\$ 6,299	\$ -	\$ -	\$ 200	\$ -	\$ -	\$ 6,299	\$ -	Infrastructure - Medium	x		93
7-Long Beach, City of-2	Los Angeles	Pacific Avenue Cycle Track	\$ 8,332	\$ 7,498	\$ 225	\$ 1,533	\$ -	\$ 5,740	\$ 225	\$ 675	\$ -	\$ 5,740	\$ 858	Infrastructure + NI - Large	x		93
7-Los Angeles, City of-11 [§]	Los Angeles	Connecting Canoga Park Through Safety and Urban Cooling Improvements	\$ 38,655	\$ 30,731	\$ 3,567	\$ -	\$ 1,921	\$ 25,243	\$ 3,567	\$ 1,921	\$ -	\$ 25,243	\$ -	Infrastructure - Large	x		93
7-Los Angeles, City of-4	Los Angeles	SRTS Berendo Middle and 3 Feeder Elementary Schools Safety Project	\$ 11,057	\$ 9,951	\$ 188	\$ -	\$ 1,588	\$ 8,175	\$ 188	\$ 1,588	\$ -	\$ 8,175	\$ -	Infrastructure - Large	x	x	93
4-Santa Clara County-1	Santa Clara	Active and Safe Routes to a Healthier City	\$ 2,510	\$ 2,510	\$ 2,510	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 2,510	Non-Infrastructure	x	x	93
5-Santa Cruz, City of-2	Santa Cruz	Santa Cruz Rail Trail Segment 7 Phase 2 Construction	\$ 12,030	\$ 9,184	\$ 9,184	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 8,634	\$ 550	Infrastructure + NI - Large	x	x	93
4-Oakland, City of-2 [§]	Alameda	East Oakland Neighborhood Bike Routes	\$ 21,859	\$ 17,269	\$ -	\$ -	\$ 17,269	\$ -	\$ -	\$ -	\$ -	\$ 17,269	\$ -	Infrastructure - Large	x		92
3-El Dorado County-3	El Dorado	Pollock Pines - Pony Express Trail Bicycle and Pedestrian Improvements	\$ 2,000	\$ 1,440	\$ 1,440	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,440	\$ -	Infrastructure - Small	x		92
11-Imperial Beach, City of-1	San Diego	9th St Active Transportation Corridor	\$ 3,354	\$ 3,018	\$ 539	\$ 2,479	\$ -	\$ -	\$ -	\$ 539	\$ -	\$ 2,479	\$ -	Infrastructure - Medium	x	x	92
7-Santa Monica, City of-2*	Los Angeles	Stewart-Pennsylvania Safety Enhancement Project	\$ 4,000	\$ 3,196	\$ 3,196	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 3,171	\$ 25	Infrastructure + NI - Medium	x		92
7-Ventura County-2	Ventura	El Rio Pedestrian Improvement and Safe Route to School Project	\$ 6,960	\$ 6,195	\$ 222	\$ 884	\$ 5,089	\$ -	\$ 222	\$ 884	\$ -	\$ 5,089	\$ -	Infrastructure - Medium	x	x	92
11-National City, City of-3	San Diego	Highland Avenue Inter-City Bike Connection	\$ 1,897	\$ 1,343	\$ 58	\$ 260	\$ 1,025	\$ -	\$ 58	\$ 260	\$ -	\$ 1,025	\$ -	Infrastructure - Small	x		92
			\$ 309,004	\$ 239,610													

*The City of Santa Monica's Stewart-Pennsylvania Safety Enhancement Project was added to the staff recommendations after a scoring error was identified.

[§]Project requires a baseline agreement. Please see the SB 1 Accountability and Transparency Guidelines for more information.

Note: The City of Porterville's Butterfield Stage Corridor (Henderson Avenue to Date Avenue) project was included in the staff recommendations released on February 8, 2021. This project has been removed from the recommendations because its scope is encompassed in the scope of the City of Porterville's Butterfield Stage Corridor (W North Grand Ave to College Ave) project.

**2021 Active Transportation Program
Small Urban and Rural Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-21-30
(\$1,000's)**

Reference Number 4.27
June 26-27, 2025
Attachment B

Amendments are shown in bold and strikethrough and highlighted in yellow

Application ID	County	Project Title	Total Project Cost	Recommended Funding	FY 21-22	FY 22-23	FY 23-24	FY 24-25	PA&ED	PS&E	ROW	CON	CON-NI	Project Type	DAC	SRTS	Final Score
2-Redding, City of-1	Shasta	Victor Ave & Cypress Ave Active Transportation (VCAT) Project	\$ 10,409	\$ 7,822	\$ 1,352	\$ 740	\$ 5,730	\$ -	\$ 1,352	\$ 338	\$ 402	\$ 5,643	\$ 87	Infrastructure + NI - Large	x	x	92
1-Clearlake, City of-1	Lake	Dam Road Extension & South Center Drive Bike/Pedestrian Improvements	\$ 997	\$ 997	\$ 997	\$ -	\$ -	\$ -	\$ -	\$ 82	\$ -	\$ 915	\$ -	Infrastructure - Small	x	x	91
5-Seaside, City of-1 [§]	Monterey	Broadway Ave Complete Street Corridor	\$ 14,001	\$ 12,041	\$ 1,576	\$ -	\$ 10,465	\$ -	\$ -	\$ 1,576	\$ -	\$ 9,450	\$ 1,015	Infrastructure + NI - Large	x	x	91
2-Karuk Tribe-1	Siskiyou	Happy Camp Complete Streets Project	\$ 12,221	\$ 9,971	\$ 600	\$ 1,901	\$ -	\$ 7,470	\$ 600	\$ 800	\$ 1,101	\$ 7,470	\$ -	Infrastructure - Large	x	x	91
2-Siskiyou County Transportation Commission-1	Siskiyou	Siskiyou- Regional Active Transportation Plan	\$ 212	\$ 202	\$ 202	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 202	Plan	x		91
10-Tuolumne County-1	Tuolumne	Jamestown Community Connectivity Project	\$ 2,300	\$ 2,071	\$ 198	\$ 147	\$ 140	\$ 1,586	\$ 198	\$ 147	\$ 140	\$ 1,586	\$ -	Infrastructure - Medium	x	x	91
6-Corcoran, City of-1	Kings	Corcoran Safe Routes to School	\$ 1,998	\$ 1,998	\$ 235	\$ -	\$ 1,763	\$ -	\$ 15	\$ 220	\$ -	\$ 1,763	\$ -	Infrastructure - Small	x	x	90
5-Santa Barbara, City of-3	Santa Barbara	Upper De La Vina Street Gap Closure and Safe Crossings	\$ 1,998	\$ 1,998	\$ 290	\$ -	\$ 37	\$ 1,671	\$ 290	\$ 29	\$ 8	\$ 1,671	\$ -	Infrastructure - Small	x		90
2-Shasta County-3 [§]	Shasta	Cottonwood Active Transportation Trunk Line Express (CATTLE) Network	\$ 17,844	\$14,273	\$1,197	\$2,556	\$76	\$3,227	\$1,197	\$1796	\$760	\$3,227	\$76	Infrastructure + NI - Large	x	x	90
			\$ 61,980	\$ 37,100													

[§]Project requires a baseline agreement. Please see the SB 1 Accountability and Transparency Guidelines for more information.

2021 Active Transportation Program
Metropolitan Planning Organization Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-21-46
(\$1,000's)

Reference Number 4.27
June 26-27, 2025
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MPO	Application ID	County	Project Title	Total Project Cost	Recommended ATP Funding	FY 21-22	FY 22-23	FY 23-24	FY 24-25	PA&ED	PS&E	ROW SUP [†]	ROW	CON SUP [†]	CON	CON-NI	Project Type	DAC	SRTS	State Score	Regional Score or Rank*
FCOG	6-Clovis, City of-1	Fresno	Sierra Vista Elementary Area Sidewalk Improvements	\$ 997	\$ 997	\$ 25	\$ 96	\$ 876	\$ -	\$ 25	\$ 96	\$ -	\$ -	\$ -	\$ 876	\$ -	Infrastructure - Small	x	x	79	74.5
FCOG	6-Coalinga, City of-1	Fresno	Coalinga East Polk Street Bike/Ped Safety and Connectivity Initiative	\$ 1,770	\$ 218	\$ -	\$ 23	\$ 144	\$ 51	\$ 23	\$ 144	\$ -	\$ 51	\$ -	\$ -	\$ -	Infrastructure + NI - Small	x		85	71.33
FCOG	6-Fresno, City of-3	Fresno	Cross, Walk & Roll! SRTS in Central Fresno	\$ 1,512	\$ 1,274	\$ 2	\$ 129	\$ -	\$ 1,143	\$ 2	\$ 119	\$ -	\$ 10	\$ -	\$ 1,122	\$ 21	Infrastructure + NI - Small	x	x	91	80
FCOG	6-Orange Cove, City of-1	Fresno	Bike Lane, Sidewalk and Crossing Improvement Project	\$ 973	\$ 973	\$ 25	\$ 109	\$ 839	\$ -	\$ 25	\$ 109	\$ -	\$ -	\$ -	\$ 839	\$ -	Infrastructure - Small	x	x	79	74.17
FCOG	6-Readley, City of-1	Fresno	Jefferson Elementary Safe Routes to School	\$ 1,428	\$ 1,353	\$ -	\$ 340	\$ -	\$ 1,013	\$ -	\$ 125	\$ -	\$ 215	\$ -	\$ 1,013	\$ -	Infrastructure - Small	x	x	N/A	72.4
KCOG	6-Bakersfield, City of-2	Kern	Chester Avenue (4th Street to Brundage Lane)	\$ 791	\$240 \$0	\$240 \$0	\$	\$	\$	\$	\$	\$	\$	\$240 \$0	\$	\$	Infrastructure - Small	x	x	80	80
KCOG	6-Delano, City of-2	Kern	ATP-5 Bike Lane and Sidewalk Gap Improvement Project	\$ 925	\$ 911	\$ -	\$ -	\$ 911	\$ -	\$ -	\$ 120	\$ -	\$ -	\$ -	\$ 716	\$ 75	Infrastructure + NI - Small	x	x	81	81
KCOG	6-Kern Council of Governments-1	Kern	Safe Routes for Cyclists in Kern County's Disadvantaged Communities	\$ 826	\$ 792	\$ 792	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 792	Non-Infrastructure	x		84	84
KCOG	9-Tehachapi, City of-1	Kern	SRTS Dennison Road Bicycle/Pedestrian Corridor Improvement Project	\$ 2,437	\$ 2,432	\$ 345	\$ 2,087	\$ -	\$ -	\$ -	\$ 225	\$ -	\$ 120	\$ -	\$ 2,087	\$ -	Infrastructure - Medium	x		86	86
MTC	4-Alameda County-2	Alameda	E. Lewelling Boulevard Safe and Complete Street for Active Transportation	\$ 9,233	\$ 2,996	\$ -	\$ 2,996	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 2,996	\$ -	Infrastructure - Large	x	x	84	100
MTC	4-Emeryville, City of-2	Alameda	40th Street Protected Bikeway and Pedestrian Improvements	\$ 13,915	\$ 1,374	\$ 1,374	\$ -	\$ -	\$ -	\$ -	\$ 1,374	\$ -	\$ -	\$ -	\$ -	\$ -	Infrastructure - Large	x		87	96
MTC	4-California Department of Transportation-3	Contra Costa	Central Avenue I-80 Undercrossing Ped/Bike Improvements	\$ 4,333	\$ 3,833	\$ 535	\$ 1,050	\$ -	\$ 2,248	\$ 535	\$ 677	\$ 148	\$ 225	\$ 477	\$ 1,771	\$ -	Infrastructure - Medium	x		83	97
MTC	4-Metropolitan Transportation Commission (MTC)-1	Marin	Richmond-San Rafael Bridge Shared Use Path Gap Closure	\$ 5,612	\$ 4,302	\$ 4,302	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 4,302	\$ -	Infrastructure - Medium	x		80	101
MTC	4-San Francisco County Transportation Authority-1 [§]	San Francisco	Yerba Buena Island Multi-use Pathway Project	\$ 89,400	\$ 3,800	\$ -	\$ 3,800	\$ -	\$ -	\$ -	\$ 3,800	\$ -	\$ -	\$ -	\$ -	\$ -	Infrastructure - Large	x		73	93
MTC	4-San Francisco Municipal Transportation Agency-1 [§]	San Francisco	Folsom Streetscape Project	\$ 38,981	\$ 7,040	\$ 7,040	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 7,040	\$ -	Infrastructure - Large	x		90	92
MTC	4-San Mateo, City of-1	San Mateo	Delaware Street Safe Routes to School Corridor	\$ 1,661	\$ 1,661	\$ 37	\$ 213	\$ 10	\$ 1,401	\$ 37	\$ 213	\$ -	\$ 10	\$ -	\$ 1,401	\$ -	Infrastructure - Small		x	72	93
MTC	4-Santa Rosa, City of-1 [§]	Sonoma	Santa Rosa US Highway 101 Bicycle and Pedestrian Overcrossing	\$ 27,100	\$ 12,000	\$ -	\$ -	\$ 12,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 12,000	\$ -	Infrastructure - Large	x	x	88	93
SACOG	3-Placerville, City of-1 [§]	El Dorado	Placerville Drive Bicycle and Pedestrian Facilities	\$ 26,913	\$ 1,220	\$ -	\$ 1,220	\$ -	\$ -	\$ -	\$ 1,040	\$ -	\$ 180	\$ -	\$ -	\$ -	Infrastructure - Large	x	x	80	85
SACOG	3-Citrus Heights, City of-1	Sacramento	Old Auburn Road Complete Streets - Phase I	\$ 12,096	\$ 2,115	\$ 609	\$ -	\$ 1,506	\$ -	\$ 609	\$ 914	\$ -	\$ 592	\$ -	\$ -	\$ -	Infrastructure - Large			71	80
SACOG	3-Elk Grove, City of-1	Sacramento	Laguna Creek Inter-Regional Trail Crossing at State Route 99	\$ 7,770	\$ 504	\$ 504	\$ -	\$ -	\$ -	\$ 504	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	Infrastructure - Large			79	81
SACOG	3-Sacramento, City of-2	Sacramento	American River Bridge Shared-use Path	\$ 7,000	\$ 2,700	\$ -	\$ 2,700	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 2,700	\$ -	Infrastructure - Large			N/A	85
SACOG	3-West Sacramento, City of-3	Yolo	Sycamore Trail (Phase 3)	\$ 2,689	\$ 2,500	\$ 80	\$ 420	\$ 2,000	\$ -	\$ 80	\$ 320	\$ -	\$ 100	\$ -	\$ 2,000	\$ -	Infrastructure - Medium	x	x	N/A	80
SACOG	3-Winters, City of-1	Yolo	SR128/I-505 Overcrossing (Br. 22-0110)/ Russell Blvd Bicycle and Pedestrian Improvements	\$ 9,953	\$ 414	\$ 414	\$ -	\$ -	\$ -	\$ 414	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	Infrastructure - Large	x		74	77
SACOG	3-Yuba County-2	Yuba	Garden Avenue Safe Route to School Project	\$ 2,500	\$ 2,320	\$ -	\$ 50	\$ 2,270	\$ -	\$ -	\$ 50	\$ -	\$ -	\$ -	\$ 2,270	\$ -	Infrastructure - Medium	x	x	85	82
SANDAG	11-La Mesa, City of-1	San Diego	La Mesa Bike and Sidewalk Connection Project	\$ 4,488	\$ 3,962	\$ 180	\$ 375	\$ 3,407	\$ -	\$ 180	\$ 375	\$ -	\$ -	\$ -	\$ 3,407	\$ -	Infrastructure - Medium	x		47	2
SANDAG	11-San Diego Association of Governments (SANDAG)-4 [§]	San Diego	Inland Rail Trail Gap Connector	\$ 15,825	\$ 12,057	\$ 1,236	\$ -	\$ -	\$ 10,821	\$ -	\$ 1,236	\$ -	\$ -	\$ -	\$ 10,821	\$ -	Infrastructure - Large	x	x	86	1
SCAG	11-EI Centro, City of-1	Imperial	EI Centro Pedestrian Improvement Project	\$ 1,771	\$ 882	\$ -	\$ -	\$ 882	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 882	\$ -	Infrastructure - Medium	x	x	74	94
SCAG	7-EI Monte, City of-1	Los Angeles	Traffic Calming for Parkway Dr/Denholm Dr	\$ 5,350	\$ 401	\$ 401	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 401	\$ -	Infrastructure - Medium	x		91	96
SCAG	7-Huntington Park, City of-1	Los Angeles	Huntington Park's Safe Routes and Childhood Obesity Project	\$ 3,757	\$ 3,757	\$ 325	\$ 3,432	\$ -	\$ -	\$ 50	\$ 275	\$ -	\$ -	\$ -	\$ 3,405	\$ 27	Infrastructure + NI - Medium	x	x	91	101
SCAG	7-Long Beach, City of-4	Los Angeles	Pine Avenue Bicycle Boulevard	\$ 4,087	\$ 3,678	\$ 90	\$ 475	\$ 514	\$ 2,599	\$ 90	\$ 475	\$ -	\$ -	\$ -	\$ 2,599	\$ 514	Infrastructure + NI - Medium	x		91	101

2021 Active Transportation Program
Metropolitan Planning Organization Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-21-46
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MPO	Application ID	County	Project Title	Total Project Cost	Recommended ATP Funding	FY 21-22	FY 22-23	FY 23-24	FY 24-25	PA&ED	PS&E	ROW SUP†	ROW	CON SUP‡	CON	CON-NI	Project Type	DAC	SRTS	State Score	Regional Score or Rank*
SCAG	7-Los Angeles, City of-10 [§]	Los Angeles	Mission Mile: Sepulveda Visioning for a Safe and Active Community	\$ 49,900	\$ 39,670	\$ 4,958	\$ -	\$ 2,125	\$ 32,587	\$ 4,958	\$ 2,125	\$ -	\$ -	\$ -	\$ 32,587	\$ -	Infrastructure - Large	x		91	100
SCAG	12-Brea, City of-1	Orange	Tracks at Brea - Gap Closure	\$ 14,046	\$ 1,787	\$ -	\$ 1,787	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,787	\$ -	\$ -	\$ -	Infrastructure - Large	x		85	105
SCAG	12-Orange County Transportation Authority (OCTA)-1 [§]	Orange	Garden Grove-Santa Ana Rails-to-Trails Gap Closure	\$ 42,397	\$ 3,000	\$ 3,000	\$ -	\$ -	\$ -	\$ 3,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	Infrastructure - Large	x		87	107
SCAG	12-Orange County-1	Orange	OC Loop Coyote Creek Bikeway (Segment O)	\$ 6,605	\$ 4,644	\$ -	\$ -	\$ 4,644	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 4,644	\$ -	Infrastructure - Medium	x		91	111
SCAG	12-Santa Ana, City of-3	Orange	Raitt Street Protected and Buffered Bike Lane	\$ 5,499	\$ 5,499	\$ 81	\$ 808	\$ 4,610	\$ -	\$ 81	\$ 808	\$ -	\$ -	\$ -	\$ 4,610	\$ -	Infrastructure - Medium	x		90	110
SCAG	8-Cathedral City, City of-1	Riverside	Downtown Cathedral City Connectors: Gap Closure & Complete Streets Improvement	\$ 5,566	\$ 4,383	\$ -	\$ 4,383	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 4,383	\$ -	Infrastructure - Medium	x		90	110
SCAG	8-Desert Hot Springs, City of-1	Riverside	Palm Drive Improvements	\$ 4,905	\$ 3,700	\$ 3,700	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 3,700	\$ -	Infrastructure - Medium	x		86.5	106.5
SCAG	8-Eastvale, City of-1	Riverside	SouthEast Eastvale SRTS Equitable Access Project	\$ 1,420	\$1,420 \$0	\$150 \$0	\$1,270 \$0	\$ —	\$ —	\$ —	\$150 \$0	\$ —	\$ —	\$ —	\$1,270 \$0	\$ —	Infrastructure - Small			87	404
SCAG	8-Riverside County-1	Riverside	Safe Routes for All - Hemet	\$ 636	\$ 348	\$ -	\$ 348	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 348	Non-Infrastructure	x	x	91	91
SCAG	8-Wildomar, City of-1	Riverside	Bundy Canyon Active Transportation Corridor	\$ 3,990	\$ 1,454	\$ 1,377	\$ 77	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,377	\$ 77	Infrastructure + NI - Medium			79	99
SCAG	8-Apple Valley, Town of-1	San Bernardino	Yucca Loma Elementary School Safe Routes to School Phase 2	\$ 986	\$ 838	\$ 191	\$ 647	\$ -	\$ -	\$ -	\$ 85	\$ -	\$ 106	\$ -	\$ 647	\$ -	Infrastructure - Small	x	x	87	107
SCAG	8-Barstow, City of-1	San Bernardino	Pedestrian, Bicycle, and Safe Routes to Schools (SR2S), Barstow	\$ 6,902	\$ 6,406	\$ -	\$ -	\$ -	\$ 6,406	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 6,406	\$ -	Infrastructure + NI - Medium	x	x	86	106
SCAG	8-Fontana, City of-4	San Bernardino	Date Elementary School Street Improvements Project	\$ 1,808	\$ 1,808	\$ 71	\$ 128	\$ -	\$ 1,609	\$ 71	\$ 128	\$ -	\$ -	\$ -	\$ 1,591	\$ 18	Infrastructure + NI - Small	x	x	88	108
SCAG	8-San Bernardino County-1	San Bernardino	Santa Ana River Trail - Phase III	\$ 6,880	\$ 1,105	\$ 1,105	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,105	\$ -	Infrastructure - Medium	x		87	107
SCAG	7-Southern California Association of Governments-2	Various	Sustainable Communities Program	\$ 4,670	\$ 4,670	\$ 4,670	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 4,670	Non-Infrastructure	x	x	N/A	N/A
SCAG	7-Oxnard, City of-1	Ventura	SRTS Safety and Enhancements Project	\$ 1,981	\$ 1,981	\$ 202	\$ 1,779	\$ -	\$ -	\$ -	\$ 202	\$ -	\$ -	\$ -	\$ 1,480	\$ 299	Infrastructure + NI - Small	x	x	86	101
SCAG	7-Oxnard, City of-2	Ventura	4th Street Mobility Improvements	\$ 6,900	\$ 650	\$ 650	\$ -	\$ -	\$ -	\$ -	\$ 650	\$ -	\$ -	\$ -	\$ -	\$ -	Infrastructure - Medium	x		75	85
SCAG	7-Ventura County-3	Ventura	Santa Rosa Road Bike Lane Improvement and Pedestrian Project (SRRBLP)	\$ 1,103	\$ 330	\$ -	\$ -	\$ 330	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 330	\$ -	Infrastructure - Small			67	82
SCAG	7-Ventura, City of-1	Ventura	Cabrillo Segment Multi-Use Path Gap Completion	\$ 1,008	\$ 1,008	\$ 183	\$ -	\$ 825	\$ -	\$ 68	\$ 110	\$ -	\$ 5	\$ -	\$ 825	\$ -	Infrastructure - Small	x		90	100
StanCOG	10-Stanislau County-1	Stanislau	Robertson Road Elementary Safe Crossing and Active Transportation Connectivity Project	\$ 1,997	\$ 1,609	\$ -	\$ -	\$ -	\$ 1,609	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,609	\$ -	Infrastructure - Small	x	x	89.5	89.5
StanCOG	10-Waterford, City of-1	Stanislau	Waterford Safe Routes to School Project - Yosemite Boulevard	\$ 946	\$ 946	\$ 15	\$ 128	\$ 803	\$ -	\$ 15	\$ 40	\$ -	\$ 88	\$ -	\$ 803	\$ -	Infrastructure - Small	x	x	81	81
TCAG	6-California Department of Transportation-7	Tulare	Ivanhoe Safe Routes to School (SR 216)	\$ 1,788	\$ 1,070	\$ 120	\$ 181	\$ -	\$ 769	\$ 120	\$ 90	\$ 22	\$ 69	\$ 165	\$ 604	\$ -	Infrastructure - Small	x	x	69	96
TCAG	6-Tulare County-3	Tulare	Tipton Sidewalk Improvements Project	\$ 3,430	\$ 1,218	\$ -	\$ -	\$ -	\$ 1,218	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,218	\$ -	Infrastructure - Medium	x	x	73	94.33
TMPO	3-Placer County-1	Placer	Kings Beach Western Approach Project	\$ 8,330	\$ 750	\$ -	\$ 260	\$ 490	\$ -	\$ -	\$ -	\$ -	\$ 260	\$ -	\$ 490	\$ -	Infrastructure - Large	x		81	81
SJCOG	10-Stockton-5	San Joaquin	Greater Downtown Bike & Ped Connectivity	\$ 1,998	\$ 1,748	\$ 75	\$ 215	\$ 1,458	\$ -	\$ 75	\$ 215	\$ -	\$ -	\$ -	\$ 1,458	\$ -	Infrastructure - Small	x		84	164.14
SJCOG	10-Stockton-6	San Joaquin	Downtown East-West Connection	\$ 1,999	\$ 1,799	\$ 110	\$ 240	\$ 1,449	\$ -	\$ 110	\$ 240	\$ -	\$ -	\$ -	\$ 1,449	\$ -	Infrastructure - Small	x		75	149.43
				\$ 493,783	\$ 174,887																

*Regional scores and ranks are on various scales (and not necessarily out of 100). Individual scoring systems are outlined in each MPO's guidelines.

§Project requires a baseline agreement. Please see the SB 1 Accountability and Transparency Guidelines for more information.

2023 Active Transportation Program
Metropolitan Planning Organization Component
Amended Program of Projects
Resolution G-25-48, Amending Resolution G-23-49
(\$1,000's)

Reference Number 4.27
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Attachment B

Amendments are shown in bold and strikethrough and highlighted in yellow

MPO	County	Application ID	Project Title	Total Project Cost	ATP Funding	FY 23-24	FY 24-25	FY 25-26	FY 26-27	PA&ED	PS&E	ROW	CON	CON-NI	Project Type	DAC	SRTS	State Score	Regional Score or Rank*
FCOG	Fresno	6-Fresno, City of-3	Downtown Neighborhood Safe Schools Crossing	\$ 1,636	\$ 1,636	\$ 148	\$ 7	\$ 1,472	\$ 9	\$ 3	\$ 145	\$ 7	\$ 1,472	\$ 9	Small Infrastructure + Non-Infrastructure	X	X	86	84
FCOG	Fresno	6-Fresno County-3	Calwa Sidewalk Project	\$ 3,429	\$ 2,697	\$ 39	\$ 314	\$ -	\$ 2,344	\$ 39	\$ 314	\$ -	\$ 2,344	\$ -	Small Infrastructure	X	X	83	82
FCOG	Fresno	6-Parlier, City of-1	City of Parlier Limitless Lane Network	\$ 3,008	\$ 3,008	\$ 15	\$ 345	\$ 2,648	\$ -	\$ 15	\$ 345	\$ -	\$ 2,648	\$ -	Small Infrastructure	X	X	89	82
FCOG	Fresno	6-Parlier, City of-2	City of Parlier Schools Corridor Active Transportation Improvements Project	\$ 3,000	\$ 3,000	\$ 270	\$ 408	\$ 2,322	\$ -	\$ 10	\$ 408	\$ -	\$ 2,322	\$ 260	Small Infrastructure + Non-Infrastructure	X	X	N/A	81
FCOG	Fresno	6-Coalinga, City of-1	Coalinga Perimeter Trail Interconnect Pacific South and Gregory North	\$ 2,016	\$ 1,774	\$ 117	\$ -	\$ 197	\$ 1,460	\$ 117	\$ 91	\$ 106	\$ 1,460	\$ -	Small Infrastructure	X		83	80
FCOG	Fresno	6-Fowler, City of-2	Fremont Elementary/ Marshall Elementary/Fowler High Safe Routes to School	\$ 600	\$ 600	\$ 5	\$ 50	\$ 545	\$ -	\$ 5	\$ 50	\$ -	\$ 545	\$ -	Small Infrastructure	X	X	71	78
FCOG	Fresno	6-Selma, City of-2	Selma Branch Canal Parkway Project	\$ 3,500	\$ 3,098	\$ 13	\$ 384	\$ -	\$ 2,701	\$ 13	\$ 384	\$ -	\$ 2,701	\$ -	Small Infrastructure	X	X	N/A	76
FCOG	Fresno	6-Fresno County-2	Tranquility Sidewalk Project	\$ 4,608	\$ 458	\$ 79	\$ 379	\$ -	\$ -	\$ 79	\$ 379	\$ -	\$ -	\$ -	Medium Infrastructure	X	X	74	74
FCOG	Fresno	6-Fresno County-5	Herndon-Barstow Elementary High Intensity Activated Crosswalk	\$ 801	\$ 603	\$ 107	\$ 496	\$ -	\$ -	\$ -	\$ 84	\$ 23	\$ 496	\$ -	Small Infrastructure	X	X	N/A	74
FCOG	Fresno	6-Huron, City of-1	Citywide Sidewalk Improvements	\$ 1,741	\$ 1,741	\$ 209	\$ 1,532	\$ -	\$ -	\$ 5	\$ 204	\$ -	\$ 1,532	\$ -	Small Infrastructure	X		69	71
KCOG	Kern	9-Tehachapi, City of-1	Valley Boulevard and Mill Street Gap Closure Project	\$ 3,266	\$ 3,266	\$ 65	\$ 315	\$ 200	\$ 2,686	\$ 65	\$ 315	\$ 200	\$ 2,686	\$ -	Small Infrastructure	X		87	87
KCOG	Kern	6-Kern County - D6-4	Kern River Parkway Multi-use Path Safety & Connectivity Project	\$ 8,035	\$ 6,800	\$ -	\$ 900	\$ -	\$ 5,900	\$ -	\$ 900	\$ -	\$ 5,900	\$ -	Medium Infrastructure	X		81	81
KCOG	Kern	6-Kern County - D6-5	Safe Route To School (SRTS) ADA Crosswalk Safety	\$ 2,342	\$ 1,760	\$ -	\$ -	\$ -	\$ 1,760	\$ -	\$ -	\$ -	\$ 1,760	\$ -	Small Infrastructure	X	X	78	78
KCOG	Kern	6-Wasco, City of-1	Central Avenue Class I & Class II Bicycle Trails	\$ 660	\$ 660	\$ 5	\$ 71	\$ 584	\$ -	\$ 5	\$ 71	\$ -	\$ 584	\$ -	Small Infrastructure	X		73	73
KCOG	Kern	6-Delano, City of-1	ATP-6 SRTS Sidewalk Gap and Crosswalk Improvement Project	\$ 703	\$ 703	\$ -	\$ 75	\$ 628	\$ -	\$ -	\$ 75	\$ -	\$ 628	\$ -	Small Infrastructure	X	X	71	71
KCOG	Kern	6-Taft, City of-1	10th St & San Emidio St - Intersection Safety Improvements	\$ 455	\$ 455	\$ -	\$ 47	\$ 408	\$ -	\$ 5	\$ 42	\$ -	\$ 408	\$ -	Small Infrastructure	X		70	70
KCOG	Kern	6-Bakersfield, City of-3	H Street Corridor (SR-204 to Hwy 58)	\$ 8,454	\$ 3,154	\$ -	\$ 3,154	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 3,154	\$ -	Medium Infrastructure	X	X	66	66
SACOG	Sacramento	3-Elk Grove, City of-1	Laguna Creek Inter-Regional Trail SR 99 Overcrossing and Gap Closure	\$ 10,900	\$ 6,874	\$ -	\$ -	\$ -	\$ 6,874	\$ -	\$ -	\$ -	\$ 6,874	\$ -	Large Infrastructure			66	83
SACOG	Sacramento	3-Citrus Heights, City of-3	Arcade Cripple Creek Extension	\$ 8,084	\$ 7,155	\$ 500	\$ -	\$ 806	\$ 5,849	\$ 500	\$ 600	\$ 206	\$ 5,849	\$ -	Medium Infrastructure	X		N/A	81
SACOG	Placer	3-Roseville, City of-1	Dry Creek Greenway East Multi-Use Trail, Phase 2	\$ 8,942	\$ 6,063	\$ -	\$ -	\$ -	\$ 6,063	\$ -	\$ -	\$ -	\$ 5,848	\$ 215	Medium Infrastructure + Non-Infrastructure			66	81
SACOG	El Dorado	3-El Dorado County-2	El Dorado Trail / Missouri Flat Road Bicycle/Pedestrian Overcrossing	\$ 5,850	\$ 3,271	\$ 3,271	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 3,271	\$ -	Medium Infrastructure			80	79
SACOG	Sacramento	3- Sacramento County Regional Parks-1	Dry Creek Parkway Trail	\$ 8,696	\$ 7,704	\$ -	\$ -	\$ 975	\$ 6,729	\$ -	\$ 975	\$ -	\$ 6,729	\$ -	Medium Infrastructure			N/A	78
SACOG	Sacramento	3-Sacramento County-2	Bell Street Safe Routes to School	\$ 9,949	\$ 8,808	\$ 270	\$ 2,102	\$ -	\$ 6,436	\$ 270	\$ 340	\$ 1,762	\$ 6,304	\$ 132	Medium Infrastructure + Non-Infrastructure	X	X	82	78
SACOG	Sacramento	3-Sacramento, City of-3	9th Street Separated Bikeway Project	\$ 2,899	\$ 2,564	\$ 132	\$ -	\$ 325	\$ 2,107	\$ 132	\$ 325	\$ -	\$ 2,107	\$ -	Small Infrastructure	X		N/A	77
SACOG	Yolo	3-West Sacramento, City of-2	West Capitol Avenue Regional Connection Bicyclist and Pedestrian Safety Improvements	\$ 909	\$ 735	\$ -	\$ 735	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 735	\$ -	Small Infrastructure	X		N/A	77
SACOG	Sacramento	3-Folsom, City of-1	Folsom-Placerville Rail Trail Gap Closure Project	\$ 3,048	\$ 1,700	\$ 200	\$ -	\$ 1,500	\$ -	\$ -	\$ 200	\$ -	\$ 1,500	\$ -	Small Infrastructure			84	76
SACOG	Yolo	3-West Sacramento, City of-3	North 5th Street Complete Streets & Connectivity Project	\$ 3,536	\$ 638	\$ 89	\$ -	\$ 549	\$ -	\$ 89	\$ 310	\$ 239	\$ -	\$ -	Medium Infrastructure	X		N/A	76
SANDAG	San Diego	11-San Diego Association of Governments (SANDAG)-3	Bayshore to Imperial Bikeway: Connecting Regional Bikeways	\$ 68,596	\$ 4,614	\$ 4,614	\$ -	\$ -	\$ -	\$ 4,614	\$ -	\$ -	\$ -	\$ -	Large Infrastructure	X	X	87	1
SANDAG	San Diego	11-National City, City of-5	Bayshore Bikeway Segment 5	\$ 9,588	\$ 2,072	\$ 2,072	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 2,072	\$ -	Medium Infrastructure	X		N/A	2
SANDAG	San Diego	11-Chula Vista, City of-1	F Street Promenade Phase 1, from Bay Boulevard to Broadway	\$ 18,845	\$ 9,762	\$ 78	\$ -	\$ 1,295	\$ 8,389	\$ 78	\$ 1,295	\$ -	\$ 8,389	\$ -	Large Infrastructure	X	X	75	3
SANDAG	San Diego	11-National City, City of-6	8th Street Bicycle and Pedestrian Rail Crossing Enhancements	\$ 6,373	\$ 2,248	\$ 600	\$ -	\$ 1,648	\$ -	\$ 100	\$ 370	\$ 130	\$ 1,648	\$ -	Medium Infrastructure	X		N/A	4
SANDAG	San Diego	11-San Diego Association of Governments (SANDAG)-5	Pacific Highway Coastal Rail Trail Airport Connection (PACTAC)	\$ 88,131	\$ 3,818	\$ 3,818	\$ -	\$ -	\$ -	\$ 3,818	\$ -	\$ -	\$ -	\$ -	Large Infrastructure	X		76	6

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MPO	County	Application ID	Project Title	Total Project Cost	ATP Funding	FY 23-24	FY 24-25	FY 25-26	FY 26-27	PA&ED	PS&E	ROW	CON	CON-NI	Project Type	DAC	SRTS	State Score	Regional Score or Rank*
SANDAG	San Diego	11-El Cajon, City of-1	Main Street Green Street Phase 2 Roundabout	\$ 6,800	\$ 5,984	\$ 575	\$ -	\$ 5,409	\$ -	\$ 3	\$ 572	\$ -	\$ 5,409	\$ -	Medium Infrastructure	X		67	9
SANDAG	San Diego	11-San Diego, City of-6	Accessibility Safety Plan for Individuals with Disabilities in Disadvantaged Communities	\$ 1,500	\$ 1,239	\$ 1,239	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,239	Plan	X		67	10
SANDAG	San Diego	11-San Diego, City of-5	San Diego CicloSDias Pilot	\$ 500	\$ 500	\$ 500	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 500	Non-Infrastructure	X		61	11
SANDAG	San Diego	11-San Diego Association of Governments (SANDAG)-6	Bridging the North Park/Mid-City Gap: Robinson Bikeway	\$ 9,530	\$ 5,172	\$ 546	\$ 4,626	\$ -	\$ -	\$ -	\$ -	\$ 546	\$ 4,626	\$ -	Medium Infrastructure	X		N/A	12
SANDAG	San Diego	11-Lemon Grove, City of-3	Connect Main Street Phases 2 and 3	\$ 4,891	\$ 4,890	\$ 374	\$ -	\$ 4,516	\$ -	\$ -	\$ 374	\$ -	\$ 4,516	\$ -	Medium Infrastructure			N/A	13
SANDAG	San Diego	11-Lemon Grove, City of-1	Connect Main Street Phases 4 thru 6	\$ 8,085	\$ 8,004	\$ 115	\$ 1,194	\$ -	\$ 6,695	\$ 115	\$ 1,194	\$ -	\$ 6,695	\$ -	Medium Infrastructure	X		76	14
SANDAG	San Diego	11-San Diego Association of Governments (SANDAG)-7	Bayshore Bikeway Barrio Logan Phase 2: Intersection Safety Improvements	\$ 9,345	\$ 6,344	\$ 6,344	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 6,344	\$ -	Medium Infrastructure	X		N/A	15
SANDAG	San Diego	11-Chula Vista, City of-2	Bayshore Bikeway Segment 6A	\$ 3,810	\$ 3,010	\$ 100	\$ 150	\$ 2,760	\$ -	\$ 100	\$ 150	\$ -	\$ 2,760	\$ -	Medium Infrastructure	X		78	17
SANDAG	San Diego	11-National City, City of-2	22nd Street Separated Bikeway	\$ 3,290	\$ 3,288	\$ 148	\$ 380	\$ -	\$ 2,760	\$ 148	\$ 380	\$ -	\$ 2,760	\$ -	Small Infrastructure	X		87	19
SANDAG	San Diego	11-San Diego Association of Governments (SANDAG)-4	Uptown Phase 4: Mission Hills to Old Town Bikeway	\$ 8,800	\$ 982	\$ 339	\$ 464	\$ 179	\$ -	\$ 339	\$ 366	\$ 98	\$ 179	\$ -	Medium Infrastructure	X	X	83	21
SCAG	Imperial	11-Imperial County Transportation Commission-1	Pedestrian Improvementsfor the Calexico Intermodal Transportation Center	\$ 1,173	\$ 1,073	\$ -	\$ 1,073	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,073	\$ -	Small Infrastructure	X		60	80
SCAG	Imperial	11-El Centro, City of-1	City of El Centro Pedestrian Improvement Project	\$ 1,300	\$ 1,200	\$ -	\$ -	\$ 1,200	\$ -	\$ -	\$ -	\$ -	\$ 1,200	\$ -	Small Infrastructure	X		56	76
SCAG	Imperial	11-Calipatria, City of-1	City of Calipatria - Bonita Place Pedestrian Safety Project	\$ 997	\$ 997	\$ 88	\$ 909	\$ -	\$ -	\$ 20	\$ 68	\$ -	\$ 909	\$ -	Small Infrastructure	X		48	68
SCAG	Los Angeles	7-Los Angeles, City of-8	Boyle Heights Community Connectivity Project	\$ 37,725	\$ 32,019	\$ 3,395	\$ -	\$ 2,886	\$ 25,738	\$ 3,395	\$ 2,037	\$ 849	\$ 25,738	\$ -	Large Infrastructure	X	X	89	99
SCAG	Los Angeles	7-Pasadena, City of-1	North Lake Avenue Pedestrian and Safety Enhancement Project	\$ 10,116	\$ 9,938	\$ 120	\$ 702	\$ -	\$ 9,116	\$ 120	\$ 672	\$ 30	\$ 9,116	\$ -	Large Infrastructure	X		88	98
SCAG	Los Angeles	7-Huntington Park, City of-1	Huntington Park's Safe Routes for Students and Seniors	\$ 4,761	\$ 4,261	\$ 55	\$ 320	\$ 3,886	\$ -	\$ 55	\$ 320	\$ -	\$ 3,886	\$ -	Medium Infrastructure	X	X	87	97
SCAG	Los Angeles	7-San Gabriel Valley Council of Governments-1	Pomona Safe and Active: Bike/Ped Safety Improvements & Gap Closure	\$ 9,998	\$ 5,976	\$ 418	\$ 5,558	\$ -	\$ -	\$ -	\$ 299	\$ 119	\$ 5,558	\$ -	Medium Infrastructure	X		87	97
SCAG	Los Angeles	7-California State Polytechnic University, Pomona (Cal Poly Pomona)-1	Cal Poly Pomona Campuswide Active Transportation Plan	\$ 299	\$ 299	\$ -	\$ 299	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 299	Plan	X	X	87	96
SCAG	Los Angeles	7-Pico Rivera, City of-1	Pico Rivera Active Transportation Master Plan	\$ 411	\$ 411	\$ 411	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 411	Plan	X		85	95
SCAG	Los Angeles	7-Artesia, City of-1	Pioneer Boulevard Improvements Project, City of Artesia	\$ 3,549	\$2,755 \$0	\$116 \$0	\$306 \$0	\$2,333 \$0	\$ -	\$116 \$0	\$306 \$0	\$ -	\$2,333 \$0	\$ -	Medium Infrastructure	X	X	87	94
SCAG	Los Angeles	7-Los Angeles County-4	Rosewood/West Rancho Dominguez Vision Zero Traffic Safety Enhancement Project	\$ 25,163	\$ 10,730	\$ 766	\$ 1,150	\$ 8,814		\$ 766	\$ 1,150	\$ -	\$ 8,814	\$ -	Large Infrastructure	X		89	94
SCAG	Los Angeles	7-Paramount, City of-1	West Paramount Utility Easement Multi-Use Path - Phase 1	\$ 9,661	\$ 9,661	\$ 10	\$ 765	\$ 8,886	\$ -	\$ 10	\$ 750	\$ 15	\$ 8,886	\$ -	Medium Infrastructure	X		85	94
SCAG	Los Angeles	7-Los Angeles County-5	Walnut Park Pedestrian Plan Implementation	\$ 8,395	\$ 2,446	\$ 175	\$ 262	\$ 2,009	\$ -	\$ 175	\$ 262	\$ -	\$ 2,009	\$ -	Medium Infrastructure	X		83	93
SCAG	Los Angeles	7-Los Angeles County-6	Unincorporated Los Angeles County Safe Routes to School Plan	\$ 750	\$ 750	\$ 750	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 750	Plan	X	X	83	93
SCAG	Los Angeles	7-Los Angeles, City of-10	SRTS Center City Schools Neighborhood Safety & Climate Resilience Project	\$ 36,238	\$ 30,766	\$ 1,099	\$ -	\$ 4,395	\$ 25,272	\$ 1,099	\$ 4,395	\$ -	\$ 25,272	\$ -	Large Infrastructure	X	X	83	93
SCAG	Los Angeles	7-Glendale, City of-2	Phase 1 of Citywide Pedestrian Master Plan	\$ 10,000	\$ 9,000	\$ 9,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 9,000	\$ -	Medium Infrastructure	X	X	83	92
SCAG	Los Angeles	7-Lancaster, City of-1	City of Lancaster 5th Street Corridor School Connections Project	\$ 8,332	\$ 6,655	\$ 471	\$ 711	\$ -	\$ 5,473	\$ 471	\$ 711	\$ -	\$ 5,473	\$ -	Medium Infrastructure	X	X	86	91
SCAG	Los Angeles	7-South Gate, City of-2	WSAB LRT Stations First-Last Mile Bikeway Safety and Access Project	\$ 3,375	\$ 3,375	\$ -	\$ 3,375	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 3,375	\$ -	Small Infrastructure	X	X	81	91
SCAG	Los Angeles	7-West Covina, City of-1	West Covina Safe Routes to School & Pedestrian Safety Project	\$ 2,561	\$ 2,561	\$ 10	\$ 180	\$ 2,371	\$ -	\$ 10	\$ 180	\$ -	\$ 2,371	\$ -	Small Infrastructure	X	X	88	91
SCAG	Los Angeles	7-Carson, City of-1	City of Carson Master Bicycle Plan	\$ 897	\$ 897	\$ 897	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 897	Plan	X		80	90
SCAG	Los Angeles	7-Los Angeles County-10	Pedestrian Plans for Five High-Collision Disadvantaged Communities in LA County	\$ 1,968	\$ 1,968	\$ -	\$ 1,968	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,968	Plan	X		80	90
SCAG	Los Angeles	7-Santa Monica, City of-1	Wilshire Active Transportation Safety Project	\$ 5,957	\$ 4,765	\$ -	\$ 4,765	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 4,765	\$ -	Medium Infrastructure	X		82	89
SCAG	Los Angeles	7-South El Monte, City of-1	Merced Avenue Greenway	\$ 3,488	\$ 2,561	\$ -	\$ 2,561	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 2,561	\$ -	Small Infrastructure	X		83	88

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MPO	County	Application ID	Project Title	Total Project Cost	ATP Funding	FY 23-24	FY 24-25	FY 25-26	FY 26-27	PA&ED	PS&E	ROW	CON	CON-NI	Project Type	DAC	SRTS	State Score	Regional Score or Rank*
SCAG	Los Angeles	7-Avalon, City of-1	Tremont Five Corners School Safety Roundabouts	\$ 4,092	\$ 3,238	\$ 3,238	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 3,210	\$ 28	Medium Infrastructure + Non-Infrastructure	X	X	86	87
SCAG	Los Angeles	7-Diamond Bar, City of-1	Diamond Bar Boulevard Complete Streets Project	\$ 7,047	\$ 3,936	\$ -	\$ 3,936	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 3,936	\$ -	Medium Infrastructure			85	87
SCAG	Los Angeles	7-South Gate, City of-1	Tweedy Boulevard Complete Streets, Phase II	\$ 6,594	\$ 5,257	\$ -	\$ 5,257	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 5,257	\$ -	Medium Infrastructure	X	X	82	87
SCAG	Los Angeles	7-Downey, City of-1	South Downey Safe Routes to School Phase II Project	\$ 1,145	\$ 1,145	\$ 165	\$ 120	\$ 860	\$ -	\$ 65	\$ 120	\$ -	\$ 860	\$ 100	Small Infrastructure + Non-Infrastructure	X	X	83	85
SCAG	Los Angeles	7-Pomona, City of-1	San Jose Creek Multi-Use Bikeway in the City of Pomona	\$ 13,123	\$ 11,623	\$ -	\$ 11,623	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 11,305	\$ 318	Large Infrastructure + Non-Infrastructure	X		82	85
SCAG	Los Angeles	7-La Puente, City of-1	City of La Puente's Safe Routes for Students Improvement Project	\$ 4,389	\$ 4,384	\$ -	\$ 325	\$ 4,059	\$ -	\$ -	\$ 300	\$ 25	\$ 4,059	\$ -	Medium Infrastructure	X	X	79	82
SCAG	Los Angeles	7-Los Angeles County-1	Los Nietos Pedestrian Access Improvements	\$ 6,542	\$ 5,233	\$ 561	\$ -	\$ 374	\$ 4,298	\$ 561	\$ 374	\$ -	\$ 4,298	\$ -	Medium Infrastructure	X		78	81
SCAG	Los Angeles	7-Signal Hill, City of-1	E. Burnett Street Historical District Pedestrian and Bike Enhancement Project	\$ 2,894	\$ 2,785	\$ 192	\$ 306	\$ 2,287	\$ -	\$ 192	\$ 306	\$ -	\$ 2,287	\$ -	Small Infrastructure	X		79	81
SCAG	Los Angeles	7-Los Angeles County-9	Lennox Vision Zero Traffic Safety Enhancements	\$ 1,141	\$ 1,141	\$ 1,141	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,141	\$ -	Quick Build			N/A	80
SCAG	Los Angeles	7-Carson, City of-2	City of Carson City-wide Community Safety Improvements	\$ 3,467	\$ 3,451	\$ 30	\$ 175	\$ -	\$ 3,246	\$ 30	\$ 175	\$ -	\$ 3,246	\$ -	Small Infrastructure	X		75	78
SCAG	Los Angeles	7-Cudahy, City of-1	Salt Lake Avenue Pedestrian Accessibility Project	\$ 7,125	\$ 7,125	\$ 275	\$ 632	\$ 675	\$ 5,543	\$ 275	\$ 632	\$ 675	\$ 5,543	\$ -	Medium Infrastructure	X	X	68	78
SCAG	Los Angeles	7-Santa Clarita, City of-2	Orchard Village Road Protected Pedestrian and Bicycle Facility	\$ 764	\$ 764	\$ 764	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 764	\$ -	Quick Build			N/A	77
SCAG	Los Angeles	7-San Gabriel Valley Council of Governments-3	East San Gabriel Valley Sustainable Multimodal Improvement Project	\$ 64,829	\$ 100	\$ 100	\$ -	\$ -	\$ -	\$ 100	\$ -	\$ -	\$ -	\$ -	Large Infrastructure	X		58	66
SCAG	Los Angeles	7-Maywood, City of-1	Randolph Street Bike and Facilities Improvement Project	\$ 1,375	\$ 145	\$ -	\$ 15	\$ 130	\$ -	\$ 15	\$ 130	\$ -	\$ -	\$ -	Small Infrastructure	X		58	63
SCAG	Orange	12-Anaheim, City of-1	Rio Vista Safe Routes to School Project	\$ 1,312	\$ 1,312	\$ 40	\$ 293	\$ 979	\$ -	\$ 40	\$ 173	\$ 120	\$ 979	\$ -	Small Infrastructure	X	X	88	108
SCAG	Orange	12-Santa Ana, City of-16	Mendez Int Advance Learning Santiago ES Sierra Int SRTS	\$ 9,987	\$ 9,987	\$ 50	\$ 900	\$ -	\$ 9,037	\$ 50	\$ 900	\$ -	\$ 9,037	\$ -	Medium Infrastructure	X	X	88	105
SCAG	Orange	12-Santa Ana, City of-20	Memory Lane and Flower Street Bikeway	\$ 6,264	\$ 5,000	\$ -	\$ 450	\$ -	\$ 4,550	\$ -	\$ 450	\$ -	\$ 4,550	\$ -	Medium Infrastructure	X		85	105
SCAG	Orange	12-Santa Ana, City of-15	MacArthur Intermediate and Taft Elementary SRTS	\$ 4,900	\$ 4,900	\$ 50	\$ 650	\$ -	\$ 4,200	\$ 50	\$ 650	\$ -	\$ 4,200	\$ -	Medium Infrastructure	X	X	88	105
SCAG	Orange	12-Buena Park, City of-1	Dale / Whitaker Complete Streets Project	\$ 4,595	\$ 4,368	\$ 765	\$ 3,603	\$ -	\$ -	\$ 100	\$ 665	\$ 130	\$ 3,473	\$ -	Medium Infrastructure	X		81	101
SCAG	Orange	12-Santa Ana, City of-5	Jackson Elementary and Diamond Elementary SRTS	\$ 8,262	\$ 8,262	\$ 100	\$ 850	\$ -	\$ 7,312	\$ 100	\$ 850	\$ -	\$ 7,312	\$ -	Medium Infrastructure	X	X	84	101
SCAG	Orange	12-Santa Ana, City of-12	Madison ES Roosevelt-Walker Academy Century HS SRTS	\$ 9,990	\$ 9,990	\$ 50	\$ 1,100	\$ -	\$ 8,840	\$ 50	\$ 1,100	\$ -	\$ 8,840	\$ -	Medium Infrastructure	X	X	89	101
SCAG	Orange	12-Fullerton, City of-1	Bridging the Gap: Nutwood Avenue Bicycle and Pedestrian Mobility Enhancements	\$ 7,140	\$ 7,140	\$ -	\$ 535	\$ 6,605	\$ -	\$ 50	\$ 400	\$ 85	\$ 6,605	\$ -	Medium Infrastructure	X	X	80	100
SCAG	Orange	12-Anaheim, City of-2	Anaheim Pedestrian Crosswalk Safety Improvements Project	\$ 1,112	\$ 1,112	\$ 42	\$ 128	\$ -	\$ 942	\$ 42	\$ 128	\$ -	\$ 942	\$ -	Small Infrastructure	X		82	99
SCAG	Orange	12-Orange County Transportation Authority (OCTA)-1	Next STEP (Safe Travels Education Program)	\$ 850	\$ 850	\$ 850	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 850	Non-Infrastructure	X		67	82
SCAG	Orange	12-Santa Ana, City of-1	Santa Ana High School and Heninger Elementary School SRTS	\$ 8,222	\$ 8,222	\$ 120	\$ 1,200	\$ -	\$ 6,902	\$ 120	\$ 1,200	\$ -	\$ 6,902	\$ -	Medium Infrastructure	X	X	84	17
SCAG	Riverside	8-Riverside County-7	Riverside County Safe Routes for All City of Moreno Valley	\$ 442	\$ 442	\$ -	\$ 442	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 442	Non-Infrastructure	X	X	87	107
SCAG	Riverside	8-Desert Hot Springs, City of-1	Hacienda Avenue East SRTS Phase II Improvement Project	\$ 9,922	\$ 8,422	\$ -	\$ -	\$ 8,422	\$ -	\$ -	\$ -	\$ -	\$ 8,422	\$ -	Medium Infrastructure	X	X	89	102
SCAG	Riverside	8-Menifee, City of-1	Harvest Valley Elementary Safe Routes to School	\$ 5,230	\$ 4,354	\$ 124	\$ 1,483	\$ 2,747	\$ -	\$ 124	\$ 417	\$ 1,066	\$ 2,747	\$ -	Medium Infrastructure	X	X	89	96
SCAG	Riverside	8-Desert Hot Springs, City of-2	Palm Drive Improvements I 10 to Camino Aventura	\$ 9,973	\$ 8,975	\$ 8,975	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 8,975	\$ -	Medium Infrastructure	X		76	96
SCAG	Riverside	8-Riverside, City of-1	Five Points Neighborhood Pedestrian Safety Improvements	\$ 7,416	\$ 6,525	\$ -	\$ -	\$ 1,134	\$ 5,391	\$ -	\$ -	\$ 1,134	\$ 5,391	\$ -	Medium Infrastructure	X	X	88	91
SCAG	Riverside	8-Moreno Valley, City of-1	ADA Curb Ramps Remediation Project	\$ 1,523	\$ 1,523	\$ 100	\$ 170	\$ 1,253	\$ -	\$ 100	\$ 170	\$ -	\$ 1,253	\$ -	Small Infrastructure	X	X	84	91
SCAG	Riverside	8-Riverside, City of-4	Riverside Civil Rights Walk	\$ 3,216	\$ 3,216	\$ 200	\$ 75	\$ 2,941	\$ -	\$ 200	\$ 75	\$ -	\$ 2,941	\$ -	Small Infrastructure	X		80	87
SCAG	Riverside	8-Riverside, City of-2	Mitchell Avenue Sidepath Gap Closure	\$ 7,465	\$ 6,756	\$ -	\$ 245	\$ 2,516	\$ 3,995	\$ -	\$ 245	\$ 2,516	\$ 3,995	\$ -	Medium Infrastructure	X		83	86

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SCAG	Riverside	8-Meniffee, City of-2	Romoland Elementary Safe Routes to School	\$ 5,561	\$ 4,880	\$ 131	\$ 527	\$ 4,222	\$ -	\$ 131	\$ 527	\$ -	\$ 4,222	\$ -	Medium Infrastructure	X	X	77	84
SCAG	Riverside	8-Palm Desert, City of-1	Palm Desert Bicycle/Low Speed Electric Vehicle Route	\$ 3,488	\$1,301 \$0	\$1,301 \$0	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$1,301 \$0	\$ -	Small Infrastructure + Non-Infrastructure	X		65	81
SCAG	San Bernardino	8-Montclair, City of-1	Montclair Safe Routes to School Implementation Project	\$ 6,335	\$ 5,701	\$ 521	\$ 5,180	\$ -	\$ -	\$ 130	\$ 391	\$ -	\$ 5,104	\$ 76	Medium Infrastructure + Non-Infrastructure	X	X	89	109
SCAG	San Bernardino	8-Upland, City of-1	City of Upland Mobility Master Plan	\$ 300	\$ 300	\$ 300	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 300	Plan	X		84	104
SCAG	San Bernardino	8-San Bernardino County-2	Bloomington Area Schools Pedestrian Safety Improvements	\$ 3,383	\$ 3,383	\$ 302	\$ 581	\$ -	\$ 2,500	\$ 302	\$ 201	\$ 380	\$ 2,500	\$ -	Small Infrastructure	X		84	104
SCAG	San Bernardino	8-San Bernardino County-1	San Bernardino Valley Safe Routes to School Plan (Unincorporated Schools)	\$ 500	\$ 500	\$ 500	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 500	Plan	X	X	81	101
SCAG	San Bernardino	8-Apple Valley, Town of-1	Powhatan Road Complete Streets, Apple Valley	\$ 1,735	\$ 1,562	\$ 202	\$ 1,360	\$ -	\$ -	\$ -	\$ 202	\$ -	\$ 1,360	\$ -	Small Infrastructure	X		80	100
SCAG	San Bernardino	8-Twenty-nine Palms, City of-1	Sullivan Road/Hatch Road Side Path/Bicycle Lanes	\$ 3,480	\$ 3,480	\$ 60	\$ 420	\$ 3,000	\$ -	\$ 60	\$ 220	\$ 200	\$ 3,000	\$ -	Small Infrastructure	X	X	78	98
SCAG	San Bernardino	8-Rialto, City of-1	Rialto Pacific Electric Trail Extension	\$ 7,822	\$ 7,037	\$ 185	\$ 725	\$ 6,127	\$ -	\$ 185	\$ 725	\$ -	\$ 5,922	\$ 205	Medium Infrastructure + Non-Infrastructure	X		75	95
SCAG	San Bernardino	8-Yucaipa, City of-1	15th Street (Avenue D to Yucaipa Blvd)	\$ 320	\$ 320	\$ 320	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 320	\$ -	Small Infrastructure	X		75	95
SCAG	San Bernardino	8-Adelanto, City of-1	Adelanto Safe Routes to School	\$ 9,121	\$ 9,121	\$ 375	\$ 875	\$ 7,871	\$ -	\$ 375	\$ 875	\$ -	\$ 7,871	\$ -	Medium Infrastructure + Non-Infrastructure	X	X	71	91
SCAG	San Bernardino	8-Needles, City of-1	City of Needles Schools and Seniors Sidewalk Project	\$ 1,658	\$ 1,565	\$ 54	\$ 275	\$ -	\$ 1,236	\$ 54	\$ 162	\$ 113	\$ 1,236	\$ -	Small Infrastructure	X	X	71	91
SCAG	San Bernardino	8-Barstow, City of-1	Pedestrian, Bicyclist, and Safety Improvements	\$ 4,140	\$ 4,140	\$ 622	\$ -	\$ 622	\$ 2,896	\$ 622	\$ 622	\$ -	\$ 2,896	\$ -	Medium Infrastructure	X		69	89
SCAG	San Bernardino	8-Fontana, City of-1	Foothill Boulevard Active Transportation Improvement	\$ 9,971	\$ 4,925	\$ -	\$ 4,925	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 4,925	\$ -	Medium Infrastructure	X		69	89
SCAG	Ventura	7-Thousand Oaks, City of-1	Lynn Road Bike Lanes and Pedestrian Improvements	\$ 6,852	\$ 2,602	\$ -	\$ -	\$ 2,602	\$ -	\$ -	\$ -	\$ -	\$ 2,602	\$ -	Small Infrastructure			86	101
SCAG	Ventura	7-Thousand Oaks, City of-2	Hillcrest Drive Class IV Bike lanes and Pedestrian Improvements	\$ 2,840	\$ 2,350	\$ -	\$ 2,350	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 2,350	\$ -	Small Infrastructure			86	101
SCAG	Ventura	7-Ventura, City of-1	Santa Paula Trail, East Ventura Station to e/o Wells Road	\$ 27,133	\$ 6,716	\$ 6,716	\$ -	\$ -	\$ -	\$ -	\$ 4,438	\$ 2,278	\$ -	\$ -	Large Infrastructure	X	X	78	96
SCAG	Ventura	7-Ventura County-2	Piru Pedestrian Improvement Project	\$ 3,450	\$ 3,450	\$ 200	\$ -	\$ 390	\$ 2,860	\$ 200	\$ 390	\$ -	\$ 2,860	\$ -	Small Infrastructure	X	X	55	73
SCAG	Ventura	7-Fillmore, City of-1	City of Fillmore Active Transportation Program Needs Assessment	\$ 70	\$70 \$0	\$70 \$0	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$70 \$0	Plan	X		55	70
SCAG	Ventura	7-Simi Valley, City of-1	Simi Valley Bicycle Master Plan	\$ 250	\$ 250	\$ 250	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 250	Plan	X		40	55
SCAG	Ventura	7-Simi Valley, City of-2	Arroyo Simi Greenway Phase 5 AP#2	\$ 1,310	\$ 751	\$ 92	\$ 659	\$ -	\$ -	\$ 14	\$ 72	\$ 6	\$ 659	\$ -	Small Infrastructure			29	42
SJCOG	San Joaquin	10-San Joaquin Regional Rail Commission-1	East Channel Street Streetscape and Connectivity Project	\$ 10,492	\$ 3,500	\$ 3,500	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 3,500	\$ -	Large Infrastructure	X		N/A	178
SJCOG	San Joaquin	10-San Joaquin County-2	Countywide Sidewalks Connectivity Plan	\$ 566	\$ 273	\$ 273	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 273	Plan	X		82	147
SJCOG	San Joaquin	10-Stockton, City of-4	Main and Market Complete Streets (Phase 1)	\$ 10,142	\$ 8,226	\$ 1,333	\$ 6,893	\$ -	\$ -	\$ -	\$ 1,333	\$ -	\$ 6,893	\$ -	Large Infrastructure	X	X	76	146
SJCOG	San Joaquin	10-Tracy, City of-1	East Schulte Safety and Multimodal Community Corridor - MacArthur Phase	\$ 1,712	\$ 1,712	\$ -	\$ 53	\$ 200	\$ 1,459	\$ 53	\$ 200	\$ -	\$ 1,459	\$ -	Medium Infrastructure	X		66	132
StanCOG	Stanislaus	10-Ceres, City of-2	Building on Active Transportation Connectivity and Access within the City	\$ 2,858	\$ 2,858	\$ -	\$ 256	\$ 2,602	\$ -	\$ -	\$ 256	\$ -	\$ 2,602	\$ -	Small Infrastructure	X		N/A	103
StanCOG	Stanislaus	10-Stanislaus County-1	Denair School Safe Crossing and Active Transportation Connectivity Project	\$ 3,498	\$ 1,978	\$ -	\$ -	\$ -	\$ 1,978	\$ -	\$ -	\$ -	\$ 1,978	\$ -	Small Infrastructure		X	N/A	99
StanCOG	Stanislaus	10-Waterford, City of-2	Waterford Tim Bell Road Pedestrian Improvements	\$ 1,993	\$ 1,993	\$ 15	\$ 170	\$ 1,808	\$ -	\$ 15	\$ 55	\$ 115	\$ 1,808	\$ -	Small Infrastructure	X		N/A	95
StanCOG	Stanislaus	10-Oakdale, City of-1	Southwest Downtown - Safe Routes to School Project	\$ 998	\$ 968	\$ 968	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 968	\$ -	Small Infrastructure	X	X	59	94
StanCOG	Stanislaus	10-Ceres, City of-1	Improving Pedestrian Safety on Central Ave and Hackett Road Corridor	\$ 2,612	\$ 2,495	\$ 80	\$ 850	\$ -	\$ 1,565	\$ 80	\$ 213	\$ 637	\$ 1,565	\$ -	Small Infrastructure	X	X	78	94
TCAG	Tulare	6-Woodlake, City of-1	West Sequoia Avenue Multi-Modal Improvements Project	\$ 2,922	\$ 2,532	\$ -	\$ 2,532	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 2,532	\$ -	Small Infrastructure	X	X	78	100
TCAG	Tulare	6-Tulare County-2	Poplar Pedestrian Connectivity Project	\$ 3,182	\$ 2,539	\$ -	\$ 485	\$ -	\$ 2,054	\$ -	\$ 200	\$ 285	\$ 2,054	\$ -	Small Infrastructure	X	X	88	95
TCAG	Tulare	6-Tulare County Association of Governments-1	Tule River Tribe Complete Streets and Two Pedestrian Bridges Project, Phase 1	\$ 2,981	\$ 2,981	\$ 168	\$ 397	\$ 2,416	\$ -	\$ 168	\$ 357	\$ -	\$ 2,416	\$ 40	Small Infrastructure + Non-Infrastructure	X	X	87	94

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TCAG	Tulare	6-Visalia, City of-1	Goshen - Visalia Corridor (GVC) Improvement Project, Phase 1	\$ 3,816	\$ 795	\$ -	\$ 795	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 795	\$ -	Medium Infrastructure	X	X	87	92
TMPO	El Dorado	3-South Lake Tahoe, City of-1	Pioneer Trail Pedestrian Improvement Project Phase II	\$ 4,980	\$ 1,200	\$ 1,200	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,200	\$ -	Medium Infrastructure	X	X	N/A	326
TMPO	El Dorado	3-El Dorado County-6	Apache Avenue Pedestrian Safety and Connectivity Project	\$ 3,502	\$ 1,701	\$ 1,701	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,701	\$ -	Medium Infrastructure		X	N/A	335
MTC	Alameda	4-Oakland, City of-1	Bancroft Avenue Greenway	\$ 34,675	\$ 29,311	\$ -	\$ 5,446	\$ -	\$ 23,865	\$ -	\$ 5,446	\$ -	\$ 23,865	\$ -	Large Infrastructure	X	X	88	102
MTC	Alameda	4-Alameda County-2	Mission Boulevard Safe and Complete Street for Active Transportation	\$ 32,683	\$ 25,000	\$ 25,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 25,000	\$ -	Large Infrastructure	X	X	87	101
MTC	Alameda	4-Alameda County Transportation Commission-1	East Bay Greenway Multimodal, Phase 1	\$ 120,947	\$ 19,500	\$ -	\$ 19,500	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 19,500	\$ -	Large Infrastructure	X		74	100
MTC	Alameda	4-Alameda County-3	San Lorenzo Creekway: Building Equitable Active Transportation in Alameda County	\$ 33,477	\$ 17,200	\$ -	\$ -	\$ -	\$ 17,200	\$ -	\$ -	\$ -	\$ 17,200	\$ -	Large Infrastructure + Non-Infrastructure	X	X	87	99
MTC	Alameda	4-Alameda County-4	Oakland Making Moves: Active Oakland Neighborhoods	\$ 1,000	\$ 999	\$ 999	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 999	Non-Infrastructure	X	X	88	98
MTC	Alameda	4-Berkeley, City of-2	Washington Elementary and Berkeley High SR2S Project	\$ 1,511	\$ 1,511	\$ 30	\$ 150	\$ 1,331	\$ -	\$ 30	\$ 150	\$ -	\$ 1,331	\$ -	Small Infrastructure	X	X	88	98
MTC	Marin	4-San Rafael, City of-2	Canal Neighborhood Active Transportation Enhancements Project	\$ 5,154	\$ 4,123	\$ 158	\$ 630	\$ -	\$ 3,335	\$ 158	\$ 630	\$ -	\$ 3,335	\$ -	Medium Infrastructure	X	X	76	97
MTC	Marin	4-San Rafael, City of-1	San Rafael Canal Crossing Project	\$ 23,525	\$ 3,925	\$ -	\$ 1,575	\$ -	\$ 2,350	\$ 1,575	\$ 2,350	\$ -	\$ -	\$ -	Large Infrastructure	X	X	72	97
MTC	Contra Costa	4-San Pablo, City of-1	Broadway-El Portal Safe Routes Project, City of San Pablo	\$ 9,143	\$ 7,248	\$ 12	\$ 1,295	\$ -	\$ 5,941	\$ 12	\$ 1,295	\$ -	\$ 5,941	\$ -	Medium Infrastructure + Non-Infrastructure	X	X	85	96
MTC	Alameda	4-Alameda County Transportation Commission-2	San Pablo Avenue Safety and Bus Bulbs Project	\$ 22,740	\$ 9,000	\$ -	\$ 9,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 9,000	\$ -	Large Infrastructure	X		81	93
MTC	Contra Costa	4-Concord, City of-1	Willow Pass Road Bikeway Connection Project	\$ 4,058	\$ 2,835	\$ -	\$ -	\$ 2,835	\$ -	\$ -	\$ -	\$ -	\$ 2,835	\$ -	Medium Infrastructure	X	X	81	93
MTC	Marin	4-Corte Madera, Town of-1	Central Marin Regional Pathways Gap Closure Project	\$ 2,075	\$ 1,500	\$ 1,500	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,500	\$ -	Small Infrastructure	X	X	76	93
MTC	Sonoma	4-Healdsburg, City of-1	Healdsburg Avenue Complete Streets Project	\$ 14,774	\$ 11,819	\$ -	\$ -	\$ 11,819	\$ -	\$ -	\$ -	\$ -	\$ 11,819	\$ -	Large Infrastructure	X		87	92
MTC	San Mateo	4-San Mateo County-1	Santa Cruz Avenue/Alameda de las Pulgas Complete Street Project	\$ 6,629	\$ 5,435	\$ 5,435	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 5,435	\$ -	Medium Infrastructure	X	X	83	91
MTC	Santa Clara	4-San Jose, City of-1	Story-Keyes Bikeway Project	\$ 41,098	\$ 3,656	\$ -	\$ -	\$ -	\$ 3,656	\$ -	\$ -	\$ -	\$ 3,656	\$ -	Large Infrastructure	X		81	91
				\$ 1,288,075	\$ 678,690														

*Regional scores and ranks are on various scales (and not necessarily out of 100). Individual scoring systems are outlined in each MPO's guidelines.

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.28, Action

Prepared By: Anja Aulenbacher
Associate Deputy Director

Published Date: June 13, 2025

Subject: Amendment to the 2025 Active Transportation Program Statewide and Small Urban and Rural Components, Resolution G-25-49, Amending Resolution G-24-76

Recommendation:

Staff recommends that the California Transportation Commission (Commission) approve an amendment to the 2025 Active Transportation Program with the following actions:

- Amend the Stanislaus Council of Governments' Church Street Mobility Enhancement Project programmed in the Statewide component to add \$3,258,000 to the construction phase in fiscal year 2028-29, for a total programmed amount of \$7,795,000.
- Amend the City of Visalia's Goshen Visalia Corridor Connection Project programmed in the Small Urban and Rural component to add \$31,672,000 to the construction phase in fiscal year 2028-29, and \$300,000 to the construction non-infrastructure phase in fiscal year 2025-26, for a total programmed amount of \$34,608,000.

Issue:

On December 5, 2024, the Commission adopted the 2025 Active Transportation Program Statewide and Small Urban and Rural components. At the time of program adoption, the Commission did not have enough programming capacity available to fully fund one project in the Statewide component and one project in the Small Urban and Rural component. Both projects were at the funding score threshold of their respective components.

The adopted program of projects for the Statewide component included \$84,350,000 for nine projects. Within the Statewide component, the Stanislaus Council of Governments received \$4,537,000 of its \$7,795,000 request for the Church Street Mobility Enhancement Project, leaving a \$3,258,000 funding shortfall. The adopted program of projects for the Small Urban and Rural component included \$16,870,000 for four projects. Within the Small Urban and Rural component, the City of Visalia received \$2,636,000 of its \$34,608,000 request for the

Goshen Visalia Corridor Connection Project, leaving a \$31,972,000 funding shortfall. At the time of programming, the Commission committed to working with the agencies to deliver the full projects.

After adoption of the 2025 Active Transportation Program Statewide and Small Urban and Rural components, Commission staff identified savings from canceled and lapsed projects or project phases. Therefore, sufficient programming capacity is now available to fully fund both projects. The recommended programming amendments are summarized below:

- **Stanislaus Council of Governments – Church Street Mobility Enhancement Project (Statewide component), in thousands:**

Programming	PA&ED	PS&E	ROW	CON	CON-NI	Total Award
Current Programming	\$321 FY 25-26	\$733 FY 26-27	\$240 FY 26-27	\$3,243 FY 28-29	\$0	\$4,537
Amended Programming	\$321 FY 25-26	\$733 FY 26-27	\$240 FY 26-27	\$6,501 FY 28-29	\$0	\$7,795
Proposed Change	None	None	None	+\$3,258	None	+\$3,258

- **City of Visalia – Goshen Visalia Corridor Connection Project (Small Urban and Rural component), in thousands:**

Programming	PA&ED	PS&E	ROW	CON	CON-NI	Total Award
Current Programming	\$0	\$0	\$0	\$2,636 FY 28-29	\$0	\$2,636
Amended Programming	\$0	\$0	\$0	\$34,308 FY 28-29	\$300 FY 25-26	\$34,608
Proposed Change	None	None	None	+\$31,672	+\$300	+\$31,972

Approval of this amendment will result in new programming totals of \$87,608,000 in the 2025 Active Transportation Program Statewide component and \$48,842,000 in the 2025 Active Transportation Program Small Urban and Rural component.

Background:

The Active Transportation Program was created by Senate Bill 99 (Committee on Budget and Fiscal Review, Chapter 359, Statutes of 2013) and Assembly Bill 101 (Committee on Budget, Chapter 354, Statutes of 2013) to encourage increased use of active modes of transportation, such as biking and walking. Senate Bill 1 (Beall, Chapter 5, Statutes of 2017) directs additional funding from the Road Maintenance and Rehabilitation Account to the Active Transportation Program. Along with the program's overall purpose of encouraging walking and biking, the program aims to increase the share of walking and biking trips, increase safety and mobility for

non-motorized users, help regional agencies achieve greenhouse gas reduction goals, enhance public health, ensure that disadvantaged communities fully share in program benefits, and provide a broad spectrum of projects to benefit many types of active transportation users.

The 2025 Active Transportation Program Guidelines, which the Commission adopted at its March 22, 2024 meeting, describe the policies, standards, criteria, and procedures for the program's development, adoption, and management. The Commission established the 2025 Active Transportation Program as a four-year program (fiscal years 2025-26 through 2028-29).

The Commission adopted the 2025 Active Transportation Program Statewide and Small Urban and Rural components on December 5, 2024.

Resolution G-25-49, Amending Resolution G-24-76

Be It Resolved, that the California Transportation Commission does hereby amend the 2025 Active Transportation Program to:

- Amend the Stanislaus Council of Governments' Church Street Mobility Enhancement Project programmed in the Statewide component to add \$3,258,000 to the construction phase in fiscal year 2028-29 for a total programmed amount of \$7,795,000.
- Amend the City of Visalia's Goshen Visalia Corridor Connection Project programmed in the Small Urban and Rural component to add \$31,672,000 to the construction phase in fiscal year 2028-29 and \$300,000 to the construction non-infrastructure phase in fiscal year 2025-26 for a total programmed amount of \$34,608,000.

Be It Further Resolved, that all other provisions stipulated in Resolution G-24-76 remain in effect.

Attachments:

- Attachment A: Amended 2025 Active Transportation Program Statewide Component Program of Projects
- Attachment B: Amended 2025 Active Transportation Program Small Urban and Rural Component Program of Projects

California Transportation Commission
2025 Active Transportation Program
Statewide Component
(\$1,000s)
Resolution G-25-49, Amending Resolution G-24-76

CTC Application ID	Project Title	County	Total Project Cost	ATP Fund Request	25-26	26-27	27-28	28-29	PA&ED	PS&E	RW	CON	CON-NI	Project Type	DAC	SRTS	Final Score
5-Salinas, City of-1	John Street and Williams Road Safe Routes to Schools Project and Program	Monterey	\$ 9,955	\$ 7,954	\$ 1,650	\$ 550	\$ 5,754	\$ -	\$ 150	\$ 500	\$ 50	\$ 5,754	\$ 1,500	Infrastructure + Non-Infrastructure - Medium	Yes	Yes	100
6-Madera, City of-1	Madera Citywide Safe Routes to School	Madera	\$ 7,756	\$ 6,201	\$ -	\$ -	\$ 6,201	\$ -	\$ -	\$ -	\$ -	\$ 6,201	\$ -	Infrastructure - Medium	Yes	Yes	99.5
6-Visalia, City of-2	Highland Community Connectivity	Tulare	\$ 7,194	\$ 5,470	\$ 520	\$ -	\$ -	\$ 4,950	\$ -	\$ -	\$ -	\$ 4,950	\$ 520	Infrastructure + Non-Infrastructure - Medium	Yes	Yes	98
3-Grass Valley, City of-1	Wolf Creek Community and Connectivity	Nevada	\$ 16,300	\$ 12,990	\$ 195	\$ 1,700	\$ -	\$ 11,095	\$ 195	\$ 1,350	\$ 350	\$ 11,095	\$ -	Infrastructure - Large	Yes	No	98
7-San Gabriel Valley Council of Governments-1	Safe Paths Pomona: At-Grade Pedestrian and Bike Safety	Los Angeles	\$ 26,158	\$ 20,162	\$ 8	\$ 2,001	\$ 18,153	\$ -	\$ 8	\$ 1,701	\$ 300	\$ 18,153	\$ -	Infrastructure - Large	Yes	Yes	97.5
7-Inglewood, City of-1	Westchester-Veterans Station Multimodal Connection	Los Angeles	\$ 9,013	\$ 7,656	\$ 1,539	\$ -	\$ 6,117	\$ -	\$ -	\$ -	\$ 1,539	\$ 6,117	\$ -	Infrastructure - Medium	Yes	No	97
6-Visalia, City of-4	Beyond Bike Lanes - Elevating Santa Fe Street	Tulare	\$ 14,424	\$ 11,390	\$ 520	\$ -	\$ -	\$ 10,870	\$ -	\$ -	\$ -	\$ 10,870	\$ 520	Infrastructure + Non-Infrastructure - Large	Yes	No	96.5
7-Los Angeles County-1	West Rancho Dominguez Walks: Providing Safer Access to Schools and Parks	Los Angeles	\$ 9,990	\$ 7,990	\$ 1,784	\$ -	\$ 6,206	\$ -	\$ 512	\$ 792	\$ 480	\$ 6,206	\$ -	Infrastructure - Medium	Yes	No	96
10-Stanislaus Council of Governments-1‡	Church Street Mobility Enhancement	Stanislaus	\$ 9,744	\$7,795 \$4,537	\$ 321	\$ 973	\$ -	\$ 6,501	\$ 321	\$ 733	\$ 240	\$ 6,501	\$ -	Infrastructure - Medium	Yes	Yes	96
			\$ 110,534	\$87,608 \$84,350													

Notes

‡Stanislaus Council of Governments requested \$7,795 for the Church Street Mobility Enhancement Project. However, only \$4,537 in programming capacity remained in the Statewide Component at the time of programming (December 5, 2024). Due to available savings in the ATP, the Commission is able to program the remaining request amount.

Abbreviations, Acronyms, and Initialisms
CON: Construction phase
DAC: Project benefits a disadvantaged community
NI: Non-Infrastructure
PA&ED: Project Approval & Environmental Document phase
PS&E: Plans, Specifications & Estimates phase
R/W: Right-of-way phase
R/W: Right-of-way phase
SRTS: Safe Routes to School project

California Transportation Commission
2025 Active Transportation Program
Small Urban and Rural Component
(\$1,000s)
Resolution G-25-49, Amending Resolution G-24-76

CTC Application ID	Project Title	County	Total Project Cost	ATP Fund Request	25-26	26-27	27-28	28-29	PA&ED	PS&E	RW	CON	CON-NI	Project Type	DAC	SRTS	Final Score
9-Big Pine Paiute Tribe of the Owens Valley-1	Big Pine Paiute Tribal Active Transportation Plan	Inyo	\$ 251	\$ 251	\$ 251	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 251	Plan	Yes	No	95
1-Mendocino County-1	Covelo/Round Valley Safe Routes to School	Mendocino	\$ 6,181	\$ 6,181	\$ 220	\$ 880	\$ -	\$ 5,081	\$ 220	\$ 880	\$ -	\$ 5,081	\$ -	Infrastructure - Medium	Yes	Yes	95
9-Inyo County-1	Connecting Tecopa: Bicycle and Pedestrian Safety Corridor	Inyo	\$ 9,877	\$ 7,802	\$ 791	\$ 1,632	\$ -	\$ 5,379	\$ 791	\$ 618	\$ 1,014	\$ 5,379	\$ -	Infrastructure - Medium	Yes	No	95
6-Visalia, City of-3‡	Goshen Visalia Corridor Connection Project	Tulare	\$ 43,300	\$34,608	\$ 300	\$ -	\$ -	\$ 34,308	\$ -	\$ -	\$ -	\$ 34,308	\$ 300	Infrastructure + Non-Infrastructure - Large	Yes	Yes	95
				\$48,842													
			\$ 59,609	\$16,870													

Notes

‡The City of Visalia requested \$34,608,000 for the Goshen Visalia Corridor Connection Project. However, only \$2,636 in programming capacity remained in the Small Urban and Rural component at the time of programming (December 5, 2024). Due to available savings in the ATP, the Commission is able to program the remaining request amount.

Abbreviations, Acronyms, and Initialisms	
CON:	Construction phase
DAC:	Project benefits a disadvantaged community
NI:	Non-infrastructure
PA&ED:	Project Approval & Environmental Document phase
PS&E:	Plans, Specifications & Estimates phase
R/W:	Right-of-way phase
R/W:	Right-of-way phase
SRTS:	Safe Routes to School project

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 2.2c.(3), Action

Prepared By: Cherry Zamora
Associate Deputy Director

Published Date: June 13, 2025

Subject: Approval of Project for Future Consideration of Funding – Bridgeport Rehabilitation Project, Resolution E-25-40

Recommendation:

Staff recommends the California Transportation Commission (Commission), as a Responsible Agency under the California Environmental Quality Act (CEQA), approve the attached Resolution E-25-40 (Attachment A), which accepts the Final Environmental Impact Report for the Bridgeport Rehabilitation (Rehab) Project (Project) in Mono County; approves the Project for future consideration of funding; makes CEQA Findings (Attachment C); and adopts a Statement of Overriding Considerations (Attachment D).

Issue:

The California Department of Transportation (Caltrans) is the CEQA Lead Agency for the Project. The Project is located on U.S. Route 395 from post mile 76.0 to post mile 80.6 in Mono County. The Project would rehabilitate pavement, replace non-standard highway facilities, and make other improvements. Specifically, it would rehabilitate pavement, bring shoulders to standard widths, upgrade metal beam guardrails, provide flatter grading of a side slope, improve culverts, and upgrade pedestrian facilities.

For all projects that are seeking funding through a program under the purview of the Commission, full compliance with CEQA is required. The Commission will not allocate funds to projects for design, right-of-way, or construction until the environmental document is complete, and the Commission has approved the environmentally cleared project for future consideration of funding.

Background:

On December 30, 2024, Caltrans certified the Final Environmental Impact Report for the Project. Caltrans found that the Project would have significant and unavoidable impacts on riparian habitat and state protected wetlands. These impacts would remain significant and unavoidable. Mitigation for impacts to riparian habitat and state protected wetlands through the

California Department of Fish and Wildlife were not found feasible, due to non-acceptance of in-lieu fees by the California Department of Fish and Wildlife, lack of mitigation banks, infeasibility of on-site planting, and infeasibility of purchasing lands for mitigation.

No other resource areas require mitigation to achieve a less than significant level of impact.

The Commission, in its independent judgment as a CEQA responsible agency, has reviewed and considered the Final Environmental Impact Report prepared by Caltrans. The Commission's Findings and Statement of Overriding Considerations, included in Attachment C and Attachment D respectively, have been prepared pursuant to CEQA.

Attachments:

- Attachment A: Resolution
- Attachment B: Notice of Determination
- Attachment C: California Transportation Commission - Findings
- Attachment D: California Transportation Commission - Statement of Overriding Considerations
- Attachment E: Lead Agency Request for Approval of Project for Future Consideration of Funding Resolution E-25-40
 - Attachment 1: Map
 - Attachment 2: California Department of Transportation -- Findings
 - Attachment 3: California Department of Transportation -- Statement of Overriding Considerations

CALIFORNIA TRANSPORTATION COMMISSION

**Resolution for Future Consideration of Funding
09-MNO-395, PM 76.0/80.6
Resolution E-25-40**

- 1.1 WHEREAS**, the California Department of Transportation (Caltrans) has completed a Final Environmental Impact Report pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines for the following project:
- Rehabilitate pavement, replace nonstandard highway facilities, and make other improvements on US 395 from Post Miles (PM) 76.0 to 80.6 in the community of Bridgeport in Mono County (PPNO 2648)
- 1.2 WHEREAS**, Caltrans has certified that a Final Environmental Impact Report has been completed pursuant to CEQA and the State CEQA Guidelines for its implementation; and
- 1.3 WHEREAS**, the California Transportation Commission (Commission), as a responsible agency, has considered the information contained in the Final Environmental Impact Report; and
- 1.4 WHEREAS**, the project will have a significant effect on the environment; and
- 1.5 WHEREAS**, the Commission has made findings as required by California Code of Regulations, title 14, section 15096, subdivision (h); and
- 1.6 WHEREAS**, the Commission has adopted a Statement of Overriding Considerations pursuant to California Code of Regulations, title 14, section 15093;
- 2.1 NOW, THEREFORE, BE IT RESOLVED** that the Commission does hereby approve the above referenced project to allow for consideration of funding.

NOTICE OF DETERMINATION

To: Office of Land Use and Climate Innovation
1400 Tenth Street, Room 121
Sacramento, CA 95814

From: California Transportation Commission
Attn: Cherry Zamora
1120 N Street, MS 52
Sacramento, CA 95814
(916) 654-4245

Subject: Filing of Notice of Determination in compliance with Section 21108 of the Public Resources Code.

Project Title: Bridgeport Rehab Project

2023030750	Dennee Alcala	(760) 784-4236
State Clearinghouse Number	Lead Agency Contact Person	Area Code/Telephone

Project Location (include county): The project is located on United States Route 395 (US 395) from post mile 76.0 to post mile 80.6 in Mono County.

Project Description: Rehabilitate pavement, replace nonstandard highway facilities, and make other improvements. Specifically, the project would rehabilitate pavement, bring shoulders to standard widths, upgrade metal beam guardrails, provide flatter grading of a side slope, improve culverts, and upgrade pedestrian facilities.

This is to advise that the California Transportation Commission has approved the above-described (Lead Agency/ X Responsible Agency) project on June 26-27, 2025, and has made the following determinations regarding the above-described project:

1. The project (X will/ will not) have a significant effect on the environment.
2. X An Environmental Impact Report was prepared for this project pursuant to the provisions of CEQA.
 A Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures (were/ X were not) made a condition of the approval of the project.
4. Mitigation reporting or monitoring plan (was / X was not) adopted for this project.
5. A Statement of Overriding Considerations (X was / was not) adopted for this project.
6. Findings (X were/ were not) made pursuant to the provisions of CEQA.

The above identified document with comments and responses and record of project approval is available to the General Public at: Caltrans District 9, 500 South Main Street, Bishop, CA 93514.

TANISHA TAYLOR

Signature (Public Agency)

Date

Executive Director

California Transportation Commission

Title

Date received for filing at OPR:



Project Name: Bridgeport Rehabilitation (Rehab) Project
DIST-CO-RTE-PM: 09-MNO-395-76.0/80.6
EA: 09-37460
EFIS ID: 0918000018
SCH#: 2023030750

CALIFORNIA TRANSPORTATION COMMISSION FINDINGS OF FACT

FOR

THE BRIDGEPORT REHAB PROJECT TO REHABILITATE PAVEMENT, REPLACE NONSTANDARD HIGHWAY FACILITIES, AND MAKE OTHER IMPROVEMENTS ON U.S. ROUTE 395 FROM POST MILES 76.00 TO 80.60 IN THE COMMUNITY OF BRIDGEPORT IN MONO COUNTY.

The following information is presented to comply with California Environmental Quality Act (CEQA) Guidelines, California Code of Regulations, title 14, sections 15091 and 15096, and also title 21, section 1501 et seq. Reference is made to the Final Environmental Impact Report (EIR) for the project, which is the basic source for the information.

The following effects have been identified in the Final EIR as resulting from the project. Effects found not to be significant have not been included.

Riparian Habitat and State Protected Wetlands

Adverse Environmental Effects:

The project will impact approximately 0.334 acre of California Department of Fish and Wildlife jurisdictional waters. The California Department of Transportation (Caltrans) has reviewed various design options and has found none that avoid or further reduce impacts to riparian habitat and state protected wetlands.

Findings:

Specific economic, legal, social, technological, or other considerations, including provision of employment opportunities for highly trained workers, make infeasible the mitigation measures or project alternatives identified in the Final EIR.



Statement of Facts:

Mitigation for CEQA impacts to riparian habitat and state protected wetlands through the California Department of Fish and Wildlife are not feasible for the following reasons:

- The California Department of Fish and Wildlife does not accept in-lieu fees as an acceptable form of mitigation, although in-lieu fees will be paid to the Lahontan Regional Water Quality Control Board and to the Army Corp of Engineers to mitigate for these same impacts.
- In Mono County, no mitigation bank with a service area that covers the project's location is available, and no approved entities in the County have been self-certified by the California Department of Fish and Wildlife to hold lands and/or endowment/enhancement fees.
- On-site planting within the Caltrans operational right-of-way is not possible due to driver safety constraints, specifically clear recovery zone and sight distance requirements. In addition, increasing special-species habitat within Caltrans operational right-of-way may attract more wildlife and result in a greater number of vehicle strikes.
- Future Caltrans projects and maintenance activities may impact potential on-site planting mitigation sites. Recent Executive Orders (Executive Order [E.O.] B-52-18, E.O. N-05-19, and E.O. N-82-20), as outlined in the 2021 California Wildfire and Forest Resilience Action Plan, have directed the Caltrans Vegetation Management Program to reduce fuels and increase wildfire resiliency along the State Highway System.
- "Permittee-responsible" mitigation, which occurs when Caltrans purchases property with the goal of establishing habitat and conservation easements, is not feasible in this area. Only 6 percent of the land in Mono County is privately owned. Lands classified as mitigation lands must be removed from the local tax base, which is unattractive to local government. Caltrans cannot legally go through the condemnation process to acquire mitigation lands and is required to purchase from willing sellers of which there are few in the area. Per the Mono County General Plan, "the majority of private land within the county is concentrated in community areas, with the remainder dispersed throughout the county in isolated parcels. Within existing community boundaries, some communities have limited land available for additional development; expansion of some communities beyond existing boundaries is limited by the public ownership of surrounding lands." These challenges apply to the purchase of mitigation parcels as well as to development. Caltrans District 9 did attempt to purchase land in Mono County to establish opportunities for future mitigation from 2015-2024, but was unsuccessful in meeting resource agency requirements for creation of a mitigation parcel.



For this reason, this Environmental Impact Report has been prepared along with a Statement of Overriding Considerations based on the determination that although there is potential for significant impacts to aquatic resources as a result of this project, no feasible mitigation exists to reduce those impacts to less than significant under CEQA.

DOCUMENT AVAILABILITY

Documents or other material which constitute the record of the proceedings upon which the California Transportation Commission's decision is based are available at: Caltrans District 9, 500 South Main Street, Bishop, CA 93514

Tanisha Taylor

Executive Director

Signature

Date



Project Name: Bridgeport Rehabilitation (Rehab) Project

DIST-CO-RTE-PM: 09-MNO-395-76.0/80.6

EA: 09-37460

EFIS ID: 0918000018

SCH #: 2023030750

**CALIFORNIA TRANSPORTATION COMMISSION
STATEMENT OF OVERRIDING CONSIDERATIONS**

FOR

**THE BRIDGEPORT REHAB PROJECT TO REHABILITATE PAVEMENT, REPLACE
NONSTANDARD HIGHWAY FACILITIES, AND MAKE OTHER IMPROVEMENTS ON
U.S. ROUTE 395 FROM POST MILES 76.00 TO 80.60 IN THE COMMUNITY OF
BRIDGEPORT IN MONO COUNTY.**

The following information is presented to comply with California Environmental Quality Act (CEQA) Guidelines, California Code of Regulations, title 14, sections 15091 and 15096, and also title 21, section 1501 et seq. Reference is made to the Final Environmental Impact Report (EIR) for the project, which is the basic source for the information.

The California Transportation Commission (Commission), in its independent judgment as a CEQA responsible agency, reviewed and considered the Final EIR prepared by the California Department of Transportation (Caltrans) and finds that the Final EIR contains a complete, objective, and substantiated reporting of the project's potential impacts.

The following impacts have been identified as significant and not fully mitigable:

- **Riparian habitat and state protected wetlands.** The project will impact approximately 0.334 acre of California Department of Fish and Wildlife jurisdictional waters

Overriding considerations that support approval of this recommended project are as follows:

Caltrans has reviewed various design options and has found none that avoid or further reduce impacts to riparian habitat and state protected wetlands.



Mitigation for CEQA impacts to riparian habitat and state protected wetlands through the California Department of Fish and Wildlife are not feasible for the following reasons:

- The California Department of Fish and Wildlife does not accept in-lieu fees as an acceptable form of mitigation, although in-lieu fees will be paid to the Lahontan Regional Water Quality Control Board and to the Army Corp of Engineers to mitigate for these same impacts.
- In Mono County, no mitigation bank with a service area that covers the project's location is available, and no approved entities in the County have been self-certified by the California Department of Fish and Wildlife to hold lands and/or endowment/enhancement fees.
- On-site planting within the Caltrans operational right-of-way is not possible due to driver safety constraints, specifically clear recovery zone and sight distance requirements. In addition, increasing special-species habitat within Caltrans operational right-of-way may attract more wildlife and result in a greater number of vehicle strikes.
- Future Caltrans projects and maintenance activities may impact potential on-site planting mitigation sites. Recent Executive Orders (Executive Order [E.O.] B-52-18, E.O. N-05-19, and E.O. N-82-20), as outlined in the 2021 California Wildfire and Forest Resilience Action Plan, have directed the Caltrans Vegetation Management Program to reduce fuels and increase wildfire resiliency along the State Highway System.
- "Permittee-responsible" mitigation, which occurs when Caltrans purchases property with the goal of establishing habitat and conservation easements, is not feasible in this area. Only 6 percent of the land in Mono County is privately owned. Lands classified as mitigation lands must be removed from the local tax base, which is unattractive to local government. Caltrans cannot legally go through the condemnation process to acquire mitigation lands and is required to purchase from willing sellers of which there are few in the area. Per the Mono County General Plan, "the majority of private land within the county is concentrated in community areas, with the remainder dispersed throughout the county in isolated parcels. Within existing community boundaries, some communities have limited land available for additional development; expansion of some communities beyond existing boundaries is limited by the public ownership of surrounding lands." These challenges apply to the purchase of mitigation parcels as well as to development. Caltrans District 9 did attempt to purchase land in Mono County to establish opportunities for future mitigation from 2015-2024, but was unsuccessful in meeting resource agency requirements for creation of a mitigation parcel.



Selection of the “no-build” alternative would avoid impacts to riparian habitat and state protected wetlands within the project area. This alternative would maintain the existing facilities within the project limits on U.S. Route 395 as is and would result in no project-related construction activities taking place. The no-build alternative would not meet the project purpose and need because it would not address pavement, drainage, or Complete Streets, or upgrade non-standard highway features on the proposed segment of U.S. Route 395 within the project limits. The no-build alternative was determined not to be feasible as pavement and related infrastructure would continue to deteriorate without completion of the project.

To the extent the significant effects of the project are not avoided or substantially lessened to a level of insignificance under CEQA but have been substantially lessened under the National Environmental Policy Act (NEPA), Regional Water Quality Control Board, and Army Corps of Engineer standards, the Commission, having reviewed and considered the information contained in the Final EIR for the Bridgeport Rehab project, and having reviewed and considered the information contained in the public record, and having balanced the benefits of the project against the unavoidable effects which remain, finds such unmitigated effects to be acceptable in consideration of the overriding considerations discussed herein.

Tanisha Taylor

Executive Director

Signature

Date

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.2c.(3), Action Item

Prepared By: Jeremy Ketchum, Chief
Division of Environmental Analysis

Subject: **APPROVAL OF A PROJECT FOR FUTURE CONSIDERATION OF FUNDING
RESOLUTION E-25-40**

ISSUE:

Should the California Transportation Commission (Commission), as a responsible agency, approve the attached Resolution E-25-40?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission, as a responsible agency, approve the attached Resolution E-25-40.

BACKGROUND:

09-Mno-395, PM 76.00/80.60 Resolution E-25-40

The attached resolution proposes to approve for future consideration of funding the following project for which a Final Environmental Impact Report (FEIR) has been completed:

- Bridgeport Rehab. United States Highway (US) 395 in Mono County. Rehabilitate pavement, replace nonstandard highway facilities, and make other improvements in the community of Bridgeport. Specifically, it would rehabilitate pavement, bring shoulders to standard widths, upgrade metal beam guardrails, provide flatter grading of a side slope, improve culverts, and upgrade pedestrian facilities. (PPNO 2648)

The project is located on US 395 from Post Miles (PM) 76.00 to 80.60 in Mono County. The Department proposes to rehabilitate pavement, replace nonstandard highway facilities, and make other improvements in the community of Bridgeport. Specifically, it would rehabilitate pavement, bring shoulders to standard widths, upgrade metal beam guardrails, provide flatter grading of a side slope, improve culverts, and upgrade pedestrian facilities. The project is

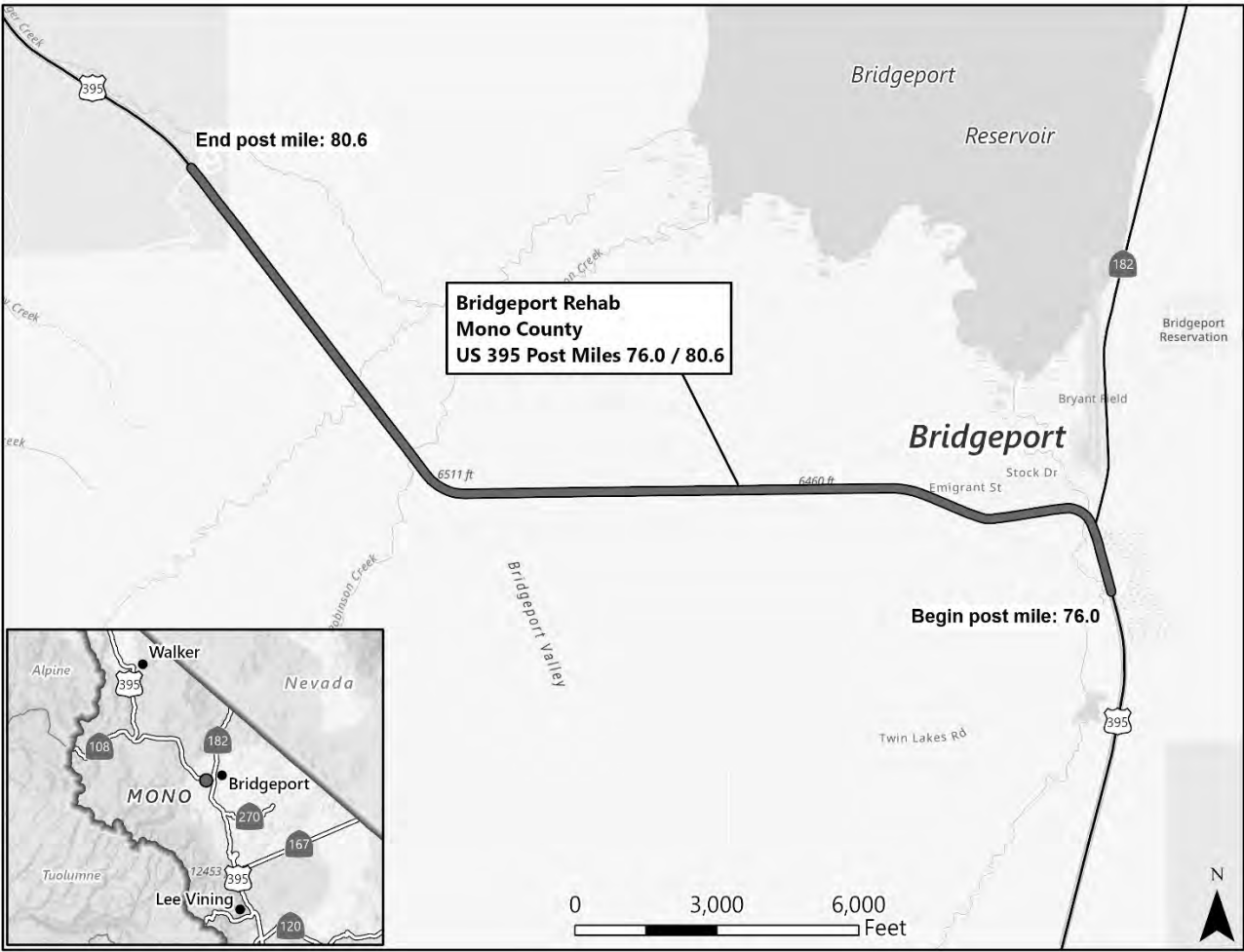
CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSIONReference No.: 2.2c.(3)
June 26-27, 2025
Page 2 of 2

currently programmed in the 2024 State Highway Operation and Protection Program. The total programmed amount which includes Right of Way (Support and Capital), and Construction (Support and Capital) is \$18,325,000. Construction will begin in Fiscal Year 2025-26. The scope, as described for the preferred alternative, is consistent with the project scope as programmed by the Commission in the 2024 SHOPP.

A copy of the FEIR has been provided to Commission staff. Previously, an MND was produced for the project dated May 23, 2023, that received a Future Consideration of Funding under Resolution E-23-113 on August 16, 2023. However, upon further evaluation, the following impacts have been identified as significant and not fully mitigatable: impacts to riparian habitat and state protected wetlands. As a result, the FEIR with a Statement of Overriding Considerations has been prepared pursuant to the California Environmental Quality Act (CEQA). Mitigation for CEQA impacts to riparian habitat and state protected wetlands through the California Department of Fish and Wildlife were not found feasible, due to non-acceptance of in-lieu fees, lack of mitigation banks, infeasibility of on-site planting, and infeasibility of purchasing lands for mitigation.

Attachments

Attachment 1



Bridgeport Rehab



Project Name: Bridgeport Rehab Project
DIST-CO-RTE-PM: 09-MNO-395-76.0/80.6
EA: 09-37460
EFIS ID: 0918000018

CALIFORNIA DEPARTMENT OF TRANSPORTATION FINDINGS

FOR

THE BRIDGEPORT REHAB PROJECT TO REHABILITATE PAVEMENT, REPLACE
NONSTANDARD HIGHWAY FACILITIES, AND MAKE OTHER IMPROVEMENTS ON
U.S. ROUTE 395 FROM POST MILES 76.00 TO 80.60 IN THE COMMUNITY OF
BRIDGEPORT IN MONO COUNTY.

The following information is presented to comply with State CEQA Guidelines (Title 14 California Code of Regulations, Division 6, Chapter 3, Section 15091) and the Department of Transportation and California Transportation Commission Environmental Regulations (Title 21, California Code of Regulations, Division 2, Chapter 11, Section 1501 et seq.). Reference is made to the Final Environmental Impact Report (FEIR) for the project, which is the basic source for the information.

The following effects have been identified in the EIR as resulting from the project. Effects found not to be significant have not been included.

Riparian Habitat and State Protected Wetlands

Adverse Environmental Effects:

The project will impact approximately 0.334 acre of California Department of Fish and Wildlife jurisdictional waters. Caltrans has reviewed various design options and has found none that avoid or further reduce impacts to riparian habitat and state protected wetlands.

Findings:

Specific economic, legal, social, technological, or other considerations, including provision of employment opportunities for highly trained workers, make infeasible the mitigation measures or project alternatives identified in the final EIR.

Statement of Facts:

Mitigation for CEQA impacts to riparian habitat and state protected wetlands through the California Department of Fish and Wildlife are not feasible for the following reasons:

- The California Department of Fish and Wildlife does not accept in-lieu fees as an acceptable form of mitigation, although in-lieu fees will be paid to the Lahontan Regional Water Quality Control Board and to the Army Corp of Engineers to mitigate for these same impacts.
- In Mono County, no mitigation bank with a service area that covers the project's location is available, and no approved entities in the County have been self-certified by the California Department of Fish and Wildlife to hold lands and/or endowment/enhancement fees.
- On-site planting within the Caltrans operational right-of-way is not possible due to driver safety constraints, specifically clear recovery zone and sight distance requirements. In addition, increasing special-species habitat within Caltrans operational right-of-way may attract more wildlife and result in a greater number of vehicle strikes.
- Future Caltrans projects and maintenance activities may impact potential on-site planting mitigation sites. Recent Executive Orders (E.O. B-52-18, E.O. N-05-19, and E.O. N-82-20), as outlined in the California Wildfire and Forest Resilience Action Plan (2021), have directed the Caltrans Vegetation Management Program to reduce fuels and increase wildfire resiliency along the State Highway System.
- "Permittee-reponsible" mitigation, which occurs when Caltrans purchases property with the goal of establishing habitat and conservation easements is not feasible in this area. Only 6% of the land in Mono County is privately owned. Lands classified as mitigation lands must be removed from the local tax base, which is unattractive to local government. Caltrans cannot legally go through the condemnation process to acquire mitigation lands, and is required to purchase from willing sellers of which there are few in the area. Per the Mono County General Plan, "the majority of private land within the county is concentrated in community areas, with the remainder dispersed throughout the county in isolated parcels. Within existing community boundaries, some communities have limited land available for additional development; expansion of some communities beyond existing boundaries is limited by the public ownership of surrounding lands." These challenges apply to the purchase of mitigation parcels as well as to development. Caltrans District 9 did attempt to purchase land in Mono County to establish opportunities for future mitigation from 2015-2024, but was unsuccessful in meeting resource agency requirements for creation of a mitigation parcel.

For this reason, this Environmental Impact Report has been prepared along with a Statement of Overriding Considerations based on the determination that although there is potential for significant impacts to aquatic resources as a result of this project, no feasible mitigation exists to reduce those impacts to less than significant under CEQA.

Kirsten Helton

Deputy District 9 Director,
Planning and Environmental
Analysis



Signature

12/30/2024



Project Name: Bridgeport Rehab Project
DIST-CO-RTE-PM: 09-MNO-395-76.0/80.6
EA: 09-37460
EFIS ID: 0918000018

**CALIFORNIA DEPARTMENT OF TRANSPORTATION
STATEMENT OF OVERRIDING CONSIDERATIONS**

FOR

THE BRIDGEPORT REHAB PROJECT TO REHABILITATE PAVEMENT, REPLACE
NONSTANDARD HIGHWAY FACILITIES, AND MAKE OTHER IMPROVEMENTS ON
U.S. ROUTE 395 FROM POST MILES 76.00 TO 80.60 IN THE COMMUNITY OF
BRIDGEPORT IN MONO COUNTY.

The following information is presented to comply with State CEQA Guidelines (Title 14 California Code of Regulations, Division 6, Chapter 3, Section 15093), and the Department of Transportation and California Transportation Commission Environmental Regulations (Title 21 California Code of Regulations, Division 2, Chapter 11, Section 1501 et seq.). Reference is made to the Final Environmental Impact Report (FEIR) for the project, which is the basic source for the information.

The following impacts have been identified as significant and not fully mitigable:

- Riparian habitat and state protected wetlands.
The project will impact approximately 0.334 acre of California Department of Fish and Wildlife jurisdictional waters

Overriding considerations that support approval of this recommended project are as follows:

Caltrans has reviewed various design options and has found none that avoid or further reduce impacts to riparian habitat and state protected wetlands.

Mitigation for CEQA impacts to riparian habitat and state protected wetlands through the California Department of Fish and Wildlife are not feasible for the following reasons:

- The California Department of Fish and Wildlife does not accept in-lieu fees as an acceptable form of mitigation, although in-lieu fees will be paid to the Lahontan Regional Water Quality Control Board and to the Army Corp of Engineers to mitigate for these same impacts.
- In Mono County, no mitigation bank with a service area that covers the project's location is available, and no approved entities in the County have been self-certified



by the California Department of Fish and Wildlife to hold lands and/or endowment/enhancement fees.

- On-site planting within the Caltrans operational right-of-way is not possible due to driver safety constraints, specifically clear recovery zone and sight distance requirements. In addition, increasing special-species habitat within Caltrans operational right-of-way may attract more wildlife and result in a greater number of vehicle strikes.
- Future Caltrans projects and maintenance activities may impact potential on-site planting mitigation sites. Recent Executive Orders (E.O. B-52-18, E.O. N-05-19, and E.O. N-82-20), as outlined in the California Wildfire and Forest Resilience Action Plan (2021), have directed the Caltrans Vegetation Management Program to reduce fuels and increase wildfire resiliency along the State Highway System.
- “Permittee-reponsible” mitigation, which occurs when Caltrans purchases property with the goal of establishing habitat and conservation easements is not feasible in this area. Only 6% of the land in Mono County is privately owned. Lands classified as mitigation lands must be removed from the local tax base, which is unattractive to local government. Caltrans cannot legally go through the condemnation process to acquire mitigation lands, and is required to purchase from willing sellers of which there are few in the area. Per the Mono County General Plan, “the majority of private land within the county is concentrated in community areas, with the remainder dispersed throughout the county in isolated parcels. Within existing community boundaries, some communities have limited land available for additional development; expansion of some communities beyond existing boundaries is limited by the public ownership of surrounding lands.” These challenges apply to the purchase of mitigation parcels as well as to development. Caltrans District 9 did attempt to purchase land in Mono County to establish opportunities for future mitigation from 2015-2024, but was unsuccessful in meeting resource agency requirements for creation of a mitigation parcel.

Selection of the “no-build” alternative would avoid impacts to riparian habitat and state protected wetlands within the project area. This alternative would maintain the existing facilities within the project limits on U.S. Route 395 as is and would result in no project-related construction activities taking place. The no-build alternative would not meet the project purpose and need because it would not address pavement, drainage or Complete Streets or upgrade non-standard highway features on the proposed segment of U.S. Route 395 within the project limits. The no-build alternative was determined not to be feasible as pavement and related infrastructure would continue to deteriorate without completion of the project.

To the extent the significant effects of the project are not avoided or substantially lessened to a level of insignificance under CEQA but have been substantially lessened under NEPA, Regional Water Quality Control Board and Army Corps of Engineer standards, Caltrans, having reviewed and considered the information contained in the Final EIR for the Bridgeport Rehab project, and having reviewed and considered the information contained in the public record, and having balanced the benefits of the project against the unavoidable effects which remain, finds such unmitigated effects to be acceptable in consideration of the overriding considerations discussed herein.



Kirsten Helton

Deputy District 9 Director,
Planning & Environmental
Analysis

Signature

12/30/2024

Date

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5d.(2), Action Item

Prepared By: Dina El-Tawansy
District 04 – Director

Subject: **ALLOCATION FOR PROJECT WITH COSTS THAT EXCEED THE
PROGRAMMED AMOUNT BY MORE THAN 20 PERCENT
PPNO 2904K/EA 1Q820 – SAN FRANCISCO COUNTY –
UNITED STATES HIGHWAY 101
RESOLUTION FP-24-85**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) allocation request for \$118,000,000 for Construction of the State Highway Operation and Protection Program (SHOPP) Roadway Rehabilitation project on United States Highway (US) 101, in San Francisco County, to award the project?

RECOMMENDATION:

The Department recommends that the Commission approve the requested allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located along US 101 in the City of San Francisco (City), from the San Mateo County line to Market Street, in San Francisco County. The project will rehabilitate approximately 15 lane-miles of the roadway and drainage facilities, and upgrade signs, concrete barrier, and transportation management system elements which include traffic monitoring stations. This project is utilizing the Construction Manager/General Contractor (CMGC) delivery process.

FUNDING AND PROGRAMMING STATUS:

In May 2020, this project was programmed in the SHOPP for \$45,556,000 in Construction Capital and \$6,833,000 in Construction Support for allocation in Fiscal Year 2023-24. In October 2020, the project was amended to correct the PPNO. In May 2023, the project was amended again to increase Construction Capital to \$69,027,000 due to a change in the pavement repair strategy. The change in the repair strategy from spot slab replacement to full concrete pavement replacement for approximately 10 lane-miles of the project was based on an updated pavement survey showing increased deterioration and materials recommendation. In October 2023, the project was selected to utilize the CMGC delivery method, and the CMGC was brought on board in April 2024. In June 2024, the project requested an 18-month allocation time extension for the Construction phase. However, the request was deferred to the August 2024 meeting and approved for 14 months.

In May 2025, the Engineer's Estimate reflected the need of \$107,000,000 in Construction Capital (55.0 percent over the programmed amount) based on the Agreed to Price with the CMGC, and \$11,000,000 in Construction Support (61.0 percent over the programmed amount). The Department plans to award the project in August 2025 and begin construction in September 2025. Construction is planned for three construction seasons with a duration of 590 working days.

REASON FOR COST INCREASE:

The Construction Capital estimate is greater than the programmed amount due to updated quantities and unit prices based on current market conditions and recent price escalation, additional drainage system work, added shoulder replacement necessary for the change in pavement strategy, an extensive traffic management plan in the center of the City, implementation of new temporary barrier standards, and incorporation of work from an overlapping project. The Construction Support estimate is greater than the programmed amount due to the increase in the number of working days, additional work, and the complex construction staging.

Capital Cost Increase:

The project's Construction Capital was programmed in May 2020 and was increased in May 2023 through an amendment due to a new pavement replacement strategy. In addition, other factors contributed to the cost increase. The project report originally anticipated 380 working days for the duration of the construction. However, the current estimate is 590 working days, which changes the midpoint of construction and increases the time-related overhead cost.

The Department has seen higher than anticipated bids particularly in the City for bid items such as barrier rail, partially due to a limited number of contractors willing to bid on projects within high traffic volume areas. This project has over four miles of median barrier upgrade. Recent economic uncertainty and risk of tariffs affecting prices and availability for concrete and steel have been factored into the estimate. In addition, this project has a high need for materials

storage space with limited availability and high cost of real estate in the City.

As the design progressed, it was determined that all the drainage systems under the proposed concrete pavement and shoulders needed to be replaced. At the time of the project report, the systems were not clean enough to identify the full extent of the replacement work. The initial cost did not assume full replacement of all the drainage systems. The original drainage work consisted of replacing 7 culverts and raising 45 drainage inlets. The project will now replace 60 drainage systems which includes modifying or adjusting 46 drainage inlets.

The original scope of work did not account for replacing the outside shoulders beyond the work needed to replace the drainage inlets. During the design phase, it was determined that shoulder replacement was necessary through a segment of the project limits to support the concrete paving operations, facilitate traffic staging, and address long term motorist safety. The existing outside shoulder pavement is not sufficient to support the weight of the concrete paving machine and it will be difficult to match the existing shoulder gutter with the new pavement. In addition, to replace the drainage systems, significant portions of the shoulders would need to be excavated and replaced, resulting in a patchwork of repaired pavement and inefficiencies. Furthermore, additional roadway width is needed to accommodate the temporary barrier system associated with the center median and drainage work. Finally, a new, smooth outside shoulder on the same plane as the traveled way benefits long term traffic safety over the existing shoulder condition. For all these reasons, the Department decided to replace the shoulders with a traffic-rated pavement section and incorporate additional edge-of-pavement safety elements which include curbs and guardrail.

To accomplish the concrete pavement and the barrier rail reconstruction, multiple long-term freeway closures are required as these operations cannot be done within the limited nightly construction windows available. There are limited weekends available for extended closures due to impacts on the City. Nighttime work windows for the drainage and other work are also restricted. The US 101 freeway corridor does not have many lanes, and alternative routes are limited in the densely populated adjacent communities.

The Department implemented the new standard temporary barrier system instead of the existing temporary concrete barrier. The manufacturing industry is ramping up production of the new barrier but currently has an insufficient stockpile of the new barrier and must accelerate the fabrication to meet the immediate need for a substantial quantity of barrier upon the start of construction of this project.

This project incorporates fiber optic conduit installation work from an overlapping project (EA 2Q740/PPNO 2027J) through a segment of the project limits to reduce rework of constructed elements and delays. It is critical to install the conduit prior to the construction of the concrete pavement to avoid damaging the new pavement and prevent conflicts between two contractors in the same area. The two projects can be scheduled around each other for the remaining fiber optic work outside this segment.

The refinements that were made to the design based on the updated quantities and unit prices, additional drainage system work, shoulder repaving, robust traffic management plan, inclusion

of the new temporary barrier, and incorporation of fiber optic work from an overlapping project, amount to an increase of \$36,473,000 in capital costs.

Support Cost Increase:

The support cost increase is due to the addition of 210 working days (from 380 to 590) to address additional work, a more complex traffic management plan, and extensive public outreach and engagement than was originally anticipated. Multiple work shifts in a day, and multiple extended full and partial freeway closures will be required to perform the work. Most of the work will be performed at night with multilane closures. There are four 55-hour full freeway closures for the concrete pavement work and nine 55-hour partial freeway closures for the replacement of the concrete barrier.

This project is one of four major projects in the area that are expected to be in construction concurrently. The Department expects extensive coordination between the contractors and Department staff along with substantial public information and outreach efforts in support of the work.

The additional work, complex traffic management, extensive coordination with the public, and additional working days have led to an increase of \$4,167,000 in support costs.

CONSEQUENCES:

If this allocation request is not approved, the Department will not be able to award the contract to the CMGC and start construction to address the critical deficiencies of the existing roadway and roadside elements along this segment of US 101. Reprogramming for future delivery or repackaging to advertise through the normal Design-Bid-Build delivery method will risk higher costs due to escalation, further asset degradation, emergency repairs, and the need to re-engage with the local stakeholders and communities about staging and traffic handling.

FINANCIAL RESOLUTION:

Resolved, that \$107,000,000 be allocated from the Budget Act of 2023, Budget Act Item 2660-302-0890 and Non-Budget Act Item 2660-802-3290 for Construction Capital, and \$11,000,000 for Construction Support, to provide funds to advertise this SHOPP project.

Attachment

2.5 Highway Financial Matters

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5d.(2)	Allocation of Project with Construction Cost that Exceeds 20 Percent of the Programmed Amount			Resolution FP-24-85
1 \$118,000,000 San Francisco 04-SF-101 0.0/M5.45	In the City and County of San Francisco, from San Mateo County line to Market Street. <u>Outcome/Outputs:</u> Rehabilitate roadway, upgrade signs, concrete barrier, and Transportation Management System (TMS) elements, rehabilitate drainage, and upgrade facilities to Americans with Disabilities Act (ADA) standards. This is a Construction Manager/General Contractor (CMGC) project.	04-2904K SHOPP/23-24 CON ENG \$6,833,000 CONST \$69,027,000 0418000341 3,4 1Q820	505-3290 RMRA 001-0890 FTF 20.10.201.120 2017-18 802-3290 RMRA 2023-24 302-0890 FTF 20.20.201.120	\$1,262,000 <u>\$9,738,000</u> \$11,000,000

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5d.(3), Action Item

Prepared By: Dina El-Tawansy
District 04 – Director

Subject: **ALLOCATION FOR PROJECT WITH COSTS THAT EXCEED THE
PROGRAMMED AMOUNT BY MORE THAN 20 PERCENT
PPNO 2091L/EA 3X340 – SANTA CLARA COUNTY – STATE ROUTE 82
RESOLUTION FP-24-86**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) allocation request for \$14,000,000 for the Project Approval and Environmental Document (PA&ED) Capital Outlay Support, for the locally-administered State Transportation Improvement Program (STIP) Central Bikeway project on State Route (SR) 82 and local roads, in Santa Clara County, to complete the PA&ED phase?

RECOMMENDATION:

The Department recommends that the Commission approve the Santa Clara Valley Transportation Authority's (SCVTA) requested allocation for this locally-administered STIP project.

PROJECT DESCRIPTION:

This project is located on SR 82 in the cities of San Jose and Santa Clara, from the intersection of El Camino Real (SR 82) and Lawrence Expressway, and runs along El Camino Real, and then along the local roads The Alameda, Hedding Street, North 10th Street/North 11th Street couplet, Taylor Street, and Mabury Road, in Santa Clara County. The project will construct a Class IV bikeway.

FUNDING AND PROGRAMMING STATUS:

In March 2024, this project was programmed in the STIP for \$8,500,000 in PA&ED for allocation in Fiscal Year 2024-25.

In March 2025, the SCVTA updated the estimate and reflected the need for \$14,000,000 in PA&ED allocation (64.7 percent over the programmed amount). The project is expected to complete the PA&ED activities by June 2028.

REASON FOR COST INCREASE:

The PA&ED estimate is greater than the current programmed amount due to additional studies that are required for the California Environmental Quality Act/National Environmental Policy Act environmental clearance, further analysis and mitigation planning for right of way and utility impacts, and an increase in public engagement that is above what was anticipated.

The project will construct an approximately 11-mile-long Class IV bikeway with a segment that is on State right of way. The bikeway will connect two major public rail transit facilities. The design will include bicycle-specific signals, protected phases at the intersections, and raised crossings as well as construction of transit boarding islands.

The initial assumption was that a lower level of design would be sufficient for environmental clearance and right of way determinations. However, further evaluations found the need for a more detailed design approach at this stage to meet multimodal connectivity requirements. As a result, additional engineering work will be done during PA&ED. While this additional effort will increase early-phase work, it reduces risks by ensuring the environmental document is based on accurate design and that right of way and utility impacts are clearly defined. This early work might also reduce the Plans, Specifications, and Estimate phase work. Additional scope element changes were needed due to stakeholder input (types of bike lanes, landscaping, and bus boarding), and a more detailed understanding of the right of way constraints. Property impact assessments resulted in a need for further analysis and mitigation planning. The project design will be developed beyond 30 percent to allow for earlier geotechnical work and utility relocation planning to reduce risks to the project. The additional resources will allow the completion of the environmental studies, and further analysis and mitigation planning for right-of-way needs and utility coordination. This also includes increased public engagement efforts to address concerns with the locals, which will help deliver the project on schedule. Therefore, additional resources in the amount of \$5,500,000 will be required to complete the PA&ED phase. The PA&ED phase is scheduled to be completed by June 2028.

CONSEQUENCES:

If this allocation request is not approved, SCVTA will not be able to proactively address environmental and right of way concerns and clear the project to move through the delivery process. Funds for the project will be lost and the project will have to be reprogrammed which will result in delays and could result in higher costs due to escalation.

FINANCIAL RESOLUTION:

Resolved, that \$14,000,000 be allocated from the Budget Act of 2024, Budget Act Items 2660-301-0042 and 2660-301-0890, for PA&ED to provide funds to complete the pre-construction phase for this locally-administered STIP project.

Attachment

2.5 Highway Financial Matters

Project No.		PPNO		
Allocation Amount		Program/Year		
Recipient		Phase		
RTPA/CTC	Project Title	Prgm'd Amount		
County	Location	Project ID	Budget Year	
Dist-Co-Rte	Project Description	Adv Phase	Item # Fund Type	Amount by
Postmile	Project Support Expenditures	EA	Program Code	Fund Type

2.5d.(3)

Allocation of Project with COS Cost that Exceeds 20 Percent of the Programmed Amount

Resolution FP-24-86

1	Central Bikeway. In the cities of San Jose and Santa Clara: the Central Bikeway project will construct a Class IV physically separated bikeway. Its alignment will start at the intersection of El Camino Real (SR 82) and Lawrence Expressway, and run along El Camino Real, The Alameda, Hedding Street, North 10th Street/North 11th Street couplet, Taylor Street, and Mabury Road.			04-2091L	2024-25	
\$14,000,000				RIP/24-25	301-0042 SHA	\$1,606,000
				PA&ED	301-0890 FTF	<u>\$12,394,000</u>
				\$8,500,000	20.20.075.600	<u>\$14,000,000</u>
Santa Clara Valley Transportation Authority				\$14,000,000		
MTC	<u>Outputs</u>	<u>Unit</u>	<u>Total</u>	0425000235		
Santa Clara	Bicycle lane mile(s)	Miles	5.4	4PAED		
04-SCI-82				3X340		
Var	The additional \$5,500,000 RIP increase will be funded with Santa Clara County RIP shares.					

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5d.(4), Action Item

Prepared By: Scott Eades
District 05 - Director

Subject: ALLOCATION FOR PROJECT WITH COSTS THAT EXCEED THE
PROGRAMMED AMOUNT BY MORE THAN 20 PERCENT
PPNO 2850/EA 1K020 – MONTEREY COUNTY – STATE ROUTE 1
RESOLUTION FP-24-87

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) allocation request for \$3,673,000 for Construction of the State Highway Operation and Protection Program (SHOPP) Major Damage (Permanent Restoration) project on State Route (SR) 1, in Monterey County, to advertise the project?

RECOMMENDATION:

The Department recommends that the Commission approve the requested allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on SR 1 near Gorda, from north of White Creek Bridge to south of Los Burros Road. The project will realign the roadway, replace temporary safety features with permanent safety devices, and install erosion control.

FUNDING AND PROGRAMMING STATUS:

In June 2020, this project was programmed in the SHOPP for \$2,176,000 in Construction Capital and \$838,000 in Construction Support for allocation in Fiscal Year 2022-23. In June 2022, the project was amended to move out the allocation of the Construction and Right of Way Capital phases to 2024-25 due to the acquisition of a required parcel that was needed to be completed through the condemnation process. A concurrent project development

expenditure time extension amendment for the Plans, Specifications, and Estimate phase is on this month's Commission agenda.

In March 2025, the Engineer's Estimate reflected the need of \$2,835,000 in Construction Capital (30.3 percent over the programmed amount) and \$838,000 in Construction Support (no increase over the programmed amount). The Department plans to advertise the project in August 2025 and begin construction in December 2025. Construction is planned for one construction season with a duration of 55 working days.

REASON FOR COST INCREASE:

The Construction Capital estimate is greater than the programmed amount due to the change in the rehabilitation strategy of the pavement structural section, increase in material cost for roadway rehabilitation, and the addition of aesthetic treatments.

This project is the follow-up project to the emergency work conducted in response to the 2017 Mud Creek landslide (EA 1H780/PPNO 2688). The project initially included obtaining a parcel of land owned by the State Lands Commission (SLC). This required additional coordination and resulted in moving out the allocation of the Construction phase by two years. Negotiations with the SLC to obtain the parcel of land were slow, and keeping the parcel in the project scope risked project delivery failure. In an effort to keep the project on schedule, the Department and SLC ended negotiations when the parcel was removed from the project scope in late 2024. As a result, the limits of hydroseeding and plant establishment were modified.

As the project progressed, it was found that the roadway condition continued to deteriorate which resulted in an extensive rehabilitation strategy. As the slide has stopped moving, settlement of the area continues. After consultation with the Department's Geotechnical and Materials group, the strategy changed from a hot mix asphalt overlay to reconstruction of the structural section. The new rehabilitation strategy led to an increase in the capital cost greater than what was programmed.

In addition, the project required a Coastal Development Permit from the California Coastal Commission (CCC). The CCC required substantial conditions for the permit, including more extensive aesthetic treatments than what was originally anticipated. The treatments include coloring concrete, staining steel elements, changes to the temporary barrier type, protection for historic resources, and slope protection elements. In March 2025, the permit was approved following the conclusion of negotiations with the CCC.

The refinement to the roadway rehabilitation strategy and addition of the aesthetic treatments, amount to an increase of \$659,000 in capital costs.

CONSEQUENCES:

If this allocation request is not approved, the Department will not be able to address the critical deficiencies of the existing roadway and roadside elements. To address all of the deficiencies, the project would have to be reprogrammed, which would result in delays and could result in higher costs due to escalation.

FINANCIAL RESOLUTION:

Resolved, that \$2,835,000 be allocated from the Budget Act of 2023, Budget Act Item 2660-302-0890 and Non-Budget Act Item 2660-802-3290 for Construction Capital, and \$838,000 for Construction Support, to provide funds to advertise this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5d.(4)	Allocation of Project with Construction Cost that Exceeds 20 Percent of the Programmed Amount		Resolution FP-24-87	
1 \$3,673,000 Monterey 05-Mon-1 8.7/9.1	Near Gorda, from north of White Creek Bridge to 2.0 miles south of Los Burros Road. <u>Outcome/Outputs</u> : Realign highway, replace temporary safety features with permanent safety devices, and install erosion control measures. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	05-2850 SHOPP/24-25 CON ENG \$838,000 CONST \$2,176,000 0518000106 3,4 1K020	505-3290 RMRA 001-0890 FTF 20.10.201.131 2017-18 802-3290 RMRA 2023-24 302-0890 FTF 20.20.201.131	\$96,000 <u>\$742,000</u> \$838,000 \$325,000 <u>\$2,510,000</u> \$2,835,000
	<u>Performance Measure</u> : Planned: 1.0, Actual: 1.0 Location(s) CEQA - CE, 07/28/2021; Re-validation 04/16/2025 NEPA - CE, 07/28/2021; Re-validation 04/16/2025			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5d.(5), Action Item

Prepared By: Gloria Roberts
District 07 – Director

Subject: ALLOCATION FOR PROJECT WITH COSTS THAT EXCEED THE
PROGRAMMED AMOUNT BY MORE THAN 20 PERCENT
PPNO 5625/EA 36150 – LOS ANGELES COUNTY – STATE ROUTE 1
RESOLUTION FP-24-88

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) allocation request for \$5,150,000 for the Plans, Specifications, and Estimate (PS&E) and \$2,520,000 in Right of Way (RW) Capital Outlay Support (COS), for the State Highway Operation and Protection Program (SHOPP) Pavement Preservation project on State Route (SR) 1, in Los Angeles County, to complete the phases?

RECOMMENDATION:

The Department recommends that the Commission approve the requested allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on SR 1 in and near the cities of Santa Monica, Los Angeles, and Malibu, from Colorado Avenue to south of Cross Creek Road, in Los Angeles County. The project will rehabilitate the pavement and upgrade guardrail, culverts, pedestrian facilities, and transportation management system (TMS) elements consisting of traffic census stations, pedestrian tunnel lighting, signals, signs, and a closed-circuit television (CCTV) camera.

FUNDING AND PROGRAMMING STATUS:

In March 2022, this project was programmed in the SHOPP for \$3,140,000 in PS&E and \$1,159,000 in RW Support for allocation in Fiscal Year 2023-24. In June 2024, the project requested an allocation time extension for the PS&E and RW Support phases. However, the

request was deferred to the August 2024 meeting and approved for six months. In December 2024, the project received a 6-month allocation time extension amendment (totaling 12 months) for the PS&E and RW Support phases.

In April 2025, the Department updated the estimate and reflected the need for \$5,150,000 in PS&E (64.0 percent over the programmed amount) and \$2,520,000 in RW Support (117.4 percent over the programmed amount). The project is scheduled to be Ready to List (RTL) and complete the RW Support activities by November 2026, and complete the remaining PS&E activities by July 2027. A concurrent project development expenditure time extension for the Project Approval and Environmental Document (PA&ED) phase is on this month's Commission agenda.

REASON FOR COST INCREASE:

Originally, the project proposed to rehabilitate the distressed asphalt concrete pavement with an overlay, reconstruct damaged pavement structural sections, replace or repair drainage culverts, install TMS elements (consisting of traffic census stations), and upgrade guardrail, curb ramps, and bus pads along an approximately 12-mile segment of SR 1.

However, the PS&E estimate is greater than the current programmed amount due to the need to incorporate additional safety enhancement items for all modes of transportation. The RW Support estimate is greater than the current programmed amount due to additional right of way requirements that have been identified as a result of the need to incorporate the safety enhancement items.

PS&E Cost Increase:

As a result of the project limits being declared as a safety priority corridor in late 2023, the Department committed to include additional elements into the project scope to accelerate the implementation of the improvements. During the PA&ED phase, the Department conducted a road safety audit with multiple stakeholders and identified a list of safety enhancement measures that should be incorporated into the project. As a result, additional resources will be required to incorporate safety enhancement elements into the project. The safety enhancement elements include approximately 5 miles of Class II bike lanes, 300 feet of sidewalk, 2 new crosswalks, tunnel lighting, sign replacement, and a CCTV camera to minimize conflicts through this segment of SR 1.

The various design teams will require an additional \$1,592,000 to incorporate the added scope. In addition, \$250,000 will be required for geotechnical drilling for the proposed CCTV camera, signal modifications, signs, and a retaining wall needed for sidewalk work.

Therefore, additional resources in the amount of \$2,010,000 will be required to complete the PS&E phase. The project is anticipated to be RTL by November 2026 and complete the PS&E phase by July 2027.

RW Support Cost Increase:

Initially, the right of way efforts identified 16 parcels that would be utilized as temporary construction easements (TCEs). However, during the PA&ED phase, the total number of required parcels increased to 20, which now consists of a fee parcel, a highway easement parcel, eight highway easements, and 10 TCEs. This increase in the number and complexity of the acquisition parcels will result in additional right of way support efforts to acquire the rights and certify the project.

In addition, this project is within the Palisades fire recovery area. Extensive engagement will need to continue with the coordination of ongoing recovery and rebuilding efforts along this segment of SR 1.

Therefore, additional resources in the amount of \$1,361,000 will be required to complete the RW Support phase by November 2026.

CONSEQUENCES:

If this allocation request is not approved, the Department will not be able to proceed with the design of this project, which proposes to address the critical deficiencies of the existing roadway and incorporate required safety enhancement measures. This would postpone the safety enhancement items as requested by the community to improve safety along this segment of SR 1. To address all of the deficiencies, the project will have to be reprogrammed, which will result in delays and could result in higher costs due to escalation.

FINANCIAL RESOLUTION:

Resolved, that \$5,150,000 be allocated for PS&E and \$2,520,000 be allocated for RW Support to provide funds to complete the pre-construction phases for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5d.(5)	Allocation of Project with COS Cost that Exceeds 20 Percent of the Programmed Amount					Resolution FP-24-88	
1 07-LA-1 35.2/46.9	5625 0719000287	In and near the cities of Santa Monica, Los Angeles, and Malibu, from Colorado Avenue to south of Cross Creek Road. <u>Outcome/Outputs:</u> Rehabilitate pavement, upgrade guardrail, Transportation Management System (TMS) elements, and culverts, and upgrade facilities to Americans with Disabilities Act (ADA) standards.	36150	25-26	PS&E RW Sup	\$3,140,000 \$1,159,000	\$5,150,000 \$2,520,000
<u>Program Code</u> 201.121 - Pavement Preservation (CAPM)							
CEQA - CE, 05/16/2025							
Amendment to time extension for an additional six months, for a total of twelve months, for PS&E and R/W Sup approved under Waiver 24-199; December 2024.							
Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.							

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5d.(6), Action Item

Prepared By: Gloria Roberts
District 07 - Director

Subject: **ALLOCATION FOR PROJECT WITH COSTS THAT EXCEED THE
PROGRAMMED AMOUNT BY MORE THAN 20 PERCENT
PPNO 5733/EA 37080 – LOS ANGELES COUNTY – INTERSTATE 5
RESOLUTION FP-24-89**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) allocation request for \$21,458,000 for Construction of the State Highway Operation and Protection Program (SHOPP) Roadway Rehabilitation project on Interstate 5 (I-5), in Los Angeles County, to advertise the project?

RECOMMENDATION:

The Department recommends that the Commission approve the requested allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on I-5 in the City of Burbank, from north of San Fernando Boulevard to south of Cohasset Street. The project will rehabilitate approximately four lane miles of the existing roadway.

FUNDING AND PROGRAMMING STATUS:

In March 2022, this project was programmed in the SHOPP for \$10,934,000 in Construction Capital and \$1,894,000 in Construction Support for allocation in Fiscal Year 2025-26. In October 2023, the project was amended to move up the allocation of the Construction and Right of Way Capital phases to 2024-25 to deliver the improvements sooner. In May 2024, the project was amended again to increase Construction Capital to \$12,720,000 due to change in

staging, modifications to a ramp metering system, and unit price increase for roadway materials, and Construction Support to \$2,140,000 due to the need of additional inspectors.

In April 2025, the Engineer's Estimate reflected the need of \$19,318,000 in Construction Capital (51.9 percent over the programmed amount) and \$2,140,000 in Construction Support (same as the programmed amount). The Department plans to advertise the project in August 2025 and begin construction in December 2025. Construction is planned for one construction season with a duration of 240 working days.

REASON FOR COST INCREASE:

The Construction Capital estimate is greater than the programmed amount due to a change to the structural section material and method of removal of the existing structural section.

The initial cost estimate for the structural section was based on the unit prices for regular concrete pavement and lean concrete base. Due to the traffic in the Los Angeles area, lanes cannot be closed for more than 55-hours. Therefore, reconstruction of the lanes have to be done utilizing 55-hour closures, which necessitates the use of rapid-set concrete for the pavement and the base materials. As a result, the EE was updated accordingly and resulted in higher costs.

In addition, the original estimate for removing the existing structural section was categorized as roadway excavation. Upon further review, it was determined that the removal of the existing structural section requires a specification change. As a result, the bid item and specifications were changed from roadway excavation to remove concrete and base. Since the unit cost for remove concrete and base is significantly higher than roadway excavation, the adjustment also contributed to the overall cost increase.

The refinements that were made to the structural section bid items amount to an increase of \$6,598,000 in capital costs.

CONSEQUENCES:

If this allocation request is not approved, the Department will not be able to advertise the contract to address the critical deficiencies of the existing roadway. To address all the deficiencies, the project will have to be reprogrammed, which will result in delays and could result in higher costs due to escalation.

FINANCIAL RESOLUTION:

Resolved, that \$19,318,000 be allocated from the Budget Act of 2023, Budget Act Item 2660-302-0890 and Non-Budget Act Item 2660-802-3290 for Construction Capital, and \$2,140,000 for Construction Support, to provide funds to advertise this SHOPP project.

Attachment

2.5 Highway Financial Matters

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5d.(6)	Allocation of Project with Construction Cost that Exceeds 20 Percent of the Programmed Amount			Resolution FP-24-89
1 \$21,458,000 Los Angeles 07-LA-5 30.6/31.6	In Burbank, from 0.1 mile north of San Fernando Boulevard to 0.3 mile south of Cohasset Street. <u>Outcome/Outputs</u> : Rehabilitate roadway. This project will improve safety and ride quality. 			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5d.(7), Action Item

Prepared By: Grace Magsayo
District 10 – Director

Subject: **ALLOCATION FOR PROJECT WITH COSTS THAT EXCEED THE
PROGRAMMED AMOUNT BY MORE THAN 20 PERCENT
PPNO 3433/EA 1H560 – ALPINE COUNTY – STATE ROUTE 88
RESOLUTION FP-24-90**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) allocation request for \$38,633,000 for Construction of the State Highway Operation and Protection Program (SHOPP) Maintenance Facilities project on State Route (SR) 88, in Alpine County, to advertise the project?

RECOMMENDATION:

The Department recommends that the Commission approve the requested allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located along SR 88 near Kirkwood, at the Caples Lake Maintenance Station (Facility), in Alpine County. The project will construct a new dormitory, remove the existing dormitory, reconstruct the existing sand shed structure, rehabilitate the generator building, and install an electrical vehicle charging station.

FUNDING AND PROGRAMMING STATUS:

In May 2020, this project was programmed in the SHOPP for \$24,948,000 in Construction Capital and \$4,148,000 in Construction Support for allocation in Fiscal Year 2023-24. In June 2024, the project requested an allocation time extension for the Construction phase. However, the request was deferred to the August 2024 meeting and approved for four months. In October 2024, the project received a 7-month allocation time extension amendment (totaling

11 months) for the Construction phase. In May 2025, the project received a 3-month allocation time extension amendment (totaling 14 months) for the Construction phase.

In April 2025, the Engineer's Estimate reflected the need of \$33,863,000 in Construction Capital (35.7 percent over the programmed amount) and \$4,770,000 in Construction Support (15.0 percent over the programmed amount). The Department plans to advertise the project in August 2025 and begin construction in October 2025. Construction is planned for 5 construction seasons with a duration of 765 working days. A concurrent project development expenditure time extension for the Plans, Specifications, and Estimate phase is on this month's Commission agenda.

REASON FOR COST INCREASE:

The Construction Capital estimate is greater than the programmed amount due to design refinements that resulted in an increase to the size of the new dormitory. The Construction Support estimate is greater than the programmed amount due to the increase in the number of working days as a result of the modified scope for the project.

Capital Cost Increase:

During the Project Approval and Environmental Document phase, the dormitory building size was increased to better support seasonal staff per the request of the Department's Field Maintenance office. The Facility is situated between Carson Pass and Carson Spur, both of which are prone to frequent avalanches. It is essential to have sufficient accommodations in place to ensure that all personnel can be sheltered in case of an emergency.

The Facility will also be utilized to house and train staff to use winter operation equipment to improve safety. The permanent intermittent staff come from various Maintenance Stations in the valley region which has limited experience with harsh environments.

In addition, the Facility serves as a winter and avalanche operations center where state and federal partners can gather during shifts as they patrol SR 88 and federal lands. This partnership plays a vital role for the Department's engagement, and offers an space as needed during extreme weather events that close the Carson Spur and Carson Pass. The Facility's location also serves the Kirkwood community during these events and plays a vital role in improving lives and communities through transportation.

As a result, the design of the dormitory and other buildings were revised. The dormitory was initially proposed to be approximately 16,300 square feet in size, with 24 rooms and a height of 3 stories. The size of this building was increased to approximately 20,500 square feet, with 36 rooms and a height of 5 stories to meet the needs of Maintenance staff. Construction of the new dormitory was moved to a new site as Maintenance staff wanted to maintain the use of the existing building during construction. This caused an increase in the architectural and structural design as well as structural elements due to increased loading. As a result of the additional stories, it also required an increase to the plumbing, electrical, heating, ventilation, and air conditioning, and fire suppression labor and material costs. Additional electronic safety and security features were added to ensure that the dormitory met the current building codes.

The change in the design of the dormitory affected the work for the Facility in general. The project plans required additional earthwork, water supply and sewer system revisions, and construction access points.

The design of the sand storage structure increased by approximately 250 square feet to accommodate new equipment and led to upgrading the mechanical systems which include exhaust fan requirements, and plumbing needs.

The State Fire Marshall permitting process resulted in changes to the design of the Facility. The size of the proposed generator building stayed the same since the Project Initiation Document phase but the cost was revised to accommodate the increase in the structural integrity of the building due to the larger roll up doors to accommodate the fire tank and generators. The current fire suppression system at the site required the addition of water storage tanks, a new fire pump, and a sprinkler system.

The refinements that were made to the design based on the needs of Maintenance staff and requirements from the State Fire Marshal permit, amount to an increase of \$8,915,000 in capital costs.

Support Cost Increase:

The support cost increase is due to the addition of 415 working days (from 350 to 765) as a result of the increase in new dormitory building floors and relocation of the building to a new location on site. In addition, resources are also required to install an electrical vehicle charging station. This has led to an increase of \$622,000 in support costs.

CONSEQUENCES:

If this allocation request is not approved, the Department will not be able to advertise the contract to address the critical deficiencies of the existing Maintenance Station which is too small to house staff that work in the area. To address all of the deficiencies, the project will have to be reprogrammed, which will result in delays and could result in higher costs due to escalation.

FINANCIAL RESOLUTION:

Resolved, that \$33,863,000 be allocated from the Budget Act of 2024, Budget Act Item 2660-303-0042 for Construction Capital, and \$4,770,000 for Construction Support, to provide funds to advertise this SHOPP project.

Attachment

2.5 Highway Financial Matters

2.5 Highway Financial Matters				PPNO				
Project No.				Program/Year				
Allocation Amount				Phase				
County	Location			Prgm'd Amount		Budget Year		
Dist-Co-Rte	Project Description			Project ID		Item # Fund Type		Amount by
Postmile				Adv Phase		Program Code		Fund Type
EA								
2.5d.(7)				Allocation of Project with Construction Cost that Exceeds 20 Percent of the Programmed Amount				Resolution FP-24-90
1	Near Kirkwood, at the Caples Lake Maintenance Station.			10-3433		001-0042 SHA		\$4,770,000
\$38,633,000	Outcome/Outputs: Reconstruct a dormitory and sand shed structures, and rehabilitate a generator building, and install new Zero Emission Vehicle charging station.			SHOPP/23-24		20.10.201.352		
				CON ENG				
Alpine				\$4,148,000		2024-25		
10-Alp-88				CONST		303-0042 SHA		\$33,863,000
1.9	Preliminary Engineering			\$24,948,000		20.20.201.352		
		Budget	Expended	1017000184				
		\$853,000	\$742,891	3,4				
		\$2,979,600	\$2,804,167	1H560				
		\$50,000	\$6,352					
Performance Measure:								
Planned: 1.0, Actual: 1.0 Location(s)								
CEQA - ND, 02/28/2022; Re-validation 05/22/2024								
NEPA - CE, 02/10/2022; Re-validation 05/28/2024								
Future consideration of funding approved under Resolution E-22-48; July 2022.								
Amendment to time extension for an additional ten months, for a total of fourteen months, for CONST and CON ENG approved under Waiver 25-87; May 2025.								
As part of this allocation request, the Department is requesting to extend the completion of CONST and CON ENG an additional 24 months beyond the 36 month deadline.								
Performance Measure: Location(s)								
	Unit	Good	Fair	Poor	Quantity			
Existing Condition	Square feet	0.0	0.0	18,620.0	18,620.0			
Post Condition	Square feet	22,032.0	0.0	0.0	22,032.0			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5d.(8), Action Item

Prepared By: Grace Magsayo
District 10 - Director

Subject: **ALLOCATION FOR PROJECT WITH COSTS THAT EXCEED THE
PROGRAMMED AMOUNT BY MORE THAN 20 PERCENT
PPNO 3515/EA 1L860 – VARIOUS COUNTIES – VARIOUS STATE ROUTES
RESOLUTION FP-24-91**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) allocation request for \$3,324,000 for Construction of the State Highway Operation and Protection Program (SHOPP) Transportation Management Systems project on various State Routes (SRs), in various counties, to advertise the project?

RECOMMENDATION:

The Department recommends that the Commission approve the requested allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project locations are spread out on SRs 16, 49, 88, 104, 108, 124, 132, and 140, at various locations, in Mariposa, Amador, and Tuolumne counties. The project will replace and install new transportation management system (TMS) elements consisting of vehicle detection systems and closed-circuit television (CCTV) cameras, and construct a maintenance vehicle pullout (MVP).

FUNDING AND PROGRAMMING STATUS:

In March 2022, this project was programmed in the SHOPP for \$1,900,000 in Construction Capital and \$630,000 in Construction Support for allocation in Fiscal Year 2024-25.

In April 2025, the Engineer's Estimate (EE) reflected the need of \$2,600,000 in Construction Capital (36.8 percent over the programmed amount) and \$724,000 in Construction Support (14.9 percent over the programmed amount). The Department plans to advertise the project in July 2025 and begin construction in October 2025. Construction is planned for one construction season with a duration of 85 working days.

REASON FOR COST INCREASE:

The Construction Capital and Support estimates are greater than the programmed amount due to the refinement of the estimate to reflect current market conditions and an increase in the number of working days for the project.

Capital Cost Increase:

During the Project Approval and Environmental Document phase, the cost estimate increased as additional field work provided more detailed information for the installation of electrical field elements at 12 locations with consideration for environmental and right of way impacts. The refined cost estimate at this time was within 120 percent of the programmed amount.

During the Plans, Specifications, and Estimate phase, the Department's Electrical Maintenance office requested an MVP at one of the locations which was included in the scope of work to provide a safe area for electrical maintenance staff. The MVP was required due to the roadway geometrics and placement of the CCTV. The estimate was refined again to reflect current market conditions. The addition of an MVP and the updated EE resulted in significant cost increases to mobilization, time-related overhead, traffic electrical items, and department-furnished materials for a total increase of \$700,000 in capital costs.

Support Cost Increase:

The support cost increase is due to the addition of 25 working days (from 60 to 85) as a result of the refinement to the workplan. This has led to an increase of \$94,000 in support costs.

CONSEQUENCES:

If this allocation request is not approved, the Department will not be able to advertise the contract to address the critical deficiencies of the existing TMS elements to provide accurate traffic volumes which is the fundamental tool for transportation planning and engineering for infrastructure planning and traffic analysis. To address all of the deficiencies, the project will have to be reprogrammed, which will result in delays and could result in higher costs due to escalation.

FINANCIAL RESOLUTION:

Resolved, that \$2,600,000 be allocated from Non-Budget Act Item 2660-802-3290 for Construction Capital, and \$724,000 for Construction Support, to provide funds to advertise this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type																				
2.5d.(8)	Allocation of Project with Construction Cost that Exceeds 20 Percent of the Programmed Amount		Resolution FP-24-91																					
1 \$3,324,000	In Mariposa, Amador, and Tuolumne Counties, on Routes 49, 16, 88, 104, 108 124, 132 and 140 at various locations. <u>Outcome/Outputs:</u> Replace and install new Transportation Management System (TMS) elements.	10-3515 SHOPP/24-25 CON ENG \$630,000	505-3290 RMRA 20.10.201.315	\$724,000																				
Mariposa 10-Mpa-49 Var	Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	<u>Budget</u> \$520,000 <u>Expended</u> \$371,063 \$1,900,000 1020000179 3,4 1L860	2017-18 802-3290 RMRA 20.20.201.315	\$2,600,000																				
<u>Performance Measure:</u> Planned: 14.0, Actual: 14.0 Field element(s)																								
CEQA - CE, 08/09/2023; Re-validation 04/23/2025 NEPA - CE, 08/09/2023; Re-validation 04/23/2025																								
<table border="1"> <tr> <td colspan="5">Performance Measure: Transportation Management Systems (TMS)</td></tr> <tr> <td></td><td><u>Unit</u></td><td><u>Good (Operational)</u></td><td><u>Poor (Not Operational)</u></td><td><u>Quantity</u></td></tr> <tr> <td>Existing Condition</td><td>Field element(s)</td><td>0.0%</td><td>100.0%</td><td>10.0</td></tr> <tr> <td>Post Condition</td><td>Field element(s)</td><td>100.0%</td><td>0.0%</td><td>14.0</td></tr> </table>					Performance Measure: Transportation Management Systems (TMS)						<u>Unit</u>	<u>Good (Operational)</u>	<u>Poor (Not Operational)</u>	<u>Quantity</u>	Existing Condition	Field element(s)	0.0%	100.0%	10.0	Post Condition	Field element(s)	100.0%	0.0%	14.0
Performance Measure: Transportation Management Systems (TMS)																								
	<u>Unit</u>	<u>Good (Operational)</u>	<u>Poor (Not Operational)</u>	<u>Quantity</u>																				
Existing Condition	Field element(s)	0.0%	100.0%	10.0																				
Post Condition	Field element(s)	100.0%	0.0%	14.0																				

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(4), Action Item

Prepared By: Scott Eades
District 05 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CAPITAL OUTLAY SUPPORT PHASE**
PPNO 2798/EA 1J900 – SANTA BARBARA COUNTY –
UNITED STATES HIGHWAY 101
RESOLUTION FA-24-94

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$1,600,000 in Capital Outlay Support (COS), for the State Highway Operation and Protection Program (SHOPP) Pavement Preservation project on United States Highway (US) 101, in Santa Barbara County, to complete the Plans, Specifications, and Estimate (PS&E) phase?

RECOMMENDATION:

The Department recommends that the Commission approve this request for a COS supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on US 101 in and near the cities of Santa Barbara and Goleta, from south of the Milpas Street Undercrossing to North Fairview Avenue, in Santa Barbara County. The project will rehabilitate the pavement and drainage systems, replace bridge rail and concrete barrier, perform structure seismic restoration work, upgrade pedestrian facilities, install transportation management system (TMS) elements, and enhance highway worker safety. The TMS elements consist of traffic count loops and a closed-circuit television camera. The enhanced highway worker safety elements consist of relocating irrigation facilities, contrast surface treatment at gore areas, a maintenance vehicle pullout, slope paving under bridges, and vegetation control under guardrail.

FUNDING AND PROGRAMMING STATUS:

In May 2020, this project's PS&E phase was programmed for \$3,636,000 in the SHOPP for allocation in Fiscal Year 2022-23. In October 2021, the project was amended to update the scope to include concrete barrier work to improve safety. In March 2023, the project received a PS&E allocation for \$3,736,000. The project has not received any G-12 funds for the PS&E phase. The PS&E phase is currently 80 percent complete, with a remaining budget of \$260,000, which is insufficient to complete the phase. If this request for additional funding is approved, it is anticipated the project will be Ready to List (RTL) by June 2026, and complete the remaining PS&E activities by January 2027.

REASON FOR COST INCREASE:

The project realized cost increases due to additional resources that are required to address various assets that were incorporated through various amendments.

The project proposes to rehabilitate the pavement and drainage systems, replace bridge rail and concrete barrier, perform structure seismic restoration work, upgrade pedestrian facilities, install TMS elements, and enhance highway worker safety along an approximately 10-mile segment of US 101. This project has processed multiple amendments, some of which are due to added Infrastructure Investment and Jobs Act funding, increasing the construction costs to address asset needs which include a weigh-in-motion system, curb ramps, crosswalks, refined structural bridge seismic retrofit needs, unique specialty design details, electrical needs, and landscape mitigation needs. Each of these design elements has added layers of review and complexity during PS&E development. The funding for the Construction phase was addressed through these amendments. However, the PS&E phase could not be adjusted given its programmed year.

The project includes 96 concrete transition barriers which require unique designs from the Department's Division of Engineering Services (DES) group and were not initially included as design details for the plans. The DES group also encountered unanticipated seismic retrofit challenges involving the bridge foundation for a column, abutment deficiencies, and more extensive column retrofit needs to meet the current seismic design requirements. These findings during design required additional resources for refined structural analysis by DES to address the bridge retrofit scope, and revisions to the traffic handling to perform the work. In addition, the Department's Landscape Architecture group spent additional efforts during the design phase to successfully include landscape mitigation needs within the project in lieu of a more costly child project later.

During the prior phases, the Department identified storm-drain work near the Union Pacific Railroad (UPRR), but determined there would be minimal to no impacts with the railroad right of way. However, during PS&E development, it was found that the storm-drain work encroaches into UPRR right of way, and coordination efforts increased beyond just the anticipated flagging needs. The additional work involves a permanent drainage easement and a temporary construction easement to facilitate light grading at the outlet.

Therefore, supplemental funds in the amount of \$1,600,000 are required to address these design elements, finalize the contract documents to RTL the project, and complete the remaining PS&E activities through contract approval by January 2027.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to RTL the project which will address critical deficiencies of the existing pavement and roadside elements along this segment of US 101. To address all of the deficiencies, the project would need to be reprogrammed in a future SHOPP cycle which could lead to the potential of a project cost increase.

FINANCIAL RESOLUTION:

Resolved, that \$1,600,000 be allocated to provide funds to complete the PS&E phase for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project #		PPNO		
Allocation Amount		Program		
Recipient		Phase		
County		Program Code		This
Dist-Co-Rte	Location	Project ID		Supplemental
Postmile	Project Description	Adv Phase	Original Amount	Request
EA				
2.5e.(4) COS Supplemental Funds for Previously Voted Project			Resolution FA-24-94	
1	In and near the cities of Santa Barbara and Goleta, from south of Milpas Street Undercrossing to North Fairview Avenue.	05-2798	\$3,736,000	\$1,600,000
\$1,600,000		SHOPP		
		PS&E		
Department of	Outcome/Output: Rehabilitate pavement, replace bridge rail and concrete barrier, seismic restoration, install	20.10.201.121		
Transportation		0518000085		
Santa Barbara	Transportation Management System (TMS) elements,	1		
05-SB-101	rehabilitate drainage systems, and enhance highway worker safety.	1J900		
12.4/22.6				
	Supplemental funds are needed to complete PS&E.			
	CEQA - CE, 03/16/2023			
	NEPA - CE, 3/16/2023			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(5), Action Item

Prepared By: Scott Eades
District 05 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CAPITAL OUTLAY SUPPORT PHASE
PPNO 2853/EA 1K010 – MONTEREY COUNTY – STATE ROUTE 1
RESOLUTION FA-24-95**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$620,000 in Capital Outlay Support (COS), for the State Highway Operation and Protection Program (SHOPP) Major Damage (Permanent Restoration) project on State Route (SR) 1, in Monterey County, to complete the Plans, Specifications, and Estimate (PS&E) phase?

RECOMMENDATION:

The Department recommends that the Commission approve this request for a COS supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on SR 1 near Lucia, approximately one mile south of Big Creek Bridge, in Monterey County. The project will construct a tieback wall, restore the roadway and drainage systems, and install erosion control.

FUNDING AND PROGRAMMING STATUS:

In August 2019, this project's PS&E phase was programmed for \$1,607,000 in the SHOPP for allocation in Fiscal Year 2020-21. In June 2020, the project was amended to move out the allocation of the PS&E and Right of Way Support phases to 2022-23 due to a delay to continue environmental studies to identify potential impacts and mitigation strategies, and increased PS&E to \$1,743,000 due to the escalation associated with postponement. In October 2022, the project received a PS&E allocation for \$1,663,000. The project has not received any G-12

funds for the PS&E phase. The PS&E phase is currently 60 percent complete, with a remaining budget of \$371,000, which is insufficient to complete the phase. If this request for additional funding is approved, it is anticipated to Ready to List (RTL) the project by November 2025, and complete the remaining PS&E activities by June 2026.

REASON FOR COST INCREASE:

The project realized cost increases due to changes to the design as a result of the changes to the geotechnical conditions at the site.

The project proposes to construct a tieback wall, restore the roadway and drainage systems, and install erosion control at one location along SR 1. This project is located in a highly geotechnically complex area. The southern portion of the project is in the slip plane of an unnamed moving landslide. Since this project was programmed, the Regents Slide occurred and has not been repaired due to ongoing geotechnical challenges (approximately 0.25 miles north of project site). Since project initiation, the project area has experienced consecutive years of heavy winter rainfall. Ongoing geotechnical investigations have been underway at this site. The Department has evaluated multiple options to address geotechnical movement. As a result, the Department is designing additional earth retaining structures alongside the retaining wall that will be replaced. These proposed retaining walls will stabilize the area within the project limits, and allow the roadway prism and drainage elements to be restored.

Therefore, supplemental funds in the amount of \$620,000 are required to complete the design of the additional earth retaining structures, and finalize the contract documents to RTL the project, and complete the remaining PS&E activities through contract approval by June 2026.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to RTL the project which will address the permanent restoration of the major damage that occurred to the existing roadway and roadside elements at this location on SR 1. To address all of the restoration work, the project would need to be reprogrammed in a future SHOPP cycle which could lead to the potential of a project cost increase.

FINANCIAL RESOLUTION:

Resolved, that \$620,000 be allocated to provide funds to complete the PS&E phase for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project #		PPNO		
Allocation Amount		Program		
Recipient		Phase		
County		Program Code		
Dist-Co-Rte	Location	Project ID		This
Postmile	Project Description	Adv Phase	Original Amount	Supplemental
		EA		Request
2.5e.(5)	COS Supplemental Funds for Previously Voted Project			Resolution FA-24-95
1	Near Lucia, from 0.6 mile to 0.8 mile south of Big Creek Bridge.	05-2853	\$1,663,000	\$620,000
\$620,000		SHOPP		
		PS&E		
Department of	<u>Outcome/Output:</u> Construct tieback wall, restore roadway	20.10.201.131		
Transportation	and drainage facilities, and install permanent erosion control	0518000105		
Monterey	measures.	1		
05-Mon-1		1K010		
27.5/27.7	Supplemental funds are needed to complete PS&E.			
	CEQA - CE, 09/21/2020			
	NEPA - CE, 9/21/2020			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(8), Action Item

Prepared By: Gloria Roberts
District 07 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CAPITAL OUTLAY SUPPORT PHASE
PPNO 5484/EA 35320 – LOS ANGELES COUNTY – INTERSTATE 405
RESOLUTION FA-24-96**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$300,000 in Capital Outlay Support (COS), for the State Highway Operation and Protection Program (SHOPP) Pavement Preservation project on Interstate 405 (I-405), in Los Angeles County, to complete the Plans, Specifications, and Estimate (PS&E) phase?

RECOMMENDATION:

The Department recommends that the Commission approve this request for a COS supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on I-405 in and near the cities of Long Beach, Signal Hill, Los Angeles, and Carson, from the Orange County line to south of I-110, in Los Angeles County. The project will rehabilitate the pavement, bridge, transportation management system (TMS) elements, and drainage systems; upgrade signs, lighting, and pedestrian facilities; improve safety, and replace copper cabling with fiber optics, landscaping, and irrigation systems; install rock blanket and minor concrete paving; and construct maintenance vehicle pullouts.

FUNDING AND PROGRAMMING STATUS:

In May 2020, this project's PS&E phase was programmed for \$6,043,000 in the SHOPP for allocation in Fiscal Year 2021-22. In May 2021, the project was amended to decrease PS&E to \$4,800,000 due to a refined support cost estimate, design efficiency, and a revised construction

staging plan. In June 2022, the project received a PS&E allocation for \$5,664,000. In December 2022, the project was amended again to revise the scope due to additional assets that were identified during the Project Approval and Environmental Document phase. In November 2023, the project received G-12 funds in the amount of \$766,400 for the PS&E phase. In June 2024, the project requested an allocation time extension for the Construction phase. However, the request was deferred to the August 2024 meeting and approved for four months. In September 2024, the project achieved Ready to List (RTL).

In October 2024, the project received an allocation for the Construction phase. In March 2025, bids were opened and analyzed for validation. The Department received three bids but had to reject all bids in the best interest of the State. Therefore, the Department decided to re-advertise the contract after it is repackaged. In May 2025, the project received a 9-month award time extension for the Construction phase. A supplemental funds request for Construction Capital will be required to re-advertise the project at the August 2025 Commission meeting.

The PS&E phase has a remaining budget of \$48,000, which is insufficient to repackage, re-advertise, and approve the contract. If this request for additional funding is approved, it is anticipated that the project will have a revised PS&E package by August 2025, and complete the remaining PS&E activities by January 2026. A concurrent project development expenditure time extension for the PS&E phase is on this month's Commission agenda.

REASON FOR COST INCREASE:

The project realized cost increases due to the rejection of the bid results, which have resulted in repackaging the PS&E, and will require re-advertising, awarding, and approving the contract.

The project proposes to rehabilitate the pavement, bridge, TMS elements, and drainage systems; upgrade signs, lighting, and pedestrian facilities; improve safety, and replace copper cabling with fiber optics, landscaping, and irrigation systems; install rock blanket and minor concrete paving; and construct maintenance vehicle pullouts along an approximately 13-mile segment of I-405.

In January 2025, when the contract was advertised, the Department received 210 bidder inquiries from 13 potential contractors. Based on these inquiries, the Department issued two addenda to address the discrepancies and inconsistencies that were encountered during the bidders' review process. The addenda revised approximately 200 plan sheets, including layouts, typical cross sections, traffic signing and striping, specifications, and bid quantities. In March 2025, bids were opened, and three bids were received. The bids were analyzed for validation, and they were found to be non-responsive, front loaded, or too high to award to the lowest responsive bidder. Therefore, the Department decided to reject all the bids and re-advertise the contract after it is repackaged. The repackaging of the PS&E will require incorporating approximately 200 addenda sheets, updating the PS&E to meet the mid-cycle standard plans released in April 2025, revising the PS&E to use the latest compliant temporary barrier, and upgrading the electrical pull boxes to prevent vandalism and theft. The Department

will also make plan changes to address several bidder inquiries regarding the construction of the overhead signs behind existing soundwalls.

The Department has almost depleted the remaining budget to complete the remaining PS&E activities of the original contract. Therefore, supplemental funds in the amount of \$300,000 are required to complete the repackaging of the contract documents, re-advertise the contract, and complete the remaining PS&E activities through contract approval by January 2026.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to re-advertise and award the contract that addresses the critical deficiencies of the existing roadway and roadside elements along this segment of I-405. To address all of the deficiencies, the project will need to be reprogrammed in a future SHOPP cycle which could lead to the potential of a project cost increase.

FINANCIAL RESOLUTION:

Resolved, that \$300,000 be allocated to provide funds to complete the PS&E phase for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project #		PPNO		
Allocation Amount		Program		
Recipient		Phase		
County		Program Code		This
Dist-Co-Rte	Location	Project ID		Supplemental
Postmile	Project Description	Adv Phase	Original Amount	Request
EA				
2.5e.(8) COS Supplemental Funds for Previously Voted Project			Resolution FA-24-96	
1	In and near the cities of Long Beach, Signal Hill, Los Angeles, and Carson, from the Orange County line to south of Route 110.	07-5484	\$6,430,400	\$300,000
\$300,000		SHOPP		
		PS&E		
Department of	Outcome/Output: Rehabilitate pavement, upgrade signs, rehabilitate bridge, upgrade lighting, improve safety, rehabilitate Transportation Management System (TMS) elements and replace copper cabling with fiber, rehabilitate culverts, replace landscape and irrigation system, rock blanket paving, minor concrete paving, grind concrete pavement, construct maintenance vehicle pullouts, and upgrade facilities to Americans with Disabilities Act (ADA) standards. This project is necessary to extend pavement service life and improve ride quality.	20.10.201.121		
Transportation		0718000253		
Los Angeles		1		
07-LA-405		35320		
0.0/12.8				
	Supplemental funds are needed to complete PS&E.			
	CEQA - CE, 01/20/2022; Re-validation 06/24/2024			
	NEPA - CE, 1/20/2022; Re-validation 6/24/2024			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(9), Action Item

Prepared By: Ryan Dermody
District 09 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CAPITAL OUTLAY SUPPORT PHASE
PPNO 2648/EA 37460 – MONO COUNTY – UNITED STATES HIGHWAY 395
RESOLUTION FA-24-97**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$2,095,000 in Capital Outlay Support (COS), for the State Highway Operation and Protection Program (SHOPP) Roadway Rehabilitation project on United States Highway (US) 395, in Mono County, to complete the Plans, Specifications, and Estimate (PS&E) phase?

RECOMMENDATION:

The Department recommends that the Commission approve this request for a COS supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on US 395 in and near Bridgeport, from south of State Route 182 to Buckeye Road, in Mono County. The project will rehabilitate the pavement and drainage systems, upgrade pedestrian facilities, and replace guardrail.

FUNDING AND PROGRAMMING STATUS:

In May 2020, this project's PS&E phase was programmed for \$695,000 in the SHOPP for allocation in Fiscal Year 2022-23. In June 2023, the project received a 2-month allocation time extension for the PS&E and Right of Way Support phases. In August 2023, the project received a PS&E allocation for \$695,000. The project has not received any G-12 funds for the PS&E phase. The PS&E phase is currently 30 percent complete, with a remaining budget of \$197,000, which is insufficient to complete the phase. If this request for additional funding is

approved, it is anticipated to Ready to List (RTL) the project by February 2026, and complete the remaining PS&E activities by September 2026.

REASON FOR COST INCREASE:

The project realized cost increases due to the need for consultant staff to complete the PS&E phase as scheduled.

The project proposes to rehabilitate the pavement and drainage systems, upgrade pedestrian facilities, and replace guardrail along an approximately 4.5-mile segment of US 395. In June 2024, during the PS&E phase, the workplan was found to not capture all of the required activities and related resources. The workplan omitted base mapping, and did not include complete funding for design and structure work. The discrepancy should have been identified, and the workplan updated prior to the allocation request. The Department explored brokering staff from neighboring Districts to assist, however, competing priorities and staffing levels required the use of a consultant. Since the allocation, the Department has been working on achieving 30 percent design, delivering right of way requirements, performing structural work, obtaining permits, and conducting significant public engagement. However, to keep the project on schedule, the Department will require consultant staff to complete work and achieve RTL as anticipated. The use of consultant staff with Department oversight is a significant component of the supplemental request.

Therefore, supplemental funds in the amount of \$2,095,000 are required to continue with the design, develop the contract documents to RTL the project, and complete the remaining PS&E activities through contract approval by September 2026.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to RTL the project which will address the critical deficiencies of the existing roadway and roadside elements along this segment of US 395. To address all of the deficiencies, the project will need to be reprogrammed in a future SHOPP cycle which could lead to the potential of a project cost increase.

FINANCIAL RESOLUTION:

Resolved, that \$2,095,000 be allocated to provide funds to complete the PS&E phase for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project #		PPNO		
Allocation Amount		Program		
Recipient		Phase		
County		Program Code		
Dist-Co-Rte	Location	Project ID		This
Postmile	Project Description	Adv Phase	Original Amount	Supplemental
		EA		Request
2.5e.(9)	COS Supplemental Funds for Previously Voted Project			Resolution FA-24-97
1	Near Bridgeport, from south of Route 182 to Buckeye Road.	09-2648	\$695,000	\$2,095,000
\$2,095,000		SHOPP		
	<u>Outcome/Output:</u> Rehabilitate pavement, upgrade facilities to	PS&E		
Department of	Americans with Disabilities Act (ADA) standards, replace	20.10.201.120		
Transportation	guardrail, and rehabilitate drainage systems.	0918000018		
Mono		1		
09-Mno-395	Supplemental funds are needed to complete PS&E.	37460		
76.00/80.60	CEQA - MND, 05/23/2023; FEIR, 12/30/2024			
	NEPA - CE, 05/23/2023			
	Future consideration of funding approved under			
	Resolution E-23-113; August 2023.			
	Concurrent consideration of funding under			
	Resolution E-25-40; June 2025.			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(10), Action Item

Prepared By: Grace Magsayo
District 10 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CAPITAL OUTLAY SUPPORT PHASE
PPNO 1044/EA 0J720 – ALPINE COUNTY – STATE ROUTES 4 AND 89
RESOLUTION FA-24-98**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$158,000 in Capital Outlay Support (COS), for the State Highway Operation and Protection Program (SHOPP) Pavement Preservation project on State Route (SR) 4 and SR 89, in Alpine County, to complete the Plans, Specifications, and Estimate (PS&E) phase?

RECOMMENDATION:

The Department recommends that the Commission approve this request for a COS supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on SR 4 and SR 89 near Bear Valley, on SR 4 from the Calaveras County line to SR 89, and on SR 89, north of SR 4 (at postmile 10.8), in Alpine County. The project will rehabilitate the pavement and drainage systems, replace guardrail and signs, place rock slope protection, install an animal crossing, and enhance highway worker safety.

FUNDING AND PROGRAMMING STATUS:

In May 2020, this project's PS&E phase was programmed for \$2,196,000 in the SHOPP for allocation in Fiscal Year 2021-22. In June 2022, the project received a 6-month allocation time extension for the PS&E and Right of Way (RW) Support phases. In October 2022, the project received a PS&E allocation for \$2,322,000. In June 2024, the project requested an allocation time extension for the Construction phase. However, the request was deferred to the

August 2024 meeting and approved for 6 months. In August 2024, the project received G-12 funds in the amount of \$432,200. In December 2024, the project received an 8-month allocation time extension amendment (for a total of 14 months) for the Construction phase. The PS&E phase is currently 95 percent complete, with a remaining budget of \$62,000, which is insufficient to complete the phase. If this request for additional funding is approved, it is anticipated to Ready to List (RTL) the project by June 2025, and complete the remaining PS&E activities by January 2026.

REASON FOR COST INCREASE:

The project realized cost increases due to the incorporation of an animal crossing and delay in processing the United States Forest Service (USFS) permit.

The project proposes to rehabilitate the pavement and drainage systems, replace guardrail and signs, place rock slope protection, install an animal crossing, and enhance highway worker safety along an approximately 32-mile segment of SR 4 and install a maintenance vehicle pullout at one location on SR 89. The project requires permits from two resource agencies to perform the proposed work on this project. The two agencies are the California Department of Fish and Wildlife (CDFW) and the USFS.

The CDFW permit requires wildlife mitigation. In November 2023, after the PS&E phase began, an animal crossing option was introduced by CDFW to mitigate the wildlife impacts. After consultation with CDFW, the option proposes to modify an existing culvert to include a pathway for animals to use. This required detailed plans since standard details do not exist for this type of animal crossing. Special details were developed which required additional time for review and approval of the structural details. The extent of resources required to design the animal crossing could not be assessed at the earlier stage of the project.

Temporary construction encroachment permits are required from the USFS to determine the construction window for this project. The permits are required by two regional divisions and the process took longer than anticipated to acquire. The Department has been working with USFS for the temporary construction encroachment permits since December 2023. The USFS had concerns with the duration of lane closures and selection of staging areas. After lengthy coordination with USFS, adjustments were made to the traffic closure specifications and staging areas. In February 2025, the special use permit was approved. The 14-month delay required additional resources to complete the PS&E and RW Support phases.

The Design and Office Engineering teams need additional time to review and revise the PS&E package with the most recent standard plans and specification updates after the agreement was reached with the USFS. The current budget is sufficient to achieve RTL. However, supplemental funds in the amount of \$158,000 are required to complete the remaining PS&E activities from advertising through approval of the contract by January 2026.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to RTL the project that addresses the critical deficiencies of the existing pavement to preserve and extend the service life and improve ride quality, and roadside elements along this segment of SR 4 and SR 89. To address all of the deficiencies, the project will need to be reprogrammed in a future SHOPP cycle which could lead to the potential of a project cost increase.

FINANCIAL RESOLUTION:

Resolved, that \$158,000 be allocated to provide funds to complete the PS&E phase for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project #		PPNO		
Allocation Amount		Program		
Recipient		Phase		
County		Program Code		
Dist-Co-Rte	Location	Project ID		This
Postmile	Project Description	Adv Phase	Original Amount	Supplemental
		EA		Request
2.5e.(10)	COS Supplemental Funds for Previously Voted Project			Resolution FA-24-98
1	Near Bear Valley, from Calaveras County line to Route 89; also on Route 89 at 0.9 mile north of Route 4 (PM 10.84).	10-1044	\$2,754,200	\$158,000
\$158,000		SHOPP		
		PS&E		
Department of	<u>Outcome/Output:</u> Rehabilitate pavement, replace guardrail and signs, place Rock Slope Protection (RSP), rehabilitate drainage systems, and enhance highway worker safety.	20.10.201.121		
Transportation		1018000271		
Alpine		1		
10-Alp-4	(G13 Contingency)	0J720		
R0.0/31.7				
	Supplemental funds are needed to complete PS&E.			
	CEQA - MND, 09/01/2022; Re-validation 02/14/2024			
	NEPA - CE, 09/1/2022; Re-validation 02/14/2024			
	Future consideration of funding approved under Resolution E-22-83; October 2022.			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(11), Action Item

Prepared By: Grace Magsayo
District 10 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CAPITAL OUTLAY SUPPORT PHASE**
PPNO 3439/EA 0V690 – MERCED COUNTY – STATE ROUTE 59
RESOLUTION FA-24-99

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$250,000 in Capital Outlay Support (COS), for the State Highway Operation and Protection Program (SHOPP) Pavement Preservation project on State Route (SR) 59, in Merced County, to complete the Plans, Specifications, and Estimate (PS&E) phase?

RECOMMENDATION:

The Department recommends that the Commission approve this request for a COS supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on SR 59 near the City of Merced, from SR 152 to Reilly Road, in Merced County. The project will rehabilitate the pavement, drainage systems, and bridge decks; upgrade guardrail and lighting; replace sign panels and bridge; and construct rumble strips.

FUNDING AND PROGRAMMING STATUS:

In May 2020, this project's PS&E phase was programmed for \$1,962,000 in the SHOPP for allocation in Fiscal Year 2022-23. In August 2022, the project received a PS&E allocation for \$1,962,000. In October 2023, the project received G-12 funds in the amount of \$396,200. In June 2024, the project requested a 19-month allocation time extension for the Construction phase. However, the request was deferred to the August 2024 meeting and approved for

12 months. In March 2025, the project received a 7-month allocation time extension amendment (totaling 19 months) for the Construction phase. The PS&E phase is currently 90 percent complete, with a remaining budget of \$148,000, which is insufficient to complete the phase. If this request for additional funding is approved, it is anticipated to Ready to List (RTL) the project by November 2025, and complete the remaining PS&E activities by May 2026. A concurrent project development expenditure time extension for the PS&E phase is on this month's Commission agenda.

REASON FOR COST INCREASE:

The project realized cost increases due to task orders that were required to perform hydraulic and geotechnical studies, design modifications to address flooding events, additional resources to process the 408 permit, and updates to the PS&E package with the most recent standards.

The project proposes to rehabilitate the pavement, drainage systems, and bridge decks; upgrade guardrail and lighting; replace sign panels and bridge; and construct rumble strips along an approximately 12-mile segment of SR 59.

The replacement of Mariposa Creek Bridge was the critical path on this project. All of the early PS&E work to establish elevations and design the bridge required hydraulic, structure hydraulic, and geotechnical studies. Some of this work was performed through task orders which required resources above what was initially anticipated.

With the flooding events in early 2023, the Department had to reconfirm the established elevations and address the drainage of floodwater surrounding the Mariposa Creek Bridge. The flooding events also helped identify other areas within the project limits that required to be addressed. This work was not anticipated and required additional hydraulic studies and recommendations to finalize the design.

With several factors effecting the resources, G-12 funds for the PS&E phase were requested in October 2023. It was anticipated that the project would reach RTL within the 12-month allocation time extension for the Construction phase. However, after receiving the G-12 funding, the Department experienced delays in acquiring the 408 permit, and resulted in an amended 7-month allocation time extension to reach RTL. The Department was notified by the Army Corps of Engineers that the Pacific Gas and Electric (PG&E) Company is required to apply for a 408 permit separately. As a result, the Department's Environmental and Hydraulics office coordinated the permit application efforts with PG&E and the Central Valley Flood Protection Board to facilitate the separate permit applications for this project and provide information as needed. This required additional resources beyond what was anticipated.

The PS&E package was developed utilizing the standard plans and specifications from earlier versions. Since the project was delayed by 19 months, the PS&E package was revisited and updated to the most recent release of standard plans and specifications.

Therefore, supplemental funds in the amount of \$250,000 are required to complete the permit application process, contract documents to RTL the project, and the remaining PS&E activities

from advertising through approval of the contract by May 2026.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to RTL the project that addresses the critical deficiencies of the existing pavement and roadside elements, and flooding issues along this segment of SR 59. To address all of the deficiencies, the project will need to be reprogrammed in a future SHOPP cycle which could lead to the potential of a project cost increase.

FINANCIAL RESOLUTION:

Resolved, that \$250,000 be allocated to provide funds to complete the PS&E phase for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project #		PPNO		
Allocation Amount		Program		
Recipient		Phase		
County		Program Code		This
Dist-Co-Rte	Location	Project ID		Supplemental
Postmile	Project Description	Adv Phase	Original Amount	Request
		EA		
2.5e.(11)	COS Supplemental Funds for Previously Voted Project			Resolution FA-24-99
1	Near Merced, from Route 152 to Reilly Road.	10-3439	\$2,358,200	\$250,000
\$250,000		SHOPP		
Department of	<u>Outcome/Output:</u> Rehabilitate pavement, upgrade guardrail	PS&E		
Transportation	and lighting, replace sign panels, construct rumble strips,	20.10.201.121		
Merced	rehabilitate drainage, replace one bridge, and rehabilitate	1018000284		
10-Mer-59	two bridge decks.	1		
R0.0/12.1	(G13 Contingency)	0V690		
	Supplemental funds are needed to complete PS&E.			
	CEQA - MND, 04/14/2022			
	NEPA - CE, 04/18/2022			
	Future consideration of funding approved under			
	Resolution E-22-68; August 2022.			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(12), Action Item

Prepared By: Ann Fox
District 11 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CAPITAL OUTLAY SUPPORT PHASE
PPNO 1352/EA 43063 – SAN DIEGO COUNTY – STATE ROUTE 75 AND
INTERSTATE 5
RESOLUTION FA-24-100**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$14,800,000 in Capital Outlay Support (COS), for the State Highway Operation and Protection Program (SHOPP) Bridge Rehabilitation and Replacement project on State Route (SR) 75 and Interstate 5 (I-5), in San Diego County, to complete the Plans, Specifications, and Estimate (PS&E) phase?

RECOMMENDATION:

The Department recommends that the Commission approve this request for a COS supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on SR 75 and I-5 in the City of San Diego, at the San Diego-Coronado Bay Bridge (No. 57-0857) and N75-N5 Connector Overcrossing (No. 57-0912G); and on Interstate 5 at the S5-S75 Connector Overcrossing (No. 57-0939H), in San Diego County. The project will install suicide deterrent fencing along the bridges.

FUNDING AND PROGRAMMING STATUS:

In January 2023, this project's PS&E phase was programmed for \$14,000,000 in the SHOPP for allocation in Fiscal Year 2022-23. The project also received a PS&E allocation for \$14,000,000 concurrently to the programming. In October 2024, the project was amended to change the alternative delivery method from Design-Bid-Build to Progressive Design-Build

(PD-B) as the project entails design and construction of a unique suicide deterrent system that does not fit the traditional highway transportation project mold. The project has not received any G-12 funds for the PS&E phase. The PS&E phase is currently 30 percent complete, with a remaining budget of \$10,100,000, which is insufficient to complete the phase. If this request for additional funding is approved, it is anticipated to Ready to List (RTL) the project by March 2026, and complete the remaining PS&E activities by September 2026.

REASON FOR COST INCREASE:

The project realized cost increases due to additional resources required to convert and administer the PD-B delivery method.

In January 2023, when the project was programmed and allocated for the PS&E phase, the amount did not account for costs related to administering the Department's first project that will be utilizing the PD-B delivery method. The San Diego-Coronado Bay Bridge Suicide Deterrent project includes a physical suicide deterrent system which is non-typical highway transportation scope and outside of the Department's expertise. The project is unique and complex and requires external expertise to complete the design and to ensure successful completion of the project. The project's purpose is to install the suicide deterrence fencing in the timeliest manner to reduce suicides and suicide attempts. Considering these and additional factors, it was decided to convert the project to the new innovative PD-B delivery method. With PD-B, the Department and Design-Builder are targeting to accelerate the completion of the project by 12 or more months.

The project has achieved 30 percent design, conducted a Value Analysis Study, prepared a mockup package that was constructed in 2023, and procured an Independent Cost Estimator (ICE), and the Design-Builder. The Value Analysis Study and mockup required more resources than anticipated.

Seven task orders have been executed with the Design-Builder totaling \$4,500,000. The task orders include reviewing and becoming familiar with the overall project and the Department's 30 percent design, validating the design, and initial independent quality management tasks, partnering, environmental support, survey work, railroad coordination, and agreement support. They also include the opinion of probable construction cost (OPCC) 1 efforts, starting the risk and innovation logs, establishing baseline construction means and methods, and advancement of the design beyond 30 percent. Reviewing and becoming familiar with the project by the Design-Builder was required since they are the Engineer of Record (EOR).

As the EOR, the Design-Builder is responsible for completing the final design and for delivering the project. Based on discussions with the Design-Builder, and the ICE, it is estimated that the total Design-Builder's PS&E phase effort will cost approximately \$16,700,000 (includes the task orders). Although undetermined, the amount assumes the design will be carried to 100 percent before moving to construction, and the project may be split into three separate construction contracts. Based on the same assumptions, it has been estimated that the ICE efforts will cost approximately \$2,200,000, and the remaining Department's PS&E phase efforts will cost an

additional \$6,000,000. The Department will continue to be highly involved in the development of the final design and all other project development tasks including, leading the environmental and right of way efforts, developing construction contracts, participating in multiple OPCC efforts, performing reviews, approving the final design, applying for a Buy America Waiver, and managing the project, ICE, and Design-Builder contracts. Since this project will serve as the first pilot project for this new innovative delivery method, additional resources are required for extensive coordination and engagement.

Therefore, supplemental funds in the amount of \$14,800,000 are required to achieve RTL and complete the contract documents and remaining PS&E activities through approval of the contract by September 2026.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to complete this project that will address the critical need for suicide deterrent fencing on the San Diego-Coronado Bay Bridge in a timely manner. To address all of the deficiencies, the project will need to be reprogrammed in a future SHOPP cycle which could lead to the potential of a project cost increase.

FINANCIAL RESOLUTION:

Resolved, that \$14,800,000 be allocated to provide funds to complete the PS&E phase for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project #		PPNO		
Allocation Amount		Program		
Recipient		Phase		
County		Program Code		
Dist-Co-Rte	Location	Project ID		This
Postmile	Project Description	Adv Phase	Original Amount	Supplemental
		EA		Request
2.5e.(12)	COS Supplemental Funds for Previously Voted Project			Resolution FA-24-100
1	In the city of San Diego, on San Diego-Coronado Bay Bridge	11-1352	\$14,000,000	\$14,800,000
\$14,800,000	No. 57-0857 and E75-N5 Connector Overcrossing No. 57	SHOPP		
	-0912G; also on Route 5 at S5-S75 Connector Overcrossing	PS&E		
Department of	No. 57-0939H (PM R13.8/R14.3).	20.10.201.110		
Transportation		1119000044		
San Diego	<u>Outcome/Output:</u> Install suicide deterrent fences along	1		
11-SD-75	bridges.	43063		
R20.1/R22.3	This is a Progressive Design-Build (PD-B) project.			
	(PS&E, R/W Sup, R/W Cap Only)			
	Supplemental funds are needed to complete PS&E.			
	CEQA - MND, 06/23/2022			
	NEPA - CE, 06/27/2022			
	Future consideration of funding approved under			
	Resolution E-21-101; December 2022.			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(1), Action Item

Prepared By: Sergio Aceves
District 03 – Director

Subject: **SUPPLEMENTAL FUNDS ALLOCATION FOR AWARD OF A PREVIOUSLY
VOTED PROJECT**
**PPNO 5142/EA 0J560 – PLACER AND NEVADA COUNTIES – INTERSTATE 80
RESOLUTION FA-24-101**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$3,104,000 in Construction Capital for the State Highway Operation and Protection Program (SHOPP) Promoting Resilient Operations for Transformative, Efficient, and Cost-saving Transportation (PROTECT) Program project on Interstate 80 (I-80), in Placer and Nevada counties, to award the construction contract?

RECOMMENDATION:

The Department recommends that the Commission approve the requested supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is on I-80 near Emigrant Gap, from east of the Drum Forebay Overcrossing to east of the Troy Road Undercrossing, in Placer and Nevada counties. The project will rehabilitate drainage systems, upgrade transportation management system (TMS) elements, and construct maintenance vehicle pullouts. The TMS elements consist of traffic monitoring stations, variable message sign systems, an automatic vehicle classification system, camera systems, and a roadside weather information system.

FUNDING AND PROGRAMMING STATUS:

In March 2022, the project was programmed for \$11,600,000 in Construction Capital and \$1,380,000 in Construction Support in the SHOPP for allocation in Fiscal Year 2024-25. In May 2023, the project was amended to increase Construction Capital to \$13,899,000 and Construction Support to \$1,580,000 due to the change in the culvert rehabilitation strategy to use climate resilient materials which will be funded by the Infrastructure Investment and Jobs Act. In January 2025, the project was allocated for \$13,889,000 in Construction Capital and \$1,580,000 in Construction Support. Bids were opened in April 2025, and the amount required to award to the lowest bidder exceeded the amount available from project allocated and G-12 funds. Therefore, the Department is requesting supplemental funds in the amount of \$3,104,000 in Construction Capital to award the construction contract.

REASON FOR COST INCREASE:

In March 2025, the Department advertised the contract and held bid opening in April 2025. The contract received five bids, of which the lowest bid was 19.0 percent above the Engineer's Estimate (EE). However, the bidder was found to be non-responsive. The second lowest bid is 19.6 percent above the EE. The remaining three bids range from 34.4 to 51.8 percent over the EE.

The cost increases on this project are largely attributed to the variable message sign system, time-related overhead, reinforced concrete pipe, and temporary barrier system. The EE relied on the latest available cost data and the unit prices were adjusted to match market prices. However, the bids still came in higher than anticipated. The contractor stated that the subcontractor and supplier pricing were major factors in their bid. The major unit price differences, between the EE and lowest responsive bid, are related to the following bid items:

- Variable Message Sign System - 150 percent higher (\$1,525,000)
- Time-Related Overhead - 90 percent higher (\$440,000)
- 24-Inch Reinforced Concrete Pipe - 55 percent higher (\$437,000)
- Temporary Barrier System - 130 percent higher (\$419,000)

In aggregate, these items amount to an increase of \$2,821,000 over the EE. The total request includes minor adjustments to other unit bid prices, supplemental work, Department furnished materials, and contingency.

The Department has determined that this request for supplemental funds is in the best interest of the State to be able to award this project to the lowest responsive bidder, avoid delays in implementing this important PROTECT Program project and add any extra costs to the Department if the project has to be repackaged and/or re-advertised. The Department has exercised all feasible measures to minimize costs in carrying out work related to this project and has determined that this request is well-supported and is the only viable alternative.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to address critical deficiencies of the existing drainage systems, upgrading TMS elements, and constructing maintenance vehicle pullouts. To address all of the deficiencies, the project will be delayed and reprogrammed in a future SHOPP cycle, at a possible higher project cost.

FINANCIAL RESOLUTION:

Resolved, that \$3,104,000 be allocated from the Budget Act of 2023, Budget Act Item 2660-302-0890 and Non-Budget Act Item 2660-802-3290 for Construction Capital, to provide funds to award the construction contract of this SHOPP project.

Attachment

2.5 Highway Financial Matters				PPNO		
Project #				Program		
Allocation Amount				Funding Year		
Recipient				Item #		
<u>RTPA/CTC</u>	Project Title			Fund Type	State	State
County	Location			Program Codes	Federal	Federal
Dist-Co-Rte	Project Description			Project ID	Current Amount	Additional
Postmile	Project Support Expenditures			Adv Phase	by Fund Type	Amount by
				EA		Fund Type
						Amount by
						Fund Type

2.5e.(1)	Supplemental Funds for Previously Voted Project				Resolution FA-24-101
1	In Placer and Nevada Counties, near Emigrant Gap, from 0.3 mile east of Drum Forebay Overcrossing to 0.2 mile east of Troy Road Undercrossing.	03-5142			
\$3,104,000		SHOPP/2017-18			
		CONST			
Department of	<u>Outcome/Outputs:</u>	802-3290 RMRA	\$1,389,000	\$310,000	\$1,699,000
Transportation	Rehabilitate drainage systems, upgrade Transportation	20.20.201.407			
<u>PCTPA</u>	Management System (TMS) elements, and construct	SHOPP/2023-24			
Placer	Maintenance Vehicle Pullouts (MVPs).	CONST			
03-Pla-80		302-0890 FTF	\$12,500,000	\$2,794,000	\$15,294,000
49.3/68.7	Total revised amount \$16,993,000.	20.20.201.407			
		0319000297			
	Supplemental funds are needed to award.	4			
		0J560			
	CEQA - CE, 06/21/2023; Re-validation 12/03/2024				
	NEPA - CE, 06/21/2023; Re-validation 12/03/2024				

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(2), Action Item

Prepared By: Sergio Aceves
District 03 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CONSTRUCTION COMPLETION PHASE**
PPNO 6930/EA 1H630 – SACRAMENTO COUNTY – STATE ROUTE 99
RESOLUTION FA-24-102

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$1,000,000 in Construction Support for the State Highway Operation and Protection Program (SHOPP) Operational Improvements project on State Route (SR) 99, in Sacramento County, to complete the construction contract?

RECOMMENDATION:

The Department recommends that the Commission approve the requested supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on SR 99, in the City of Elk Grove (City), from Elk Grove Boulevard to Calvine Road, in Sacramento County. The project will construct two southbound auxiliary lanes and a concrete median barrier, upgrade drainage systems and ramp metering systems, and extend a storage lane at an off-ramp.

FUNDING AND PROGRAMMING STATUS:

In May 2020, the project was programmed for \$18,100,000 in Construction Capital and \$2,000,000 in Construction Support in the SHOPP for allocation in Fiscal Year 2022-23. In March 2022, the project was amended to increase Construction Capital to \$19,700,000 due to updated scope to include a concrete median barrier, an additional ramp meter, and extend a storage lane at an off-ramp. In June 2023, the project received a 2-month allocation time

extension for the Construction phase. In August 2023, the project was allocated for \$23,096,000 in Construction Capital and \$2,000,000 in Construction Support. In January 2024, the project was awarded for \$20,185,900 in Construction Capital. In April 2024, construction began with 170 working days. The project has not received any G-12 funds for the Construction phase. The remaining funds are currently at \$10,673,000 in Construction Capital with no Construction Support funds. The project is 50 percent complete with 60 working days remaining. The planned Construction Contract Acceptance is scheduled for October 2025.

REASON FOR COST INCREASE:

The project realized cost increases due to contract change orders which include the need to redesign various drainage systems, replace fencing, and stabilize slope, as well as task orders to utilize consultant staff for administering construction.

The project includes 13 drainage systems along this segment of SR 99. As drainage system upgrade work began, additional resources were required to address conflicts and redesign the systems at three locations. At one location, the culvert's slope required adjustment to discharge to the existing topography. The adjustment had to accommodate a trash net to prevent mixing debris with storm water runoff. At another location, the proposed drainage system's culvert was found to be in conflict with the newly constructed transition barrier between a concrete barrier and guardrail. The redesign required modifying the transition barrier by extending the concrete barrier past the drainage inlet. This prevented the connection from the transition railing to guardrail from conflicting with the drainage system. At a third location, the drainage system was designed to be relocated and reconnected to the existing City drainage system due to the construction of the auxiliary lane. However, the removal of a portion of the City's culvert and drainage inlet was not included in the plans and had to be incorporated to complete the drainage work.

The project includes resetting the existing chain link fence to accommodate construction activities and project elements. During construction, some of the existing fencing was found to be damaged and could not be reused. Additional resources were required for the inspection of removing the damaged fence and installing new fencing.

During construction and inspection of the facility, the Department's Storm Water coordinator identified an existing embankment slope that required slope stabilization. The embankment is along the right shoulder of the Elk Grove Boulevard off-ramp. This has resulted in additional resources to design and inspect the placement of rock slope protection to secure the steep slope over a drainage inlet located at the bottom of the embankment.

The Department encountered construction staffing changes that resulted in a shortage of inspectors and resident engineers. As a result, consultant staff is being brought in under a task order to provide these services.

These efforts for additional design support, inspection, and administration of the contract including change orders have resulted in an increased total of \$1,000,000 in support costs and an additional 30 working days to complete construction and close out the project.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to complete construction of this project to make operational improvements along this segment of SR 99 and minimize delays to the traveling public. If the construction contract is further delayed, it will likely result in claims by the contractor. To complete construction at a later time, another project will have to be programmed in a future SHOPP cycle at a possible higher project cost.

FINANCIAL RESOLUTION:

Resolved, that \$1,000,000 be allocated for Construction Support, to provide funds to complete the Construction phase for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project #		PPNO			
Allocation Amount		Program			
Recipient		Funding Year			
<u>RTPA/CTC</u>		Item #			
County	Project Title	Fund Type		State	State
Dist-Co-Rte	Location	Program Codes	State	Federal	Federal
Postmile	Project Description	Project ID	Federal	Additional	Revised
	Project Support Expenditures	Adv Phase	Current Amount	Amount by	Amount by
		EA	by Fund Type	Fund Type	Fund Type

2.5e.(2)**Supplemental Funds for Previously Voted Project****Resolution FA-24-102**

1	In Elk Grove, from Elk Grove Boulevard to Calvine Road.	03-6930			
\$1,000,000		SHOPP			
Department of	<u>Outcome/Outputs:</u> Construct two southbound auxiliary lanes,	CON ENG			
Transportation	upgrade culvert, construct concrete median barrier, extend	001-0042 SHA	\$229,000	\$115,000	\$344,000
<u>SACOG</u>	storage lane at southbound Elk Grove Boulevard offramp, and	001-0890 FTF	\$1,771,000	\$885,000	\$2,656,000
Sacramento	upgrade five ramp metering systems. This project will reduce	20.10.201.310			
03-Sac-99	congestion, improve highway operation and mobility.	0316000193			
12.7/16.3	Total revised amount \$3,000,000.	3			
		1H630			
	Supplemental funds are needed to complete construction.				
	CEQA - CE, 03/29/2022; Re-validation 06/21/2023				
	NEPA - CE, 03/29/2022; Re-validation 06/21/2023				

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(3), Action Item

Prepared By: Scott Eades
District 05 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CONSTRUCTION COMPLETION PHASE**
PPNO 2700/EA 1H860 – SANTA BARBARA COUNTY –
UNITED STATES HIGHWAY 101
RESOLUTION FA-24-103

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$1,700,000 in Construction Support for the State Highway Operation and Protection Program (SHOPP) Pavement Preservation project on United States Highway (US) 101, in Santa Barbara County, to complete the construction contract?

RECOMMENDATION:

The Department recommends that the Commission approve the requested supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on US 101 near Gaviota, from south of Gaviota Beach State Park to the Old Coast Highway, in Santa Barbara County. The project will rehabilitate the pavement.

FUNDING AND PROGRAMMING STATUS:

In October 2017, this project was programmed in the SHOPP for \$59,218,000 in Construction Capital and \$5,026,000 in Construction Support for allocation in Fiscal Year 2021-22. In December 2018, the project was amended to update the performance measure to align with the most recent pavement survey and corrected program code. In June 2021, the project was amended again to increase Construction Capital to \$61,719,000 and Construction Support to \$5,525,000 to install retaining walls to support guardrail and concrete barriers at multiple

locations with narrow shoulders. In June 2022, the project received a 4-month allocation time extension for the Construction phase. In August 2022, the project was further amended to fully program the previously unfunded Construction phase and increase Construction Support to \$6,214,000. The project was also allocated for \$54,740,000 in Construction Capital and \$6,214,000 in Construction Support. In February 2023, the project was awarded for \$51,084,500 for Construction Capital. In March 2023, construction began with 330 working days. In May 2025, the project received partial G-12 funds in the amount of \$4,260,000 in Construction Capital. The project has not received any G-12 funds for Construction Support. The remaining funds are currently at \$20,875,000 in Construction Capital and \$1,572,000 in Construction Support. The project is 70 percent complete with 185 working days remaining. The planned Construction Contract Acceptance (CCA) is December 2025.

REASON FOR COST INCREASE:

The project realized Construction Support cost increases due to weather related delays, encountering differing site conditions, additional traffic control required to complete work, resources required by design staff to address construction challenges, and the use of consultant staff.

The project is rehabilitating the pavement along an approximately 6-mile segment of US 101. Prior to the start of construction in 2023, a series of strong winter storms hit the project area between January and March 2023, which created multiple slides within the project limits. As a result, several separate emergency projects were created within the project limits, which impacted the project's schedule and required additional coordination. The coordination also included the hot mix asphalt paving, since cold weather and rain events changed the sequence of work around the emergency projects. This resulted in the addition of approximately 90 working days to the project which increased support resources by approximately \$150,000.

The strong winter storms also degraded the pavement conditions within the project limits. This resulted in extensive pavement, drainage, and slope failures which led to many contract change orders (CCOs) and redesign to address these failures. The original scope proposed construction of several retaining walls along the shoulder/embankment to install the proposed guardrail. However, the storms eroded the embankment which required earthwork to restore the shoulder before the retaining walls were constructed. The design of the retaining walls was re-evaluated and several CCOs were issued for the removal of the proposed retaining walls from the plans, and further earthwork was added to install the guardrail. This resulted in additional support resources during the development of the CCOs. The Department issued and administered approximately 70 CCOs to address these events. These unanticipated differing site conditions resulted in the addition of approximately 200 working days to the project which increased support resources by approximately \$250,000.

The unanticipated emergency projects which were addressing multiple slides within the project limits impacted constructability of the proposed work. This resulted in rescheduling and resequencing the work. Extensive double shifts and night work were required to keep the project on schedule. Unplanned lane reductions to work around the emergency projects

significantly increased delays and reduced productivity. The project's median barrier work was delayed due to limited work windows for several days which led to multiple shutdowns and re-starts of work as a result of traffic handling safety. This resulted in the addition of approximately 80 working days to the project which increased support resources by approximately \$200,000.

In addition to the unanticipated impacts stated above, additional support resources for Department staff from design, environmental, construction, and other offices were required during construction. This was above what was anticipated due to the project's design consultant not being available during construction. The Department's staff required additional time to become familiar with the project to efficiently address numerous construction challenges. This resulted in increased support resources by approximately \$150,000.

As a result of the additional work, modification and extension of the schedule, and the completion of the remaining work, the Department augmented their support staff with consultants to minimize delays and achieve CCA by December 2025. Consultant augmentation increased support resources by approximately \$950,000.

These efforts have resulted in a total of \$1,700,000 in support costs.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to complete construction to address all of the critical deficiencies of the existing pavement along this segment of US 101. If the construction contract is further delayed, it will likely result in claims by the contractor. To address all of the deficiencies, another project will have to be programmed in a future SHOPP cycle at a possible higher project cost.

FINANCIAL RESOLUTION:

Resolved, that \$1,700,000 be allocated for Construction Support, to provide funds to complete the Construction phase for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project #		PPNO			
Allocation Amount		Program			
Recipient		Funding Year			
<u>RTPA/CTC</u>		Item #			
County	Project Title	Fund Type		State	State
Dist-Co-Rte	Location	Program Codes	State	Federal	Federal
Postmile	Project Description	Project ID	Federal	Additional	Revised
	Project Support Expenditures	Adv Phase	Current Amount	Amount by	Amount by
		EA	by Fund Type	Fund Type	Fund Type

2.5e.(3)**Supplemental Funds for Previously Voted Project****Resolution FA-24-103**

1	Near Gaviota, from 0.1 mile south of Gaviota Beach State Park to Old Coast Highway.	05-2700			
\$1,700,000		SHOPP			
		CON ENG			
Department of	<u>Outcome/Outputs:</u>	505-3290 RMRA	\$712,800	\$195,000	\$907,800
Transportation	Remove and replace pavement with Rubberized Hot Mix	001-0890 FTF	\$5,501,200	\$1,505,000	\$7,006,200
<u>SBCAG</u>	Asphalt (RHMA). This project will extend the pavement service	20.10.201.121			
Santa Barbara	life and improve ride quality.	0517000002			
05-SB-101		3			
46.2/R52.3	Total revised amount \$7,914,000.	1H860			
	Supplemental funds are needed to complete construction.				
	CEQA - CE, 02/27/2020; Re-validation 05/11/2022				
	NEPA - CE, 02/27/2020; Re-validation 05/11/2022				

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(6), Action Item

Prepared By: Scott Eades
District 05 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CONSTRUCTION COMPLETION PHASE**
PPNO 0226K/EA 3307C – SAN LUIS OBISPO COUNTY –
STATE ROUTES 41 AND 46
RESOLUTION FA-24-104

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$7,200,000 in Construction Support for the State Transportation Improvement Program (STIP) Route 46/41 Wye project on State Route (SR) 41 and SR 46, in San Luis Obispo County, to complete the construction contract?

RECOMMENDATION:

The Department recommends that the Commission approve the requested supplemental funds allocation for this STIP project.

PROJECT DESCRIPTION:

This project is located on SR 41 and SR 46 near Cholame, west of Davis Road to west of Antelope Road, in San Luis Obispo County. The project will convert 2-lane conventional highway to a 4-lane expressway. This project has utilized the Construction Management/General Contractor (CMGC) delivery process.

FUNDING AND PROGRAMMING STATUS:

In March 2014, this project was programmed in the STIP for \$62,000,000 in Construction Capital and \$9,500,000 in Construction Support for allocation in Fiscal Year 2019-20. In March 2018, during the adoption of the 2018 STIP, the project was updated to increase Construction Capital to \$97,800,000 and Construction Support to \$13,400,000. In June 2022,

the project received a 20-month allocation time extension for the Construction phase. In December 2022, the project was allocated for \$155,870,000 in Construction Capital and \$15,550,000 in Construction Support. In March 2023, the project was awarded to the CMGC for \$155,869,900 in Construction Capital. In April 2023, construction began with 500 working days. The project has not received any G-12 funds for the Construction phase. The remaining funds are currently at \$50,700,000 in Construction Capital and \$2,429,000 in Construction Support. The project is 65 percent complete with 230 working days remaining. The planned Construction Contract Acceptance is June 2026.

STIP Construction Component	Allocated Amount	Supplemental Amount	Revised Allocation Amount	Percent Over Allocated Amount
Support	\$15,550,000	\$7,200,000	\$22,750,000	46.3

REASON FOR COST INCREASE:

The project realized support cost increases due to the need for additional Department and consultant staff to administer the project, change in the construction method for earthwork, material failure and rework of constructed items, change in the construction schedule, and an increase in the number of working days to complete the project.

The project is located in a rural area which required the Department to rely on consultant staff to administer the contract and serve as field inspectors, office support staff, lab technicians, and surveyors at the field office location. This heavier reliance on consultant staff was unanticipated and resulted in a significant cost increase from the original workplan.

In addition, as a result of environmental requirements and compliance issues, several Department environmental staff are required to be on site two to three times per week instead of one time per month. The project is located in a region that is undeveloped and contains many important natural resources and sensitive species. Permits for the project are complex and require surveys and monitoring before, during, and after project construction. Additional resources are required due to recent listings of threatened species which require additional agency consultations and ongoing discussions with adjacent landowners about wildlife connectivity. In addition, an increase in nesting bird activity requires additional monitoring and deterrence. The Department's roadway and structure design staff have also required additional resources above what was anticipated to support the construction staff for requests for information and issuance of contract change orders.

During the pre-construction CMGC delivery process, the assumption was made that the earthwork and paving would be constructed utilizing the automated machine guidance system. The subcontractor for the earthwork and paving opted out of utilizing this method and utilized the traditional construction methods. This is a more labor-intensive method and resulted in an increase to the resources required for surveying and inspection work. In addition, due to material failure and rework of constructed items, additional resources were required for

re-inspection and remediation by materials and construction staff. Concrete and hot mix asphalt materials have been rejected for being out of compliance, poor workmanship, quality control issues, and compaction test failures. For the over 750 structure backfill test locations, approximately one out of five are retests. Furthermore, as a result of the changes to the contractor's construction schedule, they need to construct four bridge superstructures during this year's jurisdictional work window. To keep the project on schedule and avoid liquidated damages, the contractor is working 10-hour shifts, 6 days per week. This has resulted in requiring additional resources for construction staff inspecting this work.

The original number of working days on this project was 500. As a result of the roadway rework and due to severe weather conditions, the duration for consultant and Department staff to administer the project has increased. A contract change order is being issued to increase the number of working days by 225 for a total of 725. Therefore, the project will need an additional \$7,200,000 in support costs to complete construction.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to complete construction of this project. If the construction contract is further delayed, it will likely result in claims by the contractor. To complete construction at a later time, another project will have to be programmed in a future STIP cycle at a possible higher project cost.

FINANCIAL RESOLUTION:

Resolved, that \$7,200,000 be allocated for Construction Support, to provide funds to complete the Construction phase for this STIP project.

Attachment

2.5 Highway Financial Matters

Project #	Project Title	PPNO			
Allocation Amount	Location	Program			
Recipient	Project Description	Funding Year			
<u>RTPA/CTC</u>	Project Support Expenditures	Item #			
County		Fund Type			
Dist-Co-Rte		Program Codes	State	State	State
Postmile		Project ID	Federal	Federal	Federal
		Adv Phase	Current Amount	Additional	Revised
		EA	by Fund Type	Amount by	Amount by
				Fund Type	Fund Type

2.5e.(6)	Supplemental Funds for Previously Voted Project	Resolution FA-24-104
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1	Route 46/41 Wye. Near Cholame, from 0.7 miles west of Davis Road to 0.5 miles west of Antelope Road.	05-0226K		
\$7,200,000		IIP		
	Convert to a 4 lane expressway.	CON ENG		
Department of		001-0042 SHA	\$1,783,600	\$1,783,600
Transportation		001-0890 FTF	\$13,766,400	\$13,766,400
<u>SLOCOG</u>	<u>Outputs</u>	20.10.025.700		
San Luis Obispo	New roadway lane-mile(s)			
05-SLO-46		IIP		
54.1/57.8	Total revised amount \$178,619,900.	CON ENG		
		001-0042 SHA		\$826,000
	Supplemental funds are needed to complete construction.	001-0890 FTF		\$6,374,000
		20.10.025.700		
	CEQA - EIR, 05/19/2008; Re-validation 06/22/2022	IIP/2022-23		
	NEPA - FONSI, 05/19/2008; Re-validation 06/22/2022	CONST		
	Future consideration of funding approved under	301-0042 SHA	\$10,834,800	\$10,834,800
	Resolution E-06-23; July 2006.	301-0890 FTF	\$83,626,100	\$83,626,100
		20.20.025.700		
	Addendum to the previously approved future consideration of	IIP/2022-23		
	funding approved under Resolution E-22-111; December 2022.	CONST		
	Right of Way Certification: 06/29/2022	304-6058 TFA	\$57,419,000	\$57,419,000
		20.20.025.700		
		RIP/2022-23		
		CONST		
		301-0042 SHA	\$457,700	\$457,700
		301-0890 FTF	\$3,532,300	\$3,532,300
		20.20.075.600		
		0514000028		
		3,4		
		3307C		

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5e.(7), Action Item

Prepared By: Michael Navarro
District 06 – Director

Subject: **SUPPLEMENTAL FUNDS FOR CONSTRUCTION COMPLETION PHASE**
PPNO 6708/EA 0R210 – MADERA COUNTY – STATE ROUTE 41
RESOLUTION FA-24-105

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request for an additional \$1,269,000 in Construction Support for the State Highway Operation and Protection Program (SHOPP) Roadway Rehabilitation project on State Route (SR) 41, in Madera County, to complete the construction contract?

RECOMMENDATION:

The Department recommends that the Commission approve the requested supplemental funds allocation for this SHOPP project.

PROJECT DESCRIPTION:

This project is located on SR 41 near the City of Fresno, from north of Avenue 15 to south of SR 145, in Madera County. The project will rehabilitate the roadway, widen shoulders and a bridge, and reconstruct culverts.

FUNDING AND PROGRAMMING STATUS:

In April 2016, this project was programmed in the SHOPP for \$10,184,000 in Construction Capital and \$2,722,000 in Construction Support for allocation in Fiscal Year 2019-20. In May 2019, the project was amended to increase Construction Capital to \$10,900,000 and Construction Support to \$3,000,000, due to a delay in the delivery schedule that was moved out to 2020-21. In December 2019, the project was amended again to increase Construction Capital to \$13,900,000 due to a higher cost for California Tiger salamander mitigation, three

plant species, and the cost of a detour that will be implemented. In June 2021, the project received a 14-month construction allocation time extension. In August 2022, the project was allocated for \$17,999,000 in Construction Capital and \$3,550,000 in Construction Support. In January 2023, the project was awarded for \$16,017,000 for Construction Capital. In May 2023, construction began with 225 working days. The project has not received any G-12 funds for the Construction phase. The remaining funds are currently at \$2,188,000 in Construction Capital and \$140,000 in Construction Support. The project is 90 percent complete with 30 working days remaining. The planned Construction Contract Acceptance (CCA) is June 2025 to complete punch list items of the project. After CCA there will be ongoing support to complete the off-site mitigation property (Madera Pools) and closeout of the project.

REASON FOR COST INCREASE:

The project realized Construction Support cost increases due to the improper placement of surplus soil, and the mitigation for anticipated impacts from the project.

The project proposes to rehabilitate the roadway, widen shoulders and bridge, and reconstruct culverts along an approximately 3-mile segment of SR 41. As earthwork operations were being completed to rehabilitate the roadway and widen shoulders, the contractor negotiated with a private property owner adjacent to the project site to dispose of surplus soil on their land. Approximately 1,100 cubic yards of surplus soil were stockpiled in an area identified on the plans as an environmentally sensitive area (ESA) during two shifts of night-time operation. This resulted in the damage of biological and cultural resources. For the biological impacts, there were 2.6 acres of direct impacts to California Tiger salamander upland habitat. This area of direct impact will require mitigation that was not accounted for in the project's existing Biological Opinion (BO) and Incidental Take Permit (ITP). As a result of the surplus soil being placed in the identified ESA, the project is not in compliance with the BO issued by the United States Fish and Wildlife Services (USFWS) and the ITP issued by the California Department of Fish and Wildlife (CDFW). For the cultural impacts, it also violated the 2020 cultural resources Memorandum of Agreement (MOA). The MOA was signed by the Department, State Historic Preservation Officer, North Fork Rancheria of Chukchansi of Mono Indians, and Picayune Rancheria of Chukchansi Indians. Therefore, additional resources are required to amend both the MOA and the biological permits from both agencies.

Apart from the improper placement of the surplus soil, there are also increased support costs associated for the split off biological mitigation project (EA 0R211/PPNO 6708A), with the previously identified biological mitigation associated with the ITP and BO. Due to the lack of agency approved species mitigation credits, the biological mitigation could not be completed before construction started. After years of additional biological surveys and negotiation, CDFW recently issued a conceptual approval of a proposed mitigation property (Madera Pools) which is part of the Department's inventory that was purchased and enhanced for future mitigation purposes. The USFWS also provided a conceptual approval for the Department to move forward with the property. To finalize the permittee responsible for mitigation at this property, the land will need to be approved through the CDFW lands package and USFWS process. The

land will need to be transferred out of the Department's inventory to an agency approved landowner or land manager with an endowment and conservation easement for in-perpetuity protection. The additional coordination was not anticipated and impacted support costs.

The additional support hours required to complete these efforts are anticipated to span approximately two years to finalize the mitigation due to the need for recording documents, transferring fee title or conservation easement, closing the real estate transaction in coordination with CDFW and USFWS, and other required ongoing support until the closeout of the project after CCA.

The additional resources required amount to \$1,269,000 in support costs. Of these support costs, \$145,000 will be required to address the mitigation of the surplus soil being spread over the ESA, \$920,000 will be utilized to address the previously identified biological mitigation, and the remaining \$204,000 will cover closeout of the project.

CONSEQUENCES:

If this request for supplemental funds is not approved, the Department will not be able to meet the environmental commitments and complete construction to address all of the critical deficiencies of the existing roadway, bridge, and culverts along this segment of SR 41. If the construction contract is further delayed, it will likely result in claims by the contractor. To address all of the deficiencies, another project will have to be programmed in a future SHOPP cycle at a possible higher project cost.

FINANCIAL RESOLUTION:

Resolved, that \$1,269,000 be allocated for Construction Support, to provide funds to complete the Construction phase for this SHOPP project.

Attachment

2.5 Highway Financial Matters

Project #		PPNO			
Allocation Amount		Program			
Recipient		Funding Year			
<u>RTPA/CTC</u>		Item #			
County	Project Title	Fund Type		State	State
Dist-Co-Rte	Location	Program Codes	State	Federal	Federal
Postmile	Project Description	Project ID	Federal	Additional	Revised
	Project Support Expenditures	Adv Phase	Current Amount	Amount by	Amount by
		EA	by Fund Type	Fund Type	Fund Type

2.5e.(7)**Supplemental Funds for Previously Voted Project****Resolution FA-24-105**

1	Near Fresno, from 0.1 mile north of Avenue 15 to 0.1 mile south of Route 145.	06-6708			
\$1,269,000		SHOPP			
		CON ENG			
Department of	<u>Outcome/Outputs:</u> Rehabilitate roadway by resurfacing the	505-3290 RMRA	\$407,200	\$146,000	\$553,200
Transportation	pavement, widen shoulders and bridge, and reconstruct	001-0890 FTF	\$3,142,800	\$1,123,000	\$4,265,800
<u>MCTC</u>	culverts. This project will improve safety and ride quality.	20.10.201.120			
Madera		0614000058			
06-Mad-41	Total revised amount \$4,819,000.	3			
6.3/9.2		0R210			
	Supplemental funds are needed to complete construction.				
	CEQA - MND, 05/13/2020; Re-validation 05/04/2022				
	NEPA - FONSI, 05/13/2020; Re-validation 05/04/2022				
	Future consideration of funding approved under				
	Resolution E-20-51; June 2020.				

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6g.(2), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **SUPPLEMENTAL FUNDS ALLOCATION FOR THE TRANSIT AND INTERCITY
RAIL CAPITAL PROGRAM – (2018:13) PENINSULA CORRIDOR
ELECTRIFICATION EXPANSION PROJECT (BATTERY ELECTRIC MULTIPLE
UNIT DEMONSTRATION TRAINSET) COMPONENT
RESOLUTION TIRCP-2425-72S**

ISSUE:

Should the California Transportation Commission (Commission) approve a request for an additional \$1,000,000 for the Construction (CON) phase of the Transit and Intercity Rail Capital Program (TIRCP) (2018:13) Peninsula Corridor Electrification Expansion Project (Battery Electric Multiple Unit Demonstration Trainset) component (PPNO CP039A), in various counties?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve a request for an additional \$1,000,000 for the CON phase of the TIRCP (2018:13) Peninsula Corridor Electrification Expansion Project (Battery Electric Multiple Unit Demonstration Trainset) component (PPNO CP039A), in various counties.

BACKGROUND:

In August 2023, the Commission allocated \$79,000,000 for the CON phase of the Battery Electric Multiple Unit Demonstration Trainset component (PPNO CP039A), under Resolution TIRCP-2324-16. In May 2025, the Commission approved an allocation amendment to revise the project title and include the request to extend the completion of construction an additional 18 months beyond the Timely Use of Funds deadline, as originally requested by the implementing agency.

PROJECT LOCATION AND DESCRIPTION:

The Battery Electric Multiple Unit Demonstration Trainset component (PPNO CP039A), is located in the counties of San Francisco, San Mateo, and Santa Clara and will construct a demonstration project to design and operate a Battery-Electric Multiple Unit (BEMU) on the non-electrified corridor. The train will be charged while operating on overhead power in the electrified service area, and will operate on battery power when traveling on the non-electrified corridor between San Jose and Gilroy.

FUNDING STATUS:

The current total programmed amount for the CON phase of the Battery Electric Multiple Unit Demonstration Trainset component (PPNO CP039A) is \$79,000,000. However, to complete the CON phase, an additional \$1,000,000 from the TIRCP January 2023 Cycle 6 General Augmentation award is needed.

REASONS FOR THE COST INCREASE:

The Battery Electric Multiple Unit Demonstration Trainset component (PPNO CP039A), is necessary to fund the purchase of the BEMU train to complete the demonstration project. Upon completion, the demonstration will realize reduced reliance on the use of carbon-based energy and reductions in greenhouse gas (GHG) emissions as the BEMU train will replace a diesel train on a non-electrified corridor. This supplemental request will allow the Peninsula Corridor Joint Powers Board to complete the purchase of the BEMU, which is needed to decrease the GHG emissions. At the time of allocation, the amount approved was \$79,000,000, however it should have been \$80,000,000. This supplemental request is needed to correct the allocation amount to \$80,000,000 as initially planned.

CONSEQUENCES:

If this request for supplemental funds is not approved, the construction completion of the Battery Electric Multiple Unit Demonstration Trainset component (PPNO CP039A) will halt the current project timeline and cause delays in the transition away from diesel trains.

FINANCIAL RESOLUTION:

Be it Resolved, that an additional \$1,000,000 be allocated from the Budget Act of 2023, Budget Act Item 2660-301-0046R, to complete the CON phase of the TIRCP (2018:13) Peninsula Corridor Electrification Expansion Project (Battery Electric Multiple Unit Demonstration Trainset) component (PPNO CP039A).

Attachment

2.6 Mass Transportation Financial Matters

Project #	Allocation Amount	Recipient	Project Title	Location	Project Description	PPNO Program Funding Year Item # Fund Type Program Codes Project ID Adv Phase	State Federal Current Amount by Fund Type	State Federal Additional Amount by Fund Type	State Federal Revised Amount by Fund Type
<u>RTPA/CTC</u>									
District-County									
2.6g.(2)	Supplemental - Transit and Intercity Rail Capital Program							Resolution TIRCP-2425-72S	
1	\$1,000,000	Peninsula Corridor Joint Powers Board	(2018:13) Peninsula Corridor Electrification Expansion Project (Battery Electric Multiple Unit Demonstration Trainset). Procurement and delivery of one four-car zero-emission bi-level Battery-Electric Multiple Unit (BEMU) demonstration trainset to Caltrain to operate on non-electrified track.			04-CP039A TIRCP/2023-24 CONST 301-0046R GF 30.20.301.100	\$79,000,000		\$79,000,000
<u>MTC</u> 04-Various			<u>Outcome/Outputs:</u> Reduced greenhouse gas emissions and diesel pollution from replacement of Caltrain diesel service.			TIRCP/2023-24 CONST 301-0046R GF 30.20.301.100 0025000152 S R392GI		\$1,000,000	\$1,000,000
			Total revised amount \$80,000,000.						
			CEQA - EIR, 02/01/2014						
			Future consideration of funding approved under Resolution E-16-93; December 2016.						
			Addendum to the previously approved future consideration of funding approved under Resolution E-23-127A; August 2023.						
			Right of Way Certification: N/A						
			Peninsula Corridor Joint Powers Board will exercise an option in an existing TIRCP funded contract.						
			This project will be implemented using the design build method.						
			In July 2023, Project 2018:13 was awarded \$80,000,000 Cycle 6 General Fund Augmentation funding.						

2.6g.(8)

TIRCP SUPPLEMENTAL FUND ALLOCATION

TIRCP (2018:27) Southern California Optimized Rail Expansion
(SCORE) (Simi Valley Double Track) component,
in Ventura County. (PPNO CP033G)
Resolution TIRCP-2425-75S

MEETING MATERIALS FOR THIS ITEM WILL BE
PROVIDED PRIOR TO THE JUNE 26-27, 2025
CALIFORNIA TRANSPORTATION COMMISSION MEETING.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.1a.(1), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **SHOPP AMENDMENT 24H-012**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request to amend the 2024 State Highway Operation and Protection Program (SHOPP) under SHOPP Amendment 24H-012?

RECOMMENDATION:

The Department recommends that the Commission approve SHOPP Amendment 24H-012 that will amend the 2024 SHOPP, in accordance with Senate Bill (SB) 486 and the Road Maintenance and Rehabilitation Program component of SB 1, which require the Commission to approve changes to projects in the SHOPP or to adopt new projects being amended into the SHOPP.

The Department recommends 35 new capital projects be amended into the 2024 SHOPP, as detailed in 2.1a.(1a). This amendment, summarized below, would be funded from the Bridge Preservation, Collision Reduction, Major Damage Restoration, Mobility, 2024 SHOPP programming capacity and the Infrastructure Investment and Jobs Act. These projects are consistent with the 2022 Transportation Asset Management Plan (TAMP).

2024 SHOPP Summary of New Projects	No.	FY 2024-25 (\$1,000)	FY 2025-26 (\$1,000)	FY 2026-27 (\$1,000)	FY 2027-28 (\$1,000)
Bridge Preservation	1	\$0	\$2,600	\$0	\$3,000
Collision Reduction	8	\$1,934	\$3,651	\$7,186	\$26,220
Major Damage Restoration	25	\$128,278	\$0	\$0	\$0
Mobility	1	\$0	\$0	\$0	\$3,480
Total New Amendments	35	\$130,212	\$6,251	\$7,186	\$32,700

Also, the Department recommends to begin the development of one new Long Lead project, as detailed in 2.1a.(1c). Resolution G-00-13, established in June 2000, provides the Department with authority to develop Long Lead SHOPP projects which require periods longer than the standard four-year SHOPP cycle. Long Lead projects must identify challenges that require additional time beyond the typical four years to complete.

The Department further recommends that the capital projects detailed in 2.1a.(1d), 2.1a.(1e) and 2.1a.(1f) be amended in the 2024 SHOPP to update scope, schedule, cost and make other technical changes. The amended projects are consistent with the Commission-adopted 2022 TAMP.

BACKGROUND:

In each even-numbered year, the Department prepares a 4-year SHOPP which defines major capital improvements necessary to preserve and protect the State Highway System. Periodically, the Department proposes amendments to the SHOPP to address newly identified needs prior to the next programming cycle. Between programming cycles, the Department updates scope, schedule and cost to effectively deliver projects.

SB 486, approved by the Governor on September 30, 2014, requires Commission approval of projects amended into the SHOPP.

Attachments

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
Bridge Preservation					
1 05-Mon-1 62.97 2696 0516000163 1H800	Near Carmel-by-the-Sea, at the Garrapata Creek Bridge No. 44-0018. Replace bridge railing to meet current traffic safety standards. PA&ED: 7/16/2027 R/W: 1/12/2029 RTL: 7/15/2029 BC: 2/25/2030	29-30	PA&ED PS&E R/W Sup *Con Sup *R/W Cap *Const Cap Total	\$2,600 \$2,500 \$500 \$2,200 \$1,000 \$13,147 \$21,947	25-26 27-28 27-28 29-30 29-30 29-30 201.112 Assembly: 30 Senate: 17 Congress: 19
<u>Performance Measure</u> 0.01 Annual fatal and serious injury collision(s)		* Phase not authorized			
Concurrent COS allocation under Resolution FP-24-96; June 2025.					
Collision Reduction					
2 02-Sha-44 R26.94/R27.62 3921 0224000119 1K540	Near Shingletown, from 1.0 mile west to 0.3 mile west of Wilson Hill Road/Emigrant Trail. Widen shoulders, install centerline and shoulder rumble strips, install guardrail, and replace signs. PA&ED: 10/27/2026 R/W: 10/27/2027 RTL: 1/19/2028 BC: 5/30/2028	27-28	PA&ED PS&E R/W Sup Con Sup R/W Cap Const Cap Total	\$820 \$790 \$100 \$610 \$31 \$2,680 \$5,031	24-25 26-27 26-27 27-28 27-28 27-28 201.010 Assembly: 1 Senate: 1 Congress: 1
<u>Performance Measure</u> 0.38 Annual fatal and serious injury collision(s)					
Concurrent COS allocation under Resolution FP-24-96; June 2025.					

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.	
3 02-Teh-99 13.4/13.9 3919 0224000113 1K470	Near Los Molinos, from 0.1 mile south of Butler Street to 0.3 mile north of Taft Street. Widen roadway to construct two way left turn lane, and replace signs, fencing, and culverts. PA&ED: 3/2/2027 R/W: 9/5/2028 RTL: 11/28/2028 BC: 5/8/2029	28-29	PA&ED PS&E R/W Sup * Con Sup * R/W Cap * Const Cap Total	\$1,020 \$890 \$200 \$780 \$30 \$2,970 \$5,890	24-25 26-27 26-27 28-29 28-29 28-29	201.010 Assembly: 3 Senate: 1 Congress: 1
<u>Performance Measure</u> 0.12 Annual fatal and serious injury collision(s)		* Phase not authorized				
Concurrent COS allocation under Resolution FP-24-96; June 2025.						
4 03-ED-50 48.8/70.0 3368 0324000235 0N970	Near Kyburz, Strawberry, and Meyers, from 0.1 mile west of Kyburz Drive to Chiapa Drive. Install curve warning signs, radar feedback signs, chevron signs, roadside signs, and centerline rumblestrips. PA&ED: 6/9/2026 R/W: 6/15/2027 RTL: 7/1/2027 BC: 11/10/2027	27-28	PA&ED PS&E R/W Sup Con Sup R/W Cap Const Cap Total	\$540 \$1,100 \$70 \$610 \$50 \$2,500 \$4,870	24-25 25-26 25-26 27-28 27-28 27-28	201.010 Assembly: 1 Senate: 4 Congress: 3
<u>Performance Measure</u> 3.60 Annual fatal and serious injury collision(s)						
Concurrent COS allocation under Resolution FP-24-96; June 2025.						

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY		Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
5 03-Sac-160 L2.73/L6.976 7299 0325000005 1N340	Near Rio Vista, from 0.1 mile north of Sherman Island Cross Road to Three Mile Slough. Install centerline rumble strips, place striping, and upgrade guardrail. PA&ED: 3/13/2026 R/W: 9/23/2027 RTL: 10/8/2027 BC: 5/22/2028	27-28	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$640 \$630 \$50 \$460 \$35 <u>\$3,500</u> \$5,315	24-25 25-26 25-26 27-28 27-28 27-28	201.010 Assembly: 11 Senate: 3 Congress: 7
<u>Performance Measure</u> 0.70 Annual fatal and serious injury collision(s) Concurrent COS allocation under Resolution FP-24-96; June 2025.						
6 04-Sol-80 2.33 2922E 0423000325 3Y870	In Vallejo, at westbound Route 780 Connector to eastbound Route 80. Install additional safety lights, upgrade concrete barrier, power source to flashing beacons, and replace top pavement layer with open-graded asphalt pavement. PA&ED: 11/2/2026 R/W: 4/14/2028 RTL: 6/1/2028 BC: 1/17/2029	27-28	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$877 \$1,415 \$32 \$949 \$10 <u>\$1,718</u> \$5,001	25-26 26-27 26-27 27-28 27-28 27-28	201.010 Assembly: 11 Senate: 3 Congress: 8
<u>Performance Measure</u> 0.07 Annual fatal and serious injury collision(s) Concurrent COS allocation under Resolution FP-24-96; June 2025.						

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
7 04-Son-116 36.92 2927T 0424000445 1X790	In Petaluma, at the intersection with Pine View Way. Construct intersection improvements. PA&ED: 1/15/2027 R/W: 8/6/2028 RTL: 8/15/2028 BC: 2/15/2029	28-29	PA&ED PS&E R/W Sup * Con Sup * R/W Cap * Const Cap Total	\$914 \$989 \$216 \$1,370 \$78 \$7,394 \$10,961	24-25 26-27 26-27 28-29 28-29 28-29 28-29 201.010 Assembly: 12 Senate: 2 Congress: 2
<u>Performance Measure</u> 0.80 Annual fatal and serious injury collision(s) Concurrent COS allocation under Resolution FP-24-96; June 2025.					
8 04-Son-121 9.54/10.36 2917J 0421000152 2W230	Near Schellville, from Ramal Road to 0.4 mile south of Napa Road. Realign 2 horizontal curves, widen shoulders, and construct centerline and shoulder rumble strips. PA&ED: 8/2/2027 R/W: 4/2/2029 RTL: 6/1/2029 BC: 3/1/2030	28-29	PA&ED PS&E R/W Sup * Con Sup * R/W Cap * Const Cap Total	\$3,651 \$5,446 \$557 \$6,871 \$1,393 \$40,529 \$58,447	25-26 27-28 27-28 28-29 28-29 28-29 28-29 201.010 Assembly: 12 Senate: 3 Congress: 4
<u>Performance Measure</u> 0.25 Annual fatal and serious injury collision(s) Concurrent COS allocation under Resolution FP-24-96; June 2025.					
9 08-Riv-215 R10.89/R11.6 3025A 0824000209 1R020	In Murrieta, from 0.2 mile to 1.0 mile north of Los Alamos Road. Upgrade guardrail. PA&ED: 7/1/2026 R/W: 2/1/2027 RTL: 3/1/2027 BC: 8/15/2027	26-27	PA&ED PS&E R/W Sup Con Sup R/W Cap Const Cap Total	\$1,130 \$1,476 \$109 \$1,060 \$29 \$1,087 \$4,891	24-25 25-26 25-26 26-27 26-27 26-27 26-27 201.010 Assembly: 71 Senate: 32 Congress: 48
<u>Performance Measure</u> 0.15 Annual fatal and serious injury collision(s) Concurrent COS allocation under Resolution FP-24-96; June 2025.					

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
Major Damage Restoration					
10 01-DN-101 12.0/16.5 4957 0125000013 0P430	Near Crescent City, from 0.5 mile south to 4.0 miles north of Wilson Creek Road. Repair retaining walls, drainage systems, and roadway. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$25 \$2,125 \$25 <u>\$8,500</u> \$10,675	201.130 Assembly: 2 Senate: 2 Congress: 2
<u>Note:</u> On March 14, 2025, a landslide was reported in the area. The landslide, resulting from heavy winter rains, damaged a retaining walls and drainage. The roadway prism has undergone localized variability in settlement due to widespread earth movement causing roadway cracks, undulations, and bumps, thereby affecting the drivability. This project will repair retaining walls, drainage systems, and roadway.					
11 01-DN-199 9.1/9.4 4999 0125000123 0Q080	Near Gasquet, from 2.0 miles to 1.7 miles east of Hardscrabble Creek Bridge. Stabilize slope with a retaining wall, replace culvert, and repair roadway and guardrail. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$25 \$1,500 \$25 <u>\$4,500</u> \$6,050	201.130 Assembly: 2 Senate: 2 Congress: 2
<u>Note:</u> On February 25, 2025, maintenance crews reported a slipout that resulted in culvert separation and damage to the roadway, embankment, and guardrail. Based on geotechnical investigation, the combination of the steep incline of the embankment, saturated soils, and recent winter storms caused the slope failure and culvert separation. This project will repair the slope with a Mechanically Stabilized Earth (MSE) wall, replace the damaged culvert, and repair the roadway and guardrail.					

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
12 01-Hum-36 27.9 4993 0125000108 0Q030	Near Bridgeville, at 2.6 miles east of Little Larabee Creek Bridge. Abandon existing culvert, install new culvert, reconstruct slope, place Rock Slope Protection (RSP), and repair roadway.	24-25	PA&ED	\$0	201.130
			PS&E	\$0	Assembly: 2
			R/W Sup	\$25	Senate: 2
			Con Sup	\$390	Congress: 2
			R/W Cap	\$25	
			<u>Const Cap</u>	<u>\$1,300</u>	
			Total	\$1,740	
	<u>Performance Measure</u> 1.0 Location(s)				
	<u>Note:</u> Heavy rains in mid-February 2025 caused slope erosion on the outlet side of a failed culvert. This resulted in failure of the roadway embankment and damage to the pavement. This project will abandon the existing culvert, install a new culvert, reconstruct the slope, place Rock Slope Protection (RSP), and repair the roadway.				
13 01-Hum-101 R42.3 3233 0125000130 0Q130	Near Shively, at 1.0 mile south of Barkdull Road Undercrossing. Replace culvert and repair roadway.	24-25	PA&ED	\$0	201.130
			PS&E	\$0	Assembly: 2
			R/W Sup	\$25	Senate: 2
			Con Sup	\$800	Congress: 2
			R/W Cap	\$25	
			<u>Const Cap</u>	<u>\$2,500</u>	
			Total	\$3,350	
	<u>Performance Measure</u> 1.0 Location(s)				
	<u>Note:</u> On March 10, 2025, maintenance forces reported a hole in the paved shoulder along the northbound lane. Upon investigation, it was determined that the culvert at this location had separated. This separation, in combination with heavy rains, caused a large void between the culvert and pavement that extended into the traveled way. This project will replace the existing culvert and repair the roadway.				
14 01-Hum-299 R21.0/R22.0 4981 0125000090 0P910	Near Blue Lake, from 1.4 miles to 0.4 miles west of Chezem Road. Repair roadway, replace guardrail, and reconstruct drainage systems.	24-25	PA&ED	\$0	201.130
			PS&E	\$0	Assembly: 2
			R/W Sup	\$25	Senate: 2
			Con Sup	\$500	Congress: 2
			R/W Cap	\$25	
			<u>Const Cap</u>	<u>\$1,500</u>	
			Total	\$2,050	
	<u>Performance Measure</u> 1.0 Location(s)				
	<u>Note:</u> In February 2025, heavy winter storms caused a 300-foot section of the roadway to sink. The sink spans the full width of the roadway in both eastbound and westbound lanes and has resulted in damage to the roadway prism, pavement, guardrail, and adjacent drainage systems. This project will repair the roadway, replace guardrail, and reconstruct drainage systems.				

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
15 01-Men-1 0.6 4987 0125000096 0P960	Near Gualala, at Old Stage Road. Abandon existing culvert, perform geotechnical studies, install larger culvert suitable for fish passage requirements, and repair roadway. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$25 \$3,300 \$25 <u>\$9,900</u> \$13,250	201.130 Assembly: 2 Senate: 2 Congress: 2
<u>Note:</u> On February 5, 2025, maintenance forces reported a sinkhole at the intersection of Route 1 and Old Stage Road. The damage was reportedly caused by culvert separation and heavy winter rains. This project will abandon the existing culvert, perform geotechnical studies, install a larger culvert suitable for fish passage requirements, and repair the roadway.					
16 01-Men-1 41.8/42.5 3231 0125000125 0Q100	Near Whitesboro, from 0.6 mile south to 0.1 mile north of Navarro Ridge Road. Repair and stabilize slope, install subsurface drainage, and replace erosion control. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$25 \$600 \$25 <u>\$2,000</u> \$2,650	201.130 Assembly: 2 Senate: 2 Congress: 2
<u>Note:</u> On January 22, 2025, following winter rain, construction crews reported that a recently modified slope had failed at several locations. On March 5, 2025, a geotechnical investigation was performed, and it was determined that four sections of the slope require repair. This project will repair and protect the slope sections using a combination of grading and compaction, excavation and backfilling with soil-filled Rock Slope Protection (RSP), installation of subsurface drainage, and replacement of erosion control.					

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
17 01-Men-101 97.4 4990 0125000102 0P990	Near Riverdale, at 0.1 mile south of Bridges Creek Bridge. Reconstruct slope, place Rock Slope Protection (RSP), and repair the overside drain and roadway. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$25 \$600 \$25 \$2,000 \$2,650	201.130 Assembly: 2 Senate: 2 Congress: 2
<u>Note:</u> On February 6, 2025, maintenance forces reported a slope and drainage system failure at this location. The damage was the result of heavy rains that overwhelmed the overside drain and caused the slope erosion. This project will reconstruct the slope, place Rock Slope Protection (RSP), and repair the overside drain and roadway.					
18 01-Men-128 R28.1 4997 0125000121 0Q060	Near Boonville, at 0.1 mile south of Schoenahl Road. Repair embankment, reconstruct loss of yard, place Rock Slope Protection (RSP), and repair perimeter fencing. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$25 \$500 \$25 \$1,500 \$2,050	201.130 Assembly: 2 Senate: 2 Congress: 2
<u>Note:</u> On February 18, 2025, maintenance forces reported significant erosion along the Anderson Creek embankment adjacent to the Boonville Maintenance Station. The erosion was caused by increased flow in the creek from heavy winter storms and has resulted in approximately 100 feet of property loss along the maintenance yard perimeter and damage to the perimeter fencing. This project will repair the embankment between the creek and the maintenance yard, reconstruct loss of yard with imported borrow, place Rock Slope Protection (RSP), and repair the perimeter fencing.					

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
19 01-Men-162 16.1/16.2 4989 0125000097 0P970	Near Dos Rios, at 0.8 mile to 0.9 mile east of Laytonville Dos Rios Road. Construct new retaining wall, and repair existing retaining walls, drainage systems, and roadway. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$25 \$2,000 \$25 \$10,000 \$12,050	201.130 Assembly: 2 Senate: 2 Congress: 2
<u>Note:</u> On February 5, 2025, heavy winter rains caused a landslide to occur between two existing retaining walls. The landslide has caused the roadway to crack with a 6-inch horizontal offset, resulting in damage to the existing guardrail, subsurface drainage, and adjacent retaining walls. This project will construct a new soldier pile wall to connect the two existing walls and stabilize the roadway prism, as well as repair the ends of the existing walls, subsurface drainage systems, and roadway.					
20 02-Sis-96 46.6 3940 0225000120 2K620	Near Happy Camp, 2.3 miles east of Gordons Ferry Road. Replace underdrain and repair slope and roadway. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$600 \$0 \$2,970 \$3,570	201.130 Assembly: 1 Senate: 1 Congress: 1
<u>Note:</u> On February 25, 2025, maintenance forces reported multiple cracks in the pavement along a 225-foot section of the roadway. At the time of reporting, the cracks were on average 3 inches wide, with depths of up to 3.5 feet, and showed signs of continuous widening. It was also observed that runoff was percolating into the shoulder and contributing to the pavement failure, despite an existing underdrain system. This project will remove the existing underdrain, install a deeper underdrain, and repair the slope and roadway.					

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.	
21 03-But-70 36.42/42.35 2312 0325000202 2N010	Near Pulga, from 1.0 mile north of Deadwood Road to 0.3 mile north of Mill Creek Bridge. Repair slopes, replace culverts, and place Rock Slope Protection (RSP). <u>Performance Measure</u> 8.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$500 \$0 \$2,500 \$3,000	24-25 24-25	201.130 Assembly: 3 Senate: 1 Congress: 1
<u>Note:</u> On February 18, 2025, following heavy winter rain, maintenance forces reported multiple slipout locations within the project limits. Upon further investigation, it was determined that multiple drainage systems in the area were also damaged and in need of repair. This project will replace the damaged culverts, repair the slopes, and place Rock Slope Protection (RSP).						
22 04-Mrn-1 18.91/19.29 2929P 0425000224 3X260	Near Woodville, from 1.3 miles to 1.7 miles north of Horseshoe Hill Road. Construct retaining wall, repair roadway, drainage system, and reconstruct slope embankment. <u>Performance Measure</u> 3.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$350 \$0 \$990 \$1,340	24-25 24-25	201.130 Assembly: 12 Senate: 2 Congress: 2
<u>Note:</u> On January 14, 2025, a roadway slipout had been reported. On January 22, 2025, an investigation found excessive rain from early January 2025 resulted in surface water and over saturated soils that caused a disconnected cross culvert, slope erosion, roadway settlement, and two roadway slipouts. This project will reconstruct the roadway slope embankment and backfill with engineering fill, replace the damaged sections of 2 cross culverts, construct a drainage inlet, repair roadway slipouts and install gabion baskets to stabilize the roadway.						

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
23 04-Mrn-101 3.0 2929S 0425000237 3X360	Near Marin City, at 0.5 mile north of Rodeo Avenue. Remove sediment and debris from roadway and drainage system and reconstruct slope embankment. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$15 \$200 \$30 <u>\$720</u> \$965	24-25 24-25 24-25 24-25
	<u>Note:</u> A heavy storm beginning February 13, 2025 saturated soils, resulting in a landslide the next day, sending mud and debris down the slope, covering the two of four southbound lanes. This project will remove debris from catchment area at the toe of slope and roadway, remove loose rocks and damaged trees from the slope, and clean sediment and debris from the drainage systems.				
24 04-Nap-29 45.51 2930A 0425000263 3X490	Near Calistoga, at 6.0 miles north of Tubbs Lane. Remove mud and debris, reconstruct slope embankments, regrade unlined ditch, and place Rock Slope Protection (RSP) to stabilize slope embankments. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$210 \$40 <u>\$660</u> \$910	24-25 24-25 24-25
	<u>Note:</u> In February 2024, heavy rains caused an embankment slipout at the southbound cut slope, resulting in mud clogging the drainage ditch, and sending mud and debris down the slope, covering the roadway with mud and debris. This project will remove mud and debris, reconstruct slope embankments, regrade an unlined ditch, and place Rock Slope Protection (RSP) to stabilize slope embankments.				

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
25 04-Nap-121 19.5 2929M 0425000226 3X280	Near Moskowite Corner, at 2.6 miles south of Route 128. Clear and remove debris, construct retaining wall, stabilize slope embankment, and repair pavement. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$40 \$1,750 \$80 <u>\$3,600</u> \$5,470	24-25 24-25 24-25 24-25
	<p><u>Note:</u> Early February 2025 storms brought excessive rainfall that caused water seepage into pavement cracks and oversaturated soils, resulting in pavement settlement, additional pavement cracks, and pavement movement. Additionally, a landslide occurred along the southbound cut slope. This project will clear and remove debris, design and construct a retaining wall, repair pavement by using a combination of sheet piles to avoid soil creep and lightweight Expanding Polyurethane Material (EPM) injections to fill voids, and place Rock Slope Protection (RSP) to stabilize slope embankment.</p>				
26 04-SCI-237 R9.14R 2930C 0425000270 3X520	In Milpitas, at eastbound McCarthy Boulevard onramp. Repair pavement and reconstruct slope embankment. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$421 \$0 <u>\$1,202</u> \$1,623	24-25 24-25 24-25
	<p><u>Note:</u> On February 24, 2025, a field investigation found tension cracks or separation between the lip of curb and gutter and pavement, measuring 123 feet and 193 feet. The damages were likely caused by storm events filling water into these tension cracks and gradually increasing the depth and width of the cracks while also saturating the adjacent slope embankment. This project will repair pavement by injecting Lightweight Expanded Polyurethane Material (LEPM) to fill voids, and reconstruct the slope embankment.</p>				

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
27 04-Sol-80 6.7 2929V 0425000264 3X500	Near Vallejo, at 1.1 mile east of Route 37/80 Separation. Haul away and dispose slide material, reconstruct slope embankment, and repair drainage systems, pavement, and dike. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$700 \$0 <u>\$2,645</u> \$3,345	24-25 24-25
					201.130 Assembly: 4, 11 Senate: 3 Congress: 4, 8
	<u>Note:</u> Heavy winter storms from early February 2025 brought heavy rains that caused saturated soils and unstable slopes, resulting in a landslide onto the Solano Bike Trail (covering approximately 26,000 square feet), cracked trail pavement, and a full trail closure to public use. This project will haul away and dispose of slide material, reconstruct the slope embankment using engineered buttress fill, and repair drainage systems, pavement, and asphalt dike.				
28 04-Son-116 15.4 2930G 0425000286 3X570	Near Noel Heights, at 0.6 mile west of Green Valley Road. Remove loose soil material, and tree stump, construct retaining wall, and install cable railing. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$265 \$30 <u>\$500</u> \$795	24-25 24-25 24-25
					201.130 Assembly: 2 Senate: 2 Congress: 2
	<u>Note:</u> Heavy rains from March 13 - 17, 2025 caused over saturated soils and high water creek flows that resulted in slope failure from the roadside above and an undermined toe of slope from a lodged tree stump below near the creek. This project will remove loose soil material, and tree stump, construct a retaining wall, backfill using engineering imported fill, and install cable railing.				

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
29 07-LA-1 43.7 6301 0725000183 2XR10	Near Malibu, at 0.4 miles east of Las Flores Canyon Road. Remove and replace fire damaged structures and equipment. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$1,400 \$100 \$825 \$100 <u>\$12,375</u> \$14,800	201.130 Assembly: 42 Senate: 24 Congress: 32
<u>Note:</u> The Palisades Fire began on January 7, 2025 and quickly spread, burning over 23,700 acres. On January 13, 2025, maintenance and engineering forces investigated the impact the fire had on the Las Flores Maintenance Station and determined it had caused extensive structural and equipment damage. This project will remove and replace the damaged equipment building, fuel island and tank, storage sheds, mechanical, electrical, and wastewater components, and construct a new retaining wall.					
30 07-LA-1 R39.89/40.04 6369 0725000181 2XS10	In the city of Los Angeles (Pacific Palisades), from 0.3 mile north of Porto Marina Way to Getty Villa Drive. Reconstruct retaining wall and associated drainage systems. <u>Performance Measure</u> 1.0 Location(s)	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$300 \$100 \$1,300 \$100 <u>\$8,200</u> \$10,000	201.130 Assembly: 42 Senate: 24 Congress: 32
<u>Note:</u> On January 7, 2025, the Palisades Fire began and spread quickly through the region. The wildfire burned over 23,700 acres and was 100% contained as of February 11, 2025. The Governor issued a State of Emergency Proclamation on January 7, 2025 and the President issued a State of Emergency Declaration on January 8, 2025. The wildfire destroyed state assets and residences and forced evacuations. The wildfire damaged and burned the slopes and the timber lagging retaining wall. This project will reconstruct the retaining wall and the associated drainage systems.					

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
31 07-LA-Var Var 6298 0725000182 2XQ60	In Los Angeles County, on various routes at various locations. Remove damaged and fallen trees. <u>Performance Measure</u> 22.0 Location(s) <u>Note:</u> On January 7, 2025, high winds started in the area and contributed to fires and also resulted in fallen trees. The winds caused some trees to lose their stability, resulting in either falling or severely leaning. This poses a potential hazard to the traveling public and state and private property. This project will remove damaged and fallen trees at various locations.	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$100 \$0 <u>\$900</u> \$1,000	24-25 24-25 201.130 Assembly: 41, 42, 44, 46, 48, 49, 51, 52, 54, 56, 57 Senate: 20, 22, 24, 25, 26, 27, 28, 30, 32 Congress: 26, 28, 29, 30, 31, 32, 34, 37, 38, 42
32 11-SD-8 R41.8 1616 1125000211 43282	Near Pine Valley, at 0.1 mile west of Pine Valley Creek Bridge. Replace polyester concrete overlay and joint seals. <u>Performance Measure</u> 2.0 Location(s) <u>Note:</u> On February 13, 2025, maintenance forces responded to an emergency call during a storm event to make temporary pavement repairs on the eastbound Pine Valley Bridge. On February 22, 2025, engineering forces investigated the site and recommended repairs for both the left and right bridges. This project will replace the polyester concrete overlays and joint seals for both bridges.	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$1,900 \$0 <u>\$9,500</u> \$11,400	24-25 24-25 201.130 Assembly: 75 Senate: 40 Congress: 48

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists. Perf. Meas.
33 11-SD-Var Var 1610 1125000122 43278	In San Diego County, at Routes 8, 15, 78, and 805 at various locations. Remove and clear debris and vegetation. This project will utilize the Emergency Limited Bid contract method. PA&ED: N/A R/W: N/A RTL: 6/19/2025 BC: 7/19/2025 <u>Performance Measure</u> 5.0 Location(s) Concurrent CONST and CON ENG allocation under Resolution FP-24-95; June 2025. <u>Note:</u> Recent storm events have resulted in sediment and debris buildup, and excessive vegetation growth, in multiple drainage channels throughout San Diego county. This accumulation reduces drainage capacity of the channels and poses a fire risk. This project will remove the excess vegetation to reduce fire risk and restore the drainage channels to their original cross sections.	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$1,900 \$0 <u>\$9,500</u> \$11,400	201.131 Assembly: 74, 76, 78, 79, 80 Senate: 18, 38, 39, 40 Congress: 48, 49, 50, 51, 52
34 12-ORA-39 3.00/3.25 3116 1225000093 0V840	In Huntington Beach, from 0.1 mile south to 0.2 mile north of Ellis Avenue. Abandon culvert, install new culvert, and repair sidewalk. <u>Performance Measure</u> 1.0 Location(s) <u>Note:</u> On February 11, 2025, maintenance forces discovered a sinkhole at the project location. It was determined that the sinkhole was caused by a culvert that had failed during the recent storm event. This project will abandon the existing culvert, install a new culvert, and repair the adjacent sidewalk.	24-25	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$495 \$0 <u>\$1,650</u> \$2,145	201.130 Assembly: 70 Senate: 36 Congress: 47

List of New 2024 SHOPP Capital Project Amendments

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)	Program Code Leg./Congress. Dists. Perf. Meas.
Mobility				
35 04-Nap-121 7.2/7.4 1483B 0414000097 0J890	In the city of Napa, at the intersection with Third Street/East Avenue/Coombsville Road. Construct intersection improvements. Additional STIP RIP contribution of \$1,153,000 for R/W cap; Additional contribution of \$2,400,000 for PA&ED, \$3,510,000 for PS&E, \$600,000 for R/W sup from City of Napa; Additional contribution of \$2,000,000 for R/W cap from Metropolitan Transportation Commission; Additional contribution of \$833,000 for Con sup, \$1,847,000 for R/W cap, \$17,000,000 for Con cap from Napa Valley Transportation Authority.	27-28	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$0 \$0 \$2,480 \$0 <u>\$1,000</u> \$3,480
			27-28 27-28	201.310 Assembly: 4 Senate: 3 Congress: 4
	PA&ED: 5/1/2026 R/W: 3/31/2028 RTL: 4/30/2028 BC: 12/1/2028			
	<u>Performance Measure</u> 400.0 Daily vehicle hour(s) of delay (DVHD)			

List of New 2024 SHOPP Long Lead Project Amendments

Resolution G-00-13, established in June 2000, provides the Department with the means to develop SHOPP projects which require periods longer than the four-year SHOPP cycle. The Commission authorized the Department to program projects for development only when appropriate. Long lead projects must identify challenges that require additional time beyond the typical four years to complete.

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work	FY	Project Costs (\$1,000)		Program Code Leg./Congress. Dists Perf. Meas.
Sustainability and Miscellaneous					
1 04-Sol-220 3.2 2927B 0424000242 1X070	Near Howard Landing, at Steamboat Slough Ferry (J-Mack) Bridge No. 23-0037; also on Route 84 at Real McCoy II Ferry Bridge No. 23-0245 (PM2.49). Replace ferries. (Long Lead Project) PA&ED: 12/31/2027 R/W: 6/28/2030 RTL: 7/31/2030 BC: 5/30/2031 <u>Performance Measure</u> 0.0 Centerline mile(s) Concurrent COS allocation under Resolution FP-24-96; June 2025.	30-31	PA&ED * PS&E * R/W Sup * Con Sup * R/W Cap * Const Cap Total	\$4,000 \$7,956 \$25 \$9,588 \$136 \$47,803 \$69,508	25-26 27-28 27-28 30-31 30-31 30-31 201.999 Assembly: 9, 11 Senate: 3 Congress: 4, 7
* Phase not authorized					

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
1	Near Smith River, from 0.5 mile south of Rowdy Creek Bridge East Denney Street to Rowdy Creek Bridge Road . Construct left turn lane at Timbers Boulevard, install lighting and widen shoulders.	25-26	PA&ED	\$1,093	Prior	\$1,093	Prior
01-DN-101		201.010	PS&E	\$1,027	Prior	\$1,027	Prior
39.2/39.6 39.2/39.7			R/W Sup	\$187	Prior	\$187	Prior
1152			Con Sup	\$1,260	25-26	\$1,260	25-26
0122000010			R/W Cap	\$110	25-26	\$110	25-26
0L430	<u>Performance Measure</u>		<u>Const Cap</u>	<u>\$3,623</u>	25-26	<u>\$3,623</u>	25-26
	0.11 Annual fatal and serious injury collision(s)		Total	\$7,300		\$7,300	
	0.24 Annual fatal and serious injury collision(s)						

Note: Update postmile limits and description after the refinement of scope and project limits. Change in performance measures is to reflect the new methodology.

2	Near Crescent City, at intersection with Elk Valley Cross Road . Construct roundabout and place signage .	25-26	PA&ED	\$710	Prior	\$710	Prior
01-DN-199		201.010	PS&E	\$922	Prior	\$922	Prior
T0.8 T0.6R/0.8			R/W Sup	\$211	Prior	\$211	Prior
1154			Con Sup	\$2,125	25-26	\$2,125	25-26
0122000073			R/W Cap	\$23	25-26	\$23	25-26
0L990	<u>Performance Measure</u>		<u>Const Cap</u>	<u>\$7,094</u>	25-26	<u>\$7,094</u>	25-26
	0.28 Annual fatal and serious injury collision(s)		Total	\$11,085		\$11,085	
	0.32 Annual fatal and serious injury collision(s)						

Note: Adjust postmiles and description due to extended limits of striping to address roundabout impacts to the turning movements at local roads. Change in performance measures is to reflect the new methodology.

3	Near Bridgeville, at Hely Creek Bridge No. 04-0092, Little Larabee Creek Bridge No. 04-0102 (PM 25.3), and Butte Creek Bridge No. 04-0116. Environmental mitigation for revegetation and monitoring for project EA 0C500.	25-26 27-28	PA&ED	\$0		\$0	
01-Hum-36		201.112	PS&E	\$0		\$0	
11.3/34.6			R/W Sup	\$0		\$0	
2363M			Con Sup	\$393	25-26	\$469	27-28
0122000096			R/W Cap	\$18	25-26	\$18	27-28
0C501	<u>Performance Measure</u>		<u>Const Cap</u>	<u>\$376</u>	25-26	<u>\$472</u>	27-28
	0.00 Annual fatal and serious injury collision(s)		Total	\$787		\$959	

Note: Delay fiscal year due to increase in number of working days for construction of the parent project. Construction capital and support increase is due to change in entity that will be doing mitigation work and to also account for the permanent erosion control work that was previously missed.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
4 01-Hum-101 73.3/75.4 2569 0122000046 0L780	Near Eureka, from 1.5 miles south of Herrick Avenue to Sunset Avenue. Construct pedestrian and bicycle infrastructure and rehabilitate drainage systems. <u>Performance Measure</u> 2,225.0 Bicycle and pedestrian infrastructure (linear feet)	26-27 201.400	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$2,029 \$2,027 \$650 \$2,119 \$1,488 <u>\$5,180</u> \$13,493	Prior 25-26 25-26 26-27 26-27 26-27 26-27	\$2,029 \$2,027 \$650 \$2,119 \$1,488 <u>\$5,180</u> \$13,493	Prior 26-27 26-27 26-27 26-27 26-27 26-27

Note: The alternatives developed were found to be not feasible due to impacts to wastewater treatment plant and a private parcel. Additional developed alternative needs a Permit To Enter (PTE) to do technical studies, but the PTE is stuck in legal challenges. This has delayed the environmental technical studies, thus delaying the completion of PA&ED and delaying the start of PS&E and R/W phases. The construction schedule will be adjusted in the future.

5 02-Sha-5 R20.3/R27.9 3792 0220000065 1J390	In and near Shasta Lake City, from 0.7 mile south of Pine Grove Avenue to north of Bridge Bay Road. Rehabilitate drainage system. <u>Performance Measure</u> 39.0 Culvert(s) (ea) 36.0 Culvert(s) (ea)	25-26 201.151	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$1,440 \$930 \$300 \$1,050 \$644 \$8,926 \$13,284	Prior Prior Prior 25-26 25-26 25-26 25-26	\$1,440 \$930 \$300 \$1,050 \$644 \$7,720 \$12,084	Prior Prior Prior 25-26 25-26 25-26 25-26
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Note: Reduction in construction capital and performance is due to elimination of some culvert segments that were found to be outside of the state's right of way.

Performance Measure: Culverts					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Linear feet	0.0	0.0	4,577.5	4,577.5
Post Condition	Linear feet	4,395.5	0.0	0.0	4,395.5

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
6 02-Sha-299 41.1/55.3 3789 0220000061 1J350	Near Redding, from west of Seaman Gulch Road to west of Fenders Ferry Road at various locations. Rehabilitate drainage systems.	25-26 201.151	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$1,750 \$910 \$180 \$950 \$496 <u>\$4,240</u> \$8,526	Prior Prior Prior 25-26 25-26 25-26 25-26	\$1,750 \$910 \$180 \$950 \$496 <u>\$4,240</u> \$8,526	Prior Prior Prior 25-26 25-26 25-26 25-26
	<u>Performance Measure</u>						
	26.0 Culvert(s) (ea)						
	39.0 Culvert(s) (ea)						

Note: As a result of scope refinement during design phase, it was realized that additional culvert segments to some drainage systems will be required. This has resulted in change in performance.

Performance Measure: Culverts					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Linear feet	0.0	392.9	1,504.3	1,897.2
Post Condition	Linear feet	2,086.6	0.0	0.0	2,086.6

7 02-Sha-299 57.5/59.0 3763 0219000152 0J710	Near Montgomery Creek, from 0.1 mile west of Woodhill Drive to 1.0 mile west of Big Bend Road. Improve curves, widen shoulders, correct cross slope, and install guardrail.	25-26 201.010	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$1,710 \$1,170 \$460 \$1,740 \$701 <u>\$6,199</u> \$11,980	Prior Prior Prior 25-26 25-26 25-26 25-26	\$1,710 \$1,170 \$460 \$1,740 \$701 <u>\$8,450</u> \$14,231	Prior Prior Prior 25-26 25-26 25-26 25-26
	<u>Performance Measure</u>						
	0.40 Annual fatal and serious injury collision(s)						
	0.14 Annual fatal and serious injury collision(s)						

Note: Additional construction capital is needed due to increase in quantities and unit costs for drainage, roadway excavation, structural section, and traffic control. Change in performance measures is to reflect the new methodology.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
8	Near Hamburg, at Scott River Bridge No. 02-0079.	26-27	PA&ED	\$2,130	Prior	\$2,130	Prior
02-Sis-96	Replace bridge existing bridge deck and widen	201.110	PS&E	\$1,840	25-26	\$2,340	25-26
71.2	bridge.		R/W Sup	\$190	25-26	\$190	25-26
3660			Con Sup	\$3,860	26-27	\$3,860	26-27
0216000125	<u>Performance Measure</u>		R/W Cap	\$543	26-27	\$543	26-27
0H730	1.0 Bridge(s)		<u>Const Cap</u>	<u>\$12,060</u>	26-27	<u>\$12,060</u>	26-27
			Total	\$20,623		\$21,123	

Note: During project development, it was determined that the deck replacement would require extensive widening to accommodate traffic and worker safety. In addition, scour was discovered around the existing foundations. Therefore, additional PS&E support is needed to pursue the bridge replacement strategy on a new alignment. As design and estimates are further developed, changes to construction and R/W costs will be revised at a later date.

Performance Measure: Bridge(s)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	0.0	7,330.0	7,330.0
Post Condition	Square feet	9,858.0	0.0	0.0	9,858.0

9	Near Yreka, at Dry Gulch Bridge No. 02-0012 and	26-27	PA&ED	\$810	Prior	\$810	Prior
02-Sis-263	Shasta River Bridge No. 02-0014. Rehabilitate	201.110	PS&E	\$740	25-26	\$740	25-26
54.5/56.4	bridges.		R/W Sup	\$30	25-26	\$30	25-26
3785			Con Sup	\$1,530	26-27	\$3,196	26-27
0220000059	<u>Performance Measure</u>		R/W Cap	\$54	26-27	\$54	26-27
1J330	2.0 Bridge(s)		<u>Const Cap</u>	<u>\$14,390</u>	26-27	<u>\$22,336</u>	26-27
			Total	\$17,554		\$27,166	

Concurrent COS allocation under Resolution
FP-24-96; June 2025.

Note: Increase construction support as there was an error in calculating resource dollars and also due to the increase in working days. Further studies revealed that the extent of unsound concrete and the area of repair is significantly more than anticipated. This will require temporary support structures and silane waterproofing. This has resulted in increased construction capital in addition to increase in unit costs for scaffolding, concrete repair, and structures items.

Performance Measure: Bridge(s)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	25,489.0	0.0	25,489.0
Post Condition	Square feet	25,489.0	0.0	0.0	25,489.0

List of 2024 SHOPP Amendments
(Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
10	In Colusa, El Dorado, Sutter, and Sacramento	26-27	PA&ED	\$510	Prior	\$510	Prior
03-Col-5	Counties, on Routes 5, 49, 99, and 104 at various	201.010	PS&E	\$900	25-26	\$900	25-26
R6.67/R7.94 3468 7207 0324000166 03240	locations. Install curve warning signs, flashing beacons, rumble strips, radar speed feedback signs, and intersection warning systems, and upgrade roadside signs.		R/W Sup	\$40	25-26	\$40	25-26
			Con Sup	\$930	26-27	\$930	26-27
			R/W Cap	\$50	26-27	\$50	26-27
			<u>Const Cap</u>	<u>\$4,000</u>	26-27	<u>\$4,000</u>	26-27
			Total	\$6,430		\$6,430	

Performance Measure

1.17 Annual fatal and serious injury collision(s)

Note: Change duplicate PPNO.

11	In Butte, Colusa, and Sutter, and Yolo Counties, on	25-26	PA&ED	\$1,730	Prior	\$1,730	Prior
03-Col-5	Routes 5, 46, 20, 70, and 505 at various locations.	201.111	PS&E	\$1,200	Prior	\$1,200	Prior
Var	Bridge scour mitigation.		R/W Sup	\$330	Prior	\$330	Prior
8145			Con Sup	\$1,130	25-26	\$750	25-26
0319000304	<u>Performance Measure</u>		R/W Cap	\$3,340	25-26	\$2,105	25-26
0J630	<u>9.0 Bridge(s)</u>		<u>Const Cap</u>	\$3,920	25-26	\$1,950	25-26
	4.0 Bridge(s)		Total	\$11,620		\$8,065	

Note: After further field inspections, it was determined that five structures do not require rock slope protection, but rather routine maintenance and repairs. The reduction in scope resulted in change in description and performance, and decrease in costs.

Performance Measure: Bridge(s)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	10,376.0	166,109.0	176,485.0
Post Condition	Square feet	10,376.0	0.0	166,109.0	176,485.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
12	In and near Orland and Hamilton City, from Route 5	25-26	PA&ED	\$1,360	Prior	\$1,360	Prior
03-Gle-32	to Butte County line. Rehabilitate pavement and	201.121	PS&E	\$1,770	Prior	\$1,770	Prior
L0.0/10.9	drainage systems, and upgrade facilities to		R/W Sup	\$1,140	Prior	\$1,140	Prior
3790	Americans with Disabilities Act (ADA) standards.		Con Sup	\$1,670	25-26	\$1,670	25-26
0319000291			R/W Cap	\$657	25-26	\$657	25-26
0J500	<u>Performance Measure</u>		<u>Const Cap</u>	\$13,960	25-26	\$16,375	25-26
	20.7 Lane mile(s)		Total	\$20,557		\$22,972	

Note: Additional construction capital is needed due to increase in unit costs for rubberized asphalt, hot mix asphalt, and pavement grinding. Some items, such as concrete transitions for guardrails on bridges and railroad crossing work, were previously omitted in the cost estimate.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	20.7	0.0	20.7
Post Condition	Lane mile(s)	20.7	0.0	0.0	20.7

13	Near Truckee, at Donner Summit Safety Roadside	26-27	PA&ED	\$160	Prior	\$160	Prior
03-Nev-80	Rest Areas (SRRAs) in eastbound and westbound	27-28	PS&E	\$740	25-26	\$800	25-26
R5.3R/R5.8R	(PM R5.6L) directions. Rehabilitate water and sewer	201.250	R/W Sup	\$90	25-26	\$90	25-26
4325	lines and construct new median utility line cross		Con Sup	\$550	26-27	\$1,020	27-28
0323000003	over.		R/W Cap	\$360	26-27	\$360	27-28
3J790			<u>Const Cap</u>	\$2,300	26-27	\$5,520	27-28
	<u>Performance Measure</u>		Total	\$4,170		\$7,950	
	2.0 Location(s)						

Note: On a Minor A project EA 03-1J400, the utility company informed the Department that separate cross overs will be needed to house wet and dry utilities and that the existing utility cross over cannot be used for both. Therefore, the scope of that other project is being included in this project. The project description, schedule, and costs are being updated to account for this additional work.

Performance Measure: Safety Roadside Rest Area (SRRA)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Each	0.0	2.0	0.0	2.0
Post Condition	Each	2.0	0.0	0.0	2.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
14 03-Pla-267 0.00/9.63 5728 0321000197 2J190	Near Truckee, from Nevada County line to Dolly Varden Avenue. Rehabilitate pavement and drainage systems, upgrade guardrail and facilities to Americans with Disabilities Act (ADA) standards, and extend southbound truck climbing lane.	26-27 201.121	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$3,280 \$3,130 \$600 \$3,850 \$1,240 \$31,900 \$44,000	Prior 25-26 25-26 26-27 26-27 26-27 26-27	\$3,280 \$3,130 \$600 \$3,850 \$1,240 \$33,500 \$45,600	Prior 25-26 25-26 26-27 26-27 26-27 26-27
	<u>Performance Measure</u> 19.3 Lane mile(s)						

Note: Increase in construction capital is due to work on additional 22 drainage systems which will be impacted because of the proposed widening.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	19.3	0.0	19.3
Post Condition	Lane mile(s)	19.3	0.0	0.0	19.3

15 03-Yol-16 21.2/21.9 8679 0323000285 4J880	Near Cadenasso, from 0.3 mile east of County Road 79 to 0.2 mile west of County Road 81. Construct 4-foot shoulders, rumble strips, and retaining wall, rehabilitate drainage systems, and upgrade signs and guardrail.	26-27 27-28 201.010	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$1,150 \$1,310 \$280 \$1,770 \$115 \$4,900 \$9,525	Prior 25-26 25-26 26-27 26-27 26-27 26-27	\$1,150 \$1,310 \$280 \$1,770 \$115 \$4,900 \$9,525	Prior 25-26 25-26 27-28 27-28 27-28 27-28
	<u>Performance Measure</u> 0.23 Annual fatal and serious injury collision(s) 0.57 Annual fatal and serious injury collision(s)						

Note: Delay project delivery by one fiscal year to allow for sufficient time needed for R/W Certification due to numerous utility relocations. This delay will not cause any delays to the beginning of construction season. Change in performance measures is to reflect the new methodology.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
16 04-Ala-238 0.0/7.8 2912J 0421000323 3W180	In and near Fremont and Union City, from Route 680 to Lexington Avenue. Rehabilitate pavement, construct Class 4 bike lanes, pedestrian and bicycle infrastructure, landscape planting, and upgrade curb ramps to Americans with Disabilities Act (ADA) standards.	27-28 29-30 201.121	PA&ED * PS&E * R/W Sup * Con Sup * R/W Cap * <u>Const Cap</u> Total	\$3,982 \$5,439 \$647 \$10,492 \$1,158 \$42,713 \$64,395	Prior 25-26 25-26 27-28 27-28 27-28 27-28	\$3,982 \$6,118 \$695 \$11,373 \$1,255 \$46,301 \$69,724	Prior 28-29 28-29 29-30 29-30 29-30 29-30

Performance Measure

30.9 Lane mile(s)

* Phase not authorized

Note: Extensive complete streets effort on the route result in increased impacts to and R/W acquisition from the Mission San Jose Community Park requiring enhanced consultations and studies to comply with federal statutes. As a result, the project delivery is delayed by two years and cost increased for PS&E, R/W support, R/W capital, construction support, and construction capital costs due to escalation.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	30.9	0.0	30.9
Post Condition	Lane mile(s)	30.9	0.0	0.0	30.9

17 04-CC-123 0.0/2.198 2031N 0419000452 0AA21	In El Cerrito and Richmond , from Alameda County line to Route 80. Rehabilitate pavement and upgrade facilities to Americans with Disabilities Act (ADA) standards upgrade safety lighting.	25-26 201.121	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$835 \$1,532 \$179 \$2,260 \$25 \$9,122 \$13,953	Prior Prior Prior 25-26 25-26 25-26 25-26	\$835 \$1,532 \$179 \$2,260 \$25 \$10,490 \$15,321	Prior Prior Prior 25-26 25-26 25-26 25-26
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Performance Measure

11.2 Lane mile(s)

Note: It was determined that the current cost estimate would require an additional \$6,000,000 construction capital and \$1,500,000 construction support. To keep close to programmed costs but to also prioritize public safety, the PDT has chosen to eliminate all complete streets work and add safety lightings. This project will update description and increase construction capital cost to reflect the updated scope.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	11.2	0.0	11.2
Post Condition	Lane mile(s)	11.2	0.0	0.0	11.2

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)					
18 04-CC Ala-24 R0-04 R5.8/R6.2 0480B 0414000011 0J540	In Orinda, at the Caldecott Tunnel No. 28-0015R, 28-0015, and 28-0015L.; also in Contra Costa at Caldecott Tunnel (R0.0/R0.6). Rehabilitate and upgrade ventilation system of Caldecott Tunnel Bores 1, 2, and 3. (G13 Contingency) <u>Performance Measure</u> 3.0 Bridge(s)	25-26 201.110	PA&ED PS&E R/W Sup * Con Sup R/W Cap * <u>Const Cap</u> Total	\$6,269 \$14,400 \$184 \$20,850 \$253 \$163,000 \$204,956	Prior 24-25 24-25 25-26 25-26 25-26 25-26	\$6,269 \$14,400 \$184 \$20,850 \$253 \$200,652 \$242,608	Prior 24-25 24-25 25-26 25-26 25-26 25-26	
			* Phase not authorized					

Note: Increase scope of work by adding upgraded ventilation system. As a result, change description and increase costs for construction capital. Also, change post mile limits to include areas of guardrail and traffic control.

Performance Measure: Bridge(s)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	785,287.0	0.0	785,287.0
Post Condition	Square feet	785,287.0	0.0	0.0	785,287.0

19 04-Mrn-37 14.5 0332C 0419000019 2Q500	Near Novato, at Petaluma River Bridge No. 27-0013. Rehabilitate bridge deck, upgrade railings, replace fender system, and mitigate bridge scour. (G13 Contingency)	23-24 201.110	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$1,810 \$3,630 \$110 \$0 \$130 \$0 \$5,680	Prior Prior Prior 23-24 23-24 23-24 23-24	\$1,810 \$3,630 \$110 \$7,030 \$130 \$31,040 \$43,750	Prior Prior Prior 23-24 23-24 23-24 23-24
	<u>Performance Measure</u> 1.0 Bridge(s)						

Concurrent CONST and CON ENG allocation under Resolution FP-24-95; June 2025.

Note: Fully program unfunded phases of this previous G13 Contingency project.

Performance Measure: Bridge(s)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	0.0	146,830.0	146,830.0
Post Condition	Square feet	146,830.0	0.0	0.0	146,830.0

(Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)	
20	In and near Novato, from Route 101 to Atherton	25-26	PA&ED	\$0	\$0
04-Mrn-37	Avenue. Replace Novato Creek Bridge No.	201.999	PS&E	\$0	\$0
R11.2/13.8	27-0011L/R and construct drainage systems.		R/W Sup	\$0	\$0
2925W	Additional contribution of \$15,000,000 for PS&E from	*	Con Sup	\$25,000	25-26 \$30,000 25-26
0424000193	the General Fund provided by Assembly Bill 179 (AB		R/W Cap	\$200	25-26 \$4,200 25-26
4Q321	179); Additional contribution of \$25,000,000 for	*	<u>Const Cap</u>	<u>\$130,000</u>	25-26 <u>\$186,000</u> 25-26
	Const Cap from Transportation Authority of		Total	\$155,200	\$220,200
	Marin (TAM).				
	(G13 Contingency)				
			* Phase not authorized		

Performance Measure

0.2 Centerline mile(s)

Note: Update description adding additional funding from TAM for pedestrian and bike path. Increase construction capital due to bridge structure involved items including increased cost index, foundations and pile depth, added stage construction, and transition structures. Increase construction support due to a revised traffic handling plan and more extensive than anticipated environmental monitoring requirements, and RW capital to fund environmental mitigation.

21	Near Napa, from 2.8 miles north of Vichy Avenue to	23-24	PA&ED	\$1,635	Prior	\$1,635	Prior
04-Nap-121	1.4 miles south of Route 128 at various locations.	201.131	PS&E	\$2,053	Prior	\$2,053	Prior
13.4/20.7	Repair and restore slope washouts with Rock Slope		R/W Sup	\$199	Prior	\$199	Prior
2022D	Protection (RSP), erosion control, steel sheet piles,		Con Sup	\$1,361	23-24	\$1,361	23-24
0418000153	and drainage upgrades.		R/W Cap	\$639	23-24	\$639	23-24
0Q790			<u>Const Cap</u>	<u>\$4,304</u>	23-24	<u>\$2,789</u>	23-24
	Performance Measure		Total	\$10,191		\$8,676	

Performance Measure

5.0 Location(s)

Concurrent CONST and CON ENG allocation under Resolution FP-24-95: June 2025.

Note: Split plant establishment and environmental mitigation into project EA 0Q79A/PPNO 04-2929W from parent project EA 0Q790/PPNO 04-2022D.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)	
22 04-Nap-121 13.4/20.7 2929W 0425000234 0Q79A	Near Napa, from 2.8 miles north of Vichy Avenue to 1.4 miles south of Route 128 at various locations. Plant establishment and environmental mitigation work for permanent restoration EA 0Q790. <u>Performance Measure</u> 0.0 Location(s)	27-28 201.131	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$200 25-26 \$10 25-26 \$675 27-28 \$30 27-28 \$600 27-28 \$1,515

Note: Split plant establishment and environmental mitigation into project EA 0Q79A/PPNO 04-2929W from parent project EA 0Q790/PPNO 04-2022D.

23 04-SCI-880 0.0/10.502 2915J 0419000436 4Q770	In and near San Jose and Milpitas, from Route 280 to 0.1 mile north of Dixon Landing Road. Rehabilitate pavement, upgrade curb ramps to Americans with Disabilities Act (ADA), guardrail, and rectangular rapid flashing beacons (RRFBs). <u>Performance Measure</u> 81.7 Lane mile(s)	27-28 29-30 201.121	PA&ED * PS&E * R/W Sup * Con Sup * R/W Cap * <u>Const Cap</u> Total	\$3,471 Prior \$7,877 25-26 \$265 25-26 \$8,534 27-28 \$220 27-28 \$75,647 27-28 \$96,011	\$3,471 Prior \$8,620 28-29 \$290 28-29 \$9,073 29-30 \$234 29-30 \$80,458 29-30 \$102,146
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* Phase not authorized

Note: The PDT identified several additional storm drains also requiring repair that will need a higher level environmental assessment requiring additional time. As a result, the project delivery is delayed by three years and cost increased for PS&E, R/W support, R/W capital, construction support, and construction capital costs due to escalation.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	19.7	62.0	0.0	81.7
Post Condition	Lane mile(s)	81.7	0.0	0.0	81.7

List of 2024 SHOPP Amendments
(Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
24	In the City and County of San Francisco, from San	23-24	PA&ED	\$2,594	Prior	\$2,594	Prior
04-SF-101	Mateo County line to Market Street. Rehabilitate	201.120	PS&E	\$4,500	Prior	\$4,500	Prior
0.0/T4.9	roadway, upgrade signs, concrete barrier, and		R/W Sup	\$223	Prior	\$223	Prior
2904K	Transportation Management System (TMS)		Con Sup	\$0	23-24	\$6,833	23-24
0418000341	elements, rehabilitate drainage, and upgrade		R/W Cap	\$250	23-24	\$250	23-24
1Q820	facilities to Americans with Disabilities Act (ADA)		<u>Const Cap</u>	\$0	23-24	<u>\$69,027</u>	23-24
	standards.		Total	\$7,567		\$83,427	
	(G13 Contingency)						

Performance Measure

15.4 Lane mile(s)

Concurrent Greater than 20 percent CONST and CON ENG allocation under Resolution FP-24-85; June 2025.

Note: Fully program unfunded phases of this previous G13 Contingency project.

Performance Measure: Pavement					
	Unit	Good	Fair	Poor	Quantity
Existing Condition	Lane mile(s)	1.1	11.6	2.7	15.4
Post Condition	Lane mile(s)	15.4	0.0	0.0	15.4

25	In the City and County of San Francisco, from south	25-26	PA&ED	\$751	Prior	\$751	Prior
04-SF-101	of Silver Avenue to 16th Street, at various locations.	26-27	PS&E	\$857	Prior	\$857	Prior
1.7/4.2	Rehabilitate highway planting.	201.210	R/W Sup	\$80	Prior	\$80	Prior
2028C			Con Sup	\$895	25-26	\$895	26-27
0419000029	<u>Performance Measure</u>		R/W Cap	\$10	25-26	\$10	26-27
2Q600	18.0 Acre(s)		<u>Const Cap</u>	\$4,251	25-26	\$4,251	26-27
			Total	\$6,844		\$6,844	

Note: Delay project delivery by one year to allow overlapping roadway rehabilitation project EA 04-1Q820 to complete construction before beginning this landscape construction. Originally, EA 04-1Q820 was to complete construction by spring 2027 but has recently received a time extension and is now expecting construction completion in early 2028.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
26	Near San Gregorio, at San Gregorio Creek Bridge	25-26	PA&ED	\$2,866	Prior	\$2,866	Prior
04-SM-1	No. 35-0030. Bridge seismic retrofit and upgrade	26-27	PS&E	\$2,932	23-24	\$2,932	23-24
17.9/18.0	bridge rails.	201.113	R/W Sup	\$183	23-24	\$183	23-24
2021J			Con Sup	\$3,160	25-26	\$8,126	26-27
0418000035	<u>Performance Measure</u>		R/W Cap	\$438	25-26	\$1,782	26-27
0Q010	1.0 Bridge(s)		<u>Const Cap</u>	\$12,280	25-26	\$43,854	26-27
			Total	\$21,859		\$59,743	

Note: Increase construction capital and support due to price index escalation, added architectural features to bridge, added retaining walls, COZEED, additional on-site environmental mitigation and increased number of working days. Increase RW capital due the additional mitigation. Update existing condition performance to reflect inventory. Delay delivery year one year due to a delayed release of the Draft Environmental Document.

Performance Measure: Bridge(s)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	13,515.0	0.0	0.0	13,515.0
Post Condition	Square feet	13,515.0	0.0	0.0	13,515.0

27	In Daly City, at northbound Route 280 to southbound	25-26	PA&ED	\$651	Prior	\$651	Prior
04-SM-280	Route 1 Connector No. 35-0179G. Seismic retrofit.	201.113	PS&E	\$1,266	Prior	\$1,266	Prior
R25.3			R/W Sup	\$48	Prior	\$48	Prior
2024H	<u>Performance Measure</u>		Con Sup	\$1,298	25-26	\$1,480	25-26
0418000311	1.0 Bridge(s)		R/W Cap	\$7	25-26	\$30	25-26
1Q640			<u>Const Cap</u>	\$1,689	25-26	\$6,480	25-26
			Total	\$4,959		\$9,955	

Note: Increase construction capital and support due to added scope of work including a seismic retrofit at three footings and increase of structural costs to be in alignment with latest market conditions. There is no change to the number of working days. Increase RW capital due to additional funds needed to provide positive underground utility verification within the limits of this added scope.

Performance Measure: Bridge(s)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	0.0	17,943.0	17,943.0
Post Condition	Square feet	17,943.0	0.0	0.0	17,943.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
28	In and near Vallejo, Fairfield and Vacaville, from	25-26	PA&ED	\$445	Prior	\$445	Prior
04-Sol-80	Route 29 to Nut Tree Road. Replace traffic striping	201.010	PS&E	\$510	24-25	\$510	24-25
1.0/R28.0	and pavement markings at ramps.		R/W Sup	\$12	24-25	\$12	24-25
2915C			Con Sup	\$509	25-26	\$663	25-26
0422000412	<u>Performance Measure</u>		R/W Cap	\$48	25-26	\$48	25-26
1Y000	0.09 Annual fatal and serious injury collision(s)		<u>Const Cap</u>	\$1,464	25-26	\$1,814	25-26
	0.20 Annual fatal and serious injury collision(s)		Total	\$2,982		\$3,492	

Note: Update performance measures to reflect the new methodology. Increase construction capital due to a refined cost estimate adding COZEEP, and additional traffic control, and erosion control. On review of the traffic control system, the number of working days was found to be insufficient and therefore increased from 30 to 65. As a result, construction support is also increased.

29	Near Rio Vista, at Miner Slough Bridge No. 23-0035.	25-26	PA&ED	\$0	Prior	\$0	Prior
04-Sol-84	Replace bridge.	26-27	PS&E	\$2,291	Prior	\$2,291	Prior
12.0/12.40		201.116	R/W Sup	\$800	Prior	\$800	Prior
0886	<u>Performance Measure</u>		Con Sup	\$8,000	25-26	\$14,410	26-27
0400000343	1.0 Bridge(s)		R/W Cap	\$2,800	25-26	\$2,800	26-27
0G660			<u>Const Cap</u>	\$44,400	25-26	\$58,550	26-27
			Total	\$58,291		\$78,851	

Note: This project will be constructed on existing non-engineered levees which introduces complexities in both design and constructability. As a result, the delivery schedule will be delayed by one year. Also, construction capital and support will increase due to additional soldier pile depth, unit price increases to match current bids, added scope for temporary roadway and retaining wall to reduce lengthy traffic closures, and increased number of working days.

Performance Measure: Bridge(s)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	6,986.0	0.0	6,986.0
Post Condition	Square feet	17,600.0	0.0	0.0	17,600.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
30	Near Sea Ranch, from north of Moonraker Road to	25-26	PA&ED	\$829	Prior	\$829	Prior
04-Son-1	north of Leeward Spur, at various locations.	26-27	PS&E	\$1,361	Prior	\$1,361	Prior
51.1/56.4	Rehabilitate drainage systems.	201.151	R/W Sup	\$495	Prior	\$495	Prior
2907A			Con Sup	\$1,710	25-26	\$1,710	26-27
0420000285	<u>Performance Measure</u>		R/W Cap	\$796	25-26	\$796	26-27
0W740	23.0 Culvert(s) (ea)		<u>Const Cap</u>	\$4,286	25-26	\$4,874	26-27
	14.0 Culvert(s) (ea)		Total	\$9,477		\$10,065	

Note: During PA&ED phase, a site investigation determined that multiple culverts are undersized and require a temporary creek diversion system to replace and upsize. As a result, an increase to construction capital and a decrease in performance is needed to stay on budget. Remaining culverts, deemed to be in fair condition, will be programmed in a future delivery year. Delay project delivery due to 3 additional required parcel acquisitions.

Performance Measure: Culverts					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Linear feet	0.0	1,024.5	0.0	1,024.5
Post Condition	Linear feet	1,024.5	0.0	0.0	1,024.5

31	Near the city of Sonoma, from Waterman Avenue 0.2 mile east of Siesta Way to Lomita Avenue.	25-26	PA&ED	\$1,000	Prior	\$1,000	Prior
04-Son-12		201.015	PS&E	\$1,010	Prior	\$1,010	Prior
34.93/36.4	Construct left-turn lane onto Verano Avenue, install		R/W Sup	\$470	Prior	\$470	Prior
35.75/36.1	audible accessible pedestrian signals (APS), and		Con Sup	\$1,230	25-26	\$1,500	25-26
2921L	upgrade facilities to Americans with Disabilities Act		R/W Cap	\$150	25-26	\$150	25-26
0423000292	(ADA) standards.		<u>Const Cap</u>	\$4,480	25-26	\$5,800	25-26
3Y710			Total	\$8,340		\$9,930	
	<u>Performance Measure</u>						
	0.04 Annual fatal and serious injury collision(s)						
	0.02 Annual fatal and serious injury collision(s)						

Note: Increase construction capital and support due to design modifications including additional retaining wall, adding plant establishment period and COZEED, and elimination of Rectangular Rapid Flashing Beacons (RRFBs). Update post mile, description, and performance due to the removal of these RRFBs.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
34	Near Carmel-by the-Sea, at Rocky Creek Bridge No.	27-28	PA&ED	\$1,221	Prior	\$1,221	Prior
05-Mon-1	44-0036. Replace bridge rail, overlay bridge deck,	28-29	PS&E	\$2,123	25-26	\$2,123	26-27
60.1	and replace bridge approach railing.	201.112	R/W Sup	\$48	25-26	\$48	26-27
3104			* Con Sup	\$2,154	27-28	\$2,154	28-29
0522000172	<u>Performance Measure</u>		* R/W Cap	\$37	27-28	\$37	28-29
1Q140	0.01 Annual fatal and serious injury collision(s)		* <u>Const Cap</u>	<u>\$6,532</u>	27-28	<u>\$6,532</u>	28-29
			Total	\$12,115		\$12,115	

* Phase not authorized

Note: Update delivery schedule due to additional time needed in preliminary engineering for the bridge rail design alternative to be approved.

35	Near Castroville, Moss Landing, and Watsonville,	25-26	PA&ED	\$2,875	Prior	\$2,875	Prior
05-Mon-1	from north of Molera Road to Santa Cruz County	26-27	PS&E	\$3,578	Prior	\$3,578	Prior
R90.980/R102.031	line. Rehabilitate pavement, upgrade Transportation	201.121	R/W Sup	\$179	Prior	\$179	Prior
2889	Management System (TMS) elements, upgrade		Con Sup	\$3,088	25-26	\$3,088	26-27
0519000034	facilities to Americans with Disabilities Act (ADA)		R/W Cap	\$102	25-26	\$157	26-27
1K870	standards, and replace guardrail and sign panels.		<u>Const Cap</u>	<u>\$16,816</u>	25-26	<u>\$19,090</u>	26-27
			Total	\$26,632		\$28,967	

Performance Measure

22.4 Lane mile(s)

Note: Update to delivery year to address environmental impacts from newly encountered Monarch Butterfly endangered species. Right of way capital, and construction to address inflationary cost of asphalt, mobilization, and traffic contract items, as well for an increased number of existing utility positive location verifications (POSLOC) increased from 25 to 59. Update to the existing condition in the performance table.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	3.4	19.0	0.0	22.4
Post Condition	Lane mile(s)	22.4	0.0	0.0	22.4

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
36	Near San Ardo and San Lucas, from Cattlemen	25-26	PA&ED	\$2,078	Prior	\$2,078	Prior
05-Mon-101	Road to south of Paris Valley Road Overcrossing.	26-27	PS&E	\$2,616	Prior	\$2,616	Prior
R22.00/R28.00	Rehabilitate roadway, reconstruct guardrail and	201.122	R/W Sup	\$65	Prior	\$65	Prior
3009	drainage systems, and replace Transportation		Con Sup	\$5,608	25-26	\$5,608	26-27
0519000119	Management System (TMS) elements and signs.		R/W Cap	\$80	25-26	\$80	26-27
1M350			<u>Const Cap</u>	<u>\$37,114</u>	25-26	<u>\$37,114</u>	26-27
	<u>Performance Measure</u>		Total	\$47,561		\$47,561	
	24.0 Lane mile(s)						

Note: Updated delivery year to align construction schedules with planned combined for construction with project EA 1K490 scheduled year of delivery.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	2.9	19.6	1.5	24.0
Post Condition	Lane mile(s)	24.0	0.0	0.0	24.0

37	In and near the cities of Santa Barbara and Goleta,	25-26	PA&ED	\$3,325	Prior	\$3,325	Prior
05-SB-101	from south of Milpas Street Undercrossing to North	201.121	PS&E	\$3,636	Prior	\$3,636	Prior
12.4/22.6	Fairview Avenue. Rehabilitate pavement, replace		R/W Sup	\$346	Prior	\$346	Prior
2798	bridge rail and concrete barrier, seismic restoration,		Con Sup	\$7,293	25-26	\$8,293	25-26
0518000085	install Transportation Management System (TMS)		R/W Cap	\$425	25-26	\$425	25-26
1J900	elements, rehabilitate drainage systems, and		<u>Const Cap</u>	<u>\$52,677</u>	25-26	<u>\$61,700</u>	25-26
	enhance highway worker safety.		Total	\$67,702		\$77,725	

Performance Measure
56.6 Lane mile(s)

Note: Update construction cost to address refined cost estimate and increases to multiple contract items including bridge rail replacement and retrofit, rubberized asphalt, mobilization, and traffic control, as well as an additional 45 working days for the constructability of 96 concrete anchor blocks.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	13.3	43.3	0.0	56.6
Post Condition	Lane mile(s)	56.6	0.0	0.0	56.6

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)					
38	Near Santa Barbara, from Dos Pueblos Creek	27-28	PA&ED	\$2,646	Prior	\$2,646	Prior	
05-SB-101	Undercrossing to Route 1. Rehabilitate pavement,	201.999	PS&E	\$4,339	25-26	\$4,339	25-26	
30.1/R48.8	replace sign panels, upgrade guardrail, and improve		R/W Sup	\$185	25-26	\$375	25-26	
3072	wildlife crossing. Includes federal Wildlife Crossings		Con Sup	\$7,824	27-28	\$7,824	27-28	
0521000172	Pilot Program (WCPP) Grant amount of \$8,000,000.		R/W Cap	\$188	27-28	\$453	27-28	
1P130			<u>Const Cap</u>	<u>\$50,924</u>	27-28	<u>\$50,924</u>	27-28	
	<u>Performance Measure</u>		Total	\$66,026		\$66,561		
	1.0 Location(s)							

Note: Update right of way due to additional utility relocations and verifications needed for scope refinement of the wildlife crossing.

39	In and near Guadalupe and Santa Maria, from Route	25-26	PA&ED	\$1,415	Prior	\$1,415	Prior	
05-SB-166	1 to Route 101. Rehabilitate pavement, upgrade	27-28	PS&E	\$2,863	Prior	\$2,863	Prior	
0.000/8.927	Transportation Management System (TMS)	201.121	R/W Sup	\$763	Prior	\$763	Prior	
3006	elements, guardrail, and signs, install complete		Con Sup	\$4,263	25-26	\$4,590	27-28	
0519000093	streets and safety improvements, improve pedestrian		R/W Cap	\$69	25-26	\$467	27-28	
1M310	crossing, and upgrade facilities to Americans with		<u>Const Cap</u>	<u>\$18,381</u>	25-26	<u>\$21,210</u>	27-28	
	Disabilities Act (ADA) standards. Signalize		Total	\$27,754		\$31,308		
	intersections, and install traffic signal preemption in							
	UPRR rail crossing and median.							

(Additional contribution to PS&E, RW Support, RW Capital, Construction Support, and Construction Capital from City of Guadalupe and Santa Barbara County Association of Governments)

Performance Measure
22.6 Lane mile(s)

Note: Update delivery year to address right of way challenges. The project includes multiple utility relocations, r/w acquisitions, and coordination with railroad for a new railroad crossing. The properties involve railroad, agriculture, and businesses. Update right of way capital due to refined R/W data sheet that reflects an increase in the number of parcels from 16 to 36. Update construction to account for associated escalation.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	22.6	0.0	22.6
Post Condition	Lane mile(s)	22.6	0.0	0.0	22.6

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
40	In and near the city of Santa Cruz, from south of	25-26	PA&ED	\$1,591	Prior	\$1,591	Prior
05-SCr-1	River Street (Route 9) to north of Western Drive.	26-27	PS&E	\$2,119	Prior	\$2,119	Prior
17.5/20.2	Rehabilitate pavement, reconstruct guardrail, replace	201.121	R/W Sup	\$903	Prior	\$903	Prior
2725	sign panels, modify transit stops, upgrade facilities to		Con Sup	\$2,111	25-26	\$2,111	26-27
0519000067	Americans with Disabilities Act (ADA) standards,		R/W Cap	\$190	25-26	\$990	26-27
1M110	update crosswalks, and repair Class 2 bike lanes.		<u>Const Cap</u>	<u>\$9,892</u>	25-26	<u>\$15,672</u>	26-27
			Total	\$16,806		\$23,386	

Performance Measure

~~8.3 Lane mile(s)~~

8.2 Lane mile(s)

Note: Update delivery year and right of way capital to address the number of utility verifications needed from 40 to 348 locations to evaluate potential utility conflicts with ADA curb ramp locations in downtown urban area. Update performance to reflect the latest pavement data.

Performance Measure: Pavement

	Unit	Good	Fair	Poor	Quantity
Existing Condition	Lane mile(s)	0.1	8.0	0.0	8.1
Post Condition	Lane mile(s)	8.2	0.0	0.0	8.2

41	At the San Lorenzo River, in Castle Rock State Park.	25-26	PA&ED	\$300	Prior	\$300	Prior
05-SCr-9	Replace Waterman Gap culvert with a bridge to	201.999	PS&E	\$2,824	Prior	\$2,824	Prior
19.2	improve fish passage. Includes federal Culvert		R/W Sup	\$387	Prior	\$387	Prior
2880F	Aquatic Organism Passage (AOP) Grant amount of		Con Sup	\$1,888	25-26	\$1,888	25-26
0523000059	\$6,000,000.		R/W Cap	\$55	25-26	\$81	25-26
1K901	(Additional contribution of \$2,000,000 from State		<u>Const Cap</u>	<u>\$6,253</u>	25-26	<u>\$9,171</u>	25-26
	Parks.)		Total	\$11,707		\$14,651	

Performance Measure

1.0 Location(s)

Note: Update right of way and construction capital to address refined design in bridge profile, and a refined structure estimate for items such as mobilization, bridge removal, irrigation, National Pollutant Discharge Elimination System (NPDES), and streambed restoration of fish passage.

List of 2024 SHOPP Amendments
(Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
42	In Watsonville, from Freedom Boulevard to Beck	25-26	PA&ED	\$1,926	Prior	\$1,926	Prior
05-SCr-152	Street. Construct bulbouts and high visibility	201.010	PS&E	\$1,896	25-26	\$1,896	25-26
T2.5/0.7	crosswalks and upgrade a pedestrian beacon.		R/W Sup	\$46	25-26	\$46	25-26
3105			Con Sup	\$1,578	25-26	\$1,578	25-26
0522000174	<u>Performance Measure</u>		R/W Cap	\$62	25-26	\$296	25-26
1Q150	0.38 Annual fatal and serious injury collision(s)		<u>Const Cap</u>	\$4,618	25-26	\$5,415	25-26
	0.24 Annual fatal and serious injury collision(s)		Total	\$10,126		\$11,157	

Note: Update right of way capital due to additional utility verifications needed that were identified after utility mapping was completed. Update construction capital due to refined cost estimate for various contract items. Update performance measures to reflect the new methodology.

43	Near Morro Bay, at Toro Creek Bridge No. 49-0068L.	25-26	PA&ED	\$1,200	Prior	\$1,200	Prior
05-SLO-1	Replace bridge.	201.116	PS&E	\$2,419	Prior	\$2,419	Prior
32.6			R/W Sup	\$159	Prior	\$159	Prior
3165	<u>Performance Measure</u>		Con Sup	\$1,800	25-26	\$2,500	25-26
0523000125	1.0 Bridge(s)		R/W Cap	\$2,306	25-26	\$2,306	25-26
1R100			<u>Const Cap</u>	\$13,948	25-26	\$13,248	25-26
			Total	\$21,832		\$21,832	

Note: Update construction cost due to updated bridge construction schedule and refined cost estimate. Split biological monitoring work into project EA 1R101/PPNO 05-3165X from parent project EA 1R100/PPNO 05-3165.

Performance Measure: Bridge(s)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	0.0	4,639.0	4,639.0
Post Condition	Square feet	4,639.0	0.0	0.0	4,639.0

44	Near Morro Bay, at Toro Creek Bridge No.	26-27	PA&ED	\$0			
05-SLO-1	49-0068L. Biological monitoring for bridge	201.116	PS&E	\$0			
32.6	preservation project EA 1R100.		R/W Sup	\$0			
3165X	<u>Performance Measure</u>		Con Sup	\$40	26-27		
0525000210	0.0 Bridge(s)		R/W Cap	\$15	26-27		
1R101			<u>Const Cap</u>	\$0			
			Total	\$55			

Note: Split biological monitoring work into project EA 1R101/PPNO 05-3165X from parent project EA 1R100/PPNO 05-3165.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)					
45	Near Shaver Lake, from 0.6 mile west to 0.2 mile	24-25	PA&ED	\$4,800	Prior	\$4,800	Prior	
06-Fre-168	west of Huntington Lake Road. Construct sidehill	201.131	PS&E	\$4,300	Prior	\$4,300	Prior	
49.1/49.5	viaduct structure.		R/W Sup	\$240	Prior	\$240	Prior	
7061			Con Sup	\$7,500	24-25	\$7,500	24-25	
0620000065	<u>Performance Measure</u>		R/W Cap	\$681	24-25	\$681	24-25	
1A090	1.0 Location(s)		<u>Const Cap</u>	\$45,000	24-25	\$40,055	24-25	
			Total	\$62,521		\$57,576		

Note: Split landscape mitigation planting into project EA 1A091/PPNO 06-8148 and split biological off-site Riparian mitigation planting and monitoring into Project EA 1A092/PPNO 06-8151 from parent project EA 1A090/PPNO 06-7061.

46	Near Shaver Lake, from 0.6 mile west to 0.2 mile	29-30	PA&ED			\$0		
06-Fre-168	west of Huntington Lake Road. Landscape	201.131	* PS&E			\$735	28-29	
49.1/49.5	mitigation planting for permanent restoration		* R/W Sup			\$10	28-29	
8148	project EA 1A090.		* Con Sup			\$700	29-30	
0625000089	<u>Performance Measure</u>		* R/W Cap			\$5	29-30	
1A091	0.0 Location(s)		* <u>Const Cap</u>			\$1,660	29-30	
			Total			\$3,110		

* Phase not authorized

Note: Split landscape mitigation planting into project EA 1A091/PPNO 06-8148 from parent project EA 1A090/PPNO 06-7061.

47	Near Shaver Lake, from 0.6 mile west to 0.2 mile	25-26	PA&ED			\$0		
06-Fre-168	west of Huntington Lake Road. Biological off-site	201.131	PS&E			\$0		
49.1/49.5	Riparian planting mitigation and monitoring for		R/W Sup			\$715	25-26	
8151	permanent restoration project EA 1A090.		Con Sup			\$0		
0625000218	<u>Performance Measure</u>		R/W Cap			\$1,120	25-26	
1A092	0.0 Location(s)		<u>Const Cap</u>			\$0		
			Total			\$1,835		

Note: Split biological off-site Riparian mitigation planting and monitoring into Project EA 1A092/PPNO 06-8151 from parent project EA 1A090/PPNO 06-7061.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
48	In Fresno County, on Routes 180, 5, 33, 41, 63, 168,	25-26	PA&ED	\$2,460	Prior	\$2,460	Prior
06-Fre-180	198, 245, and 269 at various locations. Rehabilitate	26-27	PS&E	\$1,960	Prior	\$1,960	Prior
Var	drainage systems.	201.151	R/W Sup	\$955	Prior	\$955	Prior
7071			Con Sup	\$2,120	25-26	\$2,120	26-27
0620000076	<u>Performance Measure</u>		R/W Cap	\$890	25-26	\$890	26-27
1A730	104.0 Culvert(s) (ea)		<u>Const Cap</u>	\$9,340	25-26	\$10,369	26-27
			Total	\$17,725		\$18,754	

Note: Update delivery year due to challenges in mountainous site conditions that require 24 months to complete right of way work. Update construction cost estimate for various contract items such as Earthwork, state furnished materials, and traffic cost. Split ITP funding assurance for mitigation requirements, as well as Riparian mitigation planting and monitoring into Project EA 1A731/PPNO 06-8154 from parent project EA 1A730/PPNO 06-7071.

Performance Measure: Culverts					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Linear feet	98.8	8,387.4	370.6	8,856.8
Post Condition	Linear feet	8,856.8	0.0	0.0	8,856.8

49	In Fresno County, on Routes 180, 5, 33, 41, 63,	27-28	PA&ED	\$0			
06-Fre-180	168, 198, 245, and 269 at various locations. ITP	201.151	PS&E	\$500	25-26		
Var	funding assurance and Riparian mitigation and		R/W Sup	\$1,940	25-26		
8154	monitoring for culvert rehabilitation project		Con Sup	\$424	27-28		
0625000170	EA 1A730.		R/W Cap	\$2,366	27-28		
1A731	<u>Performance Measure</u>		<u>Const Cap</u>	\$462	27-28		
	0.0 Culvert(s) (ea)		Total	\$5,692			

Note: Split ITP funding assurance for California Tiger Salamander (CTS), Crotchs Bumble Bee (CBB), and Carpenteria shrubbery mitigation requirements, as well as Riparian mitigation planting and monitoring into Project EA 1A731/PPNO 06-8154 from parent project EA 1A730/PPNO 06-7071.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
50	Near McKittrick, from south of Lokern Road to 0.9	26-27	PA&ED	\$0		\$0	
06-Ker-33	mile south of Route 46. Environmental mitigation	201.121	PS&E	\$0		\$0	
40.4/59.3	funding assurance for pavement rehabilitation project		R/W Sup	\$75	25-26	\$75	25-26
8091	EA 0Y130.		Con Sup	\$0		\$0	
0624000074			R/W Cap	\$3,500	26-27	\$9,620	26-27
0Y131	<u>Performance Measure</u>		<u>Const Cap</u>	<u>\$0</u>	26-27	<u>\$0</u>	26-27
	0.0 Lane mile(s)		Total	\$3,575		\$9,695	

Note: Update right of way capital cost to account for an amended Incidental Take Permit (ITP) that now requires a multiple-species permit, and additional mitigation needs of parent EA 0Y130 refined scope that requires additional construction staging areas.

51	In and near Kettleman City, from 0.3 mile north of	26-27	PA&ED	\$3,266	Prior	\$3,266	Prior
06-Kin-41	Utica Avenue to the Fresno County line; also on	201.151	PS&E	\$2,382	25-26	\$1,500	25-26
11.97/R48.20	Route 33, 43, 198, and 269 at various locations.		R/W Sup	\$2,706	25-26	\$250	25-26
8088	Rehabilitate drainage systems.		Con Sup	\$2,163	26-27	\$400	26-27
0622000122			R/W Cap	\$832	26-27	\$1,800	26-27
1E950	<u>Performance Measure</u>		<u>Const Cap</u>	<u>\$13,341</u>	26-27	<u>\$9,500</u>	26-27
	86.0 Culvert(s) (ea)		Total	\$24,690		\$16,716	
	51.0 Culvert(s) (ea)						

Note: Updated scope, cost and performance of project resulting in cost savings. In review and scope refinement, it was determined several culverts no longer required rehabilitation due to discovery that either they were rehabilitated so their condition had changed from poor to good or were abandoned.

Performance Measure: Culverts					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Linear feet	0.0	2,086.0	2,448.0	4,534.0
Post Condition	Linear feet	4,534.0	0.0	0.0	4,534.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
52	Near Coarsegold, at the intersection with Road 416.	25-26	PA&ED	\$1,300	Prior	\$1,300	Prior
06-Mad-41	Raise vertical profile of Road 416, and install lighting, signs, and striping at the intersection.	201.010	PS&E	\$970	Prior	\$970	Prior
23.5			R/W Sup	\$465	Prior	\$465	Prior
8016			Con Sup	\$700	25-26	\$700	25-26
0622000114	<u>Performance Measure</u>		R/W Cap	\$165	25-26	\$1,129	25-26
1E910	0.56 Annual fatal and serious injury collision(s)		<u>Const Cap</u>	\$2,915	25-26	\$3,565	25-26
	0.45 Annual fatal and serious injury collision(s)		Total	\$6,515		\$8,129	
<p><u>Note:</u> Update construction and right of way capital due to refined cost estimate for increased quantity of earthwork and full take r/w acquisition needed for driveway conflict. Change in performance measures is to reflect the new methodology.</p>							
53	In the cities of Long Beach and Los Angeles, from the Orange County line to Figueroa Street. Install stormwater treatment Best Management Practices (BMPs), including bioswales and Design Pollution Prevention Infiltration Areas (DPPIAs).	25-26 26-27	PA&ED	\$1,040	Prior	\$1,040	Prior
07-LA-1			PS&E	\$1,881	Prior	\$1,881	Prior
0.0/11.5		201.335	R/W Sup	\$18	Prior	\$18	Prior
5686			Con Sup	\$1,483	25-26	\$2,338	26-27
0719000353			R/W Cap	\$138	25-26	\$253	26-27
36700			<u>Const Cap</u>	\$2,626	25-26	\$3,188	26-27
	<u>Performance Measure</u>		Total	\$7,180		\$8,718	
	13.1 Acre(s) treated/pollutant						
	16.9 Acre(s) treated/pollutant						

Note: Fifteen BMPs were added to the scope to meet stormwater requirements by offsetting lost acreage from other projects. Additionally, a new Maintenance Vehicle Pullout (MVP), landscaping, and irrigation improvements have increased project cost. An easement is also needed, increasing R/W capital. The project is delayed one year to avoid conflict with the Vincent Thomas Bridge deck replacement, which will require a portion of this project for traffic detours.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
54	In and near the cities of Santa Monica, Los Angeles,	25-26	PA&ED	\$4,085	Prior	\$4,085	Prior
07-LA-1	and Malibu, from Colorado Avenue to south of Cross	26-27	PS&E	\$3,140	23-24	\$3,140	23-24
35.2/46.9	Creek Road. Rehabilitate pavement, upgrade	201.121	R/W Sup	\$1,159	23-24	\$1,159	23-24
5625	guardrail, Transportation Management System		Con Sup	\$4,226	25-26	\$4,226	26-27
0719000287	(TMS) elements, and culverts, and upgrade facilities		R/W Cap	\$2,360	25-26	\$2,360	26-27
36150	to Americans with Disabilities Act (ADA) standards.		<u>Const Cap</u>	<u>\$38,537</u>	25-26	<u>\$38,537</u>	26-27
			Total	\$53,507		\$53,507	

Performance Measure

57.2 Lane Miles

Concurrent Greater than 20 percent PS&E and R/W
Sup allocation under Resolution FP-24-88;
June 2025.

Note: During the environmental phase, a Road Safety Audit (RSA) was conducted within the project limits, prompted by a fatal collision. The RSA identified safety improvements not in the original scope. As a result, additional time was needed for field reviews and coordination with cities and agencies for additional safety measures, thus delaying environmental phase and extending project delivery by one year.

Performance Measure: Pavement

	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	55.3	1.9	57.2
Post Condition	Lane mile(s)	57.2	0.0	0.0	57.2

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
55 07-LA-1 4.5/7.1 5575 0719000180 36480	In Long Beach, from Temple Avenue to De Forest Avenue. Upgrade traffic signals, crosswalks, curb ramps, sidewalks, driveways, and Accessible Pedestrian Signals (APS) to Americans with Disabilities Act (ADA) standards.	25-26	PA&ED	\$2,483	Prior	\$2,483	Prior
		27-28	PS&E	\$4,193	Prior	\$4,193	Prior
		201.361	R/W Sup	\$5,065	Prior	\$5,065	Prior
			Con Sup	\$3,806	25-26	\$3,806	27-28
			R/W Cap	\$5,496	25-26	\$5,496	27-28
			<u>Const Cap</u>	<u>\$18,157</u>	25-26	<u>\$18,157</u>	27-28
		Total	\$39,200		\$39,200		
	<u>Performance Measure</u>						
	127.0 Curb ramp(s)						
	114.0 Curb ramps(s)						

Note: The planned delivery year was delayed due to several factors. Of 91 parcels needed for Temporary Construction Easements (TCE), 25 require condemnation. Construction also overlaps with the LA'28 Olympics and Vincent Thomas Bridge deck replacement closure, with the latter requiring a portion of this project for traffic detours. Performance was reduced due to some curb ramps that were upgraded by another project or converted to driveways.

Performance Measure: Curb ramps					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Each	0.0	0.0	97.0	97.0
Post Condition	Each	114.0	0.0	0.0	114.0

(Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
56	In the cities of Los Angeles, Glendale, La Canada	25-26	PA&ED	\$2,027	Prior	\$2,027	Prior
07-LA-2	Flintridge, and Pasadena, from Verdugo Road to 0.3	201.335	PS&E	\$2,156	24-25	\$2,156	24-25
R17.0/27.0	mile west of the Angeles Forest; also on Route 210,		R/W Sup	\$31	24-25	\$31	24-25
R17.0/26.72	from 0.2 mile west of Wentworth Street to		Con Sup	\$2,259	25-26	\$2,843	25-26
5973	Washington Boulevard (PM R10.6/R23.5). Construct		R/W Cap	\$72	25-26	\$96	25-26
0721000254	Best Management Practices (BMPs) to achieve		<u>Const Cap</u>	<u>\$6,916</u>	25-26	<u>\$8,375</u>	25-26
37930	statewide National Pollutant Discharge Elimination		Total	\$13,461		\$15,528	
	System (NPDES) permit compliance units for Total						
	Maximum Daily Load (TMDL).						

Performance Measure

~~90.7 Acres Treated/Pollutant~~

83.9 Acres Treated/Pollutant

Concurrent COS allocation under Resolution
FP-24-96: June 2025.

Note: Increase in construction capital and support is due to the addition of Maintenance Vehicle Pullouts (MVPs), additional traffic control measures, and increase in construction working days. Change in performance and postmile limits is attributed to the removal of stormwater BMPs that were found to have limited access or are included on a separate project.

57	In the city of Los Angeles, near San Fernando, from	25-26	PA&ED	\$590	Prior	\$590	Prior
07-LA-5	0.2 mile north of Roxford Street Undercrossing to 0.2	201.010	PS&E	\$1,180	24-25	\$1,180	24-25
42.8/R43.6	mile south of San Fernando Road Overhead. Install		R/W Sup	\$17	24-25	\$17	24-25
6127	High Friction Surface Treatment (HFST) and		Con Sup	\$1,120	25-26	\$1,448	25-26
0723000251	guardrail, rehabilitate pavement consisting of slab		R/W Cap	\$52	25-26	\$44	25-26
39580	replacement , and improve drainage system.		<u>Const Cap</u>	\$2,911	25-26	\$5,971	25-26
			Total	\$5,870		\$9,250	

Performance Measure

~~4.20 Annual fatal and serious injury collision(s)~~

0.40 Annual fatal and serious injury collision(s)

Concurrent COS allocation under Resolution
FP-24-96: June 2025.

Note: Add scope of work to replace damaged slabs prior to installing HFST to avoid possible delamination. Increase capital construction and support due to above added scope, added structural section relating to drainage, pavement grinding, additional drainage, traffic control items. Update description to reflect added scope. Decrease R/W capital due to reduced utility verification locations. Update performance measures to reflect the new methodology.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)
58 07-LA-39 40.0/44.4 5381 0718000117 34770	Near Falling Springs, from 1.8 miles north of Crystal Lake Road to Route 2. Rehabilitate and reopen a 4.4 mile segment of Route 39 as an evacuation route. (Long Lead Project) (PA&ED, PS&E, R/W Sup only)	29-30 28-29 201.150	PA&ED \$7,900 Prior PS&E \$7,000 25-26 \$8,632 25-26 R/W Sup \$100 25-26 \$178 25-26 * Con Sup \$7,000 29-30 \$13,201 28-29 * R/W Cap \$100 29-30 \$3 28-29 * Const Cap \$35,000 29-30 \$51,582 28-29 Total \$57,100 \$81,496
	<u>Performance Measure</u> 1.0 Location(s)		

* Phase not authorized

Note: Program PS&E and R/W support phases on this long lead project because the environmental document has been completed. Advance the year of delivery to expedite the reopening of the route so that it can be used as an evacuation route in case emergencies such as wildfires. Update project description to match the preferred alternative. Increase in cost is based on the estimate for the preferred alternative.

59 07-LA-110 23.7/25.0 5415 0717000342 34300	In the city of Los Angeles, from Route 101 to Amador Street (PM 25.0L/R). Construct auxiliary lanes, make intersection improvements, and reconstruct bridges.	27-28 201.310	PA&ED \$10,000 Prior PS&E \$0 R/W Sup \$0 Con Sup \$0 R/W Cap \$0 Const Cap \$0 Total \$10,000
	<u>Performance Measure</u> 691.0 Daily vehicle hour(s) of delay (DVHD)		

Note: Delete project. This project was a joint project between City of Los Angeles and the Department. The City has reconsidered their objectives and are no longer pursuing the proposed improvements. Other viable alternatives without City's involvement are not feasible.

60 07-LA-170 R17.0 5739 0720000151 37140	In the city of Los Angeles, at Alexandria Park. Financial Contribution Only (FCO) to Los Angeles Department of Water and Power (LADWP) to construct infiltration gallery to treat stormwater runoff.	25-26 201.335	PA&ED \$0 PS&E \$0 R/W Sup \$0 Con Sup \$0 R/W Cap \$0 Const Cap \$10,315 25-26 Total \$10,315
	<u>Performance Measure</u> 171.3 Acre(s) treated/pollutant		

Note: Delete project because LADWP has decided to put this project on an indefinite hold as the project site is currently being used by the City of Los Angeles to shelter people experiencing homelessness. The Department plans to reprogram this project within another location and under a future fiscal year.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
61	In and near Pasadena and Arcadia, from Allen	25-26	PA&ED	\$609	Prior	\$609	Prior
07-LA-210	Avenue to Santa Anita Avenue. Rehabilitate	201.151	PS&E	\$1,522	Prior	\$1,522	Prior
R27.4/R31.9	drainage systems.		R/W Sup	\$23	Prior	\$23	Prior
5629			Con Sup	\$1,115	25-26	\$1,115	25-26
0719000293	<u>Performance Measure</u>		R/W Cap	\$320	25-26	\$67	25-26
36190	42.0 Culvert(s) (ea)		<u>Const Cap</u>	\$2,004	25-26	\$2,359	25-26
	8.0 Culvert(s) (ea)		Total	\$5,673		\$5,695	

Note: Increase in construction capital is due to required excavation of Aerially Deposited Lead (ADL) material and shoring for the replacement of 3 existing culverts that was not originally identified. Changed performance because four culverts were removed from the project scope due to unanticipated permit from LA metro that would considerably delay project delivery. Decrease in R/W Capital is due to decrease in acquisition and utility relocation needs.

Performance Measure: Culverts					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Linear feet	0.0	632.0	187.0	819.0
Post Condition	Linear feet	819.0	0.0	0.0	819.0

62 07-LA-213 0.000/9.984 5371 0718000076 34660	In and near the cities of Los Angeles, Rancho Palos Verdes, Lomita, and Torrance, from West 25th Street to Route 405. Rehabilitate pavement, upgrade guardrail, install complete streets elements, including bike lanes, and crosswalk improvements, new sidewalk , and upgrade facilities curb ramps and signal to Americans with Disabilities Act (ADA) standards.	25-26 201.121	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$910 \$2,420 \$775 \$4,828 \$999 \$26,441 \$36,373	Prior Prior Prior 25-26 25-26 25-26	\$910 \$2,420 \$775 \$7,200 \$1,286 \$46,179 \$58,770	Prior Prior Prior 25-26 25-26 25-26
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Performance Measure
42.9 Lane mile(s)

Note: Additional sidewalk was added to the scope to provide continuous connectivity along the corridor resulting in an increase of R/W capital, construction capital and support, and change in project description. Additionally, increases in material and labor as well as additional project elements like roadway digouts, retaining walls, and traffic related items have also increased project cost.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	42.9	0.0	42.9
Post Condition	Lane mile(s)	42.9	0.0	0.0	42.9

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
63 07-LA-710 19.567/19.893 5938 0722000123 38410	In Bell, at the Florence Avenue interchange. Improve pedestrian and bicycle infrastructure by reconfiguring interchange, modifying sidewalks, upgrading railings, and upgrading facilities to Americans with Disability Act (ADA) standards.	27-28 28-29 201.400	PA&ED PS&E R/W Sup * Con Sup * R/W Cap * <u>Const Cap</u> Total	\$1,799 \$4,169 \$451 \$6,526 \$307 \$15,202 \$28,454	Prior 25-26 25-26 27-28 27-28 27-28 27-28	\$1,799 \$4,169 \$451 \$6,526 \$307 \$15,202 \$28,454	Prior 26-27 26-27 28-29 28-29 28-29 28-29
	<u>Performance Measure</u> 3,642.0 Bicycle and pedestrian infrastructure (linear feet)						* Phase not authorized

Note: Based on comments received from a public meeting, additional project studies are needed to verify the feasibility and cost effectiveness of all proposed alternatives within the original Project Initiation Report. As a result, the project delivery year is delayed by one year.

64 07-Ven-118 14.7/15.6 5406 0718000176 35010	In and near Moorpark, from Grimes Canyon Road to Montair Drive. Construct Commercial Vehicle Enforcement Facility (CVEF) and Weigh-In-Motion (WIM) scale.	25-26 26-27 201.321	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$1,358 \$3,183 \$439 \$3,740 \$2,069 \$23,640 \$34,429	Prior Prior Prior 25-26 25-26 25-26	\$1,358 \$3,183 \$439 \$3,740 \$2,069 \$23,640 \$34,429	Prior Prior Prior 26-27 26-27 26-27
	<u>Performance Measure</u> 1.0 Location(s)						

Note: Delays in obtaining a utility connection permit from Ventura County Public Works have delayed PS&E. Utility connection permit application requires ownership of parcels at the time of application, which are still pending acquisition. Pending parcels have potential to have hazardous waste material and will cause project delays.

Performance Measure: Weigh-in-Motion Scales					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Each	0.0	0.0	1.0	1.0
Post Condition	Each	1.0	0.0	0.0	1.0

List of 2024 SHOPP Amendments

(Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
65	In and near Moorpark and Simi Valley, from 0.1 mile	25-26	PA&ED	\$1,670	Prior	\$1,670	Prior
07-Ven-118	east of New Los Angeles Avenue to Rocky Peak	201.335	PS&E	\$2,860	Prior	\$2,860	Prior
T18.3/R32.4	Road. Construct stormwater Best Management		R/W Sup	\$15	Prior	\$15	Prior
6196	Practices (BMPs) to meet requirements of National		Con Sup	\$2,525	25-26	\$2,525	25-26
0721000249	Pollutant Discharge Elimination System (NPDES)		R/W Cap	\$53	25-26	\$53	25-26
38030	permit.		<u>Const Cap</u>	<u>\$10,702</u>	25-26	<u>\$9,525</u>	25-26
			Total	\$17,825		\$16,648	

Performance Measure

~~105.9 Acre(s) treated/pollutant~~

72.1 Acres(s) treated/pollutant

Note: Several BMP locations were removed from scope due to size constraints and maintainability issues, while other locations had treated areas recalculated based on updated survey information. Four access roads were added to the scope to maintain new BMPs as well as additional landscape, irrigation and plant establishment not originally identified. These updates have resulted in decrease of performance and change in construction capital.

66	Near Temecula, from 0.1 mile west to 0.2 mile east	25-26	PA&ED	\$1,599	Prior	\$1,599	Prior
08-Riv-371	of Homestead Road. Construct westbound left-turn	26-27	PS&E	\$1,273	24-25	\$1,273	24-25
67.1/67.4	lane and retaining walls, and rehabilitate drainage	201.010	R/W Sup	\$226	24-25	\$226	24-25
3021J	system.		Con Sup	\$1,288	25-26	\$1,288	26-27
0823000053			R/W Cap	\$94	25-26	\$94	26-27
1N530	<u>Performance Measure</u>		<u>Const Cap</u>	<u>\$2,164</u>	25-26	<u>\$2,164</u>	26-27
	0.04 Annual fatal and serious injury collision(s)		Total	\$6,644		\$6,644	

Performance Measure

~~0.04 Annual fatal and serious injury collision(s)~~

0.06 Annual fatal and serious injury collision(s)

Concurrent twelve month allocation time extension for PS&E and R/W Sup under Waiver 25-95; June 2025.

Note: During PA&ED phase, a sacred burial ground for Cahuilla Tribe was discovered within the Department's right of way which triggers an elevated environmental document, requiring additional time to complete PA&ED phase. As a result, the project delivery must be delayed by 1 year. Additionally, update performance measures to reflect the new methodology.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
67	Near Big Bear Lake, from 2.0 miles south of	25-26	PA&ED	\$1,694	Prior	\$1,694	Prior
08-SBd-18	Holcomb Valley Road to Camp Rock Road.	201.121	PS&E	\$1,642	Prior	\$1,642	Prior
56.2/66.9	Rehabilitate pavement and drainage systems,		R/W Sup	\$944	Prior	\$944	Prior
3016F	upgrade guardrail, replace sign panels, construct		Con Sup	\$3,691	25-26	\$3,691	25-26
0820000076	rumble strips, widen shoulders, and apply pavement		R/W Cap	\$150	25-26	\$450	25-26
1L420	delineation.		<u>Const Cap</u>	<u>\$18,760</u>	25-26	<u>\$21,612</u>	25-26
			Total	\$26,881		\$30,033	

Performance Measure

17.8 Lane mile(s)

Note: Increase construction capital due to overall additional quantities of asphalt and traffic control system, reduced quantities of aggregate base, and unit cost adjustments. Increase RW capital is due to additional need for positive location of underground utilities, to fund a contribution agreement with the Bureau of Land Management, and to provide additional funding for higher escrow and condemnation costs.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	13.1	4.7	17.8
Post Condition	Lane mile(s)	17.8	0.0	0.0	17.8

68	In and near Barstow, from Stoddard Wells Road to	25-26	PA&ED	\$1,480	Prior	\$1,480	Prior
08-SBd-247	Route 15. Rehabilitate pavement, replace sign	201.121	PS&E	\$1,417	Prior	\$1,417	Prior
73.2/78.096	panels, add bike lanes, and upgrade facilities to		R/W Sup	\$1,415	Prior	\$1,415	Prior
3015A	Americans with Disabilities Act (ADA) standards.		Con Sup	\$1,524	25-26	\$1,524	25-26
0819000155			R/W Cap	\$652	25-26	\$2,099	25-26
1L090			<u>Const Cap</u>	<u>\$9,399</u>	25-26	<u>\$9,399</u>	25-26
			Total	\$15,887		\$17,334	

Performance Measure

12.3 Lane mile(s)

Note: During the PA&ED phase, positive location of underground utilities has identified a gas line conflict and requires relocation. As a result, additional RW capital funds are required.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	12.3	0.0	12.3
Post Condition	Lane mile(s)	12.3	0.0	0.0	12.3

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
69	In San Bernardino County, at various locations.	25-26	PA&ED	\$514	Prior	\$514	Prior
08-SBd-Var	Upgrade traffic census stations.	201.315	PS&E	\$1,043	Prior	\$1,043	Prior
Var			R/W Sup	\$45	Prior	\$45	Prior
3014F	<u>Performance Measure</u>		Con Sup	\$1,113	25-26	\$1,113	25-26
0819000126	40.0 Field element(s)		R/W Cap	\$23	25-26	\$23	25-26
1K870	39.0 Field element(s)		<u>Const Cap</u>	<u>\$3,542</u>	25-26	<u>\$3,542</u>	25-26
			Total	\$6,280		\$6,280	

Note: Reduce performance due to one field element already programmed under SHOPP project EA 08-1J300 and currently in construction. Update pre-construction conditions to reflect current inventory conditions.

Performance Measure: TMS Elements				
	<u>Unit</u>	<u>Good (Operational)</u>	<u>Poor (Not Operational)</u>	<u>Quantity</u>
Existing Condition	Field element(s)	16.2%	83.8%	37.0
Post Condition	Field element(s)	100.0%	0.0%	39.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
70 09-Ker-178 92.0/R93.4 2685 0919000069 38330	In and near Ridgecrest, from Redrock Inyokern Road to Clodt Road; also from 0.1 mile west of Mahan Street to the San Bernardino County line (99.0/104.6). Rehabilitate pavement and drainage systems, upgrade lighting and Transportation Management System (TMS) elements, replace guardrail and sign panels, upgrade facilities to Americans with Disabilities Act (ADA) standards, and construct Class 2 bike lanes as complete street elements. (Long Lead Project)	29-30 201.120 201.121	PA&ED PS&E R/W Sup * Con Sup R/W Cap * <u>Const Cap</u> Total	\$3,117 \$4,000 \$3,800 \$9,213 \$3,400 <u>\$34,700</u> \$58,230	Prior 24-25 24-25 29-30 29-30 29-30 29-30	\$3,117 \$4,400 \$4,180 \$9,213 \$2,700 <u>\$34,700</u> \$58,310	Prior 24-25 24-25 29-30 29-30 29-30 29-30

* Phase not authorized

Performance Measure
23.8 Lane mile(s)

Concurrent COS allocation under Resolution
FP-24-96; June 2025.

Future SB1 Baseline Agreement required.

Note: Fully program previously unfunded PS&E, RW support, and RW capital phases of this Long Lead Project. Increase PS&E and RW support costs due to refined scope. Decrease RW capital cost due to a decrease in acquisition cost and utilities. Change pavement strategy and program code from major rehabilitation to capital maintenance due to technical correction and existing pavement survey.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	23.8	0.0	23.8
Post Condition	Lane mile(s)	23.8	0.0	0.0	23.8

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
71	Near Kirkwood, on Routes 88, and 89 in Alpine,	24-25	PA&ED	\$2,094	Prior	\$2,094	Prior
10-Alp-88	Amador, and El Dorado Counties at various	201.315	PS&E	\$2,589	Prior	\$2,589	Prior
2.00/25.00	locations. Install new Transportation Management		R/W Sup	\$1,073	Prior	\$1,073	Prior
3436	System (TMS) elements and construct Maintenance		Con Sup	\$0	24-25	\$2,033	24-25
1018000275	Vehicle Pullouts (MVPs).		R/W Cap	\$20	24-25	\$20	24-25
1G020	(G13 Contingency)		Const Cap	\$0	24-25	\$9,082	24-25
			Total	\$5,776		\$16,891	

Performance Measure

21.0 Field element(s)

Concurrent CONST and CON ENG allocation under Resolution FP-24-95; June 2025.

Note: Fully program unfunded phases of this previous G13 Contingency project.

Performance Measure: TMS Elements				
Unit	Good (Operational)	Poor (Not Operational)	Quantity	
Existing Condition Field element(s)	0.0%	100.0%	4.0	
Post Condition Field element(s)	100.0%	0.0%	21.0	

72	In Angels Camp, on Main Street from north of	25-26	PA&ED	\$1,727	Prior	\$1,727	Prior
10-Cal-49	Stockton Road to north of Francis Street; also on	201.310	PS&E	\$2,158	Prior	\$2,158	Prior
8.5/9.1	Route 49 4 from 0.2 miles west and east of Main		R/W Sup	\$1,248	Prior	\$1,248	Prior
3434	Street (PM R20.9/R21.3). Construct roundabouts,		Con Sup	\$1,530	25-26	\$1,953	25-26
1017000057	upgrade facilities to Americans with Disabilities Act		R/W Cap	\$3,639	25-26	\$3,639	25-26
1H010	(ADA) standards, and enhance pedestrian and		Const Cap	\$9,103	25-26	\$16,445	25-26
	bicyclist safety.		Total	\$19,485		\$27,170	

Performance Measure

204.0 Daily vehicle hour(s) of delay (DVHD)

Note: Update description to project to correct a secondary route reference from 49 to 4. Updated construction cost due to scope refinement that identified a stormwater regulatory requirement change that requires an addition of a drainage basin for stormwater treatment mitigation. Scope refinement included additional complete streets, staged construction, and traffic handling.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)				
73	In Los Banos, from west of Badger Flat Road to west	25-26	PA&ED	\$1,281	Prior	\$1,281	Prior
10-Mer-152	of Santa Fe Road. Rehabilitate pavement and	201.121	PS&E	\$2,244	Prior	\$2,244	Prior
18.50/23.00	drainage systems, upgrade Transportation		R/W Sup	\$1,146	Prior	\$1,146	Prior
3425	Management System (TMS) elements, roadway		Con Sup	\$3,142	25-26	\$3,298	25-26
1018000274	signs, guardrail and facilities to Americans with		R/W Cap	\$239	25-26	\$6,017	25-26
1E980	Disabilities Act (ADA) standards.		<u>Const Cap</u>	<u>\$21,919</u>	25-26	\$27,401	25-26
			Total	\$29,971		\$41,387	

Performance Measure

18.0 Lane mile(s)

Note: Updated right of way capital and construction due to extensive public outreach which identified a STAA truck turning improvement study was needed and a deficiency of sidewalk and bike lanes which resulted in the limits of existing facilities to be addressed were increased to include an additional 5,556 linear feet of sidewalk and 5,002 linear feet of bike lanes. The number of parcels required increased from 28 to 54 parcels.

Performance Measure: Pavement					
	Unit	Good	Fair	Poor	Quantity
Existing Condition	Lane mile(s)	0.0	18.0	0.0	18.0
Post Condition	Lane mile(s)	18.0	0.0	0.0	18.0

74 10-Mpa-49 27.000/48.835 3618 1022000060 1P020	In and near Coulterville, from 2.0 miles north of Mt. Ophir Road to Tuolumne County line; also on Route 140 at Miles Creek Bridge No. 40-0015 (PM 3.5) and 40-0016 (PM 3.64); also in Stanislaus County, on Route 132 at Snake Ravine Bridge No. 38-0062 (PM 46.82). Rehabilitate pavement and drainage systems, upgrade bridge rail, mitigate bridge scour, incorporate complete streets features, and upgrade facilities to Americans with Disabilities Act (ADA) standards.	27-28 28-29 201.121	PA&ED PS&E R/W Sup * Con Sup * R/W Cap * <u>Const Cap</u> Total	\$2,461 \$3,562 \$1,322 \$5,557 \$507 <u>\$42,492</u> \$55,901	Prior 26-27 26-27 27-28 27-28 27-28 27-28	\$2,461 \$3,562 \$1,322 \$5,557 \$507 <u>\$42,492</u> \$55,901	Prior 26-27 26-27 28-29 28-29 28-29 28-29
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* Phase not authorized

Performance Measure

42.8 Lane mile(s)

Note: Update delivery year to account for additional mitigation measure needed and additional right of way acquisitions needed to preserve historic features important to our local partners.

Performance Measure: Pavement					
	Unit	Good	Fair	Poor	Quantity
Existing Condition	Lane mile(s)	0.0	42.8	0.0	42.8
Post Condition	Lane mile(s)	42.8	0.0	0.0	42.8

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)					
75	Near Stockton, from Route 99 to Stanislaus County	23-24	PA&ED	\$2,660	Prior	\$2,660	Prior	
10-SJ-4	line (PM 38.059); also in Stanislaus County from San	201.121	PS&E	\$2,701	Prior	\$2,701	Prior	
19.5/38.1	Joaquin County line to 0.5 mile east of Dunton Road		R/W Sup	\$657	Prior	\$657	Prior	
3277	(PM 0.0/7.2). Rehabilitate pavement, upgrade		Con Sup	\$0	23-24	\$5,250	23-24	
1017000178	guardrail, construct rumble strips, upgrade facilities		R/W Cap	\$379	23-24	\$379	23-24	
1C050	to Americans with Disabilities Act (ADA) standards,		<u>Const Cap</u>	\$0	23-24	\$36,437	23-24	
	rehabilitate drainage systems, install Transportation		Total	\$6,397		\$48,084		
	Management System (TMS) elements, and upgrade							
	bridge railing.							
	(G13 Contingency)							

Performance Measure

50.8 Lane mile(s)

Concurrent CONST and CON ENG allocation under
Resolution FP-24-95; June 2025.

Note: Fully program unfunded phases of this previous G13 Contingency project.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	4.8	44.1	1.9	50.8
Post Condition	Lane mile(s)	50.8	0.0	0.0	50.8

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code				Project Costs (\$1,000)		
76	In Stockton, from Route 5 to Wilson Road at various	25-26	PA&ED	\$770	Prior	\$770	Prior	
10-SJ-4	locations, and on Route 5 at March Lane. Replace	27-28	PS&E	\$1,940	Prior	\$1,940	Prior	
R15.9/R17.7	Transportation Management System (TMS)	201.315	R/W Sup	\$220	Prior	\$220	Prior	
3516	elements, rehabilitate drainage systems and upgrade		Con Sup	\$1,850	25-26	\$1,850	27-28	
1020000192	facilities to Americans with Disabilities Act (ADA)		R/W Cap	\$114	25-26	\$394	27-28	
1L890	standards.		<u>Const Cap</u>	<u>\$9,430</u>	25-26	<u>\$13,663</u>	27-28	
			Total	\$14,324		\$18,837		

Performance Measure

~~24.0 Field element(s)~~

22.0 Field element(s)

Note: Update to delivery year, cost, and performance. Schedule delay and right of way capital cost increase due to increase right of way parcel acquisitions. Construction capital cost increase due to cost increases to several items of work including earthwork, pavement, and traffic lighting system. Performance reduced due to one signal replacement and one new camera that were already completed in separate project.

Performance Measure: TMS Elements

	Unit	Good (Operational)	Poor (Not Operational)	Quantity
Existing Condition	Field element(s)	0.0%	100.0%	11.0
Post Condition	Field element(s)	100.0%	0.0%	22.0

77	Near Manteca, at the intersection with Murphy Road.	25-26	PA&ED	\$1,530	Prior	\$1,530	Prior	
10-SJ-120	Improve safety by constructing roundabout.	27-28	PS&E	\$2,467	24-25	\$2,467	24-25	
10.82		201.010	R/W Sup	\$498	24-25	\$498	24-25	
3620	<u>Performance Measure</u>		Con Sup	\$1,159	25-26	\$1,280	27-28	
1022000221	0.12 Annual fatal and serious injury collision(s)		R/W Cap	\$942	25-26	\$1,737	27-28	
1Q010			<u>Const Cap</u>	<u>\$5,870</u>	25-26	<u>\$6,500</u>	27-28	
			Total	\$12,466		\$14,012		

Note: Update delivery year to account for right of way parcel acquisition challenges which will require 26 months of lead time to resolve six parcels changing from simple to complex. Update right of way capital and construction for associated escalation cost and six right of way complex parcels, and implementing interim safety measures.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
78	Near Westley, from south of Frank Cox Road to	25-26	PA&ED	\$2,004	Prior	\$2,004	Prior
10-Sta-33	south of Howard Road. Construct left-turn	26-27	PS&E	\$2,359	24-25	\$2,359	24-25
18.6/19.2	channelized lane.	201.010	R/W Sup	\$440	24-25	\$440	24-25
3560			Con Sup	\$1,787	25-26	\$1,787	26-27
1021000169	<u>Performance Measure</u>		R/W Cap	\$762	25-26	\$762	26-27
1N340	0.13 Annual fatal and serious injury collision(s)		<u>Const Cap</u>	<u>\$5,500</u>	25-26	<u>\$5,500</u>	26-27
	0.14 Annual fatal and serious injury collision(s)		Total	\$12,852		\$12,852	

Note: Update delivery year to account for bridge redesign, including a culvert relocation. It was determined in the later stages of PA&ED that the culvert needed to be addressed as a utility relocation since it was owned by a local irrigation district. So this requires extensive coordination and review of alternative non-standard bridge design for the shoulder width. Update performance measures to reflect the new methodology. Interim safety measures to be implemented.

79	In Modesto, at the Modesto Maintenance Station	25-26	PA&ED	\$1,299	Prior	\$1,299	Prior
10-Sta-99	(908 North Emerald Avenue). Reconstruct crew	27-28	PS&E	\$3,091	Prior	\$3,091	Prior
R16.8	building and construct a new office building, storage	201.352	R/W Sup	\$65	Prior	\$65	Prior
3527	building, mechanic shop, equipment shop, fuel		Con Sup	\$2,243	25-26	\$2,355	27-28
1020000182	island, water filtration system , canopies, and a		R/W Cap	\$281	25-26	\$281	27-28
1M430	Zero Emission Vehicle (ZEV) charging station.		<u>Const Cap</u>	<u>\$29,233</u>	25-26	\$37,429	27-28
			Total	\$36,212		\$44,520	
	<u>Performance Measure</u>						
	1.0 Location(s)						

Note: Scope refinement and new information regarding new heavy equipment and vehicles recently ordered including new hydrogen vehicles prompted an update to construction cost and the delivery year in order to address a change in the preferred alternative which includes additional needs for the demolition of six buildings, new equipment shop, new storage bins, new fuel island, level 3 EV chargers, new water filtration system, and additional pavement and drainage.

Performance Measure: Transportation Related Facilities					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	0.0	20,818.0	20,818.0
Post Condition	Square feet	37,467.0	0.0	0.0	37,467.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code			Project Costs (\$1,000)		
80 10-Sta-120 8.9/9.5 3540 1021000168 1N330	Near Oakdale, east of Orange Blossom Road to east of Wamble Road. Widen roadway to install left-turn channelized lane. <u>Performance Measure</u> 0.06 Annual fatal and serious injury collision(s)	25-26 201.010	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$905 \$1,176 \$149 \$1,162 \$509 \$6,774 \$10,675	Prior 24-25 24-25 25-26 25-26 25-26 25-26	\$905 \$1,176 \$149 \$1,530 \$1,255 \$8,178 \$13,193	Prior 24-25 24-25 25-26 25-26 25-26 25-26

Note: Update right of capital and construction estimate to address drainage design refinement, offsite mitigation, and utility relocations. After field reviews and property owner engagement, a need to improve the discharge of onsite storm water and flooding of adjacent areas. Right of way capital needed to account for increases in offsite mitigation acquisition and utility relocations of electrical poles, pull boxes, and controlling box at southwest corner of intersection.

81 11-SD-5 R22.3 1381 1119000193 43087	In the city of San Diego, at Clairemont Drive Overcrossing Bridge No. 57-0429. Repair bridge deck, replace traffic signal, make bicycle and pedestrian safety improvements, and upgrade facilities to Americans with Disabilities Act (ADA) standards. <u>Performance Measure</u> 1.0 Bridge(s)	25-26 201.110	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$354 \$826 \$30 \$1,154 \$25 \$11,101 \$13,490	Prior Prior Prior 25-26 25-26 25-26	\$354 \$826 \$30 \$2,800 \$233 \$21,300 \$25,543	Prior Prior Prior 25-26 25-26 25-26
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Note: During PA&ED, it was found that some structural and roadway items such as galvanic anodes, a raised median, and lighting system were not originally included in the scope. Rising unit costs have also significantly increased project cost. Also, a change in planned closures extended working days from 40 to 200, raising construction support costs. Additionally, more R/W capital is required due to higher railroad flagging costs and utility relocation.

Performance Measure: Bridge(s)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	0.0	75,573.0	75,573.0
Post Condition	Square feet	75,573.0	0.0	0.0	75,573.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)		
82	In the city of San Diego, from Route 805 to Route 56;	25-26	PA&ED	\$0		\$0
11-SD-5	also on Route 56 from Route 5 to Carmel Valley	201.335	PS&E	\$0	25-26	\$200 25-26
R30.4/R32.7	Road (PM 0.0/3.1). Financial Contribution Only		R/W Sup	\$0		\$0
1431	(FCO) to the city of San Diego to restore the Los		Con Sup	\$0		\$0
1121000047	Penasquitos Lagoon and reduce sediment transport		R/W Cap	\$0		\$0
43123	to the lagoon to achieve statewide National Pollutant		<u>Const Cap</u>	<u>\$2,024</u>	25-26	<u>\$2,139</u> 25-26
	Discharge Elimination System (NPDES) permit		Total	\$2,024		\$2,339
	compliance units for Total Maximum Daily Load					
	(TMDL).					

Performance Measure

46.0 Acre(s) treated/pollutant

Note: Additional construction capital is needed due to project cost inflation and escalation from the time the project was initiated in 2021. PS&E support resources needed for the Department to assist in constructability reviews.

83	Near Boulevard, at the Boulevard Maintenance	25-26	PA&ED	\$837	Prior	\$837	Prior
11-SD-8	Station at 40945 Old Highway 80. Replace	28-29	PS&E	\$2,306	Prior	\$2,306	Prior
R67.6	maintenance station building, construct wash rack,	201.352	R/W Sup	\$35	Prior	\$35	Prior
1398	upgrade electrical service, and install solar panels		* Con Sup	\$2,684	25-26	\$2,684	28-29
1119000204	and a Zero Emission Vehicle (ZEV) charging station.		* R/W Cap	\$5	25-26	\$5	28-29
43096			* <u>Const Cap</u>	<u>\$11,896</u>	25-26	<u>\$29,660</u>	28-29
			Total	\$17,757		\$35,527	

Performance Measure

1.0 Location(s)

* Phase not authorized

Note: Update in delivery year and construction capital. A funding shortfall in the 2024 SHOPP has resulted in this project moving out of the current SHOPP cycle and into the 2026 SHOPP. Additional construction capital is needed to fund the programmed scope. Rising labor and material costs as well as evolving building code and regulations have contributed significantly to project cost increases.

Performance Measure: Transportation Related Facilities					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Square feet	0.0	0.0	5,600.0	5,600.0
Post Condition	Square feet	7,857.0	0.0	0.0	7,857.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
84	Near Rainbow, from 0.7 mile to 0.1 mile south of	25-26	PA&ED	\$298	Prior	\$298	Prior
11-SD-15	Mission Road. Install high-tension cable barrier and	201.010	PS&E	\$368	Prior	\$368	Prior
R49.9/R50.5	guardrail to improve safety.		R/W Sup	\$0		\$0	
1515			Con Sup	\$509	25-26	\$707	25-26
1123000180	<u>Performance Measure</u>		R/W Cap	\$0		\$0	
43185	0.28 Annual fatal and serious injury collision(s)		<u>Const Cap</u>	\$2,704	25-26	\$3,453	25-26
	0.24 Annual fatal and serious injury collision(s)		Total	\$3,879		\$4,826	

Note: Unit prices on some items such as mobilization and high-tension cable barrier, have significantly increased since last estimate. Increase in construction support is due to the additional 250 plant establishment days that were added to the construction schedule during design per environmental requirements. Update performance measures to reflect the new methodology.

85	In the city of San Diego, from Route 5 to west of	25-26	PA&ED	\$877	Prior	\$877	Prior
11-SD-56	Carmel Valley Road. Rehabilitate pavement, install	201.121	PS&E	\$1,807	Prior	\$1,807	Prior
0.0/2.9	Light Emitting Diode (LED) lighting, and upgrade		R/W Sup	\$129	Prior	\$129	Prior
1400	facilities to Americans with Disabilities Act (ADA)		Con Sup	\$3,202	25-26	\$3,202	25-26
1119000202	standards.		R/W Cap	\$109	25-26	\$358	25-26
43093	<u>Performance Measure</u>		<u>Const Cap</u>	\$17,165	25-26	\$26,500	25-26
	13.0 Lane mile(s)		Total	\$23,289		\$32,873	

Note: During PA&ED, it was determined that additional items not included in the PID estimate, such as asphaltic binder, seal coat, signing/stripping, and ramp metering, would be required for the project. Rising unit cost for contract items like rubberized asphalt, grinding, electrical, mobilization, thermoplastic pavement marking, and minor concrete also have increased project cost significantly. Additional R/W capital is needed due to unforeseen utility impacts.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	1.9	11.1	0.0	13.0
Post Condition	Lane mile(s)	13.0	0.0	0.0	13.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)					
86	Near Lakeside and Eucalyptus Hills, from 0.2 mile	23-24	PA&ED	\$1,128	Prior	\$1,128	Prior	
11-SD-67	north of Riverford Road to Ellie Lane. Rehabilitate	201.151	PS&E	\$1,461	Prior	\$1,461	Prior	
R4.1/15.9	culverts, upgrade lighting, enhance highway worker		R/W Sup	\$305	Prior	\$305	Prior	
1314	safety, and upgrade facilities to Americans with		Con Sup	\$3,076	23-24	\$3,076	23-24	
1118000095	Disabilities Act (ADA) standards.		R/W Cap	\$663	23-24	\$663	23-24	
43031			<u>Const Cap</u>	<u>\$12,200</u>	23-24	<u>\$11,910</u>	23-24	
	<u>Performance Measure</u>		Total	\$18,833		\$18,543		
	34.0 Culverts							
	33.0 Culverts							

Concurrent CONST and CON ENG allocation under Resolution FP-24-95; June 2025.

Note: Split plant establishment into project EA 43287/PPNO 11-1623 from parent project EA 43031/PPNO 11-1314. Reduction in performance is due to elimination of one culvert that was addressed in a separate project.

Performance Measure: Culverts					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Linear feet	0.0	1,566.0	1,697.7	3,263.7
Post Condition	Linear feet	3,263.7	0.0	0.0	3,263.7

87	Near Lakeside and Eucalyptus Hills, from 0.2	28-29	PA&ED	\$0				
11-SD-67	mile north of Riverford Road to Ellie Lane. Plant	201.151	PS&E	\$35	27-28			
R4.1/15.9	establishment work for pavement rehabilitation		R/W Sup	\$0				
1623	project EA 11-43031		Con Sup	\$58	28-29			
1125000224			R/W Cap	\$0				
43287	<u>Performance Measure</u>		<u>Const Cap</u>	<u>\$197</u>	28-29			
	0.0 Culvert(s)		Total	\$290				

Note: Split plant establishment into project EA 43287/PPNO 11-1623 from parent project EA 43031/PPNO 11-1314.

Performance Measure: Culverts					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition		0.0	0.0	0.0	0.0
Post Condition		0.0	0.0	0.0	0.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)																																						
88 11-SD-78 44.7/15.3 4476 1122000231 43163	In San Marcos, from 0.3 mile west to 0.3 mile east of Mission Road. Modify auxiliary lanes. This is a Construction Manager/General Contractor (CMGC) project. <u>Performance Measure</u> 204.0 Daily vehicle hour(s) of delay (DVHD)	25-26 201.310	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$1,546 \$850 \$3,050 \$2,041 <u>\$18,550</u> \$26,037	Prior Prior Prior 25-26 25-26 <u>25-26</u>																																				
<u>Note:</u> Deleted project. The scope of this project is being delivered by a non-SHOPP project, EA 11-2T240.																																									
89 11-SD-78 37.2/60.0 1383 1119000197 43089	Near Ramona, from east of Magnolia Avenue to west of Wynola Road. Rehabilitate pavement and drainage systems, replace roadside sign panels, and upgrade facilities to Americans with Disabilities Act (ADA) standards. <u>Performance Measure</u> 45.6 Lane mile(s)	25-26 201.121	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$1,864 \$1,746 \$1,039 \$3,821 \$435 <u>\$25,809</u> \$34,714	Prior Prior Prior 25-26 25-26 25-26 25-26	\$1,864 \$1,746 \$1,039 \$3,821 \$735 <u>\$25,809</u> \$35,014	Prior Prior Prior 25-26 25-26 25-26 25-26																																		
<u>Note:</u> Increase in R/W capital is due to additional utility conflicts that would need relocation, which were not originally identified.																																									
<table><tr><td colspan="6">Performance Measure: Pavement</td></tr><tr><td></td><td><u>Unit</u></td><td><u>Good</u></td><td><u>Fair</u></td><td><u>Poor</u></td><td><u>Quantity</u></td><td colspan="3"></td></tr><tr><td>Existing Condition</td><td>Lane mile(s)</td><td>0.0</td><td>45.6</td><td>0.0</td><td>45.6</td><td colspan="3"></td></tr><tr><td>Post Condition</td><td>Lane mile(s)</td><td>45.6</td><td>0.0</td><td>0.0</td><td>45.6</td><td colspan="3"></td></tr></table>									Performance Measure: Pavement							<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>				Existing Condition	Lane mile(s)	0.0	45.6	0.0	45.6				Post Condition	Lane mile(s)	45.6	0.0	0.0	45.6			
Performance Measure: Pavement																																									
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>																																				
Existing Condition	Lane mile(s)	0.0	45.6	0.0	45.6																																				
Post Condition	Lane mile(s)	45.6	0.0	0.0	45.6																																				
90 12-Ora-5 21.1/R29.1 2750 1223000008 0T950	In and near Irvine and Tustin, from 0.3 mile north of Bake Parkway to Red Hill Avenue. Modify High-Occupancy Vehicle (HOV) lane to address service degradation. <u>Performance Measure</u> 400.0 Daily vehicle hour(s) of delay (DVHD)	27-28 28-29 201.310	PA&ED PS&E R/W Sup * Con Sup * R/W Cap * <u>Const Cap</u> Total	\$778 \$1,068 \$0 \$902 \$8 <u>\$4,642</u> \$7,398	Prior 25-26 27-28 27-28 27-28	\$778 \$1,068 \$0 \$902 \$8 <u>\$4,642</u> \$7,398	Prior 25-26 28-29 28-29 28-29																																		

* Phase not authorized

Note: Additional environmental analyses and a public scoping meeting are required, which will need additional time to process the environmental document resulting in project schedule delay.

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
91 12-Ora-22 R0.9/R1.9 R0.6/R1.9 2870A 1221000088 0T020	In Garden Grove, from Valley View Street Route 405 to Springdale Street. Construct auxiliary lane and modify exit ramp. <u>Performance Measure</u> 0.14 Annual fatal and serious injury collision(s)	25-26 201.010	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$1,136 \$1,384 \$298 \$1,978 \$40 \$7,118 \$11,924	Prior Prior Prior 25-26 25-26 25-26	\$1,136 \$1,384 \$298 \$2,571 \$80 \$8,000 \$13,469	Prior Prior Prior 25-26 25-26 25-26

Note: Increase in construction support and capital is due to the additional effort needed for the relocation of tolling system and fiber line that was recently installed by the County of Orange. The change in post miles is also a result of the relocation of the fiber line. Additional R/W capital is needed for the ingress/egress access during construction that was previously not identified.

92 12-Ora-22 R12.1/R13.164 2982 1219000088 0S080	In and near the cities of Santa Ana and Orange, from west of Cambridge Street to Route 55; also at the Orange Maintenance Station at 691 South Tustin Street. Upgrade and install new Transportation Management System (TMS) elements, construct guardrail, reconstruct buildings at the Orange Maintenance Station, construct bicycle and pedestrian improvements, install Zero Emission Vehicle (ZEV) chargers, and construct stormwater treatment Best Management Practices (BMPs).	25-26 201.315	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$1,383 \$3,220 \$0 \$4,910 \$0 \$29,195 \$38,708	Prior Prior 25-26 25-26	\$1,383 \$3,220 \$0 \$5,330 \$45,020 \$54,953	Prior Prior 25-26 25-26
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Performance Measure
13.0 Field element(s)
11.0 Field element(s)

Note: Additional 7,414 square feet of space requirement was identified after Fire Marshal review to satisfy current operational needs. However, because the size of the site cannot accommodate this required building pad area, the building must be reconfigured into a two-story structure. Increase construction capital and support due to the added building space. Decrease performance due to two field elements already addressed under EA 12-0P42U.

Performance Measure: TMS Elements				
	<u>Unit</u>	<u>Good (Operational)</u>	<u>Poor (Not Operational)</u>	<u>Quantity</u>
Existing Condition	Field element(s)	0.0%	0.0%	0.0
Post Condition	Field element(s)	100.0%	0.0%	11.0

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code		Project Costs (\$1,000)			
93	Near Lake Forest, from 0.5 mile south of Portola	25-26	PA&ED	\$2,484	Prior	\$2,484	Prior
12-Ora-241	Parkway to 0.4 mile south of Windy Ridge Toll Plaza;	201.131	PS&E	\$3,297	24-25	\$3,297	24-25
24.5/35.7	also on Route 133 from 0.5 mile south of Irvine		R/W Sup	\$0		\$0	
5498	Boulevard to Route 241(PM 11.4/13.6). Restore fire		Con Sup	\$3,509	25-26	\$3,819	25-26
1222000083	damaged assets, including guardrail, drainage		R/W Cap	\$10	25-26	\$331	25-26
0T730	systems, traffic control devices, signs, and electrical		<u>Const Cap</u>	<u>\$22,490</u>	25-26	<u>\$26,726</u>	25-26
	systems; also make drainage improvements to		Total	\$31,790		\$36,657	
	increase resiliency against natural disasters.						

Performance Measure

16.0 Location(s)

Note: Additional construction capital is needed due to newly identified damaged electrical elements requiring replacement. This has led to more roadway and shoulder closures, increasing project costs and the need for additional construction support. Moreover, additional R/W capital is necessary to address impacts within the Conservation Habitat Area, which the project was recently discovered to encroach upon.

**List of 2024 SHOPP Amendments
(Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)
for Senate Bill 1 Projects**

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)	
1	Near Lancaster, at Avenue G Overcrossing Bridge	25-26	PA&ED	\$0
07-LA-14	No. 53-1860. Financial Contribution Only (FCO) to	201.322	PS&E	\$0
R71.0	city of Lancaster to replace bridge.		R/W Sup	\$0
5728			Con Sup	\$0
0720000106	<u>Performance Measure</u>		R/W Cap	\$0
37010	1.0 Bridge(s)		<u>Const Cap</u>	\$2,000 <u>25-26</u>
			Total	\$2,000

Note: Delete project. The Department was informed by the City of Lancaster that the locally sponsored interchange improvement project (EA 07-30950) will not be able to meet the delivery timeline, with local funding not available until FY 34/35. Although the bridge is rated to be in good condition, the Department would have contributed to improve goods movement with the planned widening of the bridge, thus no plans to reprogram this project.

List of 2024 SHOPP Long Lead Project Amendments

Resolution G-00-13, established in June 2000, provides the Department with the means to develop SHOPP projects which require periods longer than the four-year SHOPP cycle. The Commission authorized the Department to program projects for development only when appropriate. Long lead projects must identify challenges that require additional time beyond the typical four years to complete.

Project No.
Dist-Co-Rte
PM
PPNO
Project ID
EA

Project Location and Description of Work
Performance Measure

FY
Program Code

Project Costs
(\$1,000)

1	Near Crescent City, at Last Chance Grade, from	30-31	PA&ED	\$10,000	Prior	\$10,000	Prior
01-DN-101	Wilson Creek Bridge to 3.8 miles north of Wilson	201.131	PS&E	\$0		\$40,000	25-26
42.0/15.5	Creek Bridge. Permanent roadway restoration and		R/W Sup	\$0		\$0	
12.7/16.5	realignment through a constructed tunnel bypass.		Con Sup	\$0		\$0	
1112	This is a Construction Manager/General		R/W Cap	\$205	30-31	\$205	30-31
0115000099	Contractor (CMGC) project.		<u>Const Cap</u>	<u>\$0</u>		<u>\$0</u>	
0F280	(Long Lead Project)		Total	\$10,205		\$50,205	

Performance Measure

1.0 Location(s)

Note: Update the postmiles and project description to include the selected alternative for a tunnel bypass and to include CMGC as the delivery method. PA&ED was initially programmed and allocated at \$10 million and the allocation was later adjusted to \$50 million. PS&E will be programmed at \$40 million to secure initial funding for design activities in the fiscally constrained 2024 SHOPP for this unique permanent restoration project with a construction capital estimate of \$2-3 billion. The remaining PS&E funding will be programmed in the 2026 SHOPP when funding capacity is available.

2	Near Elk Grove, from Mokelumne River Bridge to	30-31	PA&ED	\$4,350	Prior	\$4,350	Prior
03-Sac-5	Lambert Road Undercrossing. Raise roadway profile,	201.999	* PS&E	\$6,560	27-28	\$6,560	27-28
0.21/4.63	rehabilitate pavement roadway and drainage	201.122	* R/W Sup	\$570	27-28	\$570	27-28
5881	systems, construct concrete barrier and maintenance		* Con Sup	\$10,640	30-31	\$10,640	30-31
0323000171	vehicle pullouts, and upgrade signs, facilities to		* R/W Cap	\$890	30-31	\$890	30-31
4J470	Americans with Disabilities Act (ADA) standards, and		* <u>Const Cap</u>	<u>\$110,700</u>	30-31	<u>\$85,414</u>	30-31
	Transportation Management System (TMS)		Total	\$133,710		\$108,424	
	elements.						
	(Long Lead Project)						

* Phase not authorized

Performance Measure

~~4.4 Centerline mile(s)~~

16.3 Lane mile(s)

Note: The median barrier and paving work is being removed as the location did not meet the safety warrant. ADA elements were also determined to not be necessary. The location is no longer susceptible to sea level rise and the hydraulic analysis shows that a 10-year storm did not overtop the roadway. Roadway rehabilitation is still needed. These updates have resulted in change in program code, description, performance and costs.

Performance Measure: Pavement					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	0.0	14.9	1.4	16.3
Post Condition	Lane mile(s)	16.3	0.0	0.0	16.3

List of 2024 SHOPP Long Lead Project Amendments

Resolution G-00-13, established in June 2000, provides the Department with the means to develop SHOPP projects which require periods longer than the four-year SHOPP cycle. The Commission authorized the Department to program projects for development only when appropriate. Long lead projects must identify challenges that require additional time beyond the typical four years to complete.

Project No.
Dist-Co-Rte
PM
PPNO
Project ID
EA

Project Location and Description of Work
Performance Measure

FY
Program Code

Project Costs
(\$1,000)

3	Near Gaviota and Las Cruces, from north of	28-29	PA&ED	\$3,598	Prior	\$3,598	Prior
05-SB-101	Beckstead Overcrossing to 1.2 miles north of Route	201.150	* PS&E	\$3,489	26-27	\$3,489	26-27
45.0/R50.0	1. Stabilize roadway shoulder, replace failed		* R/W Sup	\$204	26-27	\$204	26-27
3015	retaining walls, and improve fish passage.		* Con Sup	\$1,605	28-29	\$1,605	28-29
0519000150	(Long Lead Project)		* R/W Cap	\$502	28-29	\$502	28-29
1M440			* <u>Const Cap</u>	<u>\$15,653</u>	28-29	<u>\$20,653</u>	28-29
	<u>Performance Measure</u>		Total	\$25,051		\$30,051	

* Phase not authorized

Note: Update construction capital due to continued refinement of scope to address staging and traffic control demands as a result of constructability review and the development of fish passage remediation.



The following comment letter was received directly by the California Transportation Commission and subsequently added to the Department's agenda item for reference.



United States Department of the Interior

NATIONAL PARK SERVICE
Redwood National Park
1111 Second St.
Crescent City, CA 95531



IN REPLY REFER TO:
3.1.03(REDW-8481)

June 3, 2025

Darnell Grisby, Chair
California Transportation Commission
1120 N Street, MS-52
Sacramento, CA 95814



Subject: Caltrans \$40M Programming Request for Last Chance Grade Design Phase

Dear Chair Grisby,

The National Park Service (NPS) is aware that the California Department of Transportation's (Caltrans) has requested \$40 million from the 2024 State Highway Operation and Protection Program (SHOPP) for the Design phase of the Last Chance Grade Permanent Restoration project in Del Norte County.

Last Chance Grade is a critical transportation corridor along U.S. Highway 101 that connects rural northwestern California communities with essential services, economic opportunities, and the broader state transportation network. Chronic landslides and geologic instability have long jeopardized the reliability of this route, threatening public safety, emergency response, and the regional economy.

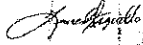
This \$40 million down payment represents the essential first year of funding for the PS&E phase, which is estimated to total \$225 million. The remaining \$185 million is anticipated for programming in the 2026 SHOPP. Beginning the Design phase in January 2026 will keep the project on schedule for a December 2030 Ready to List (RTL) milestone.

Timely investment in the PS&E phase is vital to sustaining project momentum following years of environmental review, public engagement, and preliminary engineering. Advancing this project is not only about infrastructure, but also about protecting communities, preserving economic resilience, and ensuring Californians can rely on one of the only north-south lifelines in the region.

Redwood National and State Parks leadership has worked very closely with Caltrans on this project as it occurs solely within Redwood National and State Parks, an administrative park unit cooperatively managed with California Department of Parks and Recreation. The level of careful planning and support for park resources and values, demonstrated by Caltrans planners, cannot be overstated. This project is ready for full planning and design development, to realize a stable US Highway 101 corridor and minimize impact to park resources. We respectfully urge the Commission to approve the \$40 million programming request at the June 2025 meeting and continue to support full funding for the Design phase in the 2026 SHOPP.

We look forward to your favorable review of this project request and to your continued leadership and commitment to addressing California's most urgent infrastructure need.

Sincerely,



Digitally signed by LEONEL
ARGUELLO
Date: 2025.06.03 15:51:41 -0700

Leonel Arguello
Superintendent (acting)
Redwood National Park
National Park Service

900 Northcrest Drive, PMB 16
Crescent City, California 95531
www.dnlte.org



Tamera Leighton, Executive Director
Tamera@dnlte.org
Desk: (707) 465-3878
Cell: (707) 218-6424

June 3, 2025

California Transportation Commission
1120 N Street, MS-52
Sacramento, CA 95814



Subject: Support for Last Chance Grade Design Phase

Dear Commissioners:

Delivering a solution to the traveling public and our local community has been the highest priority project for Del Norte Local Transportation Commission for decades. We're grateful for your support to date, and we need to keep going. Please program \$40 million in the 2024 SHOPP as requested by Caltrans for the Design phase of the Last Chance Grade Permanent Restoration project in Del Norte County.

The Del Norte region is unwavering in its support for a permanent solution at Last Chance Grade. I have collected resolutions of support from every organization I could identify, and every vote has been unanimous. We recognize this is a long process (I've been working on it since 2006), and our support for this vital west coast US Highway 101 will not waver.

Maintaining the demanding project schedule is a financially responsible action for California and is vital to sustaining the project momentum. This request of \$40 million for the first year of the design phase keeps us moving forward. The remaining \$185 million is anticipated for programming in the 2026 SHOPP and this will keep the project on schedule for a December 2030 Ready to List (RTL) milestone.

Moving this project forward is about protecting the Del Norte region, preserving our economic resilience, and ensuring we can rely on our only north-south lifeline in our region.

Please approve the \$40 million programming request at the June 2024 meeting and continue to support full funding for the Design phase in the 2026 SHOPP.

We're here to be strong partners with you in every way possible, and we're grateful for your commitment to our most urgent need.

Sincerely,

A handwritten signature in blue ink that reads "Tamera Leighton".

Tamera Leighton, Executive Director
Del Norte Local Transportation Commission

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.1a.(6), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **SHOPP AMENDMENT 24H-013**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request to amend the 2024 State Highway Operation and Protection Program (SHOPP) under SHOPP Amendment 24H-013?

RECOMMENDATION:

The Department recommends that the Commission approve SHOPP Amendment 24H-013 that will amend the 2024 SHOPP, in accordance with Senate Bill (SB) 486 and the Road Maintenance and Rehabilitation Program component of SB 1, which require the Commission to approve changes to projects in the SHOPP or to adopt new projects being amended into the SHOPP.

The Department further recommends that the capital project detailed in 2.1a.(6d) be amended in the 2024 SHOPP to update scope, schedule, cost and make other technical changes. The amended project is consistent with the Commission-adopted 2022 Transportation Asset Management Plan.

BACKGROUND:

In each even-numbered year, the Department prepares a 4-year SHOPP which defines major capital improvements necessary to preserve and protect the State Highway System. Periodically, the Department proposes amendments to the SHOPP to address newly identified needs prior to the next programming cycle. Between programming cycles, the Department updates scope, schedule and cost to effectively deliver projects.

SB 486, approved by the Governor on September 30, 2014, requires Commission approval of projects amended into the SHOPP.

Attachment

List of 2024 SHOPP Amendments (Cost, Scope, Schedule and Technical Changes. Includes Federal Emergency Relief.)

Project No. Dist-Co-Rte PM PPNO Project ID EA	Project Location and Description of Work Performance Measure	FY Program Code	Project Costs (\$1,000)					
1 10-Mpa-140 42.00/42.70 3636 1023000226 0P924	Near El Portal and Yosemite National Park, from 1.2 miles west to 0.5 mile west of South Fork Merced River. Construct rock shed. This is a Construction Manager/General Contractor (CMGC) project.	24-25 201.131	PA&ED PS&E R/W Sup Con Sup R/W Cap <u>Const Cap</u> Total	\$0 \$3,100 \$100 \$0 \$5,040 \$0 \$8,240		\$0 Prior Prior 24-25 23-24 24-25	\$0 \$3,100 \$100 \$23,000 \$5,040 \$364,560 \$395,800	Prior Prior 24-25 23-24 24-25
	<u>Performance Measure</u> 1.0 Location(s)							
	Concurrent CONST and CON ENG allocation under Resolution FP-24-105; June 2025.							

Note: Project achieved Agreed To Price April 28, 2025. Fully program previously unfunded phases of this project.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.1s.(1), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **SENATE BILL 1 TRADE CORRIDOR ENHANCEMENT PROGRAM – PROJECT
SCOPE AMENDMENT – EV OASIS SOUTH A PROJECT COMPONENTS
RESOLUTION TCEP-P-2425-08**

ISSUE:

Should the California Transportation Commission (Commission) approve a project scope amendment for two Cycle 3 Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) EV Oasis South A project components, in San Bernardino County, to revise the scope?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve this project scope amendment for the two Cycle 3 SB 1 TCEP EV Oasis South A project components mentioned below, in San Bernardino County, to revise the scope.

- Petro Ontario component (PPNO 6199A)
- TA Barstow component (PPNO 6199C)

DISCUSSION:

The EV Oasis South A project (PPNO 6199) was originally programmed as one project with seven electric vehicle charging sites and infrastructure across Southern California. In June 2024, the Commission approved a project scope amendment, under Resolution TCEP-P-2324-12, to remove a site and increase the capacity of the charging equipment on the six remaining sites to keep the outputs consistent with the baseline agreement. The project scope amendment split the project into two project components, EV Oasis South A (PPNO 6199) and EV Oasis South B (PPNO 6240), with three sites each.

The Department is now requesting a project scope change for each of the five sites that removes all six overnight chargers, upgrades all six 400kw chargers to 1,000kw, and adds

Megawatt Charging Standard (MCS) which is designed for heavy-duty vehicles and provides a higher charging power capacity, which will increase the overall charging capacity of each site. These changes are due to unanticipated design and operational safety issues with back-in charging stalls associated with the overnight chargers. Due to the back-in charging stalls, blind spots increased the safety concerns making it extremely difficult for operators to safely navigate backward into charging bays without spotters or rear camera systems in comparison to a pull through design. As a result of the scope change, there will be a decrease in charging time and the revised design ensures safety issues are addressed. There is no cost increase regarding the TCEP funding, however, there is a minor impact on the projects schedule, which will be adjusted accordingly.

ANALYSIS:

The Department's analysis has concluded that the unanticipated design and operational safety issues at each site is beyond the control of the applicant. The revised scope will remove overnight chargers, upgrade 400kw chargers to 1,000kw, and add MCS while increasing the overall charging capacity of each site.

ANALYSIS RECOMMENDATION:

Based on the Commission's scope change guidance, as well as the analysis of the proposed scope changes, the Department has determined that this scope change will not significantly reduce the overall project benefit. There is no major change to the project benefits due to the overnight chargers not being included in initial calculations. Therefore, the Department supports the proposed scope changes for this project.

There are concurrent Project Scope Amendments for PPNOs 6240A, 6240B, and 6240C, as well as a concurrent time extension request for PPNOs 6199A and 6240A on this month's Commission agenda.

BACKGROUND:

TCEP Guidelines stipulate that any agency implementing a TCEP project, present scope changes to the Department in a timely manner. The Department will make a recommendation to the Commission for final approval with the understanding that scope changes that are significant and result in a decrease in project benefits may result in either a reduction of TCEP funds or removal from the program.

Attachments

Project Scope Change Request

Caltrans' Analysis and Recommendations

May 15, 2025

PROJECT NAME: EV Oasis South A – Petro Ontario

PPNO: 6199A

DATE OF AGENCY REQUEST FOR SCOPE CHANGE: June 26, 2025

Summary

This scope change is for EV Oasis South A - Petro Ontario, which is under the EV Oasis South project. The Project will construct five public charging sites across Southern California to support medium- and heavy-duty (MHD) zero-emission freight vehicles (ZEVs). By deploying DC fast chargers (DCFCs), the project aligns with the Trade Corridor Enhancement Program (TCEP) goals of improving freight efficiency and advancing infrastructure that supports zero-emission goods movement.

Proposed Scope Change per Site:

- Upgrade (6) - 400 kW chargers to (3) - 1000kW chargers (serving the existing 6 pull through stalls)
- Remove (6) - 50 kW overnight chargers
- Increase overall charging capacity to 3,000kW
- Addition of Megawatt Charging Standard (MCS)

Approved vs Proposed Scope Comparison Per Site:

Project Element	Approved Scope Per Site	Proposed Scope Per Site
Locations with ZEV Infrastructure	1	1
Total Number of Charging Stalls	12	6
Number of DCFC Charging Ports	12	6
Charging Connector Types	Combined Charging System (CCS)	CCS and MCS
Charger Configuration	(6) - 400 kW DCFC + 6 - 50 kW overnight EVSEs per site	(3) - 1000kW chargers (serving the existing 6 pull through stalls)
On-Site Charger Capacity (kW)	2,700 kW per site (2,400 kW DCFC + 300 kW overnight)	3,000 kW per site
Energy Storage System – Capacity (MWh / MW)	3.9 MWh / 1.9 MW per site	3.9 MWh / 1.9 MW per site
Solar Canopy Capacity (kWh)	0.4 per site	0.4 per site

Issues:

Proposed Scope Change: The new scope change proposes a reduction in the number of overnight chargers due to unanticipated design (back in stalls) and operational safety issues. **Analysis:** Upgrade of the capacity of charging stalls from 400kW to 1000kW increasing charging capacity, decreasing charging time, and ensures safety issues are addressed and align with Caltrans, NHTSA, AAA and industry standards.

Cost increase with proposed scope: There is a cost increase due to the change of scope. **Analysis:** Cost increase will be marginal and will be covered by the implementing agency – bp pulse. No increase will be requested in TCEP funding from this proposed change.

Impact to Project Schedule: There will be an impact to the project schedule: **Analysis:** Time extension requests for project allocation will be requested at the May and June 2025 to accommodate the impact to the project schedule due to the proposed scope change.

Outputs/Outcomes

Proposed reduction in outputs due to removal of the (6) 50kW overnight chargers & stalls. **Analysis:** Although there is a reduction in the number of stalls, the charging capacity will be increased and will maintain the outcomes as agreed upon in the baseline agreement servicing the pull through stalls. There will be no decrease in charging capacity.

Performance Metrics show a decrease as a result of this proposed scope change. **Analysis:** Although the proposed scope change presents / shows a 40% reduction across most categories from the initial scope, (for instances under Particulate Matter), which could be influenced by the removal of the overnight charger, there is still a reduction. There is also an increase in the battery capacity for the project sites that provide benefits in line with the overall project that will not pose a significant impact/

05/01/25 ePPRs for June Meeting Scope Change														
		Site 1: Petro Ontario (6199A)	Site 2: TA Coachella (6199 B) (Drop March 2025)	Site 3: TA Barstow (6199 C)	Site 4: TA Wheeler Ridge (Arvin) (6240A)	Site 5: Petro Wheeler Ridge (Lebec) (6240B)	Site 6: TA Buttonwillow (6240C)	Site 7 : TA Ontario (Dropped June 2024)	Programmed Metric Totals PPNO 6199 (7 sites) (June 2023)	Metric Totals in Baseline Agreement PPNO 6199 & 6240 (June 2024) (6 sites)	Metric Totals after drop (March 2025) (5 sites)	Proposed Metric Totals after scope change (June 2025) (5 sites)	Difference from previous to current	Difference from beginning to current
Performance Metric														
Air Quality & GHG (only 'Change' required)														
Particulate Matter	PM 2.5 Tons	-2.47		-2.47	-2.47	-2.47	-2.47		-43.97	-24.68	-20.57	-12.34	40%	72%
Particulate Matter	PM 10 Tons	-2.55		-2.55	-2.55	-2.55	-2.55		-45.44	-25.5	-21.25	-12.75	40%	72%
Carbon Dioxide (CO2)	Tons	-84,370.20		-84,370.20	-84,370.20	-84,370.20	-84,370.20		-1,503,566.39	-843701.62	-703,084.68	-421,851.00	40%	72%
Volatile Organic Compounds (VOC)	Tons	-30.20		-30.20	-30.20	-30.20	-30.2		-538.19	-302	-252	-151.00	40%	72%
Sulphur Dioxides (Sox)	Tons	-1.11		-1.11	-1.11	-1.11	-1.11		-19.8	-11	-9	-5.55	39%	72%
Carbon Monoxide (CO)	Tons	-123.09		-123.09	-123.09	-123.09	-123.088		-2,193.56	-1230.88	-1,025.73	-615.44	40%	72%
Nitrogen Oxides (Nox)	Tons	-341.58		-341.58	-341.58	-341.58	-341.576		-6,087.26	-3415.76	-2,846.47	-1,707.88	40%	72%
Safety														
Number of Fatalities		-0.108		-0.108	-0.108	-0.108	-0.108		-1.92	-1.08	-0.9	-0.54	40%	72%
Fatalities per 100 Million VMT		-0.32		-0.32	-0.32	-0.32	-0.32		-0.16	-0.31784	-0.27	-1.59		
Number of Serious Injuries		-2.43		-2.43	-2.43	-2.43	-2.43		-43.39	-24.34	-20.28	-12.17	40%	72%
Number of Serious Injuries per 100 Million VMT		-0.72		-0.72	-0.72	-0.72	-0.72		-3.6	-7.2	-6	-3.60	40%	0%
Economic Development														
Jobs Created (Only 'Build' Required)		4		4	4	4	4		28	48	40	20.00	50%	29%
Cost Effectiveness (Only 'Change' Required)														
Cost Benefit Ratio	Ratio	0.5604		0.5604	0.5604	0.5604	0.5604		9.12	5.60	4.67	2.80	40%	69%

Recommendation: Support

Project Scope Change Request

Caltrans' Analysis and Recommendations

May 15, 2025

PROJECT NAME: EV Oasis South A – TA Barstow

PPNO: 6199C

DATE OF AGENCY REQUEST FOR SCOPE CHANGE: June 26, 2025

Summary

This scope change is for EV Oasis South A – TA Barstow, which is under the EV Oasis South project. The Project will construct five public charging sites across Southern California to support medium- and heavy-duty (MHD) zero-emission freight vehicles (ZEVs). By deploying DC fast chargers (DCFCs), the project aligns with the Trade Corridor Enhancement Program (TCEP) goals of improving freight efficiency and advancing infrastructure that supports zero-emission goods movement.

Proposed Scope Change per Site:

- Upgrade (6) - 400 kW chargers to (3) - 1000kW chargers (serving the existing 6 pull through stalls)
- Remove (6) - 50 kW overnight chargers
- Increase overall charging capacity to 3,000kW
- Addition of Megawatt Charging Standard (MCS)

Approved vs Proposed Scope Comparison Per Site:

Project Element	Approved Scope Per Site	Proposed Scope Per Site
Locations with ZEV Infrastructure	1	1
Total Number of Charging Stalls	12	6
Number of DCFC Charging Ports	12	6
Charging Connector Types	Combined Charging System (CCS)	CCS and MCS
Charger Configuration	(6) - 400 kW DCFC + 6 - 50 kW overnight EVSEs per site	(3) - 1000kW chargers (serving the existing 6 pull through stalls)
On-Site Charger Capacity (kW)	2,700 kW per site (2,400 kW DCFC + 300 kW overnight)	3,000 kW per site
Energy Storage System – Capacity (MWh / MW)	3.9 MWh / 1.9 MW per site	3.9 MWh / 1.9 MW per site
Solar Canopy Capacity (kWh)	0.4 per site	0.4 per site

Issues:

Proposed Scope Change: The new scope change proposes a reduction in the number of overnight chargers due to unanticipated design (back in stalls) and operational safety issues. **Analysis:** *Upgrade of the capacity of charging stalls from 400kW to 1000kW increasing charging capacity, decreasing charging time, and ensures safety issues are addressed and align with Caltrans, NHTSA, AAA and industry standards.*

Cost increase with proposed scope: There is a cost increase due to the change of scope. **Analysis:** *Cost increase will be marginal and will be covered by the implementing agency – bp pulse. No increase will be requested in TCEP funding from this proposed change.*

Impact to Project Schedule: There will be an impact to the project schedule: **Analysis:** *Time extension requests for project allocation will be requested at the May and June 2025 to accommodate the impact to the project schedule due to the proposed scope change.*

Outputs/Outcomes

Proposed reduction in outputs due to removal of the (6) 50kW overnight chargers & stalls. **Analysis:** *Although there is a reduction in the number of stalls, the charging capacity will be increased and will maintain the outcomes as agreed upon in the baseline agreement servicing the pull through stalls. There will be no decrease in charging capacity.*

Performance Metrics show a decrease as a result of this proposed scope change. **Analysis:** *Although the proposed scope change presents / shows a 40% reduction across most categories from the initial scope, (for instances under Particulate Matter), which could be influenced by the removal of the overnight charger, there is still a reduction. There is also an increase in the battery capacity for the project sites that provide benefits in line with the overall project that will not pose a significant impact/*

05/01/25 ePPRs for June Meeting Scope Change														
		Site 1: Petro Ontario (6199A)	Site 2: TA Coachella (6199 B) (Drop March 2025)	Site 3: TA Barstow (6199 C)	Site 4: TA Wheeler Ridge (Arvin) (6240A)	Site 5: Petro Wheeler Ridge (Lebec) (6240B)	Site 6: TA Buttonwillow (6240C)	Site 7 : TA Ontario (Dropped June 2024)	Programmed Metric Totals PPNO 6199 (7 sites) (June 2023)	Metric Totals in Baseline Agreement PPNO 6199 & 6240 (June 2024) (6 sites)	Metric Totals after drop (March 2025) (5 sites)	Proposed Metric Totals after scope change (June 2025) (5 sites)	Difference from previous to current	Difference from beginning to current
Performance Metric														
Air Quality & GHG (only 'Change' required)														
Particulate Matter	PM 2.5 Tons	-2.47		-2.47	-2.47	-2.47	-2.47		-43.97	-24.68	-20.57	-12.34	40%	72%
Particulate Matter	PM 10 Tons	-2.55		-2.55	-2.55	-2.55	-2.55		-45.44	-25.5	-21.25	-12.75	40%	72%
Carbon Dioxide (CO2)	Tons	-84,370.20		-84,370.20	-84,370.20	-84,370.20	-84,370.20		-1,503,566.39	-843,701.62	-703,084.68	-421,851.00	40%	72%
Volatile Organic Compounds (VOC)	Tons	-30.20		-30.20	-30.20	-30.20	-30.2		-538.19	-302	-252	-151.00	40%	72%
Sulphur Dioxides (Sox)	Tons	-1.11		-1.11	-1.11	-1.11	-1.11		-19.8	-11	-9	-5.55	39%	72%
Carbon Monoxide (CO)	Tons	-123.09		-123.09	-123.09	-123.09	-123.088		-2,193.56	-1230.88	-1,025.73	-615.44	40%	72%
Nitrogen Oxides (Nox)	Tons	-341.58		-341.58	-341.58	-341.58	-341.576		-6,087.26	-3415.76	-2,846.47	-1,707.88	40%	72%
Safety														
Number of Fatalities		-0.108		-0.108	-0.108	-0.108	-0.108		-1.92	-1.08	-0.9	-0.54	40%	72%
Fatalities per 100 Million VMT		-0.32		-0.32	-0.32	-0.32	-0.32		-0.16	-0.31784	-0.27	-1.59		
Number of Serious Injuries		-2.43		-2.43	-2.43	-2.43	-2.43		-43.39	-24.34	-20.28	-12.17	40%	72%
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Economic Development														
Jobs Created (Only 'Build' Required)		4		4	4	4	4		28	48	40	20.00	50%	29%
Cost Effectiveness (Only 'Change' Required)														
Cost Benefit Ratio	Ratio	0.5604		0.5604	0.5604	0.5604	0.5604		9.12	5.60	4.67	2.80	40%	69%

Recommendation: Support

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.1s.(2), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **SENATE BILL 1 TRADE CORRIDOR ENHANCEMENT PROGRAM – PROJECT
SCOPE AMENDMENT – EV OASIS SOUTH B PROJECT COMPONENTS
RESOLUTION TCEP-P-2425-09**

ISSUE:

Should the California Transportation Commission (Commission) approve a project scope amendment for three Cycle 3 Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) EV Oasis South B project components, in Kern County, to revise the scope?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve this project scope amendment for the three Cycle 3 SB 1 TCEP EV Oasis South B project components mentioned below, in Kern County, to revise the scope.

- TA Wheeler Ridge component (PPNO 6240A)
- Petro Wheeler Ridge component (PPNO 6240B)
- TA Buttonwillow component (PPNO 6240C)

DISCUSSION:

The EV Oasis South A project (PPNO 6199) was originally programmed as one project with seven electric vehicle charging sites and infrastructure across Southern California. In June 2024, the Commission approved a project scope amendment, under Resolution TCEP-P-2324-12, to remove a site and increase the capacity of the charging equipment on the six remaining sites to keep the outputs consistent with the baseline agreement. The project scope amendment split the project into two projects, EV Oasis South A (PPNO 6199) and EV Oasis South B (PPNO 6240), with three sites each. In March 2025, the Commission approved a project scope amendment, under Resolution TCEP-P-2425-04, to remove a site from the EV Oasis South A project (PPNO 6199) due to insufficient utility power availability.

The Department is now requesting a project scope change for each of the five sites that remove all six overnight chargers, upgrades all six 400kw chargers to 1,000kw, and adds Megawatt Charging Standard (MCS) which is designed for heavy-duty vehicles and provides a higher charging power capacity, which will increase the overall charging capacity of each site. These changes are due to unanticipated design and operational safety issues with back-in charging stalls associated with the overnight chargers. Due to the back-in charging stalls, blind spots increased the safety concerns making it extremely difficult for operators to safely navigate backward into charging bays without spotters or rear camera systems in comparison to a pull through design. As a result of the scope change, there will be a decrease in charging time and the revised design ensures safety issues are addressed. There is no cost increase regarding the TCEP funding, however, there is a minor impact on the projects schedule, which will be adjusted accordingly.

ANALYSIS:

The Department's analysis has concluded that the unanticipated design and operational safety issues at each site is beyond the control of the applicant. The revised scope will remove overnight chargers, upgrade 400kw chargers to 1,000kw, and add MCS while increasing the overall charging capacity of each site.

ANALYSIS RECOMMENDATION:

Based on the Commission's scope change guidance, as well as the analysis of the proposed scope changes, the Department has determined that this scope change will not significantly reduce the overall project benefit. There is no major change to the project benefits due to the overnight chargers not being included in initial calculations. Therefore, the Department supports the proposed scope changes for this project.

There are concurrent Project Scope Amendments for PPNOs 6199A and 6199C, as well as a concurrent time extension request for 6199A and 6240A on this month's Commission agenda.

BACKGROUND:

TCEP Guidelines stipulate that any agency implementing a TCEP project, present scope changes to the Department in a timely manner. The Department will make a recommendation to the Commission for final approval with the understanding that scope changes that are significant and result in a decrease in project benefits may result in either a reduction of TCEP funds or removal from the program.

Attachments

Project Scope Change Request

Caltrans' Analysis and Recommendations

May 15, 2025

PROJECT NAME: EV Oasis South B – TA Wheeler Ridge

PPNO: 6240A

DATE OF AGENCY REQUEST FOR SCOPE CHANGE: June 26, 2025

Summary

This scope change is for EV Oasis South B – TA Wheeler Ridge, which is under the EV Oasis South project. The Project will construct five public charging sites across Southern California to support medium- and heavy-duty (MHD) zero-emission freight vehicles (ZEVs). By deploying DC fast chargers (DCFCs), the project aligns with the Trade Corridor Enhancement Program (TCEP) goals of improving freight efficiency and advancing infrastructure that supports zero-emission goods movement.

Proposed Scope Change per Site:

- Upgrade (6) - 400 kW chargers to (3) - 1000kW chargers (serving the existing 6 pull through stalls)
- Remove (6) - 50 kW overnight chargers
- Increase overall charging capacity to 3,000kW
- Addition of Megawatt Charging Standard (MCS)

Approved vs Proposed Scope Comparison Per Site:

Project Element	Approved Scope Per Site	Proposed Scope Per Site
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Number of DCFC Charging Ports	12	6
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Charger Configuration	(6) - 400 kW DCFC + 6 - 50 kW overnight EVSEs per site	(3) - 1000kW chargers (serving the existing 6 pull through stalls)
On-Site Charger Capacity (kW)	2,700 kW per site (2,400 kW DCFC + 300 kW overnight)	3,000 kW per site
Energy Storage System – Capacity (MWh / MW)	3.9 MWh / 1.9 MW per site	3.9 MWh / 1.9 MW per site
Solar Canopy Capacity (kWh)	0.4 per site	0.4 per site

Issues:

Proposed Scope Change: The new scope change proposes a reduction in the number of overnight chargers due to unanticipated design (back in stalls) and operational safety issues. **Analysis:** Upgrade of the capacity of charging stalls from 400kW to 1000kW increasing charging capacity, decreasing charging time, and ensures safety issues are addressed and align with Caltrans, NHTSA, AAA and industry standards.

Cost increase with proposed scope: There is a cost increase due to the change of scope. **Analysis:** Cost increase will be marginal and will be covered by the implementing agency – bp pulse. No increase will be requested in TCEP funding from this proposed change.

Impact to Project Schedule: There will be an impact to the project schedule: **Analysis:** Time extension requests for project allocation will be requested at the May and June 2025 to accommodate the impact to the project schedule due to the proposed scope change.

Outputs/Outcomes

Proposed reduction in outputs due to removal of the (6) 50kW overnight chargers & stalls. **Analysis:** Although there is a reduction in the number of stalls, the charging capacity will be increased and will maintain the outcomes as agreed upon in the baseline agreement servicing the pull through stalls. There will be no decrease in charging capacity.

Performance Metrics show a decrease as a result of this proposed scope change. **Analysis:** Although the proposed scope change presents / shows a 40% reduction across most categories from the initial scope, (for instances under Particulate Matter), which could be influenced by the removal of the overnight charger, there is still a reduction. There is also an increase in the battery capacity for the project sites that provide benefits in line with the overall project that will not pose a significant impact/

05/01/25 ePPRs for June Meeting Scope Change																		
		Site 1: Petro Ontario (6199A)	Site 2: TA Coachella (6199 B) (Drop March 2025)	Site 3: TA Barstow (6199 C)	Site 4: TA Wheeler Ridge (Arvin) (6240A)	Site 5: Petro Wheeler Ridge (Lebec) (6240B)	Site 6: TA Buttonwillow (6240C)	Site 7 : TA Ontario (Dropped June 2024)	Programmed Metric Totals PPNO 6199 (7 sites) (June 2023)		Metric Totals in Baseline Agreement PPNO 6199 & 6240 (June 2024) (6 sites)		Metric Totals after drop (March 2025) (5 sites)		Proposed Metric Totals after scope change (June 2025) (5 sites)		Difference from previous to current	Difference from beginning to current
Performance Metric																		
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Particulate Matter	PM 2.5 Tons	-2.47		-2.47	-2.47	-2.47	-2.47		-43.97	-24.68	-20.57	-12.34					40%	72%
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Carbon Dioxide (CO2)	Tons	-84,370.20		-84,370.20	-84,370.20	-84,370.20	-84,370.20		-1,503,566.39	-843,701.62	-703,084.68	-421,851.00					40%	72%
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Sulphur Dioxides (Sox)	Tons	-1.11		-1.11	-1.11	-1.11	-1.11		-19.8	-11	-9	-5.55					39%	72%
Carbon Monoxide (CO)	Tons	-123.09		-123.09	-123.09	-123.09	-123.088		-2,193.56	-1230.88	-1,025.73	-615.44					40%	72%
Nitrogen Oxides (Nox)	Tons	-341.58		-341.58	-341.58	-341.58	-341.576		-6,087.26	-3415.76	-2,846.47	-1,707.88					40%	72%
Safety																		
Number of Fatalities		-0.108		-0.108	-0.108	-0.108	-0.108		-1.92	-1.08	-0.9	-0.54					40%	72%
Fatalities per 100 Million VMT		-0.32		-0.32	-0.32	-0.32	-0.32		-0.16	-0.31784	-0.27	-1.59						
Number of Serious Injuries		-2.43		-2.43	-2.43	-2.43	-2.43		-43.39	-24.34	-20.28	-12.17					40%	72%
Number of Serious Injuries per 100 Million VMT		-0.72		-0.72	-0.72	-0.72	-0.72		-3.6	-7.2	-6	-3.60					40%	0%
Economic Development																		
Jobs Created (Only 'Build' Required)		4		4	4	4	4		28	48	40	20.00					50%	29%
Cost Effectiveness (Only 'Change' Required)																		
Cost Benefit Ratio	Ratio	0.5604		0.5604	0.5604	0.5604	0.5604		9.12	5.60	4.67	2.80					40%	69%

Recommendation: Support

Project Scope Change Request

Caltrans' Analysis and Recommendations

May 15, 2025

PROJECT NAME: EV Oasis South B – Petro Wheeler Ridge

PPNO: 6240B

DATE OF AGENCY REQUEST FOR SCOPE CHANGE: June 26, 2025

Summary

This scope change is for EV Oasis South B – Petro Wheeler Ridge, which is under the EV Oasis South project. The Project will construct five public charging sites across Southern California to support medium- and heavy-duty (MHD) zero-emission freight vehicles (ZEVs). By deploying DC fast chargers (DCFCs), the project aligns with the Trade Corridor Enhancement Program (TCEP) goals of improving freight efficiency and advancing infrastructure that supports zero-emission goods movement.

Proposed Scope Change per Site:

- Upgrade (6) - 400 kW chargers to (3) - 1000kW chargers (serving the existing 6 pull through stalls)
- Remove (6) - 50 kW overnight chargers
- Increase overall charging capacity to 3,000kW
- Addition of Megawatt Charging Standard (MCS)

Approved vs Proposed Scope Comparison Per Site:

Project Element	Approved Scope Per Site	Proposed Scope Per Site
Locations with ZEV Infrastructure	1	1
Total Number of Charging Stalls	12	6
Number of DCFC Charging Ports	12	6
Charging Connector Types	Combined Charging System (CCS)	CCS and MCS
Charger Configuration	(6) - 400 kW DCFC + 6 - 50 kW overnight EVSEs per site	(3) - 1000kW chargers (serving the existing 6 pull through stalls)
On-Site Charger Capacity (kW)	2,700 kW per site (2,400 kW DCFC + 300 kW overnight)	3,000 kW per site
Energy Storage System – Capacity (MWh / MW)	3.9 MWh / 1.9 MW per site	3.9 MWh / 1.9 MW per site
Solar Canopy Capacity (kWh)	0.4 per site	0.4 per site

Issues:

Proposed Scope Change: The new scope change proposes a reduction in the number of overnight chargers due to unanticipated design (back in stalls) and operational safety issues. **Analysis:** Upgrade of the capacity of charging stalls from 400kW to 1000kW increasing charging capacity, decreasing charging time, and ensures safety issues are addressed and align with Caltrans, NHTSA, AAA and industry standards.

Cost increase with proposed scope: There is a cost increase due to the change of scope. **Analysis:** Cost increase will be marginal and will be covered by the implementing agency – bp pulse. No increase will be requested in TCEP funding from this proposed change.

Impact to Project Schedule: There will be an impact to the project schedule: **Analysis:** Time extension requests for project allocation will be requested at the May and June 2025 to accommodate the impact to the project schedule due to the proposed scope change.

Outputs/Outcomes

Proposed reduction in outputs due to removal of the (6) 50kW overnight chargers & stalls. **Analysis:** Although there is a reduction in the number of stalls, the charging capacity will be increased and will maintain the outcomes as agreed upon in the baseline agreement servicing the pull through stalls. There will be no decrease in charging capacity.

Performance Metrics show a decrease as a result of this proposed scope change. **Analysis:** Although the proposed scope change presents / shows a 40% reduction across most categories from the initial scope, (for instances under Particulate Matter), which could be influenced by the removal of the overnight charger, there is still a reduction. There is also an increase in the battery capacity for the project sites that provide benefits in line with the overall project that will not pose a significant impact/

05/01/25 ePPRs for June Meeting Scope Change														
		Site 1: Petro Ontario (6199A)	Site 2: TA Coachella (6199 B) (Drop March 2025)	Site 3: TA Barstow (6199 C)	Site 4: TA Wheeler Ridge (Arvin) (6240A)	Site 5: Petro Wheeler Ridge (Lebec) (6240B)	Site 6: TA Buttonwillow (6240C)	Site 7 : TA Ontario (Dropped June 2024)	Programmed Metric Totals PPNO 6199 (7 sites) (June 2023)	Metric Totals in Baseline Agreement PPNO 6199 & 6240 (June 2024) (6 sites)	Metric Totals after drop (March 2025) (5 sites)	Proposed Metric Totals after scope change (June 2025) (5 sites)	Difference from previous to current	Difference from beginning to current
Performance Metric														
Air Quality & GHG (only 'Change' required)														
Particulate Matter	PM 2.5 Tons	-2.47		-2.47	-2.47	-2.47	-2.47		-43.97	-24.68	-20.57	-12.34	40%	72%
Particulate Matter	PM 10 Tons	-2.55		-2.55	-2.55	-2.55	-2.55		-45.44	-25.5	-21.25	-12.75	40%	72%
Carbon Dioxide (CO2)	Tons	-84,370.20		-84,370.20	-84,370.20	-84,370.20	-84,370.20		-1,503,566.39	-843,701.62	-703,084.68	-421,851.00	40%	72%
Volatile Organic Compounds (VOC)	Tons	-30.20		-30.20	-30.20	-30.20	-30.2		-538.19	-302	-252	-151.00	40%	72%
Sulphur Dioxides (Sox)	Tons	-1.11		-1.11	-1.11	-1.11	-1.11		-19.8	-11	-9	-5.55	39%	72%
Carbon Monoxide (CO)	Tons	-123.09		-123.09	-123.09	-123.09	-123.088		-2,193.56	-1230.88	-1,025.73	-615.44	40%	72%
Nitrogen Oxides (Nox)	Tons	-341.58		-341.58	-341.58	-341.58	-341.576		-6,087.26	-3415.76	-2,846.47	-1,707.88	40%	72%
Safety														
Number of Fatalities		-0.108		-0.108	-0.108	-0.108	-0.108		-1.92	-1.08	-0.9	-0.54	40%	72%
Fatalities per 100 Million VMT		-0.32		-0.32	-0.32	-0.32	-0.32		-0.16	-0.31784	-0.27	-1.59		
Number of Serious Injuries		-2.43		-2.43	-2.43	-2.43	-2.43		-43.39	-24.34	-20.28	-12.17	40%	72%
Number of Serious Injuries per 100 Million VMT		-0.72		-0.72	-0.72	-0.72	-0.72		-3.6	-7.2	-6	-3.60	40%	0%
Economic Development														
Jobs Created (Only 'Build' Required)		4		4	4	4	4		28	48	40	20.00	50%	29%
Cost Effectiveness (Only 'Change' Required)														
Cost Benefit Ratio	Ratio	0.5604		0.5604	0.5604	0.5604	0.5604		9.12	5.60	4.67	2.80	40%	69%

Recommendation: Support

Project Scope Change Request

Caltrans' Analysis and Recommendations

May 15, 2025

PROJECT NAME: EV Oasis South B – TA Buttonwillow

PPNO: 6240C

DATE OF AGENCY REQUEST FOR SCOPE CHANGE: June 26, 2025

Summary

This scope change is for EV Oasis South B – TA Buttonwillow, which is under the EV Oasis South project. The Project will construct five public charging sites across Southern California to support medium- and heavy-duty (MHD) zero-emission freight vehicles (ZEVs). By deploying DC fast chargers (DCFCs), the project aligns with the Trade Corridor Enhancement Program (TCEP) goals of improving freight efficiency and advancing infrastructure that supports zero-emission goods movement.

Proposed Scope Change per Site:

- Upgrade (6) - 400 kW chargers to (3) - 1000kW chargers (serving the existing 6 pull through stalls)
- Remove (6) - 50 kW overnight chargers
- Increase overall charging capacity to 3,000kW
- Addition of Megawatt Charging Standard (MCS)

Approved vs Proposed Scope Comparison Per Site:

Project Element	Approved Scope Per Site	Proposed Scope Per Site
Locations with ZEV Infrastructure	1	1
Total Number of Charging Stalls	12	6
Number of DCFC Charging Ports	12	6
Charging Connector Types	Combined Charging System (CCS)	CCS and MCS
Charger Configuration	(6) - 400 kW DCFC + 6 - 50 kW overnight EVSEs per site	(3) - 1000kW chargers (serving the existing 6 pull through stalls)
On-Site Charger Capacity (kW)	2,700 kW per site (2,400 kW DCFC + 300 kW overnight)	3,000 kW per site
Energy Storage System – Capacity (MWh / MW)	3.9 MWh / 1.9 MW per site	3.9 MWh / 1.9 MW per site
Solar Canopy Capacity (kWh)	0.4 per site	0.4 per site

Issues:

Proposed Scope Change: The new scope change proposes a reduction in the number of overnight chargers due to unanticipated design (back in stalls) and operational safety issues. **Analysis:** Upgrade of the capacity of charging stalls from 400kW to 1000kW increasing charging capacity, decreasing charging time, and ensures safety issues are addressed and align with Caltrans, NHTSA, AAA and industry standards.

Cost increase with proposed scope: There is a cost increase due to the change of scope. **Analysis:** Cost increase will be marginal and will be covered by the implementing agency – bp pulse. No increase will be requested in TCEP funding from this proposed change.

Impact to Project Schedule: There will be an impact to the project schedule: **Analysis:** Time extension requests for project allocation will be requested at the May and June 2025 to accommodate the impact to the project schedule due to the proposed scope change.

Outputs/Outcomes

Proposed reduction in outputs due to removal of the (6) 50kW overnight chargers & stalls. **Analysis:** Although there is a reduction in the number of stalls, the charging capacity will be increased and will maintain the outcomes as agreed upon in the baseline agreement servicing the pull through stalls. There will be no decrease in charging capacity.

Performance Metrics show a decrease as a result of this proposed scope change. **Analysis:** Although the proposed scope change presents / shows a 40% reduction across most categories from the initial scope, (for instances under Particulate Matter), which could be influenced by the removal of the overnight charger, there is still a reduction. There is also an increase in the battery capacity for the project sites that provide benefits in line with the overall project that will not pose a significant impact/

05/01/25 ePPRs for June Meeting Scope Change														
		Site 1: Petro Ontario (6199A)	Site 2: TA Coachella (6199 B) (Drop March 2025)	Site 3: TA Barstow (6199 C)	Site 4: TA Wheeler Ridge (Arvin) (6240A)	Site 5: Petro Wheeler Ridge (Lebec) (6240B)	Site 6: TA Buttonwillow (6240C)	Site 7 : TA Ontario (Dropped June 2024)						
Performance Metric														
Air Quality & GHG (only 'Change' required)														
Particulate Matter	PM 2.5 Tons	-2.47		-2.47	-2.47	-2.47	-2.47		-43.97	-24.68	-20.57	-12.34	40%	72%
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Sulphur Dioxides (Sox)	Tons	-1.11		-1.11	-1.11	-1.11	-1.11		-19.8	-11	-9	-5.55	39%	72%
Carbon Monoxide (CO)	Tons	-123.09		-123.09	-123.09	-123.09	-123.088		-2,193.56	-1230.88	-1,025.73	-615.44	40%	72%
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Safety														
Number of Fatalities		-0.108		-0.108	-0.108	-0.108	-0.108		-1.92	-1.08	-0.9	-0.54	40%	72%
Fatalities per 100 Million VMT		-0.32		-0.32	-0.32	-0.32	-0.32		-0.16	-0.31784	-0.27	-1.59		
Number of Serious Injuries		-2.43		-2.43	-2.43	-2.43	-2.43		-43.39	-24.34	-20.28	-12.17	40%	72%
Number of Serious Injuries per 100 Million VMT		-0.72		-0.72	-0.72	-0.72	-0.72		-3.6	-7.2	-6	-3.60	40%	0%
Economic Development														
Jobs Created (Only 'Build' Required)		4		4	4	4	4		28	48	40	20.00	50%	29%
Cost Effectiveness (Only 'Change' Required)														
Cost Benefit Ratio	Ratio	0.5604		0.5604	0.5604	0.5604	0.5604		9.12	5.60	4.67	2.80	40%	69%

Recommendation: Support

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.13, Action

Prepared By: Beverley Newman-Burckhard
Associate Deputy Director

Published Date: June 13, 2025

Subject: 2022 Trade Corridor Enhancement Program Amendment, Resolution G-25-44,
Amending Resolution G-25-37

Recommendation:

Staff recommends the California Transportation Commission (Commission) approve an amendment to the 2022 Trade Corridor Enhancement Program, as reflected in Attachment B. If approved, the Otay Mesa East Port of Entry Project will be programmed as follows:

- **Otay Mesa East Port of Entry Project**
 - Otay Mesa East Land Port of Entry - Early Work Package component (PPNO 0999H), \$36,141,000 for the construction phase in fiscal year 2024-25.
 - Commercial Vehicle Enforcement Facility Design and Construction – Segment 2C (PPNO 0999G), programmed with \$64,000,000 for the construction phase in fiscal year 2024-25.
 - East Otay Mesa Land Port of Entry – Construction component (PPNO 0999I), programmed with \$56,116,000 for the construction phase in fiscal year 2024-25.
 - Otay Mesa East Port of Entry Technology Package component (PPNO 0999J), programmed with \$20,400,000 for the construction phase in fiscal year 2024-25.
 - Otay Mesa East Port of Entry Construction – Underground Utilities and Grading Work Package component (PPNO 0999K), programmed with \$15,000,000 for the construction phase in fiscal year 2024-25.

Issue:

The Otay Mesa East Port of Entry Project, which was programmed in the 2022 Trade Corridor Enhancement Program, was jointly nominated by the California Department of Transportation (Caltrans) and the San Diego Association of Governments. The project was originally programmed with two components with a total programmed amount of \$140,000,000. The project was initially programmed as follows:

1. East Otay Mesa Land Port of Entry – Segment 3 component programmed with \$85,800,000 in Trade Corridor Enhancement Program funds.
2. Commercial Vehicle Enforcement Facility Design and Construction – Segment 2C component programmed with \$54,200,000 in Trade Corridor Enhancement Program funds.

On June 28, 2024, the Commission approved a program amendment to create a third project component, named the Otay Mesa East Land Port of Entry – Early Work Package component, and to change the name of the East Otay Mesa Land Port of Entry – Segment 3 component to the East Otay Mesa Land Port of Entry – Construction component. Additionally, the Commission approved supplemental programming, requested by Caltrans, of \$51,657,000 in Trade Corridor Enhancement Program funds, bringing the total programmed amount to \$191,657,000. With the approval of these actions, the project was programmed as follows:

1. Otay Mesa East Land Port of Entry - Early Work Package component programmed with \$75,000,000 for the construction phase in fiscal year 2024-25.
2. East Otay Mesa Land Port of Entry – Construction component programmed with \$52,657,000 for the construction phase in fiscal year 2024-25.
3. Commercial Vehicle Enforcement Facility Design and Construction – Segment 2C component programmed with \$64,000,000 for the construction phase in fiscal year 2024-25.

On August 16, 2024, the Commission made a partial allocation of \$36,141,000 to the Otay Mesa East Land Port of Entry – Early Work Package component. Subsequently, on October 18, 2024, the Commission approved a program amendment to re-program the remaining programmed balance of \$38,859,000 and the associated scope of work from the Otay Mesa East Land Port of Entry – Early Work Package component to the East Otay Mesa Land Port of Entry – Construction component. With the approval of this action, the project was programmed as follows:

1. Otay Mesa East Land Port of Entry - Early Work Package component programmed with \$36,141,000 for the construction phase in fiscal year 2024-25.
2. East Otay Mesa Land Port of Entry – Construction component programmed with \$91,516,000 for the construction phase in fiscal year 2024-25.
3. Commercial Vehicle Enforcement Facility Design and Construction – Segment 2C programmed with \$64,000,000 for the construction phase in fiscal year 2024-25.

Caltrans and the San Diego Association of Governments are now requesting a program amendment to split the East Otay Mesa Land Port of Entry – Construction component into three separate components, bringing the total number of components programmed to five. This change will allow distinct project elements to be delivered through separate work packages, enabling better schedule management, alignment with federal funding sources, and streamlined procurement of specialized features such as non-intrusive inspection technology, high mast lighting, and inspection booths. If this amendment is approved, the project will be programmed as follows:

1. **Otay Mesa East Land Port of Entry - Early Work Package component (PPNO 0999H), \$36,141,000 for the construction phase in fiscal year 2024-25:** This component will construct an initial portion of the project with infrastructure to support inspections in the near term. The component scope includes constructing southbound passenger lanes and northbound commercial lanes for port of entry operations. Additionally, the component will construct drainage facilities and perform earthwork and paving. Originally, the Early Work Package component included the construction of federal and state facilities. However, this scope of work has been moved to the East Otay Mesa Land Port of Entry – Construction component.
2. **Commercial Vehicle Enforcement Facility Design and Construction – Segment 2C (PPNO 0999G), programmed with \$64,000,000 for the construction phase in fiscal year 2024-25:** This component will construct the California Highway Patrol Commercial Vehicle Enforcement Facility located at the Otay Mesa East Port of Entry.
3. **East Otay Mesa Land Port of Entry – Construction component (PPNO 0999I), programmed with \$56,116,000 for the construction phase in fiscal year 2024-25 (revised component):** This component will construct the federal port of entry, information technology system, and the tolling system and infrastructure.
4. **Otay Mesa East Port of Entry Technology Package component (PPNO 0999J), programmed with \$20,400,000 for the construction phase in fiscal year 2024-25 (new component):** This component will install non-intrusive inspection technology to improve the efficiency of processing vehicles and cargo.
5. **Otay Mesa East Port of Entry Construction – Underground Utilities and Grading Work Package component (PPNO 0999K), programmed with \$15,000,000 for the construction phase in fiscal year 2024-25 (new component):** This component will install a site security fence, complete fine grading, and install underground utilities.

If approved, this amendment will not impact the project's approved scope, which remains intact. Additionally, the amendment will not impact the project's approved programming amount of \$191,657,000.

Background:

On June 28, 2023, the Commission adopted the 2022 Trade Corridor Enhancement Program, which included \$140,000,000 for the Otay Mesa East Port of Entry Project.

On June 28, 2024, the Commission approved a program amendment to the project, which also added supplemental programming of \$51,657,000 in Trade Corridor Enhancement Program funds, bringing the total programmed amount to \$191,657,000.

Prior to the adoption of the 2022 Trade Corridor Enhancement Program, Commission staff worked with Caltrans staff and implementing agencies to review project delivery details identified in the recommended projects' Project Programming Request for accuracy. Per the Trade Corridor Enhancement Guidelines, Commission staff can recommend additional changes to the program through program amendments.

Attachments:

- Attachment A: Resolution G-25-44, Amending Resolution G-25-37
- Attachment B: Updated 2022 Trade Corridor Enhancement Program of Projects

**CALIFORNIA TRANSPORTATION COMMISSION
2022 Trade Corridor Enhancement Program Amendment**

**RESOLUTION G-25-44
AMENDING RESOLUTION G-25-37**

- 1.1 **WHEREAS**, the California Transportation Commission (Commission) adopted the 2022 Trade Corridor Enhancement Program under Resolution G-23-46, on June 28, 2023; and
- 1.2 **WHEREAS**, the Commission approved Resolution G-23-71 at the December 6-7, 2023 Commission Meeting, amending Resolution G-23-46; and
- 1.3 **WHEREAS**, the Commission approved Resolution G-24-54 at the June 27-28, 2024 Commission Meeting, amending Resolution G-23-71; and
- 1.4 **WHEREAS**, the Commission approved Resolution G-24-73 at the October 17-18, 2024 Commission Meeting, amending Resolution G-24-54; and
- 1.5 **WHEREAS**, the Commission approved Resolution G-25-30 at the March 20-21, 2025 Commission Meeting, amending Resolution G-24-73; and
- 1.6 **WHEREAS**, the Commission approved Resolution G-25-37 at the May 15-16, 2025 Commission Meeting, amending Resolution G-25-30; and
- 1.7 **WHEREAS**, the amended 2022 Trade Corridor Enhancement Program includes \$191,657,000 in funding for the Otay Mesa East Port of Entry Project; and
- 1.8 **WHEREAS**, Resolution G-23-46 authorized Commission staff, in consultation with the California Department of Transportation and project sponsors, to make minor technical changes as needed to the 2022 Trade Corridor Enhancement Program to reflect the most current information, or to clarify the Commission's programming commitments, and to request Commission approval of any substantive changes; and
- 1.9 **WHEREAS**, the 2022 Trade Corridor Enhancement Program Guidelines allow Commission staff to bring recommended amendments to the Commission for action.
- 2.1 **NOW, THEREFORE, BE IT RESOLVED**, that the Commission approves the changes to amend the 2022 Trade Corridor Enhancement Program for the Otay Mesa East Port of Entry Project, as reflected in Attachment B; and

- 2.2 **BE IT FURTHER RESOLVED**, that all provisions stipulated in adopting Resolution G-23-46, as well as in previous amendments, including Resolutions G-23-71, G-24-54, G-24-73, G-25-30, and G-25-37 will remain in effect; and
- 2.3 **BE IT FURTHER RESOLVED**, that Resolution G-25-37 is hereby amended.

2022 Trade Corridor Enhancement Program
Amended Program of Projects
Resolution G-25-44, Amending Resolution G-25-37
(1,000s)

Reference Number 4.13
June 26-27, 2025
Attachment B

Project Title	Type	County	Nominating Agency	Region	State	Regional	Total TCEP	2023-24	2024-25	R/W	Const	PS&E	R/W Sup	Con Sup	Total Project Cost
America's Green Port Gateway															
America's Green Port Gateway: Pier B Early Rail Enhancements Project - East Expansion	Rail	Los Angeles	Southern California Association of Governments/Caltrans/City of Long Beach	Los Angeles/Inland Empire	\$ 14,934	\$ 22,401	\$ 37,335	\$ 37,336	\$ -	\$ -	\$ 35,407	\$ -	\$ -	\$ 1,929	\$ 69,363
America's Green Port Gateway: Pier B Early Rail Enhancements Project - Locomotive	Rail	Los Angeles	Southern California Association of Governments/Caltrans/City of Long Beach	Los Angeles/Inland Empire	\$ 13,243	\$ 19,864	\$ 33,107	\$ 33,106	\$ -	\$ -	\$ 32,015	\$ -	\$ -	\$ 1,091	\$ 57,654
Otay Mesa East Port of Entry															
East Otay Mesa Land Port of Entry - Construction	Border	San Diego	San Diego Association of Governments/Caltrans	San Diego/Border	\$41,867 \$6,457	\$ 49,659	\$91,616 \$56,116	\$ -	\$91,616 \$56,116	\$ -	\$91,616 \$56,116	\$ -	\$ -	\$ -	\$593,429 \$413,029
Otay Mesa East Port of Entry - Technology Package	Border	San Diego	San Diego Association of Governments/Caltrans	San Diego/Border	\$ 20,400	\$ -	\$ 20,400	\$ -	\$ 20,400	\$ -	\$ 20,400	\$ -	\$ -	\$ -	\$ 56,400
Otay Mesa East Port of Entry - Underground Utilities and Grading Work Package	Border	San Diego	San Diego Association of Governments/Caltrans	San Diego/Border	\$ 15,000	\$ -	\$ 15,000	\$ -	\$ 15,000	\$ -	\$ 15,000	\$ -	\$ -	\$ -	\$ 129,000
Otay Mesa East Land Port of Entry - Early Work Package	Border	San Diego	San Diego Association of Governments/Caltrans	San Diego/Border	\$ 27,840	\$ 8,301	\$ 36,141	\$ -	\$ 36,141	\$ -	\$ 30,891	\$ -	\$ -	\$ 5,250	\$ 36,141
CVEF Design and Construction - Segment 2C	Border	San Diego	San Diego Association of Governments/Caltrans	San Diego/Border	\$ 64,000	\$ -	\$ 64,000	\$ -	\$ 64,000	\$ -	\$ 64,000	\$ -	\$ -	\$ -	\$ 71,500
Westbound I80 Cordelia Commercial Vehicle Enforcement Facility	Border	Solano	Solano Transportation Authority/Caltrans	Bay Area/Central Valley	\$ 51,600	\$ 77,400	\$ 129,000	\$ -	\$ 129,000	\$ -	\$ 109,000	\$ -	\$ -	\$ 20,000	\$ 243,270
I-5 Harbor Drive															
I-5/SR 15/Harbor Drive 2.0	HWY	San Diego	San Diego Association of Governments/Caltrans	San Diego/Border	\$ 6,500	\$ -	\$ 6,500	\$ 6,500	\$ -	\$ -	\$ -	\$ 6,500	\$ -	\$ -	\$ 68,400
SR-15 Operational Improvements	HWY	San Diego	San Diego Association of Governments/Caltrans	San Diego/Border	\$ 6,900	\$ 5,100	\$ 12,000	\$ 12,000	\$ -	\$ 5,500	\$ -	\$ 6,500	\$ -	\$ -	\$ 62,800
I-10 Corridor Freight and Managed Lane Project															
I-10: 1-15 to Pepper Avenue (Contract 2B)*	HWY	San Bernardino	Southern California Association of Governments/San Bernardino County Transportation Authority	Los Angeles/Inland Empire	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
I-10: 1-15 to Pepper Avenue (Contract 2A)	HWY	San Bernardino	Southern California Association of Governments/San Bernardino County Transportation Authority	Los Angeles/Inland Empire	\$ -	\$ 75,000	\$ 75,000	\$ 75,000	\$ -	\$ -	\$ 75,000	\$ -	\$ -	\$ -	\$ 425,158
I-10: 1-15 to Pepper Avenue Toll System Provider (TSP D/B Contract)*	HWY	San Bernardino	Southern California Association of Governments/San Bernardino County Transportation Authority	Los Angeles/Inland Empire	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
I-10: 1-15 to Pepper Avenue Zero Emission Fueling Infrastructure A	ZEV	San Bernardino	Southern California Association of Governments/San Bernardino County Transportation Authority	Los Angeles/Inland Empire	\$ -	\$ 5,000	\$ 5,000	\$ -	\$ 5,000	\$ -	\$ 5,000	\$ -	\$ -	\$ -	\$ 7,999
I-10: 1-15 to Pepper Avenue Zero Emission Fueling Infrastructure B	ZEV	San Bernardino	Southern California Association of Governments/San Bernardino County Transportation Authority	Los Angeles/Inland Empire	\$ -	\$ 5,000	\$ 5,000	\$ -	\$ 5,000	\$ -	\$ 5,000				\$ 18,502
Tulare Six Lane and Paige Ave	HWY	Tulare	Tulare Association of Governments/Caltrans	Bay Area/Central Valley	\$ 14,788	\$ 22,181	\$ 36,969	\$ 36,969	\$ -	\$ 32,417	\$ -	\$ -	\$ 4,552	\$ -	\$ 238,143
Port of Oakland Microgrid															
Green Power Microgrid Project - Seaport Terminals	ZE	Alameda	Caltrans/Metropolitan Transportation Commission	Bay Area/Central Valley	\$ 8,882	\$ 13,324	\$ 22,206	\$ -	\$ 22,206	\$ -	\$ 22,206	\$ -	\$ -	\$ -	\$ 32,004
Green Power Microgrid Project - Harbor Facilities	ZE	Alameda	Caltrans/Metropolitan Transportation Commission	Bay Area/Central Valley	\$ 6,603	\$ 9,904	\$ 16,507	\$ -	\$ 16,507	\$ -	\$ 16,507	\$ -	\$ -	\$ -	\$ 23,791
Green Power Microgrid Project - Substations/BESS Pre-Construction	ZE	Alameda	Caltrans/Metropolitan Transportation Commission	Bay Area/Central Valley	\$ 779	\$ 1,169	\$ 1,948	\$ 1,948	\$ -	\$ -	\$ -	\$ 1,948	\$ -	\$ -	\$ 4,174
Green Power Microgrid - EV Chargers Pre-Construction	ZE	Alameda	Caltrans/Metropolitan Transportation Commission	Bay Area/Central Valley	\$ 272	\$ 408	\$ 680	\$ 680	\$ -	\$ -	\$ -	\$ 680	\$ -	\$ -	\$ 1,522
Green Power Microgrid - Solar Pre-Construction	ZE	Alameda	Caltrans/Metropolitan Transportation Commission	Bay Area/Central Valley	\$ 118	\$ 176	\$ 294	\$ 294	\$ -	\$ -	\$ -	\$ 294	\$ -	\$ -	\$ 551
Fresno UPRR Double Track	Rail	Stanislaus	San Joaquin Regional Rail Commission/Caltrans	Bay Area/Central Valley	\$ 16,000	\$ 24,000	\$ 40,000	\$ -	\$ 40,000	\$ -	\$ 30,485	\$ 2,865	\$ -	\$ 6,650	\$ 133,400
I-605 Valley Boulevard Interchange Improvements	HWY	Los Angeles	Southern California Association of Governments/Los Angeles County Metropolitan Transportation Authority	Los Angeles/Inland Empire	\$ -	\$ 33,570	\$ 33,570	\$ 33,570	\$ -	\$ -	\$ 33,570	\$ -	\$ -	\$ -	\$ 53,280
SR 91 Operational and Multimodal Improvements	HWY	Orange	Caltrans/Orange County Transportation Authority	Los Angeles/Inland Empire	\$ -	\$ 42,566	\$ 42,566	\$ 42,566	\$ -	\$ -	\$ 42,566	\$ -	\$ -	\$ -	\$ 107,670
Rt 49 Corridor Improvement	HWY	Nevada	Nevada County Transportation Commission/Caltrans	Other	\$ -	\$ 14,615	\$ 14,615	\$ -	\$ 14,615	\$ -	\$ 14,615	\$ -	\$ -	\$ -	\$ 33,100
EV Oasis South															
EV Oasis South A – Petro Ontario	ZE	San Bernardino	Caltrans/San Joaquin Valley Air Pollution Control District	Los Angeles/Inland Empire	\$ 4,683	\$ -	\$ 4,683	\$ -	\$ 4,683	\$ -	\$ 4,683	\$ -	\$ -	\$ -	\$ 8,748
EV Oasis South A – TA Barstow	ZE	San Bernardino	Caltrans/San Joaquin Valley Air Pollution Control District	Los Angeles/Inland Empire	\$ 4,682	\$ -	\$ 4,682	\$ -	\$ 4,682	\$ -	\$ 4,682	\$ -	\$ -	\$ -	\$ 7,953
EV Oasis South B – TA Wheeler Ridge	ZE	Kern	Caltrans/San Joaquin Valley Air Pollution Control District	Bay Area/Central Valley	\$ 4,683	\$ -	\$ 4,683	\$ -	\$ 4,683	\$ -	\$ 4,683	\$ -	\$ -	\$ -	\$ 9,248
EV Oasis South B – Petro Wheeler Ridge	ZE	Kern	Caltrans/San Joaquin Valley Air Pollution Control District	Bay Area/Central Valley	\$ 4,682	\$ -	\$ 4,682	\$ -	\$ 4,682	\$ -	\$ 4,682	\$ -	\$ -	\$ -	\$ 7,953
EV Oasis South B – TA Buttonwillow	ZE	Kern	Caltrans/San Joaquin Valley Air Pollution Control District	Bay Area/Central Valley	\$ 4,683	\$ -	\$ 4,683	\$ -	\$ 4,683	\$ -	\$ 4,683	\$ -	\$ -	\$ -	\$ 6,931
Southern California Hydrogen Fueling Facilities															
Southern California Hydrogen Fueling Stations - Phase 1 (Construction)	ZE	San Bernardino, Riverside, Los Angeles, San Diego	Caltrans	Los Angeles/Inland Empire/San Diego/Border	\$ 1,125	\$ -	\$ 1,125	\$ 1,125	\$ -	\$ -	\$ 1,125	\$ -	\$ -	\$ -	\$ 10,750
Southern California Hydrogen Fueling Stations - Phase 1 A (Procurement)	ZE	San Bernardino, Riverside, Los Angeles, San Diego	Caltrans	Los Angeles/Inland Empire/San Diego/Border	\$ 5,450	\$ -	\$ 5,450	\$ 5,450	\$ -	\$ -	\$ 5,450	\$ -	\$ -	\$ -	\$ 8,695

2022 Trade Corridor Enhancement Program
Amended Program of Projects
Resolution G-25-44, Amending Resolution G-25-37
(1,000s)

Project Title	Type	County	Nominating Agency	Region	State	Regional	Total TCEP	2023-24	2024-25	R/W	Const	PS&E	R/W Sup	Con Sup	Total Project Cost
Southern California Hydrogen Fueling Stations - Phase 3 (Construction)	ZE	San Bernardino, Riverside, Los Angeles, San Diego	Caltrans	Los Angeles/Inland Empire/San Diego/Border	\$ 7,595	\$ -	\$ 7,595	\$ 7,595	\$ -	\$ -	\$ 7,595	\$ -	\$ -	\$ -	\$ 15,021
Southern California Hydrogen Fueling Stations - Phase 3 A (Procurement)	ZE	San Bernardino, Riverside, Los Angeles, San Diego	Caltrans	Los Angeles/Inland Empire/San Diego/Border	\$ 10,855	\$ -	\$ 10,855	\$ 10,855	\$ -	\$ -	\$ 10,855	\$ -	\$ -	\$ -	\$ 23,756
I-710 Integrated Corridor Management (ICM)	HWY	Los Angeles	Southern California Association of Governments/Los Angeles County	Los Angeles/Inland Empire	\$ 11,140	\$ 16,700	\$ 27,840	\$ -	\$ 27,840	\$ -	\$ 27,840	\$ -	\$ -	\$ -	\$ 40,150
I-5 Managed Lanes	HWY	Sacramento	Sacramento Area Council of Governments/Sacramento Transportation	Bay Area/Central Valley	\$ -	\$ 10,000	\$ 10,000	\$ 10,000	\$ -	\$ -	\$ -	\$ 10,000	\$ -	\$ -	\$ 383,360
Southbound 99 to Westbound 58 Connector	HWY	Kern	Kern Council of Governments/Caltrans	Bay Area/Central Valley	\$ -	\$ 9,380	\$ 9,380	\$ -	\$ 9,380	\$ 6,580	\$ -	\$ 1,750	\$ -	\$ 1,050	\$ 13,400
Fix 5 Cascade Gateway	HWY	Shasta/Tehama	Shasta Regional Transportation Authority/Caltrans	Other	\$ 53,912	\$ 16,937	\$ 70,849	\$ 450	\$ 70,399	\$ -	\$ 62,919	\$ 450	\$ -	\$ 7,480	\$ 82,611
I-80 San Pablo Dam Road Interchange Improvements	HWY	Contra Costa	Metropolitan Transportation Commission/Caltrans	Bay Area/Central Valley	\$ 9,600	\$ 10,100	\$ 19,700	\$ 19,700	\$ -	\$ 15,600	\$ -	\$ 4,100	\$ -	\$ -	\$ 112,000
SR 4 Wagon Trail Realignment	HWY	Calaveras	Calaveras Council of Governments	Other	\$ -	\$ 5,250	\$ 5,250	\$ -	\$ 5,250	\$ 5,250	\$ -	\$ -	\$ -	\$ -	\$ 56,000
Sacramento County WattEV Innovative Freight Terminal	ZE	Sacramento	Sacramento County	Bay Area/Central Valley	\$ -	\$ 33,688	\$ 33,688	\$ 1,018	\$ 32,670	\$ -	\$ 32,670	\$ 1,018	\$ -	\$ -	\$ 61,850
SR 60 Potrero Boulevard Interchange	HWY	Riverside	Southern California Association of Governments/City of Beaumont	Los Angeles/Inland	\$ -	\$ 33,500	\$ 33,500	\$ -	\$ 33,500	\$ -	\$ 33,500	\$ -	\$ -	\$ -	\$ 50,000
TOWN Rail Safety Improvements	Rail	Alameda	Metropolitan Transportation Commission/City of Oakland	Bay Area/Central Valley	\$ -	\$ 30,200	\$ 30,200	\$ 2,833	\$ 27,367	\$ -	\$ 23,274	\$ 2,833	\$ -	\$ 4,093	\$ 59,005
Maritime Support Facility Access – Terminal Island	HWY	Los Angeles	Southern California Association of Governments/Port of Los Angeles	Los Angeles/Inland Empire	\$ 5,974	\$ 8,962	\$ 14,936	\$ -	\$ 14,936	\$ -	\$ 13,044	\$ -	\$ -	\$ 1,892	\$ 39,670
U.S. 395 Freight Mobility and Safety Project															
US 395 – Phase 2 Freight Mobility and Safety Project-CON Mainline	HWY	San Bernardino	Southern California Association of Governments/San Bernardino County Transportation Authority	Los Angeles/Inland Empire	\$ -	\$ 30,000	\$ 30,000	\$ 30,000	\$ -	\$ -	\$ 30,000	\$ -	\$ -	\$ -	\$ 74,583
US 395 – Phase 2 Freight Mobility and Safety Project - Zero Emission Construction	HWY	San Bernardino	Southern California Association of Governments/San Bernardino County Transportation Authority	Los Angeles/Inland Empire	\$ -	\$ 1,250	\$ 1,250	\$ 1,250	\$ -	\$ -	\$ 1,250	\$ -	\$ -	\$ -	\$ 1,625
US 395 – Phase 2 Freight Mobility and Safety Project - Zero Emission Procurement	HWY	San Bernardino	Southern California Association of Governments/San Bernardino County Transportation Authority	Los Angeles/Inland Empire	\$ -	\$ 3,750	\$ 3,750	\$ 3,750	\$ -	\$ -	\$ 3,750	\$ -	\$ -	\$ -	\$ 4,875
Five Cities Multimodal Transportation Network Enhancement Project															
Five Cities Multimodal Transportation Network Enhancement Project	HWY	San Luis Obispo	San Luis Obispo Council of Governments	Central Coast	\$ 49,666	\$ 11,628	\$ 61,294	\$ -	\$ 61,294	\$ -	\$ 52,411	\$ -	\$ -	\$ 8,883	\$ 81,094
Shell Beach Road Shared Use Path Extension	HWY	San Luis Obispo	San Luis Obispo Council of Governments	Central Coast	\$ -	\$ 4,137	\$ 4,137	\$ 508	\$ 3,629	\$ -	\$ 2,268	\$ 508	\$ -	\$ 1,361	\$ 4,355
Grant Line Road Safety Freight Mobility	HWY	Sacramento	Sacramento Area Council of Governments/Capital SouthEast Connector Joint Powers Authority	Bay Area/Central Valley	\$ -	\$ 3,000	\$ 3,000	\$ 3,000	\$ -	\$ -	\$ -	\$ 3,000	\$ -	\$ -	\$ 47,859
*Projects not funded by TCEP but part of the overall project				Total	\$ 453,046	\$ 658,120	\$ 1,111,166	\$ 377,503	\$ 733,663	\$ 65,347	\$ 939,142	\$ 42,446	\$ 4,552	\$ 59,679	\$ 3,498,343

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.15, Action

Prepared By: Kayla Giese
Assistant Deputy Director

Published Date: June 13, 2025

Subject: Local Partnership Formulaic Program Amendment, Resolution LPP-P-2425-09,
Amending Resolution LPP-P-2425-08

Recommendation:

Staff recommends the California Transportation Commission (Commission) approve an amendment to the Local Partnership Formulaic Program with the following actions, which are individually noted in Attachment A:

- Deprogram a total of \$79,000 from three projects in Alameda County.
- Deprogram \$3,147,000 from the State Route 94/State Route 125 South to East Connector Project in San Diego County.
- Amend the Roadway Rehabilitation, Street Light & Street Sign Replacement project in Sacramento County to update the project title.
- Add two projects in Alameda and Sonoma Counties and program \$2,462,000.

Issue:

Commission staff reviewed each request to amend the Local Partnership Formulaic Program, and each request is summarized below:

- Amend the below three projects in Alameda County and deprogram a total of \$79,000 in Local Partnership Formulaic Program funds from the Construction phase. A total of \$79,000 was identified as eligible cost savings at contract acceptance for the projects and deprogramming these funds will make them available for future programming by AC Transit. These funds were deallocated at the May 2025 Commission meeting under agenda item Reference Number 2.6s.(4). as follows:
 - Deprogram \$49,000 from the Division 2 Maintenance Roof Replacement Project, leaving \$431,000 allocated to the Construction phase.
 - Deprogram \$2,000 from the Division 6 Security Enhancements Phase 2 Project, leaving \$415,000 allocated to the Construction phase.

- Deprogram \$28,000 from the Division 4 Guard Booths and Gates Replacement Project, leaving \$416,000 allocated to the Construction phase.
- Amend the State Route 94/State Route 125 South to East Connector (in and near the Cities of La Mesa and Lemon Grove) Project in San Diego County and deprogram \$3,147,000 from the Right of Way phase. These funds have lapsed and will be returned to the Formulaic Program and redistributed to eligible agencies with the next fund distribution cycle.
- Amend the Roadway Rehabilitation, Street Light & Street Sign Replacement Project in Sacramento County to update the project title to Mariposa Avenue Safe Routes to School Project, which was the new name of the project at allocation and completion. This amendment will update the originally programmed project title to the allocated project title so that the city can complete the project close-out process.
- Add the Zero Emission Bus Maintenance Bays Rehabilitation Project in Alameda County and program \$1,394,000 of its 2022 and 2024 Formulaic Program funding to the Construction phase in Fiscal Year 2025-26. The project will rehabilitate maintenance bays for use on zero-emission buses at four different sites and is vital to helping AC Transit meet zero-emission bus service transition goals.
- Add the Caulfield Lane Improvements Project in Sonoma County and program \$1,068,000 of its 2022 Formulaic Program funding to the Construction phase in Fiscal Year 2025-26. The project will reconstruct and resurface pavement, implement complete streets improvements, and add protected bike lanes and additional safety improvements for all road users.

These actions leave \$53,639,000 of the 2022 Local Partnership Formulaic Program funding available for programming through June 30, 2026, and \$173,920,000 of the 2024 Local Partnership Formulaic Program funding available for programming through June 30, 2028.

Background:

Senate Bill (SB) 1 (Beall, Chapter 5, Statutes of 2017) created the Local Partnership Program. Assembly Bill 115 (Committee on Budget, Chapter 20, Statutes of 2017) clarified SB 1 language regarding local and regional transportation agency eligibility and expanded the types of projects eligible for the program. SB 1 authorizes \$200 million annually to fund the Local Partnership Program.

The Local Partnership Formulaic Program provides funding to counties, cities, districts, and regional transportation agencies in which voters have approved fees, taxes, or tolls dedicated solely to transportation improvements. Only agencies included in the Commission-adopted Local Partnership Formulaic Program Funding Distribution are eligible to request funding for projects in the Local Partnership Formulaic Program.

Resolution LPP-P-2425-09:

Be it Resolved that the Commission does hereby amend the Local Partnership Formulaic Program to:

1. Deprogram a total of \$79,000 from three projects in Alameda County.
2. Deprogram \$3,147,000 from the State Route 94/State Route 125 South to East Connector Project in San Diego County.
3. Amend the Roadway Rehabilitation, Street Light & Street Sign Replacement project in Sacramento County to update the project title.
4. Add the Zero Emission Bus Maintenance Bays Rehabilitation Project in Alameda County and the Caulfield Lane Improvements Project in Sonoma County and program a total of \$2,462,000.

Attachments:

- Attachment A: Amended Local Partnership Formulaic Program

Amended Local Partnership Formulaic Program
(\$1,000's)
Resolution LPP-P-2425-09, amending Resolution LPP-P-2425-08

Reference No. - 4.15
June 26-27, 2025
Attachment A

PPNO	County	Nominating Agency	Cycle	Multi-funded	Project Title	Implementing Agency	2017-18	2018-19	2019-20	2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	CON	Con Eng	PAED	PSE	RW	RW SUP	Total 2018 LPP Programmed	Total 2019 LPP Programmed	Total 2020 LPP Programmed	Total 2022 LPP Programming	Total 2024 LPP Programming	Total LPP Programmed	Total LPP Unprogrammed	
04-004AD	Alameda	Alameda County Transportation Commission (ACTC)	3	LPP-C, STP, CRSSA	Oakland Alameda Access	ACTC / Caltrans						\$196									\$196							\$196		
04-004AD	Alameda	Alameda County Transportation Commission (ACTC)	4	LPP-C, STP, CRSSA	Oakland Alameda Access	ACTC / Caltrans							\$7,281				\$7,281									\$7,281			\$7,281	
04-2905F	Alameda	Alameda County Transportation Commission	3	LPP-C, STP	Route 680 Southbound Express Lane from Route 84 to Alcosta Boulevard (co-nominated with Bay Area Toll Authority)	Caltrans					\$8,006							\$8,006							\$8,006			\$8,006		
04-2905F	Alameda	Alameda County Transportation Commission	3	LPP-C, STP	Route 680 Southbound Express Lane from Route 84 to Alcosta Boulevard (co-nominated with Bay Area Toll Authority)	Caltrans						\$4,003					\$4,003								\$4,003			\$4,003		
04-2103D	Alameda	Alameda County Transportation Commission	1	TCEP	7th Street Grade Separation East Segment (75GSE)	ACTC													\$0									\$0		
04-2103D	Alameda	Alameda County Transportation Commission	1	TCEP	7th Street Grade Separation East Segment (75GSE)	ACTC			\$3,180												\$3,180		\$3,180					\$3,180		
04-0800D	Alameda	Alameda County Transportation Commission	1, 2	RSP-STP	SR84 Widening and SR84-680 Interchange Improvements	Caltrans				\$8,602								\$8,602					\$4,800	\$3,802	\$12,205	\$7,281	\$0	\$31,268		
Alameda County Transportation Commission							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$4,612	\$3,968	\$3,802	\$4,003	\$4,199	\$4,003	\$3,778	\$3,778	\$3,533	\$3,508					\$7,980	\$3,802	\$12,205	\$7,281	\$0	\$31,268			
																						Funds Left to Program		\$0	\$0	\$0	\$273	\$7,041	\$7,314	
04-0300D	Alameda	Alameda-Contra Costa Transit District	3		Division 4 Guard Booths and Gates Replacement	AC Transit						\$416					\$416											\$416		
04-0300B	Alameda	Alameda-Contra Costa Transit District	3		Division 5 Security Enhancements Phase 2	AC Transit						\$415					\$415											\$415		
04-0300A	Alameda	Alameda-Contra Costa Transit District	3		Purchase Zero-Emissions Forklifts	AC Transit					\$417						\$417											\$417		
04-2320C	Alameda	Alameda-Contra Costa Transit District	2		Division 2 Maintenance Roof Replacement	AC Transit				\$431							\$431								\$431			\$431		
04-2320A	Alameda/ Contra Costa	Alameda-Contra Costa Transit District	1		Customer Service Center Rehab	AC Transit	\$50													\$50		\$50					\$50			
04-2320A	Alameda/ Contra Costa	Alameda-Contra Costa Transit District	1		Customer Service Center Rehab	AC Transit				\$765							\$765											\$765		
04-2320B	Alameda/ Contra Costa	Alameda-Contra Costa Transit District	1	LPP-C, TIRCP	Purchase Zero Emission High Capacity Buses to Support Transbay Tomorrow and Clean Corridors Plan	AC Transit				\$253							\$253			\$253					\$253			\$253		
	Alameda	Alameda-Contra Costa Transit District	4,5		Zero Emission Bus Maintenance Bays Rehabilitation Project	AC Transit							\$1,394				\$1,394								\$750	\$644	\$1,394			
Alameda-Contra Costa Transit District							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$534	\$534	\$480	\$417	\$444	\$417	\$332	\$332	\$323	\$321					\$1,968	\$431	\$1,248	\$750	\$644	\$4,141			
																						Funds Left to Program		\$0	\$0	\$0	\$0	\$0	\$0	
Amador							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$200	\$200	\$200					\$0	\$0	\$0	\$0	\$0	\$0			
																						Funds Left to Program		\$0	\$0	\$0	\$0	\$200	\$400	\$600
04-0300M	Alameda, Contra Costa, San Francisco, San Mateo, Santa Clara	Bay Area Rapid Transit District (BART)	3		BART Clipper Upgrade Program Integration	BART						\$1,928					\$1,928											\$1,928		
04-LP007	Alameda/ Contra Costa/ San Francisco	Bay Area Rapid Transit District	2, 3		Bay Area Rapid Transit District Fire Suppression System Upgrade	BART				\$85										\$85					\$85			\$85		
04-LP007	Alameda/Contra Costa/ San Francisco	Bay Area Rapid Transit District	2, 3		Bay Area Rapid Transit District Fire Suppression System Upgrade	BART				\$964							\$964								\$964			\$964		
04-LP007	Alameda/ Contra Costa/ San Francisco	Bay Area Rapid Transit District	2, 3		Bay Area Rapid Transit District Fire Suppression System Upgrade	BART				\$760							\$760								\$760			\$760		
04-2318B	San Francisco	Bay Area Rapid Transit District	1		BART Downtown San Francisco Escalator Replacement	BART			\$1,880								\$1,880						\$1,880					\$1,880		
04-2364F	Alameda	Bay Area Rapid Transit District	4		Lake Merritt Uninterruptible Power Supply Replacement and Relocation	BART							\$1,615				\$1,615									\$1,615			\$1,615	
Bay Area Rapid Transit District							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$940	\$940	\$845	\$944	\$1,004	\$944	\$808	\$807	\$1,004	\$996	\$1,615				\$1,880	\$845	\$2,882	\$1,615	\$0	\$7,232			
																						Funds Left to Program		\$0	\$0	\$0	\$0	\$2,000	\$2,000	
04-0300G	Contra Costa/ Marin	Bay Area Toll Authority	3		Richmond - San Rafael Bridge Substation Upgrade	Caltrans						\$3,563					\$1,563	\$2,000										\$3,563		
04-2351A	San Francisco	Bay Area Toll Authority	3		Yerba Buena Island West Side Bridges (co-nominated with SFCTA)	San Francisco County Transportation Authority						\$5,000					\$5,000											\$5,000		
04-2905F	Alameda	Bay Area Toll Authority	3	LPP-C, STP	Route 680 Southbound Express Lane from Route 84 to Alcosta Boulevard (co-nominated with ACTC)	Caltrans					\$10,000						\$10,000								\$10,000			\$10,000		
04-0125	Contra Costa/ Marin	Bay Area Toll Authority	1, 2		Richmond San Rafael Structural Steel Paint - tower deck and towers	Caltrans			\$19,885								\$9,385	\$10,500					\$10,234	\$9,649				\$19,885		
Bay Area Toll Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$8,118	\$8,118	\$9,649	\$6,099	\$6,445	\$6,099	\$3,696	\$3,693	\$4,847	\$4,807					\$10,234	\$9,649	\$18,563	\$0	\$0	\$38,448			
																						Funds Left to Program		\$0	\$0	\$0	\$7,369	\$9,634	\$17,043	
75-2090C	San Mateo	Caltrain	3		San Mateo Replacement Parking Track	Peninsula Corridor Joint Powers Board						\$3,773					\$3,773											\$3,773		
04-0300E	San Jose	Caltrain	3		Guadalupe River Bridge Replacement	Peninsula Corridor Joint Powers Board						\$4,606					\$4,606											\$4,606		
75-3013K	San Mateo	Caltrain	3		Bayshore Station Pedestrian Overpass Rehabilitation	Peninsula Corridor Joint Powers Board					\$3,435						\$3,435											\$3,435		
Caltrain							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$0	\$0	\$0	\$0	\$6,623	\$5,791	\$956	\$956	\$904	\$897					\$0	\$0	\$11,814	\$0	\$0	\$11,814			
																						Funds Left to Program		\$0	\$0	\$0	\$1,912	\$1,891	\$3,713	
04-0658G	San Mateo	City/County Association of Governments of San Mateo County	4	RIP	Northern Cities Expansion (Intelligent Transportation System Improvements in Daly City, Brisbane, and Colma)	Daly City, Brisbane, Colma							\$400				\$400									\$400			\$400	
04-0658G	San Mateo	City/County Association of Governments of San Mateo County	2	RIP	Northern Cities Expansion (Intelligent Transportation System Improvements in Daly City, Brisbane, and Colma)	Daly City, Brisbane, Colma				\$122										\$122					\$122			\$122		
04-0658G	San Mateo	City/County Association of Governments of San Mateo County	3	RIP	Northern Cities Expansion (Intelligent Transportation System Improvements in Daly City, Brisbane, and Colma)	Daly City, Brisbane, Colma					\$600						\$600								\$600			\$600		
04-0658D	San Mateo	C/CAG of San Mateo County	1	SCICP, LPP-C, RIP	US 101 Managed Lanes (co-nominated with San Mateo County Transportation Authority)	SMCTA			\$270											\$270			\$270					\$270		
City/County Association of Governments of San Mateo County							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$135	\$135	\$122	\$200	\$200	\$200	\$200	\$200	\$200	\$200					\$270	\$122	\$600	\$400	\$0	\$1,392			
																						Funds Left to Program		\$0	\$0	\$0	\$0	\$400	\$400	
04-2330H	San Francisco	City and County of San Francisco	3	SHOPP	13th Street Safety	San Francisco Municipal Transportation Agency (SFMTA)						\$550					\$550											\$550		
04-2330H	San Francisco	City and County of San Francisco	3	SHOPP	13th Street Safety	San Francisco Municipal Transportation Agency (SFMTA)						\$850					\$850											\$850		
04-2330G	San Francisco	City and County of San Francisco	3		Traffic Signal Upgrade - Contract 36	SFMTA						\$29					\$29									\$29			\$29	
04-2330G	San Francisco	City and County of San Francisco	3		Traffic Signal Upgrade - Contract 36	SFMTA					\$1,780						\$1,780									\$1,780			\$1,780	

Amended Local Partnership Formulaic Program
(\$1,000's)
Resolution LPP-P-2425-09, amending Resolution LPP-P-2425-08

Reference No. - 4.15
June 26-27, 2025
Attachment A

		County	Nominating Agency	Cycle	Multi-funded	Project Title	Implementing Agency	2017-18	2018-19	2019-20	2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	CON	Con Eng	PAED	PSE	RW	RW Sup	Total 2018 LPP Programmed	Total 2019 LPP Programmed	Total 2020 LPP Programmed	Total 2022 LPP Programming	Total 2024 LPP Programming	Total LPP Programmed	Total LPP Unprogrammed	
04-2330F		San Francisco	City and County of San Francisco	3		Western Addition Area - Traffic Signal Upgrades	SFMTA					\$3,179						\$3,179								\$3,179			\$3,179		
City and County of San Francisco								2018, 2019, 2020, 2022, 2024 Fund Distribution	\$0	\$0	\$0	\$5,453	\$482	\$453	\$200	\$200	\$200	\$200						\$0	\$0	\$6,388	\$0	\$0	\$6,388		
								Funds Left to Program	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0							\$0	\$0	\$400	\$400			\$800
01-3221	Lake	Clearlake		3		Clearlake 18th Avenue Improvements	Clearlake						\$500					\$500								\$500			\$500		
01-3205	Lake	Clearlake		3		Modoc Street /Second Street Pavement Rehabilitation	Clearlake				\$100							\$100								\$100			\$100		
01-3205	Lake	Clearlake		2		Modoc Street /Second Street Pavement Rehabilitation	Clearlake			\$100								\$100							\$100			\$100			
01-1890	Lake	Clearlake		1		Burns Valley School/Civic Center - Bicycle/Pedestrian Enhancements	Clearlake		\$200									\$200						\$200				\$200			
Clearlake								2018, 2019, 2020, 2022, 2024 Fund Distribution	\$100	\$100	\$100	\$200	\$200	\$200	\$200	\$200	\$200						\$200	\$100	\$600	\$0	\$0	\$900			
								Funds Left to Program	\$200	\$100	\$600	\$0	\$0	\$400	\$400										\$0	\$0	\$400	\$400			\$800
04-2090P	Contra Costa	Contra Costa Transportation Authority (CCTA)		4		Countywide Smart Signals	CCTA							\$2,732				\$2,732								\$2,732			\$2,732		
04-2090P	Contra Costa	Contra Costa Transportation Authority (CCTA)		4		Countywide Smart Signals	CCTA							\$187				\$187								\$187			\$187		
04-2091C	Contra Costa	Contra Costa Transportation Authority (CCTA)		4		Orinda 2024 Annual Pavement Rehabilitation No.4178 (co-nominated with Orinda)	Orinda							\$1,000				\$1,000								\$1,000			\$1,000		
04-2090A	Contra Costa	Contra Costa Transportation Authority		4		Orinda 2023 Annual Pavement Rehabilitation, Various Streets (co-nominated with Orinda)	Orinda							\$1,000				\$1,000								\$1,000			\$1,000		
04-2321H	Contra Costa	Contra Costa Transportation Authority		3		Automated Driving System Demonstration Program: Rosemoor First Mile/Last Mile Shared Autonomous Vehicles in Walnut Creek, On-Demand Autonomous Vehicle Shuttle Service in Martinez, and Personal Autonomous Vehicle Mobility on I-680 Corridor in San Ramon	CCTA				\$175									\$175					\$175			\$175			
04-2321H	Contra Costa	Contra Costa Transportation Authority		3		Automated Driving System Demonstration Program: Rosemoor First Mile/Last Mile Shared Autonomous Vehicles in Walnut Creek, On-Demand Autonomous Vehicle Shuttle Service in Martinez, and Personal Autonomous Vehicle Mobility on I-680 Corridor in San Ramon	CCTA					\$2,894								\$2,894					\$2,894			\$2,894			
04-2321H	Contra Costa	Contra Costa Transportation Authority		3		Automated Driving System Demonstration Program: Rosemoor First Mile/Last Mile Shared Autonomous Vehicles in Walnut Creek, On-Demand Autonomous Vehicle Shuttle Service in Martinez, and Personal Autonomous Vehicle Mobility on I-680 Corridor in San Ramon	CCTA						\$108					\$108							\$108			\$108			
04-2321H	Contra Costa	Contra Costa Transportation Authority		3		Automated Driving System Demonstration Program: Rosemoor First Mile/Last Mile Shared Autonomous Vehicles in Walnut Creek, On-Demand Autonomous Vehicle Shuttle Service in Martinez, and Personal Autonomous Vehicle Mobility on I-680 Corridor in San Ramon	CCTA						\$3,477					\$3,477							\$3,477			\$3,477			
04-2321B	Contra Costa	Contra Costa Transportation Authority		2		Innovate 680: I-680 Northbound HOT3/0V	CCTA			\$2,286											\$2,286					\$2,286			\$2,286		
04-2321E	Contra Costa	Contra Costa Transportation Authority		2		Central Avenue and Carlton Boulevard Pavement Rehabilitation	El Cerrito			\$100								\$100							\$100			\$100			
04-2321F	Contra Costa	Contra Costa Transportation Authority		2		Arnold Drive Sidewalk Gap Closure	Martinez			\$100								\$100							\$100			\$100			
04-0298E	Contra Costa	Contra Costa Transportation Authority		1	LPP-C, STIP, BACDFP	I-680/SR 4 Interchange Phase 3	CCTA		\$4,799									\$4,799						\$4,799				\$4,799			
04-2321C	Contra Costa	Contra Costa Transportation Authority		1		El Cerrito Pavement	El Cerrito		\$200									\$200						\$200				\$200			
04-2321D	Contra Costa	Contra Costa Transportation Authority		1		Martinez Pavement	Martinez		\$200									\$200						\$200				\$200			
Contra Costa Transportation Authority								2018, 2019, 2020, 2022, 2024 Fund Distribution	\$2,615	\$2,584	\$2,486	\$2,182	\$2,290	\$2,182	\$2,460	\$2,459	\$2,289	\$2,273					\$5,199	\$2,486	\$6,654	\$4,919	\$0	\$19,258			
								Funds Left to Program	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0						\$0	\$0	\$4,962	\$4,962			\$4,962	
05-3195	San Benito	Council of San Benito County Governments		3		Holister 2022 City Wide Slurry Seal	Holister						\$600					\$600								\$600			\$600		
05-3061	San Benito	Council of San Benito County Governments		2		State Route 25 Expressway Conversion - State Route 25 / 156 Interchange	Caltrans			\$2,500										\$2,500					\$2,500			\$2,500			
05-0297	San Benito	Council of San Benito County Governments		2	RIP, BIP	State Route 156 Improvement	Caltrans			\$2,500								\$2,500							\$2,500			\$2,500			
Council of San Benito County Governments								2018, 2019, 2020, 2022, 2024 Fund Distribution	\$0	\$0	\$5,000	\$200	\$200	\$200	\$200	\$200	\$200	\$200					\$0	\$5,000	\$600	\$0	\$0	\$5,600			
								Funds Left to Program	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0						\$0	\$0	\$0	\$0	\$400	\$400	\$800	
04-0090N	Contra Costa	El Cerrito		3		El Cerrito del Norte Transit-Oriented Development Complete Streets Improvements	El Cerrito						\$400					\$400								\$400			\$400		
04-2321K	Contra Costa	El Cerrito		3		El Dorado Street, Gladys Avenue, B Street Pavement, Curb, Gutter, Signage, Marking Improvements	El Cerrito			\$200								\$200								\$200			\$200		
El Cerrito								2018, 2019, 2020, 2022, 2024 Fund Distribution	\$0	\$0	\$0	\$200	\$200	\$200	\$200	\$200	\$200	\$200					\$0	\$0	\$600	\$0	\$0	\$600			
								Funds Left to Program	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0						\$0	\$0	\$400	\$400			\$800	
05-3093	San Luis Obispo	El Paso de Robles		3		Oreston Road Active Transportation and Bike and School Access Improvements	El Paso de Robles						\$600					\$600								\$600			\$600		
El Paso de Robles								2018, 2019, 2020, 2022, 2024 Fund Distribution	\$0	\$0	\$0	\$200	\$200	\$200	\$200	\$200	\$200	\$200						\$0	\$0	\$600	\$0	\$0	\$600		
								Funds Left to Program	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0							\$0	\$0	\$400	\$400			\$800
01-4804	Mendocino	Fort Bragg		4		Fort Bragg 2025 Pavement Preservation	Fort Bragg							\$400				\$400								\$400			\$400		
01-4804	Mendocino	Fort Bragg		4		Fort Bragg 2025 Pavement Preservation	Fort Bragg								\$1,200			\$1,200								\$1,200			\$1,200		
01-4758	Mendocino	Fort Bragg		3	STIP	Fort Bragg - Street Rehabilitation 2022, Various streets	Fort Bragg						\$900					\$900								\$900			\$900		
01-4720	Mendocino	Fort Bragg		2		2020 Maple Street Storm Drain and Street Rehabilitation	Fort Bragg		\$2												\$2				\$2			\$2			
01-4720	Mendocino	Fort Bragg		2		2021 Maple Street Storm Drain and Street Rehabilitation	Fort Bragg			\$98								\$98							\$98			\$98			
01-4713	Mendocino	Fort Bragg		1		2019 Street Rehabilitation	Fort Bragg		\$12												\$12				\$12			\$12			
01-4713	Mendocino	Fort Bragg		1		2020 Street Rehabilitation	Fort Bragg		\$188									\$188							\$188			\$188			
Fort Bragg								2018, 2019, 2020, 2022, 2024 Fund Distribution	\$100	\$100	\$100	\$200	\$200	\$200	\$200	\$1,400	\$200	\$200					\$200	\$100	\$600	\$1,000	\$0	\$2,500			
								Funds Left to Program	\$200	\$100	\$600	\$0	\$0	\$0	\$0	\$0	\$0	\$0							\$0	\$0	\$400	\$400			\$800
05-8046	Fresno	Fresno County Transportation Authority		3		Blackstone and McKinley Grade Separation	Fresno						\$3,902								\$3,902					\$3,902			\$3,902		
05-7062	Fresno	Fresno County Transportation Authority		1	LPP-C	Golden State Corridor Infrastructure Improvements and Economic Development Phase I and Phase II	Fresno County				\$272							\$272							\$272			\$272			
05-7056	Fresno	Fresno County Transportation Authority		3		Veterans Boulevard Interchange and Extension - Phase 4b	Fresno				\$1,904							\$1,904								\$1,904			\$1,904		
05-6995	Fresno	Fresno County Transportation Authority		2		Veterans Boulevard Interchange and Extension - Phase 4a	Fresno			\$2,173								\$2,173							\$2,173			\$2,173			
05-6929	Fresno	Fresno County Transportation Authority		1		Willow Avenue Street Improvements	Clovis		\$1,040												\$1,040				\$1,040			\$1,040			
05-6929	Fresno	Fresno County Transportation Authority		1		Willow Avenue Street Improvements	Clovis		\$3,232									\$3,232							\$3,232			\$3,232			
05-8146	Fresno	Fresno County Transportation Authority		5		Hendon Avenue Widening Milburn to Polk	Fresno									\$4,055		\$4,055								\$4,055			\$4,055		
05-6705	Fresno	Fresno County Transportation Authority		4		Excelsior Expressway	Fresno								\$4,164			\$4,164								\$4,164			\$4,164		
Fresno County Transportation Authority								2018, 2019, 2020, 2022, 2024 Fund Distribution	\$2,285	\$2,259	\$2,173	\$1,904	\$1,998	\$1,904	\$2,165	\$2,163	\$2,035	\$2,020					\$4,544	\$2,173	\$5,808	\$4,164	\$4,055	\$20,742			
								Funds Left to Program	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0							\$0	\$0	\$164	\$164			\$164

Amended Local Partnership Formulaic Program
(\$1,000's)
Resolution LPP-P-2425-09, amending Resolution LPP-P-2425-08

Reference No: 4.15
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Attachment A

PPNO	County	Nominating Agency	Cycle	Multi-funded	Project Title	Implementing Agency	2017-18	2018-19	2019-20	2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	CON	Con Eng	PAED	PSE	RW	RW Sup	Total 2018 LPP Programmed	Total 2019 LPP Programmed	Total 2020 LPP Programmed	Total 2022 LPP Programming	Total 2024 LPP Programming	Total LPP Programmed	Total LPP Unprogrammed		
11-1507	Imperial	Imperial County Local Transportation Authority	3, 4		Brawley Road Improvements on C Street from Western Avenue to First Street, Steven Street from Evelyn Avenue to Richard Avenue, and Ronald Street from Evelyn Avenue to Richard Avenue	Brawley						\$196	\$131					\$327							\$196	\$131		\$327			
11-1511	Imperial	Imperial County Local Transportation Authority	3, 4		Scaron Road Improvements between Cole Boulevard and Jasper Road	Calvesco						\$284	\$187					\$471							\$284	\$187		\$471			
11-1508	Imperial	Imperial County Local Transportation Authority	3, 4		El Centro Commercial Avenue Rehabilitation	El Centro						\$315	\$205					\$520							\$315	\$205		\$520			
11-1510	Imperial	Imperial County Local Transportation Authority	3		Holville Pedestrian and Bicycle Improvements on 9th Street between Melon Road and Olive Avenue	Holville						\$7									\$7				\$7			\$7			
11-1510	Imperial	Imperial County Local Transportation Authority	3, 4		Holville Pedestrian and Bicycle Improvements on 9th Street between Melon Road and Olive Avenue	Holville						\$49	\$40					\$89							\$49	\$40		\$89			
11-1506	Imperial	Imperial County Local Transportation Authority	3, 4		7th Street Rehabilitation and Pedestrian Improvements between South D Street and South E Street in City of Imperial	Imperial						\$148	\$101					\$249							\$148	\$101		\$249			
11-1509	Imperial	Imperial County Local Transportation Authority	3, 4		Imperial County Evan Hewes Highway Rehabilitation from Haskell Road to Bennett Road	Imperial County						\$484	\$316					\$800							\$484	\$316		\$800			
11-1513	Imperial	Imperial County Local Transportation Authority	3, 4		Westmorland South Center Street Pedestrian and Drainage Improvement	Westmorland						\$28	\$23					\$51							\$28	\$23		\$51			
11-1426	Imperial	Imperial County Local Transportation Authority	2		Orchard Road/Cedar Roadway Improvement	Holville				\$90								\$90							\$90			\$90			
11-1425	Imperial	Imperial County Local Transportation Authority	2		2020 Dogwood Road Overlay Improvements	El Centro				\$339								\$339							\$339			\$339			
11-1409	Imperial	Imperial County Local Transportation Authority	1, 2		2020 Legion Street Improvements-Phase 2	Brawley			\$209									\$209						\$53	\$156		\$209				
11-1378	Imperial	Imperial County Local Transportation Authority	1		Allen/Clark Road Improvements	Imperial							\$154					\$154							\$154			\$154			
11-1377	Imperial	Imperial County Local Transportation Authority	1		Date Street Sidewalk Improvements	Calipatria				\$41								\$41							\$41			\$41			
11-1379	Imperial	Imperial County Local Transportation Authority	1		Picacho Road Rehabilitation	Imperial County				\$523								\$523							\$523			\$523			
11-1611	Imperial	Imperial County Local Transportation Authority	5		Cole Boulevard between Scaron Road and Railroad Crossing Rehabilitation Project	Calvesco								\$191				\$191								\$191		\$191			
11-1615	Imperial	Imperial County Local Transportation Authority	5		Bowler Road Rehabilitation from Jasper Rd to Fawcett Rd	Imperial County								\$311				\$311								\$311		\$311			
11-1613	Imperial	Imperial County Local Transportation Authority	5		Glenn Road Rehabilitation Project from Evan Hewes Hwy to Cooley Rd	El Centro								\$216				\$216								\$216		\$216			
11-1614	Imperial	Imperial County Local Transportation Authority	5		Pine Avenue Revitalization and Pedestrian Improvement Project	Holville								\$38				\$38								\$38		\$38			
11-1612	Imperial	Imperial County Local Transportation Authority	5		Main Street Roadway Improvements Project	Calipatria								\$29				\$29								\$29		\$29			
11-1619	Imperial	Imperial County Local Transportation Authority	5		Western Avenue Pedestrian Improvements	Brawley								\$142				\$142								\$142		\$142			
11-1618	Imperial	Imperial County Local Transportation Authority	5		Allen Boulevard Rehabilitation	Imperial								\$115				\$115								\$115		\$115			
Imperial County Local Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$538	\$538	\$556	\$507	\$535	\$507	\$557	\$556	\$524	\$520						\$771	\$555	\$1,511	\$1,003	\$1,042	\$4,882			
																							Funds Left to Program		\$0	\$0	\$0	\$81	\$2	\$83	
07-4296	Los Angeles	Los Angeles County Metropolitan Transportation Authority (LA Metro)	4	TRICP	East San Fernando Valley Transit Corridor - Initial Operating Segment	LA Metro							\$74,944					\$74,944								\$74,944		\$74,944			
07-6055	Los Angeles	Los Angeles County Metropolitan Transportation Authority	4		Transit Access Pass (TAP) Bus Mobile Validator Replacement for TAP Municipal Partners	LA Metro							\$2,456					\$2,456								\$2,456		\$2,456			
07-5751	Los Angeles	Los Angeles County Metropolitan Transportation Authority	2	TRICP	Metro Red and Purple Line Core Capacity Improvements	LA Metro			\$18,659									\$18,659						\$18,659			\$18,659				
07-5751	Los Angeles	Los Angeles County Metropolitan Transportation Authority	3	TRICP	Metro Red and Purple Line Core Capacity Improvements	LA Metro					\$2,083							\$2,083							\$2,083			\$2,083			
07-5751	Los Angeles	Los Angeles County Metropolitan Transportation Authority	3	TRICP	Metro Red and Purple Line Core Capacity Improvements	LA Metro					\$37,506							\$37,506							\$37,506			\$37,506			
07-5751	Los Angeles	Los Angeles County Metropolitan Transportation Authority	3	TRICP	Metro Red and Purple Line Core Capacity Improvements	LA Metro				\$37,506								\$37,506							\$37,506			\$37,506			
07-5751	Los Angeles	Los Angeles County Metropolitan Transportation Authority	3	TRICP	Metro Red and Purple Line Core Capacity Improvements	LA Metro					\$37,506							\$37,506							\$37,506			\$37,506			
07-5902	Los Angeles	Los Angeles County Metropolitan Transportation Authority	1, 2		C Line (Green) Extension to Torrance	LACMTA				\$9,000								\$2,686					\$6,314			\$9,000					
07-5543	Los Angeles	Los Angeles County Metropolitan Transportation Authority	2		Transit Access Pass Bus Farebox Upgrade - Municipal Transit Operators	LACMTA				\$5,000								\$5,000						\$5,000			\$5,000				
07-5510	Los Angeles	Los Angeles County Metropolitan Transportation Authority	1		Transit Access Pass Bus Farebox and Rail Station Validator Upgrades (Metro)	LA Metro			\$8,201												\$8,201				\$8,201		\$8,201				
07-5510	Los Angeles	Los Angeles County Metropolitan Transportation Authority	1		Transit Access Pass Bus Farebox and Rail Station Validator Upgrades (Metro)	LA Metro			\$14,299									\$14,299								\$14,299					
07-5104	Los Angeles	Los Angeles County Metropolitan Transportation Authority	1		Wilshire/Rosa Parks Station Mezzanine Improvements	LACMTA			\$14,808									\$14,808								\$14,808					
07-5501	Los Angeles	Los Angeles County Metropolitan Transportation Authority	1		West Santa Ana Branch Transit Corridor (WSAB)	LACMTA			\$16,500									\$16,500								\$16,500					
Los Angeles County Metropolitan Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$29,247	\$29,247	\$29,973	\$37,506	\$39,589	\$37,506	\$41,724	\$41,790	\$37,648	\$37,389						\$58,494	\$29,973	\$114,691	\$77,400	\$0	\$286,468			
																							Funds Left to Program		\$0	\$0	\$0	\$6,024	\$76,637	\$81,061	
06-8120	Madera	Madera County Transportation Authority	4		Avenue 15 Road Rehabilitation	Madera County								\$492				\$492								\$492		\$492			
06-8056	Madera	Madera County Transportation Authority	3, 4	CRSSA	Madera 2022-23 City Streets 3R and ADA Improvements	Madera						\$180	\$154					\$334							\$180	\$154		\$334			
06-7058	Madera	Madera County Transportation Authority	3		Madera City Streets 2020-21 Roadway Rehabilitation, Reconstruction, ADA Compliance	Madera				\$306								\$306								\$306		\$306			
06-6990	Madera	Madera County Transportation Authority	2		Avenue 7 Road Rehabilitation	Madera County				\$341								\$341							\$341			\$341			
06-6990	Madera	Madera County Transportation Authority	3		Avenue 7 Road Rehabilitation	Madera County					\$318							\$318								\$318		\$318			
06-6932	Madera	Madera County Transportation Authority	1		2018-19 3R and ADA Improvements	Madera				\$180								\$180								\$180		\$180			
06-6933	Madera	Madera County Transportation Authority	1		Road 3D Curb & Gutter, Sidewalk, Shoulder Paving & Rehabilitation	Madera County				\$175								\$175								\$175		\$175			
06-6930	Madera	Madera County Transportation Authority	1		Orange Avenue and 6th Street Pavement Rehabilitation	Chowchilla				\$142								\$142								\$142		\$142			
06-6931	Madera	Madera County Transportation Authority	1		2017-18 3R and ADA Improvements	Madera				\$217								\$217								\$217		\$217			
Madera County Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$359	\$355	\$341	\$306	\$318	\$306	\$334	\$333	\$321	\$319						\$714	\$341	\$804	\$646	\$0	\$2,905			
																							Funds Left to Program		\$0	\$0	\$0	\$21	\$640	\$0	\$1,000
04-2052J	Contra Costa	Martinez	4		Downtown Pavement Resurfacing	Martinez								\$400				\$400								\$400		\$400			
04-2377	Contra Costa	Martinez	3		Martinez Pavement Resurfacing	Martinez				\$500								\$500							\$500		\$500				
04-009H	Contra Costa	Martinez	3		Morelo Avenue Sidewalk Gap Closure	Martinez				\$83								\$83								\$83		\$83			
04-009H	Contra Costa	Martinez	3		Morelo Avenue Sidewalk Gap Closure	Martinez							\$16					\$16			\$16					\$16		\$16			
04-009H	Contra Costa	Martinez	3		Morelo Avenue Sidewalk Gap Closure	Martinez						\$1						\$1			\$1					\$1		\$1			
Martinez							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$0	\$0	\$0	\$200	\$200	\$200	\$200	\$200	\$200	\$200				\$1			\$0	\$600	\$400	\$0	\$1,000			
																							Funds Left to Program		\$0	\$0	\$0	\$400	\$0	\$400	
10-3571	Merced	Merced County Transportation Authority	3		Ingriner Grade Rehabilitation	Merced County						\$786						\$786								\$786		\$786			
10-3572	Merced	Merced County Transportation Authority	3		Lincoln Boulevard Rehabilitation	Merced County						\$800						\$800								\$800		\$800			
10-3500	Merced	Merced County Transportation Authority	2		Los Banos Pioneer Road Improvement	Los Banos				\$599								\$599			\$599				\$599		\$599				
10-3487	Merced	Merced County Transportation Authority	1		Merced Street Maintenance and Rehabilitation Program	Merced					\$1,253							\$1,253							\$1,253		\$1,253				
Merced County Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$630	\$623	\$599	\$520	\$546	\$520	\$596	\$596	\$563	\$559						\$1,253	\$599	\$1,586	\$0	\$0	\$3,438			
																							Funds Left to Program		\$0	\$0	\$0	\$1,192	\$1,122		\$2,314
06-3234	Monterey	Monterey-Salinas Transit District	4		PY 20-23 Zero-Emission Bus Replacement	Monterey-Salinas Transit (MST)								\$684				\$684								\$684		\$684			

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California Transportation Commission

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PPNO	County	Nominating Agency	Cycle	Multi-funded	Project Title	Implementing Agency	2017-18	2018-19	2019-20	2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27	CON	Con Eng	PAED	PSE	RW	RW Sup	Total 2018 LPP Programmed	Total 2019 LPP Programmed	Total 2020 LPP Programmed	Total 2022 LPP Programming	Total 2024 LPP Programming	Total LPP Programmed	Total LPP Unprogrammed
03-2206	Sacramento	Sacramento Transportation Authority	3		Sacramento Corridor Improvements on Florin Road	Sacramento					\$2,348						\$2,348								\$2,348			\$2,348	
03-2204	Sacramento	Sacramento Transportation Authority	3		Folsom Natoma Street Drainage Phase 2	Folsom				\$380							\$380								\$380			\$380	
03-2205	Sacramento	Sacramento Transportation Authority	3		Gal 2021 Pavement Rehabilitation	Gal				\$119							\$119								\$119			\$119	
03-2207	Sacramento	Sacramento Transportation Authority	3	LSR	Sacramento County Street Rehabilitation in Arden-Arcade, Carmichael, Rio Linda - Phase 4	Sacramento County				\$1,137							\$1,137								\$1,137			\$1,137	
03-2208	Sacramento	Sacramento Transportation Authority	3		Sacramento County Street Rehabilitation in Fair Oaks, Orangevale - Phase 5	Sacramento County				\$2,150							\$2,150								\$2,150			\$2,150	
03-2209	Sacramento	Sacramento Transportation Authority	3		Rancho Cordova Citywide Street Rehabilitation (local streets near Folsom Boulevard, Coloma Road, Cordova Lane and Zinfandel Drive)	Rancho Cordova				\$366							\$366								\$366			\$366	
03-2216	Sacramento	Sacramento Transportation Authority	2		Iron Point Road Median Improvement	Folsom				\$123							\$123							\$123			\$123		
03-1797	Sacramento	Sacramento Transportation Authority	2		Zero Emission Bus Purchase for Airport Service	Sacramento Regional Transit District			\$991								\$991							\$991			\$991		
03-1795	Sacramento	Sacramento Transportation Authority	2		Folsom Boulevard Roadway Rehabilitation	Sacramento				\$722							\$722								\$722			\$722	
03-1792	Sacramento	Sacramento Transportation Authority	2		2020 Pavement Resurfacing	Elk Grove				\$254							\$254								\$254			\$254	
03-1791	Sacramento	Sacramento Transportation Authority	2		ADA Accessibility and Drainage Improvements	Citrus Heights				\$123							\$123								\$123			\$123	
03-1796	Sacramento	Sacramento Transportation Authority	2	LSR	Complete Streets Rehabilitation - Residential Streets: Arden Arcade Area	Sacramento County				\$973							\$973								\$973			\$973	
03-1779	Sacramento	Sacramento Transportation Authority	1		Sunrise Boulevard Roadway Rehabilitation	Rancho Cordova			\$289								\$289							\$289			\$289		
03-1779	Sacramento	Sacramento Transportation Authority	2		Sunrise Boulevard Roadway Rehabilitation	Rancho Cordova				\$118							\$118								\$118			\$118	
03-1786	Sacramento	Sacramento Transportation Authority	1		Complete Streets Rehabilitation	Sacramento County		\$268												\$268				\$268			\$268		
03-1786	Sacramento	Sacramento Transportation Authority	1		Complete Streets Rehabilitation	Sacramento County			\$2,106								\$2,106							\$2,106			\$2,106		
03-1774	Sacramento	Sacramento Transportation Authority	1		Circulator Bus Service Expansion	Sacramento Regional Transit District			\$1,287								\$1,287							\$1,287			\$1,287		
03-1775	Sacramento	Sacramento Transportation Authority	1		Roadway Rehabilitation, Street Light & Street Sign Replacement, Intersecting State Routes to School	Citrus Heights			\$299								\$299							\$299			\$299		
03-1776	Sacramento	Sacramento Transportation Authority	1		Upgraded Curb Ramps Pavement Sealing	Elk Grove		\$323									\$323								\$323			\$323	
03-1777	Sacramento	Sacramento Transportation Authority	1		Pavement Sealing	Elk Grove		\$291									\$291								\$291			\$291	
03-1668	Sacramento	Sacramento Transportation Authority	1	RIP	Green Valley Road Widening	Folsom			\$300								\$300								\$300			\$300	
03-1780	Sacramento	Sacramento Transportation Authority	1		Roadway Rehabilitation	Sacramento			\$1,748								\$1,748								\$1,748			\$1,748	
Sacramento Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$3,476	\$3,435	\$3,304	\$2,930	\$3,074	\$2,930	\$3,381	\$3,379	\$3,179	\$3,155						\$6,911	\$3,304	\$9,934	\$5,648	\$0	\$24,797	\$7,440
Funds Left to Program																						\$0	\$0	\$0	\$1,112	\$6,334			
08-1341	San Bernardino	San Bernardino County Transportation Authority (SBCTA)	4		National Trails Highway Bridge Replacements - Dagget - Yermo Road to Goffs Road	San Bernardino County								\$2,625					\$ 2,625							\$2,625			\$2,625
08-1341	San Bernardino	San Bernardino County Transportation Authority (SBCTA)	4		National Trails Highway Bridge Replacements - Dagget - Yermo Road to Goffs Road	San Bernardino County									\$2,625				\$ 2,625							\$2,625			\$2,625
08-1342	San Bernardino	San Bernardino County Transportation Authority (SBCTA)	4		National Trails Highway Bridge Replacements, Ambony to Kelbaker	San Bernardino County								\$263					\$ 263							\$263			\$263
08-1342	San Bernardino	San Bernardino County Transportation Authority (SBCTA)	4		National Trails Highway Bridge Replacements, Ambony to Kelbaker	San Bernardino County									\$263				\$ 263							\$263			\$263
08-0167M	San Bernardino	San Bernardino County Transportation Authority (SBCTA)	3	RIP/STIP, TCEP	Interstate 15 Corridor Freight Improvement: Auxiliary Lanes and Express Lanes	SBCTA						\$11,554					\$11,554								\$11,554			\$11,554	
08-1244	San Bernardino	San Bernardino County Transportation Authority	2, 3	LPP-C	Ranchero Road Widening	Hesperia				\$12,195							\$12,195							\$6,339	\$5,856		\$12,195		
08-1244	San Bernardino	San Bernardino County Transportation Authority	3	LPP-C	Ranchero Road Widening	Hesperia					\$483						\$483								\$483			\$483	
08-3009P	San Bernardino	San Bernardino County Transportation Authority	1	TCF, TCEP	+10 Corridor Contract 1 (Express Lanes - DB 2b)	SBCTA				\$6,169							\$6,169							\$6,169			\$6,169		
08-1230	San Bernardino	San Bernardino County Transportation Authority	1	LPP-C, SCCP, STIP	Redlands Passenger Rail (SB Transit Center - Redlands University)	SBCTA				\$6,169							\$6,169							\$6,169			\$6,169		
08-1325	San Bernardino	San Bernardino County Transportation Authority	4, 5	TCEP	Interstate 10 Corridor Freight and Managed Lane Project: I-15 to Sierra Avenue, Contract 2A	SBCTA									\$12,731		\$12,731								\$12,731			\$12,731	
San Bernardino County Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$6,169	\$6,169	\$6,339	\$5,856	\$6,181	\$5,856	\$6,822	\$6,818	\$6,146	\$6,104						\$12,338	\$6,339	\$17,893	\$13,640	\$4,867	\$55,077	\$7,383
Funds Left to Program																						\$0	\$0	\$0	\$1,112	\$6,334			
11-1419	San Diego	San Diego Regional Transportation Commission	4		State Route 67 Highway Improvements	Caltrans								\$4,043					\$4,043					\$656	\$119	\$3,266		\$4,043	
11-1500	San Diego	San Diego Regional Transportation Commission	4		Interstate 805 and State Route 16 Transit Priority Connectors	Caltrans								\$7,200					\$6,450			\$750				\$7,200			\$7,200
11-1328	San Diego	San Diego Regional Transportation Commission	3	ATP	Inland Rail Trail City of Vista Segment Phase 3	SANDAG								\$5,451				\$5,451								\$5,451			\$5,451
11-0356	San Diego	San Diego Regional Transportation Commission	3		State Route 94 / State Route 125 South to East Connector (in and near the Cities of La Mesa and Lemon Grove)	Caltrans					\$853										\$0	\$853				\$853			\$853
11-1487	San Diego	San Diego Regional Transportation Commission	2		COASTER Expansion	North County Transit District					\$3,067							\$3,067							\$3,067			\$3,067	
11-1487	San Diego	San Diego Regional Transportation Commission	3		COASTER Expansion	North County Transit District						\$18,133						\$18,133							\$18,133			\$18,133	
11-1354	San Diego	San Diego County Regional Transportation Commission	1		+5/Vogel Drive Street Improvements	SANDAG				\$4,130							\$4,130								\$4,130			\$4,130	
11-1354	San Diego	San Diego County Regional Transportation Commission	2		+5/Vogel Drive Street Improvements	SANDAG					\$12,000						\$12,000							\$5,340	\$6,660		\$12,000		
11-LP001	San Diego	San Diego County Regional Transportation Commission	1		LOSSAN SD Subdivision Doubletrack (CP Eastbrook - CP Shell)	SANDAG				\$1,441								\$1,441						\$1,441			\$1,441		
11-LP002	San Diego	San Diego County Regional Transportation Commission	1		LOSSAN Batiquitos Lagoon Doubletrack/Bridge (MP234.5-MP235.5)	SANDAG				\$1,250								\$1,250							\$1,250			\$1,250	
11-LP003	San Diego	San Diego County Regional Transportation Commission	1		LOSSAN San Diego Lagoon Doubletrack/Bridge/Platform (242.2-243.9)	SANDAG				\$3,500								\$3,500							\$3,500			\$3,500	
11-LP003	San Diego	San Diego County Regional Transportation Commission	1		LOSSAN San Diego Lagoon Doubletrack/Bridge/Platform (242.2-243.9)	SANDAG					\$81							\$81							\$81			\$81	
75-T0014	San Diego	San Diego County Regional Transportation Commission	1		LOSSAN SD Subdivision Sorrento to Miramar Ph2 (MP251.2-MP253)	SANDAG				\$1,720								\$1,720							\$1,720			\$1,720	
11-LP005	San Diego	San Diego County Regional Transportation Commission	1		LOSSAN SD Subdivision Signal Respacing and Optimization	North County Transit District				\$901								\$901							\$901			\$901	
75-1606	San Diego	San Diego County Regional Transportation Commission	4		Build North Coast Corridor (NCC) Balguiters - Construction Support and Additional Oversight	SANDAG									\$10,210		\$10,210								\$10,210			\$10,210	
San Diego Regional Transportation Commission							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$9,470	\$9,470	\$9,727	\$9,093	\$9,096	\$9,093	\$10,341	\$10,335	\$9,415	\$9,351						\$16,940	\$9,727	\$24,637	\$20,676	\$0	\$73,980	\$21,913
Funds Left to Program																						\$0	\$0	\$0	\$18,766				
04-2351B	San Francisco	San Francisco County Transportation Authority	4		Hillcrest Road Improvement	SFCTA								\$2,600				\$2,600								\$2,600			\$2,600
04-2351A	San Francisco	San Francisco County Transportation Authority	3		Yerba Buena Island West Side Bridges (co-nominated with BATA)	San Francisco County Transportation Authority (SFCTA)								\$4,056				\$4,056								\$4,056			\$4,056

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04-2351	San Francisco	San Francisco County Transportation Authority	4		Yerba Buena Island Multi-Use Pathway	SFCTA							\$1								\$1					\$1		\$1		
04-2351	San Francisco	San Francisco County Transportation Authority	3		Yerba Buena Island Multi-Use Pathway	SFCTA					\$1,000									\$1,000					\$1,000			\$1,000		
04-0581L	San Francisco	San Francisco County Transportation Authority	3		Interstate 280 Southbound Ocean Avenue Off-Ramp Realignment	SFCTA					\$1,050									\$1,050					\$1,050			\$1,050		
04-2319E	San Francisco	San Francisco County Transportation Authority	2		Sunset and Parkside Streets Pavement Renovation	SFPW			\$236											\$236					\$236			\$236		
04-2319E	San Francisco	San Francisco County Transportation Authority	2		Sunset and Parkside Streets Pavement Renovation	SFPW			\$2,104								\$2,007			\$97		\$97		\$2,007				\$2,104		
04-2319D	San Francisco	San Francisco County Transportation Authority	1		Memroy Boulevard Pavement Renovation	SFPW			\$1,750								\$1,750							\$1,750				\$1,750		
04-2319C	San Francisco	San Francisco County Transportation Authority	1		Parkmeadow/Twin Peaks/Glen Park Residential Pavement Renovation	SFPW	\$2,106										\$2,106							\$2,106				\$2,106		
San Francisco County Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$2,106	\$2,083	\$2,007	\$2,005	\$2,096	\$2,005	\$6,738	\$1,737	\$1,647	\$1,636						\$4,189	\$2,007	\$6,196	\$2,601	\$0	\$14,983		
Funds Left to Program																						\$0	\$0	\$0	\$5,874	\$3,283		\$9,157		
10-3045	San Joaquin	San Joaquin County Transportation Authority	2		Turner Road Interchange Operational Improvements	Lodi			\$1,629									\$1,629	-						\$1,629				\$1,629	
10-3045	San Joaquin	San Joaquin County Transportation Authority	3		Turner Road Interchange Operational Improvements	Lodi					\$3,100							\$3,100							\$3,100				\$3,100	
10-3162A	San Joaquin	San Joaquin County Transportation Authority	3	STIP, SHOPP	State Routes 99 and 120 Connector	Caltrans						\$1,349						\$1,349							\$1,349				\$1,349	
10-3162A	San Joaquin	San Joaquin County Transportation Authority	1	STIP, SHOPP	State Routes 99 and 120 Connector	Caltrans		\$3,408													\$2,558	\$850	\$3,408					\$3,408		
San Joaquin County Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$1,714	\$1,694	\$1,629	\$1,459	\$1,531	\$1,459	\$995	\$995	\$916	\$909						\$3,408	\$1,629	\$4,449	\$0	\$0	\$9,486		
Funds Left to Program																						\$0	\$0	\$0	\$1,999	\$1,825		\$3,811		
04-2091B	San Mateo	San Mateo County Transit District	4	TRCP	Replace 38 Diesel Buses with Zero Emission Buses	SanTrans							\$1,515					\$1,515								\$1,515			\$1,515	
04-2141A	San Mateo	San Mateo County Transit District	3		SanTrans Purchase Thirty Buses	SanTrans					\$59							\$59							\$59				\$59	
04-2141A	San Mateo	San Mateo County Transit District	3		SanTrans Purchase Thirty Buses	SanTrans					\$1,168	\$1,168						\$2,336							\$2,336				\$2,336	
04-2141A	San Mateo	San Mateo County Transit District	2, 3		SanTrans Purchase Thirty Buses	SanTrans					\$6,765							\$6,765							\$5,597	\$1,168			\$6,765	
04-2330E	San Mateo	San Mateo County Transit District	1, 2	TRCP	SanTrans Express Bus Pilot	SanTrans			\$2,000									\$2,000						\$1,757	\$243				\$2,000	
04-2059K	San Mateo	San Mateo County Transit District	4		Hydrogen Fuel Cell Electric Bus Infrastructure	SanTrans								\$1,514						\$1,514						\$1,514			\$1,514	
San Mateo County Transit District							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$884	\$873	\$5,840	\$1,168	\$1,227	\$1,168	\$1,515	\$1,514	\$1,413	\$1,403						\$1,757	\$5,846	\$3,563	\$3,029	\$0	\$14,189		
Funds Left to Program																						\$0	\$0	\$0	\$0	\$2,816		\$2,816		
04-0090L	San Mateo	San Mateo County Transportation Authority	3		University Avenue-Highway 101 Interchange Improvements	East Palo Alto						\$2,302						\$2,302								\$2,302			\$2,302	
04-2353A	San Mateo	San Mateo County Transportation Authority	2		Hopkins Avenue Traffic Safety Implementation	Redwood City				\$840								\$840							\$840				\$840	
04-0658D	San Mateo	San Mateo County Transportation Authority	1		US 101 Managed Lanes (co-nominated with CIGAG of San Mateo County)	SBCTA			\$1,757												\$1,757				\$1,757				\$1,757	
San Mateo County Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$884	\$873	\$840	\$754	\$794	\$754	\$758	\$758	\$787	\$702						\$1,757	\$840	\$2,302	\$0	\$0	\$4,899		
Funds Left to Program																						\$0	\$0	\$0	\$1,516	\$1,409		\$2,925		
05-7103Y	Santa Barbara	Santa Barbara County Local Transportation Authority	1	RFP	South Coast 101 HOV-Summerland (Segment 4C) Highway Planning	Caltrans						\$531						\$531							\$531				\$531	
05-7101G	Santa Barbara	Santa Barbara County Local Transportation Authority	4	SCCP	Santa Barbara U.S. 101 Multimodal Corridor - 4E South-City of Santa Barbara	Caltrans							\$158					\$158							\$158				\$158	
05-7101G	Santa Barbara	Santa Barbara County Local Transportation Authority	4	SCCP	Santa Barbara U.S. 101 Multimodal Corridor - 4E South-City of Santa Barbara	Caltrans							\$2,584					\$2,584								\$2,584			\$2,584	
05-7101B	Santa Barbara	Santa Barbara County Local Transportation Authority	3	STIP, SHOPP	US 101 Multimodal Corridor - Montecito to Santa Barbara - Segment 4D4E	SBCAG					\$3,681										\$3,681				\$3,681				\$3,681	
05-7101B	Santa Barbara	Santa Barbara County Local Transportation Authority	3	STIP, SHOPP	US 101 Multimodal Corridor - Montecito to Santa Barbara - Segment 4D4E	SBCAG					\$68										\$68				\$68				\$68	
05-1834B	Santa Barbara	Santa Barbara County Local Transportation Authority	2		Cabrillo Boulevard Pedestrian Improvements	Santa Barbara			\$822												\$450	\$372		\$822				\$822		
05-2897	Santa Barbara	Santa Barbara County Local Transportation Authority	2	SCCP-Con	Santa Claus Lane Streetscape, Coastal Access Parking and Railroad Crossing	SBCAG			\$500												\$400	\$100		\$500				\$500		
05-2985	Santa Barbara	Santa Barbara County Local Transportation Authority	1	SCCP-Con	RL 101, Santa Monica Road/Via Real Intersection Improvements	Caltrans			\$375												\$375				\$375				\$375	
05-2985	Santa Barbara	Santa Barbara County Local Transportation Authority	1	SCCP-Con	RL 101, Santa Monica Road/Via Real Intersection Improvements	Caltrans			\$298												\$274	\$24		\$298					\$298	
05-2986	Santa Barbara	Santa Barbara County Local Transportation Authority	1	SCCP-Con	Santa Claus Lane Class I Bikeway, California Coastal Trail Gap Closure	Carpenters			\$410												\$250	\$160		\$410					\$410	
05-2993	Santa Barbara	Santa Barbara County Local Transportation Authority	1	SCCP-Con	North Paddock Lane Coastal Access Improvements	Santa Barbara County			\$30												\$30				\$30				\$30	
05-2993	Santa Barbara	Santa Barbara County Local Transportation Authority	1	SCCP-Con	North Paddock Lane Coastal Access Improvements	Santa Barbara County			\$168												\$88	\$70		\$168					\$168	
05-2994	Santa Barbara	Santa Barbara County Local Transportation Authority	1	SCCP-Con	Summerland Area Coastal Access Improvements	Santa Barbara County			\$150												\$150				\$150				\$150	
05-2994	Santa Barbara	Santa Barbara County Local Transportation Authority	1	SCCP-Con	Summerland Area Coastal Access Improvements	Santa Barbara County			\$454												\$340	\$114		\$454					\$454	
Santa Barbara County Local Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$1,287	\$1,287	\$1,322	\$1,227	\$1,295	\$1,227	\$1,393	\$1,393	\$1,269	\$1,251						\$2,574	\$1,322	\$3,749	\$2,984	\$0	\$10,229		
Funds Left to Program																						\$0	\$0	\$0	\$0	\$202	\$2,511		\$2,713	
04-9039P	Santa Clara	Santa Clara Valley Transportation Authority (SCVTA)	2		Montage Expressway Pedestrian Overcrossing (Mjpas BART Station)	SCVTA			\$4,497									\$4,497							\$4,497				\$4,497	
04-2174E	Santa Clara	Santa Clara Valley Transportation Authority	1	TRCP	Eastridge to BART Regional Connector	SCVTA			\$9,442													\$9,442			\$9,442				\$9,442	
04-2174E	Santa Clara	Santa Clara Valley Transportation Authority	3	TRCP	Eastridge to BART Regional Connector	SCVTA						\$15,710						\$15,710								\$15,710				\$15,710
Santa Clara Valley Transportation Authority							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$4,747	\$4,695	\$4,497	\$5,145	\$5,420	\$5,145	\$6,709	\$5,452	\$5,104	\$5,065						\$9,442	\$4,497	\$15,710	\$0	\$0	\$29,649		
Funds Left to Program																						\$0	\$0	\$0	\$0	\$12,161	\$16,169		\$22,330	
05-3108	Santa Cruz	Santa Cruz Metropolitan Transit District (SC Metro)	3		Santa Cruz Metro Revenue Vehicle Replacement (co-nominated with SCORTC)	SC Metro						\$903						\$903								\$903			\$903	
05-3037	Santa Cruz	Santa Cruz Metropolitan Transit District	2		SC Metro Paratransit Van Replacement	SC Metro					\$302														\$302				\$302	
05-2820	Santa Cruz	Santa Cruz Metropolitan Transit District	1		Vehicle Replacement (co-nominated with SCORTC)	SC Metro			\$631																\$631				\$631	
Santa Cruz County Metropolitan Transit District							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$317	\$314	\$302	\$296	\$311	\$296	\$ 327	\$ 327	\$ 300	\$ 297							\$631	\$302	\$903	\$0	\$0	\$1,836	
Funds Left to Program																						\$0	\$0	\$0	\$0	\$654	\$597		\$1,251	
05-3108	Santa Cruz	Santa Cruz County Regional Transportation Commission (SCORTC)	3		Santa Cruz Metro Revenue Vehicle Replacement (co-nominated with SC Metro)	SC Metro						\$15						\$15								\$15			\$15	
05-0073D	Santa Cruz	Santa Cruz County Regional Transportation Commission	2		State Route 1-Freedom to State Park Auxiliary Lanes and Bus On Shoulders	SCORTC					\$302										\$302				\$302				\$302	
05-0073D	Santa Cruz	Santa Cruz County Regional Transportation Commission	3		State Route 1-Freedom to State Park Auxiliary Lanes and Bus On Shoulders	SCORTC					\$888										\$888				\$888				\$888	
05-2832	Santa Cruz	Santa Cruz County Regional Transportation Commission	1		2018 Full Depth Recycle & Overlay	Santa Cruz Co.			\$476									\$476							\$476				\$476	
05-2829	Santa Cruz	Santa Cruz County Regional Transportation Commission	1		Vehicle Replacement (co-nominated with SC Metro)	SC Metro			\$155									\$155							\$155				\$155	
Santa Cruz County Regional Transportation Commission							2018, 2019, 2020, 2022, 2024 Fund Distribution	\$317	\$314	\$302	\$ 296	\$ 311	\$ 296	\$ 327	\$ 327	\$ 300	\$ 297							\$631	\$302	\$903	\$0	\$0	\$1,836	
Funds Left to Program																						\$0	\$0	\$0	\$0	\$654	\$597		\$1,251	
04-0785C	Sonoma	Sonoma County Transportation Authority	3	STIP, LPP-C	US 101 and Hearn Avenue Regional Multimodal Interchange	Caltrans						\$5,000						\$5,000								\$5,000			\$5,000	
04-0209P	Sonoma	Sonoma County Transportation Authority	3		City of Cotati's Redwood Drive and Cypress Avenue Pavement Rehabilitation	Cotati						\$946						\$946								\$946			\$946	
04-0090F	Sonoma	Sonoma County Transportation Authority	3		Santa Rosa Purchase Zero-Emission Bus	Santa Rosa						\$551						\$551								\$551			\$551	
04-2318E	Sonoma	Sonoma County Transportation Authority	2		2020 Pedestrian and Surfacing Improvements on Old Redwood Highway, Chico Avenue and Price Avenue	Sonoma County					\$551																			

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04-2318A	Sonoma	Sonoma County Transportation Authority	1		Santa Rosa OBAG2 Bicycle and Pedestrian Improvements	Santa Rosa	\$100													\$100			\$100					\$100		
04-2318A	Sonoma	Sonoma County Transportation Authority	1		Santa Rosa OBAG2 Bicycle and Pedestrian Improvements	Santa Rosa		\$473										\$473										\$473		
04-0300Q	Sonoma	Sonoma County Transportation Authority	1	SCCP	Route 101 Marin/Sonoma Narrows - Segment C-2	Caltrans		\$579												\$579								\$579		
	Sonoma	Sonoma County Transportation Authority	4		Cañfield Lane Improvements	Posidema									\$1,068		\$1,068									\$1,068		\$1,068		
		Sonoma County Transportation Authority			2018, 2019, 2020, 2022, 2024 Fund Distribution		\$579	\$573	\$551	\$5,491	\$515	\$491	\$534	\$534	\$495	\$491							\$1,152	\$551	\$6,497	\$1,068	\$0	\$9,268		
																							\$0	\$0	\$0	\$0	\$996		\$996	
04-2090D	Marin	Sonoma Marin Area Rail Transit District (SMART)	3, 4		SMART Pathway Great Redwood Trail - San Rafael Segment	SMART						\$1,392	\$460				\$1,858								\$1,392	\$460		\$1,858		
04-2318J	Sonoma/Marin	Sonoma Marin Area Rail Transit District	3		SMART Rail Operations Equipment Expansion Phase 3	SMART					\$679						\$679								\$679			\$679		
04-2318C	Sonoma/Marin	Sonoma Marin Area Rail Transit District	2		SMART Rail Maintenance Equipment Expansion Phase 2	SMART		\$743									\$743							\$743				\$743		
04-2318B	Sonoma/Marin	Sonoma Marin Area Rail Transit District	1		SMART Rail Maintenance Equipment Expansion	SMART		\$1,553									\$1,553							\$1,553				\$1,553		
		Sonoma Marin Area Rail Transit District			2018, 2019, 2020, 2022, 2024 Fund Distribution		\$781	\$772	\$743	\$679	\$713	\$679	\$742	\$742	\$686	\$680							\$1,553	\$743	\$2,971	\$466	\$0	\$4,833		
																							\$0	\$0	\$0	\$0	\$1,356		\$2,384	
10-0944A	Stanislaus	Stanislaus County Transportation Authority	3	RIP-STIP, STIP-CRISSA	State Route 132 Phase 2: Four-lane Expressway from Dakota Avenue to Needham Street	StanCOG						\$3,217									\$3,217				\$3,217			\$3,217		
10-0944A	Stanislaus	Stanislaus County Transportation Authority	2	RIP-STIP, STIP-CRISSA	State Route 132 Phase 2: Four-lane Expressway from Dakota Avenue to Needham Street	StanCOG				\$1,196									\$1,196					\$1,196				\$1,196		
10-3414	Stanislaus	Stanislaus County Transportation Authority	1	LPP-C	Route 99/Fulworth Road Interchange Improvements	Turbo		\$2,501									\$2,501							\$2,501				\$2,501		
		Stanislaus County Transportation Authority			2018, 2019, 2020, 2022, 2024 Fund Distribution		\$1,250	\$1,243	\$1,196	\$1,055	\$1,197	\$1,055	\$1,186	\$1,185	\$1,102	\$1,094							\$2,501	\$1,196	\$3,217	\$0	\$0	\$6,914		
																							\$0	\$0	\$0	\$2,371	\$2,196		\$4,567	
05-2972	Monterey	Transportation Agency for Monterey County (TAMC)	4	LPP-C, RIP	Marina-Salinas Multimodal Corridor (Sign Traffic Flow & Safety)	Marina							\$1,200				\$1,200									\$1,200		\$1,200		
05-1790	Monterey	Transportation Agency for Monterey County	3	RIP-STIP	Scenic State Route 68 Corridor Safety and Traffic Flow	Caltrans						\$1,049							\$1,049						\$1,049			\$1,049		
05-2899	Monterey	Transportation Agency for Monterey County	2		Regional Wayfinding Program	TAMC			\$724																\$724			\$724		
05-2898	Monterey	Transportation Agency for Monterey County	1		Regional Wayfinding Program	TAMC		\$163																	\$163			\$163		
05-2936	Monterey	Transportation Agency for Monterey County	1	ATP	Fort Ord Regional Trail and Greenway/Highway 218 Segment	TAMC		\$800											\$800						\$800			\$800		
05-2820	Monterey	Transportation Agency for Monterey County	1		Route 156 Safety Improvements-Blackie Road Extension	TAMC		\$250											\$250						\$250			\$250		
05-2931	Monterey	Transportation Agency for Monterey County	1		Fort Ord Regional Trail and Greenway	TAMC		\$500											\$500						\$500			\$500		
		Transportation Agency for Monterey County			2018, 2019, 2020, 2022, 2024 Fund Distribution		\$761	\$752	\$724	\$639	\$671	\$639	\$704	\$704	\$665	\$660							\$1,613	\$724	\$1,949	\$1,250	\$0	\$5,436		
																							\$0	\$0	\$0	\$150	\$1,325		\$1,483	
04-2092H	Marin	Transportation Authority of Marin County	4		Belam Boulevard Off-Ramp Improvement Project	Transportation Authority of Marin							\$1,204				\$1,204									\$1,204			\$1,204	
04-2090E	Marin	Transportation Authority of Marin County	3		Old Redwood Highway Multi-Use Path	Larkspur						\$1,100					\$1,100								\$1,100			\$1,100		
04-2090B	Marin	Transportation Authority of Marin County	3		Grand Avenue Cycle Track	San Rafael						\$696					\$696								\$696			\$696		
04-0361M	Marin	Transportation Authority of Marin County	2	SCCP, STIP-CRISSA	Marin Sonoma Narrows - Corridor B7	Caltrans			\$3,500												\$3,500			\$3,500				\$3,500		
04-0341E	Marin	Transportation Authority of Marin County	2		North South Greenway Gap Closure - Northern Segment	Caltrans			\$1,500								\$1,500								\$1,500			\$1,500		
04-2128G	Marin	Transportation Authority of Marin County	2		Downtown SMART Station Phase 2	Novato			\$483								\$483								\$483			\$483		
04-0361G	Marin	Transportation Authority of Marin County	1	SCCP	Marin-Sonoma Narrows (Design Contract A4)	Transportation Authority of Marin		\$250											\$250						\$250			\$250		
04-0360W	Marin	Transportation Authority of Marin County	1	SCCP	Marin-Sonoma Narrows (Design Contract B1)	Transportation Authority of Marin		\$250											\$250						\$250			\$250		
04-2326A	Marin	Transportation Authority of Marin County	1		Francisco Blvd West Multi-Use Pathway (2nd Street to Anderson Drive)	San Rafael	\$502										\$502								\$502			\$502		
		Transportation Authority Marin County			2018, 2019, 2020, 2022, 2024 Fund Distribution		\$503	\$499	\$5,483	\$579	\$598	\$579	\$618	\$618	\$583	\$580							\$1,002	\$5,483	\$1,756	\$1,204	\$0	\$9,445		
																							\$0	\$0	\$0	\$32	\$1,163		\$1,195	
03-3220	Nevada	Truckee	4		Truckee Railyard Transit Center - Phase 2	Truckee							\$400						\$400							\$400			\$400	
03-3219	Nevada	Truckee	3		Truckee Railyard Transit Center - Phase 1	Truckee					\$0								\$0							\$0			\$0	
03-3218	Nevada	Truckee	3		Truckee Railyard Transit Center - Phase 1	Truckee						\$400					\$400								\$400			\$400		
03-1428	Nevada	Truckee	3		Truckee Annual Paving and Drainage Rehabilitation	Truckee			\$200								\$200								\$200			\$200		
03-1226	Nevada	Truckee	2		2020 Paving and Drainage: Bridge Street, Jibboom Street, and Church Street	Truckee			\$100								\$100								\$100			\$100		
03-1418	Nevada	Truckee	1		2018 Annual Slurry Seal Program	Truckee		\$200									\$200								\$200			\$200		
03-5487	Nevada	Truckee	5		Remaghe Bridge Street	Truckee									\$400	\$400										\$400	\$400	\$1,700		
		Truckee			2018, 2019, 2020, 2022, 2024 Fund Distribution		\$100	\$100	\$100	\$200	\$200	\$200	\$3,200	\$200	\$200	\$200							\$200	\$100	\$600	\$400	\$400	\$1,700	\$3,000	
																							\$0	\$0	\$0	\$3,000	\$0		\$3,000	
06-6421	Tulare	Tulare County Transportation Authority	4	LPP-C, STIP	State Route 99 and Caldwell Avenue Interchange	Caltrans						\$2,818					\$2,818									\$2,818			\$2,818	
06-7068	Tulare	Tulare County Transportation Authority	3		Avenue 280 Widening (Santa Fe to Lovers Lane) in Visalia	Visalia						\$70					\$70								\$70			\$70		
06-7068	Tulare	Tulare County Transportation Authority	3		Avenue 280 Widening (Santa Fe to Lovers Lane) in Visalia	Visalia						\$3,774					\$3,774								\$3,774			\$3,774		
06-7068	Tulare	Tulare County Transportation Authority	2		Avenue 280 Widening (Santa Fe to Lovers Lane) in Visalia	Visalia			\$1,387												\$1,387				\$1,387			\$1,387		
06-6891	Tulare	Tulare County Transportation Authority	1		Route 198 and Akers Street Interchange Improvement	Visalia	\$259	\$2,436									\$2,436				\$259			\$2,694				\$2,694		
		Tulare County Transportation Authority			2018, 2019, 2020, 2022, 2024 Fund Distribution		\$1,347	\$1,347	\$1,387	\$1,258	\$1,328	\$1,258	\$1,462	\$1,461	\$1,337	\$1,328							\$2,694	\$1,387	\$3,844	\$2,818	\$0	\$10,743		
																							\$0	\$0	\$0	\$105	\$2,685		\$2,770	
01-4791	Mendocino	Willits	3		South State Street and Dora Street Rehabilitation	Willits				\$800							\$800								\$800			\$800		
		Willits			2018, 2019, 2020, 2022, 2024 Fund Distribution		\$0	\$0	\$0	\$200	\$200	\$200	\$200	\$200	\$200	\$200							\$0	\$0	\$600	\$0	\$0	\$600		
																							\$0	\$0	\$0	\$400	\$400		\$800	
01-3225	Mendocino	Willits	3		2024 Asphalt Maintenance	Willits					\$400						\$400								\$400			\$400		
01-4797	Mendocino	Willits	2		2022 Willits Pavement Preservation	Willits					\$200						\$200								\$200			\$200		
01-4723	Mendocino	Willits	1		2019 Asphalt Maintenance Program	Willits			\$100								\$100							\$100			\$100			
01-4722	Mendocino	Willits	1		2018 Asphalt Maintenance Program	Willits			\$100								\$100							\$100			\$100			
01-4712	Mendocino	Willits	1		Asphalt Maintenance Program FY 17-18	Willits			\$100								\$100							\$100			\$100			
		Willits			2018, 2019, 2020, 2022, 2024 Fund Distribution		\$100	\$100	\$100	\$200	\$200	\$200	\$200	\$200	\$200	\$200							\$200	\$100	\$600	\$0	\$0	\$900		
																							\$0	\$0	\$0	\$400	\$400		\$800	
03-5917	Yuba	Yuba County	4		Pendola Extension - Emergency Route Road Rehabilitation	Yuba County							\$400				\$400									\$400			\$400	
03-2018A	Yuba	Yuba County	3	ATP	McGowan Parkway Bicycle Lane and Pedestrian Route Improvements	Yuba County					\$600						\$600								\$600			\$600		
03-2020	Yuba	Yuba County	1, 2		Urie Road Rehabilitation	Yuba County			\$300								\$300		</											

Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: June 26-27, 2025

From: TANISHA TAYLOR, Executive Director

Reference Number: 4.16, Action

Prepared By: Kayla Giese
Assistant Deputy Director

Published Date: June 13, 2025

Subject: 2024 Local Partnership Formulaic Program Fund Distribution Amendment,
Resolution LPP-P-2425-10, Amending Resolution G-24-65

Recommendation:

Staff recommends the California Transportation Commission (Commission) approve an amendment to the 2024 Local Partnership Formulaic Fund Distribution to add incentive funding as follows:

- Add \$392,000 of incentive funding for the City of Albany in Fiscal Year 2025-26.
- Add \$5,000,000 of incentive funding for Napa Valley Transportation Authority in Fiscal Year 2025-26.

Issue:

On November 5, 2024, voters in the City of Albany passed a renewal of the Safe and Accessible Sidewalks and Pathways Special Parcel Tax, a special tax imposed on every parcel of real property within the City of Albany to fund public sidewalk maintenance and obstruction removal projects. The Safe and Accessible Sidewalks and Pathways Special Parcel Tax renewal will go into effect on July 1, 2025, and makes the City of Albany eligible for the Local Partnership Formulaic Program and Incentive Funding for a renewed eligible measure.

On November 5, 2024, voters in Napa County passed the Napa Valley Transportation Authority's Measure U, a continuance of the jurisdiction's existing one-half of one-percent sales tax, to finance capital outlay expenditures that include maintenance, reconstruction, or rehabilitation projects included in the Local Streets and Roads Maintenance Program, highway improvement projects included in their Regional Transportation Enhancement Program, and Transit Fare Subsidies. Measure U will go into effect on July 1, 2025, and makes the County eligible for Local Partnership Formulaic Program Incentive Funding for a renewed eligible measure.

Approval of this amendment to the 2024 Local Partnership Formulaic Program Funding Distribution will add \$392,000 of incentive funding for the City of Albany and \$5,000,000 for the Napa Valley Transportation Authority. The incentive funding is available for programming and allocation through June 30, 2028. The set-aside incentive funding amount of \$20,000,000 for Fiscal Year 2025-26 will be reduced by \$5,392,000, leaving a remaining unused incentive funding balance of \$14,608,000. Unused incentive funding will be redistributed at the August 2025 Commission meeting based on the program funding distribution of 60 percent to the formulaic program and 40 percent to the competitive program.

Background:

Senate Bill (SB) 1 (Chapter 5, Beall, Statutes of 2017) created the Local Partnership Program, and authorized \$200 million annually to fund the Program. This legislation requires the Commission, in consultation with the California Department of Transportation, regional transportation planning agencies, local agencies and other transportation stakeholders, to develop guidelines. Assembly Bill 115 (Chapter 20, Committee on Budget, Statutes of 2017) clarified SB 1 language regarding local and regional transportation agency eligibility and expanded the types of projects eligible for the program.

The Local Partnership Formulaic Program provides funding to counties, cities, districts, and regional transportation agencies in which voters have approved fees or taxes dedicated solely to transportation improvements. Only agencies with Commission-adopted funding and committed local matching funds are eligible to receive the Local Partnership Formulaic Program funding.

The Local Partnership Formulaic Program recognizes new or renewed voter-approved self-help efforts and incentivizes jurisdictions to pursue future voter-approved tax measures, tolls, or fees. Incentive funding is set-aside each fiscal year from the Local Partnership Program. One-time incentive funding is provided to jurisdictions that seek and receive voter approval of new or renewed tax measures, tolls, or fees, if those tax measures, tolls, or fees have a minimum period of ten years, are dedicated solely to transportation, and, for sales taxes, are equal to or greater than one quarter cent. The total amount of incentive funding awarded to jurisdictions is limited to \$20 million annually. Incentive funding amounts are a minimum of \$200,000 and may range up to a maximum of \$5 million for each jurisdiction, depending on the amount of revenue generated.

Resolution LPP-P-2425-03

Be it Resolved that the Commission does hereby approve the amendment to the 2024 Local Partnership Formulaic Fund Distribution to add incentive funding as follows:

1. Add \$392,000 of incentive funding for the City of Albany in Fiscal Year 2025-26.
2. Add \$5,000,000 of incentive funding for Napa Valley Transportation Authority in Fiscal Year 2025-26.

Attachment:

- Attachment A: Amended 2024 Local Partnership Formulaic Fund Distribution

2024 Local Partnership Program			2025-26	2026-27
Local Partnership Program Annual Appropriation Snapshot			\$ 200,000	\$ 200,000
Set-Aside Incentive Funding			\$ 20,000	\$ 20,000
Competitive Program (40%)			\$ 72,000	\$ 72,000
Formulaic Program (60%)			\$ 108,000	\$ 108,000
Competitive Program (40%)			\$ 72,000	\$ 72,000
2022 Competitive Program Rollover of Unprogrammed Funds			\$ 57,923	
2022 Competitive Overprogramming			\$ (6,253)	
Subtotal Available for Competitive Program			\$ 123,670	\$ 72,000
Formulaic Program (60%)			\$ 108,000	\$ 108,000
Redistribution of Lapsed Project Funds (2020 Program) (PPNO 4810, PPNO 1512, PPNO 8055)			\$ 716	
Redistribution of Lapsed Project Funds (2022 Program) (PPNO 1512)			\$ 29	
Subtotal Available for Formulaic Program			\$ 108,745	\$ 108,000
Proposed Annual Formulaic Funding Distribution				
Tolls + VRF + parcel/property tax	7.16%		\$ 7,784	\$ 7,731
North sales tax	26.18%		\$ 28,467	\$ 28,272
South sales tax	66.66%		\$ 72,494	\$ 71,997
Funding Based on Voter-Approved Tolls, Vehicle Registration Fee (VRF), Parcel/Property Tax Revenue			2025-26	2026-27
	Revenue (100%)		Funding	Funding
Alameda County Transportation Commission (Measure F-VRF) *	\$ 12,170		\$ 200	\$ 200
Alameda-Contra Costa Transit District (Measure C1-Parcel Tax)	\$ 29,883		\$ 323	\$ 321
Bay Area Rapid Transit District (Measure RR-Parcel Tax)	\$ 92,756		\$ 1,004	\$ 996
Bay Area Toll Authority (Regional Measure (RM) 1, RM 2, RM 3-Tolls)	\$ 447,768		\$ 4,847	\$ 4,807
City and County of San Francisco (Prop. D-Bus. Tax)	\$ 16,981		\$ 200	\$ 200
City/County Association of Governments of San Mateo County (Measure M-VRF)	\$ 7,845		\$ 200	\$ 200
Orinda (Measure L-Parcel Tax)	\$ 1,736		\$ 200	\$ 200
San Francisco County Transportation Authority (Prop AA-VRF) *	\$ 4,652		\$ 200	\$ 200
Santa Clara Valley Transportation Authority (Measure B-VRF) *	\$ 19,318		\$ 209	\$ 207
Transportation Authority Marin County (Measure B-VRF) *	\$ 2,320		\$ 200	\$ 200
Yuba County (Measure D-Depletion Surcharge)	\$ 981		\$ 200	\$ 200
Totals	\$ 636,409		\$ 7,784	\$ 7,731
Funding Based on Voter-Approved Sales Tax Revenue and Population - North			2025-26	2026-27
County (Agency)	Revenue (25%)	Population (75%)	Funding	Funding
Alameda (Alameda County Transportation Commission) *	\$ 195,330	1,641,869	\$ 3,333	\$ 3,308
Amador (City of Amador)	\$ 32	193	\$ 200	\$ 200
Contra Costa (Contra Costa Transportation Authority)	\$ 122,556	1,146,626	\$ 2,289	\$ 2,273
El Cerrito	\$ 2,167	N/A	\$ 200	\$ 200
Martinez	\$ 4,686	N/A	\$ 200	\$ 200
Fresno (Fresno County Transportation Authority)	\$ 110,090	1,017,431	\$ 2,035	\$ 2,020
Lake (Clearlake)	\$ 2,617	16,553	\$ 200	\$ 200
Madera (Madera County Transportation Authority)	\$ 16,155	159,328	\$ 321	\$ 319
Marin (Transportation Authority Marin County) * ^	\$ 34,759	252,844	\$ 383	\$ 380
Marin (Sonoma Marin Area Rail Transit District) ^	\$ 24,540	-	\$ 211	\$ 209
Mendocino (Fort Bragg)	\$ 1,195	7,179	\$ 200	\$ 200
Mendocino (Point Arena)	\$ 65	445	\$ 200	\$ 200
Mendocino (Ukiah)	\$ 3,368	16,108	\$ 200	\$ 200
Mendocino (Willits)	\$ 942	4,830	\$ 200	\$ 200
Merced (Merced County Transportation Authority)	\$ 26,951	287,303	\$ 563	\$ 559
Monterey (Transportation Agency for Monterey County) ^	\$ 38,556	437,614	\$ 665	\$ 660
Monterey (Monterey-Salinas Transit District) ^	\$ 12,602	-	\$ 236	\$ 235
Napa (Napa Valley Transportation Authority)	\$ 25,095	135,029	\$ 299	\$ 297
Nevada (Truckee)	\$ 5,708	16,778	\$ 200	\$ 200
Sacramento (Sacramento Transportation Authority)	\$ 178,274	1,578,938	\$ 3,179	\$ 3,155
San Benito (Council of San Benito County Governments)	\$ 13,227	65,853	\$ 200	\$ 200
San Francisco (San Francisco County Transportation Authority) * ^	\$ 108,678	843,071	\$ 1,447	\$ 1,436
San Francisco (Caltrain) * ^	\$ 39,901	-	\$ 397	\$ 394
San Joaquin (San Joaquin County Transportation Authority)	\$ 88,077	791,408	\$ 916	\$ 909
San Mateo (San Mateo County Transportation Authority) ^	\$ 116,088	741,565	\$ 707	\$ 702
San Mateo (San Mateo County Transit District) ^	\$ 231,713	-	\$ 1,413	\$ 1,403
San Mateo (Caltrain) * ^	\$ 39,901	-	\$ 207	\$ 205
Santa Clara (Santa Clara Valley Transportation Authority) * ^	\$ 671,714	1,903,198	\$ 4,895	\$ 4,858
Santa Clara (Caltrain) * ^	\$ 39,901	-	\$ 300	\$ 298
Santa Cruz (Santa Cruz County Regional Transportation Commission) ^	\$ 27,644	262,572	\$ 300	\$ 297
Santa Cruz (Santa Cruz Metropolitan Transit District) ^	\$ 27,700	-	\$ 300	\$ 297
Sonoma (Sonoma County Transportation Authority) ^	\$ 31,832	478,152	\$ 495	\$ 491
Sonoma (Sonoma Marin Area Rail Transit District) ^	\$ 24,540	-	\$ 475	\$ 471
Stanislaus (Stanislaus County Transportation Authority)	\$ 60,839	548,744	\$ 1,102	\$ 1,094
Totals	\$ 2,327,445	12,353,631	\$ 28,467	\$ 28,272
Funding Based on Voter-Approved Sales Tax Revenue and Population - South			2025-26	2026-27
County (Agency)	Revenue (25%)	Population (75%)	Funding	Funding
Imperial (Imperial County Local Transportation Authority)	\$ 23,982	182,881	\$ 524	\$ 520
Los Angeles (Los Angeles County Metropolitan Transportation Authority)	\$ 4,399,442	9,824,091	\$ 37,648	\$ 37,389
Orange (Orange County Transportation Authority)	\$ 430,476	3,150,835	\$ 9,081	\$ 9,019
Riverside (Riverside County Transportation Commission)	\$ 282,217	2,442,378	\$ 6,882	\$ 6,835
San Bernardino (San Bernardino County Transportation Authority)	\$ 251,862	2,181,433	\$ 6,146	\$ 6,104
San Diego (San Diego Regional Transportation Commission)	\$ 426,693	3,291,101	\$ 9,415	\$ 9,351
San Luis Obispo (El Paso de Robles) ^	\$ 6,700	30,907	\$ 200	\$ 200
Santa Barbara (Santa Barbara County Local Transportation Authority)	\$ 54,452	443,623	\$ 1,260	\$ 1,251
Tulare (Tulare County Transportation Authority)	\$ 51,171	478,918	\$ 1,337	\$ 1,328
Totals	\$ 5,926,995	22,026,167	\$ 72,494	\$ 71,997
Incentive Funding			2025-26	2026-27
Beginning Balance			\$ 20,000	\$ 20,000
City of Albany (Special Parcel Tax)			\$ 392	
Napa Valley Transportation Authority (Measure U)			\$ 5,000	
Remaining Balance			\$ 14,608	\$ 20,000

* Agency listed more than once.

^ Multiple agencies within county with countywide tax.

* Incentive funds are being awarded from the FY 24-25 incentive set-aside

^ Formula Funding Distribution for FY2025-26 and 2026-27 is pending eligible measure renewal

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 4.17, Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: ACTIVE TRANSPORTATION PROGRAM – PROJECT SCOPE
AMENDMENT – MITCHELL AVENUE SIDEPATH GAP CLOSURE PROJECT
RESOLUTION ATP-P-2425-10

ISSUE:

Should the California Transportation Commission (Commission) approve a project scope amendment for the Cycle 6 Active Transportation Program (ATP) Mitchell Avenue Sidepath Gap Closure project (PPNO 1310), in Riverside County?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve this project scope amendment for the Cycle 6 ATP Mitchell Avenue Sidepath Gap Closure project (PPNO 1310), in Riverside County.

DISCUSSION:

On June 28, 2023, the Commission approved the Cycle 6 2023 ATP – Metropolitan Planning Organizations Component for nine of ten large Metropolitan Planning Organizations, which included \$6,756,000 in funding for the Mitchell Avenue Sidepath Gap Closure project (PPNO 1310), in Riverside County. The project includes closing gaps between two existing segments of a Class I sidepath trail and constructing new sidewalk along the westerly side of Mitchell Avenue in the La Sierra and La Sierra Acres neighborhoods in the City of Riverside (City).

During preliminary design, the City determined that the approved scope and original project alignment would eliminate access to property driveways and adjacent properties. As a result, the City proposes shifting the sidewalk improvements to the easterly side of Mitchell Avenue, instead of the westerly side as originally scoped, and removing the Class I sidepath trail as well. The scope change would avoid property conflicts, driveway loss, and potential eminent domain proceedings. Additionally, constructing the pedestrian improvements on the easterly

side of Mitchell Avenue would provide the same project benefits by providing the communities with continuous pedestrian improvements within the original project limits. It would also connect to another Cycle 6 ATP project - Five Points Neighborhood Pedestrian Safety Improvements project (PPNO 1314).

As part of this project amendment, the City would remove the Class I sidepath trail from the project to eliminate the overlap. The overall scope change would allow the City to close the Class I sidepath trail gaps using the existing alignment and right of way, pending future funding availability.

The project was approved for a 12-month project allocation time extension for the Plans, Specifications, and Estimate phase at the May 2025 Commission meeting.

ANALYSIS:

The Department's analysis has deemed the City's request a significant scope change due to the number of proposed changes.

The revised scope removes the Class I sidepath trail segments, shifts the sidewalk improvement and eliminates overlapping scope. However, the new alignment for the sidewalk creates an opportunity to close sidewalk gap and improve the project benefits by providing a continuous path along Mitchell Avenue beyond the original project limits, connecting to another ATP project. Based on revised cost estimates, the overall project cost has reduced from \$7,465,000 to \$7,197,000, reflecting a project savings of \$268,000. The scope change will not negatively impact the current project delivery schedule.

ANALYSIS RECOMMENDATION:

Based on the Commission's scope change guidance, as well as the analysis of the proposed scope changes, the Department has determined that this scope change will not change the overall active transportation benefits of the project as compared to the original scope. Therefore, the Department supports the proposed scope change for this project.

BACKGROUND:

Resolution G-16-29 amended the ATP Guidelines to stipulate that any agency implementing an ATP project, present scope changes to the Department for consideration prior to allocation. Therefore, the Department will make a recommendation to the Commission for final approval with the understanding that scope changes that result in a decrease in active transportation benefits may result in removal from the program.

Attachments

Project Scope Change Request

Caltrans' Analysis and Recommendations

Submittal Date: 4/2/2025

PROJECT NAME: Mitchell Avenue Sidepath Gap Closure

IMPLEMENTING AGENCY: City of Riverside

ATP ID: ATP6-08-146M

FEDERAL PROJECT NO.: 5058(113)

PPNO: 1310

DATE OF AGENCY/CT COORDINATION MEETING: 2/10/2025

FIELD REVIEW DATE (major only):

APPROVED PROJECT DESCRIPTION: Construct 0.94 miles of new sidepath with sidewalk in two segments closing gaps between existing Class I facilities through Western Riverside. Includes intersection improvements.

Purpose

This document serves as supplemental information to the Exhibit 25-D REQUEST FOR PROJECT SCOPE CHANGE (attached) completed by the City of Riverside (City) and submitted to Caltrans on 2/7/2025.

Caltrans' Recommendation(s)

As a result of Caltrans' review of the City's Scope Change Request documentation and subsequent discussions with City staff, Caltrans recommends the following action:

APPROVE AS A MAJOR SCOPE CHANGE

Scope to Be Changed

The City proposes to construct pedestrian improvements to easterly side of Mitchell Avenue instead of the westerly side in the original application. The following is a numbered list of proposed scope changes:

1. Construct pedestrian improvements on easterly side of Mitchell Avenue between Campbell Avenue and Gramercy Place. Pedestrian improvements include approximately 1,980 feet of new sidewalk, 2 new curb ramps, 2 new crosswalks and 8 light emitting diode (LED) enhanced stop signs
2. Add pedestrian improvements on easterly side of Mitchell Avenue between Gramercy Place and Norwood Avenue. The proposed improvements will provide sidewalk continuity on the easterly side of Mitchell Avenue. Pedestrian improvements include approximately 1,430 feet of new sidewalk, 1 new curb ramps and 4 LED enhanced stop signs.
3. Construct pedestrian improvements on easterly side of Mitchell Avenue between Norwood Avenue and Bushnell Avenue. Pedestrian improvements include approximately 820 feet of new sidewalk, 1 new curb ramps, 1 new crosswalk and 4 LED enhanced stop signs.
4. Remove pedestrian improvements along Mitchell Avenue improvements between Hole Avenue and Bushnell Avenue. This scope overlaps with another Cycle 6 project, **Five Points Neighborhood Pedestrian Safety Improvements**. Pedestrian improvements include approximately 1,690 feet of new sidewalk and 4 new curb ramps.
5. Remove the decomposed granite (DG) multi-use trail from the project, reducing right-of-way requirement and impacts to the property owners. Total length = 4,595 feet of DG multi-use trail.

Reason for the Scope Change

The approved pedestrian improvements on the westerly side of Mitchell Avenue would result in the elimination of most of the property owner driveway spaces, some even losing their driveways due to right of way constraints and existing roadway configuration. The original alignment for the pedestrian improvements would highly impact property owners and result in costly contiguous acquisition process through eminent domain. Placing the pedestrian improvements to the easterly side of Mitchell Avenue will achieve the project scope by providing the community with continuous pedestrian improvements within the original limits. The proposed pedestrian improvements will connect to another Cycle 6 ATP project, **Five Points Neighborhood Pedestrian Safety Improvements project**. The elimination of the DG multi-use trail from the project would remove the impacts to the existing right of way and property owners. Additionally, constructing the pedestrian improvements on the easterly side of Mitchell Avenue would allow the City to construct the DG multi-use trail within existing right of way on the westerly side in the future pending funding availability, and should not impact the overall project benefits. Currently, sections of an existing DG multi-use trail already exist on the westerly side of Mitchell Avenue, and any future sections would use the existing alignment and right of way; enhance connectivity and reduce right of way impacts to property owners. The original pedestrian improvements on the Mitchell Avenue between Hole Avenue and Bushnell Avenue are overlapping with the scope for another Cycle 6 project, **Five Points Neighborhood Pedestrian Safety Improvements project**, both projects were adopted in the Metropolitan Planning Organization component. The City proposes removing this overlapping scope from this project and completing the pedestrian improvements as included in the Five Points Neighborhood Pedestrian Safety Improvements project. Completion of both Cycle 6 projects as originally designed would provide pedestrian improvements and benefits along Mitchell Avenue, however, a gap would exist between Norwood Avenue and Gramercy Place. The City proposes closing this gap by adding pedestrian improvements on the easterly side of Mitchell Avenue between Norwood Avenue to Gramercy Place, which is included in this request. The added pedestrian improvements will provide continuous sidewalk on the easterly side of Mitchell Avenue between Campbell Avenue to Hole Avenue, increasing the project benefits.

Summary of Caltrans Analysis

Caltrans supports this request for the following reasons:

Proposed scope change's affect to benefits (Potential of the project to increase walking and biking as compared to the approved scope):

No Change - The proposed scope change is expected to have no change on the project's potential to increase walking and biking as compared to the original scope.

Although the DG multi-use trail adjacent to the original alignment of the sidewalk is being removed; reducing the project outputs, the proposed pedestrian improvements will still improve the existing active transportation system and has the potential to increase the projected benefits. The proposed pedestrian improvements should not reduce projected user counts after construction and focus on the broader effects and impacts within the community. The proposed pedestrian improvements on the easterly side of Mitchell Avenue between Norwood Avenue to Gramercy Place would provide continuity along the entire project limits without sidewalk gaps which were deficiencies in the original design using an existing alignment on the westerly side of Mitchell Avenue. Therefore, the proposed scope change has the potential to change behavior, attitudes, and conditions within the community because the proposed pedestrian improvements provide enhanced accessibility for pedestrians.

Proposed scope change's affect to benefits (Potential of the project to increase safety of pedestrians and bicyclists walking biking as compared to the approved scope):

No Change - The proposed scope change is expected to have no change on the overall safety of pedestrians and bicyclists as compared to the original scope.

Although the DG multi-use trail adjacent to the original alignment of the sidewalk is being removed; reducing the project outputs, the proposed pedestrian improvements will still enhance the existing active transportation system and has the potential to increase the projected benefits. The proposed pedestrian improvements on the easterly side of Mitchell Avenue between Norwood Avenue to Gramercy Place will provide continuity along the entire project limits. This continuous pathway will improve safety and accessibility for pedestrians and has the potential to increase the long-term benefits for both Cycle 6 ATP projects. The original safety improvements along Mitchell Avenue at the intersections of Campbell Avenue, Gramercy Place, Norwood Avenue and Well Avenue will remain as part of the project. These safety improvements include high visibility crosswalks and LED enhanced stop signs. Therefore, the proposed scope change has the potential to change behavior, attitudes, and conditions within the community because the pedestrian improvements should provide increased safety for pedestrians.

Additional Comments

Caltrans concurs with the information provided in Attachment 1- Exhibit 25-D REQUEST FOR PROJECT SCOPE CHANGE. The agency has coordinated with Caltrans staff to provide the most accurate information possible.

Caltrans' Coordination with Requesting Agency

Caltrans and the City met on February 10, 2025, to discuss the original scope and proposed scope change due to existing right of way constraints and roadway configuration.

Impact to Project Cost

If the scope change is approved, the overall project cost is projected to decrease. The original project cost was \$7,465,000 but now estimated at \$7,197,000, reflecting a project savings of \$268,000. If the project cost increase, the City will use local funds for the additional cost.

Impact to Project Schedule

The proposed scope change will not impact the current project schedule.

ATTACHMENTS

1. Exhibit 25-D REQUEST FOR PROJECT SCOPE CHANGE-Completed by Agency
2. Community Outreach
3. Original plans with changes redlined
4. Revised plans
5. Project Map - Five Points Neighborhood Pedestrian Safety Improvements Project
6. Original Engineer's Estimate with changes redlined
7. Revised Engineer's Estimate
8. Existing Photographs

ATTACHMENT 1.

Exhibit 25-D REQUEST FOR PROJECT SCOPE CHANGE



**PUBLIC WORKS
DEPARTMENT**
Engineering

City of Arts & Innovation

To: Esayas Hago
District Local Assistance Engineer
Caltrans, Office of Local Assistance
464 W 4th St, San Bernardino, CA 92401

Date: 02/05/2025
PPNO: 1310
Federal PROJECT #: 5058(113)
ATP ID #: ATP6-08-146M

Project Name (Per CTC programming): Mitchell Avenue Sidepath Gap Closure

Approved Project Description (As submitted in Application): Construct 0.94 miles of new sidepath with sidewalk in two segments closing gaps between existing Class 1 facilities through Western Riverside. Includes intersection improvements.

Approved Project Limits (As submitted in Application): Mitchell Avenue between Campbell Avenue and Hole Avenue in western Riverside. Proposed project area connects DAC to commercial & community destinations including schools & parks.

☒ **For Federally Funded Projects:** Written MPO Concurrence (see Item #7)

Current FTIP/FSTIP Description: Construct 0.94 miles of new sidepath with sidewalk in two segments closing gaps between existing Class 1 facilities through Western Riverside. Includes intersection improvements.

Current FTIP/FSTIP Limits: Mitchell Avenue between Campbell Avenue and Hole Avenue in western Riverside. Proposed project area connects DAC to commercial & community destinations including schools & parks.

Dear Esayas Hago:

The City of Riverside was awarded ATP funding on August 8th, 2023 for the Mitchell Avenue Sidepath Gap Closure project which proposes new trail, sidewalk, and road improvements in the western Riverside area. These improvements will promote connectivity to parks and schools to the residents of the La Sierra communities. With this goal in mind, the City of Riverside is seeking a scope change to the previously awarded project which will ultimately better serve the community and its residents.

Consistent with the California Transportation Commission's (CTC) "Policy on Project Amendments and Advance Project Allocations", adopted on August 18, 2016, we request that Caltrans consider, and the CTC approve the proposed Project Amendments as documented below:

REQUIRED ELEMENTS (PER CTC'S PROJECT AMENDMENT POLICY):

- I. **An explanation of the proposed scope change;**
 - A. Construct sidewalk improvements on easterly side of Mitchell Avenue instead of the westerly side for the entire project.

- B. Construct a curb, gutter, and sidewalk within the proposed limits per the City's sidewalk standards and per the City's General Plan requiring right of way to 33' from centerline.
- C. Eliminate the multipurpose trail from the entirety of this project, reducing right-of-way requirement and impacts to the property owners. This results in a reduction of the typical section from 41.5' to 33' from street centerline.
- D. Eliminate the 0.32 mile segment of Mitchell Avenue improvements between Hole Avenue and Bushnell Avenue which will be constructed by a separate project.
- E. Add the 0.30 mile segment on Mitchell Avenue from Norwood Avenue to Gramercy Place to the project providing sidewalk continuity on the easterly side of Mitchell Avenue with the project limits.
- F. Widen the street as necessary per the City's General Plan and reconstruct the existing pavement section as necessary to properly join the new concrete improvements.

The reason for the proposed scope change;

- A. After careful deliberation, placing the approved improvements on the westerly side of Mitchell Avenue would result in the elimination of most of the residents' driveway spaces, some even losing their driveways entirely with the right of way resulting in a close proximity to their dwellings. These resultant remnant parcels will not be received well by most owners resulting in a costly contiguous acquisition process through eminent domain.
- B. The construction of a curb, gutter, and sidewalk only will achieve the project goals to provide the community with pedestrian mobility while still respecting the residents' driveway accessibility. Placing the sidewalk on the easterly side of Mitchell Avenue will allow for the trail to be constructed solely on the westerly side in the future pending funding availability. Portions of the trail currently already exist on the westerly side in some segments on Mitchell allowing better future trail connectivity and reducing the right-of-way needs at that time.
- C. The elimination of the multipurpose trail preserves the existing driveway space mentioned above as well as the cost effectiveness of the project.
- D. The 0.32 mile segment of Mitchell Avenue between Hole Avenue and Bushnell Avenue is a segment also specified in the Five Points project in the City of Riverside, which was awarded ATP funding on August 7th, 2023 as part of Cycle 6. We are proposing to remove the overlapping segment from the Mitchell Avenue project scope to direct the funds to improve a different segment of Mitchell Ave mentioned below.
- E. With the removal of the 0.32 mile segment Mitchell Ave from Hole Avenue to Bushnell Avenue from this project as mentioned above, the City is proposing the 0.30 mile segment from Norwood Avenue to Gramercy Place to be improved in its place. This segment was previously not included in the grant. This proposed segment achieves a sidewalk gap closure which would provide uninterrupted sidewalk access from Campbell Ave to Hole Ave.

The impact the proposed scope change would have on the overall cost of the project;

The proposed scope change will reduce the project's overall cost by eliminating the trail, which reduces the need to acquire large extents of right-of-way from property owners.

- Original Project Cost: \$7,464,615
- Updated Project Cost: \$7,196,116

- F. An estimate of the impact the proposed scope change would have on the potential of the project to increase walking and bicycling as compared to the benefits identified in the project application (increase or decrease in benefit);**

Location/Element	Approved Scope	Proposed Change	Change in Walking/Bicycling Benefit
Mitchell Ave Improvements, switch sidepath to east side of street	West side of Mitchell Ave	East side of Mitchell Ave	Neutral
Mitchell Ave entire sidepath section	Class I multipurpose trail and Curb Sidewalk	Curb sidewalk only	Neutral
Mitchell Ave 0.32 mile segment from Hole Ave to Bushnell Ave	Part of approved project locations	Remove segment	Decrease *
Mitchel Ave 0.30 mile segment from Norwood Ave to Gramercy Pl	Not a part of approved project locations	Add segment to project scope	Increase

** These limits will be improved under a separate ongoing ATP Grant project.*

- G. An estimate of the impact the proposed scope change would have on the potential of the project to increase the safety of pedestrians and bicyclists as compared to the benefits identified in the project application (increase or decrease in benefit);**

Location/Element	Approved Scope	Proposed Change	Change in Safety Benefit
Mitchell Ave sidepath, switch sidepath to east side of street	West side of Mitchell Ave	East side of Mitchell Ave	Neutral
Mitchell Ave entire sidepath section	Class I multipurpose trail and Curb Sidewalk	Curb sidewalk only	Neutral
Mitchell Ave segment from Hole Ave to Bushnell Ave	0.32 mile segment part of approved project locations	Remove 0.32 mile segment	Decrease *
Mitchel Ave 0.30 mile segment from Norwood Ave to Gramercy Pl	Not a part of approved project locations	Add segment to project scope	Increase

** These limits will be improved under a separate ongoing ATP Grant project.*

- H. An explanation of the methodology used to develop the aforementioned estimates.**

The approved project scope aimed to improve community mobility and access to essential services for La Sierra residents by adding a sidewalk, curb and gutter, and a multipurpose trail. Safety would

also be enhanced with the installation of LED stop signs and high-visibility crosswalks. While the proposed scope eliminates the multipurpose trail, the overall safety and biking/walking benefits remain unchanged. However the proposed changes address concerns about the potential impact on residents' driveways and properties, which could significantly disrupt their daily lives. This proposed change preserves the core goals of the project while minimizing the effects on residents' properties.

- I. For projects programmed in the Metropolitan Planning Organization (MPO) component, evidence of MPO approval and the MPO rationale for their approval.

N/A

ADDITIONAL QUESTIONS:

Does this scope change require revalidation of your environmental document? (Yes/No) No.

If yes, what is the actual/estimated date of revalidation? _____

- J. Explain the additional public outreach efforts you have made with respect to this proposed scope change and provide a summary of the public response to these efforts:

The additional outreach for the proposed scope change included door-to-door visits to the homes that would be affected, as well as direct phone calls to the property owners. These efforts were conducted by the City of Riverside engineering team, who provided technical details about the project. The changes were generally well-received, as residents understood that the approved project would lead to significant driveway losses for many on the west side of Mitchell Ave. Recognizing the importance of a sidewalk, curb, and gutter, they felt that the proposed scope change would meet the goals of the original project.

REQUIRED Attachments: (check boxes of attached required documents)

- | | |
|--|--|
| <input checked="" type="checkbox"/> Original plans/workplan with changes highlighted | <input checked="" type="checkbox"/> Revised Detailed Engineer's Est. |
| <input checked="" type="checkbox"/> Revised plans/workplan | <input checked="" type="checkbox"/> Written MPO Concurrence (see Item #7) |
| <input checked="" type="checkbox"/> Original Detailed Engineer's Est. with changes highlighted | <input checked="" type="checkbox"/> Additional Revised Application Documentation |

Required revisions to the Project's Description and/or Limits:

The proposed Project Amendments documented above will require the following changes to the Project's Description and/or Limits:

Proposed changes to the Project Description: Change project description to the following, "Construct 0.92 miles of new sidewalk closing the gap, in conjuncture with a separately awarded ATP Cycle 6 project, between Class 1 facilities though Western Riverside. Includes intersection improvements."

Proposed changes to the Project Limits: Change project limits on Mitchell Ave from Campbell Ave to Gramercy Pl and Norwood Ave to Hole Ave to limits of Campbell Ave to Bushnell Ave.

For Federally Funded Projects:

Proposed changes to the FTIP/FSTIP Description: Change project description to the following, "Construct 0.92 miles of new sidewalk closing the gap, in conjuncture with a separately awarded ATP Cycle 6 project, between Class 1 facilities though Western Riverside. Includes intersection improvements."

Proposed changes to the FTIP/FSTIP Limits: Change project limits on Mitchell Ave from Campbell Ave to Gramercy Pl and Norwood Ave to Hole Ave to limits of Campbell Ave to Bushnell Ave.

Project Delivery Status:

The following is a side-by-side comparison of the original project schedule and the current project schedule. The explanations for each milestone date change is listed below:

Original CTC Allocation Dates: (as programmed by the CTC when the application was approved for funding):

PA&ED: 12/01/2023 PS&E: 06/30/2025 R/W: 06/30/2026 CON: 06/30/2027 CON-NI: N/A

Actual/Currently Anticipated CTC Allocation Dates: (at the time of this request)

PA&ED: 09/30/25 PS&E: 06/30/26 R/W: 06/30/26 CON: 06/30/27 CON-NI: N/A

Explanation for milestone changes: *Project delayed for execution of agreement and scope change. >*

Local Agency Certification:

This Request for Scope Change has been prepared in accordance with the *Procedures for Administering Local Projects in the Active Transportation Program (ATP)*. I certify that the information provided in the document is accurate and correct. I understand that if the required information has not been provided this form will be returned and the request may be delayed. Please advise us as soon as the scope change has been approved. You may direct any questions to:

_____ at _____
(name) (phone number)

Signature: Edward Lara Title: Principal Engineer Date: 02/05/2025

Agency/Commission: _____ City of Riverside

Attachments:

Distribution:

- (1) Original -DLAE
- (2) Copy – Division of Local Assistance, Headquarters ATP Program Manager
- (3) Copy – MPO/RTPA/County Transportation Commission

ATP Engineer's Checklist

Required for all Infrastructure Projects

This application checklist is to be used by the engineer in “responsible charge” of the preparation of this ATP application to ensure all of the primary elements of the application are included as necessary to meet the CTC's requirements for a PSR-Equivalent document (per CTC's ATP Guidelines and CTC's Adoption of PSR Guidelines - Resolution G-99-33) and to ensure the application is free of critical errors and omissions; allowing the application to be accurately ranked in the statewide and regional ATP selection processes.

Special Considerations for Engineers before they Sign and Stamp this document attesting to the accuracy of the application:

Chapter 7; Article 3; Section 6735 of the Professional Engineer's Act of the State of California requires engineering calculation(s) or report(s) be either prepared by or under the responsible charge of a licensed civil engineer. Since the corresponding ATP Infrastructure-application defines the scope of work of a future civil construction project and requires complex engineering principles and calculations which are based on the best data available at the time of the application, the application must be signed and stamped by a licensed civil engineer.

By signing and stamping this document, the engineer is attesting to this application's technical information and engineering data upon which local agency's recommendations, conclusions, and decisions are made. This action is governed by the Professional Engineer's Act and the corresponding Code of Professional Conduct, under Sections 6775 and 6735.

❖ For more assistance, please refer to the Caltrans ATP PSR equivalent [presentation](#) and [slides](#)

1. **Project Location Map** (Attachment C)

Engineer's Initials: EL

- a. The project limits must be clearly depicted in relation to the overall agency boundary
 - i. Include the scale of the drawing and a north arrow.

2. **Project Layout/Plans** showing **existing and proposed** conditions (Attachment D)

Engineer's Initials: EL

- a. Show project elements at a scale which allows the visual verification of the overall project “construction” limits and limits of each primary element of the project. Scale must be shown on the layout/plans.
- b. Show the full scope of the proposed project.
- c. Show all changes to existing motorized/non-motorized lane and shoulder widths. Label the proposed widths.
- d. Show agency's right-of-way (R/W) lines when permanent or temporary R/W impacts will occur. (As appropriate, also show Caltrans', Railroad, and all other government agencies R/W lines.)

Anticipated Number of R/W Takes	Cost	Time needed to Acquire
<u>30</u>	<u>\$ 1,400,000</u>	<u>12</u> Months

Anticipated Number of Easements	Cost	Time needed to Obtain
<u>(Temp)</u>	<u>\$ 42,000</u>	<u>12</u> Months

3. **Cross-section(s)** showing **existing and proposed** conditions (Attachment D)

Engineer's Initials: EL

(Must include a cross-section for each segment where the width of improvements or Right-of-way vary significantly if a typical cross section is provided)

- a. Show and dimension: changes in lane widths, **R/W lines**, side slopes, etc.
- b. Show both the width and the depth/thickness for any new pavement.

Note – Separate cross sections for existing and proposed conditions may be needed to clearly show

the before and after pavement widths/thicknesses.

4. **Project Estimate** (Attachment F)

Engineer's Initials: EL

- a. The Project Estimate (Attachment F) **must be used** for all applications that are requesting ATP Infrastructure funds. Attachment F shall be completed per the instructions and attached to the application, in the appropriate location.
- b. Each of the main project elements are broken out into separate construction items. The costs for each item are based on calculated quantities and appropriate corresponding unit costs.
 - i. Only items in the "Allowable Lump Sum Items" tab may use Lump Sum as a unit.
- c. All non-participating costs in relation to the ATP funding are clearly identified and accounted for separately from the eligible costs.
- d. Clearly identify and account for all project elements in which the applicant intends to utilize services provided by the CCC, certified community conservation corps, or tribal corps.
- e. **ALL** project development costs (including non-ATP funds) need to be accounted for in the total project cost.

5. **Crash/Safety Data, Collision maps and Countermeasures** (Part B, Question 3)

Engineer's Initials: EL

- a. Confirm that crash data shown is depicted accurately, is shown to scale, and occurred within the influence area of proposed improvements.

6. **Project Schedule, Funding, and Programming Request** (Part A6)

Engineer's Initials: EL

- a. All applicants with projects over \$1M must anticipate receiving federal ATP funding for the project and therefore the project schedules and programming included in the application must account for all applicable federal requirements and timeframes.
- b. "Completed Dates" for project Milestone Dates shown in the application have been reviewed and verified.
- c. "Expected Dates" for project Milestone Dates shown in the application account for all reasonable project timetables, including: Interagency MOUs, Caltrans agreements, CTC allocations, FHWA authorizations, federal environmental studies and approvals, federal right-of-way acquisitions, federal consultant selections, project permits, etc.
- d. The fiscal year and funding amounts shown in the Project Programming Request (PPR) must be consistent with Implementing Agency's expected project milestone dates and available matching funds.

Anticipated Environmental Studies	Cost	Time needed for the study
1. <u>BIO</u>	\$ <u>79,500</u>	<u>12</u> Months
2. <u>CULTURAL</u>	\$ <u>79,500</u>	<u>12</u> Months
3. _____	\$ _____	_____ Months

7. **Warrant Studies/Guidance** (Attachment K)

Engineer's Initials: EL

☐ **(Check if not applicable)**

- a. For new Traffic Control Signals – an engineering study that includes analysis of Signal Warrants 1- 9 (CA MUTCD) must be submitted. For ATP funding, warrants 4, 5 or 7 should be met but the final decision to install a signal must be made by the engineer. The engineering study (and any additional documentation of the engineering judgment supporting the Traffic Control Signal, if needed) must include the name and license number of the responsible engineer and must be

attached to the application in the "Additional Attachments" section (Attachment K).

8. **Additional Narration and Documentation** (Attachment K)

Engineer's Initials: EL

- a. The text in the "Narrative Questions" in the application must be consistent with and supports the engineering logic and calculations used in the development of the maps, layout/plans, cross sections, schedule and estimate. If non-standard ATP elements are included in the project (i.e. vehicular roadway widening necessary for the construction of the primary ATP elements), attach appropriate documentation demonstrating the engineering decisions and calculations that justify the inclusion of the non-standard elements.

This checklist is to be completed by the engineer in "responsible charge" of defining the project's Scope, Cost and Schedule per the expectations of the CTC's PSR Equivalent. The checklist is expected to be used during the preparation of the documents, but not initialed and stamped by the engineer until the final application and application attachments are complete and ready for submission to Caltrans.

Licensed Engineer Information:

Name (Last, First):

Lara, Edward

Title:

Principal Engineer

Engineer License Number:

63753

Signature and Date:

Edward Lara

Email Address:

elara@riversideca.gov

Phone:

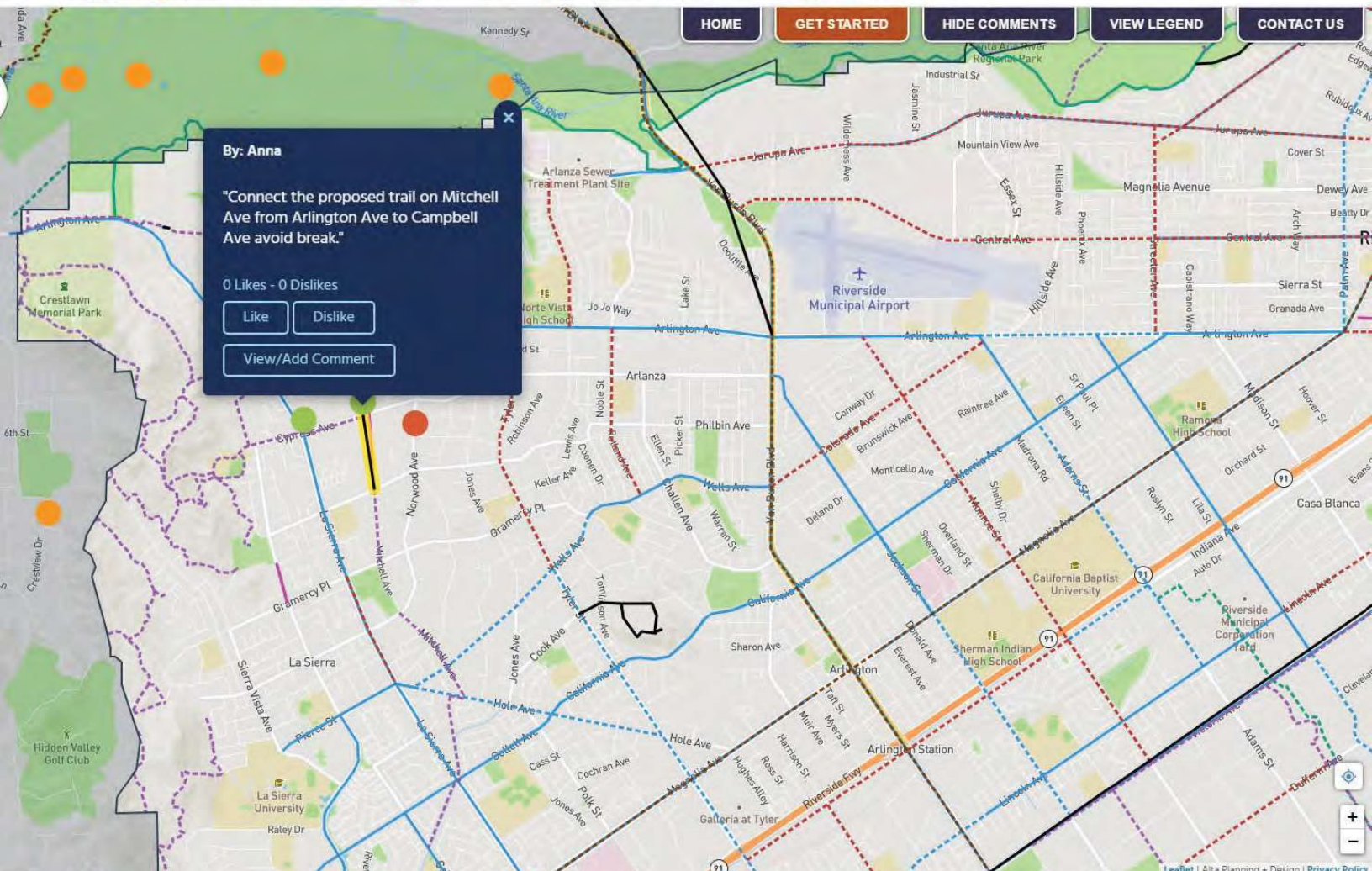
951-826-2337

Place the Engineer's Stamp below:



ATTACHMENT 2.

COMMUNITY OUTREACH



WHICH TRAILS IS YOUR PRIORITY?
¿CUAL SENDEROS ES TU PRIORIDAD?



TEXT YOUR VOTE TO
(951) 987-5782/
TEXTE SU VOTO AL
(951) 987-5782

Call or Text: 1-951-228-0022 Survey: tinyurl.com/riversidePACTsurvey Public Input Map: tinyurl.com/riversidePACT



Saturday, March 16, 2019

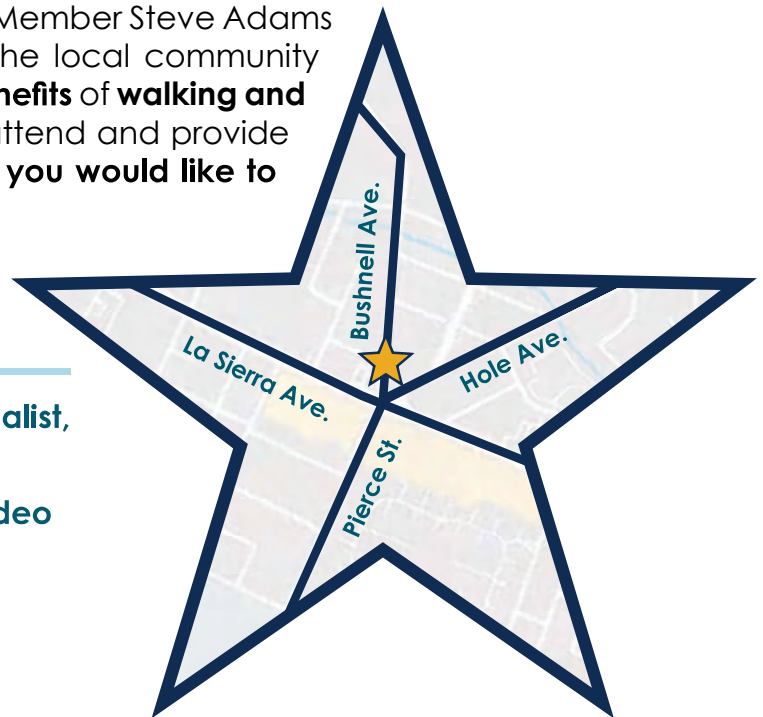
11 a.m. - 2 p.m. • Bushnell Ave. and Bogart Ave.

Join Council Member Jim Perry, Council Member Steve Adams and the City of Riverside as we bring the local community together to experience and learn the **benefits of walking and active transportation**. We invite you to attend and provide feedback on the kind of **developments you would like to see in your community**.

Activities Include:

A Special Demonstration by X Games Medalist,
Samarria Brevard

Games & Prizes • Open Streets • Bike Rodeo
Bicycle Demo • Community Visioning
Stream Mobile (La Sierra Library)
Art Installations & More!



For more information, visit: RiversideCA.gov/PublicWorks



Sábado, 16 de marzo de 2019

11 a.m. - 2 p.m. • Intersección de la avenida Bushnell y Bogart

Acompaña el Concejal Jim Perry, el Concejal Steve Adams y la ciudad de Riverside en el evento Conectando 5 Puntos donde tendremos oportunidades para experimentar y aprender los **beneficios** de **caminar y el transporte activo**. Los invitamos a asistir y proporcionar retroalimentación sobre **los tipos de desarrollo que gustaría ver en su comunidad**.

Actividades Incluyen:

Una demostración especial de la medallista de X Games, Samaria Brevard

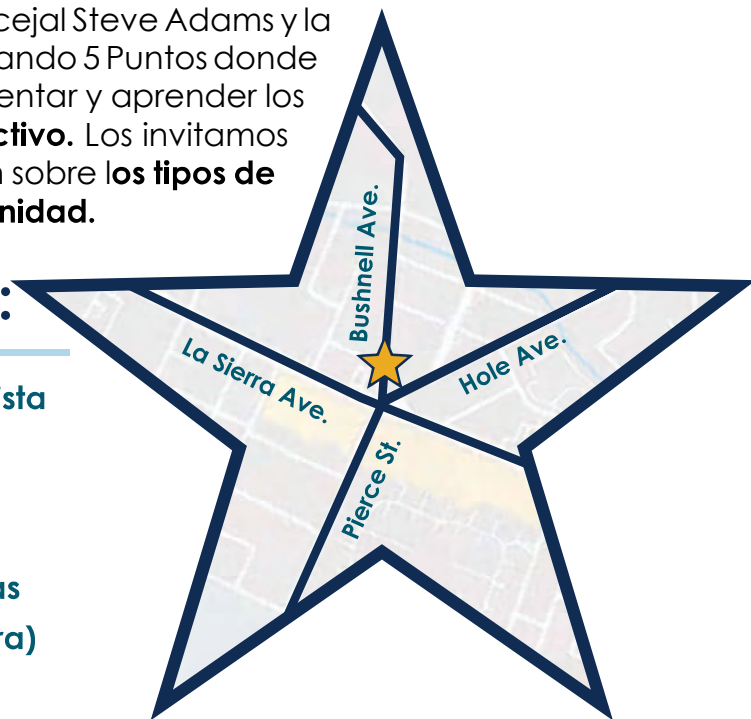
Juegos y Premios • Calles Abiertas

Demostración de Bicicleta Eléctrica

Visión comunitaria • Rodeo de Bicicletas

Automobil STREAM (Biblioteca de La Sierra)

Instalaciones de Arte y Más



Para obtener más información, por favor visite: RiversideCA.gov/PublicWorks

























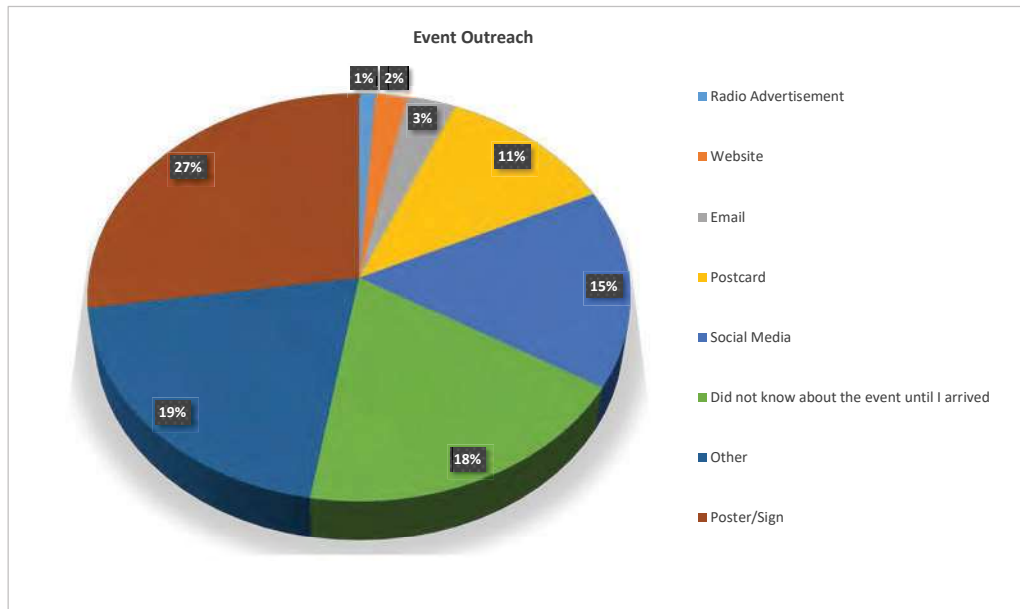




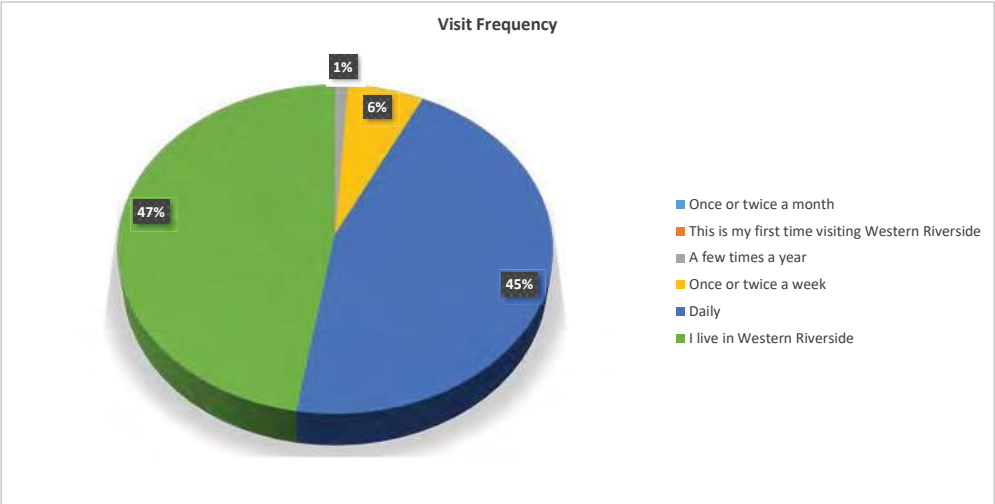
on paper. Some survey respondents did not respond to certain questions, marked multiple answers for questions that asked for one in the data where the responses for some questions may not add up to the total survey responses, or add up to more than the did not answer the question are not graphed.

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 26 27%
 95 97%
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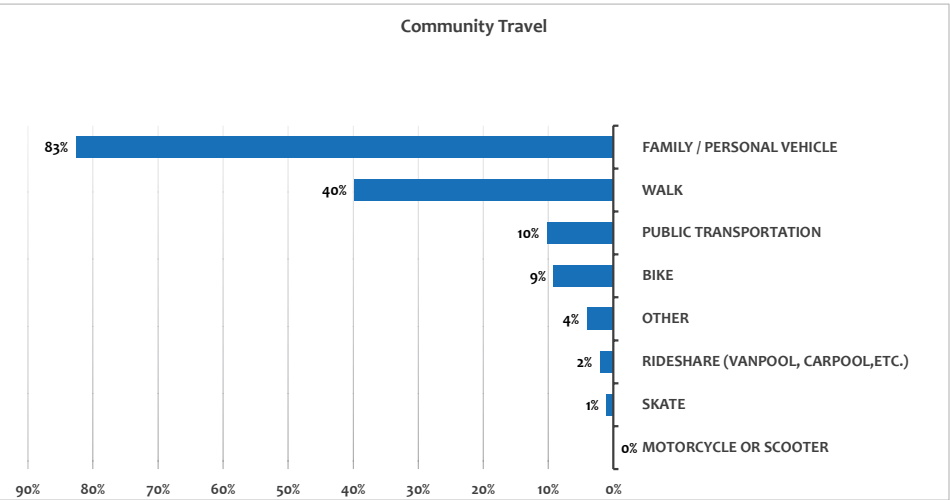


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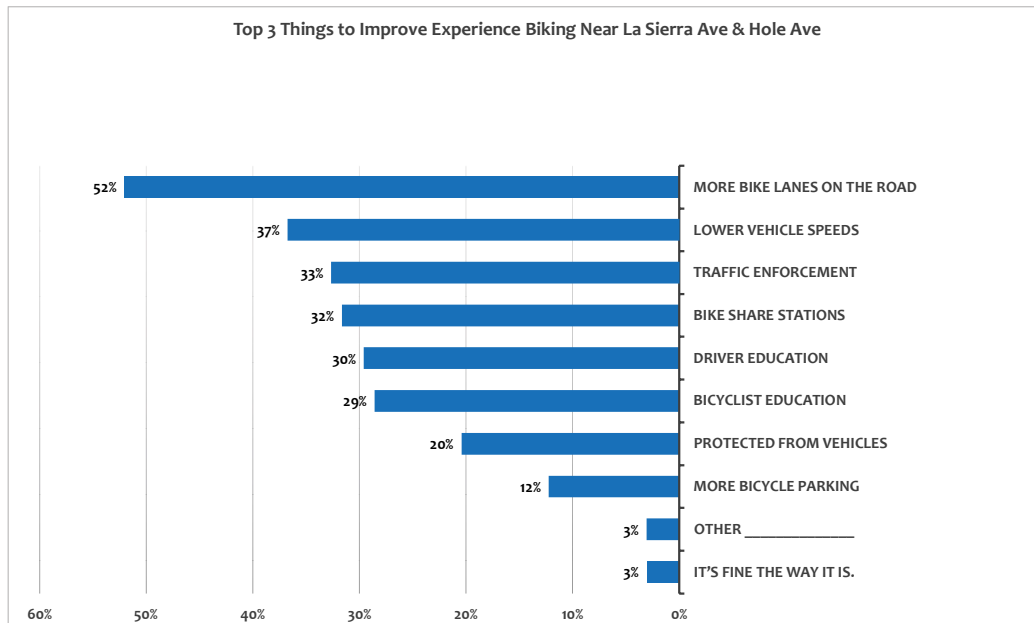
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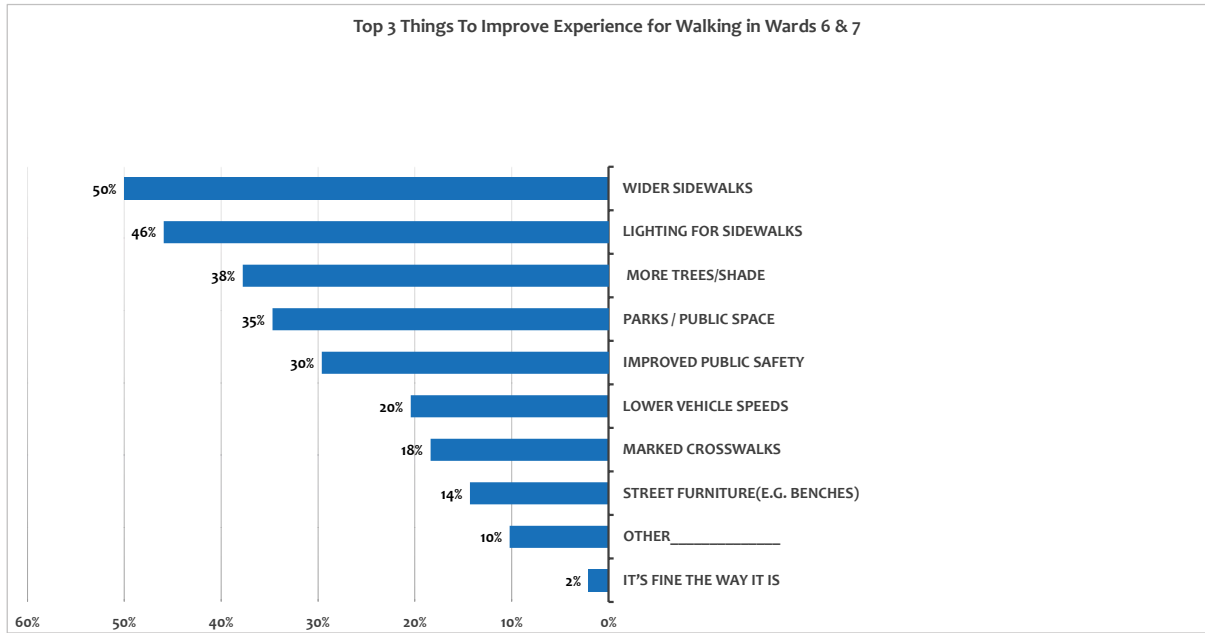
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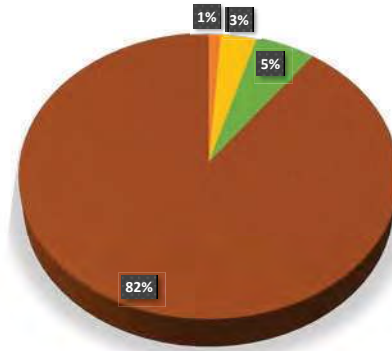
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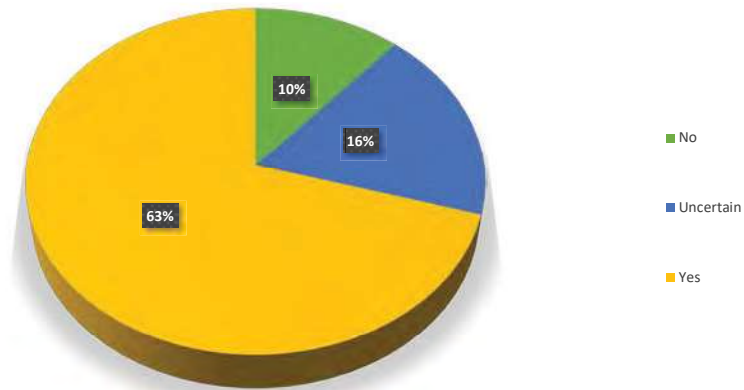
Importance of Making it Easy to Bike, Walk or Have Public Transportation Between Destination to Having a Good Community



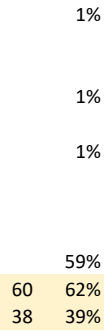
from where you
bike, or have public
r job and home

10	10%
16	16%
62	63%
88	90%
10	10%

Ability to Walk, Bike or Have Public Transit Between Job and Home Influence Relocation

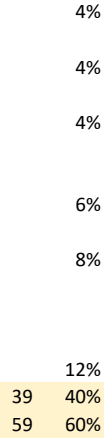


could best use the
ssed on this survey
play, dining or

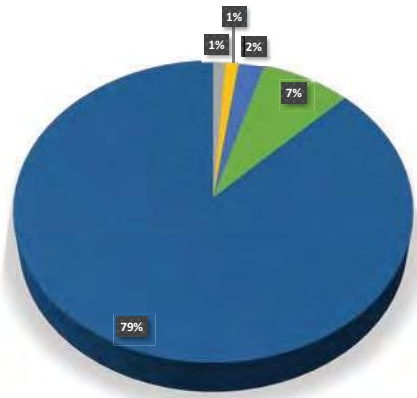


tions here, we
neighborhood needs

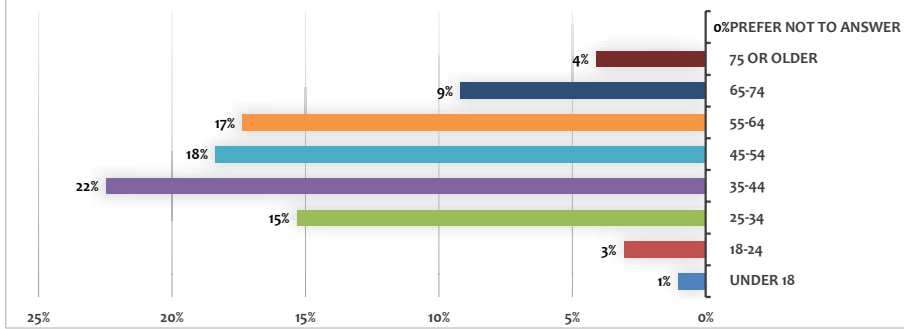
munity would like to see things like:
2%



Zipcode of Residency



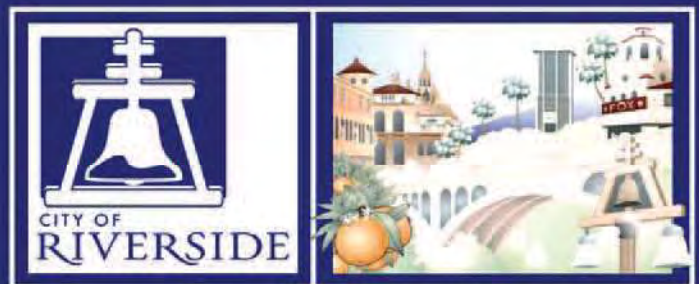
Age Groups



RIVERSIDE

GENERAL PLAN 2025

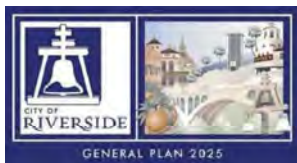
EXECUTIVE SUMMARY



GENERAL PLAN 2025

City of Riverside
Community Development Department
3900 Main Street
Riverside, CA 92522
(951) 826-5371
www.riversideca.gov

November 2007



INTRODUCTION

How We Work

Riversiders pursue the American Dream. We focus on creating economic development opportunities that provide high-skilled, high-paid employment for all members of our community.

How We Play

Riverside is an oasis. The City combines trees, water and the arts to create a distinctive, comfortable gathering place that enriches the lives of residents and visitors alike.

How We Live

We empower neighborhoods. We listen to each other and pay attention to the details because they matter. Neighborhoods enable the City and residents to work together to solve the needs of all.

How We Get Around

Riverside has responded to the traffic challenge by becoming a more self-contained city. Residents have easy access to an efficient, multi-option transportation system that enables them to meet their needs within the community.

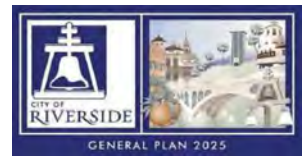
How We Learn

Excellence in education is the key to economic growth. Riversiders work together to achieve quality education at all levels.

These themes and values were reinforced by the community during 2003-2004 as City staff and consultants worked with the General Plan Citizens' Advisory Committee to develop the objectives and policies that build upon the vision and provide the structure for each of the General Plan elements. Outreach efforts to the community included, but were not limited to:

- Thirteen meetings with a twenty-plus-member Citizens' Advisory Committee consisting of residents and local business people
- Three meetings with an Arts and Culture Committee, which was instrumental in preparation of the Arts and Culture Element
- Three meetings with the Market/Magnolia Subcommittee to define specific objectives and strategies for this historic boulevard that extends the length of the City

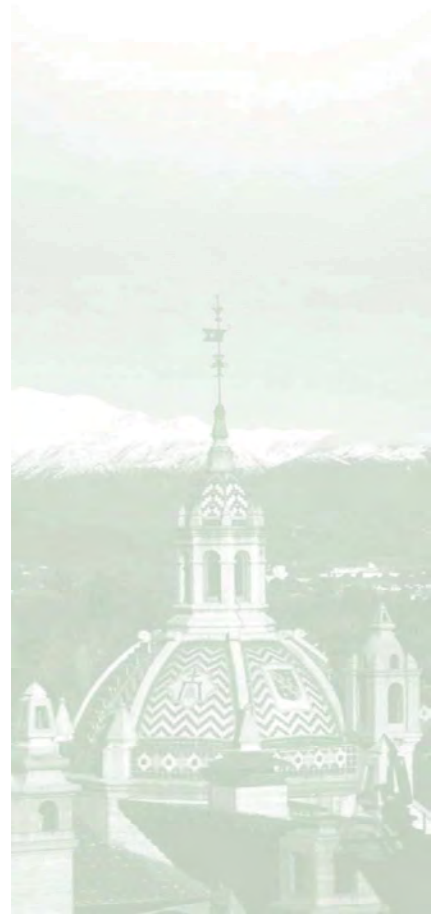
INTRODUCTION

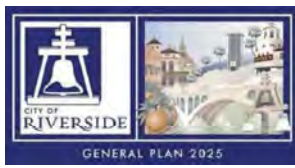


- Three meetings with the Education Subcommittee and its representatives from all of Riverside's educational institutions and organizations, which was instrumental in preparation of the Education Element
- Focused discussions with stakeholder groups, such as business and development interests, recreation enthusiasts, environmental groups, social service providers
- Meetings with each Division of the Greater Riverside Chamber of Commerce to overview the Program and encourage broad community participation
- A series of joint study sessions with the City Council and City Planning Commission prior to formal public hearings
- Two Citizens' Congress sessions for the entire community, which attracted over six hundred participants
- Public hearings before the Planning Commission and City Council



September 2003 Citizens' Congress





INTRODUCTION

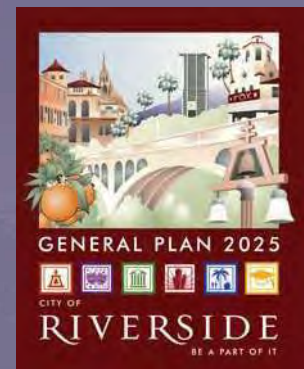
TABLE H
COMMUNITY INPUT

Community Meeting	Year	Number of Meetings	Number of Attendees	Number of Comments Received
Visioning Riverside	2002	7	1300	5000
<i>Visioning Interviews</i>	2002	41	41	246
City Council Appointed Committees				
Technical Advisory Committee (TAC)	2003	6	25	87
Citizen Advisory Committee (CAC)	2003-2004	13	42	159
Art Subcommittee	2003	4	19	107
<i>Arts Interviews</i>	2003	14	14	126
Education Subcommittee	2003	3	14	35
Magnolia/Market Subcommittee	2003	3	20	26
<i>Magnolia/Market Specific Plan Workshop</i>	2004	1	120	88
Topic Group Meetings				
Youth	2003	1	18	37
Environmental/Open Space	2003	1	12	30
Recreational Interests/Libraries	2003	1	4	19
Public Safety	2003	1	4	32
Zoning/Subdivision	2003	1	12	32
Counties/Cities	2003	1	2	12
Social Services	2003	1	9	47
Neighborhoods	2003	1	7	19
Economic Development	2003	1	0	0
City Council/City Planning Commission Workshops	2003-2004	4	6	0
Citizens' Congress 1	2003	1	360	1529
Citizens' Congress 2	2004	1	200	103
Chamber Task Force Meetings	2003-2004	6	30	60
City Planning Commission Workshops	2004-2005	7	11	11
City Planning Commission Hearings	2004-2005	18	557	557
Over 6000 Notices mailed – phone calls received in response to the mailout	2003-2004		1230	1230
Letters to the Planning Commission	2003-2005		263	263
TOTALS		138	4320	9855

CITIZEN'S CONGRESS SUMMARY

RIVERSIDE GENERAL PLAN PROGRAM

OCTOBER 2003



CITIZENS' CONGRESS SUMMARY

- Coordinate with UCR expansion plans down University Avenue
- People have a problem with police response time/delays. Suggest more patrols for visibility and/or priority response time.
- Home for the homeless
- Need a town center
- Corner grocery stores

NEIGHBORHOOD: LA SIERRA

Question 1: What are the assets of your neighborhood?

- Community trail, park, and water

Question 2: What is missing?

- La Sierra County Center needs an upgrade. Don't like Ralph's and grocery shopping options.
- Nice restaurants, nice convenient shops, and movie theater nearby. Need something like University Village at UCR
- Would like to see live theater near Park Sierra (restaurants)
- Would like a general hospital closer than the one in Moreno Valley
- Stop street racing on Mobley Avenue between Mull and Cook, and racing on Cook and Jones
- Slow down speed on streets
- Would like speed bumps
- Lightbulb repair on street post
- Convenient walking access from Metrolink Station. Freeway access on Park Sierra Avenue.
- Alternative entertainment area closer to La Sierra and convenient access via Metrolink

NEIGHBORHOOD: LA SIERRA SOUTH

Question 1: What are the assets of your neighborhood?

- Victoria Area – Prop “R” and Measure “C”
- We're still somewhat rural and want to keep it that way! Please.
- Live in Greenbelt, protected by Prop R Measure
- Don't build apartments here

Question 2: What is missing?

- Need grade separation on Magnolia Avenue entering Home Gardens
- Traffic – adequate roads/infrastructure – reasonable growth – infrastructure can't keep up with it
- Two lane roads in both directions to support all the new housing
- Need parks – Cross Street and Victoria



CITIZENS' CONGRESS SUMMARY

- Transit-Oriented Development near La Sierra Metrolink, like Orenco Station in Hillsboro, OR
- Do something with vacant land across from Metrolink: park/mixed use



CITIZENS' CONGRESS SUMMARY

- Development on RCC property? Arizona School evacuation plan uses top of hill on RCC property, what happens if developed?
- Orange groves

NEIGHBORHOOD:
LA SIERRA HILLS

Question 1: What are the assets of your neighborhood?

- Great area

Question 2: What is missing?

- Need cheaper homes
- Need more signals for schools
- Need more schools for housing
- Lake, hills

NEIGHBORHOOD:
LA SIERRA ACRES

Question 1: What are the assets of your neighborhood?

No responses

Question 2: What is missing?

- No sidewalks on Norwood. Bad street conditions. Lack of code enforcement
- Bicycle paths and sidewalks; need to have access to shopping and mall without using a vehicle
- Need parks. Street median planters would be nice too.
- Upscale restaurants in general area
- Bad condition of City property: Norwood and Gramercy maintenance yard
- Work on idea of regional park overlooking Hidden Valley Wildlife area instead of 750 homes
- Dumpy, unkempt; needs code control and resident's cooperation

NEIGHBORHOOD:
**SYCAMORE
CANYON/CANYON
SPRINGS**

Question 1: What are the assets of your neighborhood?

- You are threatening the future use of March Field by allowing expansion in flight pattern

Question 2: What is missing?

- In the rush to develop, make sure wildlife corridors are preserved

NEIGHBORHOOD:
ARLINGTON SOUTH

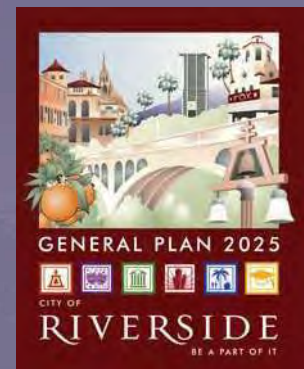
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- Nurseries are o.k.
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CITIZENS' CONGRESS SUMMARY

RIVERSIDE GENERAL PLAN PROGRAM

DATE OF EVENT: JUNE 12, 2004

On June 12, 2004, over 200 Riverside residents attended the second Citizens' Congress General Plan festival held on the beautiful California Baptist University campus. Riversiders came to learn of the Bold Moves proposed in the update General Plan, proposals that reflect residents' hopes for their City's future, as they have expressed over the past year as part of the General Plan program. Participants viewed a presentation by Mayor Ron Loveridge, Planning Director Ken Gutierrez, and the General Plan consultant team that illustrated how Riverside plans to grow smarter, balancing its desire to preserve the resources that distinguish Riverside with the need to accommodate new residents over the next 20 years. The key themes presented illustrated Riverside's vision:

- ❖ *The City will grow, but will grow smarter. City will shape growth rather than let growth shape the City.*
- ❖ *Growth will respect and enhance Riverside's natural and historic resources.*
- ❖ *Traffic will increase, even with no growth in the City of Riverside – but will be managed more effectively.*
- ❖ *Riverside grows as a major regional employment center.*
- ❖ *Riverside remains a preeminent regional education center.*
- ❖ *Riverside's stature as the region's cultural center is enhanced.*
- ❖ *Riverside continues to move toward cleaner air.*



Attendees learned of Riverside's vision for mixed-use districts at key locations along Magnolia Avenue and University Avenue, and at strategic locations in other neighborhoods, where residential living and commercial services work together to create vibrant centers. The concept of *Riverside Park* showed the City's goal to complete and enhance the ring of open space surrounding Riverside, and to connect the open spaces with landscaped travelways. The presenters identified the City's desire to strengthen partnerships with its educational and arts institutions to encourage joint use of facilities and programs toward the ends of offering Riversiders a broader



Create activities for teens like off-road trails and skateboard parks that they do not have to pay to get in.

AIR QUALITY

We have the dirtiest air in America – clean it up!

TRAFFIC/TRANSPORTATION

Madison should not be used as a thoroughfare for the “Overlook Parkway” as mentioned, it is a residential community. How would people be able to go to church?

When are you planning to widen Washington Street? I have 800 feet of frontage on the side of Washington where it will be widened. Please contact me before finalizing your plans so that we can work together on doing it right.

I don't see an integrated transportation plan – just streets.

Fix Van Buren and Magnolia Area.

Put shade over bus stops; people are getting skin cancer.

Reduce speeds on City streets to allow business districts visible to passerby and safer for vehicles and pedestrians (30 mph in blks. 75-100 comm. – 35 mph 80-80ft. collect and arterial – 40 mph 100-110 ft arterial – 45-50 mph 120-140 ft arterials).



Need lights at 3rd and Vine underpass.

The City streets are full of pot holes and deep cracks and chunks. We need competent street work.

Overlook shouldn't go through Madison/Casa Blanca.

Measure A designation of Central Avenue in front of Sierra Middle School as a major arterial needs to be changed at the RCTC.

Now that the proposed utilities customer services center is coming into the Casa Blanca area and there is only one entrance, is there going to be speed bumps installed on Emerald Street?

The City is developing these plans around the primary idea of our visible beauty and history/heritage (see our City logo). Reduce speed limits within the City to improve street/pedestrian safety and allow people to enjoy the visible beauty of all of our City.

A multi-modal transportation hub (i.e. trains, buses, etc.) is needed, perhaps located at the Metrolink Station, perhaps using the old movie theater.

We need to plan out the car: more bikes/walking.

We need to keep our bus station open for safety of inner-City and/or inner-state travelers. It's unsafe for people traveling through to stand on street.

Putting Overlook through from Canyon Crest Drive to Madison will not increase traffic on Victoria Avenue; it will reduce traffic. I live on Washington and work at UCR – currently I must use Victoria to get to work. If you put Overlook through I will not have to travel on Victoria.

Transit needs to run later, especially in Eastside.

Railroad crossing signs (old ones) on Magnolia and Merrill: they need painting and repair. We don't want them removed.

Traffic speed is too high on residential streets. (All streets with 25% residential use between intersecting streets or city blocks max speed 30 mph – All streets >26% residential along City blocks max speed 25mph – All streets >75% residential maximum use, stop signs and speed bumps.)



Review safety of allowing large semi-trailers to go westbound on Alessandro. If one of those trucks lost its brake, the potential to take out a large part of a neighborhood exists.

Metrolink routes need to run later out of Los Angeles.

Save Victoria Avenue: no through traffic.

Overlook needs to go through now! We are wasting gas and making smog and clogging Victoria.

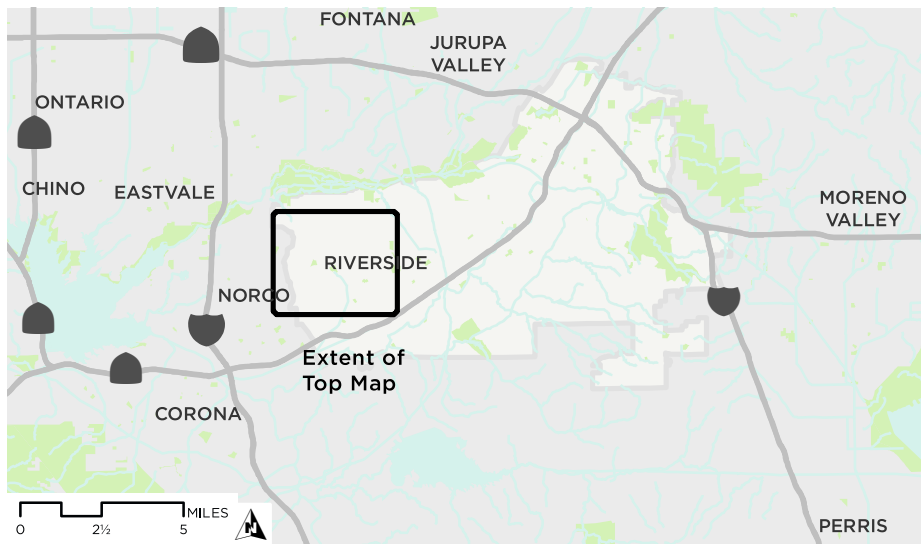
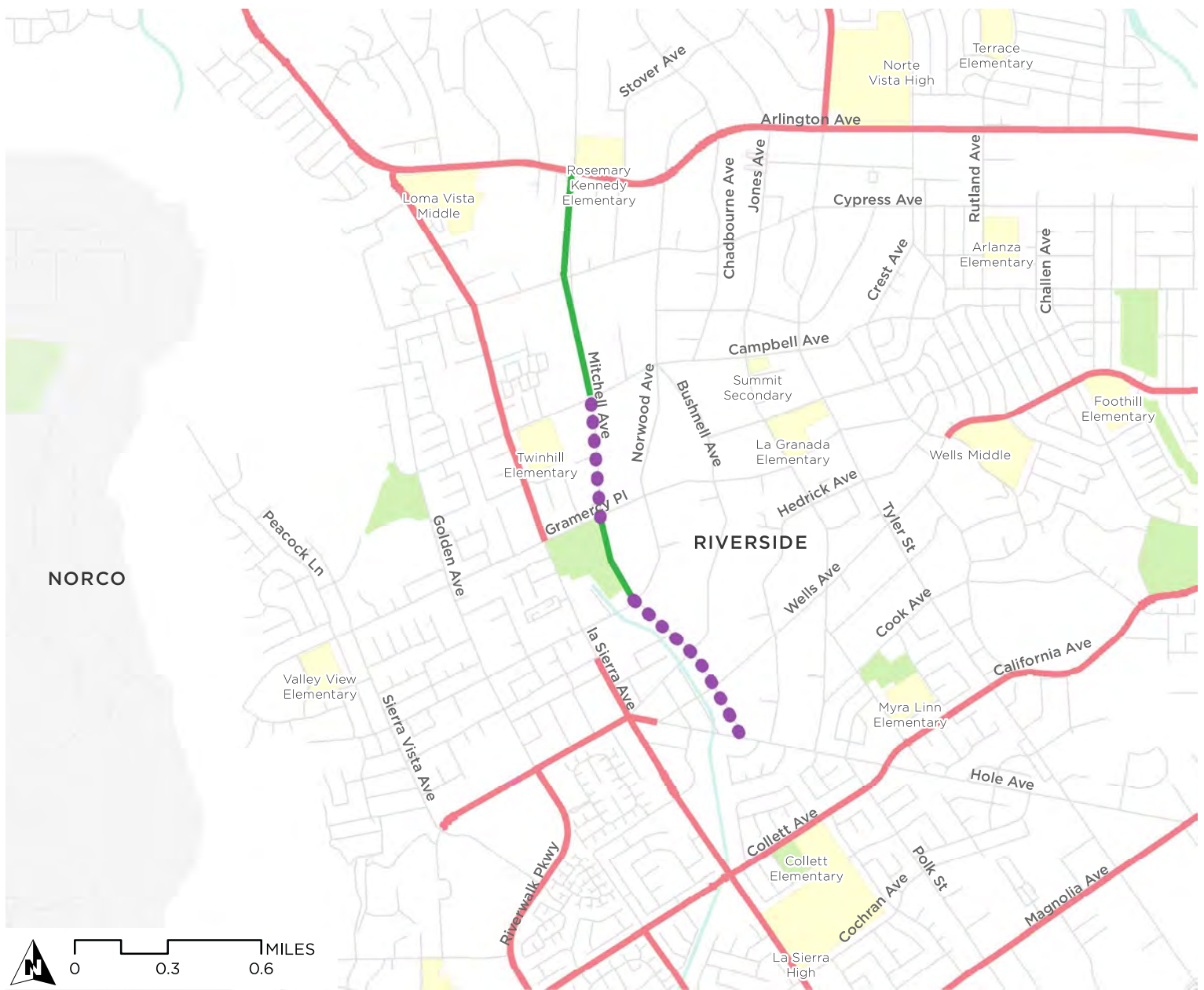
The Magnolia underpass needs to be #1 on priority, or partner with railroad to go under all the streets.

Enhance traffic enforcement on Central Avenue.

No heavy traffic through the Greenbelt; Adams Street needs to be saved.

ATTACHMENT 3

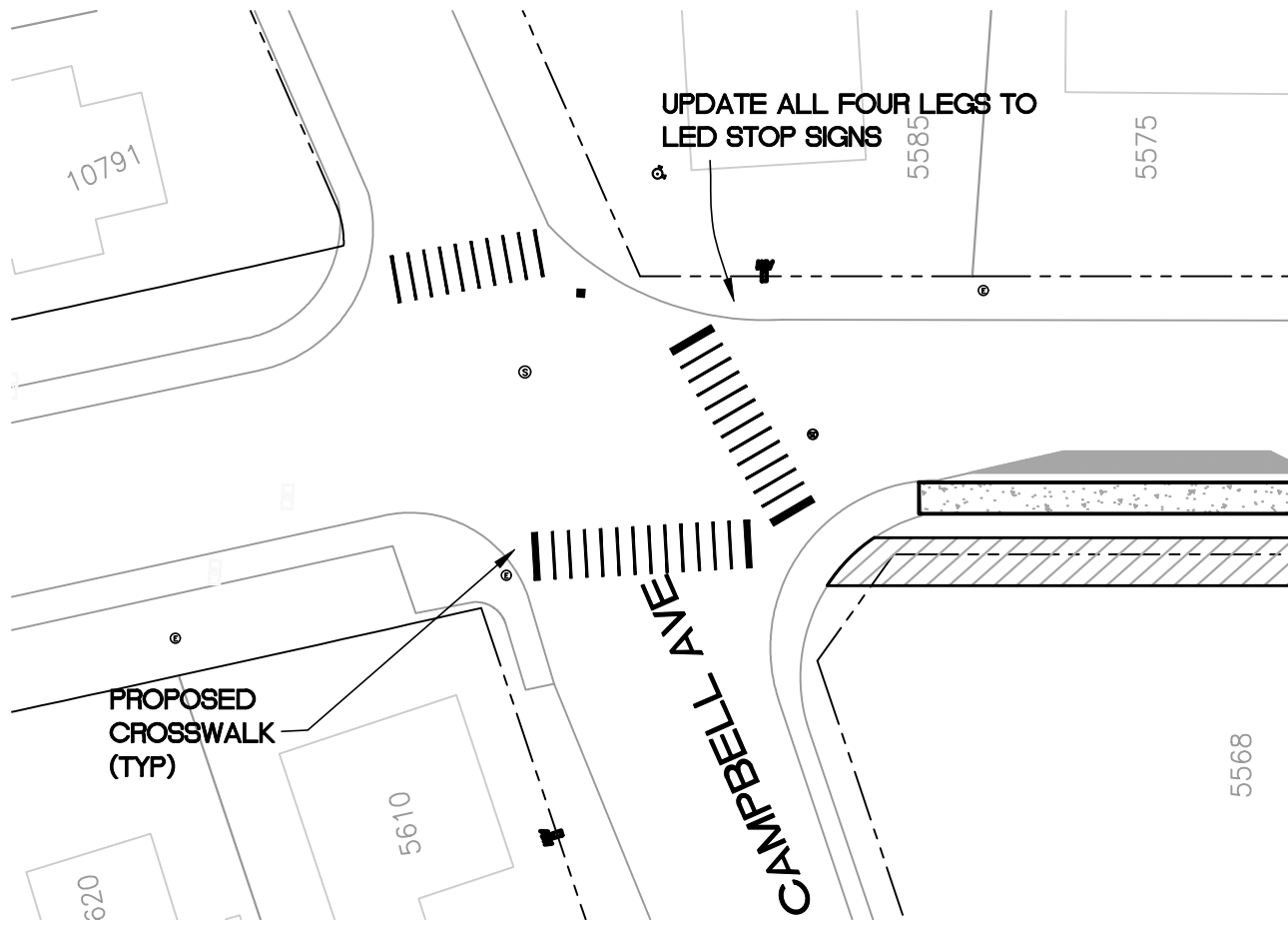
ORIGINAL PLANS WITH CHANGES RELINED



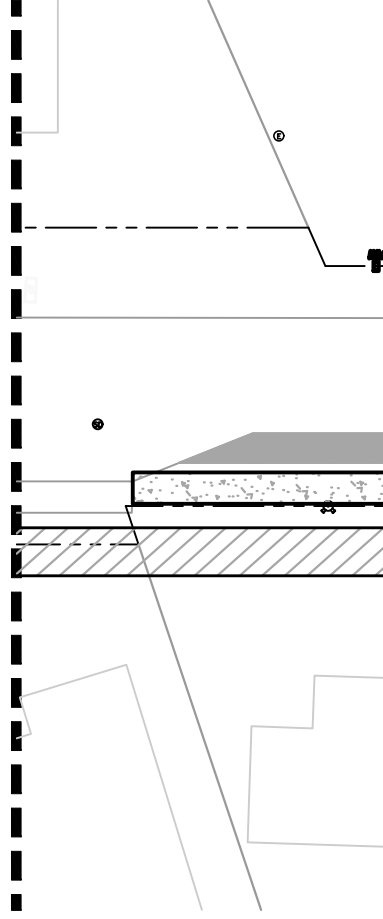
MITCHELL AVENUE SIDEPATH **ACTIVE TRANSPORTATION GRANT** PROJECT AREA Riverside, CA

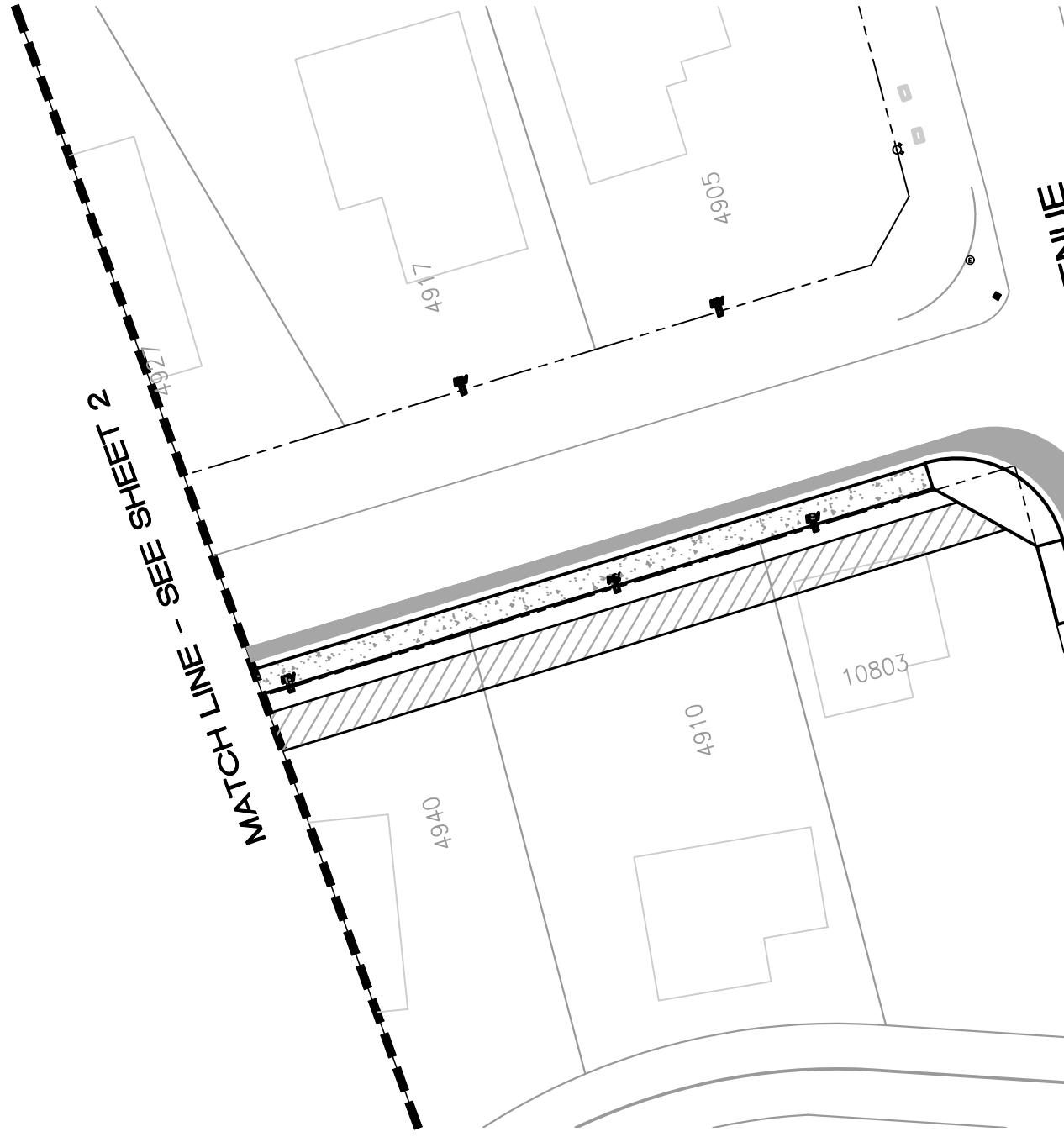
Project Locations
 ●●●●● Project Locations

Existing Bikeways
 — Bicycle Lane
 — Shared-Use Path



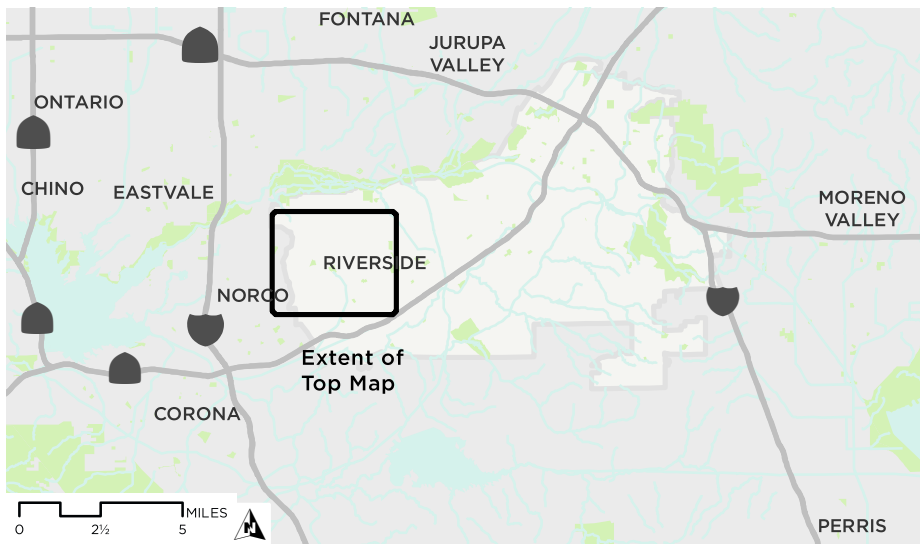
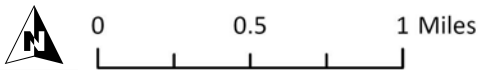
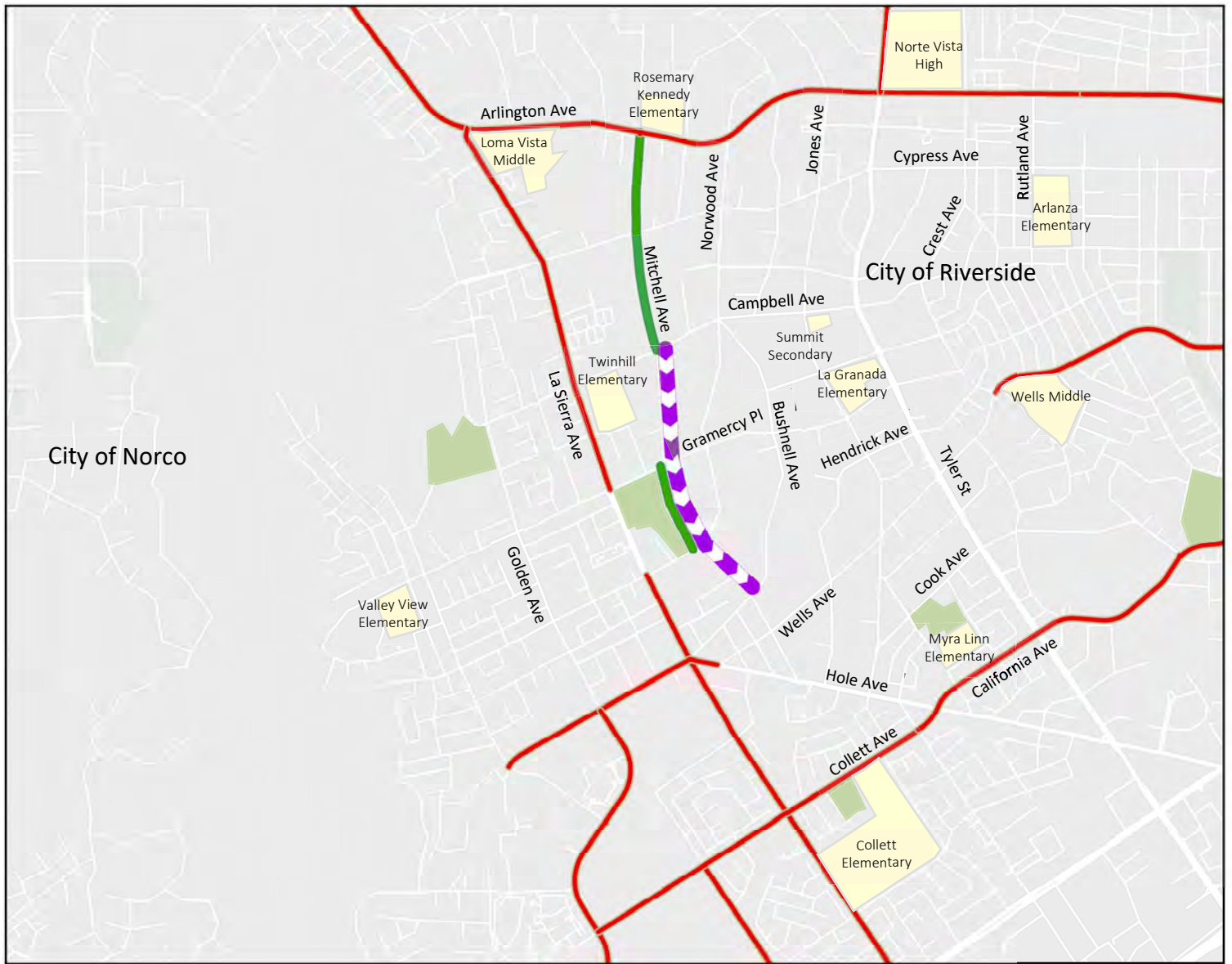
MATCH LINE - SEE SHEET 1






ATTACHMENT 4

REVISED PLANS




MITCHELL AVENUE SIDEPATH **ACTIVE TRANSPORTATION GRANT** PROJECT AREA Riverside, CA

Project Location

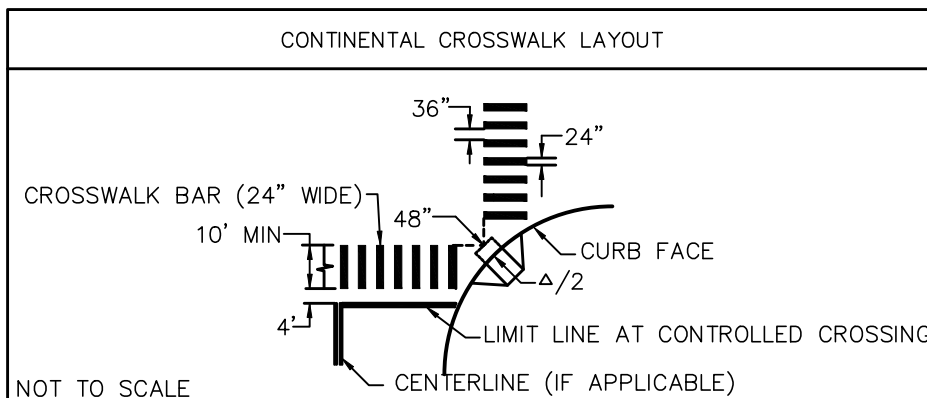
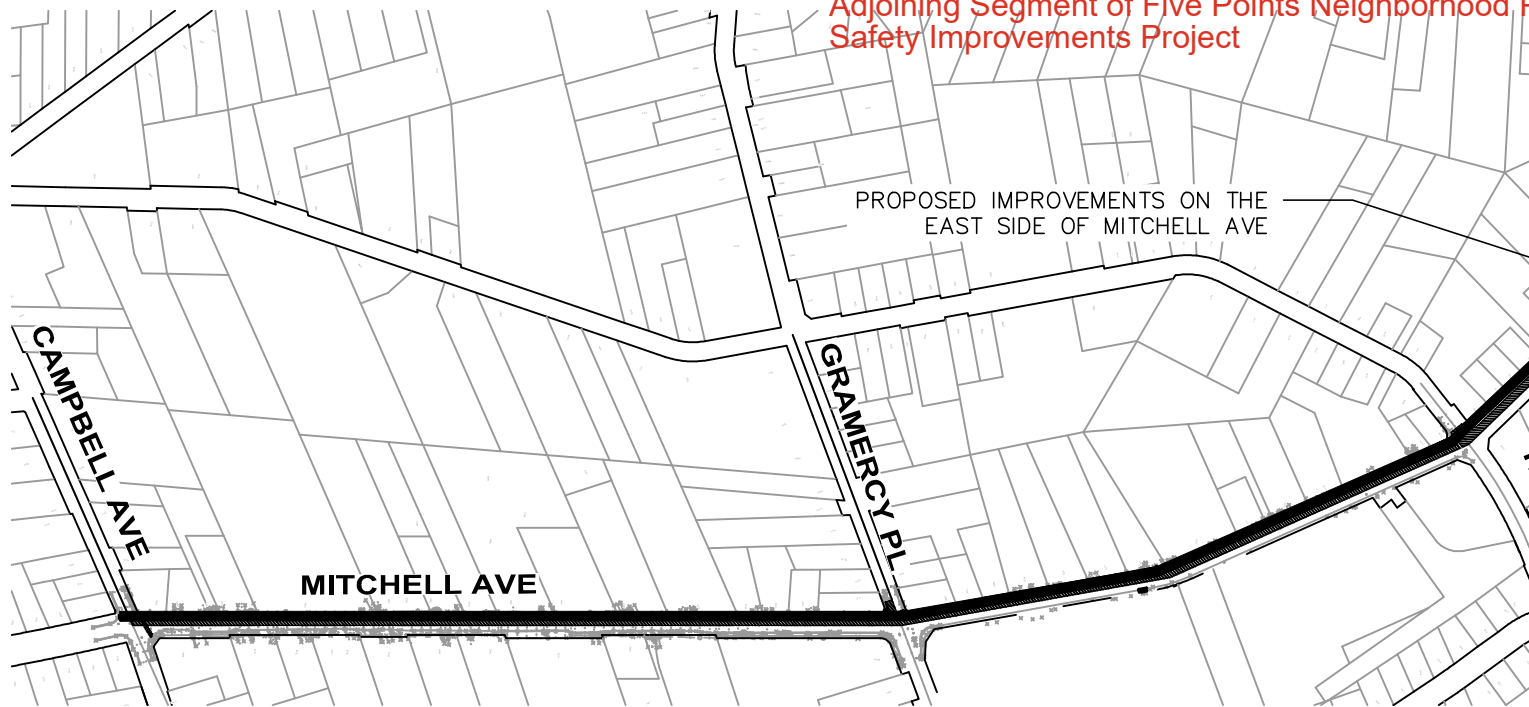
 Proposed sidewalk on easterly Side of Mitchell Ave

Existing Bikeways

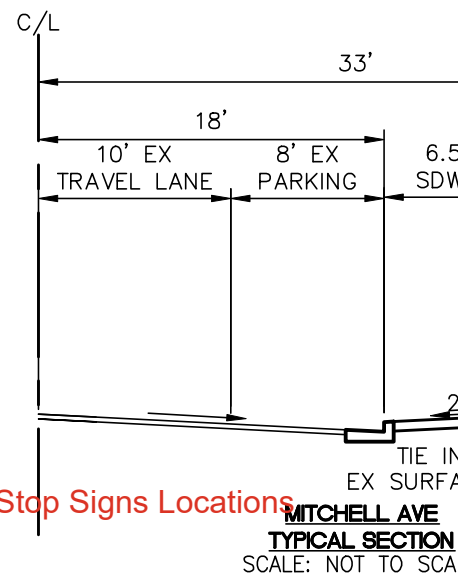
 Bicycle Lane

 Shared-Use Path

Adjoining Segment of Five Points Neighborhood
Safety Improvements Project



SCALE: 1"=500'



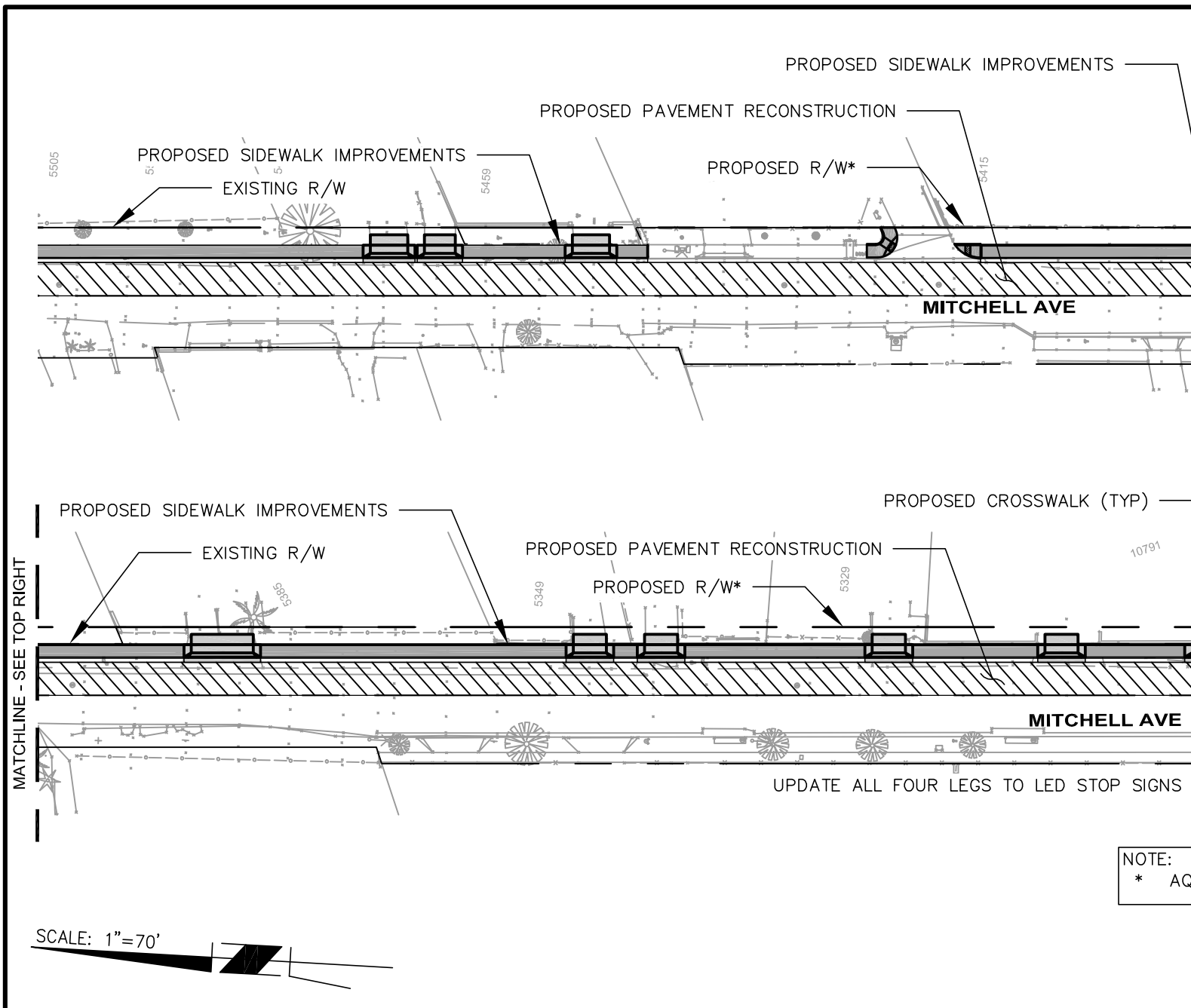
Crosswalks and LED Stop Signs Locations

• CITY OF RIVERSIDE, CALIFORNIA •

MITCHELL AVE PROJECT EXTENTS

SCALE: 1"=500' DRAWN BY: GM DATE: 1/31/2025

PEDESTRIAN SAFETY IMPROVEMENT PLANS: MITCHELL AVE



• CITY OF RIVERSIDE, CALIFORNIA •

TYPICAL STREET IMPROVEMENTS

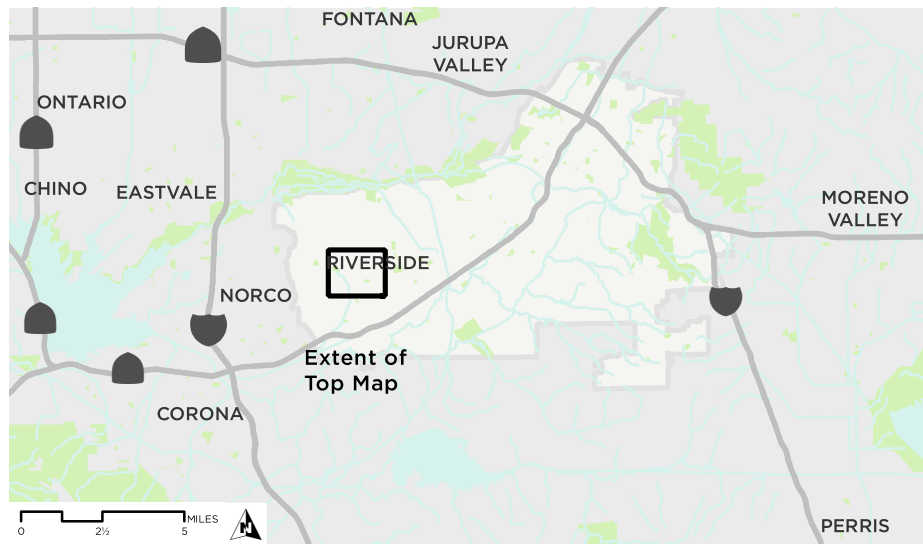
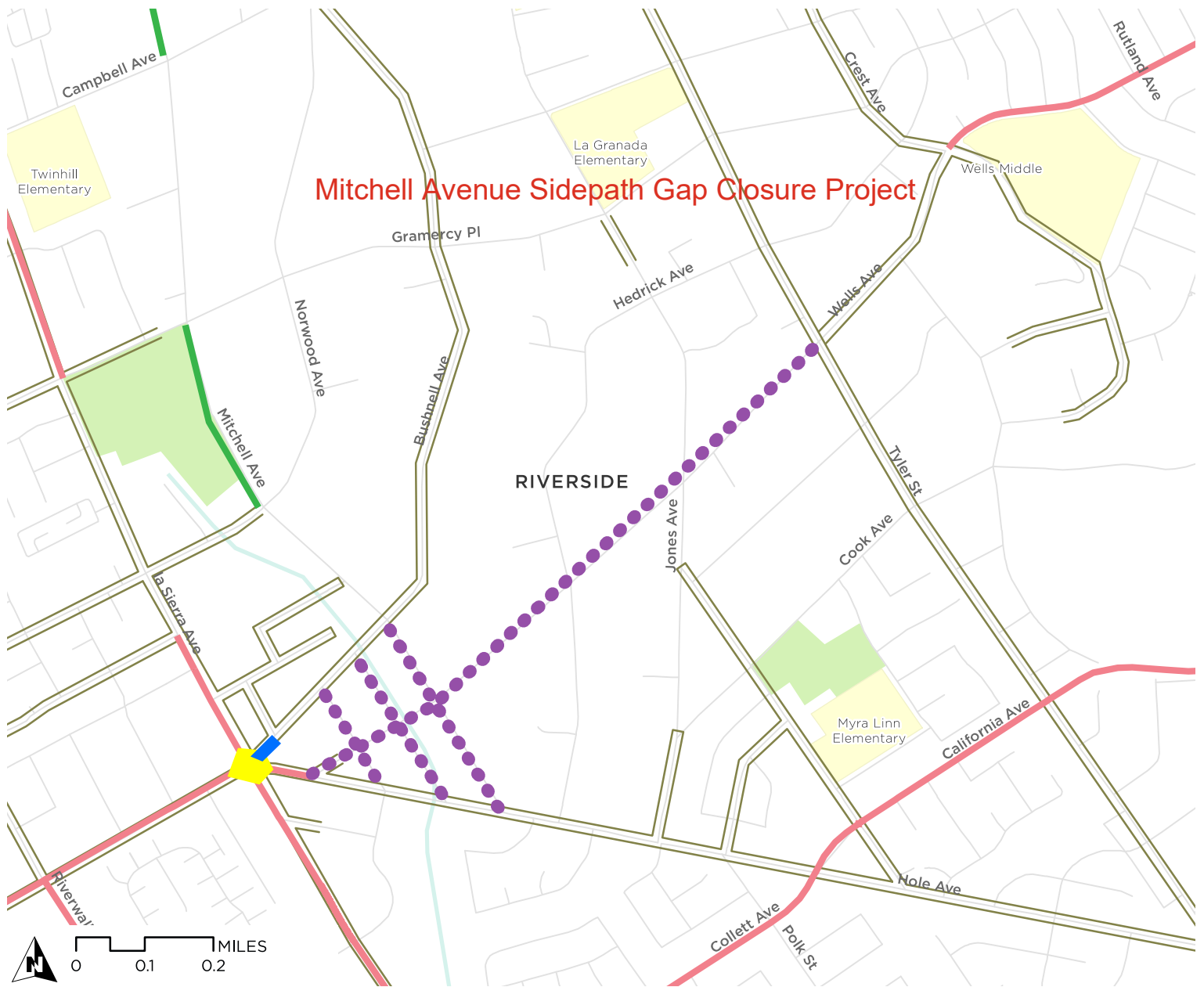
SCALE: 1"=70'

DRAWN BY: GM DATE: 1/31/2025

PEDESTRIAN SAFETY IMPROVEMENT PLANS: MITCHELL AVE

ATTACHMENT 5

**PROJECT MAP - FIVE POINTS NEIGHBORHOOD PEDESTRIAN
SAFETY IMPROVEMENTS PROJECT**



FIVE POINTS NEIGHBORHOOD PEDESTRIAN SAFETY IMPROVEMENTS ACTIVE TRANSPORTATION GRANT

PROJECT AREA
Riverside, CA

Project Locations

- New Sidewalk
- Intersections Improvements
- Pedestrian Plaza

Existing Facilities

- Bicycle Lane
- Shared-Use Path
- Existing Sidewalk

ATTACHMENT 6

ORIGINAL ENGINEER'S ESTIMATE WITH CHANGES REDLINED

Detailed Project Estimate and Total Project Costs- Cycle 6

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency: City of Riverside

Date: 6/13/2022

Project Description: Mitchell Avenue Trail

Project Location: Mitchell Avenue between Campbell and Hole

Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate: Nathan Mustafa

License #: 83654

Project Estimate and Cost Breakdown:

Project Estimate (for Construction Items Only)						Cost Breakdown					
						ATP Eligible Costs/Items		ATP Ineligible Costs/Items		Corps/CCC to construct	
Item No.	Item	Quantity	Units	Unit Cost	Total Item Cost	%	\$	%	\$	%	\$
General Overhead-Related Construction Items											
1	Mobilization	1	LS	\$50,000.00	\$50,000	100%	\$50,000				
2	Traffic Control	1	LS	\$50,000.00	\$50,000	100%	\$50,000				
3	Stormwater Protection Plan	1	LS	\$75,000.00	\$75,000	100%	\$75,000				
4	Contract Administration / Job Site Mgmt.	1	LS	\$250,000.00	\$250,000	100%	\$250,000				
5			LS					100%			
6			LS					100%			
7			LS					100%			
8			LS					100%			
9			LS					100%			
10			LS					100%			
General Construction Items											
11	Clearing and Grubbing	1	LS	\$318,000.00	\$318,000	100%	\$318,000				
12	Concrete Curb and Gutter per City Std 200	3490	LF	\$53.00	\$184,970	100%	\$184,970				
13	Concrete Driveway per City Std 302	5720	SF	\$13.00	\$74,360	100%	\$74,360				
14	Concrete Pedestrian Ramp, City Std 304	8	EA	\$4,770.00	\$38,160	100%	\$38,160				
15	Concrete Sidewalk per City Std 325	25113	SF	\$11.00	\$276,243	100%	\$276,243				
16	Existing Chain Link Fence & Gates	2905	LF	\$106.00	\$307,930	100%	\$307,930				
17	New/Relocate Existing Wood Fence	150	LF	\$53.00	\$7,950	100%	\$7,950				
18	Masonry Pilasters & Wrought Iron Fence	1	LS	\$121,900.00	\$121,900	100%	\$121,900				
19	Remove Class I Tree and Roots	13	EA	\$530.00	\$6,890	100%	\$6,890				
20	Remove Class II Tree and Roots	20	EA	\$1,060.00	\$21,200	100%	\$21,200				
21	Remove Class III Tree and Roots	19	EA	\$1,590.00	\$30,210	100%	\$30,210				
22	Water Service to relocated WM	45	EA	\$318.00	\$14,310	100%	\$14,310				
23	Concrete Driveway Match-up	5660	SF	\$16.00	\$90,560	100%	\$90,560				
24	Concrete Decorative Driveway	320	SF	\$22.00	\$7,040	100%	\$7,040				
25	Remove and Replace Mailbox	29	EA	\$64.00	\$1,856	100%	\$1,856				
26	Install Vinyl Fence	10182	LF	\$24.00	\$244,368	100%	\$244,368				
27	Construct 10' wide DG Trail	45464	SF	\$4.00	\$181,856	100%	\$181,856				
28	Construct 6" mow curb	10182	LF	\$16.00	\$162,912	100%	\$162,912				
29	Unclassified Excavation	3610	CY	\$53.00	\$191,330	100%	\$191,330				
30	Asphalt Concrete - DGAC	290	TON	\$128.00	\$37,120	100%	\$37,120				
31	Crushed Aggregate Base	225	CY	\$53.00	\$11,925	100%	\$11,925				
32	Relocate Sign & Post	18	EA	\$80.00	\$1,440	100%	\$1,440				
33	Install LED Stop Signs	16	EA	\$5,300.00	\$84,800	100%	\$84,800				
34	Install High Visibility Crosswalk	6	EA	\$10,600.00	\$63,600	100%	\$63,600				
35	Storm Drain Lateral and Catch Basin	5	EA	\$31,800.00	\$159,000	100%	\$159,000				
36	Relocate Water Meter	45	EA	\$4,240.00	\$190,800	100%	\$190,800				
37	Relocate Fire Hydrant	7	EA	\$5,300.00	\$37,100	100%	\$37,100				
38	Adjust and/or Relocate Electric Vault	4	EA	\$10,600.00	\$42,400	100%	\$42,400				
39								100%			
40								100%			
41								100%			
42								100%			
43								100%			
44								100%			
45								100%			
46								100%			
47								100%			
48								100%			
49								100%			
50								100%			
51								100%			
52								100%			
Subtotal of Construction Items:					\$3,335,230		\$3,335,230				
Construction Item Contingencies (% of Construction Items):				15.00%	\$500,285		\$500,285				
Total (Construction Items & Contingencies) cost:					\$3,835,515		\$3,835,515				

Detailed Project Estimate and Total Project Costs- Cycle 6

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency: City of Riverside

Date: 6/13/2022

Project Description: Mitchell Avenue Trail

Project Location: Mitchell Avenue between Campbell and Hole

Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate: Nathan Mustafa

License #: 83654

Project Delivery Costs:

Type of Project Cost	Cost \$	ATP Eligible Costs	Non-participating Costs	
Preliminary Engineering (PE)				
Environmental Studies and Permits(PA&ED):	\$ 159,000	\$159,000		
Plans, Specifications and Estimates (PS&E):	\$ 795,000	\$795,000		"PE" costs / "CON" costs
Total PE:	\$ 954,000	\$954,000		25% 25% Max
Right of Way (RW)				
Right of Way Engineering:	\$ 106,000	\$106,000		
Acquisitions and Utilities:	\$ 2,410,100	\$2,410,100		
Total RW:	\$ 2,516,100	\$2,516,100		
Total Pre-Construction Costs (PE+RW):	\$3,470,100	\$3,470,100		
Construction Engineering (CE)				"CE" costs / "CON" costs
Construction Engineering (CE):	\$ 159,000	\$159,000		4% 15% Max
Total Construction Costs:	\$3,994,515	\$3,994,515		
Total Project Cost:	\$7,464,615	\$7,464,615		

Documentation of Ineligible (Non-Participating) Costs:

The Engineer's logic and/or calculations for splitting costs between ATP-Eligible and Non-participating costs must be documented in this section of the Estimate form.

Separate logic is required for each item which is partly ineligible for ATP funding or is required for the construction of an ineligible item/element of the project.

Item #:	Description of Engineer's Logic: (See examples shown in the Instructions)
29-31	These items are intended to be eligible, a small amount of AC work is required to install the new sidewalk; vehicle capacity will not be added

M

ATTACHMENT 7

REVISED ENINGEER'S ESTIMATE

Detailed Project Estimate and Total Project Costs- Cycle 6

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency:	City of Riverside
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Date:	1/24/2025
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Project Description:	Mitchell Avenue Trail
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Project Location:	Mitchell Avenue between Campbell Ave and Bushnell Ave
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Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate:	Edward Lara
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License #:	63753
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Project Estimate and Cost Breakdown:

Project Estimate (for Construction Items Only)						Cost Breakdown					
						ATP Eligible Costs/Items		ATP Ineligible Costs/Items		Corps/CCC to construct	
Item No.	Item	Quantity	Units	Unit Cost	Total Item Cost	%	\$	%	\$	%	\$
General Overhead-Related Construction Items											
1	Mobilization	1	LS	\$75,000.00	\$75,000	100%	\$75,000				
2	Traffic Control	1	LS	\$50,000.00	\$50,000	100%	\$50,000				
3	Stormwater Protection Plan	1	LS	\$50,000.00	\$50,000	100%	\$50,000				
4	Contract Administration / Job Site Mgmt.	1	LS	\$250,000.00	\$250,000	100%	\$250,000				
5			LS					100%			
6			LS					100%			
7			LS					100%			
8			LS					100%			
9			LS					100%			
10			LS					100%			
General Construction Items											
11	Clearing and Grubbing	1	LS	\$375,000.00	\$375,000	100%	\$375,000				
12	Concrete Curb and Gutter per City Std 200	3800	LF	\$85.00	\$323,000	100%	\$323,000				
13	Concrete Driveway per City Std 302	10600	SF	\$20.00	\$212,000	100%	\$212,000				
14	Concrete Pedestrian Ramp, City Std 304	8	EA	\$4,770.00	\$38,160	100%	\$38,160				
15	Concrete Sidewalk per City Std 325	16100	SF	\$14.00	\$225,400	100%	\$225,400				
16	Replace Chain Link Fence and Post	1300	LF	\$110.00	\$143,000	100%	\$143,000				
17	Replace Wrought Iron Fence	100	LF	\$250.00	\$25,000	100%	\$25,000				
18	Replace Wrought Iron Fence atop Block Wall	500	LF	\$300.00	\$150,000	100%	\$150,000				
19	Replace Block Wall	150	LF	\$400.00	\$60,000	100%	\$60,000				
20	Construct Retaining Wall (3ft Height Max)	100	LF	\$300.00	\$30,000	100%	\$30,000				
21	Construct Retaining Wall (6ft Height Max)	90	EA	\$600.00	\$54,000	100%	\$54,000				
22	Reconstruct ADA Accessible Walkway	1	LS	\$25,000.00	\$25,000	100%	\$25,000				
23	Reconstruct Chain Link Sliding Gate	19	EA	\$2,500.00	\$47,500	100%	\$47,500				
24	Reconstruct Wrough Iron Sliding Gate	6	EA	\$5,000.00	\$30,000	100%	\$30,000				
25	Reconstruct Vinyl Fence	200	LF	\$40.00	\$8,000	100%	\$8,000				
26	Remove Class I Tree and Roots	5	EA	\$2,700.00	\$13,500	100%	\$13,500				
27	Remove Class II Tree and Roots	10	EA	\$5,500.00	\$55,000	100%	\$55,000				
28	Remove Class III Tree and Roots	2	EA	\$5,500.00	\$11,000	100%	\$11,000				
29	Remove and Replace Mailbox	44	EA	\$300.00	\$13,200	100%	\$13,200				
30	Adjust Water Meter	17	EA	\$400.00	\$6,800	100%	\$6,800				
31	Relocate Sign & Post	5	EA	\$100.00	\$500	100%	\$500				
32	Install LED Stop Sign	17	EA	\$3,600.00	\$61,200	100%	\$61,200				
33	Install High Visibility Crosswalk	5	EA	\$1,000.00	\$5,000	100%	\$5,000				
34	Storm Drain Lateral and Catch Basin	6	EA	\$32,000.00	\$192,000	100%	\$192,000				
35	Relocate Water Meter	18	EA	\$1,500.00	\$27,000	100%	\$27,000				
36	Relocate Fire Hydrant	6	EA	\$5,300.00	\$31,800	100%	\$31,800				
37	Install Street Light System	1	EA	\$530,800.00	\$530,800	100%	\$530,800				
38	Relocate Power Pole	1	EA	\$7,500.00	\$7,500	100%	\$7,500				
39	Relocate Manhole	1	EA	\$5,000.00	\$5,000	100%	\$5,000				
40	Adjust Manhole to Grade	12	EA	\$750.00	\$9,000	100%	\$9,000				
41	Adjust Gate Valve, ETS Gas Valve, or Cleanout Cover to Grade	25	EA	\$350.00	\$8,750	100%	\$8,750				
42	Relocate Gate Valve, ETS Gas Valve, or Cleanout Cover.	1	EA	\$1,500.00	\$1,500	100%	\$1,500				
43	Unclassified Fill	500	CY	\$50.00	\$25,000	100%	\$25,000				
44	Unclassified Excavation	500	CY	\$53.00	\$26,500	100%	\$26,500				
45	Asphalt Pavement - DGAC (4" Thick Section)	1900	TON	\$125.00	\$237,500	100%	\$237,500				
46	Construct Compacted Aggregate Base Material (7" Thick)	1800	CY	\$45.00	\$81,000	100%	\$81,000				
47	Cold Milling, Type CM3 (Full Depth Removal)	73300	SF	\$5.50	\$403,150	100%	\$403,150				
48	Construct Asphalt Rubber Aggregate Membrane	82525	SF	\$0.60	\$49,515	100%	\$49,515				
49	Removal of Unsuitable Base and/or Subgrade Material	1800	CY	\$55.00	\$99,000	100%	\$99,000				
50								100%			
51								100%			
52								100%			
Subtotal of Construction Items:					\$4,072,275		\$4,072,275				
		Construction Item Contingencies (% of Construction Items):			15.00%	\$610,841	\$610,841				
Total (Construction Items & Contingencies) cost:					\$4,683,116		\$4,683,116				

Project Delivery Costs:	
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Type of Project Cost		Cost \$		
Preliminary Engineering (PE)			ATP Eligible Costs	Non-participating Costs
Environmental Studies and Permits(PA&ED):	\$	159,000	\$159,000	
Plans, Specifications and Estimates (PS&E):	\$	795,000	\$795,000	"PE" costs / "CON" costs
Total PE:	\$	954,000	\$954,000	20% 25% Max
Right of Way (RW)				
Right of Way Engineering:	\$	240,000	\$240,000	
Acquisitions and Utilities:	\$	1,160,000	\$1,160,000	

Detailed Project Estimate and Total Project Costs- Cycle 6											
Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).											
Project Information:											
Agency:		City of Riverside					Date:			1/24/2025	
Project Description:		Mitchell Avenue Trail									
Project Location:		Mitchell Avneue between Campbell Ave and Bushnell Ave									
Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate:		Edward Lara					License #:		63753		
Total RW:		\$		1,400,000		\$1,400,000					
Total Pre-Construction Costs (PE+RW):				\$2,354,000		\$2,354,000					
Construction Engineering (CE)										"CE" costs / "CON" costs	
Construction Engineering (CE):		\$		159,000		\$159,000				3% 15% Max	
Total Construction Costs:				\$4,842,116		\$4,842,116					
Total Project Cost:				\$7,196,116		\$7,196,116				ATP Eligible Costs Non-participating Costs	
Documentation of Ineligible (Non-Participating) Costs:											
The Engineer's logic and/or calculations for splitting costs between ATP-Eligible and Non-participating costs must be documented in this section of the Estimate form.											
Separate logic is required for each item which is partly ineligible for ATP funding or is required for the construction of an ineligible item/element of the project.											
Item #:	Description of Engineer's Logic: (See examples shown in the Instructions)										
40-49	These items are intended to be eligible, a small amount of AC work is required to install the new sidewalk; vehicle capacity will not be added										

ATTACHMENT 8

EXISTING PHOTOGRAPHS



Looking North Existing sidepath on Mitchell Avenue just north of Campbell Avenue. The northern segment of the existing path runs north from Campbell Avenue north to Arlington Avenue.



Looking south on Mitchell Avenue from the Campbell Avenue intersection. The first sidepath gap extends 0.37 miles south from Campbell Avenue south to Gramercy Place. Sidewalks are inconsistent, and mostly absent.



Looking south on Mitchell Avenue. Some homeowners have added asphalt along the street for parking. Fencing is set back to the property line in most instances.



Looking south on Mitchell Avenue towards the intersection with Gramercy Place.



Looking at the north east corner of Mitchell Avenue and Gramercy Place. This will be the proposed location of a curb ramp.



Looking at the south east corner of Mitchell Avenue and Gramercy Place, adjacent to La Sierra Park. This will be the proposed location of a curb ramp and the continuation of the sidepath gap closure towards Bushnell Avenue.



Looking south on Mitchell Avenue. The stretch between Gramercy Place and Norwood Avenue has sidewalk, curb and gutter on the east side in some places.



Looking at the north east corner from the intersection of Mitchell Avenue and Norwood Avenue. Improvements to the sidewalk or curb ramp would not be need at this location.



Looking at the south east corner from the intersection of Mitchell Avenue and Norwood Avenue. Improvements to the sidewalk or curb ramp would not be need at this location.



Looking at the south on Mitchell Ave towards Bushnell Avenue. A portion of the east side of Mitchell Avenue will require no sidewalk or curb and gutter improvements as they have already been built out. The rest of the stretch will require improvements.



Looking at the north east corner from the intersection of Mitchell Avenue and Bushnell Avenue. This would be the extent of this projects improvements.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 4.18, Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: ACTIVE TRANSPORTATION PROGRAM – PROJECT SCOPE
AMENDMENT AND SEGMENTING – DELAWARE STREET SAFE ROUTES TO
SCHOOL CORRIDOR PROJECT
RESOLUTION ATP-P-2425-11

ISSUE:

Should the California Transportation Commission (Commission) approve a project scope amendment for the Cycle 5 Active Transportation Program (ATP) Delaware Street Safe Routes to School Corridor project (PPNO 2353), in San Mateo County?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve this project scope amendment for the Cycle 5 ATP Delaware Street Safe Routes to School Corridor project (PPNO 2353), in San Mateo County.

DISCUSSION:

In June 2019, the Commission approved the Metropolitan Planning Organization component of the 2019 ATP, which included \$1,661,000 in funding for the Delaware Street Safe Routes to School Corridor project (PPNO 2353), in San Mateo County. This project will replace existing underutilized Class II bicycle lanes with 0.7 miles of Class IV facilities, 0.35 miles of bicycle boulevard, crossing treatments at intersections, upgraded pedestrian facilities, and connectivity to the City of San Mateo's (City) existing and planned bicycle facilities for a fully connected portion of the planned bicycle network.

The desired outcome of the project is to encourage more bicycling activity in an area where currently only the most confident cyclists ride. This project will allow users of all ages and abilities to travel north-south through the City, and will serve nearly 1,000 elementary school students who otherwise do not have a low-stress bicycle option to access their schools.

Additionally, at the June 2025 Commission meeting, the City is requesting a project allocation for the Construction phase.

ANALYSIS:

The Department's analysis has deemed the City's request to be a significant scope change due to the segmenting of the proposed work.

The revised scope includes the addition of intersection timing changes at four intersections on Delaware Street, adding a Rectangular Rapid Flashing Beacon, and crosswalk improvements at one intersection. The proposed segmenting will move the proposed work onto a Department-administered project, that will connect this project to future Class IV facilities. The removal of the segmenting portion will save the project \$4,000 and the additional improvements will cost \$183,000, which will be covered by the City.

ANALYSIS RECOMMENDATION:

Based on the Commission's scope change and segmenting guidance, as well as the analysis of the proposed scope changes, the Department has determined that this scope change has the potential to increase overall active transportation users of the project as compared to the original scope. Therefore, the Department supports the proposed scope change for this project.

BACKGROUND:

Resolution G-16-29 amended the ATP Guidelines to stipulate that any agency implementing an ATP project, present scope changes to the Department for consideration prior to allocation. Therefore, the Department will make a recommendation to the Commission for final approval with the understanding that scope changes that result in a decrease in active transportation benefits may result in removal from the program.

Attachments

Project Scope Change and Segmenting Request Caltrans' Analysis and Recommendations

Re-Submittal Date: April 17, 2025

PROJECT NAME: Delaware Street Safe Routes to School Corridor

IMPLEMENTING AGENCY: City of San Mateo

ATP ID: ATP5-04-107M

FEDERAL PROJECT NO.: 5102(053)

PPNO: 04-2353

DATE OF AGENCY/CT COORDINATION MEETING:

FIELD REVIEW DATE (major only): N/A

APPROVED PROJECT DESCRIPTION: PS&E, ROW, CON funding for design/construction of Class IV separated bike lanes and bicycle boulevard, upgrade pedestrian facilities, and connections to existing facilities.

Purpose

This document serves as supplemental information to the Exhibit 25-D REQUEST FOR PROJECT SCOPE CHANGE and SEGMENTING (attached) completed by City of San Mateo and submitted to Caltrans on 4/16//2025.

Caltrans' Recommendation(s)

As a result of Caltrans' review of the City of San Mateo Scope Change Request documentation and subsequent discussion(s) with the City of San Mateo staff, Caltrans recommends the following action:

APPROVE AS A MAJOR SCOPE CHANGE

Scope to Be Changed

The following is a numbered list of proposed scope changes for [Phase 1](#):

1. At Bermuda Dr, Saratoga Dr, 25th Ave and 28th Ave
 - Add intersection timing changes to 4 intersections
2. At Delaware St and Pacific Blvd intersection
 - Add Rectangular Rapid Flashing Beacon (RRFB) and crosswalk improvements

The following is a numbered list of proposed scope changes for [Phase 2](#):

1. At intersection of 19th Ave and S Delaware St (As part of the segmenting request)
 - Exchange green conflict striping through the intersection to a future non-motorized intersection redesign as part of S101/SR92 Fashion Island Blvd Mobility Hub, a joint project with San Mateo County Transportation Authority (SMCTA) ([LINK](#)), which is scheduled to break ground in summer of 2026. Currently the proposed intersection improvement will include making the intersection a protected intersection.
2. At south approach leg of 19th Ave and S Delaware Street intersection
 - Exchange Class IV bike facility at the south approach to the intersection for a future Class IV facility as part of the joint project. This scope change would also prevent significant rework from having to occur for the Delaware Safe Routes to School (SRTS) project.

Reason for the Scope Change

Phase 1

Change 1- the agency is utilizing local funds to determine timing improvements/signal improvement needs along the entire project corridor.

Change 2- the agency wants to add ATP enhancements to the intersection.

Phase 2

Change 1 and 2- the proposed work is inside Caltrans ROW. The intersection shall instead be redesigned as a part of a joint project with SMCTA, which will likely include a fully protected intersection (at 19th/Delaware) as well as efforts to tie-in future Class IV facilities to the Delaware project. This scope change would also prevent significant rework from having to occur for the Delaware SRTS project.

Summary of Caltrans Analysis

Caltrans supports this request for the following reasons:

Proposed scope change's affect to benefits (Potential of the project to increase walking and biking as compared to the approved scope):

Temporary Decrease- The proposed scope change is expected to have the following impact(s) on the project's potential to increase walking and biking as compared to the original scope.

Phase 1

Change 1 and 2- The signal timing/safety improvements can encourage more users through improved travel times and safety, which can increase the potential of the project to increase walking and biking.

Phase 2

Change 1 and 2- For the purposes of this scope change, the removal of these improvements will decrease the potential for the project to increase walking and biking until the completion of the segmented S101/SR92 Fashion Island Blvd Mobility Hub project. The inclusion of work at S. Delaware and 19th Ave into the future Caltrans/SMCTA project will increase the potential to increase walking and biking, in the long term.

Proposed scope change's affect to benefits (Potential of the project to increase safety of pedestrians and bicyclists walking biking as compared to the approved scope):

Temporary Decrease - The proposed scope change is expected to have the following impact(s) on the overall safety of pedestrians and bicyclists as compared to the original scope.

Phase 1

Change 1 and 2- The intersection timing improvements and added RRFB will make intersection operations safer for non-motorized users.

Phase 2

Change 1 and 2- For the purposes of this scope change, the removal of these improvements will decrease the project's potential to increase safety of pedestrians and bicyclists. The completion of the segmented S101/SR92 Fashion Island Blvd Mobility Hub work at S. Delaware and 19th Ave in the future Caltrans/SMCTA project will make safer improvements to the intersection, in the long term.

Additional Comments

Caltrans concurs with the information provided in Attachment 1- Exhibit 25-D REQUEST FOR PROJECT SCOPE CHANGE. The agency has coordinated with Caltrans staff to provide the most accurate information possible.

Caltrans' Coordination with Requesting Agency

The Scope change request appeared straight forward and driven by the safety needs of non-motorized users; therefor, no meetings were scheduled for this scope change request.

Impact to Project Cost

Project scope changes would have a net increase on the overall cost of the project. All increases will be absorbed by local City funds (\$1,500,000).

The City's documentation shows the project savings of removing 200 LF @ \$20/LF to be approximately \$4,000. Which Caltrans does not recommend decreasing from the project's funding.

The other project additions, which appear to be fully ATP eligible, total \$182,700; which the agency intends to fund with local funding.

Impact to Project Schedule

The Construction funds are programmed in 24/25. The current Commission construction allocation request date is shown as 5/15/2025.

ATTACHMENTS

1. Exhibit 25-D REQUEST FOR PROJECT SCOPE CHANGE and SEGMENTATION- Completed by Agency
2. Original plans with changes redlined
3. Original Detailed Engineer's Estimate with changes redlined
4. MPO approval of the proposed changes

Exhibit 25-D: Request for Project Scope Change (Local ATP Projects)

The Request for Project Scope Change (Local ATP Projects) form, Exhibit 25-D, is used when the implementing agency is unable to complete the approved project scope as programmed. This Project Scope Change Request Form is intended to support the California Transportation Commission (CTC) "Policy on Project Amendments and Advance Project Allocations", adopted on August 18, 2016.

Prior to filling out this form the implementing agency is expected to:

- 1) Read the instructions, Exhibit 25-D1: Instructions for Request for Project Scope Change, and become familiar with all required elements of requests for scope changes
- 2) Hold a coordination meeting/call with Caltrans District and HQ ATP managers. See instructions for more details.



To: Xi Zhang, Teresa Mcwilliam	Date: 4/16/2025
District Local Assistance Engineer	PPNO: 2353
Caltrans, Office of Local Assistance	Federal PROJECT #: 5102(053)
California Department of Transportation	ATP ID #: ATP5-04-107M
District 4 111 Grand Ave	
Oakland, CA 94612	

Project Name (Per CTC programming): *Delaware Street Safe Routes to School Corridor*

Approved Project Description (As submitted in Application): *The Delaware Street Safe Routes to School Corridor will replace existing underutilized Class II bicycle lanes with .7 miles of Class IV facilities, and add .35 miles of bicycle boulevard in addition to crossing treatments at intersections, upgraded pedestrian facilities, and connectivity to the City's existing and planned bicycle facilities for a fully connected portion of the planned bicycle network.*

Approved Project Limits (As submitted in Application): *The project extents are South Delaware Street from 19th Avenue to Pacific Boulevard.*

☐ **For Federally Funded Projects:** Written MPO Concurrence (see Item #7)

Current FTIP/FSTIP Description: _____

Current FTIP/FSTIP Limits: _____

Dear Xi and Teresa:

Consistent with the California Transportation Commission's (CTC) "Policy on Project Amendments and Advance Project Allocations", adopted on August 18, 2016, we request that Caltrans consider, and the CTC approve the proposed Project Amendments as documented below:

REQUIRED ELEMENTS (PER CTC'S PROJECT AMENDMENT POLICY):**1) An explanation of the proposed scope change;**Phase 1:

Phase 1 will construct the first segment of this project (Segment 1) along the currently approved project alignment. There are minor changes in the approved project scope for Segment 1, as outlined in scope change request within this document. Segment 1 will be delivered as identified in the Approved Project Description, as submitted in the City's original ATP Grant Application; with minor additional scope changes outlined per the below. Construction of this segment will be funded by the ATP funds of \$1,401,000 that are currently programmed for the Construction phase of this project. To ensure the timely use of programmed funds, the City will submit the necessary Construction Allocation Request package or Time Extension Request to the District in time to be voted by the Commission no later than the regularly scheduled June 2025 California Transportation Commission Meeting.

Upon approval of the allocation request, the City will publicly bid and construct Segment 1. It is anticipated that the bidding and construction phase will occur over a period of approximately 6 months through approximately December 2025. Segment 1 will be opened to the public at this time.

Phase 2:

Phase 2 will construct the second, and final, segment of the project (Segment 2) between the intersection of 19th Ave and S Delaware St and the end of Segment 1, thereby completing the full, approved project alignment (as submitted in the application). Funding for Segment 2 will not utilize ATP funding sources. Funding for Segment 2 will pull from multiple sources (as segment 2 is part of a greater project) by the San Mateo County Transportation Authority (TA), 2023 OBAG funding, and City local funds as further described within Section 3 of this request.

Segment 2 will be constructed along the approved project alignment. From the originally proposed alignment at its northerly extent. See Attachment C for the proposed Segment 2 alignment.

Funding for Segment 2 will be provided through a partnership between the TA and the City. We expect that construction of the portion of Segment 2 will be ready for construction by approximately December 2026 and be fully constructed, and open to the public at that time.

Since Segment 2 will not be funded with ATP funds, it is important to note that construction for Segment 2 is anticipated **not to be** completed within the "timely use of funds" limitation that will be stipulated for the use of ATP funds for Segment 1. However, by the time that Segment 1 (December 2025) and Segment 2 (December 2026) are both fully constructed, the entirety of the project seeks to deliver a comprehensive "single" project in substantial conformance with the approved Project Description.

- a) Project scope reduction applies to the portion of the project that falls within Caltrans ROW, specifically at the intersection of S Delaware St and 19th Ave. Project initially proposed green conflict striping as well as new high visibility crosswalks at the subject intersection. Proposed scope change is such that the Delaware Street Safe Routes to School Corridor project will simplify the design south of the intersection (**Segment 1**). The intersection shall instead be redesigned as a part of a [joint project with SMCTA \(LINK\)](#), which will likely include a fully protected intersection (at 19th/Delaware) as well as efforts to tie-in future Class IV facilities to the Delaware

project (**Segment 2**). This scope change would also prevent significant rework from having to occur for the Delaware SRTS project.

- i) *San Mateo has determined that the scope change is minor in nature, as the future, planned improvements in Segment 2 would be far more robust and comprehensive in its approach.*
- b) Project scope additions applies to the entire project corridor, wherein the agency had used local funds to determine timing improvements/signal safety improvement needs at 4 intersections, instead of the 2 that were originally submitted and approved in the application.
 - i) *San Mateo has determined that the scope change is minor in nature and affects CON only (local funds were used for the corridor operations analysis).*
- c) Project scope addition applies to the intersection at Delaware St and Pacific Blvd intersection, where the north leg uncontrolled crossing is now proposed to be enhanced with a RRFB, instead of the 0 (for entire project) that was originally submitted and approved in the application.
 - i) *San Mateo has determined that the scope change is minor in nature.*

2) The reason for the proposed scope change;

- a) The reason for the proposed scope change is due to a conflicting, future project, with the goal of preventing rework of the pavement markings and striping at the subject intersection. This scope change would also result in project cost and time savings for the project.
- b) The reason for the proposed scope change (CON only, see (1)(b)(i)) is due to an understanding that the provision of Class IV bike lanes along this corridor would also require careful design considerations¹ for the City's intersections, meaning that a corridor analysis needed to be performed to be able to understand operational considerations pre/post-project for the existing signalized intersections.
- c) The reason for the proposed scope change is due to a desire to enhance both bike/ped safety of this crossing while providing an option for cyclists headed NB to more easily make the connection to the Hayward Park Caltrain station until the bike lanes associated with the US 101/SR 92 Mobility Hub & Smart Corridor project are implemented.

3) The impact the proposed scope change would have on the overall cost of the project;

*The overall cost of the project (**Segment 1**) is as follows (including all project delivery phases, per (b) and (c) per 100% design costs):*

- ATP funded - \$1,661,000
- Non-ATP funded (CON only, local City funds) ~\$1,500,000

Project scope changes would have a **net increase** on the overall cost of the project. All increases will be absorbed by local City funds (\$1,500,000).

- a) ~200 LF of Class IV bike lane reduction at ~\$20/linear ft (-\$4,000)
- b) Traffic Signal Improvements – Bermuda Dr (\$45,400) + Saratoga Dr (\$102,300)
- c) RRFB Installation – Pacific Blvd (\$35,000)

The overall funding plan by fund sources for the entirety of the future project (which includes **segment 2** as referenced in this document) is as follows:

Fund Source	Planned	Programmed	Allocated	Grand Total
City Match		\$ 138,636	\$ 61,364	\$ 200,000
City Congestion Relief		\$ 300,000		\$ 300,000
2022 TA Ped/Bike		\$ 1,386,364	\$ 613,636	\$ 2,000,000
LPP Formula	\$ 1,516,000			\$ 1,516,000
2025 TA Highway	\$ 9,449,000			\$ 9,449,000
2024 TA Ped/Bike			\$ 2,000,000	\$ 2,000,000
TA Oversight	\$ 401,134			\$ 401,134
2023 OBAG			\$ 3,375,000	\$ 3,375,000
2024 CPFCDs			\$ 500,000	\$ 500,000
Grand Totals	\$ 11,366,134	\$ 1,825,000	\$ 6,550,000	\$ 19,741,134

- 4) An estimate of the impact the proposed scope change would have on the potential of the project to increase walking and bicycling as compared to the benefits identified in the project application (increase or decrease in benefit);

Location/Element	Approved Scope	Proposed Change	Change in Walking/Bicycling Benefit
At intersection of 19 th Ave and S Delaware St	Green conflict striping through intersection and crosswalk upgrades to high-vis type	Remove high-vis crosswalks, or green conflict striping from scope due to future project at intersection.	Decrease
At south approach/south leg of 19 th Ave and S Delaware St intersection	Class IV bike facility at the south approach or NB approach direction of intersection.	Terminate Class IV south of intersection and implement in-kind class II bike lanes within Caltrans ROW. Future project will redesign and connect to Class IV facility at termination point.	Decrease
Intersection timing changes at Bermuda Dr, Saratoga Dr, 25 th Ave, and 28 th Ave	2 intersections	4 intersections	Increase
Delaware St & Pacific Blvd intersection / RRFB Installation + crosswalk improvements	0/none	1 intersection	Increase

- 5) An estimate of the impact the proposed scope change would have on the potential of the project to increase the safety of pedestrians and bicyclists as compared to the benefits identified in the project application (increase or decrease in benefit);

Location/Element	Approved Scope	Proposed Change	Change in Safety Benefit
At intersection of 19 th Ave and S Delaware St	Green conflict striping through intersection and crosswalk upgrades to high-vis type	Remove high-vis crosswalks, or green conflict striping from scope due to future project at intersection.	Decrease
At south approach/south leg of 19 th Ave and S Delaware St intersection	Class IV bike facility at the south approach or NB approach direction of intersection.	Terminate Class IV south of intersection and implement in-kind class II bike lanes within Caltrans ROW. Future project will redesign and connect to Class IV facility at termination point.	Decrease
Intersection timing changes at Bermuda Dr, Saratoga Dr, 25 th Ave, and 28 th Ave	2 intersections	4 intersections	Increase
Delaware St & Pacific Blvd intersection / RRFB Installation + crosswalk improvements	0	1 intersection	Increase

6) An explanation of the methodology used to develop the aforementioned estimates; and

The methodology used to develop the changes in safety and walking/bicycling benefits stem from the San Mateo's Bicycle Master Plan 2020, where Delaware Street was identified as a major north-south connector providing bike access to key community destinations. And proposed project scope reductions were assumed to have a decrease in benefits, whereas proposed scope additions were assumed to have an increase in benefits as follows:

[scope reduction] Decrease in benefits: a reduction in Class IV facility length, conflict striping, and crosswalk upgrades is likely to discourage users from being able to make connections to other bike facilities while also having potential safety impacts due to existing, faded striping at the Caltrans/City intersection at 19th Ave.

[scope additions] Increase in benefits: The intersection timing/phasing changes to 2 additional intersections as well as the RRFB installation is likely to encourage more users to make connections to other, existing bike facilities, transit options, and other land uses. San Mateo has considered several design guidances (Caltrans/NACTO) to ensure that the implementations will bring a net safety benefit in post project conditions, which will likely also encourage users of all ages and abilities to use the new facilities.

7) For projects programmed in the Metropolitan Planning Organization (MPO) component, evidence of MPO approval and the MPO rationale for their approval.

N/A – Project funded in the Statewide component.

ADDITIONAL QUESTIONS:

8) Does this scope change require revalidation of your environmental document? (No)

If yes, what is the actual/estimated date of revalidation? _____

Reason being that the categorical exemption for this project still stands despite the minor project scope reduction being proposed. Categorical exemption CEQA Sec. 15301 and PRC 21080.25 (SB288) reference

9) Explain the additional public outreach efforts you have made with respect to this proposed scope change and provide a summary of the public response to these efforts:

Public outreach efforts have been made with these scope changes already in mind in the earlier design changes. City performed 2 pop up outreach events, school related outreach (SMFCSD, Nueva School), public website with design focused videos, door to door canvassing.

City will also plan to conduct follow up outreach efforts prior to project's anticipated CTC meeting in June 2025 for CON funding.

Future, joint SMCTA project that will seek to connect to the Delaware Street Safe Routes to School Corridor project will also be performing its separate outreach efforts that are led by SMCTA.

REQUIRED Attachments: (check boxes of attached required documents)

- | | |
|--|---|
| <input checked="" type="checkbox"/> Original plans/workplan with changes highlighted | <input checked="" type="checkbox"/> Revised Detailed Engineer's Est. |
| <input checked="" type="checkbox"/> Revised plans/workplan | <input checked="" type="checkbox"/> Written MPO Concurrence (see Item #7) |
| <input checked="" type="checkbox"/> Original Detailed Engineer's Est. with changes highlighted | <input type="checkbox"/> Additional Revised Application Documentation |

Required revisions to the Project's Description and/or Limits:

The proposed Project Amendments documented above will require the following changes to the Project's Description and/or Limits: *N/A*

Proposed changes to the Project Description: *N/A*

Proposed changes to the Project Limits: *N/A*

For Federally Funded Projects: *N/A*

Proposed changes to the FTIP/FSTIP Description: _____

Proposed changes to the FTIP/FSTIP Limits: _____

Project Delivery Status:

The following is a side-by-side comparison of the original project schedule and the current project schedule. The explanations for each milestone date change is listed below:

Original CTC Allocation Dates: (as programmed by the CTC when the application was approved for funding):

PA&ED: FY 21/22 PS&E: 1/26/23 R/W: 5/17/2024 CON: FY24/25

Actual/Currently Anticipated CTC Allocation Dates: (at the time of this request)

PA&ED: 4/19/2022 PS&E: 1/26/23 R/W: 5/17/2024 CON: 6/26/2025

Explanation for milestone changes: << Only list/explain the Allocation milestones that have changed >>

Local Agency Certification:

This Request for Scope Change has been prepared in accordance with the *Procedures for Administering Local Projects in the Active Transportation Program (ATP)*. I certify that the information provided in the document is accurate and correct. I understand that if the required information has not been provided this form will be returned and the request may be delayed. Please advise us as soon as the scope change has been approved.

You may direct any questions to:

Sang Hee Cho at (650) 522-7348
(name) (phone number)

Signature: _____ Title: Associate Engineer Date: 4/16/2025

Digitally signed by Sang Hee Cho
DN: E=scho@cityofsanmateo.org,
OU=Sang Hee Cho, OU=Users,
OU=Public Works, DC=cityhall, DC=terra,
DC=pl, DC=sanmateo, DC=ca, DC=us
Date: 2025.04.16 10:23:30-0700

Agency/Commission: City of San Mateo

Attachments: 100% cost estimate highlighted, 100% plans highlighted, Grant application plans + estimate, MTC concurrence letter

Distribution:

- (1) Original -DLAE
- (2) Copy – Division of Local Assistance, Headquarters ATP Program Manager
- (3) Copy – MPO/RTPA/County Transportation Commission

NO PART OF THIS DOCUMENT MAY BE REPRODUCED IN ANY FORM INCLUDING PHOTOCOPY, RECORDING OR ANY INFORMATION RETRIEVABLE AND STORAGE SYSTEM, WITHOUT PERMISSION IN WRITING FROM SANDIS.

SAWCUT AND CONFORM LINE
RETAINING WALL
A.C. PAVEMENT
CONC. VALLEY GUTTER
CONC. SIDEWALK OR PAD
6" CURB & GUTTER
EDGE OF A.C. PAVEMENT
6" VERTICAL CURB
CENTER LINE
SANITARY SEWER MAIN
STORM DRAIN MAIN
WATER MAIN
IRRIGATION LINE
FLOW LINE
CHAIN LINK FENCE
GAS MAIN
ELECTRIC AND SIGNAL DUCT BANK
COMMUNICATION LINE
STREET LIGHT CONDUIT
CONTOUR ELEVATION LINE
SPOT ELEVATION
DIRECTION OF SLOPE
GAS METER
GAS VALVE
WATER METER
WATER VALVE
FIRE HYDRANT
POST INDICATOR VALVE
FIRE DEPARTMENT CONNECTION

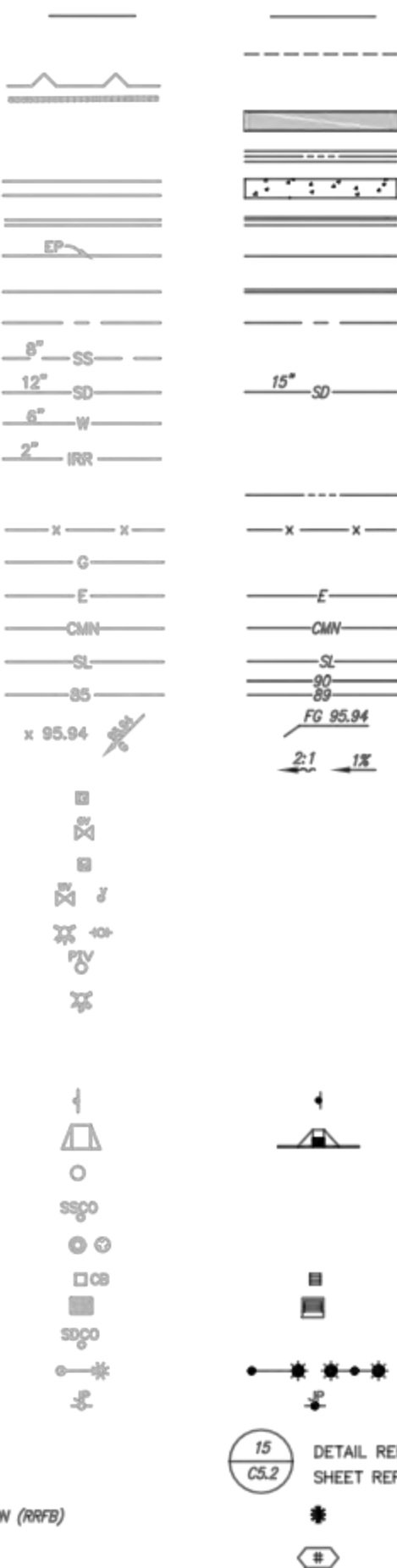
SIGN
ACCESSIBLE RAMP
SANITARY SEWER MANHOLE
SANITARY SEWER CLEANOUT
STORM DRAIN MANHOLE
STORM DRAIN CATCH BASIN
STORM DRAIN CURB INLET
STORM DRAIN CLEANOUT
ELECTROLIER
JOINT POLE

CONSTRUCTION DETAIL REFERENCE

RECTANGULAR REFLECTIVE FLASHING BEACON (RRFB)

STRIPING DETAIL #

PROPOSED



PLANS FOR DELAWARE STREET BICYCLE IMPROVEMENTS PROJECT CITY OF SAN MATEO, SAN MATEO COUNTY, CALIFORNIA CITY PROJECT NO. <46R022> MTC PROJECT NO. <ATPL-5102(053)>

ABBREVIATIONS

AB	- AGGREGATE BASE	HP	- HIGH POINT
AC	- ASPHALT CONCRETE	INV	- INVERT ELEVATION
AD	- AREA DRAIN	JP	- JOINT POLE
ADA	- AMERICANS WITH DISABILITIES ACT	JT	- JOINT TRENCH
ASB	- AGGREGATE SUBBASE	LP	- LOW POINT
BC	- BEGINNING OF CURVE	LP	- LOW POINT
BFP	- BACK FLOW PREVENTOR	MAX	- MAXIMUM
BLDG	- BUILDING	MEP	- MECHANICAL/ELECTRICAL/PLUMBING
BOS	- BOTTOM OF STEP	MH	- MANHOLE
BOW	- FG @ BOTTOM OF WALL	MIN	- MINIMUM
BVC	- BEGIN VERTICAL CURVE	MON	- MONUMENT
BW	- BACK OF WALK	N	- NORTH
C	- CONCRETE OR CIVIL	N.L.C.	- NOT IN CONTRACT
C&G	- CURB AND GUTTER	NO	- NUMBER
CB	- CATCH BASIN	NTS	- NOT TO SCALE
CIP	- CAST IRON PIPE	P	- PAVEMENT ELEVATION
CL	- CENTER LINE OR CLASS	PCC	- PORTLAND CEMENT CONCRETE /
CMP	- CORRUGATED METAL PIPE		POINT OF CONTINUOUS CURVATURE
CO	- CLEANOUT	PIV	- POST INDICATOR VALVE
CONC	- CONCRETE	PL	- PROPERTY LINE
CONST	- CONSTRUCTION OR CONSTRUCT	POC	- POINT ON CURVE
CW	- CROSSWALK	PP	- POWER POLE
CY	- CUBIC YARD	PVC	- POLYVINYL CHLORIDE PIPE
DIP	- DUCTILE IRON PIPE	R	- RADIUS
DOM	- DOMESTIC	RC	- RELATIVE COMPACTION
DW	- DOMESTIC WATER	RCP	- REINFORCED CONCRETE PIPE
DWG	- DRAWING	R/W	- RIGHT OF WAY
E	- EAST	S	- SLOPE OR SOUTH
EC	- END OF CURVE	SD	- STORM DRAIN
EP	- EDGE OF PAVEMENT	SF	- SILT FENCE
ER	- END OF RETURN	SG	- SUBGRADE
EVC	- END VERTICAL CURVE	SS	- SANITARY SEWER
ELEV	- ELEVATION	STA	- STATION
EX., EXIST.	- EXISTING	STD	- STANDARD
FC	- FACE OF CURB	S/W	- SIDEWALK
FDC	- FIRE DEPARTMENT CONNECTION	TC	- TOP OF CURB
FF	- FINISHED FLOOR	TD	- TRENCH DRAIN
FG	- FINISHED GRADE	TOE	- TOE OF SLOPE
FH	- FIRE HYDRANT	TOS	- TOP OF STAIR
FL	- FLOW LINE	TOW	- TOP OF WALL
FOUND	- FOUNDATION	TYP	- TYPICAL
FS	- FINISHED SURFACE	U/G	- UNDERGROUND
FT	- FOOT	VC	- VERTICAL CURVE
FW	- FIRE WATER	WM	- WATER METER
G	- GROUND ELEVATION	WV	- WATER VALVE
GB	- GRADE BREAK	W	- WEST
GV	- GATE VALVE	W/	- WITH
HCR	- ACCESSIBLE RAMP		

CIVIL SHEET INDEX

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4	L-2	LAYOUT PLAN
5	L-3	LAYOUT PLAN
6	L-4	LAYOUT PLAN
7	L-5	LAYOUT PLAN
8	L-6	LAYOUT PLAN
9	GD-1	GRADING PLAN
10	GD-2	GRADING PLAN
11	GD-3	GRADING PLAN
12	GD-4	GRADING PLAN
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23	SS-9	28TH AVE BUS STOP STRIPING DETAIL
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36	BMP-1	BEST MANAGEMENT PRACTICES

PROJECT DESCRIPTION

THIS PROJECT INCLUDES CLASS IV BIKE LANE IMPROVEMENTS, TRAFFIC CALMING, PAVEMENT REHABILITATION, AND TRAFFIC SIGNAL MODIFICATIONS ALONG S DELAWARE ST. FROM 19TH AVE. TO SOUTHERN TERMINUS OF DELAWARE ST.

BENCHMARK

THE VERTICAL DATUM FOR THIS SURVEY IS A CITY OF SAN MATEO BENCHMARK, BM ID E984, LOCATED AT TOP OF CURB NORTHWESTERLY END OF HILLSDALE OC ON PACIFIC BLVD.

ELEVATION = 26.705 FEET (CITY OF SAN MATEO DATUM)

BASIS OF BEARINGS

THE BEARING SHOWN AS N 34°06'01" W HEREON WAS BASED ON RECORD OF SURVEY, FILED IN BOOK 139 OF MAPS, PAGE 11, SAN MATEO COUNTY RECORDS.

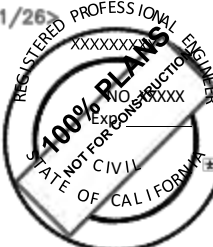
UNAUTHORIZED CHANGES AND USES

CAUTION: THE ENGINEER PREPARING THESE PLANS WILL NOT BE RESPONSIBLE FOR, OR LIABLE FOR, UNAUTHORIZED CHANGES TO OR USES OF THESE PLANS. ALL CHANGES TO THE PLANS MUST BE IN WRITING AND MUST BE APPROVED BY THE PREPARER OF THE PLANS.



PREPARED UNDER THE DIRECTION OF

<RONALD EDWARD SANZO>
R.C.E. <79305>, EXPIRES <03/31/26>
<SANDIS CIVIL ENGINEERS>



CITY OF SAN MATEO RELEASE FOR CONSTRUCTION

THE CITY OF SAN MATEO HEREBY ACCEPTS THESE PLANS FOR CONSTRUCTION, AS BEING IN GENERAL COMPLIANCE WITH PLAN PREPARATION REQUIREMENTS OF THIS GOVERNMENT. RESPONSIBILITY FOR THE COMPLETENESS AND ACCURACY OF THE PLANS AND RELATED DESIGNS RESIDES WITH THE ENGINEER AND ENGINEERING FIRM OF RECORD.

MATT FABRY
PUBLIC WORKS DIRECTOR
CITY OF SAN MATEO

KATHERINE SHEEHAN
PUBLIC WORKS DEPUTY DIRECTOR
CITY OF SAN MATEO

JAY YU
ENGINEERING MANAGER
CITY OF SAN MATEO

BETHANY LOPEZ
SENIOR ENGINEER
CITY OF SAN MATEO



NOT TO SCALE



NOT TO SCALE

REV NO	DESCRIPTION	DATE
1		
2		
3		
4		
5		



COVER SHEET
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022



DESIGNED BY: JP	DATE:
DRAWN BY: BP TS	DATE:
QC CHECKED BY: RES	DATE:
PROJECT NO. 03071	DATE:
SCALE:	DATE:
SUBMITAL 605	DATE:

SHEET 1 OF 36

- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SAN MATEO AND THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, 2018 EDITION.
- CONTRACTOR SHALL LEAVE AN EMERGENCY PHONE NUMBER WITH THE CITY OF SAN MATEO POLICE AND FIRE DEPARTMENTS.
- CONTRACTOR SHALL POST ON THE SITE, EMERGENCY TELEPHONE NUMBERS FOR CONTRACTOR, PUBLIC WORKS, AMBULANCE, POLICE, AND FIRE DEPARTMENTS.
- CONTRACTOR IS RESPONSIBLE FOR THE INTENT OF THESE PLANS AND SHALL REPORT ANY DISCREPANCIES FOUND IN THEM TO THE ENGINEER PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL CONFINE THEIR OPERATIONS AND ACTIVITIES TO WITHIN THE PROJECT LIMITS, CONSISTING OF ROAD RIGHT OF WAY, RIGHTS OF ENTRY AND/OR PROJECT CONFORMS, AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER.
- ALL MATERIALS AND WORKMANSHIP SHALL BE IN CONFORMANCE WITH THE STANDARD PLANS, SPECIFICATIONS AND REQUIREMENTS SET FORTH IN THE CITY OF SAN MATEO'S ENGINEERING STANDARDS OR AS NOTED OTHERWISE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR/ REPLACING DAMAGED UTILITIES, UNDERGROUND FACILITIES, CURB, SIDEWALK AND PAVEMENT RESULTING FROM CONSTRUCTION AND LANDSCAPING OPERATIONS.
- CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL EXISTING GRADES.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES SHOWN OR NOT WITH THE APPROPRIATE UTILITY AGENCIES PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. CONTRACTOR SHALL NOTIFY ALL PUBLIC AND PRIVATE UTILITY OWNERS 48 HOURS PRIOR TO COMMENCEMENT OF WORK ADJACENT TO THE UTILITY. CONTACT SERVICE ALERT (USA) AT 800-642-2444.
- ALL REVISIONS TO THESE PLANS MUST BE REVIEWED AND APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION AND SHALL BE INCORPORATED INTO THE DESIGN DOCUMENTS BY THE DESIGNER WITH THE CITY ENGINEER'S APPROVAL PRIOR TO THE INSTALLATION OF THE IMPROVEMENTS UNLESS DIRECTED OTHERWISE BY THE CITY ENGINEER.
- TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE THE CONTRACTOR'S RESPONSIBILITY. ALL TRAFFIC CONTROL AND DEVICES SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD), LATEST SUPPLEMENT EDITION.
- THE CONTRACTOR SHALL PROVIDE FOR CONTINUOUS INGRESS AND EGRESS TO ALL PUBLIC AND PRIVATE PROPERTIES ADJACENT TO THE WORK THROUGHOUT THE PERIOD OF CONSTRUCTION UNLESS APPROVED BY THE ENGINEER.
- EXCAVATION AND RECONSTRUCTION OF THE PAVEMENT MUST NOT EXCEED 500 LINEAR FEET AT ANY TIME. TO MINIMIZE INTERRUPTION TO THE RESIDENTS AND PUBLIC, THE CONSTRUCTION LIMITS OF PLACEMENT OF THE FINAL ASPHALT CONCRETE PAVEMENT LIFT CAN BE EXPANDED TO A FULL STREET BLOCK IF APPROVED BY THE CITY. BACKFILL ALL EXCAVATED & RECONSTRUCTED AREAS TO WITHIN 2 INCHES FROM ADJACENT GRADE BY END OF WORKING DAY.
- MATCH EXISTING STREETS, SURROUNDING LANDSCAPE, AND ALL OTHER EXISTING CONDITIONS WITH A SMOOTH TRANSITION IN PAVING, CURBS, GUTTERS, SIDEWALKS, GRADING, ETC., AND TO AVOID ANY ABRUPT OR APPARENT CHANGES IN GRADES OR CROSS SLOPES, LOW SPORTS AND HAZARDOUS CONDITIONS. PAVING CONFORMS MUST BE MADE AT A SMOOTHLY TRIMMED BUTT JOINT. DO NOT OVERLAP EXISTING PAVEMENT.
- NOTIFY THE CITY'S PUBLIC WORKS DEPARTMENT AT LEAST 72 HOURS PRIOR TO CUTTING AND OR REMOVING ANY TREE ROOTS LARGER THAN 1 INCH IN DIAMETER TO ALLOW CITY ARBORIST TO INSPECT AND MAKE RECOMMENDATIONS.
- HAUL ROUTES ARE ALLOWED ONLY ON COORDINATED STREETS AND MUST BE PRE-APPROVED BY THE CITY.
- NOTIFY ALL TRANSIT AGENCIES AND TRASH COLLECTION AND EMERGENCY SERVICES IN ADVANCE OF THE GRINDING AND PAVING SCHEDULES AND ALLOW FOR COORDINATION WITH THESE AGENCIES IN THE CONSTRUCTION SCHEDULE.
- RIGHT OF WAY IS APPROXIMATE FROM CITY RECORD DATA.

DEMOLITION NOTES

- CONTRACTOR IS TO COMPLY WITH ALL GENERAL AND STATE REQUIREMENTS INVOLVING THE REMOVAL AND DISPOSAL OF HAZARDOUS MATERIAL(S).
- CONTRACTOR'S BID IS TO INCLUDE ALL VISIBLE SURFACE AND ALL SUBSURFACE FEATURES IDENTIFIED TO BE REMOVED OR ABANDONED IN THESE DOCUMENTS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR A SITE INSPECTION TO FULLY ACKNOWLEDGE THE EXTENT OF THE DEMOLITION WORK.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY AND ALL PERMITS NECESSARY FOR ENCROACHMENT, GRADING, DEMOLITION, AND DISPOSAL OF SAID MATERIALS AS REQUIRED BY PRIVATE, LOCAL AND STATE JURISDICTIONS. THE CONTRACTOR SHALL PAY ALL FEES ASSOCIATED WITH THE DEMOLITION WORK.
- BACKFILL ALL DEPRESSIONS AND TRENCHES FROM DEMOLITION TO THE SATISFACTION OF THE GEOTECHNICAL ENGINEER.
- REMOVAL OF LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIALS TO THE SATISFACTION OF THE GEOTECHNICAL ENGINEER.
- PRIOR TO BEGINNING DEMOLITION WORK ACTIVITIES, CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES OUTLINED IN THE EROSION CONTROL PLAN & DETAILS AND THE PROJECT SWPPP IF APPLICABLE.
- THE CONTRACTOR SHALL MAINTAIN ALL SAFETY DEVICES, AND SHALL BE RESPONSIBLE FOR CONFORMANCE TO ALL LOCAL, STATE AND FEDERAL SAFETY AND HEALTH STANDARDS LAWS AND REGULATIONS.
- THE CONTRACTOR SHALL PROTECT FROM DAMAGE ALL EXISTING IMPROVEMENTS FACILITIES AND STRUCTURES WHICH ARE TO REMAIN. ANY ITEMS DAMAGED BY THE CONTRACTOR OR THEIR AGENTS OR ANY ITEMS REMOVED FOR HIS USE SHALL BE REPLACED IN EQUAL OR BETTER CONDITION AS APPROVED BY THE ARCHITECT OR OWNER'S REPRESENTATIVE.
- THIS PLAN IS NOT INTENDED TO BE A COMPLETE CATALOGUE OF ALL EXISTING STRUCTURES AND UTILITIES. THIS PLAN INTENDS TO DISCLOSE GENERAL INFORMATION KNOWN BY THE ENGINEER AND TO SHOW THE LIMITS OF THE AREA WHERE WORK WILL BE PERFORMED. THIS PLAN SHOWS THE EXISTING FEATURES TAKEN FROM A FIELD SURVEY, FIELD INVESTIGATIONS AND AVAILABLE INFORMATION. THIS PLAN MAY OR MAY NOT ACCURATELY REFLECT THE TYPE OR EXTENT OF THE ITEMS TO BE ENCOUNTERED AS THEY ACTUALLY EXIST. WHERE EXISTING FEATURES ARE NOT SHOWN, IT IS NOT IMPLIED THAT THEY ARE NOT TO BE DEMOLISHED OR RELOCATED. THE CONTRACTOR SHALL PERFORM A THOROUGH FIELD INVESTIGATION AND REVIEW OF THE SITE WITHIN THE LIMIT OF WORK SHOWN IN THIS PLAN SET TO DETERMINE THE TYPE, QUANTITY AND EXTENT OF ANY AND ALL ITEMS. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR DETERMINING THE EXTENT OF EXISTING STRUCTURES AND UTILITIES AND QUANTITY OF WORK INVOLVED IN REMOVING THESE ITEMS FROM THE SITE.
- PROTECT ALL EXISTING UTILITIES IN PLACE UNLESS OTHERWISE NOTED. REPLACE ANY DAMAGED UTILITY TO REMAIN TO KEEP OPERABLE DURING CONSTRUCTION.
- ALL UTILITY SHUT DOWNS ARE TO BE AVOIDED. IF SHUT DOWNS ARE NECESSARY, CONTRACTOR TO COORDINATE SHUT DOWN WITH UTILITY OWNER WITH 48 HOUR MINIMUM NOTICE.
- ALL EXISTING STORM DRAIN, SANITARY SEWER, AND WATER MAINS THAT SERVE EXISTING BUILDINGS MUST REMAIN OPERABLE DURING CONSTRUCTION. CONTRACTOR TO SET UP TEMPORARY SERVICE OR PUMP AS NECESSARY TO ENSURE UNINTERRUPTED SERVICE.

RECORD DRAWING NOTE

THE CONTRACTOR SHALL KEEP UP-TO-DATE AND ACCURATE A COMPLETE RECORD SET OF PRINTS OF THE CONTRACT DRAWINGS SHOWING EVERY CHANGE FROM THE ORIGINAL DRAWINGS MADE DURING THE COURSE OF CONSTRUCTION INCLUDING EXACT FINAL LOCATION, ELEVATION, SIZES, MATERIALS, AND DESCRIPTION OF ALL WORK. RECORDS SHALL BE "REDLINED" ON A SET OF CONSTRUCTION PLAN DRAWINGS. A COMPLETE SET OF CORRECTED AND COMPLETED RECORD DRAWING PRINTS SHALL BE SUBMITTED TO THE CITY ENGINEER AND DEVELOPER'S CIVIL ENGINEER PRIOR TO FINAL ACCEPTANCE FOR REVIEW AND APPROVAL BY THE CITY ENGINEER.

DISCREPANCIES

IF THERE ARE ANY DISCREPANCIES BETWEEN DIMENSIONS IN DRAWINGS AND EXISTING CONDITIONS WHICH WILL AFFECT THE WORK, THE CONTRACTOR SHALL BRING SUCH DISCREPANCIES TO THE ATTENTION OF THE ENGINEER FOR ADJUSTMENT BEFORE PROCEEDING WITH THE WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROPER FITTING OF ALL WORK AND FOR THE COORDINATION OF ALL TRADES, SUBCONTRACTORS, AND PERSONS ENGAGED UPON THIS CONTRACT.

UTILITY/POTHOLE NOTE

THE TYPES, LOCATIONS, SIZES AND /OR DEPTHS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ARE APPROXIMATE AND WERE OBTAINED FROM SOURCES OF VARYING RELIABILITY. ONLY ACTUAL EXCAVATION WILL REVEAL THE TYPES, EXTENT, SIZES, LOCATIONS AND DEPTHS OF SUCH UNDERGROUND UTILITIES. A REASONABLE EFFORT HAS BEEN MADE TO LOCATE AND DELINEATE ALL KNOWN UNDERGROUND UTILITIES. HOWEVER, THE ENGINEER CAN ASSUME NO RESPONSIBILITY FOR THE COMPLETENESS OR ACCURACY OF ITS DELINEATION OF SUCH UNDERGROUND UTILITIES WHICH MAY BE ENCOUNTERED, BUT WHICH ARE NOT SHOWN ON THESE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UNDERGROUND FACILITIES AND UTILITIES BY POTHOLING PRIOR TO COMMENCING CONSTRUCTION.

DIMENSIONS

ALL DIMENSIONS ON THE PLANS ARE IN FEET OR DECIMALS THEREOF UNLESS SPECIFICALLY CALLED OUT AS FEET AND INCHES.

DUST CONTROL NOTES

- WATER TRUCKS SHALL BE PRESENT AND IN USE AT THE CONSTRUCTION SITE. ALL PORTIONS OF THE SITE SUBJECT TO BLOWING DUST SHALL BE WATERED AS OFTEN AS DEEMED NECESSARY BY THE CLIENT/INSPECTOR IN ORDER TO INSURE PROPER CONTROL OF BLOWING DUST FOR THE DURATION OF THE PROJECT.
- ALL PUBLIC STREETS AND MEDIANS SOILED OR LITTERED DUE TO THIS CONSTRUCTION ACTIVITY SHALL BE CLEANED AND SWEEP ON A DAILY BASIS DURING THE WORK WEEK, OR AS OFTEN AS DEEMED NECESSARY BY THE CLIENT/INSPECTOR, OR TO THE SATISFACTION OF THE CITY'S DEPARTMENT OF PUBLIC WORKS.
- ALL TRUCKS HAULING SOIL, SAND, AND OTHER LOOSE MATERIALS SHALL BE COVERED WITH TARPULINS OR OTHER EFFECTIVE COVERS.
- THE CONTRACTOR SHALL DEMONSTRATE DUST SUPPRESSION MEASURES, SUCH AS REGULAR WATERING, WHICH SHALL BE IMPLEMENTED TO REDUCE EMISSIONS DURING CONSTRUCTION AND GRADING IN A MANNER MEETING THE APPROVAL OF THE CONSTRUCTION MANAGER. THIS SHALL ASSIST IN REDUCING SHORT-TERM IMPACTS FROM PARTICLES WHICH COULD RESULT IN NUISANCES THAT ARE PROHIBITED BY RULE 403 (FUGITIVE DUST).
- GRADING OR ANY OTHER OPERATIONS THAT CREATES DUST SHALL BE STOPPED IMMEDIATELY IF DUST AFFECTS ADJACENT PROPERTIES. THE CONTRACTOR SHALL PROVIDE SUFFICIENT DUST CONTROL FOR THE ENTIRE PROJECT SITE IN ACCORDANCE WITH THE PROJECT SWPPP (IF ONE EXISTS) OR AS APPLICABLE PER LOCAL REGULATIONS AT ALL TIMES. THE SITE SHALL BE SPRINKLERED AS NECESSARY TO PREVENT DUST NUISANCE. IN THE EVENT THAT THE CONTRACTOR NEGLECTS TO USE ADEQUATE MEASURES TO CONTROL DUST, THE CLIENT RESERVES THE RIGHT TO TAKE WHATEVER MEASURES ARE NECESSARY TO CONTROL DUST AND CHARGE THE COST TO THE CONTRACTOR.
- THE CONTRACTOR IS RESPONSIBLE FOR DUST CONTROL MEASURES AND FOR OBTAINING ALL REQUIRED PERMITS AND APPROVALS. ALL GRADING OPERATIONS SHALL BE SUSPENDED DURING SECOND (OR WORSE) STAGE SMOG ALERTS.


TREE PROTECTION

- PRIOR TO BEGINNING CONSTRUCTION ON SITE, CONTRACTOR SHALL IDENTIFY AND PROTECT EXISTING TREES AND PLANTS DESIGNATED AS TO REMAIN.
- PROTECT EXISTING TREES TO REMAIN FROM SPILLED CHEMICALS, FUEL OIL, MOTOR OIL, GASOLINE AND ALL OTHER CHEMICALLY INJURIOUS MATERIAL; AS WELL AS FROM PUDDLING OR CONTINUOUSLY RUNNING WATER. SHOULD A SPILL OCCUR, STOP WORK IN THAT AREA AND CONTACT THE CITY'S ENGINEER/INSPECTOR IMMEDIATELY. CONTRACTOR SHALL BE RESPONSIBLE TO MITIGATE DAMAGE FROM SPILLED MATERIAL AS WELL AS MATERIAL CLEAN UP.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ONGOING MAINTENANCE OF ALL TREES DESIGNATED TO REMAIN AND FOR MAINTENANCE OF RELOCATED TREES STOCKPILED DURING CONSTRUCTION. CONTRACTOR WILL BE REQUIRED TO REPLACE TREES THAT DIE DUE TO LACK OF MAINTENANCE.

PROJECT SITE MAINTENANCE

- SWEEP OR VACUUM THE STREET PAVEMENT AND SIDEWALKS ADJOINING THE PROJECT SITE AND THE ON-SITE PAVED AREAS ON A DAILY BASIS. SCRAPE CAKED-ON MUD AND DIRT FROM THESE AREAS BEFORE SWEEPING. CORNERS AND HARD TO REACH AREAS SHALL BE SWEEPED MANUALLY.
- NEVER CLEAN MACHINERY, EQUIPMENT OR TOOLS INTO A STREET, GUTTER OR STORM DRAIN.
- ENSURE THAT CEMENT TRUCKS, PAINTERS, OR STUCCO/PLASTER FINISHING CONTRACTORS DO NOT DISCHARGE WASH WATER FROM EQUIPMENT, TOOLS OR RINSE CONTAINERS INTO GUTTERS OR DRAINS.

DESIGNED BY: JP	DATE	BY	DATE						
DRAWN BY: BP TS	DATE	DESCRIPTION							
QC CHECKED BY: RES	DATE	REV NO	1	2	3	4	5		
PROJECT No. 623071									
SCALE									
SUBMITTAL 60%									


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NOTES

DELAWARE STREET
SAFE ROUTES TO SCHOOL

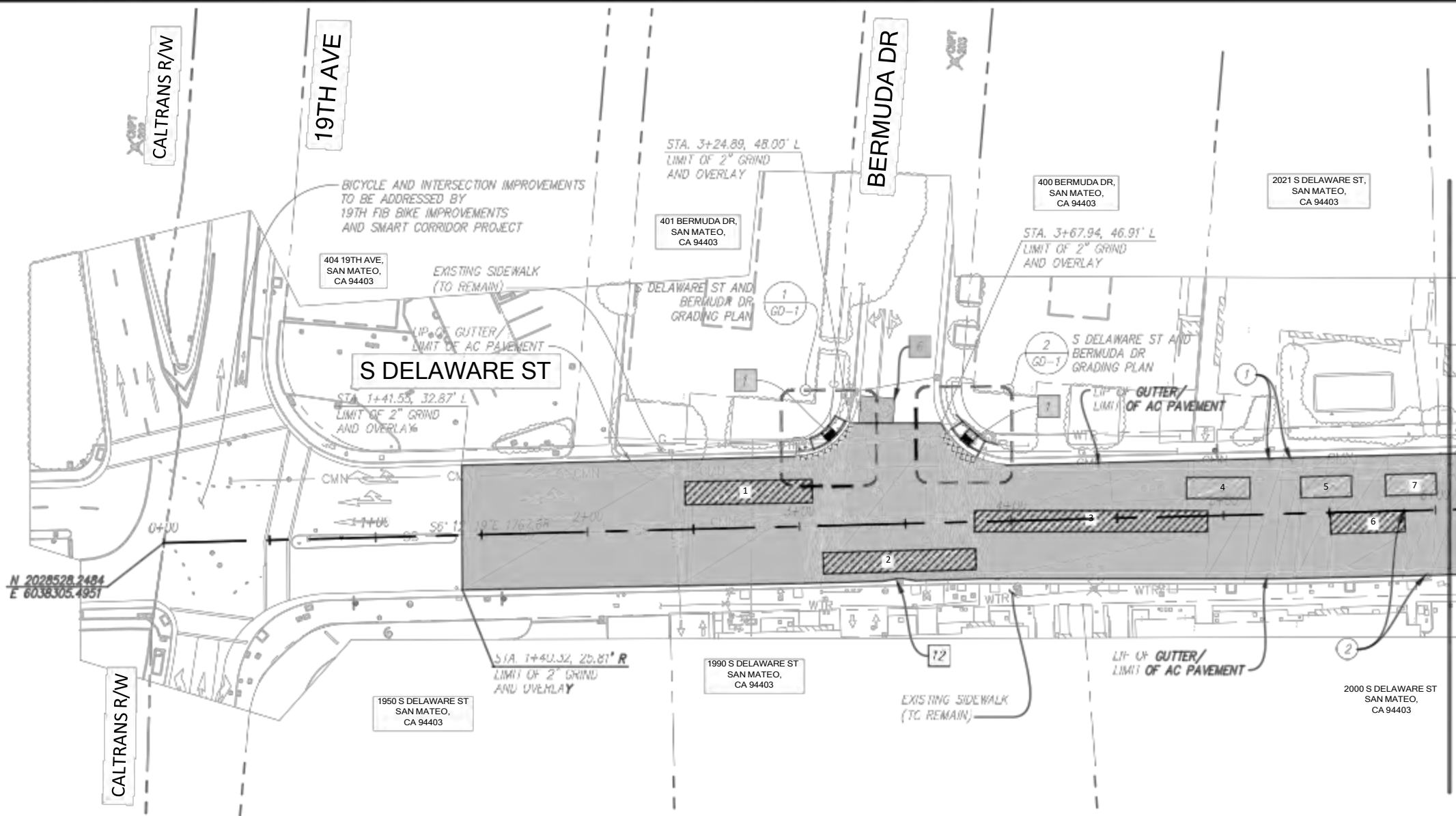
CITY PROJECT NO. 46R022



SHEET	2	OF	36
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BASE REPAIR TABLE		
#	LENGTH X WIDTH (FEET)*	AREA (SQUARE FEET)
	60' X 11'	660'
	72' X 10'	720'
	110' X 10'	1100'
	30' X 10'	300'
	24' X 10'	240'
	35' X 10'	350'
	24' X 10'	240'

* THE DIMENSIONS LISTED ARE APPROXIMATE. THE EXTENTS FOR BASE REPAIR SHALL BE CONFIRMED IN THE FIELD BY THE INSPECTOR AND CONTRACTOR.



MATCHLINE -SEE SHEET L-2

GENERAL DEMOLITION NOTES

1. LIMITS OF PAVING WORK TO BE VERIFIED **IN** THE FIELD BY CITY ENGINEER.
2. **AT** CONCRETE SIDEWALK CONFORM, DEMOLISH AND REMOVE SIDEWALK TO THE NEAREST SCORE LINE.
3. CONTRACTOR TO PROVIDE PHASING AND TEMPORARY TRAFFIC CONTROL PLANS FOR CITY REVIEW AND APPROVAL PRIOR TO START OF WORK.
4. ALL EXISTING SIGNS SHALL BE PROTECTED **IN** PLACE UNLESS SPECIFIED OTHERWISE.
5. CONTRACTOR SHALL REMOVE ALL EXISTING PAVEMENT AND BASE MATERIALS AT LOCATION OF NEW PAVEMENT SECTIONS IDENTIFIED IN THESE PLANS.

GENERAL LAYOUT NOTES

1. SEE TRAFFIC SIGNAL PLANS FOR LOCATIONS OF SIGNAL SYSTEM IMPROVEMENTS.
2. SEE STRIPING PLANS FOR PAVEMENT MARKING AND STRIPING DETAILS.
3. REPLACE **IN** KIND ANY UTILITIES DAMAGED DURING **CONSTRUCTION**.
4. PROTECT ALL EXISTING UTILITIES UNLESS INDICATED OTHERWISE. ALL EXISTING UTILITY COVERS SHALL BE ADJUSTED TO GRADE AS NEEDED.

DEMOLITION NOTES:

1. ADJUST VALVE COVER TO GRADE.
2. ADJUST MANHOLE COVER TO GRADE.

LAYOUT NOTES

1. CASE **C** CURB RAMP PER CALTRANS STD PLAN A88A.
6. GRIND EXISTING PAVEMENT MARKINGS AND SLURRY SEAL. SLURRY SEAL SHALL EXTEND BEYOND LIMITS OF MARKINGS IN RECTANGLE SHAPE.
12. REPLACE CATCH BASIN GRATE WITH CALTRANS D77B, TYPE 24, TRAFFIC-RATEL BICYCLE FRIENDLY GRATE OR APPROVED EQUIVALENT.

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DRAWN BY: BP, TS
QC CHECKED BY: RES
PROJECT NO. 623071
SCALE
SUBMITTAL 60%

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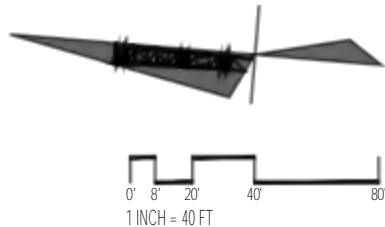
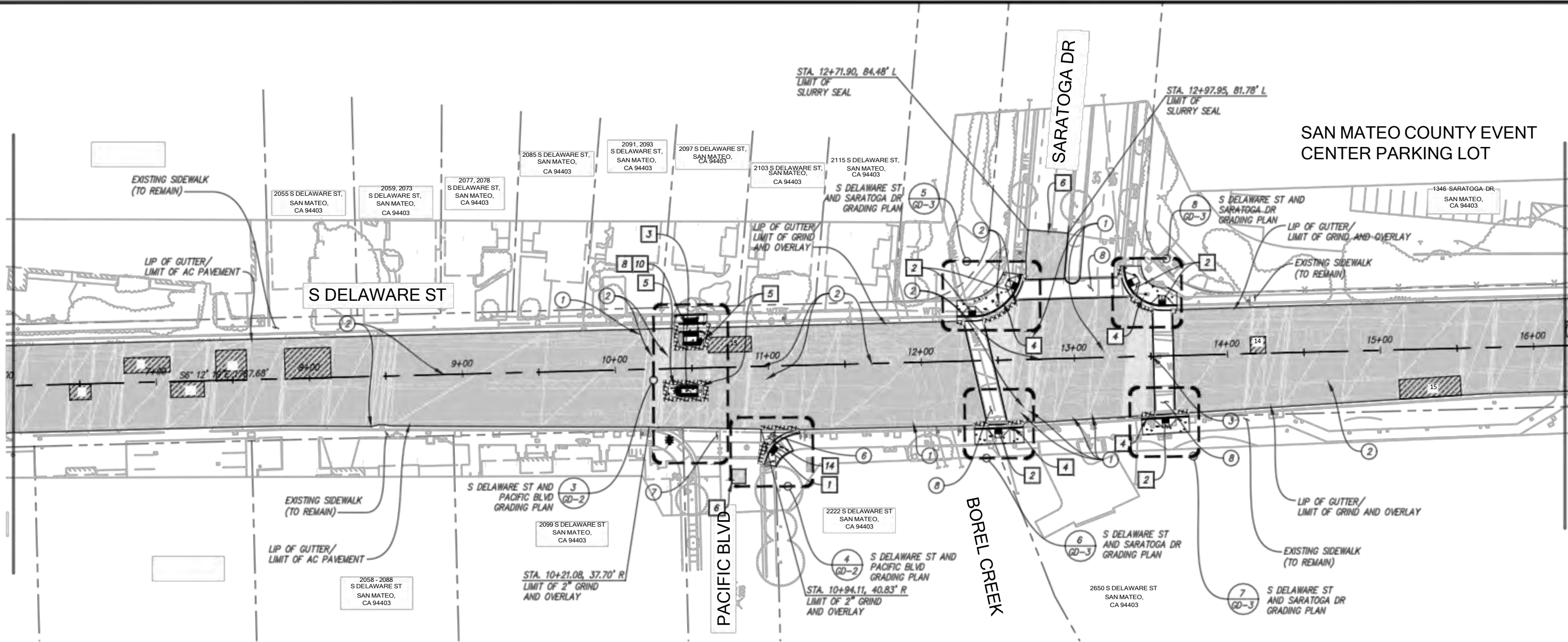
LAYOUT PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL

CITY PROJECT NO. 46R022

CITY OF SAN MATEO
CALIFORNIA

SHEET 3 OF 36

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MATCHLINE -SEE SHEET L-3

BASE REPAIR TABLE

#	LENGTH X WIDTH (FEET)	AREA (SQUARE FEET)
	14' X 10'	140'
	30' X 10'	300'
	22' X 10'	220'
	20' X 20'	400'
	30' X 20'	600'
	28' X 10'	280'
	11' X 10'	110'
	42' X 12'	504'

* THE DIMENSIONS LISTED ARE APPROXIMATE. THE EXTENTS FOR BASE REPAIR SHALL BE CONFIRMED IN THE FIELD BY THE INSPECTOR AND CONTRACTOR.

GENERAL NOTES

1. REFER TO SHEET L-1 FOR GENERAL LAYOUT AND DEMOLITION NOTES.

DEMOLITION NOTES:

- 1 ADJUST VALVE COVER TO GRADE.
- 2 ADJUST MANHOLE COVER TO GRADE.
- 3 PROTECT IN PLACE DIVERSION STRUCTURE ACCESS HATCHES TO GRADE.
- 6 DEMO AND REMOVE CONCRETE TO NEAREST SCORE LINE.
- 7 PORTION OF EXISTING VALLEY GUTTER TO BE PROTECTED IN PLACE.
- 8 PROTECT IN PLACE DECORATIVE CONCRETE CROSSWALK.

LAYOUT NOTES

- 1 CASE C CURB RAMP PER CALTRANS STD PLAN AB8A.
- 2 CASE A CURB RAMP PER CALTRANS STD PLAN AB8A.
- 3 TYPE A PASSAGEWAY PER CALTRANS STD PLAN AB8B.
- 4 TYPE A CURB, GUTTER, AND SIDEWALK PER CITY OF SAN MATEO STANDARD PLAN 141A.
- 5 VERTICAL CURB.
- 6 GRIND EXISTING PAVEMENT MARKINGS AND SLURRY SEAL. SLURRY SEAL SHALL EXTEND BEYOND LIMITS OF MARKINGS IN RECTANGLE SHAPE.
- 8 RAISED CONCRETE CROSSWALK PER GRADING PLAN GD-2.
- 10 ZURN Z708-HDS STEEL FRAME AND GRATE TRENCH DRAIN OR APPROVED EQUIVALENT AT RAISED CONCRETE CROSSWALK CONNECTION.
- 12 REPLACE CATCH BASIN GRATE WITH CALTRANS D77B, TYPE 24, RATED BICYCLE FRIENDLY GRATE OR APPROVED EQUIVALENT.
- 14 VALLEY GUTTER PER CITY OF SAN MATEO STANDARD PLAN 145.

LEGEND

- — — — — CENTER LINE
- — — — — ROW LINE
- 2" GRIND AND OVERLAY (2 CD-1)
- AC DEEP LIFT (3 CD-1)
- CONCRETE (141A CD-2)
- AC BASE REPAIR (4 CD-1)
- SLURRY SEAL
- * RECTANGULAR RAPID FLASHING BEACON (RRFB), SEE SHEET E-10 FOR MORE INFORMATION.
- — — — — FRAME AND GRATE TRENCH DRAIN

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DRAWN BY: BP, TS
QC CHECKED BY: RES
PROJECT No. 623071
SCALE
SUBMITTAL 60%

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REVISIONS
REV. NO.
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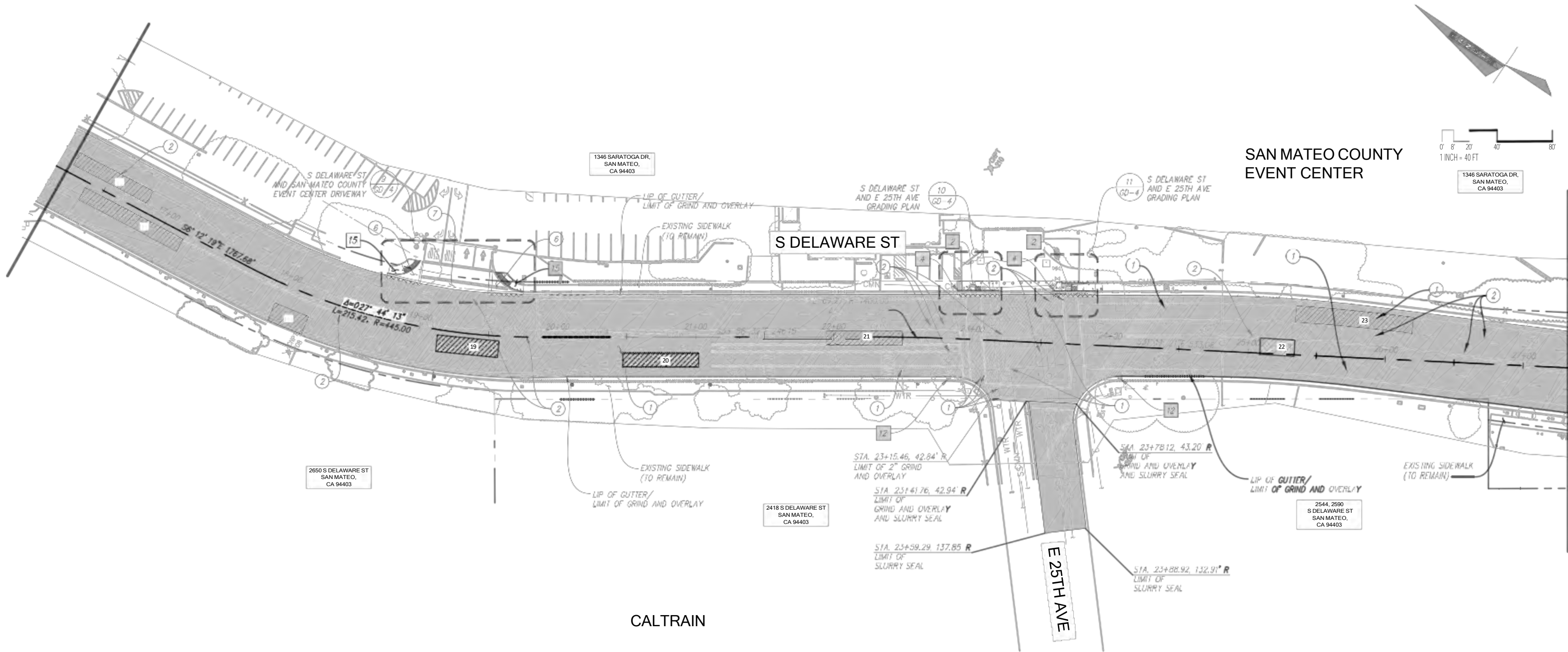
LAYOUT PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022

CITY OF SAN MATEO

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MATCHLINE -SEE SHEET L-4

BASE REPAIR TABLE

#	LENGTH X WIDTH (FEET)	AREA (SQUARE FEET)
	60 X 10'	600'
	80 X 10'	800'
	26 X 15	390'
	42 X 12'	504'
	55 X 10'	550'
	55 X 10'	550'
	25 X 12	300'
	85 X 12'	1020'

* THE DIMENSIONS LISTED ARE APPROXIMATE. THE EXTENTS FOR BASE REPAIR SHALL BE CONFIRMED IN THE FIELD BY THE INSPECTOR AND CONTRACTOR.

GENERAL NOTES

1. REFER TO SHEET L-1 FOR GENERAL LAYOUT AND DEMOLITION NOTES.

DEMOLITION NOTES:

1. ADJUST VALVE COVER TO GRADE.
2. ADJUST MANHOLE COVER TO GRADE.
3. DEMO AND REMOVE CONCRETE TO NEAREST SCORE LINE.
4. PORTION OF EXISTING VALLEY GUTTER TO BE PROTECTED IN PLACE.

LAYOUT NOTES

2. CASE A CURB RAMP PER CALTRANS STD PLAN A88A.
4. TYPE A CURB GUTTER AND SIDEWALK PER CITY OF SAN MATEO STANDARD PLAN 141A.
12. REPLACE CATCH BASIN GRATE WITH CALTRANS D77B TYPE 24, TRAFFIC-RATED BICYCLE FRIENDLY GRATE OR APPROVED EQUIVALENT.
15. CASE CH CURB RAMP PER CALTRANS STD PLAN A88B.

LEGEND

- CENTER LINE
- ROW LINE
- 2" GRIND AND OVERLAY (2 CD-1)
- AC DEEP LIFT (3 CD-1)
- CONCRETE (141A CD-2)
- AC BASE REPAIR (4 CD-1)
- SLURRY SEAL

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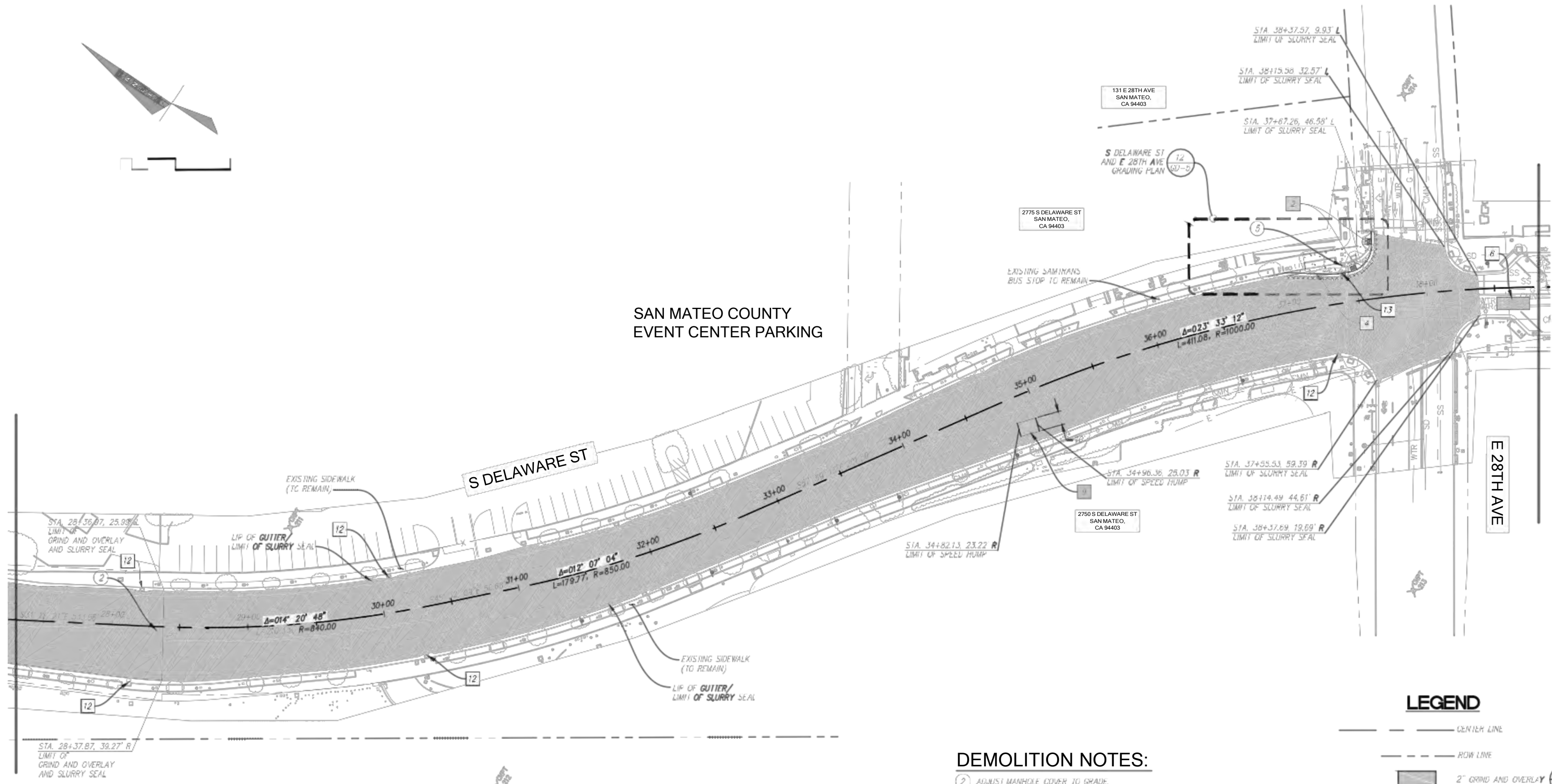


LAYOUT PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022



DESIGNED BY: JP	DATE
DRAWN BY: BP, TS	DATE
QC CHECKED BY: RES	DATE
PROJECT NO. 623071	SCALE
SUBMITTAL 60%	

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SAN MATEO COUNTY
EVENT CENTER PARKING

MATCHLINE -SEE SHEET L-5

GENERAL NOTES

1. REFER TO SHEET L-1 FOR GENERAL LAYOUT AND DEMOLITION NOTES.

DEMOLITION NOTES:

- 2. ADJUST MANHOLE COVER TO GRADE.
- 5. DEMO AND REMOVE EXISTING SD CATCH BASIN. REMOVE PIPE CONNECTING TO EXISTING WATER MAIN. PLUG EX. PIPE AT MANHOLE.

LAYOUT NOTES

- 2. CASE A CURB RAMP PER CALTRANS STD PLAN AB8A.
- 4. TYPE A CURB, GUTTER, AND SIDEWALK PER CITY OF SAN MATEO STANDARD PLAN 141A.
- 6. GRIND EXISTING PAVEMENT MARKINGS AND SLURRY SEAL. SLURRY SEAL SHALL EXTEND BEYOND LIMITS OF MARKINGS IN RECTANGLE SHAPE.
- 9. SINGLE RIGHT TURN LANE SPEED HUMP IN ADVANCE OF MIXING ZONE.
- 12. REPLACE CATCH BASIN GRATE WITH CALTRANS D77B, TYPE 24, TRAFFIC-RATED BICYCLE FRIENDLY GRATE OR APPROVED EQUIVALENT.
- 13. STORM DRAIN MANHOLE TO BE REPLACED WITH STORM DRAIN CATCH BASIN.

LEGEND

- CENTER LINE
- ROW LINE
- 2" GRIND AND OVERLAY
- AC DEEP LIFT
- CONCRETE
- SLURRY SEAL

REV NO	DESCRIPTION	BY	DATE
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LAYOUT PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022

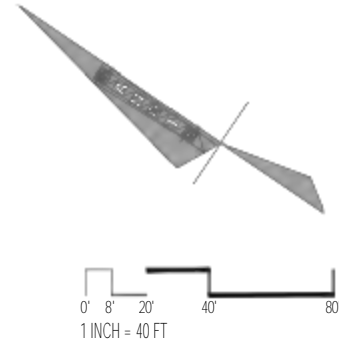
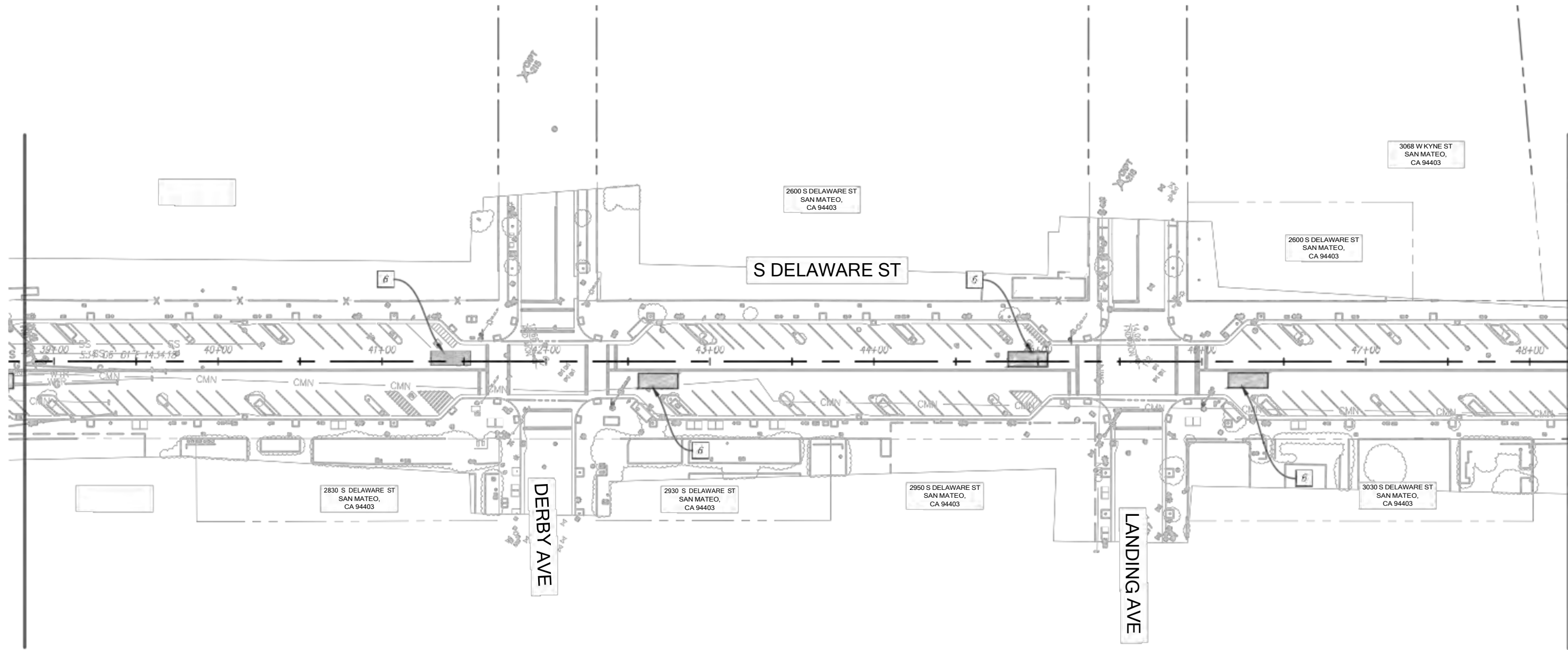


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DRAWN BY: BP TS	DATE	DATE	DATE
QC CHECKED BY: RES	DATE	DATE	DATE
PROJECT NO. 62071	SCALE	DATE	DATE
SUBMITTAL 60%	DATE	DATE	DATE

L-4

SHEET 6 OF 36

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GENERAL NOTES

1. REFER TO SHEET **L-1** FOR GENERAL LAYOUT AND DEMOLITION NOTES.

LAYOUT NOTES

- 6 GRIND EXISTING PAVEMENT MARKINGS AND SLURRY SEAL. SLURRY SEAL SHALL EXTEND BEYOND LIMITS OF MARKINGS IN RECTANGLE SHAPE.

LEGEND

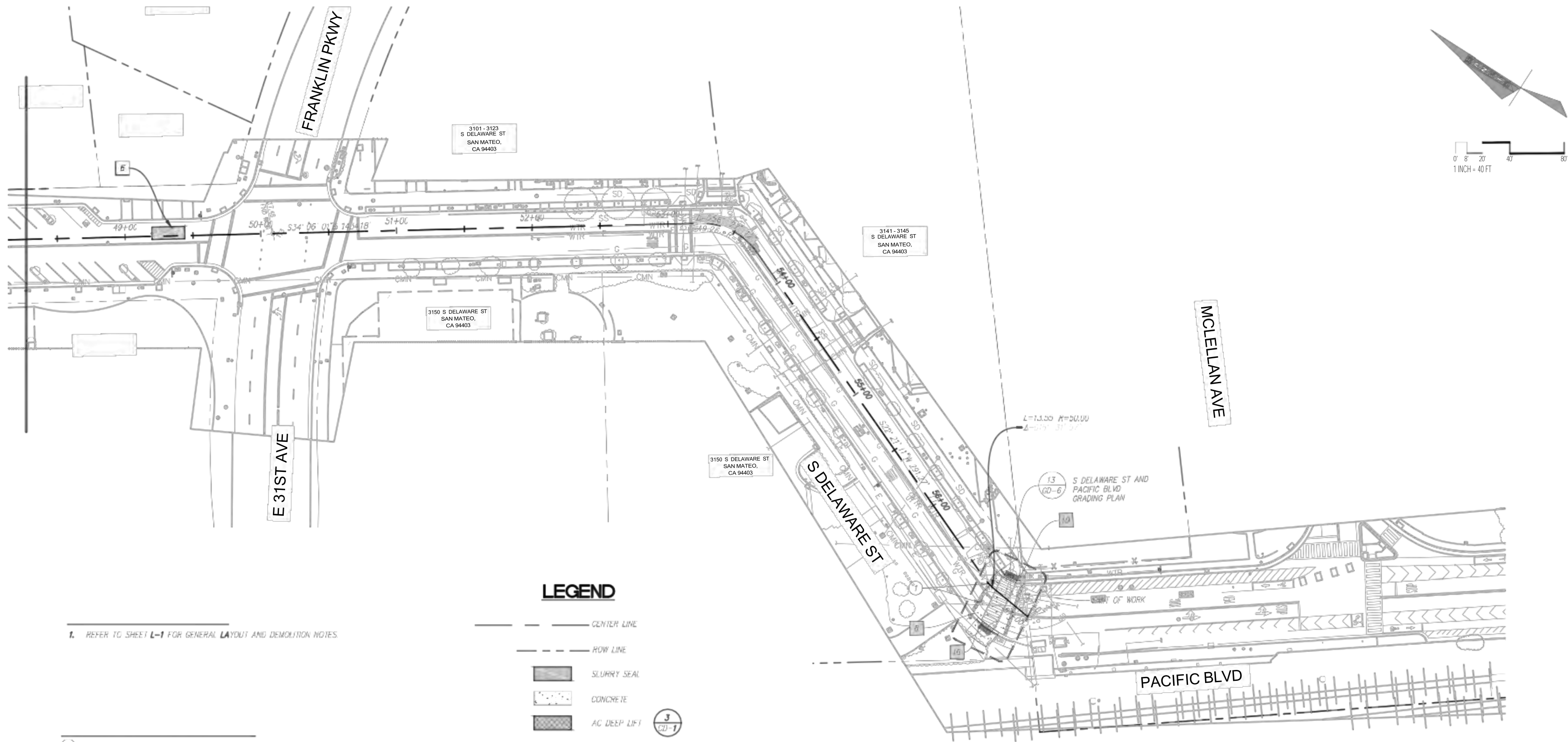


LAYOUT PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022



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1. REFER TO SHEET L-1 FOR GENERAL LAYOUT AND DEMOLITION NOTES.

- 1. ADJUST VALVE COVER TO GRADE.
- 2. EXISTING VALLEY GUTTER TO BE PROTECTED IN PLACE.

LAYOUT NOTES

- 6. GROUND EXISTING PAVEMENT MARKINGS AND SLURRY SEAL. SLURRY SEAL SHALL EXTEND BEYOND LIMITS OF MARKINGS IN RECTANGLE SHAPE.
- 8. RAISED CONCRETE CROSSWALK PER GRADING PLAN GW-B.
- 10. ZURK Z723-HUG STEEL FRAME AND GRATE TRENCH DRAIN OR APPROVED EQUIVALENT AT RAISED CONCRETE CROSSWALK CONNECTION.

LEGEND

- CENTER LINE
- ROW LINE
- SLURRY SEAL
- CONCRETE
- AC DEEP LIFT
- FRAME AND GRATE TRENCH DRAIN



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LAYOUT PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022

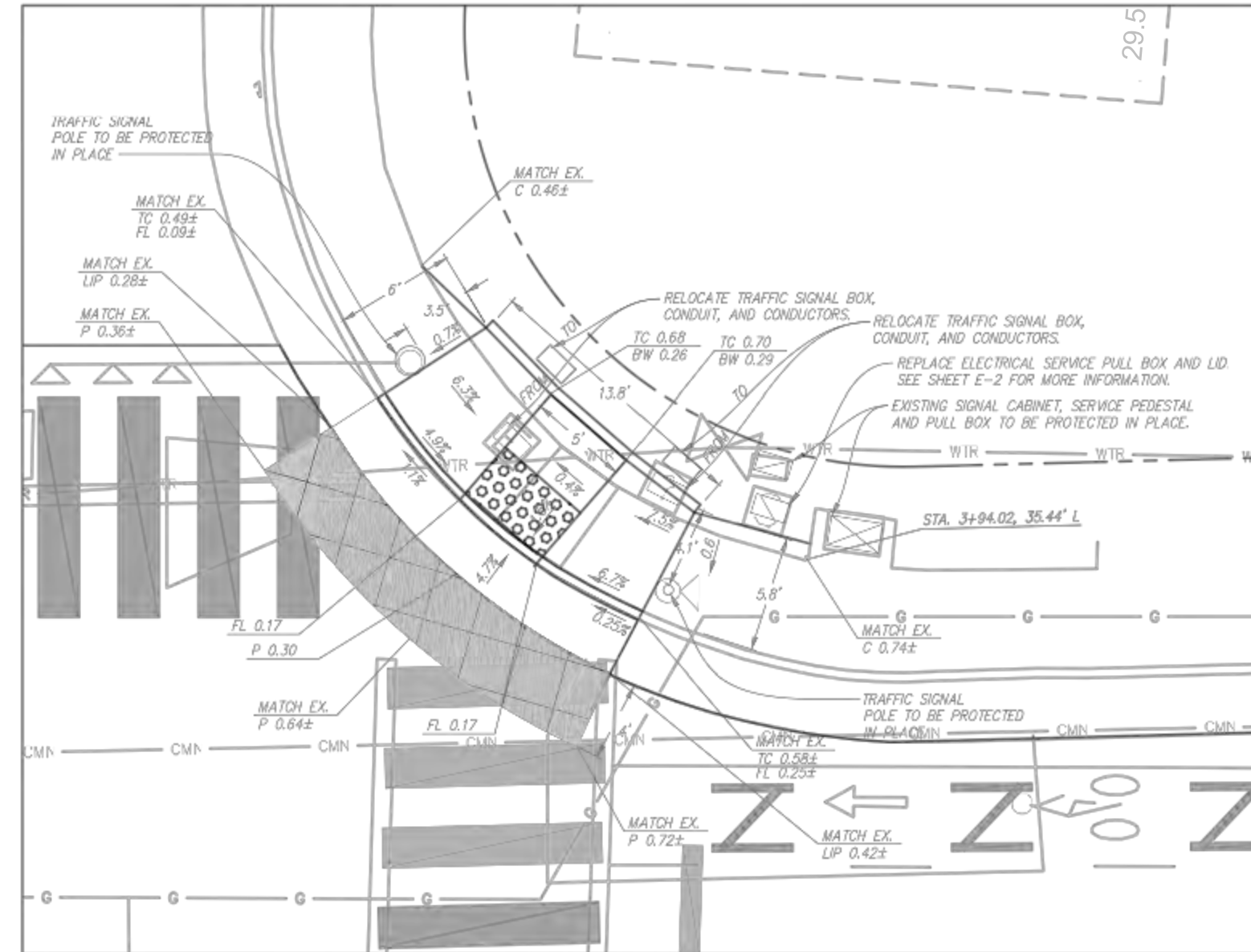


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S DELAWARE ST AND
BERMUDA DR
(NORTHEAST CORNER)

$$1''=5$$

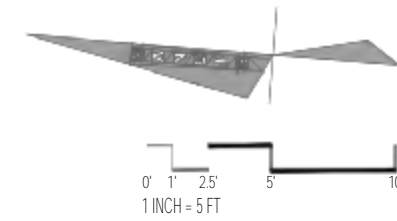
1



S DELAWARE ST AND
BERMUDA DR
(SOUTHEAST CORNER)

$$I'' = \frac{I}{2}$$

2



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NOT FOR CONSTRUCTION

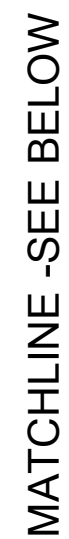
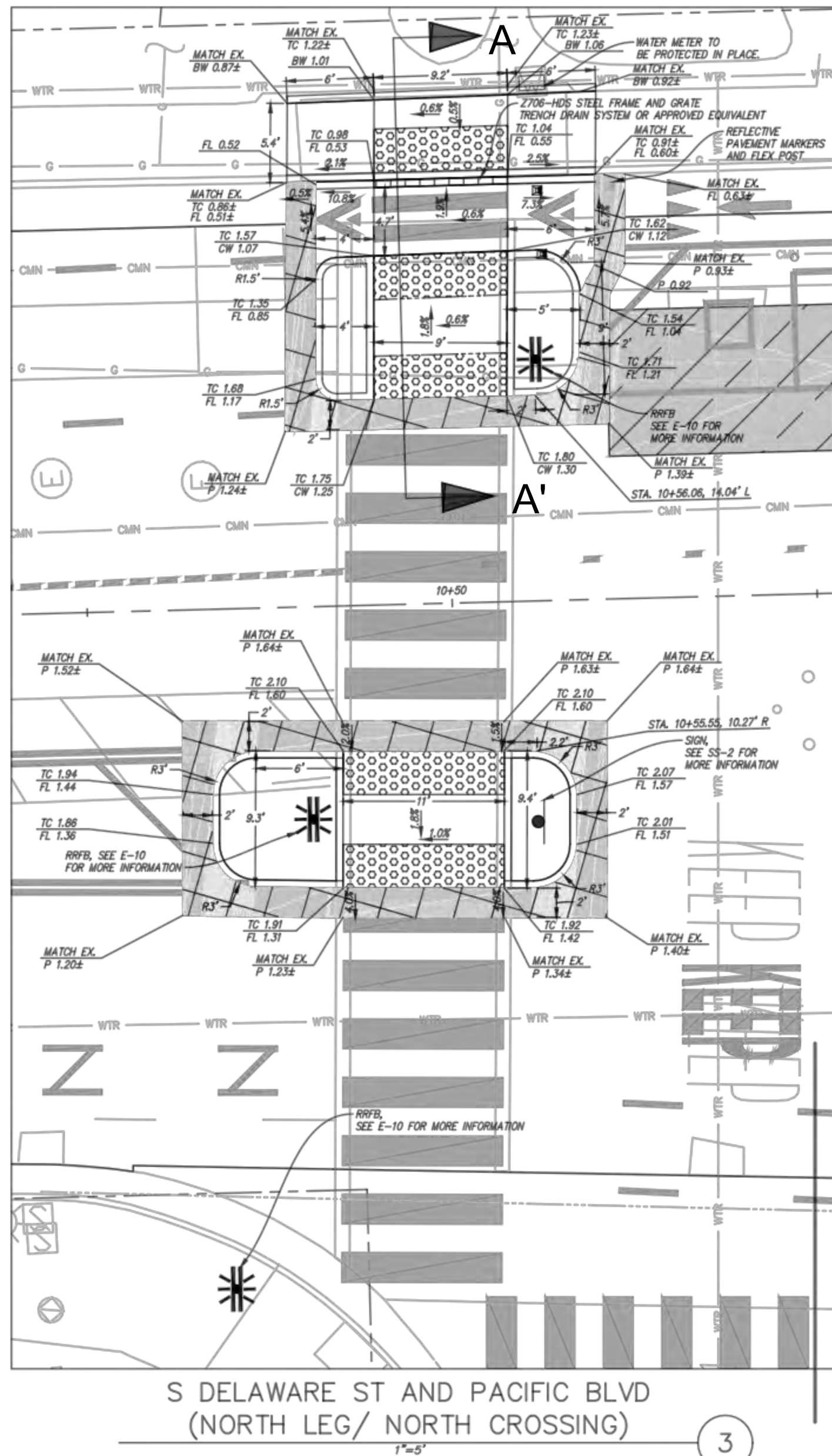
GRADING PLAN DELAWARE STREET SAFE ROUTES TO SCHOOL

CITY PROJECT NO. 46R022

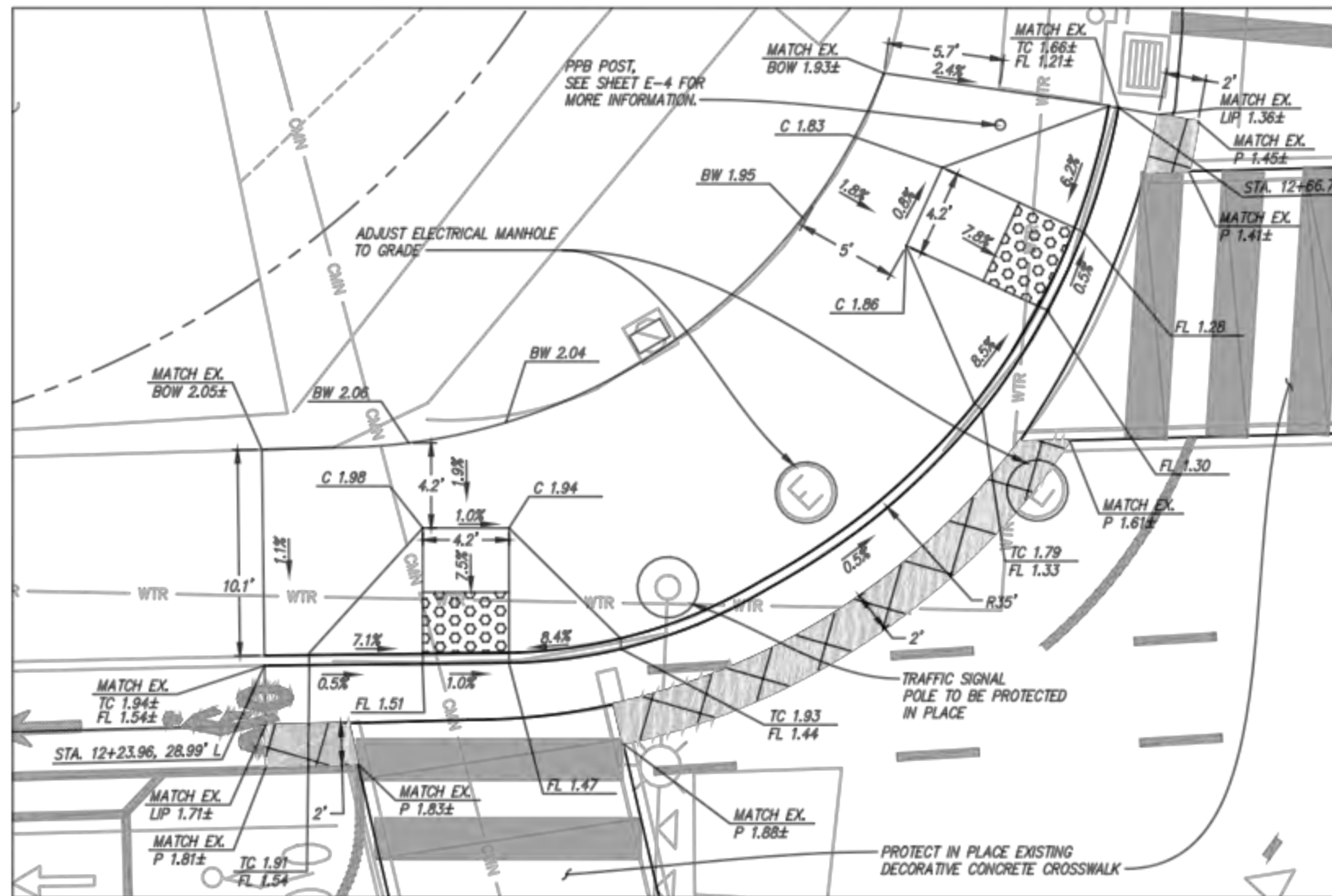


GD-1

SHEET 9 OF 36

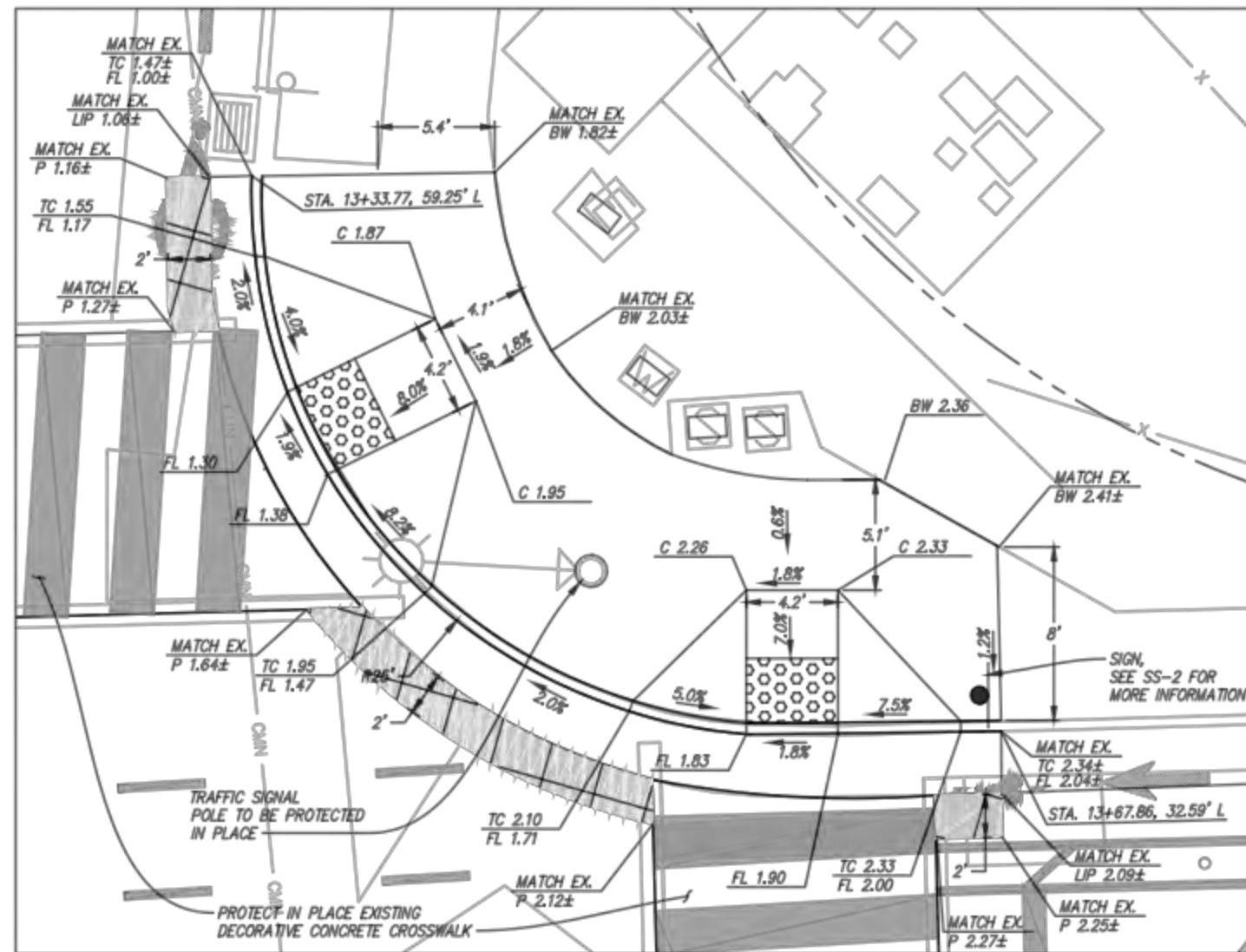


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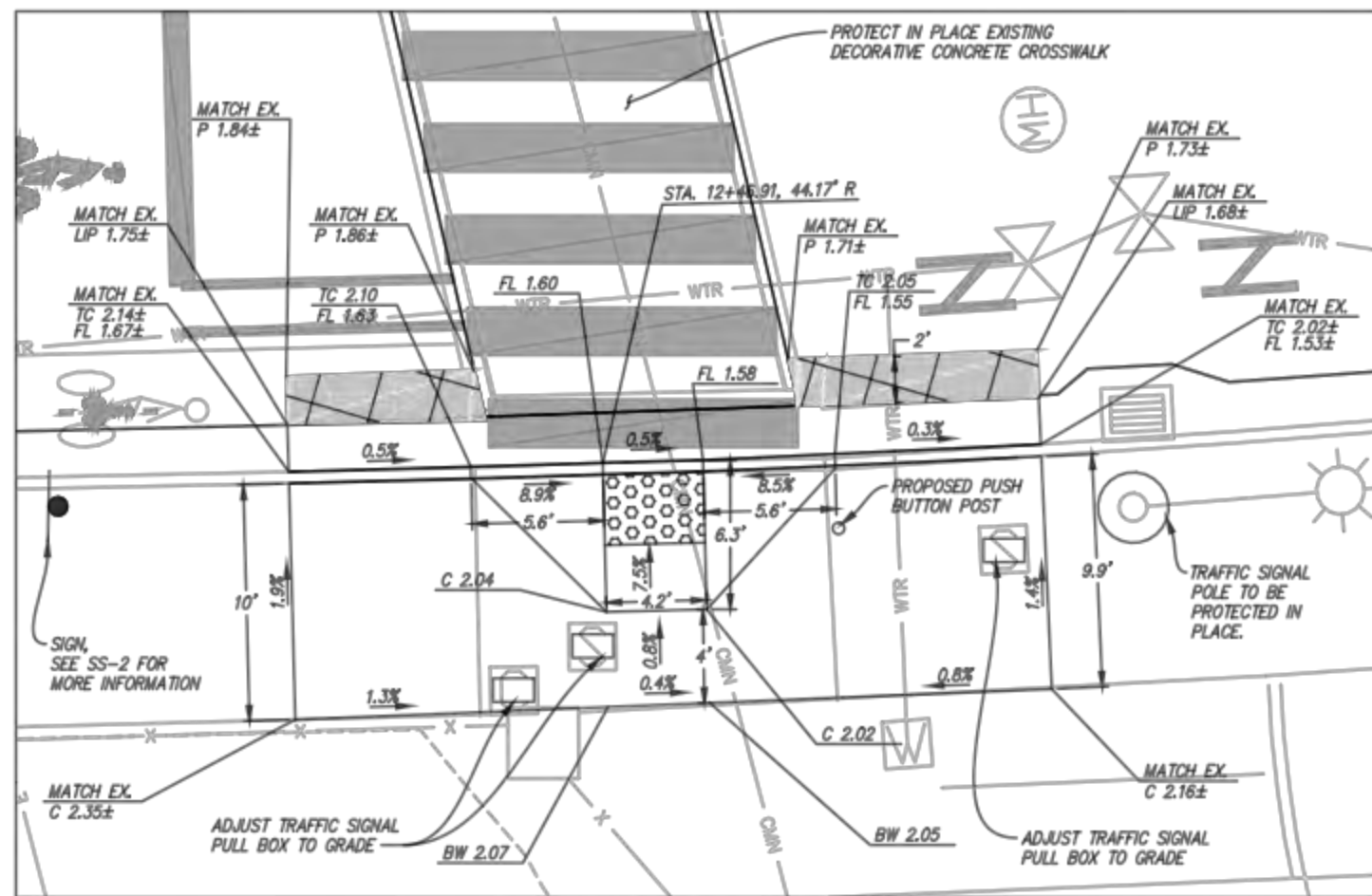
S DELAWARE ST AND
SARATOGA DR
(NORTHEAST CORNER)

5



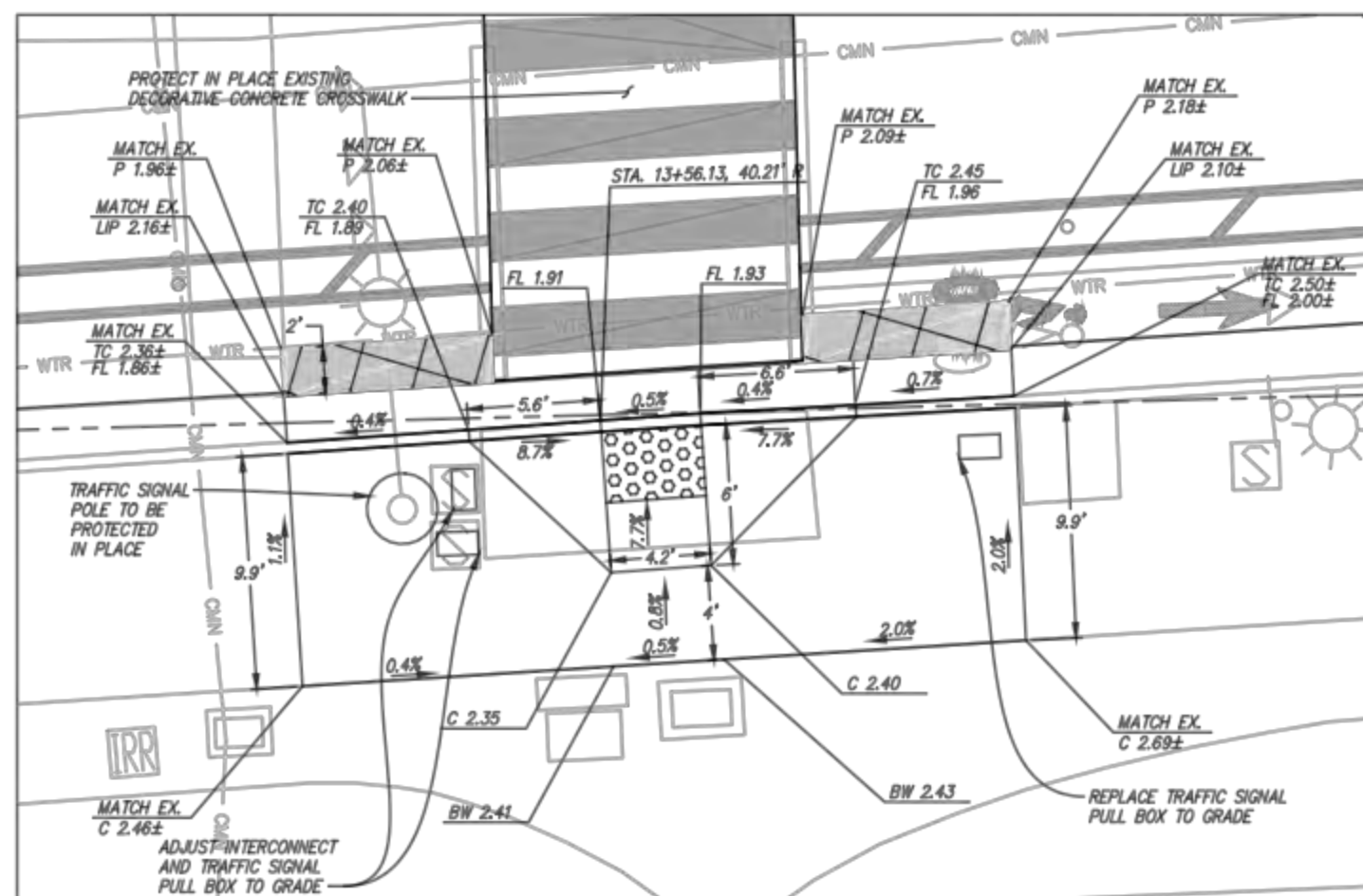
S DELAWARE ST AND
SARATOGA DR
(SOUTHEAST CORNER)

8



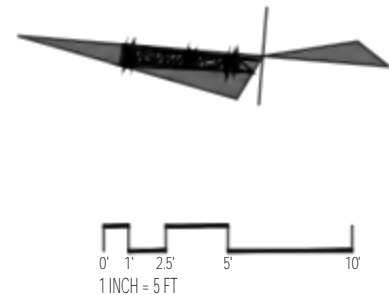
S DELAWARE ST AND
SARATOGA DR
(NORTHWEST CORNER)

6



S DELAWARE ST AND
SARATOGA DR
(SOUTHWEST CORNER)

7



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GRADING PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL

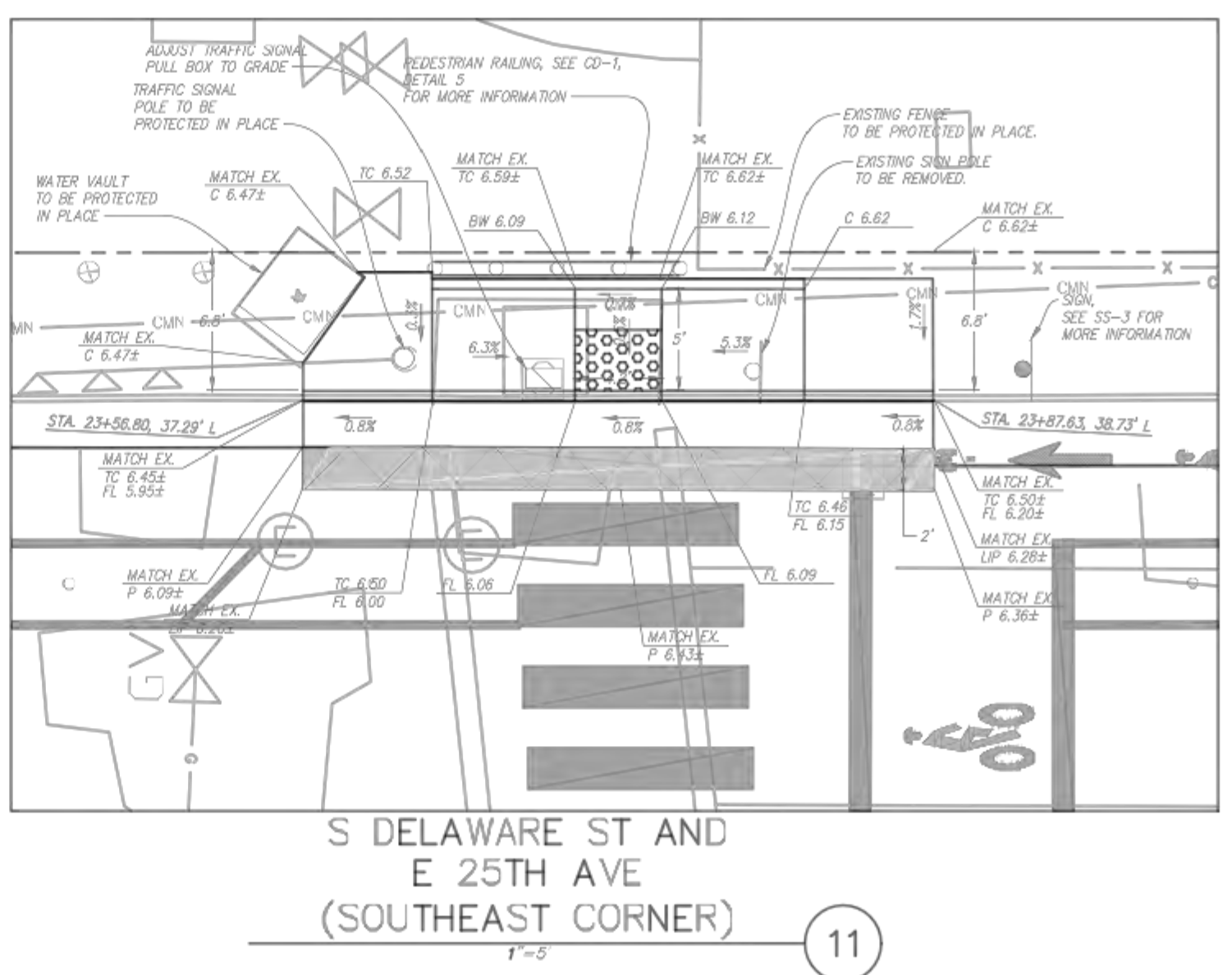
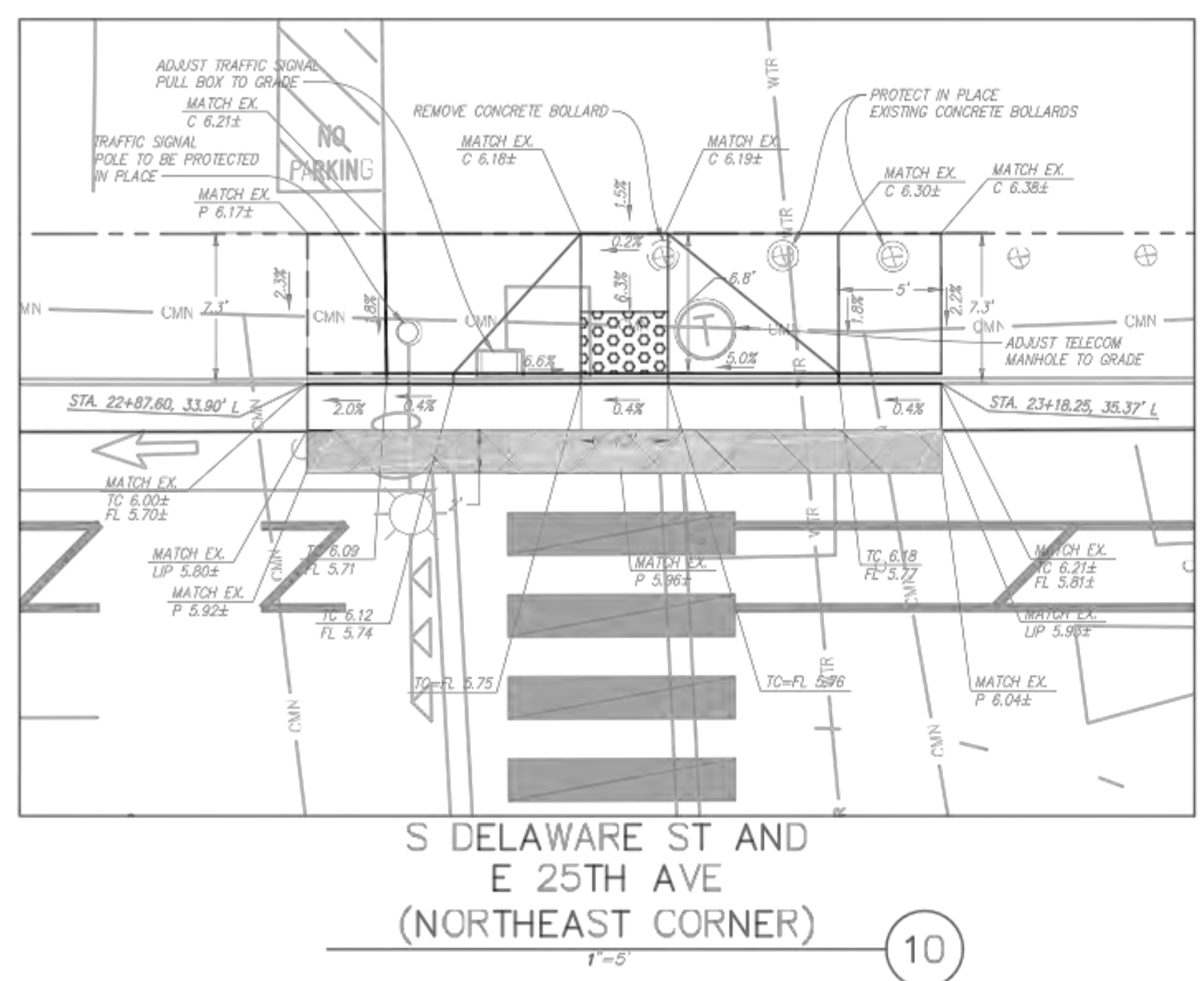
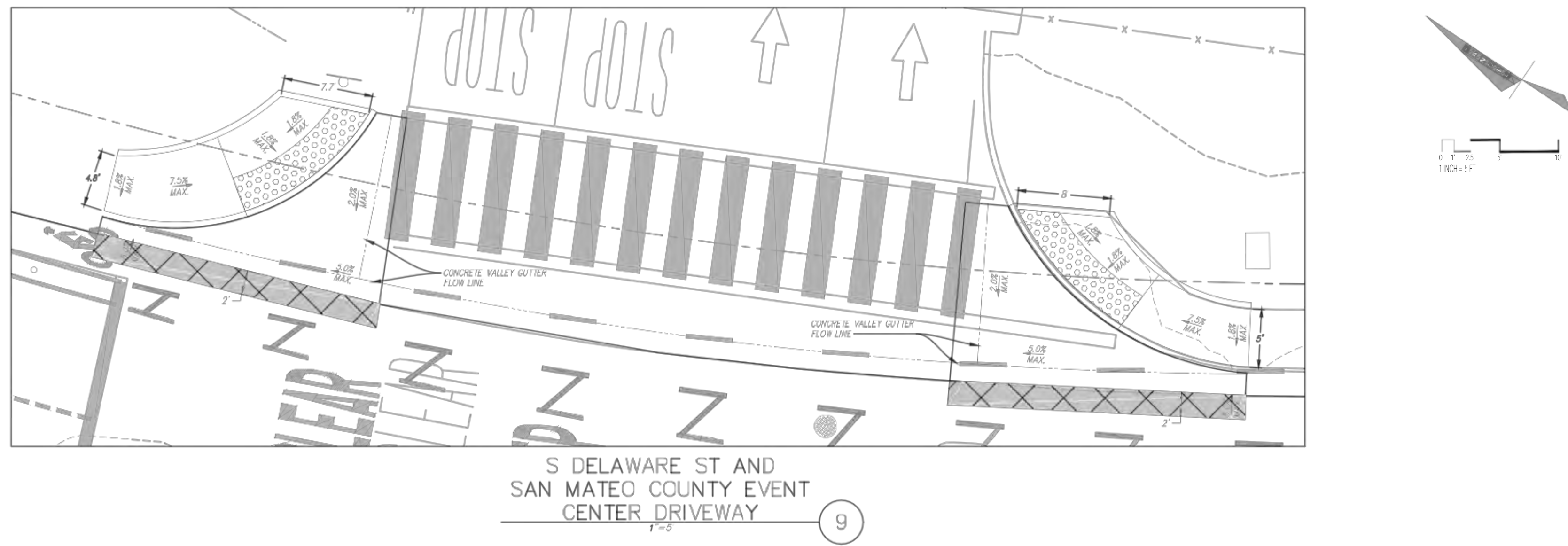
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GD-3

SHEET 11 OF 36

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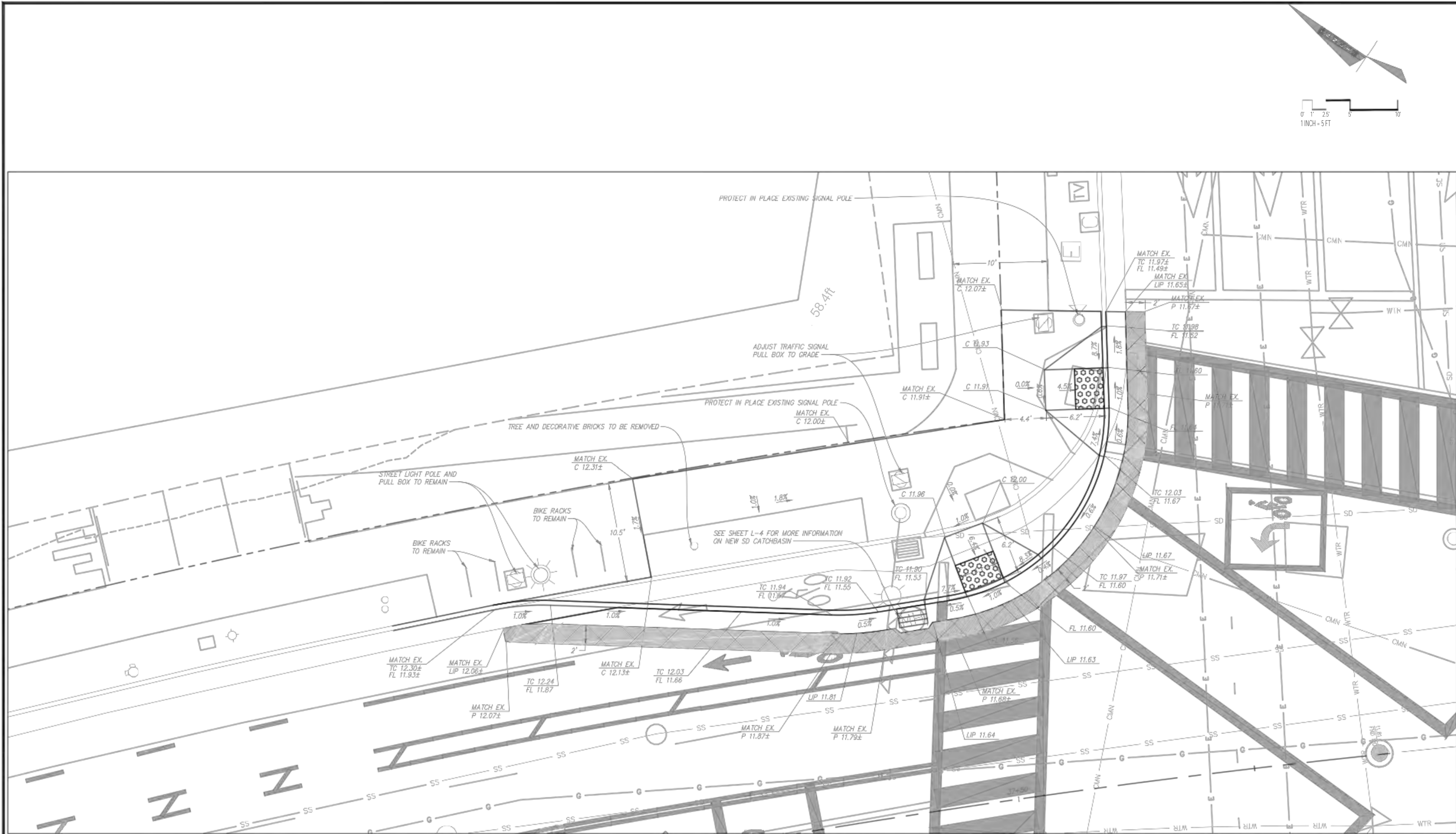


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GRADING PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022



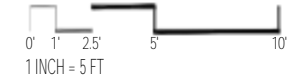
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S DELAWARE ST AND
E 28TH AVE
(NORTHEAST CORNER)

1"=5'

12



A A A A A



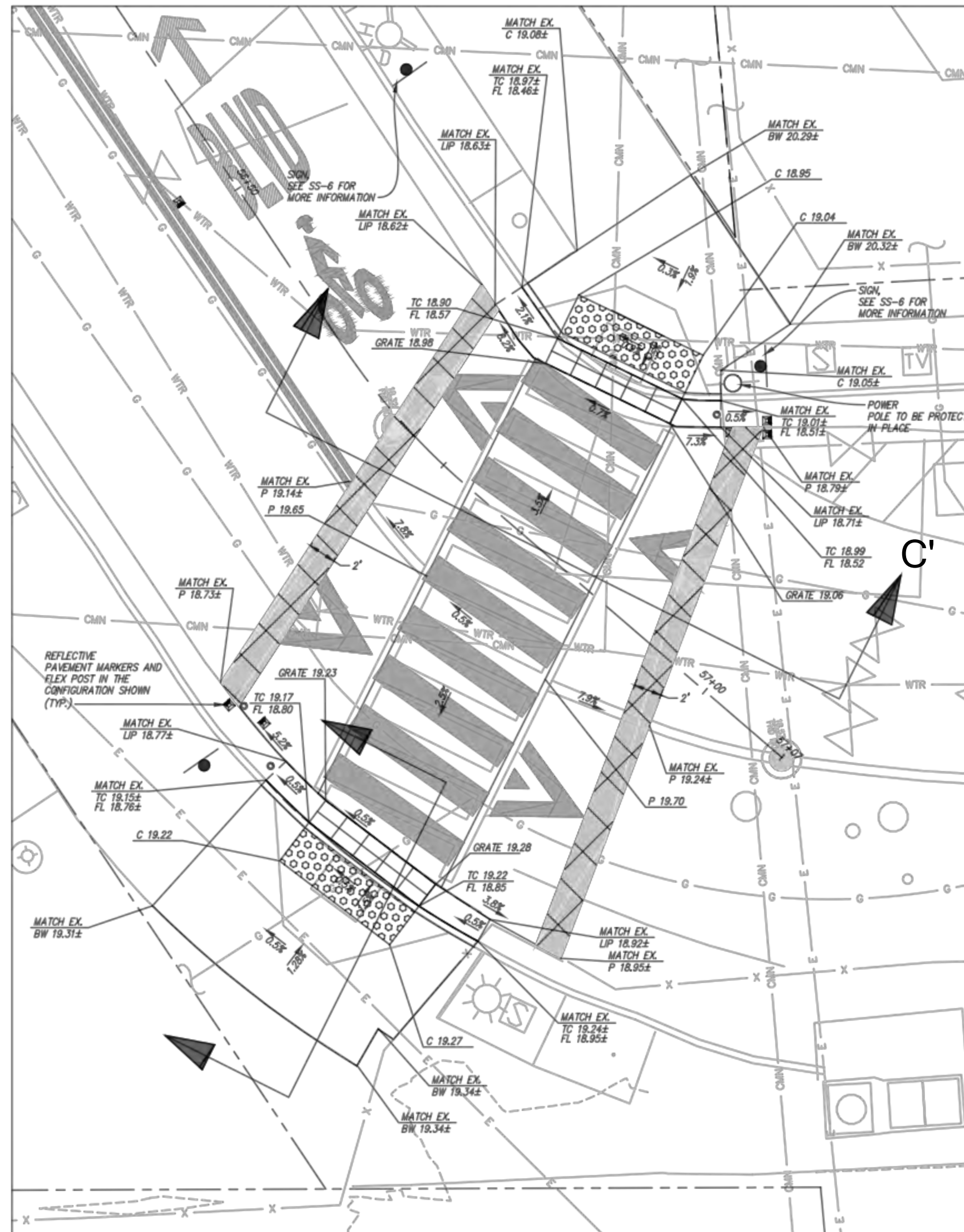
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GRADING PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022



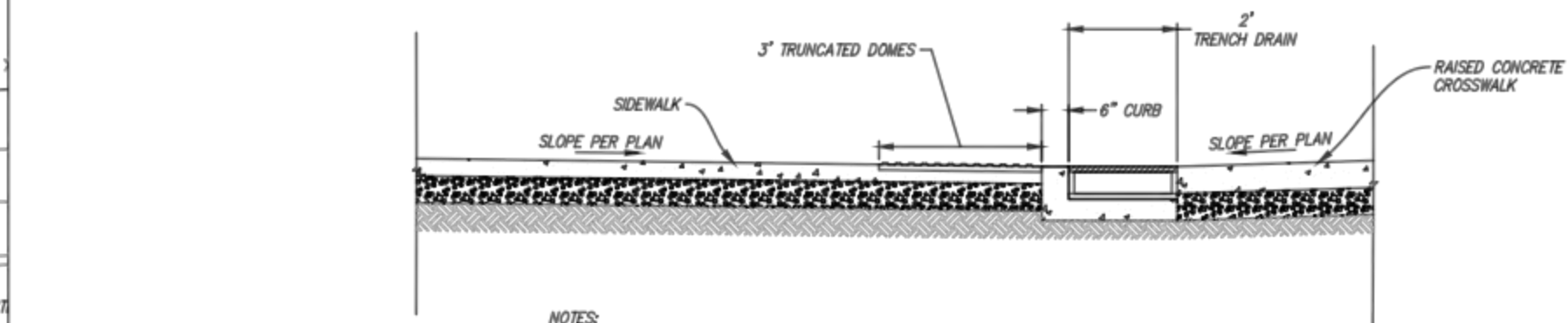
SHEET 13 OF 36

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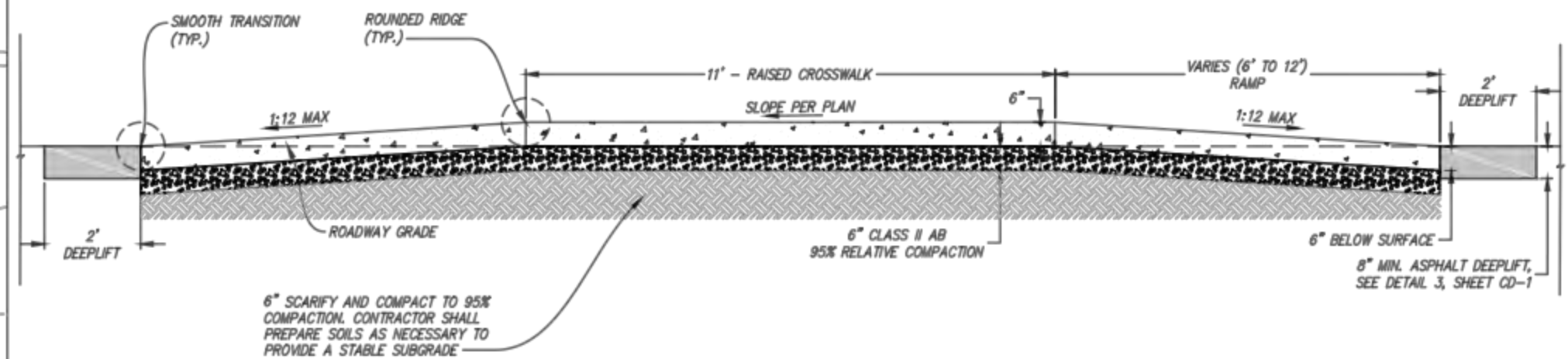
S DELAWARE ST AND
PACIFIC BLVD
(SOUTH OF 31ST AVE)

13



- NOTES:
1. SEE CURB, GUTTER, AND SIDEWALK DETAIL 141A, SHEET CD-2, FOR SECTION DEPTHS.
 2. DETECTABLE WARNING SURFACE (TRUNCATED DOMES), AS REQUIRED, PER CBC 11B-705. DETECTABLE WARNINGS SHALL BE LOCATED SO THE EDGE NEAREST THE FLUSH CURB IS 6 INCHES MIN. AND 8 INCHES MAX. FROM THE EDGE OF THE STREET OR GUTTER. DETECTABLE WARNING SHALL BE YELLOW AND APPROXIMATE FS 3353B OF FEDERAL STANDARD 595C OR APPROVED EQUIVALENT.
 3. TRENCH DRAIN SHALL BE NON-SLIP, BICYCLE FRIENDLY STEEL PLATES, BOLTED IN PLACE AND REMOVABLE FOR MAINTENANCE AND CLEANING.

SECTION B-B'
N.T.S.



- NOTES:
1. RAISED CROSSWALKS SHALL BE AT LEAST 4' AWAY FROM EDGE OF DRIVEWAY.
 2. ALL MARKINGS AND SIGNS SHALL BE REFLECTIVE.

SECTION C-C'
N.T.S.

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GRADING PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022

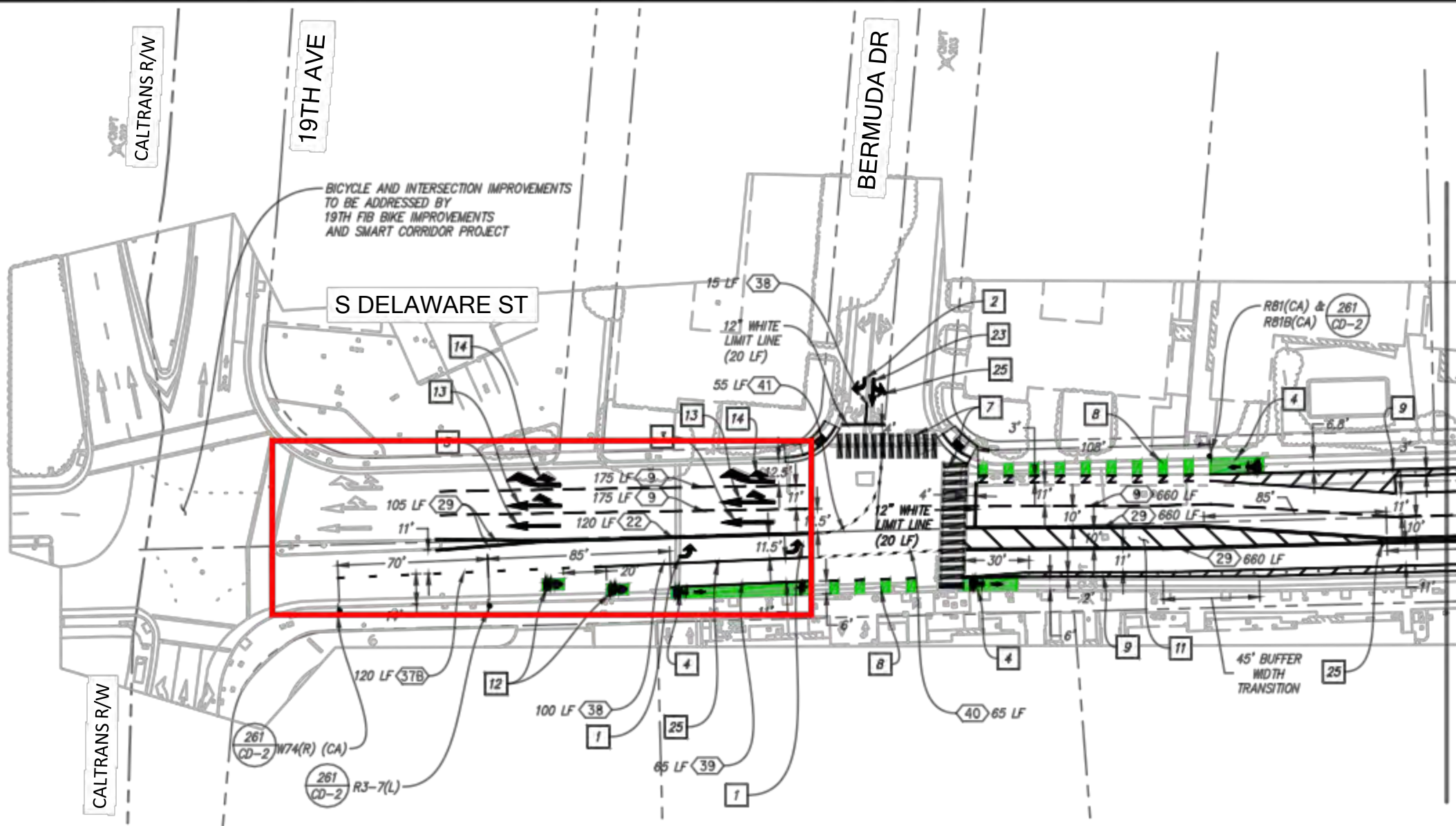


GD-6

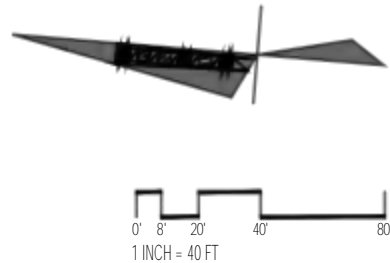
SHEET 14 OF 36

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SIGN SCHEDULE			
SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS
W74(R) (CA)	THRU TRAFFIC MERGE RIGHT	36" X 36"	1
R3-7(L)	LEFT LANE MUST TURN LEFT	36" X 36"	1
R81 (CA)	BIKE LANE	24" X 18"	1
R81B (CA)	END	8" X 5"	1



MATCHLINE -SEE SHEET SS-2



SIGNING AND STRIPING NOTES

- 1 INSTALL TYPE IV (L) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- 2 INSTALL TYPE IV (R) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- 3 INSTALL TYPE I ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- 4 INSTALL GREEN-BACKED BIKE LANE MARKING
- 7 INSTALL CONTINENTAL CROSSWALK
- 8 INSTALL BIKE LANE CONFLICT STRIPING
- 9 INSTALL BIKE LANE BUFFER WITH FLEX POSTS
- 11 INSTALL STRIPED MEDIAN
- 12 INSTALL SHARED ROADWAY BICYCLE MARKING
- 13 INSTALL TYPE II (R) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- 14 INSTALL TYPE III (R) MOD ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- 23 INSTALL TYPE VII (L) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- 25 INSTALL BLUE HYDRANT RAISED PAVEMENT MARKERS 6" FROM EDGE OF STRIPE TOWARDS THE HYDRANT SIDE.

GENERAL NOTES

- 1. STRIPING DETAILS INDICATED REFER TO THE 2018 CALTRANS STANDARD PLAN PAVEMENT MARKERS AND TRAFFIC LINES TYPICAL DETAILS.
- 2. CONTRACTOR SHALL REMOVE STRIPING/MARKINGS THAT CONFLICT WITH THIS PLAN.
- 3. STRIPING, SIGNS, AND METHODS AND MATERIALS SHALL ADHERE TO THE LATEST REVISIONS OF THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CAMUTCD) AND PROJECT SPECIFICATIONS.
- 4. ALL STRIPING SHALL BE THERMOPLASTIC UNLESS SPECIFIED OTHERWISE.
- 5. ALL EXISTING STRIPING DISTURBED DURING CONSTRUCTION ACTIVITY SHALL BE REPLACED IN KIND UNLESS OTHERWISE NOTED ON THIS PLAN.
- 6. FINAL STRIPING SHALL BE VERIFIED IN THE FIELD.
- 7. CONTRACTOR SHALL LAY OUT/ CAT TRACK PROPOSED STRIPING AND NOTIFY ENGINEER NO LESS THAN 72 HOURS BEFORE STRIPING.
- 8. REFRESH ALL CURB PAINT WITHIN THE PROJECT EXTENTS IN-KIND.
- 9. ALL EXISTING SIGNS TO REMAIN UNLESS SPECIFIED OTHERWISE.

DESIGNED BY: JP
DRAWN BY: BP TS
QC CHECKED BY: RES
PROJECT NO. 623071
SCALE
SUBMITTAL 60%

DATE
DATE
DATE

DESCRIPTION

REV NO
1
2
3
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5

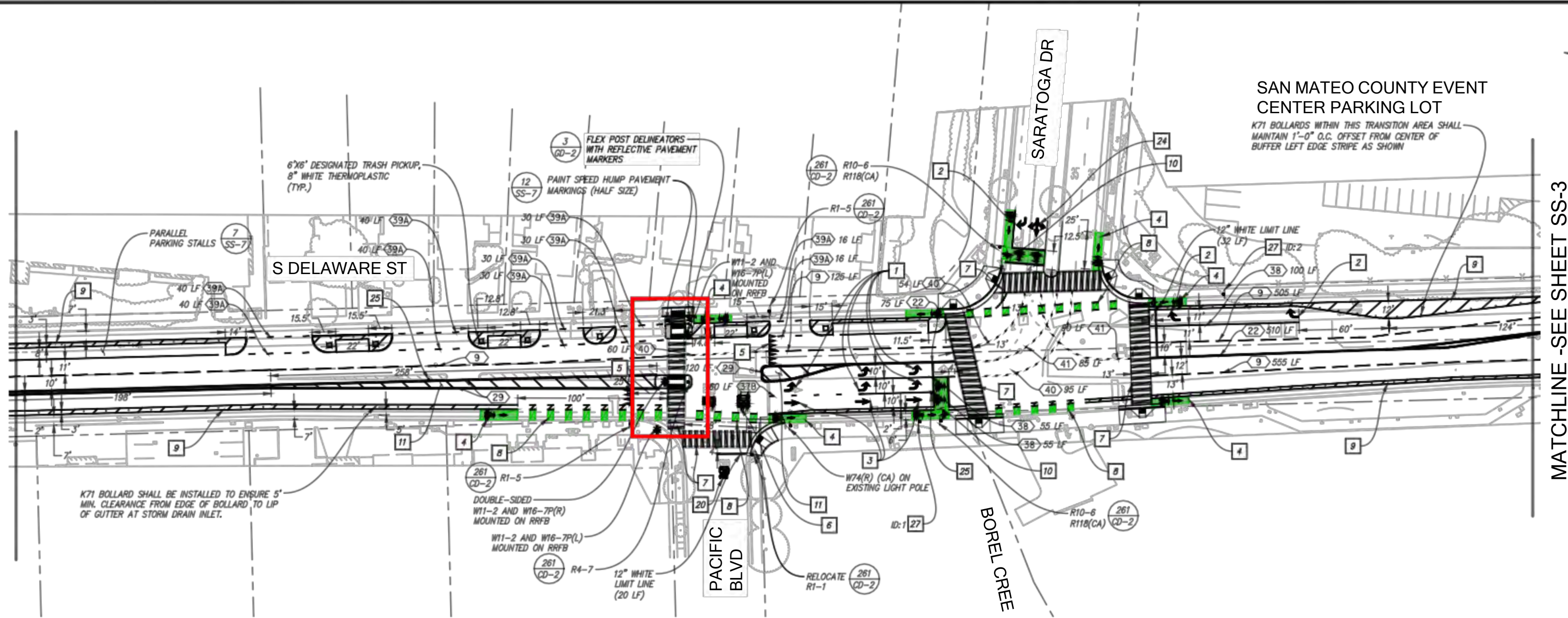
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100% PLANS
NOT FOR CONSTRUCTION

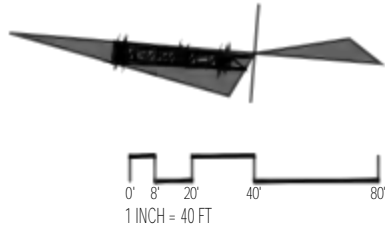
SIGNING AND STRIPING PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL

CITY PROJECT NO. 46R022

SHEET 15 OF 36



MATCHLINE -SEE SHEET SS-3



SIGN SCHEDULE

SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS
W74(R) (CA)	THRU TRAFFIC MERGE RIGHT	36" X 36"	1
R1-5	YIELD TO PEDESTRIANS	36" X 36"	2
R10-6	STOP HERE ON RED	24" X 36"	2
R118(CA)	EXCEPT BIKE	18" X 24"	2
W11-2	PEDESTRIAN CROSSING	36" X 36"	4
W16-7P(R)	DIAGONAL ARROW	24" X 12"	2
W16-7P(L)	DIAGONAL ARROW	24" X 12"	2
R4-7	KEEP RIGHT	24" X 30"	1
CUSTOM	CITY OF SAN MATEO BIKE ROUTE SIGN AND WAYFINDING SIGN, SEE DETAIL B, SS-8	24" X 18"	2

GENERAL NOTES

- STRIPING DETAILS INDICATED REFER TO THE 2018 CALTRANS STANDARD PLAN PAVEMENT MARKERS AND TRAFFIC LINES TYPICAL DETAILS.
- CONTRACTOR SHALL REMOVE STRIPING/MARKINGS THAT CONFLICT WITH THIS PLAN.
- STRIPING, SIGNS, AND METHODS AND MATERIALS SHALL ADHERE TO THE LATEST REVISIONS OF THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CAMUTCD) AND PROJECT SPECIFICATIONS.
- ALL STRIPING SHALL BE THERMOPLASTIC UNLESS SPECIFIED OTHERWISE.
- ALL EXISTING STRIPING DISTURBED DURING CONSTRUCTION ACTIVITY SHALL BE REPLACED IN KIND UNLESS OTHERWISE NOTED ON THIS PLAN.
- FINAL STRIPING SHALL BE VERIFIED IN THE FIELD.
- CONTRACTOR SHALL LAY OUT/ CAT TRACK PROPOSED STRIPING AND NOTIFY ENGINEER NO LESS THAN 72 HOURS BEFORE STRIPING.
- REFRESH ALL CURB PAINT WITHIN THE PROJECT EXTENTS IN-KIND.
- ALL EXISTING SIGNS TO REMAIN UNLESS SPECIFIED OTHERWISE.

SIGNING AND STRIPING NOTES

- INSTALL TYPE IV (L) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- INSTALL TYPE IV (R) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- INSTALL TYPE I ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- INSTALL GREEN-BACKED BIKE LANE MARKING
- INSTALL YIELD LINE PER CALTRANS STANDARD PLAN A24G
- INSTALL "KEEP CLEAR" PAVEMENT MARKING SYMBOL STANDARD PLAN A24G
- INSTALL CONTINENTAL CROSSWALK
- INSTALL BIKE LANE SKIP STRIPING
- INSTALL BIKE LANE BUFFER WITH FLEX POSTS
- INSTALL BIKE BOX
- INSTALL STRIPED MEDIAN
- INSTALL "STOP" PAVEMENT MARKING
- INSTALL TYPE VII (L) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- INSTALL TYPE VII ARROW MARKING PER CALTRANS STANDARD PLAN A24A
- INSTALL BLUE HYDRANT RAISED PAVEMENT MARKERS 6" FROM EDGE OF STRIPE TOWARDS THE HYDRANT SIDE.
- INSTALL "CITY OF SAN MATEO BIKE BOULEVARD WAYFINDING SIGN COMBINATION" PER ID PER PLANS.

DESIGNED BY: JP

DRAWN BY: BP, TS

QC CHECKED BY: RES

PROJECT NO. 62371

SCALE

SUBMITTAL 60%

DATE

DATE

DATE

DATE

DATE

DATE

DATE

DATE

DATE

DATE

DESCRIPTION

BY

DATE

DATE

DATE

100% PLANS

NOT FOR CONSTRUCTION

SIGNING AND STRIPING PLAN

DELAWARE STREET

SAFE ROUTES TO SCHOOL

CITY PROJECT NO. 46R022

SAN MATEO COUNTY

PLANNING & COMMUNITY DEVELOPMENT

SANDS

SANDS.NET

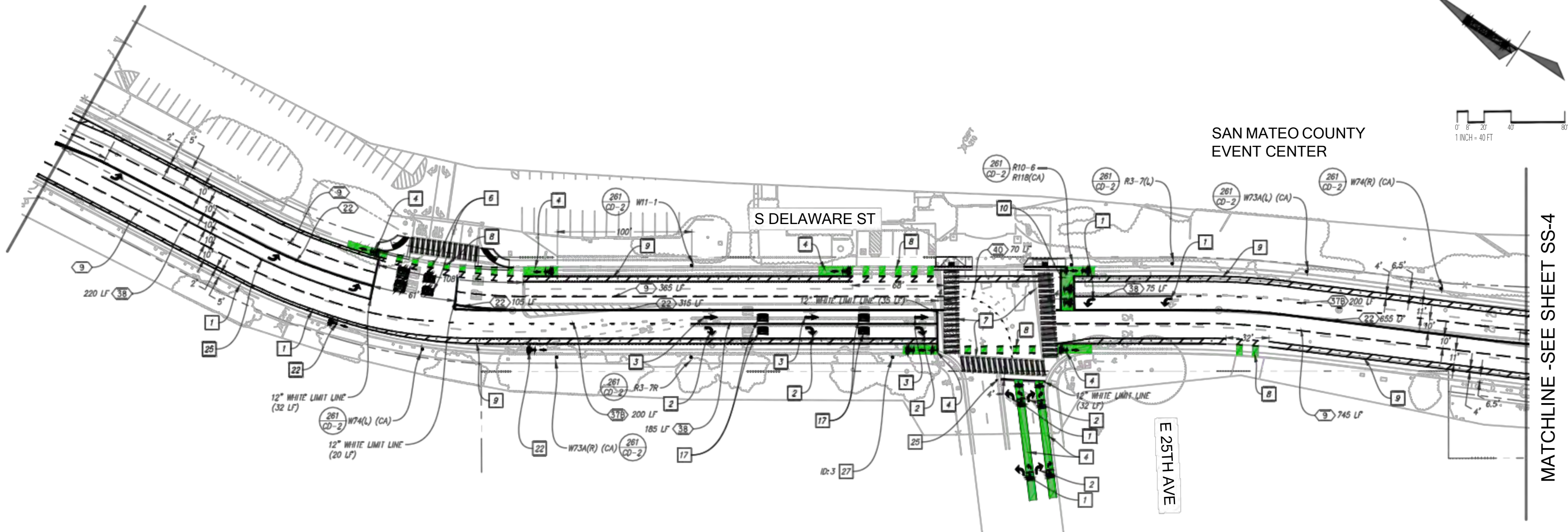
SS-2

SHEET 16 OF 36

File: X:\P\623071\4_ENGINEERING\2_PLAN SETS\3_SHEET SET\OFFSITE\6_SS-1 SIGNING AND STRIPING.dwg Date: February 21, 2025 - 4:53 PM, hpng

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MATCHLINE -SEE SHEET SS-4



SIGN SCHEDULE

SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS
W73A(L) (CA)	LEFT LANE TURNS LEFT AHEAD	36" X 36"	1
W73A(R) (CA)	RIGHT LANE TURNS RIGHT AHEAD	36" X 36"	1
W74(L) (CA)	THRU TRAFFIC MERGE LEFT	36" X 36"	1
W74(R) (CA)	THRU TRAFFIC MERGE RIGHT	36" X 36"	1
R3-7(L)	LEFT LANE MUST TURN LEFT	36" X 36"	1
R3-7(R)	RIGHT LANE MUST TURN RIGHT	36" X 36"	1
R10-6	STOP HERE ON RED	24" X 36"	1
R118(CA)	EXCEPT BIKE	18" X 24"	1
CUSTOM	CITY OF SAN MATEO BIKE ROUTE SIGN AND WAYFINDING SIGN, SEE DETAIL B, SS-8	24" X 18"	1
W11-1	BIKE	36" X 36"	1

GENERAL NOTES

1. STRIPING DETAILS INDICATED REFER TO THE 2018 CALTRANS STANDARD PLAN PAVEMENT MARKERS AND TRAFFIC LINES TYPICAL DETAILS.
2. CONTRACTOR SHALL REMOVE STRIPING/MARKINGS THAT CONFLICT WITH THIS PLAN.
3. STRIPING, SIGNS, AND METHODS AND MATERIALS SHALL ADHERE TO THE LATEST REVISIONS OF THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CMUTCD) AND PROJECT SPECIFICATIONS.
4. ALL STRIPING SHALL BE THERMOPLASTIC UNLESS SPECIFIED OTHERWISE.
5. ALL EXISTING STRIPING DISTURBED DURING CONSTRUCTION ACTIVITY SHALL BE REPLACED IN KIND UNLESS OTHERWISE NOTED ON THIS PLAN.
6. FINAL STRIPING SHALL BE VERIFIED IN THE FIELD.
7. CONTRACTOR SHALL LAY OUT/ CAT TRACK PROPOSED STRIPING AND NOTIFY ENGINEER NO LESS THAN 72 HOURS BEFORE STRIPING.
8. REFRESH ALL CURB PAINT WITHIN THE PROJECT EXTENTS IN-KIND.
9. ALL EXISTING SIGNS TO REMAIN UNLESS SPECIFIED OTHERWISE.

SIGNING AND STRIPING NOTES

1. INSTALL TYPE IV (L) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
2. INSTALL TYPE IV (R) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
3. INSTALL TYPE I ARROW MARKING PER CALTRANS STANDARD PLAN A24A
4. INSTALL GREEN BACKED BIKE LANE MARKING 
6. INSTALL "KEEP CLEAR" PAVEMENT MARKING SYMBOL STANDARD PLAN A24G
7. INSTALL CONTINENTAL CROSSWALK 
8. INSTALL BIKE LANE SKIP STRIPING 
9. INSTALL BIKE LANE BUFFER WITH FLEX POSTS 
10. INSTALL BIKE BOX 
17. INSTALL "ONLY" PAVEMENT MARKING SYMBOL STANDARD PLAN A24G
22. INSTALL BIKE LANE MARKING 
25. INSTALL BLUE HYDRANT RAISED PAVEMENT MARKERS 6" FROM EDGE OF STRIPE TOWARDS THE HYDRANT SIDE.
27. INSTALL "CITY OF SAN MATEO BIKE BOULEVARD WAYFINDING SIGN COMBINATION" PER ID PER PLANS. 

SIGNING AND STRIPING PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL

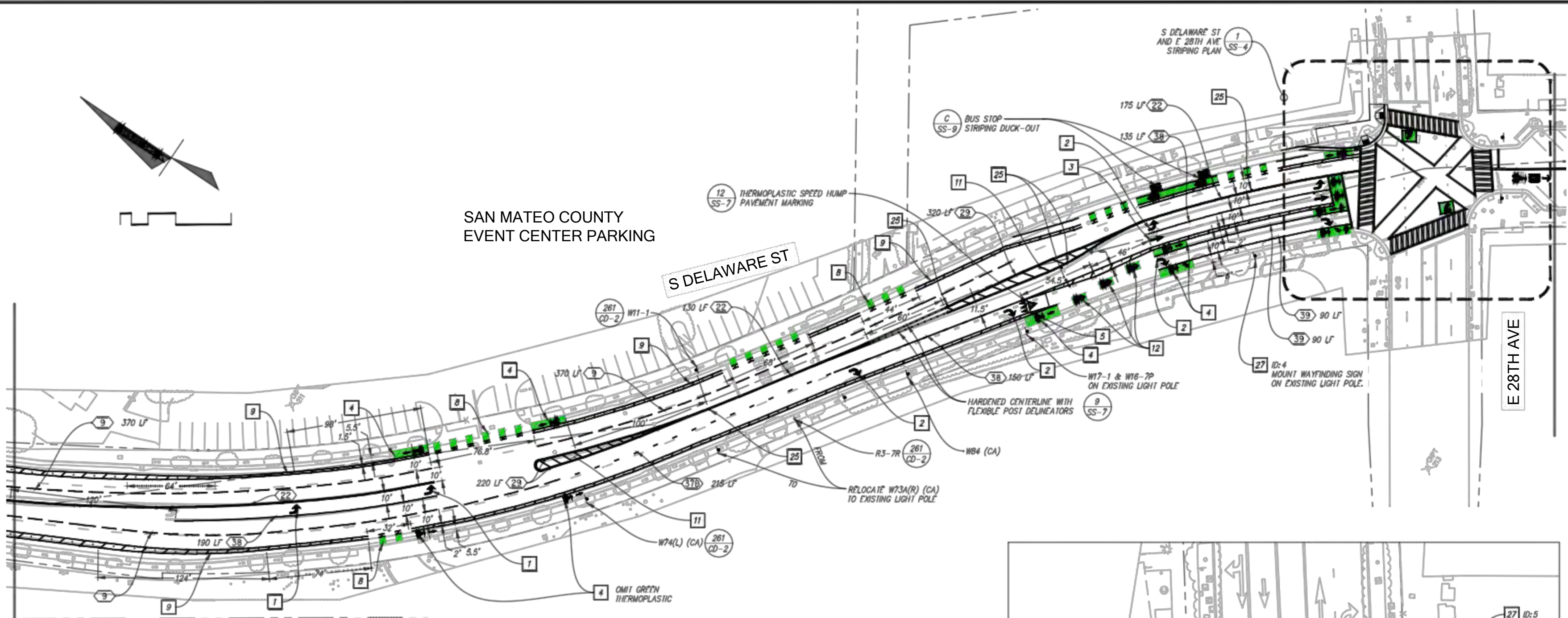
CITY PROJECT NO. 46R022



DESIGNED BY: JP	DATE
DRAWN BY: BP, TS	DATE
QC CHECKED BY: RES	DATE
PROJECT NO. 62371	
SCALE	
SUBMITTAL 60%	

SS-3

SHEET 17 OF 36



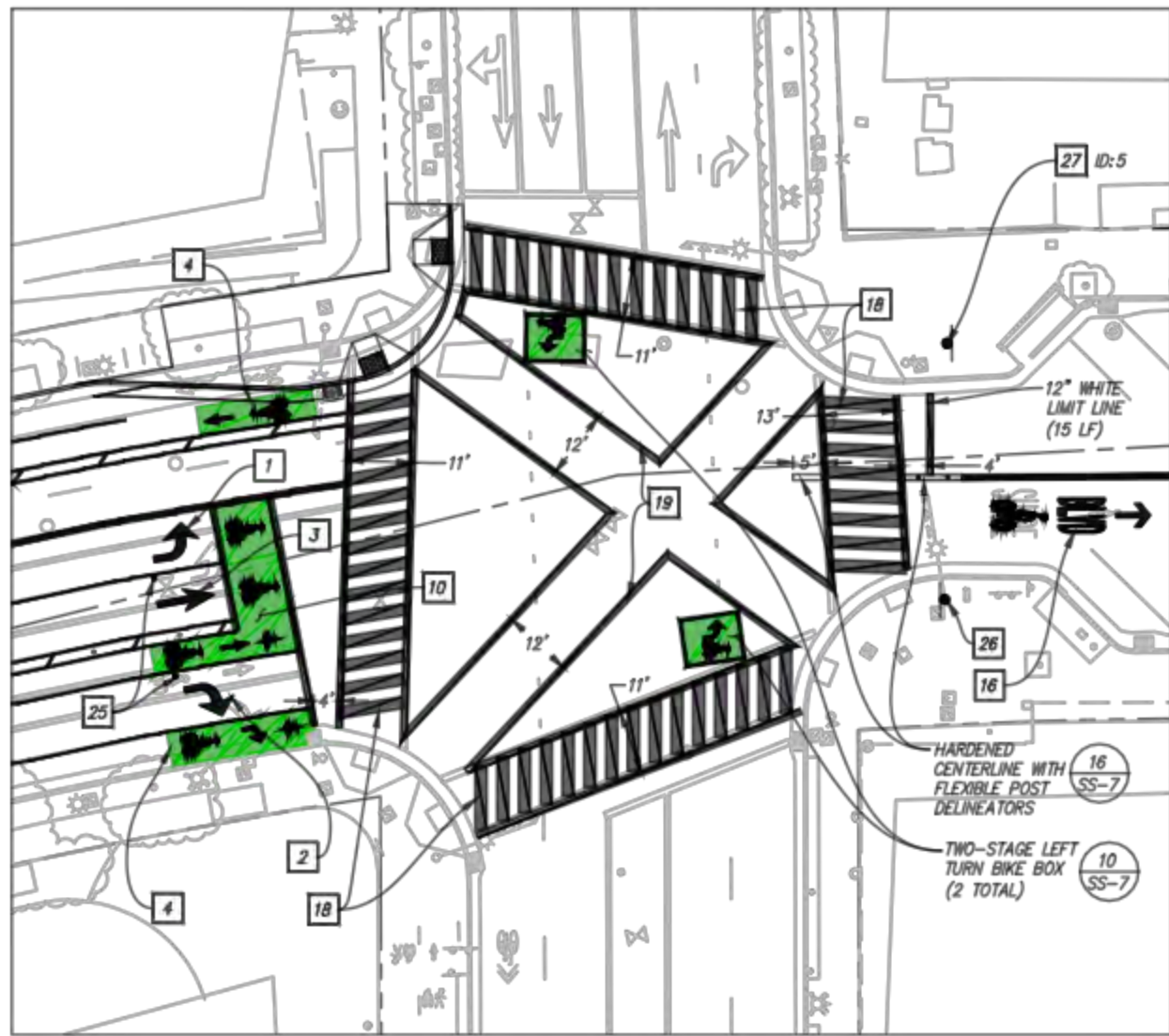
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8. REFRESH ALL CURB PAINT WITHIN THE PROJECT EXTENTS IN-KIND.
9. ALL EXISTING SIGNS TO REMAIN UNLESS SPECIFIED OTHERWISE.

SIGN SCHEDULE

SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS
W73A(R) (CA)	RIGHT LANE TURNS RIGHT AHEAD	36" X 36"	1
W74(L) (CA)	THRU TRAFFIC MERGE LEFT	36" X 36"	1
R3-7(R)	RIGHT LANE MUST TURN RIGHT	36" X 36"	1
CUSTOM	CITY OF SAN MATEO BIKE BLVD SIGN, SEE DETAIL A, SHEET SS-8	24" x 18"	1
CUSTOM	CITY OF SAN MATEO BIKE ROUTE SIGN AND WAYFINDING SIGN, SEE DETAIL B, SS-8	24" x 18"	1
W84 (CA)	SPEED HUMP AHEAD	36" X 36"	1
W17-1	SPEED HUMP	36" X 36"	1
W16-7P	DIAGONAL ARROW	24" X 12"	1
W11-1	BIKE	36" X 36"	1

SIGNING AND STRIPING NOTES

1. INSTALL TYPE IV (L) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
2. INSTALL TYPE IV (R) ARROW MARKING PER CALTRANS STANDARD PLAN A24A
3. INSTALL TYPE I ARROW MARKING PER CALTRANS STANDARD PLAN A24A
4. INSTALL GREEN BACKED BIKE LANE MARKING (13 SS-7)
5. INSTALL YIELD LINE PER CALTRANS STANDARD PLAN A24G
6. INSTALL "KEEP CLEAR" PAVEMENT MARKING SYMBOL STANDARD PLAN A24G
8. INSTALL BIKE LANE SKIP STRIPING (1 SS-7)
9. INSTALL BIKE LANE BUFFER WITH FLEX POSTS (3 SS-7)
10. INSTALL BIKE BOX (10 SS-7)
11. INSTALL STRIPED MEDIAN (2 SS-7)
12. INSTALL SHARED ROADWAY BICYCLE MARKING (4 SS-7)
15. INSTALL "BUS" PAVEMENT MARKING SYMBOL STANDARD PLAN A24G
16. INSTALL "BIKE BLVD" PAVEMENT MARKING SYMBOL (8 SS-7)
18. INSTALL YELLOW LADDER CROSSWALK PER CALTRANS STANDARD PLAN A24F
19. INSTALL YELLOW BASIC CROSSWALK PER CALTRANS STANDARD PLAN A24F
25. INSTALL BLUE HYDRANT RAISED PAVEMENT MARKERS 6" FROM EDGE OF STRIPE TOWARDS THE HYDRANT SIDE.
26. INSTALL "CITY OF SAN MATEO BIKE BOULEVARD ROUTE" SIGN ON EXISTING SIGNAL POLE. (A SS-8)
27. INSTALL "CITY OF SAN MATEO BIKE BOULEVARD WAYFINDING SIGN COMBINATION" PER ID PER PLANS. (B SS-8)



ENLARGEMENT 1
S DELAWARE ST AND
E 28TH AVE
1"=20'

1

MATCHLINE -SEE SHEET SS-5

DESIGNED BY: JP
DRAWN BY: BP TS
CHECKED BY: RES
PROJECT NO. 62071
SCALE
SUBMITAL 60%

DATE
DATE
DATE

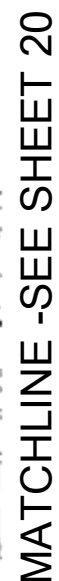
DESCRIPTION
BY
DATE

100% PLANS
NOT FOR CONSTRUCTION

SIGNING AND STRIPING PLAN
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022

SHEET 18 OF 36

SS-4



SIGN SCHEDULE			
SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS
CUSTOM	CITY OF SAN MATEO BIKE BLVD SIGN, SEE DETAIL A, SHEET SS-8	24" x 18"	4

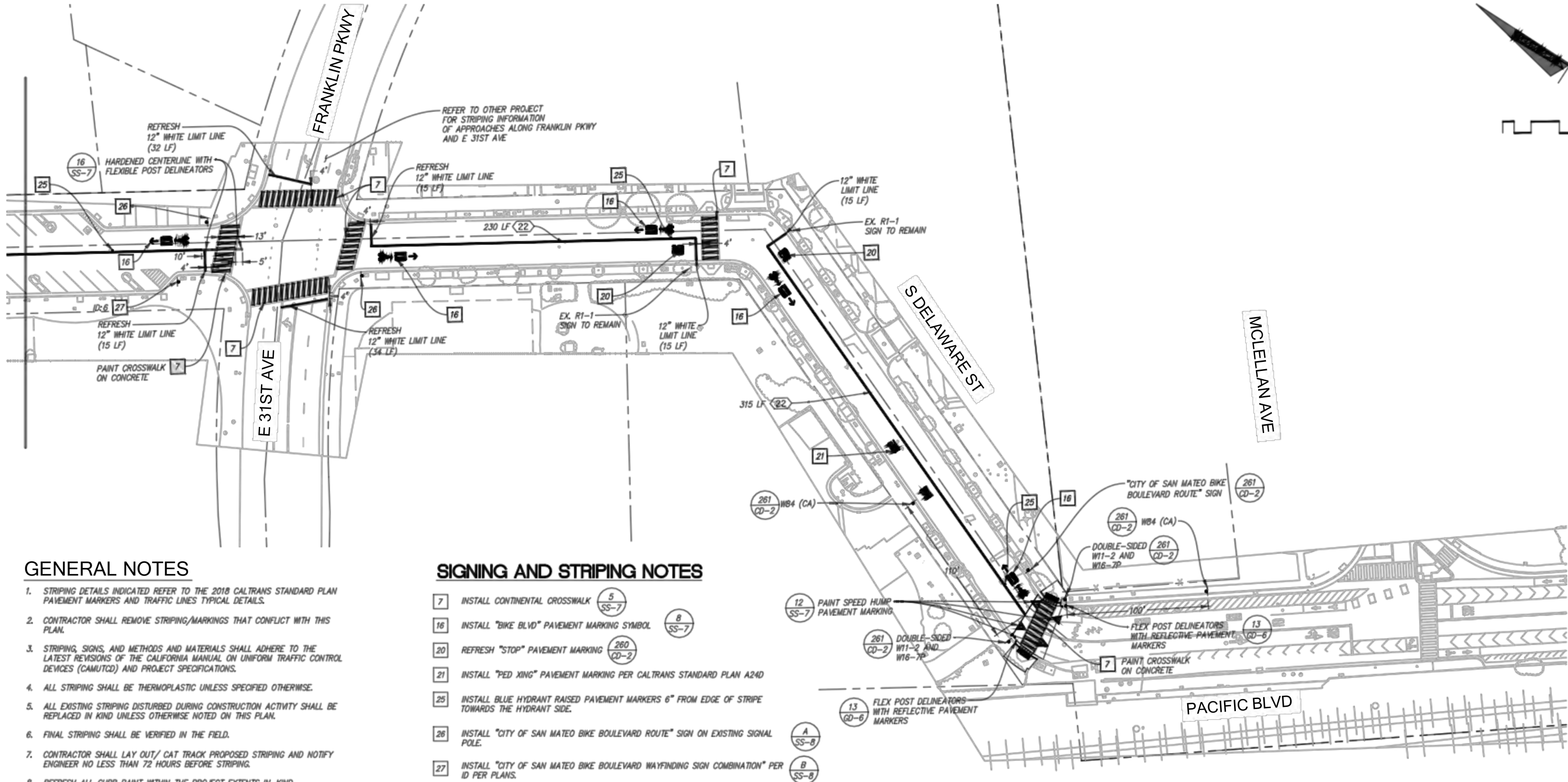
1. STRIPING DETAILS INDICATED REFER TO THE 2018 CALTRANS STANDARD PLAN PAVEMENT MARKERS AND TRAFFIC LINES TYPICAL DETAILS.
2. CONTRACTOR SHALL REMOVE STRIPING/MARKINGS THAT CONFLICT WITH THIS PLAN.
3. STRIPING, SIGNS, AND METHODS AND MATERIALS SHALL ADHERE TO THE LATEST REVISIONS OF THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CAMUTCD) AND PROJECT SPECIFICATIONS.
4. ALL STRIPING SHALL BE THERMOPLASTIC UNLESS SPECIFIED OTHERWISE.
5. ALL EXISTING STRIPING DISTURBED DURING CONSTRUCTION ACTIVITY SHALL BE REPLACED IN KIND UNLESS OTHERWISE NOTED ON THIS PLAN.
6. FINAL STRIPING SHALL BE VERIFIED IN THE FIELD.
7. CONTRACTOR SHALL LAY OUT/ CAT TRACK PROPOSED STRIPING AND NOTIFY ENGINEER NO LESS THAN 72 HOURS BEFORE STRIPING.
8. REFRESH ALL CURB PAINT WITHIN THE PROJECT EXTENTS IN-KIND.
9. ALL EXISTING SIGNS TO REMAIN UNLESS SPECIFIED OTHERWISE.

7	INSTALL CONTINENTAL CROSSWALK	$\frac{5}{SS-7}$
16	INSTALL "BIKE BLVD" PAVEMENT MARKING SYMBOL	$\frac{8}{SS-7}$
25	INSTALL BLUE HYDRANT RAISED PAVEMENT MARKERS 6" FROM EDGE OF STRIPE TOWARDS THE HYDRANT SIDE.	
26	INSTALL "CITY OF SAN MATEO BIKE BOULEVARD ROUTE" SIGN ON EXISTING SIGNAL POLE.	$\frac{A}{SS-B}$



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MATCHLINE -SEE SHEET 19



GENERAL NOTES

- 1. STRIPING DETAILS INDICATED REFER TO THE 2018 CALTRANS STANDARD PLAN PAVEMENT MARKERS AND TRAFFIC LINES TYPICAL DETAILS.
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- 3. STRIPING, SIGNS, AND METHODS AND MATERIALS SHALL ADHERE TO THE LATEST REVISIONS OF THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CAMUTCD) AND PROJECT SPECIFICATIONS.
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- 8. REFRESH ALL CURB PAINT WITHIN THE PROJECT EXTENTS IN-KIND.
- 9. ALL EXISTING SIGNS TO REMAIN UNLESS SPECIFIED OTHERWISE.

SIGNING AND STRIPING NOTES

- 7 INSTALL CONTINENTAL CROSSWALK (5 SS-7)
- 16 INSTALL "BIKE BLVD" PAVEMENT MARKING SYMBOL (8 SS-7)
- 20 REFRESH "STOP" PAVEMENT MARKING (260 CD-2)
- 21 INSTALL "PED XING" PAVEMENT MARKING PER CALTRANS STANDARD PLAN A240
- 25 INSTALL BLUE HYDRANT RAISED PAVEMENT MARKERS 6" FROM EDGE OF STRIPE TOWARDS THE HYDRANT SIDE.
- 26 INSTALL "CITY OF SAN MATEO BIKE BOULEVARD ROUTE" SIGN ON EXISTING SIGNAL POLE.
- 27 INSTALL "CITY OF SAN MATEO BIKE BOULEVARD WAYFINDING SIGN COMBINATION" PER ID PER PLANS.

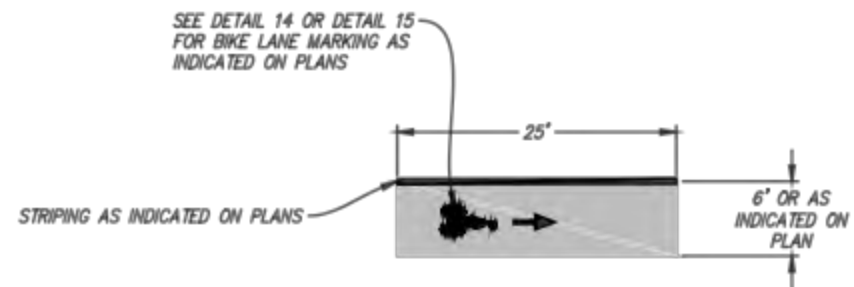
SIGN SCHEDULE

SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS
W11-2	PEDESTRIAN CROSSING	36" X 36"	4
W16-7P(R)	DIAGONAL ARROW	24" X 12"	4
W16-7P(L)	DIAGONAL ARROW	24" X 12"	4
W84 (CA)	SPEED HUMPS AHEAD	36" X 36"	2
CUSTOM	CITY OF SAN MATEO BIKE BLVD SIGN, SEE DETAIL A, SHEET SS-8	24" x 18"	3
CUSTOM	CITY OF SAN MATEO BIKE ROUTE SIGN AND WAYFINDING SIGN, SEE DETAIL B, THIS SHEET	24" x 18"	1

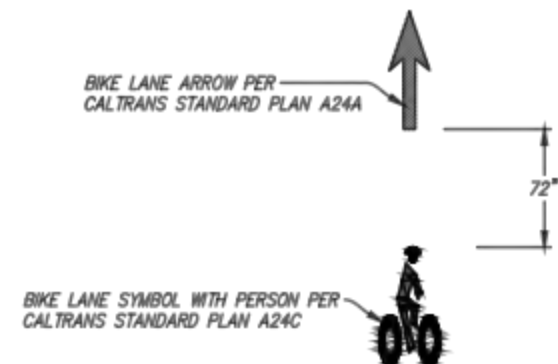
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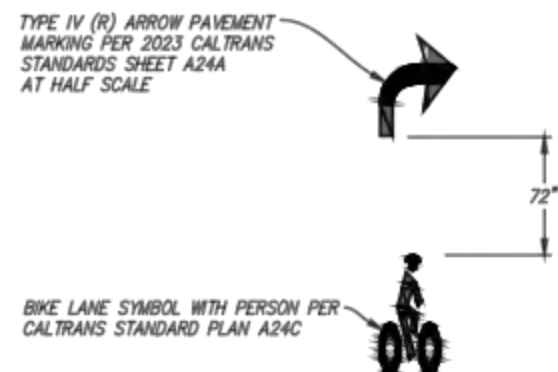
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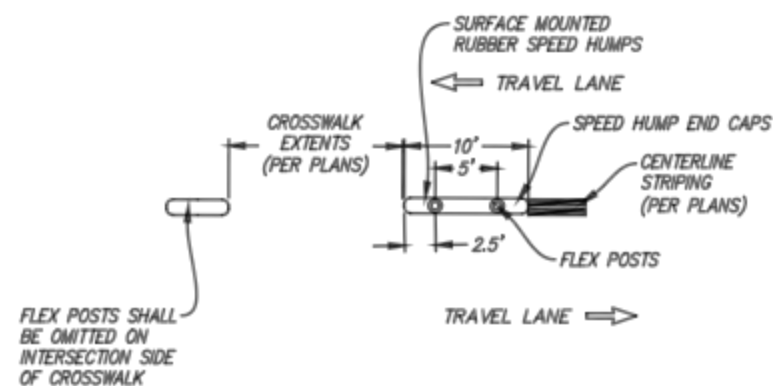
BIKE LANE
W/ GREEN THERMOPLASTIC
BACKING DETAIL
N.T.S. 13



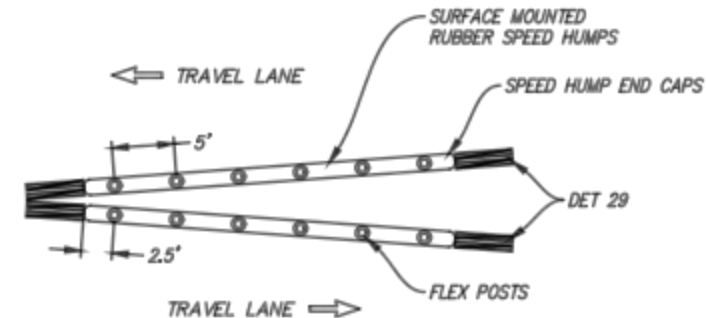
THROUGH
BIKE LANE MARKING
N.T.S. 14



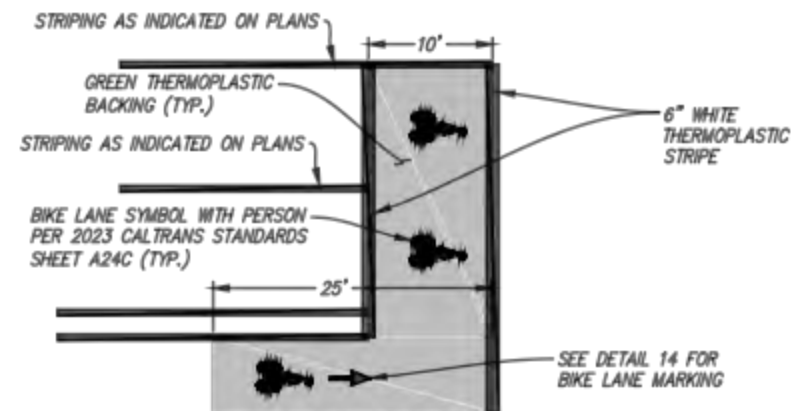
RIGHT TURN
BIKE LANE MARKING
N.T.S. 15



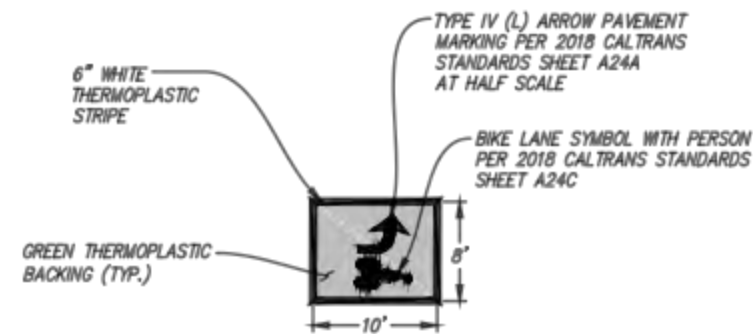
HARDENED CENTERLINE
AT INTERSECTION
N.T.S. 16



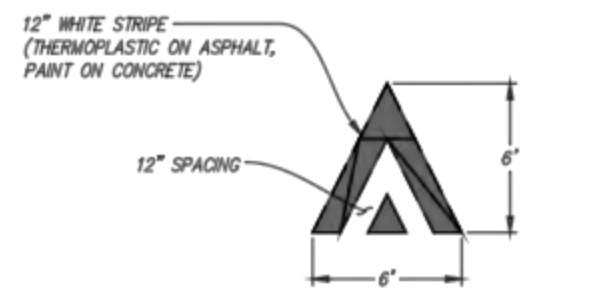
HARDENED CENTERLINE
WITH FLEXIBLE POST DETAIL
N.T.S. 9



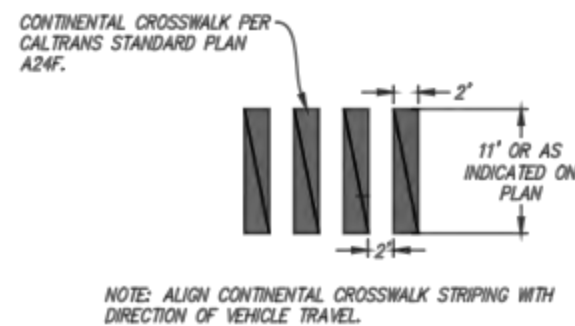
BIKE BOX
W/ GREEN THERMOPLASTIC
BACKING DETAIL
N.T.S. 10



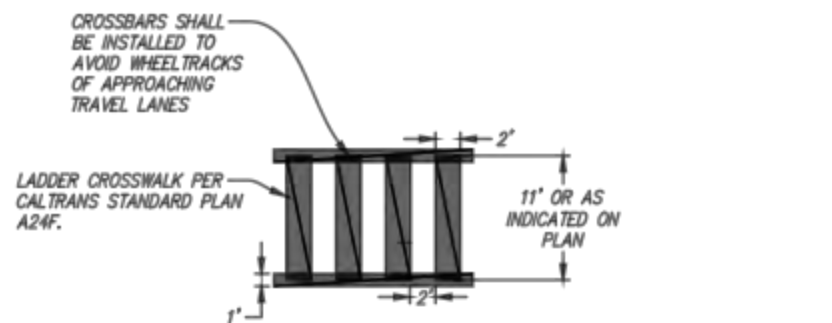
TWO-STAGE BIKE TURN-BOX
W/ GREEN THERMOPLASTIC
BACKING DETAIL
N.T.S. 11



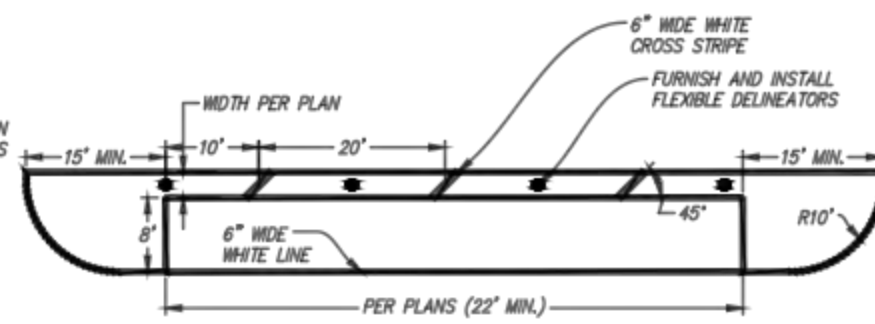
SPEED TABLE/ HUMP
PAVEMENT MARKING
N.T.S. 12



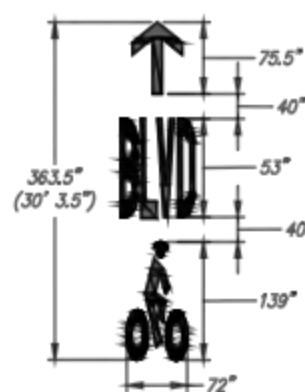
CONTINENTAL
CROSSWALK DETAIL
N.T.S. 5



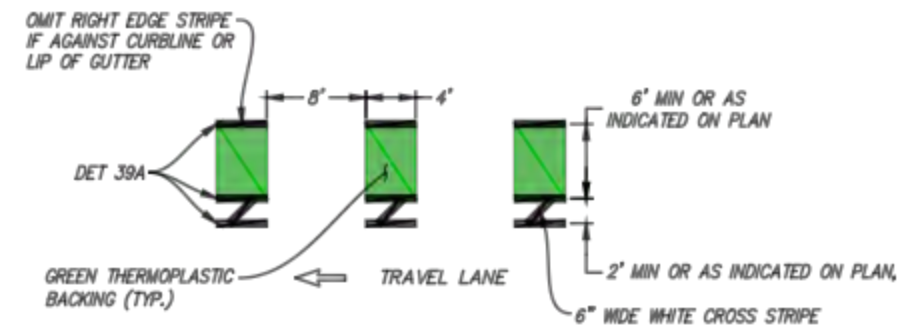
LADDER CROSSWALK DETAIL
N.T.S. 6



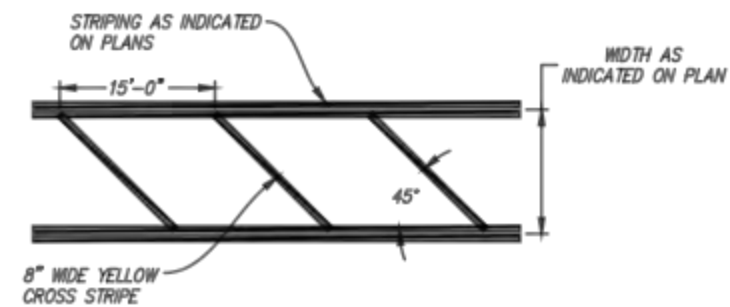
PARALLEL
PARKING STALLS
N.T.S. 7



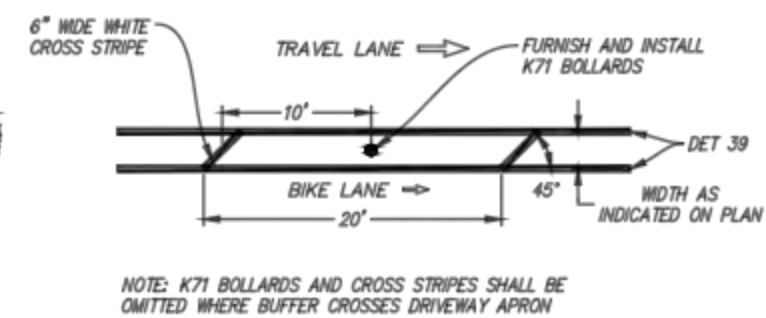
BIKE BOULEVARD
PAVEMENT MARKING DETAIL
N.T.S. 8



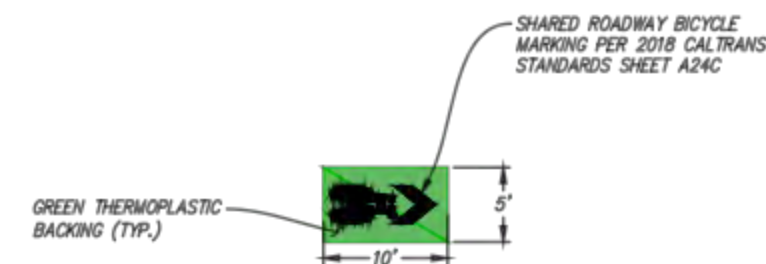
BIKE LANE
CONFLICT MARKINGS
N.T.S. 1



STRIPED MEDIAN DETAIL
N.T.S. 2



BIKE LANE BUFFER WITH
FLEXIBLE POST DETAIL
N.T.S. 3



SHARED LANE MARKING
W/ GREEN THERMOPLASTIC
BACKING DETAIL
N.T.S. 4

REV NO	DESCRIPTION	BY	DATE
1			
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100% PLANS
NOT FOR CONSTRUCTION

SIGNING AND STRIPING DETAILS
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022

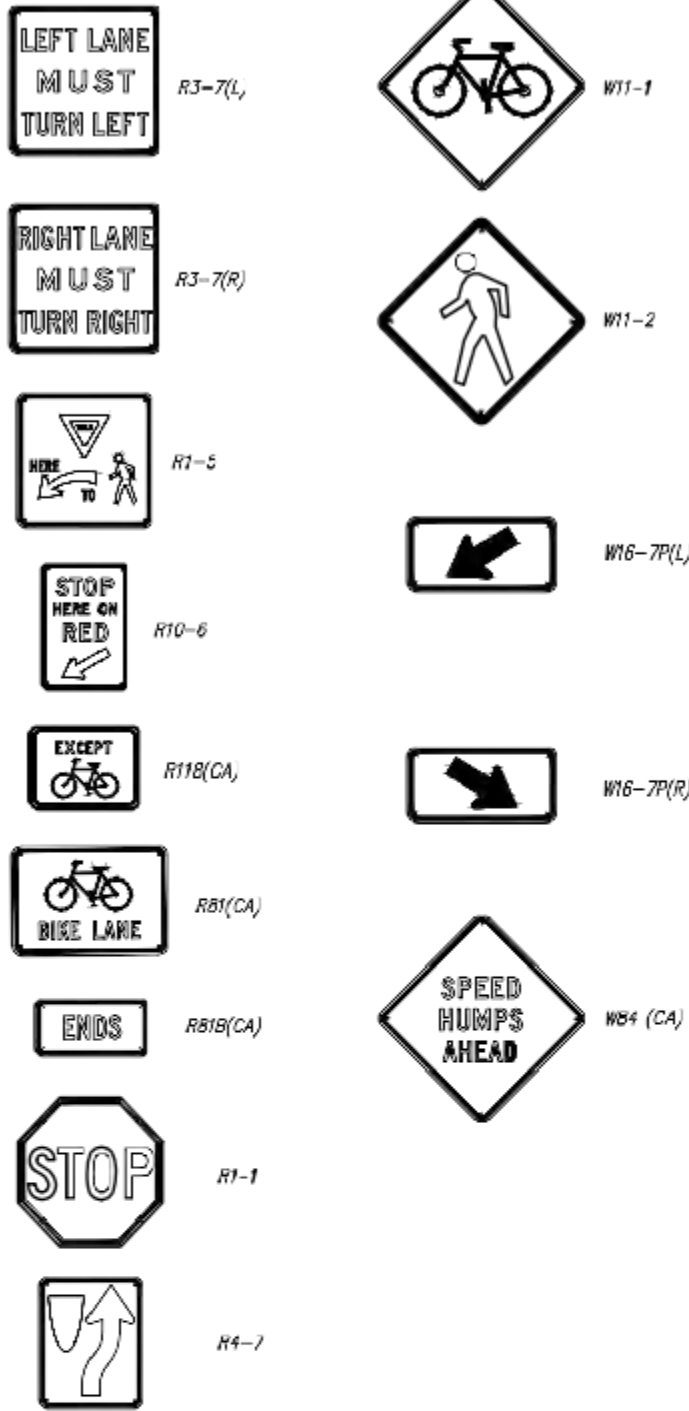
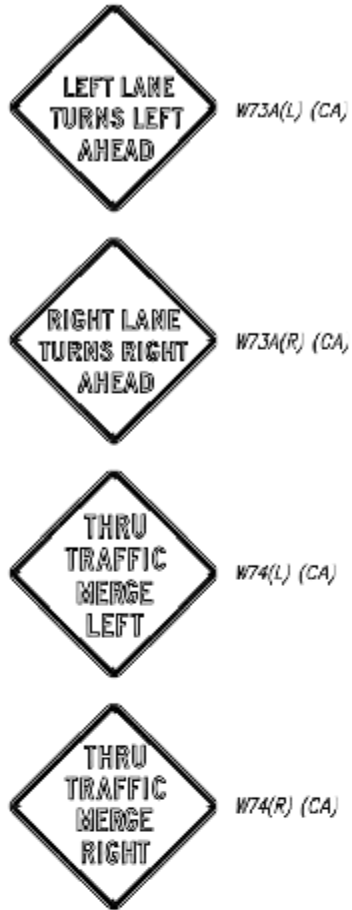


DESIGNED BY: JP	DATE
DRAWN BY: BP TS	DATE
QC CHECKED BY: RES	DATE
PROJECT NO. 62071	SCALE
SUBMITAL 60%	SUBMITAL 60%

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SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS	SHEET NO.
W73A(L) (CA)	LEFT LANE TURNS LEFT AHEAD	36" X 36"	1	SS-3
W73A(R) (CA)	RIGHT LANE TURNS RIGHT AHEAD	36" X 36"	2	SS-3,4
W74(L) (CA)	THRU TRAFFIC MERGE LEFT	36" X 36"	2	SS-3,4
W74(R) (CA)	THRU TRAFFIC MERGE RIGHT	36" X 36"	3	SS-1,2,3
R3-7(L)	LEFT LANE MUST TURN LEFT	36" X 36"	2	SS-1,3
R3-7(R)	RIGHT LANE MUST TURN RIGHT	36" X 36"	2	SS-3,4
R1-5	YIELD TO PEDESTRIANS	36" X 36"	2	SS-2
R10-6	STOP HERE ON RED	24" X 36"	3	SS-2,3
R118(CA)	EXCEPT BIKE	18" X 24"	3	SS-2,3
W11-2	PEDESTRIAN CROSSING	36" X 36"	6	SS-2,6
W16-7P(R)	DIAGONAL ARROW	24" X 12"	6	SS-2,6
W16-7P(L)	DIAGONAL ARROW	24" X 12"	6	SS-2,6
W84 (CA)	SPEED HUMPS AHEAD	36" X 36"	2	SS-6
CUSTOM	CITY OF SAN MATEO BIKE BLVD SIGN, SEE DETAIL A, THIS SHEET	24" x 18"	6	SS-4,5,6
CUSTOM	CITY OF SAN MATEO BIKE ROUTE SIGN AND WAYFINDING SIGN, SEE DETAIL B, THIS SHEET	24" x 18"	6	SS-2,3,4,6
R1-1	STOP	36" X 36"	1	SS-2
R81	BIKE LANE	12" X 8"	1	SS-1
R81B	END	8" X 5"	1	SS-1
R4-7	KEEP RIGHT	24" X 30"	1	SS-2
W11-1	BIKE	36" X 36"	1	SS-3, 4

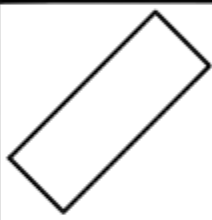
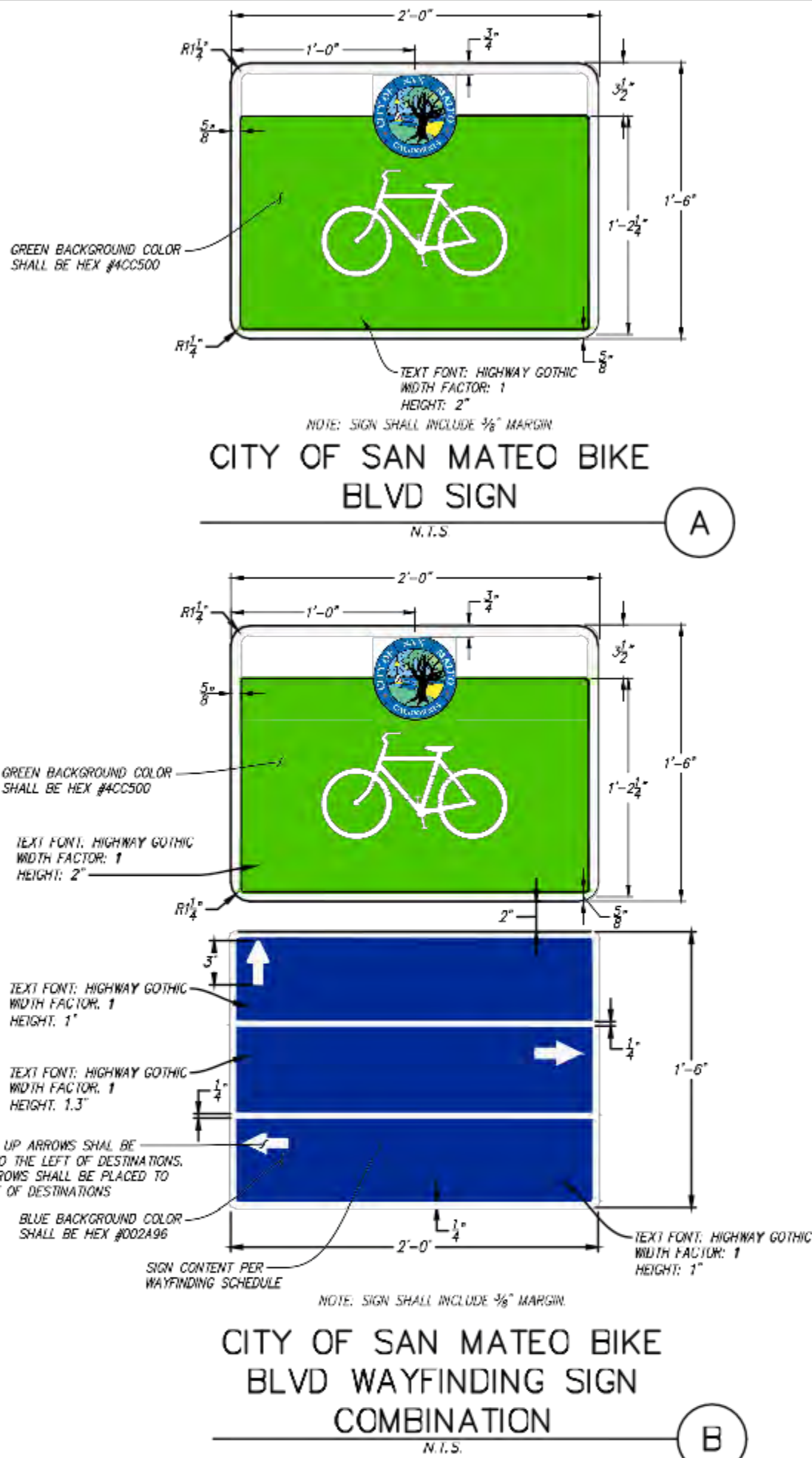
NOTE:
REFER TO TRAFFIC SIGNAL PLANS FOR SIGNAL POLE AND MAST ARM MOUNTED SIGNS.



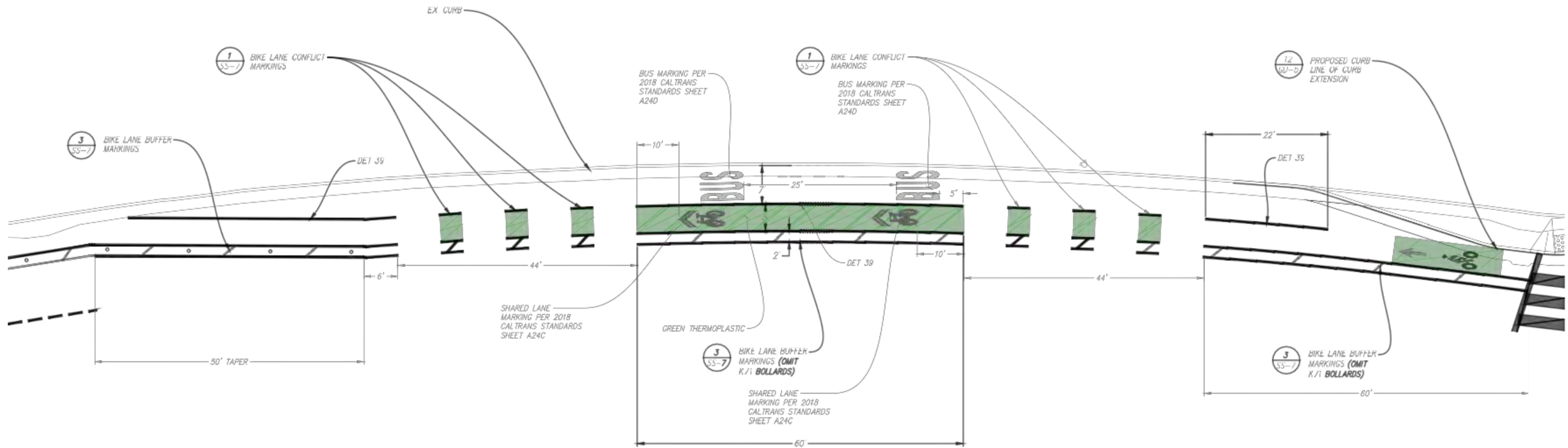
*SIGN FACES SHOWN FOR REFERENCE ONLY, REFER TO MUTCD FOR MORE INFORMATION ON SIGN DIMENSIONS AND INSTALLATION.

SIGN FACES

WAYFINDING SCHEDULE													
ID	SHEET	DESTINATIONS			DISTANCES			TIMES			ARROWS		
		1	2	3	1	2	3	1	2	3	1	2	3
1	SS-2	Sar. Mateo Event Center	Hillsdale Caltrain Station	Hillsdale Shopping Center	0.6 miles	0.8 miles	1.0 miles	4 min.	5 min.	8 min	LEFT	UP	UP
2	SS-2	Sar. Mateo Event Center	Downtown San Mateo	Hayward Park Caltrain Station	0.6 miles	1.5 miles	0.5 miles	4 min.	8 min.	4 min	RIGHT	UP	UP
3	SS-3	Hillsdale Caltrain Station	Hillsdale Shopping Center	Hillsdale High School	0.4 miles	0.8 miles	1.5 miles	2 min.	5 min.	10 min	UP	UP	UP
4	SS-4	Hillsdale Caltrain Station	Hillsdale Shopping Center	Beresford Park	0.1 miles	0.5 miles	0.8 miles	1 min.	4 min	5 min	RIGHT	RIGHT	RIGHT
5	SS-4	Hillsdale Caltrain Station	San Mateo Event Center	Beresford Park	0.1 miles	1.0 miles	0.7 miles	1 min.	5 min.	5 min	LEFT	UP	LEFT
6	SS-6	Hillsdale Caltrain Station	Hillsdale Shopping Center	Hillsdale High School	0.1 miles	0.2 miles	1.0 miles	1 min.	2 min.	6 min	RIGHT	RIGHT	RIGHT



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REV NO	DESCRIPTION	BY	DATE
1			
2			
3			
4			
5			



100% PLANS
NOT FOR CONSTRUCTION

28TH AVE BUS STOP STRIPING DETAIL
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022



DESIGNED BY: JP	DATE:
DRAWN BY: BP TS	DATE:
QC CHECKED BY: RES	DATE:
PROJECT NO: 623071	SCALE:
SUBMITTAL: 60%	

SS-9

SHEET 23 OF 36




1. ALL ELECTRICAL WORK SHALL CONFORM TO THE CITY OF SAN MATEO STANDARDS. THE LATEST CALTRANS STANDARD SPECIFICATIONS AND PLANS AND SUBSEQUENT REVISED STANDARD PLANS, AND THE LATEST VERSION OF THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD).
2. FOR ANY SIGNAL EQUIPMENT STANDARD NOTES, ABBREVIATIONS, AND LEGENDS, REFER TO CALTRANS STANDARD PLANS.
3. IN CASE OF DIFFERENCES BETWEEN CITY AND CALTRANS STANDARDS, CITY STANDARDS SHALL GOVERN.
4. ALL EXISTING ELECTRICAL EQUIPMENT TO REMAIN, UNLESS OTHERWISE NOTED, IF DAMAGED BY CONTRACTOR'S OPERATIONS, EQUIPMENT SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT THEIR EXPENSE.
5. THE CONTRACTOR SHALL CONTACT ALL UTILITIES TO VERIFY UNDERGROUND AND OVERHEAD UTILITIES. THE CONTRACTOR SHALL CALL 811, UNDERGROUND SERVICE ALERT (U.S.A.) 48 HOURS PRIOR TO ALL EXCAVATIONS.
6. THE CONTRACTOR SHALL VERIFY OVERHEAD AND UNDERGROUND CLEARANCE REQUIREMENTS WITH PG&E, AT&T, AND OTHER AFFECTED UTILITIES PRIOR TO BEGINNING OF WORK.
7. CONTRACTOR SHALL CONTACT PG&E FOR SERVICE DISCONNECTIONS, NEW SERVICE CONNECTIONS, AND VERIFY WITH PG&E REGARDING SERVICE PULL BOX AND CONDUIT REQUIREMENTS PRIOR TO BEGINNING OF WORK.
8. THE CONTRACTOR SHALL VERIFY ALL RIGHT OF WAY AND EASEMENT INFORMATION PRIOR TO CONSTRUCTION.
9. THE LOCATION OF ALL EQUIPMENT SHOWN ON THIS PLAN ARE APPROXIMATE AND ARE SUBJECT TO CHANGE TO SUIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
10. THIS PLAN IS ACCURATE FOR ELECTRICAL WORK ONLY.
11. THE CONTRACTOR SHALL VERIFY EXISTING CONDUIT CONDITIONS. NEW CONDUITS SHALL BE INSTALLED IF EXISTING CONDUITS WERE DAMAGED, AS DIRECTED BY THE ENGINEER.
12. WHERE EXISTING CONDUITS UNDER PROPOSED SIDEWALK AND CURB RAMP AREAS ARE SHOWN TO REMAIN ON THE PLAN, THE CONTRACTOR SHALL PROTECT EXISTING CONDUITS WHILE RECONSTRUCTING SIDEWALK AND CURB RAMP.
13. ALL NEW LOOP DETECTORS SHALL BE INSTALLED IN THE CENTER OF THE TRAFFIC LANE, UNLESS OTHERWISE NOTED ON PLAN. LOOP DETECTORS SHALL BE FIELD-ADJUSTED TO AVOID ANY CONFLICTS WITH EXISTING VALVE COVERS, MANHOLE COVERS, OR OTHER UTILITY FACILITIES.
14. ALL NEW PULL BOXES SHALL BE NO. 6(E), UNLESS NOTED OTHERWISE.
15. ALL NEW PULL BOXES SHALL BE LOCATED IN THE SIDEWALK WHERE APPLICABLE AND SHALL NOT BE INSTALLED WITHIN ADA TACTILE STRIP, RAMPS, OR LANDINGS.
16. ALL NEW PEDESTRIAN SIGNAL HEADS SHALL BE LED COUNTDOWN TYPE.
17. ALL NEW VEHICLE SIGNAL HEADS SHALL BE 12" LED TYPE AND SHALL HAVE BACKPLATES.
18. ALL NEW BICYCLE SIGNAL HEADS SHALL EITHER BE 4" LED TYPE WITH YELLOW HOUSING OR 8" LED TYPE WITH YELLOW HOUSINGS, BACKPLATES, AND VISORS. THE USE OF 4" AND 8" SHALL BE AS IDENTIFIED ON SIGNAL PLANS.
19. APS SHALL BE POLARA IDS2/ICCU TOUCHLESS 2-WIRE PUSH BUTTONS WITH ICPU PROCESSOR, CAMPBELL ADVISOR ACCESSIBLE PEDESTRIAN SIGNAL BY CAMPBELL OR APPROVED EQUIVALENT. FACEPLATE FOR APS SHALL BE R10-3a(CA) SIGN.
20. THE CONTRACTOR SHALL COVER AND REPAIR REMAINING HOLES AND DAMAGE ON EXISTING SIGNAL STANDARDS AFTER REMOVING EXISTING EQUIPMENT FROM EXISTING SIGNAL STANDARDS.
21. THE CONTRACTOR SHALL MAINTAIN THE OPERATION OF THE EXISTING SIGNALS AT ALL TIMES DURING CONSTRUCTION, UNLESS OTHERWISE APPROVED BY THE CITY.
22. SIGNALS SHALL BE RETURNED TO NORMAL OPERATION AT THE END OF EACH WORKING DAY AND SHALL NOT BE LEFT IN RED FLASH UNATTENDED/ OVERNIGHT UNLESS APPROVED IN ADVANCE BY ENGINEERING.
23. SALVAGED EQUIPMENT SHALL BE DELIVERED TO THE CITY OF SAN MATEO CORPORATION YARD AT 1949 PACIFIC BLVD, SAN MATEO, CA 94403. THE CONTRACTOR SHALL CALL (650)522-7300 TO ARRANGE DELIVERY PRIOR TO REMOVING EQUIPMENT.
24. F&V FOR LIGHTS AT SIGNALIZED INTERSECTIONS SHALL BE TYPE IV.
25. ALL CONDUCTORS AND CABLES SHALL BE COPPER, UNLESS OTHERWISE NOTED.
26. R/W SHOWN IS APPROXIMATE ONLY.



AB	ABANDON, IF APPLIED TO CONDUIT REMOVE CONDUCTORS
BC	INSTALL PULL BOX IN EXISTING CONDUIT RUN
CB	INSTALL CONDUIT INTO EXISTING PULL BOX
CC	CONNECT NEW AND EXISTING CONDUIT. REMOVE EXISTING CONDUCTORS AND INSTALL CONDUCTORS AS INDICATED
FI	FURNISH AND INSTALL EQUIPMENT AS INDICATED
RC	EQUIPMENT OR MATERIAL TO BE REMOVED AND BECOME THE PROPERTY OF THE CONTRACTOR
RL	RELOCATE EQUIPMENT
RS	REMOVE AND SALVAGE EQUIPMENT OR MATERIAL. EQUIPMENT TO BECOME THE PROPERTY OF THE CITY
RR	REMOVE AND REUSE EQUIPMENT
SC	SPLICE NEW TO EXISTING CONDUCTORS

		SIGNAL CONDUIT
		SIGNAL CONTROLLER CABINET
		EMERGENCY VEHICLE PREEMPTION (EVP)
		ILLUMINATED STREET NAME SIGN (ISNS)
		RETROREFLECTIVE STREET NAME SIGN (RSNS)
		PULL BOX
		PEDESTRIAN SIGNAL HEAD
		PEDESTRIAN PUSH BUTTON
		SIGNAL POLE
		PEDESTRIAN PUSH BUTTON POST
		SERVICE ENCLOSURE
		SIGN
		VIDEO DETECTION CAMERA
		CLOSED CIRCUIT TELEVISION (CCTV)
		LOOP DETECTOR
		VIDEO DETECTION ZONE
		SIGNAL HEAD
		SIGNAL MAST ARM WITH LUMINAIRE



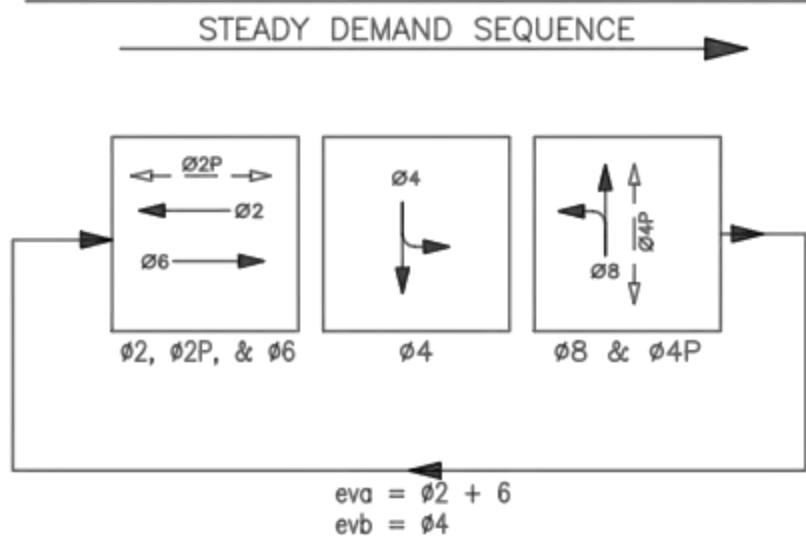
DESIGNED BY: JP		DATE:		<p>SIGNAL NOTES</p> <p>DELAWARE STREET</p> <p>SAFE ROUTES TO SCHOOL</p> <p>CITY PROJECT NO. 46R022</p>			REV NO.	DESCRIPTION	BY	DATE
DRAWN BY: BP TS		DATE:					△			
QC CHECKED BY: RES		DATE:					△			
PROJECT NO. 628071							△			
SCALE:							△			
SUBMITTAL 60%										
SHEET 24 OF 36										

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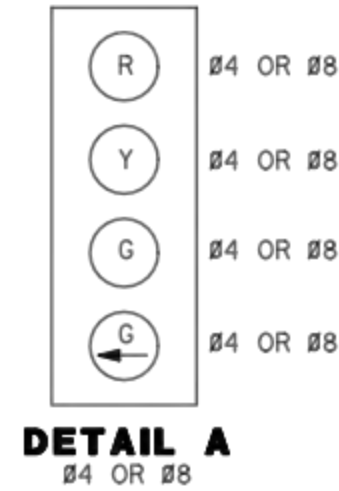
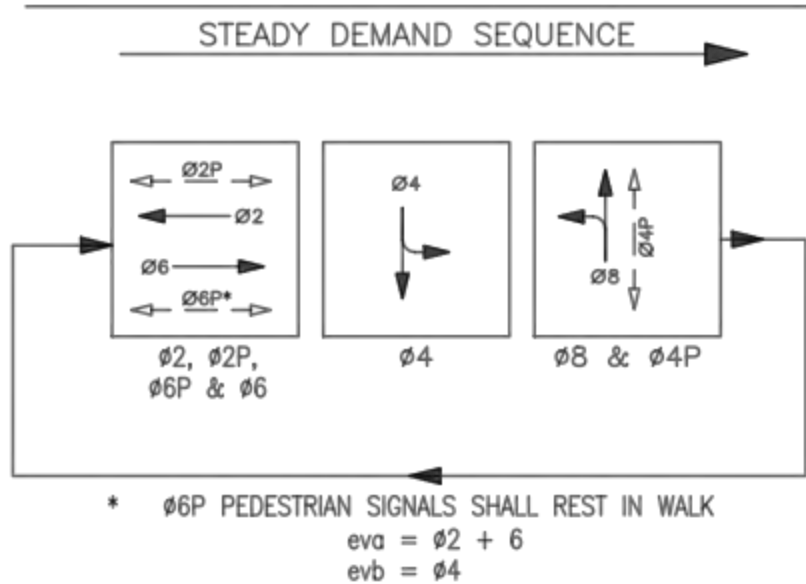
1. SEE SHEET E-1 FOR ELECTRICAL NOTES.

- 1 RS PEDESTRIAN PUSH BUTTON EQUIPMENT.
- 2 FI APS PEDESTRIAN PUSH BUTTON ON EXISTING POLE.
- 3 FI R10-11 "NO TURN ON RED" SIGN ON EXISTING POLE.
- 4 FI COUNTDOWN PEDESTRIAN SIGNAL HEAD.
- 5 AB LOOP DETECTORS IN PLACE.
- 6 FI ITERIS VANTAGE VECTOR VIDEO DETECTION CAMERA ON LUMINAIRE MAST ARM PER MANUFACTURER'S RECOMMENDATIONS. SEE CALTRANS STANDARD PLAN ES-7R FOR DETAILS.
- 7 RS EXISTING TRAFFIC SIGNAL PULL BOX. FI NEW #5 TRAFFIC SIGNAL PULL BOX OUTSIDE OF CURB RAMP AND RECONNECT EXISTING CONDUITS.
- 8 PROTECT IN PLACE EXISTING LOOP DETECTORS.
- 9 FI CCTV CAMERA ON SIGNAL POLE PER MANUFACTURER'S RECOMMENDATIONS.
- 10 RS EXISTING SIGN(S) AND FI SIGN(S) IN KIND, AS INDICATED.
- 11 FI MOUNT ISSNS ON ITS OWN MAST ARM PER MANUFACTURER'S RECOMMENDATIONS.
- 12 RC EXISTING ELECTRICAL PULL BOX. FI NEW #5 ELECTRICAL PULL BOX. PROTECT EXISTING CONDUCTORS AND CONDUITS.

EXISTING SIGNAL PHASE DIAGRAM













PROPOSED SIGNAL PHASE DIAGRAM



100% PLANS
NOT FOR CONSTRUCTION











TRAFFIC SIGNAL PLAN - BERMUDA DR
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022



CONDUCTOR SCHEDULE											
AWG	CONDUCTOR DESIGNATION	NUMBER OF CONDUCTORS									
		RUN NUMBER									
											
		EX.	EX.	EX.	EX.	NEW	EX.	EX.	EX.	EX.	EX.
#14	VEHICLE										
	Ø2	3				3	3				
	Ø4	3	3	3							
	Ø6	3	3	3							
	Ø8	3				3					
	PLU										
	Ø2	2				2	2				
	Ø4	4	2			2					
	Ø6	4	4	2							
	PPE										
	Ø2	2				2	1				
	Ø4	2	1								
SPARES	6	3	3		3	3					
	TOTAL #14	32	16	11	-	15	9	-	-	-	-
#12	PPE COMMON	2	1	1		1	1				
#10	ISNS	2				2					
#8	LIGHTING (240V)	4	2			2	2				
	SIGNAL COMMON (120V)	2	1	1		1	1				
	TOTAL #8	6	3	1	-	3	3	-	-	-	-
DETECTOR LOOP CABLE (DLC)	Ø8	1	1								
	TOTAL DETECTOR CABLES	1	1	-	-	-	-	-	-		
VIDEO DETECTION CABLE (CAT 5E)	Ø2	1				1	1				
	Ø4	1	1	1							
	Ø6	1	1								
	TOTAL VIDEO DETECTION CABLES	3	2	1	-	1	1	-	-	-	-
#6	SERVICE									3	
EVP	OPTICOM CABLE	1				1	1				
	PULL ROPE										1
SIL	FIBER										1
CCV (CAT 6)	CCV	1					1				
	SPARE	1					1				
	TOTAL CCV	2	-	-	-	-	2	-	-	-	-
	CONDUIT DESIGNATION	EX.	EX.	EX.	EX.	NEW	EX.	EX.	EX.	EX.	EX.
	CONDUIT SIZE (INCHES)	3 ½"	2 ½"	2"	1 ½"	2 ½"	2 ½"	1 ½"	1 ½"	1 ½"	2"
	CONDUIT FILL (%)	21%	21%	14%	-	15%	20%	-	-	8%	1%

NOTES:

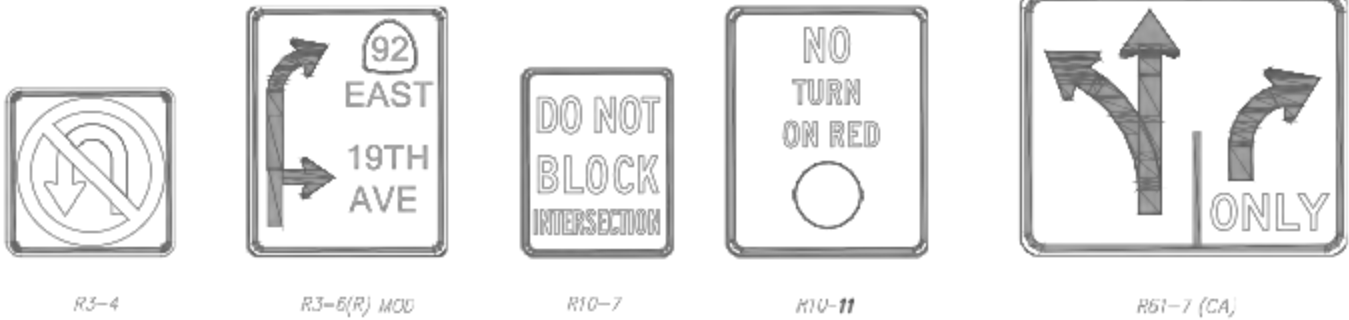
1. CABLEING AND CONDUCTOR SCHEDULES ARE SHOWN AS AN EXAMPLE OF WHAT MAY BE USED AT THE SITE. THE CONTRACTOR SHALL FIELD-VERIFY ALL EXISTING WIRING PRIOR TO BEGINNING WORK. THE CONTRACTOR SHALL MAKE FIELD ADJUSTMENTS AS NECESSARY TO PROVIDE A FULLY FUNCTIONAL SYSTEM MEETING ALL THE REQUIREMENTS OF THE CITY OF SAN MATEO. ALL PROPOSED FIELD WIRING SHALL BE APPROVED BY THE ENGINEER AND SHALL BE COMPLETED AT NO ADDITIONAL COST TO THE OWNER OR DEVELOPER. THE CONTRACTOR SHALL PROVIDE THE CITY OF SAN MATEO WITH A WIRING AS-BUILT WHEN THE WORK IS COMPLETED.
2. NEW CONDUCTORS IN THE ABOVE TABLE IS SHOWN IN BOLD.
3. STREETLIGHT CONDUCTORS SHALL BE AWG#10 FOR POLE CONNECTION AND AWG#8 IN UNDERGROUND CONDUITS, WITH 2 CONDUCTORS.
4. ISSINS TO BE FED FROM SEPARATE BREAKER IN SERVICE ENCLOSURE.

POLE AND SIGNAL SCHEDULE																	
LOCATION	STANDARD			LED LUMINAIRE (MAX WATTAGE)	VEHICLE SIGNAL			PED SIGNALS			PBA		STREET NAME SIGN	POLE LOCATION (IN INCHES)		SPECIAL REQUIREMENTS	
	TYPE	SIGNAL MAST ARM LENGTH	LUMINAIRE MAST ARM LENGTH		Ø	MAST ARM	Ø	POLE	Ø	MOUNTING	POLE QUADRANT	Ø		ARROW	POLE QUADRANT		RSNS OR ISNS
1	17-1-70	20'	15'	73 LED	6	MAS	4, 6	SV-2-TA	4, 6	SP-2-T		4			RSNS		RS EXISTING #4 PPB  APS PUSH BUTTON,  PEDESTRIAN SIGNAL HEAD.
2	16-1-70	15'	-	-	4	MAS-4B	4, 6	SV-2-TA	6	SP-1-T					RSNS		 PEDESTRIAN SIGNAL HEAD
3	19-3-70	30'	15'	73 LED	2	MAS	2, 8	SV-2-TA	2	SP-2-T		2			ISNS		RS EXISTING #2 PPB  APS PUSH BUTTON & ISNS ON ITS OWN MAST ARM.
4	19-4-100	20'	-	-	8	MAS-4B	8	SV-1-T	4	SP-1-T		2			ISNS		RS EXISTING #2 PPB  APS PUSH BUTTON & ISNS
5	1-B	-	-	-		-	2	TV-1-T	2	SP-1-T		4					RS EXISTING #4 PPB  APS PUSH BUTTON.

PROPOSED SIGN SCHEDULE

PROPOSED SIGN SCHEDULE				
SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS	SHEET NO.
R3-4	NO U-TURN	36"x36"	3	E-2
R3-6(R) MOD	MOVEMENT LANE CONTROL	36"x48"	1	E-2
W10-11	NO TURN ON RED	36"x48"	1	E-2
R10-7	DO NOT BLOCK INTERSECTION	24"x30"	1	E-2
W61-1 (CA)	MOVEMENT LANE CONTROL	54"x45"	1	E-2

NOTE:
THE ABOVE SIGN SCHEDULE EXCLUDES STREET NAME SIGNS AND IDENTIFIES ONLY THE PROPOSED SIGNS MOUNTED ON THE SIGNAL POLE
AND MAST ARM.

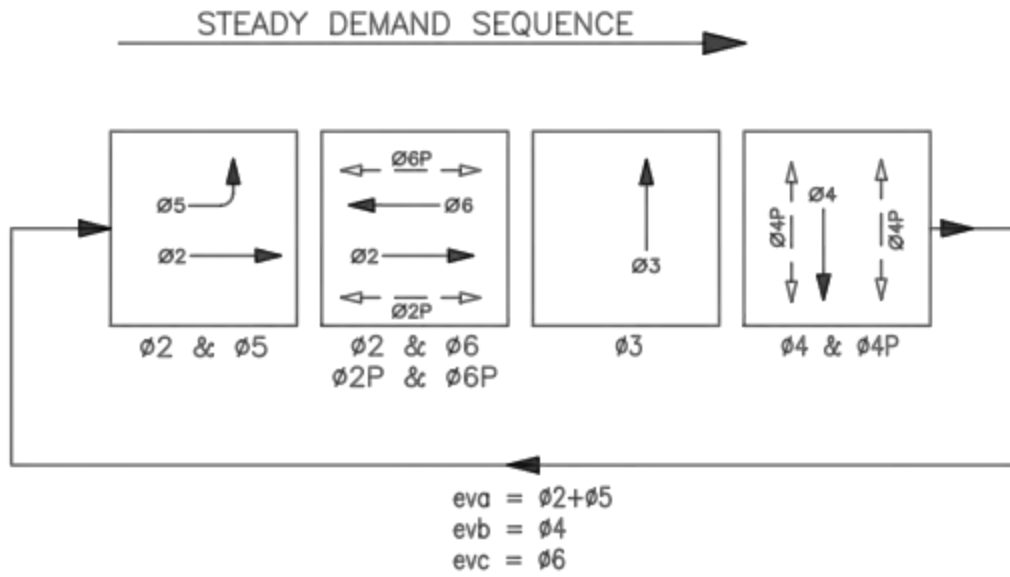


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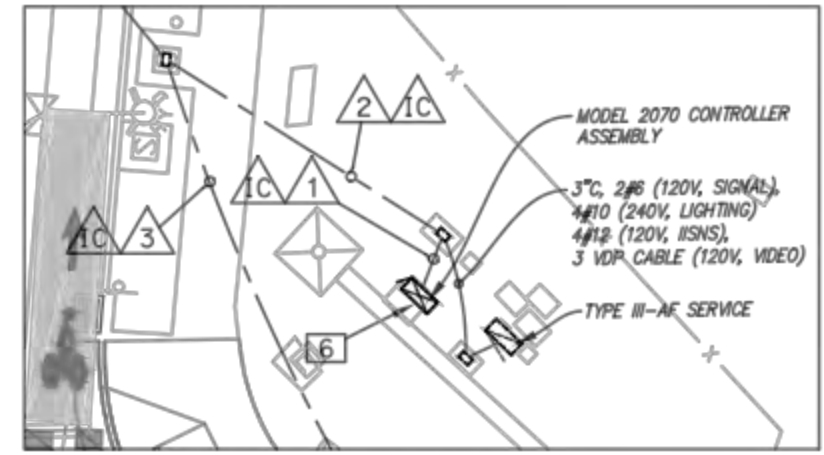
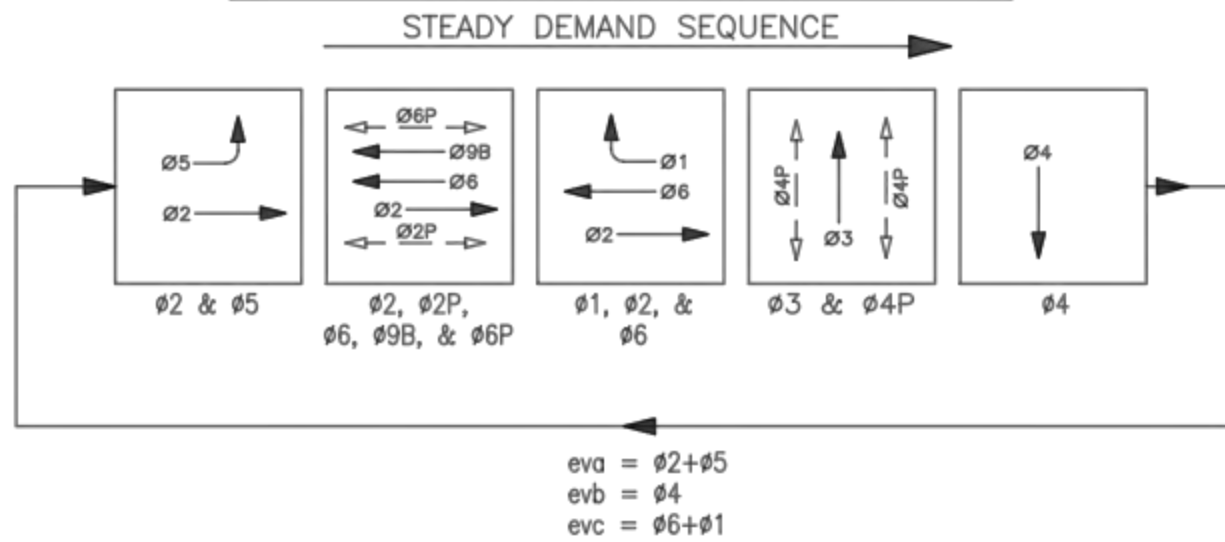
1. SEE SHEET E-1 FOR ELECTRICAL NOTES.

- 1 [RS] PEDESTRIAN PUSH BUTTON EQUIPMENT.
- 2 [FI] APS PEDESTRIAN PUSH BUTTON ON EXISTING POLE.
- 3 [FI] SIGN(S) AS INDICATED ON EXISTING POLE.
- 4 [FI] #98 4" BICYCLE SIGNAL HEAD ON POLE IN ACCORDANCE TO MUTCD 1A-16. SEE DETAIL B ON THIS SHEET FOR SIGNAL HEAD REFERENCE. SEE DETAIL C ON SHEET E-1 FOR MOUNTING DETAIL.
- 5 [RS] EXISTING #6 BALL LED MODULES. [FI] #1 RIGHT ARROW LED MODULES INTO EXISTING SIGNAL HOUSING. REMOVE CONDUCTORS FOR #1 IN SIGNAL CABINET. SEE DETAIL A ON THIS SHEET FOR REFERENCE.
- 6 [RS] EXISTING TRAFFIC SIGNAL CABINET. DEMOLISH EXISTING CABINET FOUNDATION. [FI] 342LX TRAFFIC SIGNAL CABINET ON NEW FOUNDATION. [RR] ALL EXISTING TRAFFIC SIGNAL EQUIPMENT TO NEW CABINET. THE TRAFFIC SIGNAL SHALL REMAIN FUNCTIONING UP TO AND AFTER THE SWITCH OVER. [RR] EXISTING BBS CABINET.
- 7 [RS] EXISTING VIDEO DETECTION SYSTEM. [FI] ITERIS VANTAGE VECTOR VIDEO DETECTION CAMERA ON LUMINAIRE MAST ARM PER MANUFACTURER'S RECOMMENDATIONS. CONTRACTOR SHALL FURNISH AND INSTALL ALL EQUIPMENT NECESSARY TO PROVIDE A FULLY FUNCTIONAL SYSTEM INCLUDING REPLACING THE EXISTING SYSTEM.
- 8 [FI] ITERIS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL) ON POLE TO FACE ADJACENT BICYCLE FACILITY AT INDICATED BIKE DETECTION ZONE FOR THAT FACILITY. SMART CYCLE BIKE INDICATOR SHALL BE MOUNTED BETWEEN 4 FT AND 6 FT FROM FINISH GRADE AND INSTALLED PER MANUFACTURER'S SPECIFICATIONS AND GUIDELINES.
- 9 [RS] EXISTING CCTV CAMERA. [RC] EXISTING CCTV CABLE.
- 10 [FI] CCTV CAMERA ON SIGNAL POLE PER MANUFACTURER'S RECOMMENDATIONS.
- 11 [RL] EXISTING VEHICLE HEAD AS SHOWN.
- 12 [RL] EXISTING PPB POST ON NEW FOUNDATION.
- 13 REMOVE AND DISPOSE EXISTING PPB POST FOUNDATION.
- 14 [FI] APS PEDESTRIAN PUSH BUTTON ON PROPOSED POLE/ PPB POST.
- 15 [FI] #98 8" BICYCLE SIGNAL HEAD ON POLE IN ACCORDANCE TO MUTCD 1A-16. SEE DETAIL B ON THIS SHEET FOR SIGNAL HEAD REFERENCE. SEE DETAIL D ON SHEET E-1 FOR MOUNTING DETAIL.
- 16 [RS] #6 SIGNAL HEAD. [FI] #1 SIGNAL HEAD. REUSE SALVAGED #6 SIGNAL HEAD HOUSING BY REPLACING #6 BALL LED MODULE WITH #1 RIGHT TURN ARROW LED MODULES. SEE DETAIL A ON THIS SHEET FOR REFERENCE. SEE DETAIL D ON SHEET E-1 FOR MOUNTING DETAIL.

EXISTING SIGNAL PHASE DIAGRAM



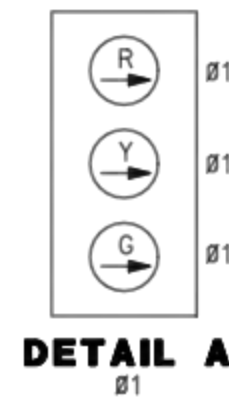
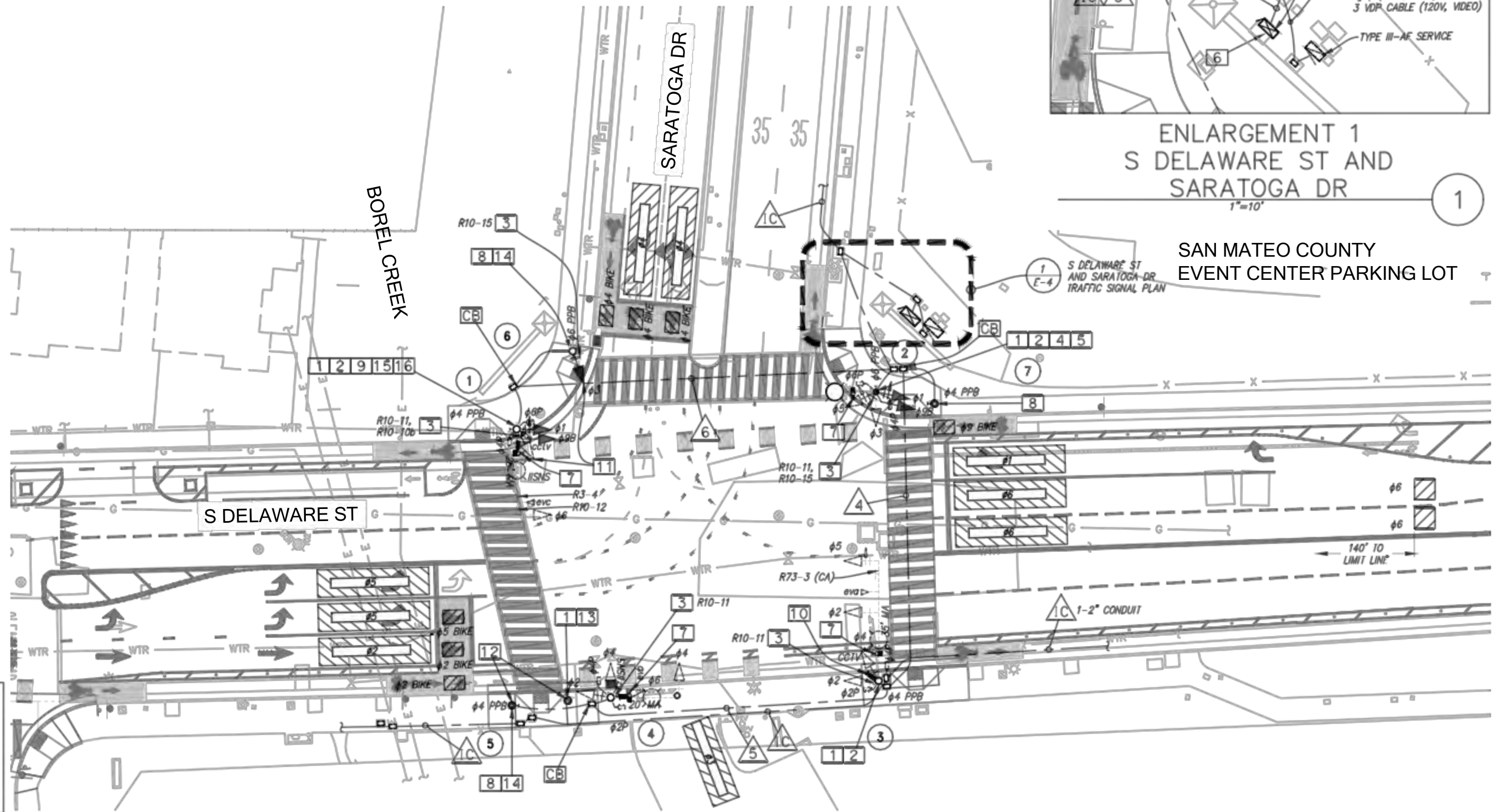
PROPOSED SIGNAL PHASE DIAGRAM



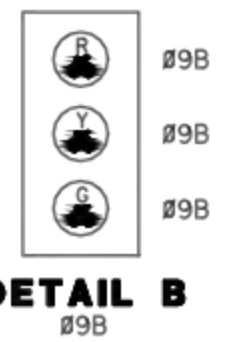
ENLARGEMENT 1 S DELAWARE ST AND SARATOGA DR

1"=10'

SAN MATEO COUNTY
EVENT CENTER PARKING LOT



DETAIL A
Ø1



DETAIL B
Ø9B

E-4

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CONDUCTOR SCHEDULE								
AWG	CONDUCTOR DESIGNATION	NUMBER OF CONDUCTORS						
		RUN NUMBER						
		EX.	EX.	EX.	EX.	EX.	EX.	EX.
#14	VEHICLE							
	#1	3	3	3			3	
	#2	3	3	3	3	3		
	#3	3	3	3			3	
	#4	3	3	3	3	3		
	#5	3	3	3	3			
	#6	6	6	6	3	3	3	
	#9B	3	3	3			3	
	PED							
	#2	2	2	2	2	2		
	#4	4	4	4	2	2	2	
	#6	2	2	2			2	
	PPB							
	#4	2	2	2	1	1	1	
	#6	1	1	1			1	
	PPB COMMON	2	2	2	1	1	1	
	SPARES	6	6	6	3	3	3	
	SMART CYCLE BIKE INDICATOR							
	#2/#5 BIKE	2	2	2	2	2		
	#4 BIKE	2	2	2			2	
	#9 BIKE	2	2	2				
	TOTAL #14	49	49	49	23	20	24	
#12	ISNS	4	4	4	2	2		
#10	SIGNAL NEUTRAL (120V)	4	4	4	2	2	2	
#8	LIGHTING (240V)	4	4	4	2	2	2	
#6	SERVICE	2						
VIDEO DETECTION CABLE (CAT 5E)	#2	1	1	1	1			
	#3	1	1	1				
	#4	1	1	1	1	1		
	#6	1	1	1			1	
	TOTAL DETECTOR CABLES	4	4	4	2	1	1	
EVF	OPTICOM CABLE	3	3	3	2	1	1	
SIC/TBS	SIGNAL INTERCONNECT+TRAILBLAZER COMM	3	3	2	2			2
CAT 6	CCTV	1	1	1	1			
	CONDUIT DESIGNATION	EX.	EX.	EX.	EX.	EX.	EX.	EX.
	CONDUIT SIZE (INCHES)	2-3"	2-3"	2-3"	2-3"	2"	2"	2-2 1/2"
	CONDUIT FILL (%)	18%	17%	17%	9%	25%	26%	1%

NOTES:

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- NEW CONDUCTORS IN THE ABOVE TABLE IS SHOWN IN BOLD.
- STREETLIGHT CONDUCTORS SHALL BE AWG#10 FOR POLE CONNECTION AND AWG#8 IN UNDERGROUND CONDUITS, WITH 2 CONDUCTORS.
- ISNS TO BE FED FROM SEPARATE BREAKER IN SERVICE ENCLOSURE.
 - UNLESS NOTED OTHERWISE.

POLE AND SIGNAL SCHEDULE																		
LOCATION	STANDARD			LED LUMINAIRE (MAX WATTAGE)	VEHICLE SIGNAL				PED. SIGNALS			PBA		STREET NAME SIGN	POLE LOCATION (IN INCHES)		SPECIAL REQUIREMENTS	
	TYPE	SIGNAL MAST ARM LENGTH	LUMINAIRE MAST ARM LENGTH		#	MAST ARM	#	POLE	#	MOUNTING	POLE QUADRANT	#	ARROW	POLE QUADRANT	RSNS OF ISNS	A		B
1	SPECIAL MAST ARM	25'	8'	120W LED	6	MAS	1, 9B	SV-1-T, SV-1-T	4, 6	SP-2-T		4	→		ISNS			[RS] EXISTING #6 SIGNAL HEAD & #6 PPB AND ISNS. [F] #4 APS PUSH BUTTONS. [F] #1 RIGHT TURN SIGNAL HEAD, #9B 8" BIKE SIGNAL HEAD, AND ISNS. SEE DETAIL D ON SHEET E-1 FOR MOUNTING DETAIL.
2	SPECIAL POLE		8'	120W LED			1, 5, 3, 9E	SV-3-1C, SV-1-T	4, 6	SP-2-T		6	→					[RS] EXISTING #6 BALL LED MODULES, #4 PPB, & #6 PPB. [F] #6 APS PUSH BUTTON. [F] #1 RIGHT TURN ARROW LED MODULES IN EXISTING #6 SIGNAL HOUSING AND #9B 4" BIKE SIGNAL HEAD. SEE DETAIL C ON SHEET E-1 FOR MOUNTING DETAIL.
3	SPECIAL MAST ARM	35'	8'	120W LED	2, 5	MAS MAS	2, 4	SV-2-TB	2, 4	SP-2-T		4	→					[RS] EXISTING #4 PPB, [F] APS PUSH BUTTON AND CCTV CAMERA.
4	SPECIAL MAST ARM	20'	8'	120W LED	4	MAS	2, 6	SV-3-TA	2, 4	SP-2-T					ISNS			[RS] ISNS. [F] ISNS.
5	PPB PUSH											4	→				48"	[F] #4 APS PUSH BUTTON AND ITERRIS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL).
6	1E						3	TV--T				6	→			62"	66"	[F] #6 APS PUSH BUTTON AND ITERRIS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL). [RR] #3 VEHICLE HEAD.
7	PPB PUSH											4	←			-108"	48"	[F] #4 APS PUSH BUTTON AND ITERRIS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL).

PROPOSED SIGN SCHEDULE				
SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS	SHEET NO.
R10-11	NO TURN ON RED	36"x48"	4	E-4
R10-15	TURNING TRAFFIC MUST YIELD TO PEDESTRIANS	30"x30"	2	E-4
R10-10b	BIKE SIGNAL	18"x24"	1	E-4

NOTE:
THE ABOVE SIGN SCHEDULE EXCLUDES STREET NAME SIGNS AND IDENTIFIES ONLY THE PROPOSED SIGNS MOUNTED ON THE SIGNAL POLE AND MAST ARM.



R10-11



R10-15



R10-10b



TRAFFIC SCHEDULE - SARATOGA DR
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022

100% PLANS
NOT FOR CONSTRUCTION



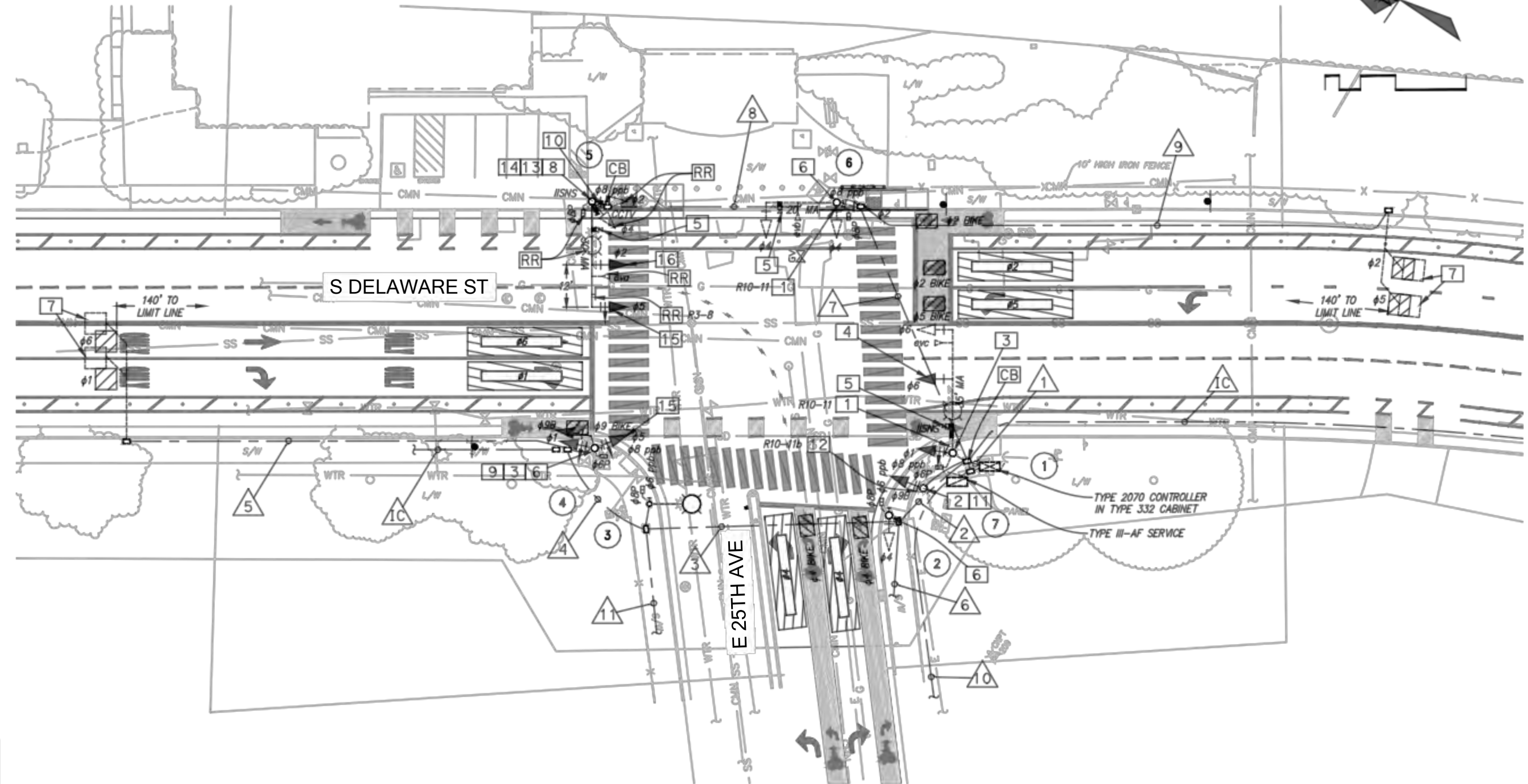
REV NO	DESCRIPTION	BY	DATE
1			
2			
3			
4			
5			

1. SEE SHEET E-1 FOR ELECTRICAL NOTES.

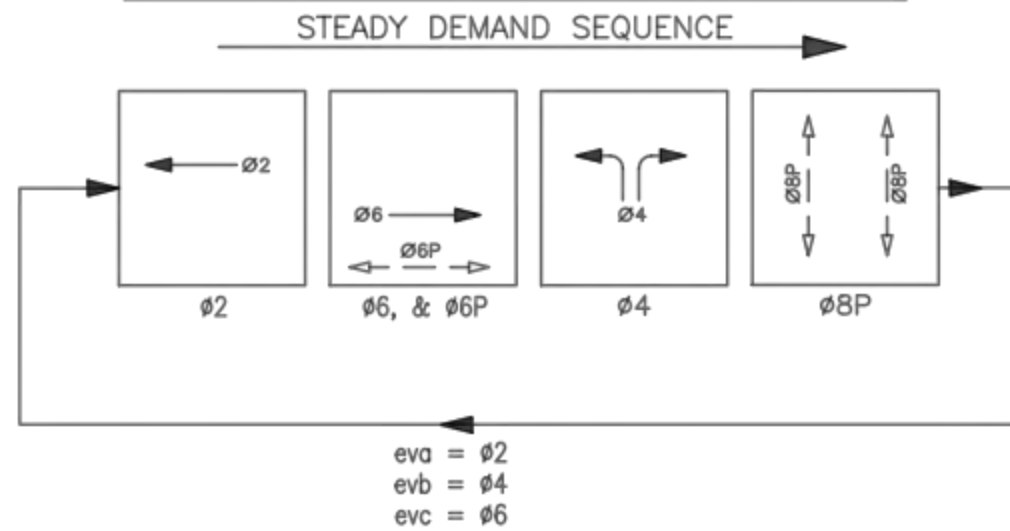
1. SEE SHEET E-1 FOR ELECTRICAL NOTES.

1 **FI** R10-11 "NO TURN ON RED" SIGN ON EXISTING POLE.

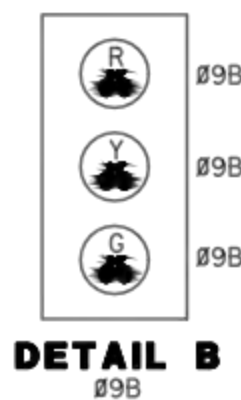
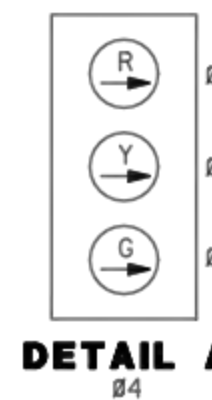
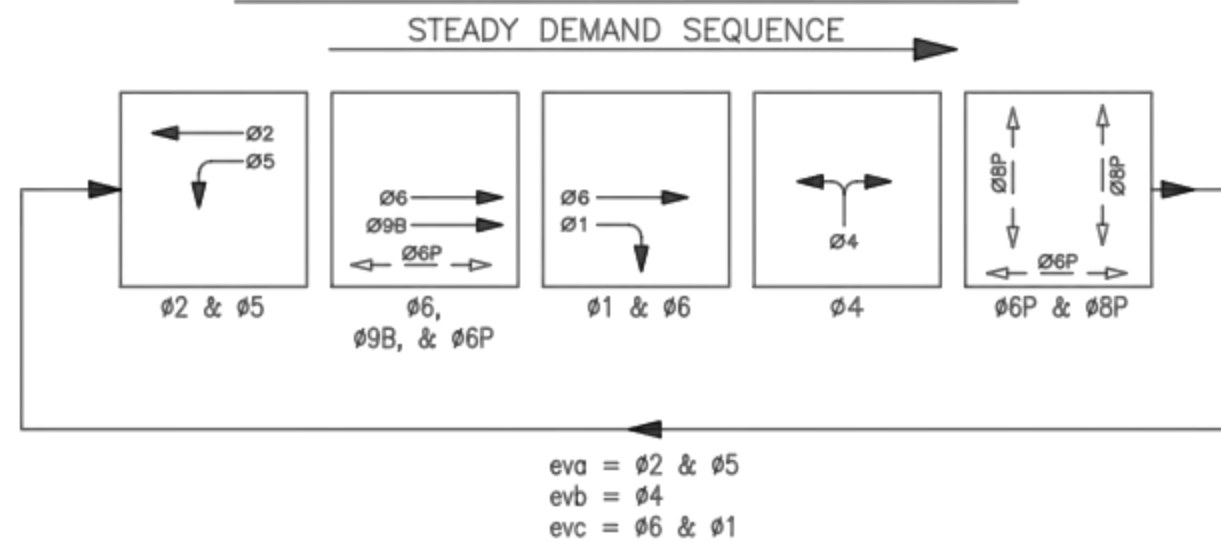
- 7 **F1** #98 8" BICYCLE SIGNAL HEAD ON POLE IN ACCORDANCE TO MUTCD 1A-16. SEE DETAIL B ON THIS SHEET FOR REFERENCE.
- 8 **RS** EXISTING #6 BALL LED MODULES. **F1** #1 RIGHT ARROW LED MODULES INTO EXISTING SIGNAL HOUSING. SEE DETAIL A ON THIS SHEET FOR REFERENCE.
- 9 **F1** #6 SIGNAL HEAD ON EXISTING MAST ARM POLE 1.
- 10 **RS** EXISTING VIDEO DETECTION SYSTEM. **F1** ITERIS VANTAGE VECTOR VIDEO DETECTION CAMERA ON LUMINAIRE MAST ARM PER MANUFACTURER'S RECOMMENDATIONS. CONTRACTOR SHALL FURNISH AND INSTALL ALL EQUIPMENT NECESSARY TO PROVIDE A FULLY FUNCTIONAL SYSTEM INCLUDING REPLACING THE EXISTING SYSTEM.
- 11 **F1** ITERIS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL) ON POLE TO FACE ADJACENT BICYCLE FACILITY AT INDICATED BIKE DETECTION ZONE FOR THAT FACILITY. SMART CYCLE BIKE INDICATOR SHALL BE MOUNTED BETWEEN 4 FT AND 6 FT FROM FINISH GRADE AND INSTALLED PER MANUFACTURER'S SPECIFICATIONS AND GUIDELINES.
- 12 **AB** LOOP DETECTORS IN PLACE.
- 13 **F1** CCTV CAMERA ON SIGNAL POLE PER MANUFACTURE'S RECOMMENDATIONS.
- 14 **F1** #98 4" BICYCLE SIGNAL HEAD ON POLE IN ACCORDANCE TO MUTCD 1A-16. SEE DETAIL B ON THIS SHEET FOR SIGNAL HEAD REFERENCE. SEE DETAIL C ON SHEET E-1 FOR MOUNTING DETAIL.
- 15 **F1** MOUNT ISSNS ON ITS OWN MAST ARM PER MANUFACTURE'S RECOMMENDATIONS.
- 16 **F1** TYPE 1-B POLE ON NEW FOUNDATION.
- 17 **F1** R10-11b "BIKE SIGNAL" SIGN ON PROPOSED POLE.
- 18 **RS** TYPE 17-1-70 POLE AND ALL MOUNTED EQUIPMENT EXCEPT WHEN INDICATED FOR **RR**. REMOVE AND DISPOSE EXISTING POLE FOUNDATION.
- 19 **F1** TYPE 19-4-100 POLE ON NEW FOUNDATION. **RR** AS INDICATED ON PLAN: #8P PEDESTRIAN SIGNAL HEAD, SIGN, #2 SIGNAL HEAD, #4 SIGNAL HEAD, LUMINAIRE FIXTURE, AND EMERGENCY VEHICLE PREEMPTION (EVP) env.
- 20 **F1** #5 SIGNAL HEAD WITH LEFT ARROW LED MODULES.
- 21 **F1** #2 SIGNAL HEAD ON MAST ARM.



STEADY DEMAND SEQUENCE



STEADY DEMAND SEQUENCE



CONDUCTOR SCHEDULE														
AWG	CONDUCTOR DESIGNATION	NUMBER OF CONDUCTORS												
		RUN NUMBER												
		EX	EX	EX	EX	EX	EX	EX	EX	EX	EX	EX	EX	EX
#14	VEHICLE													
	#1	3	3	3	3									
	#2	3						3	3					
	#4	9	3					6	3					
	#5	6	3	3	3			3	3					
	#6	3	3											
	#8B	3	3	3	3									
	PED													
	#6	4	2	2	2									
	#6	8	4	2				4	2					
	PPB													
	#6	2	2	1										
	#8	4	1	1	1			2	1					
	PPB COMMON	6	3	2	1			2	1					
#10	SPARES	6	3	3	3			3	3					
	SMART CYCLE BIKE INDICATOR													
	#2 BIKE	2						2						
	#4 BIKE	2	2											
	#8 BIKE	2	2	2	2									
	TOTAL #14	63	34	22	18	-	-	25	18	-	-	-	-	-
	ISNS	4						2	2					
	SIGNAL COMMON	2	1	1	1			1	1					
	TOTAL #10	6	1	1	1	-	-	3	3	-	-	-	-	-
	#8													
	LUMINAIRES	4	2	2				2	2					
	#2 + #5	1						1	1					
	#4	1						1						
	#6 + #1	1												
#6	VIDEO DETECTION CABLES (CAT 5E)													
	TOTAL DETECTOR CABLES	3	-	-	-	-	-	2	1	-	-	-	-	-
	EVF													
	OPTICUM CABLE	3						2	1					
	SERVICE													
	#6													
	SIGNAL INTER-CONNECT (SIC)	1	1	1	1									1
	FIBER	1	1	1	1									1
	CAT 6													
	CCIV	1						1	1					
	CONDUIT DESIGNATION	EX	EX	EX	EX	EX	EX	EX	EX	EX	EX	EX	EX	EX
	CONDUIT SIZE (INCHES)	3 1/2"	3"	2 1/2"	2 1/2"	1 1/2"	1 1/2"	2 1/2"	2 1/2"	1 1/2"	1 1/2"	1 1/2"	2"	
	CONDUIT FILL (%)	26%	11%	12%	8%	-	-	28%	20%	-	12%	-	1%	

NOTES:

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- NEW CONDUCTORS IN THE ABOVE TABLE IS SHOWN IN BOLD.
- STREETLIGHT CONDUCTORS SHALL BE AWG#10 FOR POLE CONNECTION, AND AWG#8 IN UNDERGROUND CONDUITS, WITH 2 CONDUCTORS
- ISNS TO BE FED FROM SEPARATE BREAKER INSIDE SERVICE ENCLOSURE

POLE AND SIGNAL SCHEDULE																			
LOCATION	STANDARD			LED LUMINAIRE (MAX WATTAGE)	VEHICLE SIGNAL				PED. SIGNALS			PBA		STREET NAME SIGN	POLE LOCATION (IN INCHES)		SPECIAL REQUIREMENTS		
	TYPE	SIGNAL MAST ARM LENGTH	LUMINAIRE MAST ARM LENGTH		Ø	MAST ARM	Ø	POLE	Ø	MOUNTING	POLE QUADRANT	Ø	ARROW		A	B			
1	19-3-100	35'	15'	139	6, 6	MAS. MAS	1	SV-1-T	6	SP-1-T		8	←	ISNS			RS EXISTING Ø6 BALL LED MODULES. [F] Ø" RIGHT ARROW LED MODULES IN EXISTING Ø6 HOUSING AND Ø6 MAS.		
2	1-B						4	SV-1-T	8	SP-1-T		6	→				[F] ITEMS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL).		
3	15TS		15'	73					8	SP-1-T		6	←						
4	1-B						2, 1, 9E	1V-2-1, SV-1-T	6	SP-1-T		8	→				RS EXISTING Ø6 BALL LED MODULES. [F] Ø1 RIGHT ARROW LED MODULES IN EXISTING Ø6 HOUSING AND Ø8B 4" BIKE SIGNAL HEAD AND ITEMS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL). SEE DETAIL E ON SHEET E-1 FOR MOUNTING DETAIL.		
5	19-4-100	30	15'	73	2 5	MAS MAS	2, 4	SV-2-TA	8	SP-1-T		8	←	ISNS		30	[F] ISNS, CANTILEVER OFF SIGNAL POLE PERPENDICULAR TO MAST ARM (TOWARDS SOUTH)		
6	16-1-70	20'	-		4	MAS	2, 4	SV-2-TA	8	SP-1-T		8	←	RSNS			[F] ITEMS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL).		
7	1-B						9E	1V-1-T							204	84	[F] Ø8B 8" BIKE SIGNAL HEAD		

SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS	SHEET NO.
R10-11	NO TURN ON RED	36"x48"	2	E-6
R10-10b	BIKE SIGNAL	18"x24"	1	E-4

NOTE:
THE ABOVE SIGN SCHEDULE EXCLUDES STREET NAME SIGNS AND IDENTIFIES ONLY THE PROPOSED SIGNS MOUNTED ON THE SIGNAL POLE AND MAST ARM.



R10-11

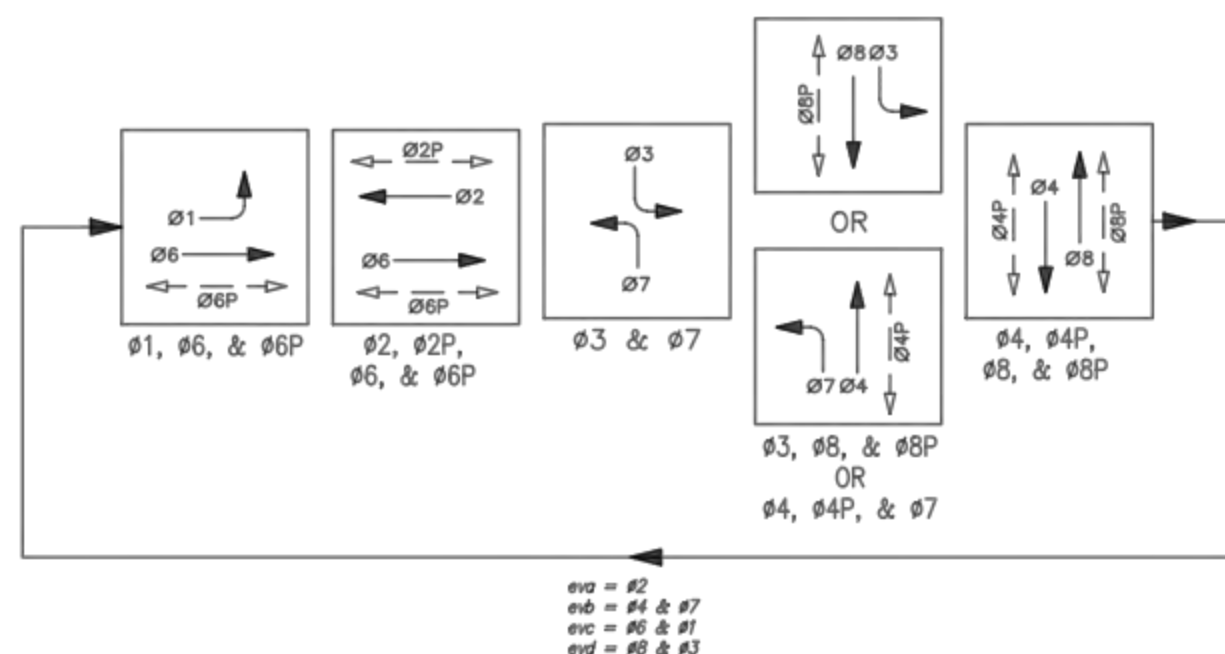


R10-10b

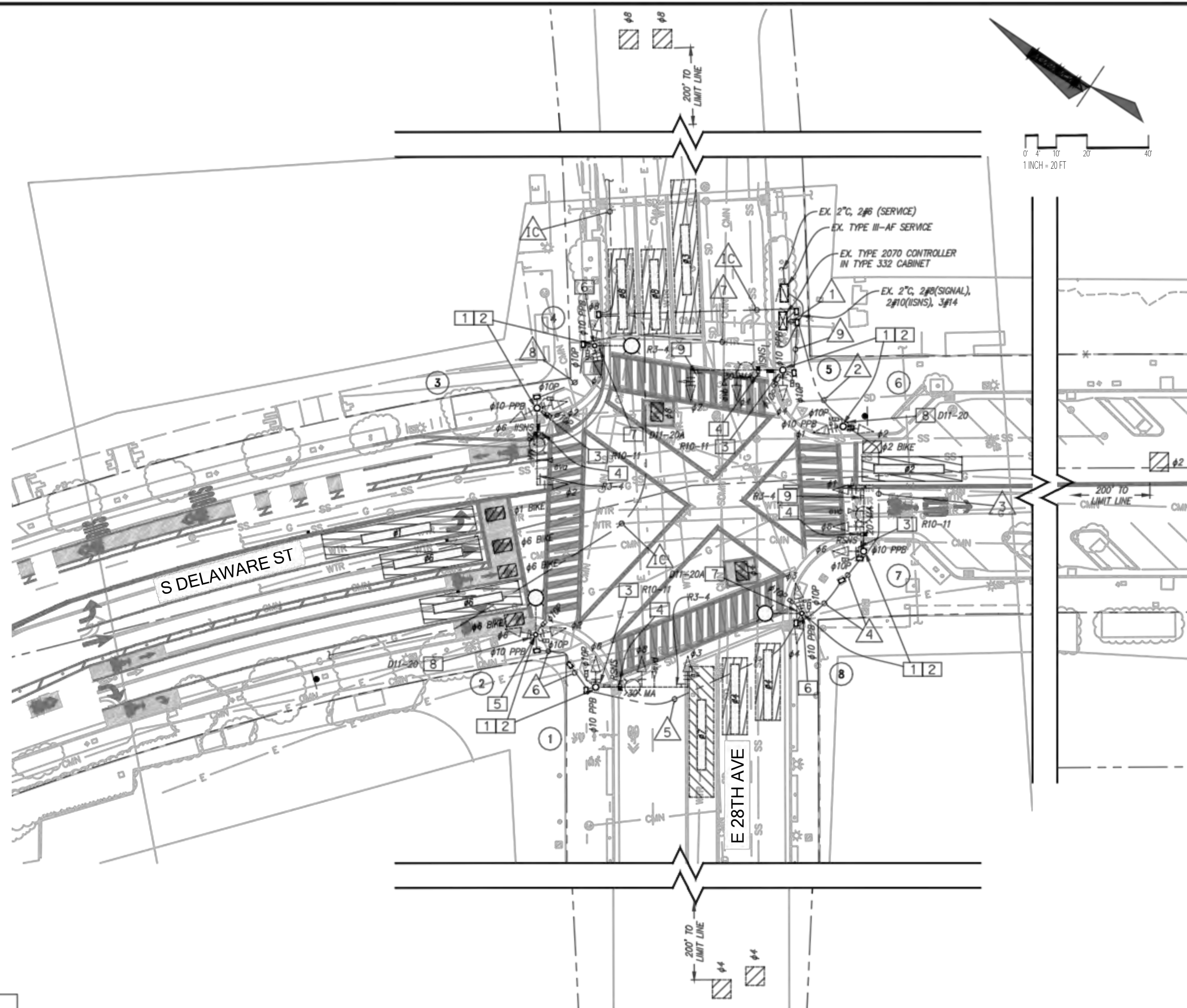
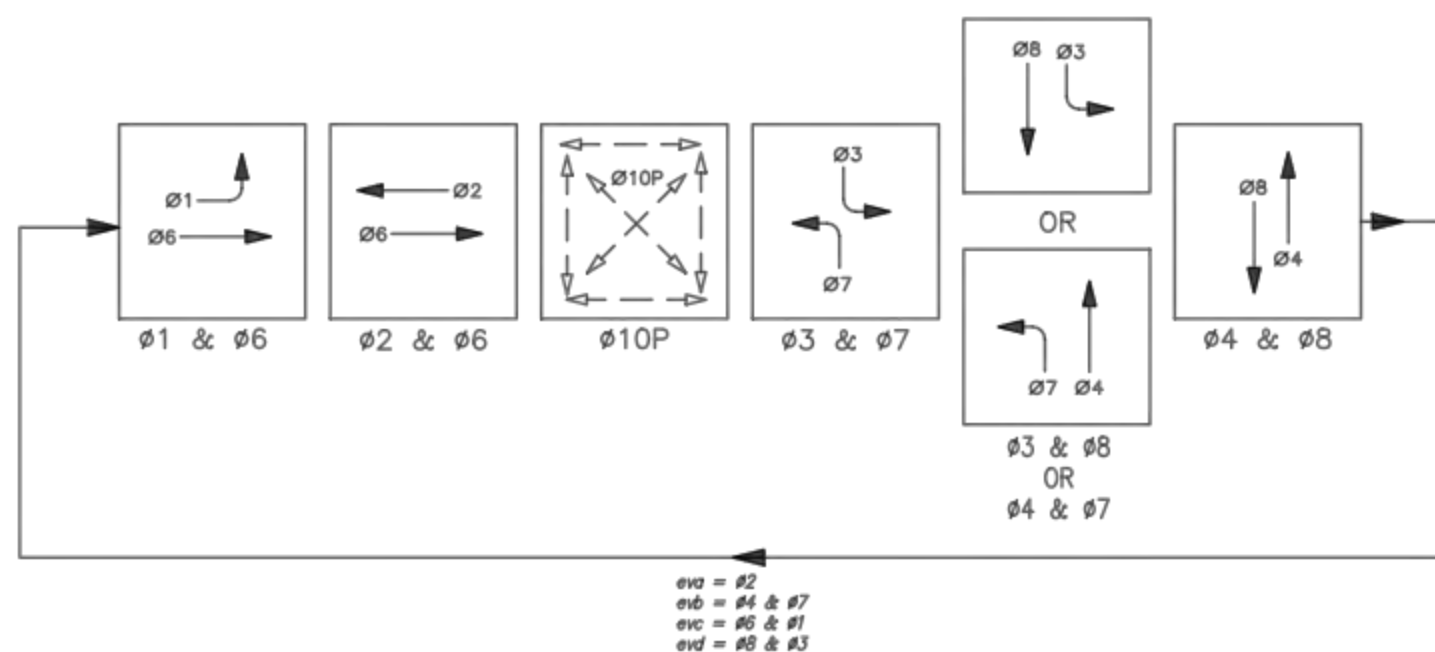


- 1 **RS** PEDESTRIAN PUSH BUTTON EQUIPMENT.
- 2 **R1** APS PEDESTRIAN PUSH BUTTON ON EXISTING POLE.
- 3 **R1** R10-11 "NO TURN ON RED" SIGN ON EXISTING POLE.
- 4 **RS** EXISTING VIDEO DETECTION SYSTEM. **R1** ITERS VANTAGE VECTOR VIDEO DETECTION CAMERA ON LUMINAIRE MAST ARM PER MANUFACTURER'S RECOMMENDATIONS. CONTRACTOR SHALL FURNISH AND INSTALL ALL EQUIPMENT NECESSARY TO PROVIDE A FULLY FUNCTIONAL SYSTEM INCLUDING REPLACING THE EXISTING SYSTEM.
- 5 **R1** ITERS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL) ON POLE TO FACE ADJACENT BICYCLE FACILITY AT INDICATED BIKE DETECTION ZONE FOR THAT FACILITY. SMART CYCLE BIKE INDICATOR SHALL BE MOUNTED BETWEEN 4 FT AND 6 FT FROM FINISH GRADE AND INSTALLED PER MANUFACTURER'S SPECIFICATIONS AND GUIDELINES.
- 6 **R1** ITERS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL) ON POLE TO FACE ADJACENT TWO-STAGE BIKE BOX AT INDICATED BIKE DETECTION ZONE FOR THAT FACILITY. SMART CYCLE BIKE INDICATOR SHALL BE MOUNTED BETWEEN 4 FT AND 6 FT FROM FINISH GRADE AND INSTALLED PER MANUFACTURER'S SPECIFICATIONS AND GUIDELINES.
- 7 **R1** D11-20A "BIKE LEFT TURN BOX" SIGN ON EXISTING POLE.
- 8 **R1** D11-20 "BIKE LEFT TURN MAY USE TURN BOX" SIGN ON EXISTING POLE.
- 9 **RS** EXISTING SIGN ON MAST POLE. **R1** R3-4 "NO U-TURN" SIGN ON EXISTING POLE.

STEADY DEMAND SEQUENCE



STEADY DEMAND SEQUENCE



TRAFFIC SIGNAL PLAN - E 28TH AVE
DELAWARE STREET
SAFE ROUTES TO SCHOOL
 CITY PROJECT NO. 46R022



CONDUCTOR SCHEDULE												
AWG	CONDUCTOR DESIGNATION	NUMBER OF CONDUCTORS										
		RUN NUMBER										
		EX.	EX.	EX.	EX.	EX.	EX.	EX.	EX.	EX.	EX.	EX.
#14	VEHICLE											
	#1	6	6	3								
	#2	9	6	3	3	3	3	3	3			
	#3	6	6	6	6	3						
	#4	9	6	6	6	3			3		3	
	#6	9	6	6	3	3	3	3	3			
	#7	6							3		3	
	#8	6	3	3	3	3		3				
	PED											
	#10	16	10	8	6	4	2	4	2	2		
	PPB											
	#10	16	10	8	6	4	2	4	2	2		
	SPARES	9	3	3	3	3	3	3	3	3		
	SMART CYCLE BIKE INDICATOR											
#14	#1/#6 BIKE	2	2	2	2	2	2					
	#4 BIKE (TWO-STAGE BIKE BOX)	2	2	2	2							
	#8 BIKE (TWO-STAGE BIKE BOX)	2						2				
	TOTAL #14	96	60	50	40	28	15	28	13	13	-	
#10	ISNS							2	2	2		
#8	LUMINAIRE		2	2	2	2	2	2	2	2		
VIDEO DETECTION CABLE (CAT 5E)	#1, #6, #1 BIKE, #6 BIKE	1	1	1								
	#2, #2 BIKE	1						1	1			
	#3, #6	1	1	1	1	1						
	#4, #7	1								1		
	TOTAL VDC	4	2	2	1	1	-	1	1	1		
EVV	OPTICUM CABLE	4	2	2	1	1		1	1	1		
	PULL ROPE	1	1	1	1	1	1	1	1	1	-	
SIC	FIBER										1	
	CONDUIT DESIGNATION	EX.	EX.	EX.	EX.	EX.	EX.	EX.	EX.	EX.	EX.	
	CONDUIT SIZE (INCHES)	2-3"	3"	3"	3"	3"	3"	3"	3"	3"	3"	
	CONDUIT FILL (%)	20%	24%	22%	18%	13%	8%	14%	10%	10%	EX.	

NOTES:

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- NEW CONDUCTORS IN THE ABOVE TABLE IS SHOWN IN BOLD.
- STREETLIGHT CONDUCTORS SHALL BE AWG#10 FOR POLE CONNECTION AND AWG#8 IN UNDERGROUND CONDUITS, WITH 2 CONDUCTORS.
- ISNS TO BE FED FROM SEPARATE BREAKER IN SERVICE ENCLOSURE.

POLE AND SIGNAL SCHEDULE																	
LOCATION	STANDARD			LED LUMINAIRE (MAX WATTAGE)	VEHICLE SIGNAL			PED. SIGNALS			PBA		STREET NAME SIGN	POLE LOCATION (IN INCHES)		SPECIAL REQUIREMENTS	
	TYPE	SIGNAL MAST ARM LENGTH	LUMINAIRE MAST ARM LENGTH		#	MAST ARM	#	POLE	#	MOUNTING	POLE QUADRANT	#	ARROW	POLE QUADRANT	RSNS OR ISNS	A	B
1	19-4-100	30'	8'	EX.	3, 8	MAS MAS	8	SV-1-T	10	SP-1-T		10	←		RSNS		
2	15TS		8'	EX.			2, 6	SV-2-T	10	SP-2-T		10	→				1) #10 PED SIGNAL AND ITERS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL) ON EXISTING POLE.
3	19-4-100	25'	8'	EX.	2	MAS	2, 6	SV-2-T	10	SP-2-T		10	←		ISNS		1) #10 PED SIGNAL ON EXISTING POLE [RS, RSNS, 1) ISNS.
4	15TS		8'	EX.			7, 8	SV-2-T	10	SP-1-T		10	→				1) ITERS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL) ON EXISTING POLE.
5	19-4-100	30'	8'	EX.	4, 7	MAS MAS	4	SV-1-T	10	SP-2-T		10	←		ISNS		1) #10 PED SIGNAL ON EXISTING POLE [RS, RSNS, 1) ISNS.
6	1-B						1, 2	TV-2-T	10	SP-1-T		10	→				
7	19-4-100	20'	8'	EX.	1, 6	MAS MAS	6	SV-1-T	10	SP-1-T		10	←		RSNS		
8	15TS		8'	EX.			3, 4	SV-2-T	10	SP-2-T		10	→				1) #10 PED SIGNAL AND ITERS SMARTCYCLE BIKE INDICATOR (OR APPROVED EQUAL) ON EXISTING POLE.

PROPOSED SIGN SCHEDULE				
SIGN CODE	SIGN MESSAGE	PANEL SIZE	NO. OF SIGNS	SHEET NO.
R10-11	NO TURN ON RED	36"x48"	4	E-8
D11-20	LEFT TURN MAY USE TURN BOX	12"x18"	2	E-8
D11-20A	LEFT TURN BOX	12"x18"	2	E-8
R3-4	NO U-TURN	36"x36"	2	E-8

NOTE:
THE ABOVE SIGN SCHEDULE EXCLUDES STREET NAME SIGNS AND IDENTIFIES ONLY THE PROPOSED SIGNS MOUNTED ON THE SIGNAL POLE AND MAST ARM.



R10-11



D11-20

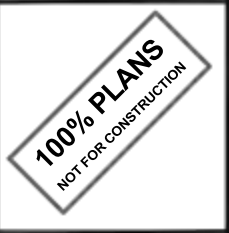


D11-20A



R3-4

REV NO.	DESCRIPTION	BY	DATE
1			
2			
3			
4			
5			



TRAFFIC SCHEDULE - E 28TH AVE
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022



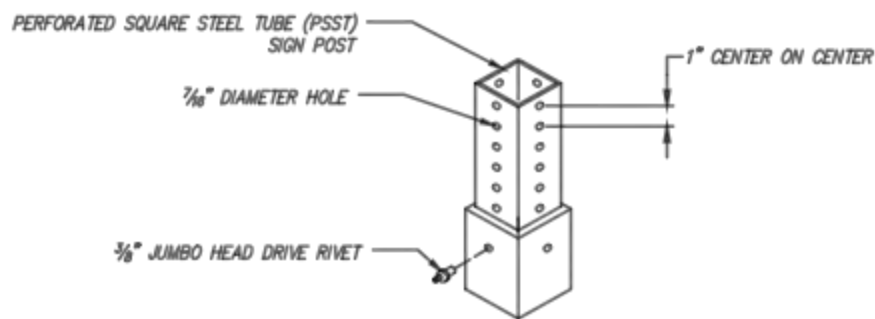
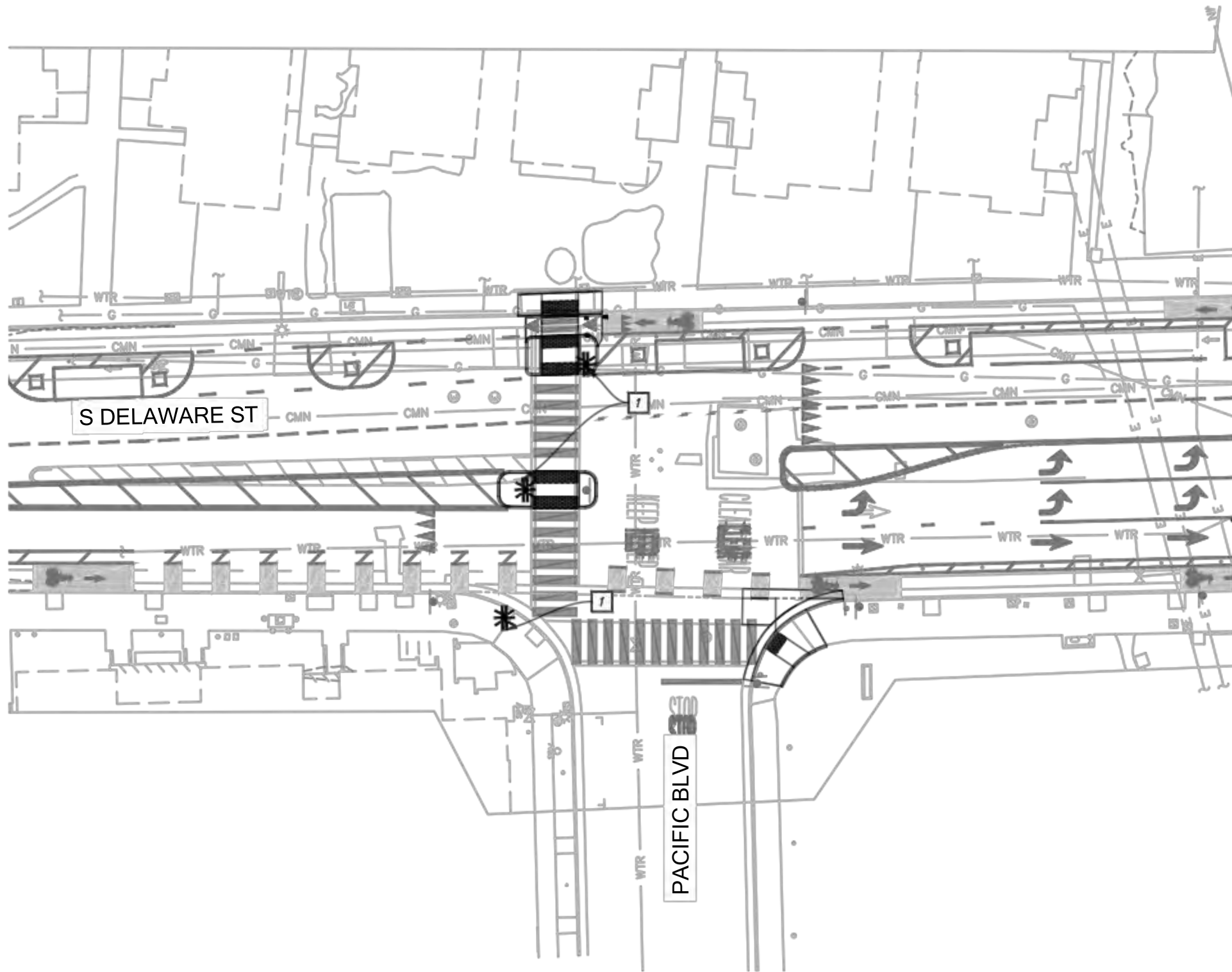
DESIGNED BY: JP	DATE
DRAWN BY: BP TS	DATE
QC CHECKED BY: RES	DATE
PROJECT NO. 623071	
SCALE	
SUBMITTAL 60%	

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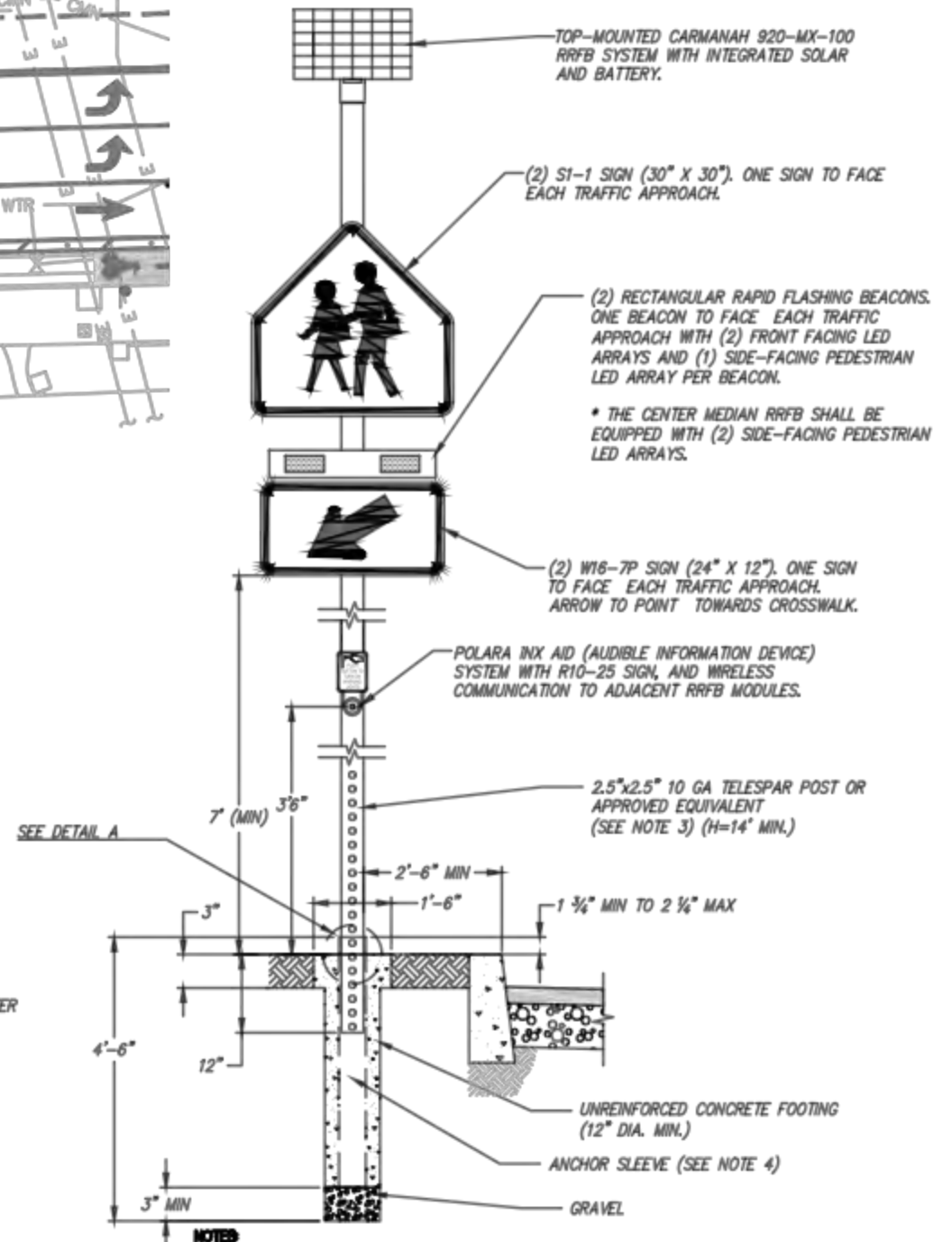
1. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH NEC 2023, CITY OF SAN MATEO, AND CALTRANS STANDARDS AND APPLICABLE PERMITS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IDENTIFYING EXISTING STREET LIGHTING CIRCUITS PRIOR TO CONSTRUCTION.

SHEET NOTES

- 1 FURNISH, CONFIGURE, AND INSTALL NEW SOLAR-POWERED DOUBLE-SIDED RRFB SYSTEM WITH AUDIBLE INFORMATION DEVICE (AID) AND INDICATED SIGNAGE ON A NEW POLE AND FOUNDATION PER DETAIL 1, THIS SHEET.

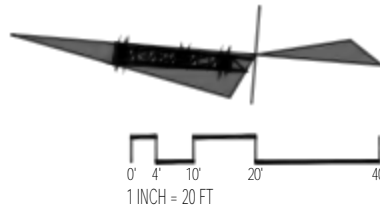


A TELES PAR POLE CONNECTION



- NOTES:
1. RRFB EQUIPMENT SHALL BE DOUBLE-SIDED CARMANAH R920-MX-100 RRFB (RECTANGULAR RAPID FLASHING BEACONS) SYSTEM WITH POLARA INX AID (AUDIBLE INFORMATION DEVICE) ACTUATION BUTTON, POWERED WITH 85W SINGLE PANEL CONFIGURATION SOLAR PANEL, AND 105 AH BATTERY CAPACITY.
 2. CONTRACTOR SHALL INSTALL EQUIPMENT PER THE MANUFACTURER SPECIFICATIONS AND REQUIREMENTS.
 3. THE SIGN POST SHALL HAVE 7/16" DIAMETER PERFORATIONS 1" ON CENTER ON ALL FOUR SIDES FOR THE FULL LENGTH.
 4. ALL ANCHOR SLEEVES SHALL BE EMBEDDED IN PCC.


1 RECTANGULAR RAPID FLASHING BEACON DETAIL



LEGEND


- * DOUBLE-SIDED RECTANGULAR RAPID FLASHING BEACON (RRFB) ASSEMBLY.

REV. NO.	DESCRIPTION	BY	DATE
1			
2			
3			
4			
5			

**SANDS**
SANDS.NET

100% PLANS
NOT FOR CONSTRUCTION

RRFB PLAN - PACIFIC BLVD
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022

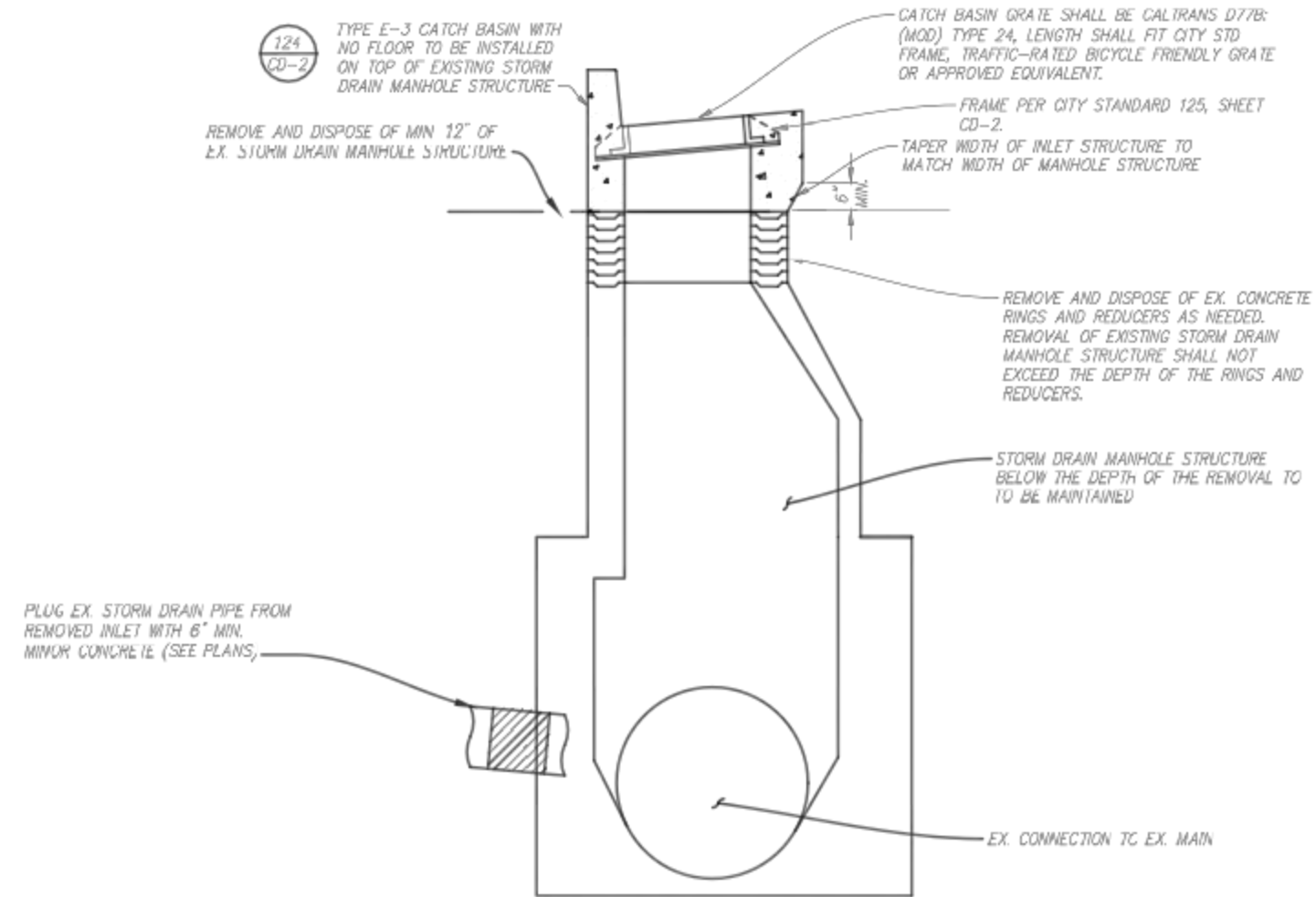


DESIGNED BY: JP	DATE
DRAWN BY: BP TS <td>DATE</td>	DATE
QC CHECKED BY: RES <td>DATE</td>	DATE
PROJECT NO. 623071 <td>DATE</td>	DATE
SCALE <td>DATE</td>	DATE
SUBMITTAL 60% <td>DATE</td>	DATE

E-10

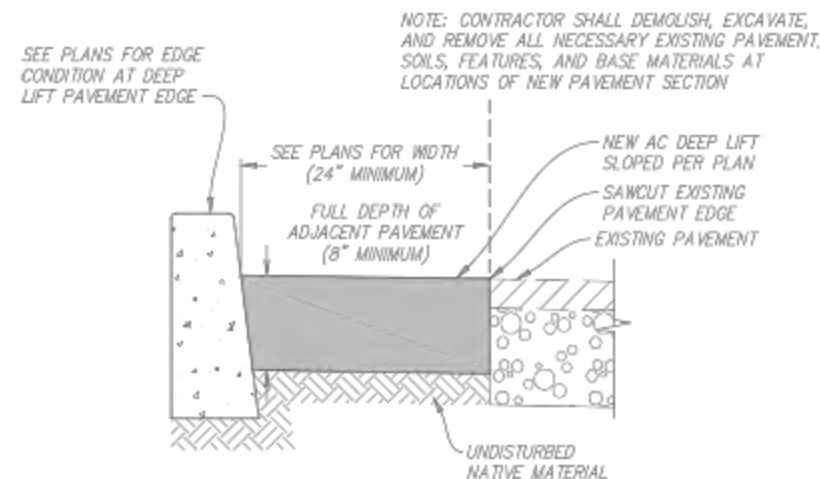
SHEET 33 OF 36

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CONVERT SD MANHOLE TO SD INLET

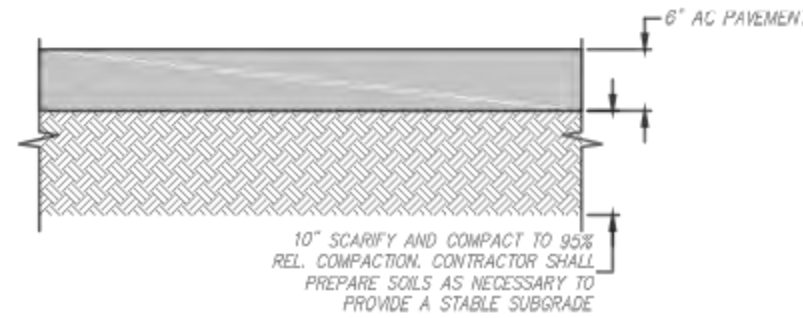
6



ASPHALT DEEP LIFT

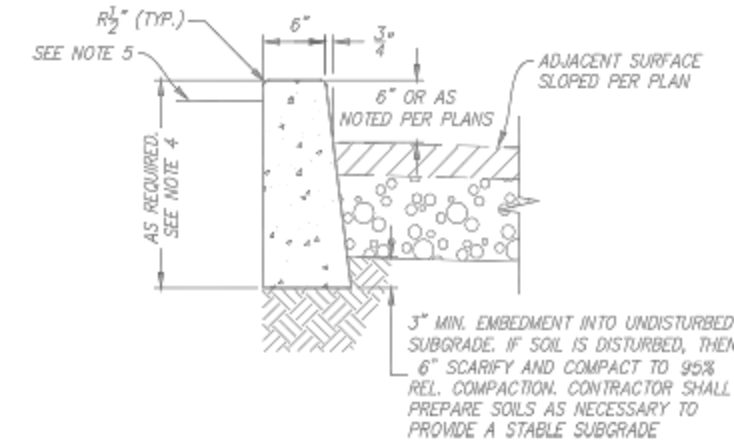
3

NOTE: CONTRACTOR SHALL DEMOLISH, EXCAVATE, AND REMOVE ALL NECESSARY EXISTING PAVEMENT, SOILS, FEATURES, AND BASE MATERIALS AT LOCATIONS OF NEW PAVEMENT SECTION



AC BASE REPAIR

4



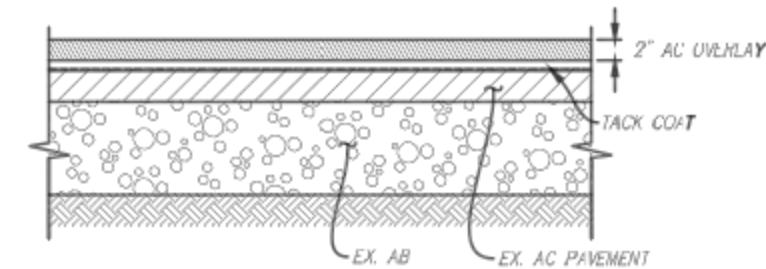
6" VERTICAL CURB

1

NOTES:

- ALL EXPOSED CONCRETE EDGES (HORIZONTAL AND VERTICAL) SHALL BE 1/2" RADIUS.
- #4x12" SLIP DOWELS AND 1/4" EXPANSION JOINTS TO BE PROVIDED AT 20' ON CENTER ALONG THE LENGTH OF CURB. MATERIAL FOR EXPANSION JOINTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM DESIGNATION D 1751.
- REFER TO PLANS FOR ADJACENT PAVEMENT SECTIONS.
- TOTAL CURB HEIGHT IS BASED ON ADJACENT PAVEMENT SECTION, PLUS CURB HEIGHT PER PLAN, PLUS 3" BELOW FULL PAVEMENT SECTION (14" MINIMUM).
- SEE PLANS FOR ADJACENT SURFACE MATERIAL AND SLOPE. GRADE SHALL BE FLUSH WITH TOP OF CURB WHEN SURFACE IS PAVED. WHEN ADJACENT TO LANDSCAPING, REFER TO LANDSCAPE PLANS FOR GRADE AT BACK OF CURB.
- CONTRACTOR SHALL DEMOLISH, EXCAVATE, AND REMOVE ALL NECESSARY EXISTING PAVEMENT, SOILS, FEATURES, AND BASE MATERIALS AT LOCATIONS OF NEW PAVEMENT SECTION AND CURB INSTALLATION.

NOTE: CONTRACTOR SHALL DEMOLISH, EXCAVATE, AND REMOVE ALL NECESSARY EXISTING PAVEMENT, SOILS, FEATURES, AND BASE MATERIALS AT LOCATIONS OF NEW PAVEMENT SECTION



ASPHALT OVERLAY

2

CONSTRUCTION DETAILS
DELAWARE STREET
SAFE ROUTES TO SCHOOL

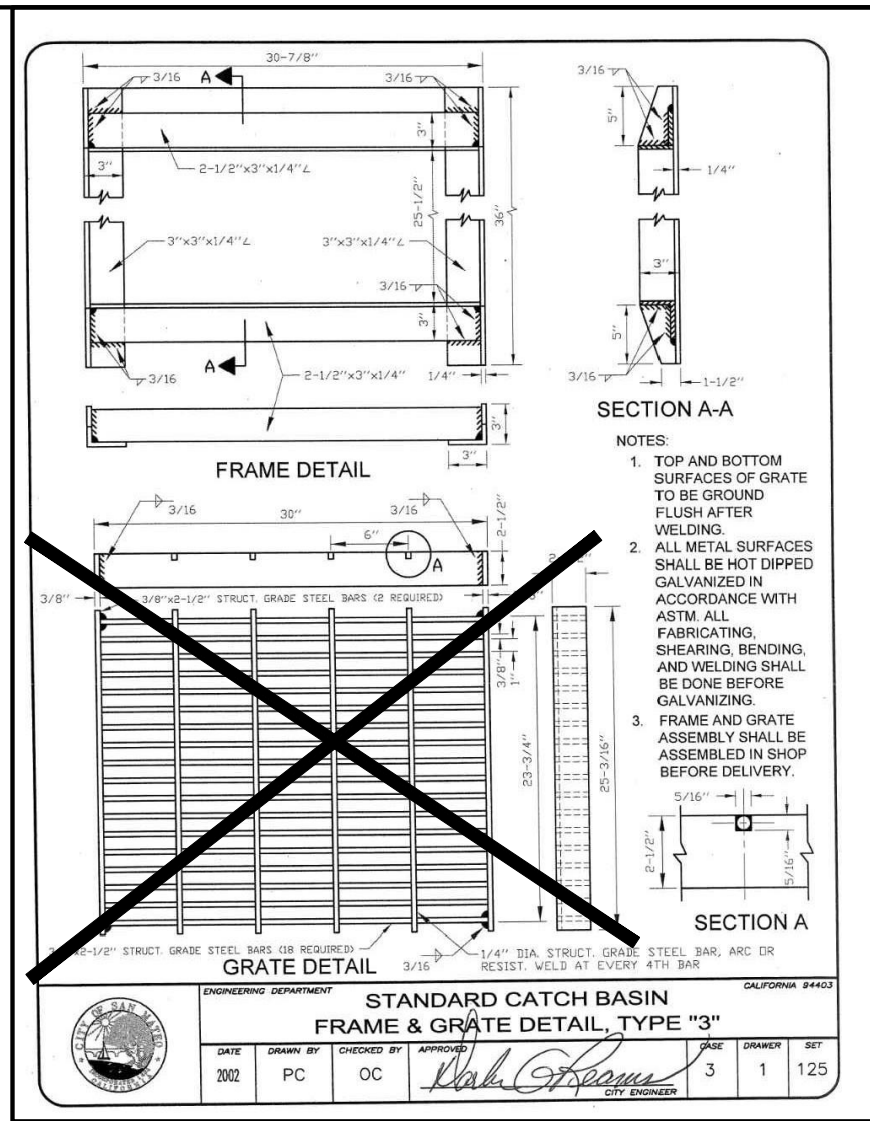
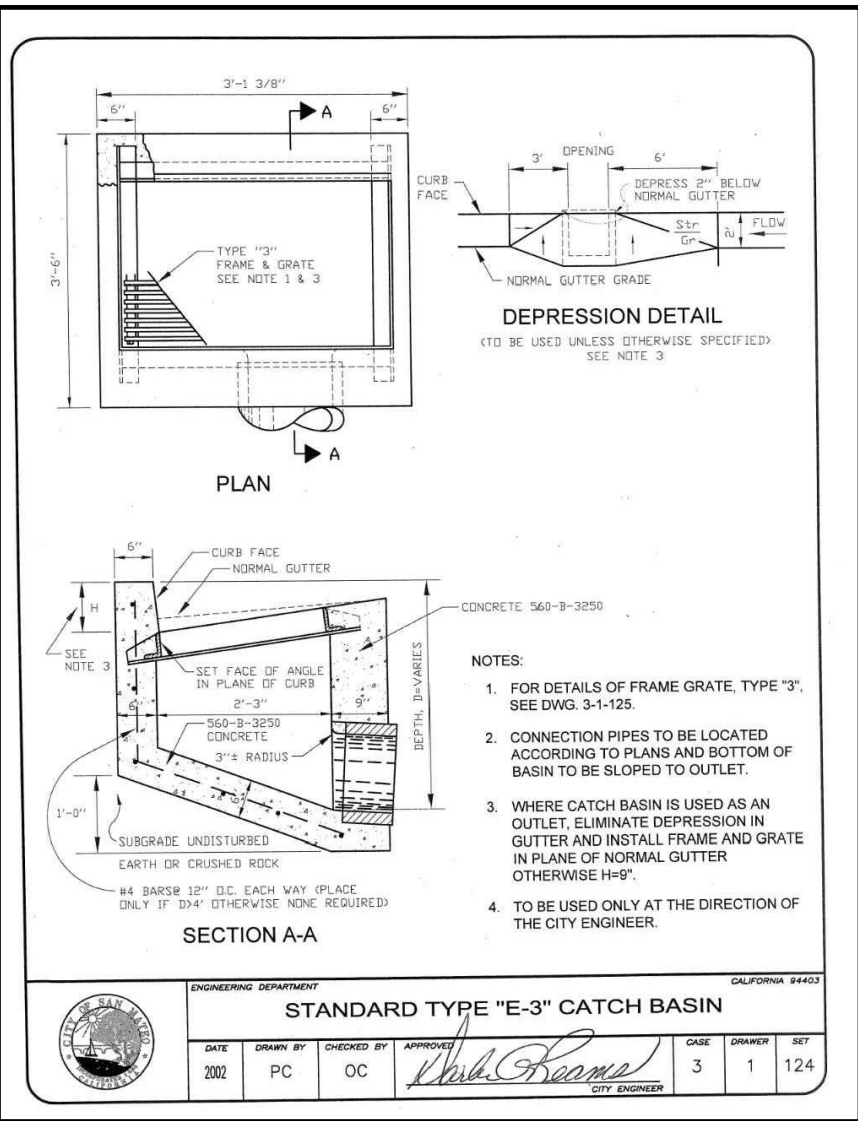
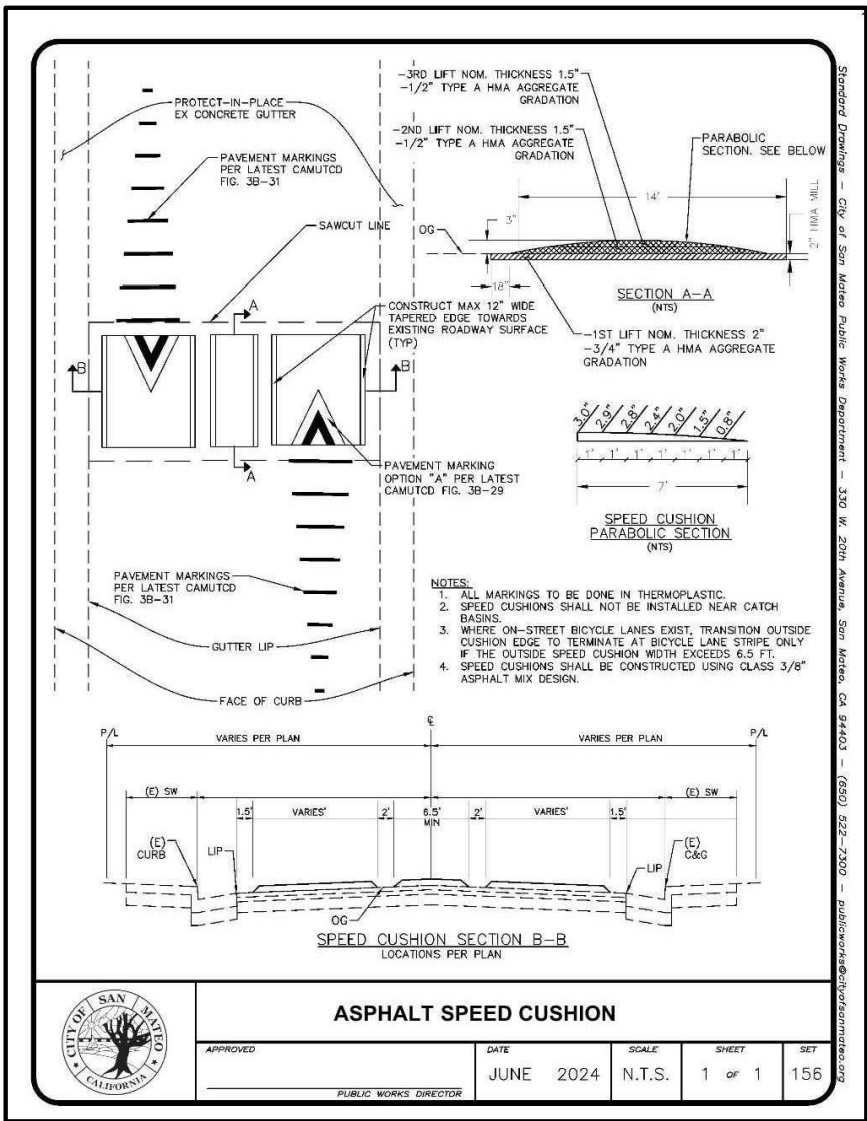
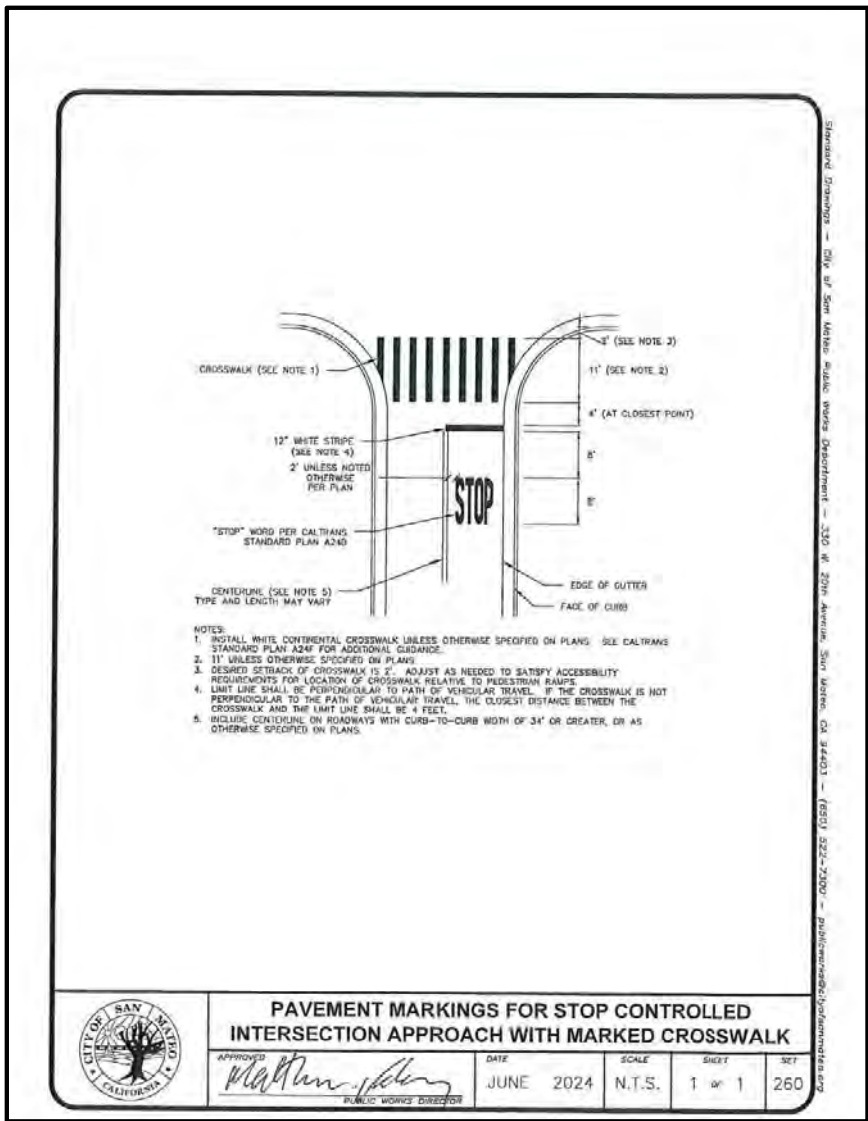
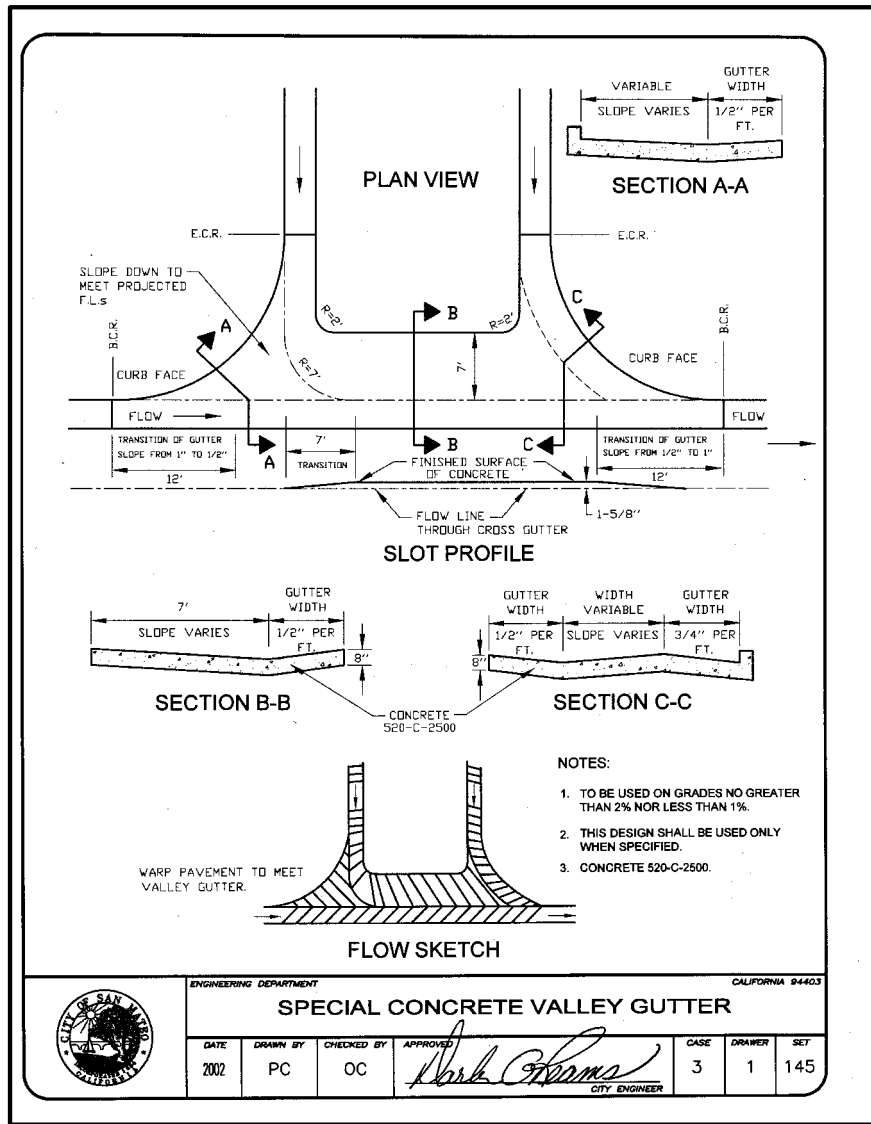
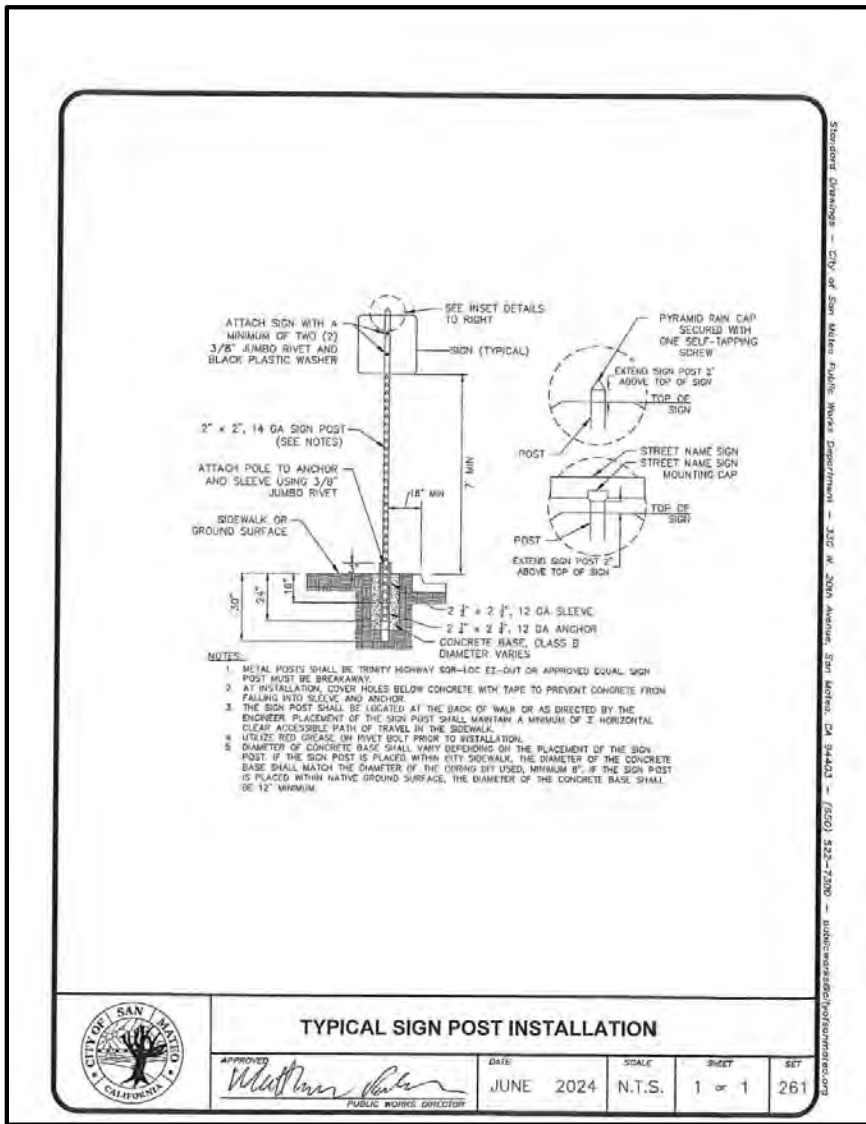
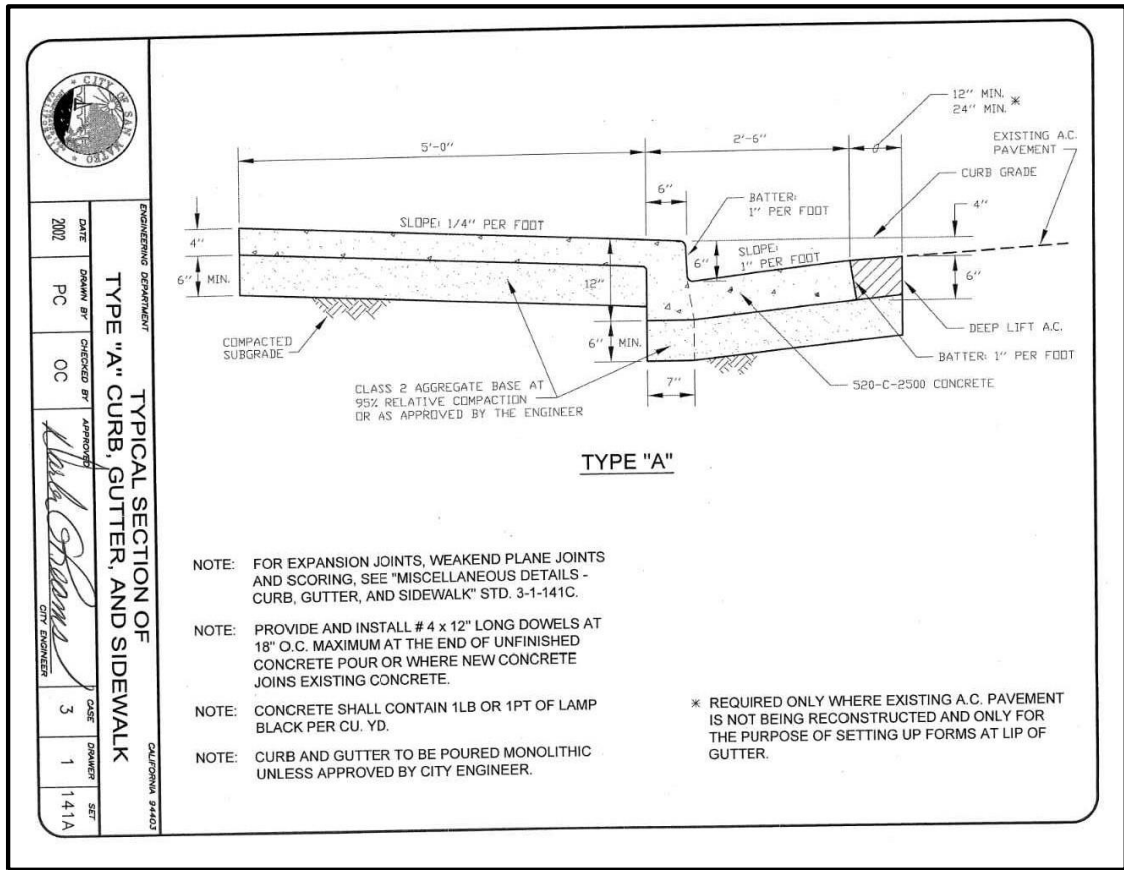
CITY PROJECT NO. 46R022



DESIGNED BY: JP	DATE
DRAWN BY: BP TS	DATE
QC CHECKED BY: RES	DATE
PROJECT NO. 623071	
SCALE	
SUBMITTAL 60%	

SHEET 34 OF 36

CD-1

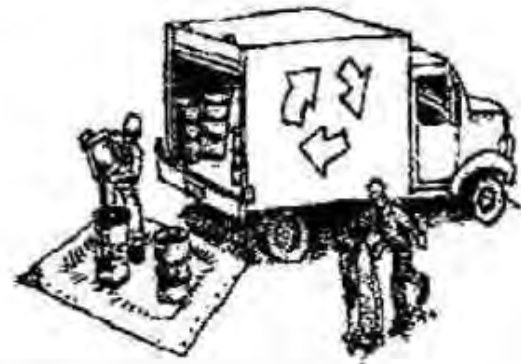




Construction Best Management Practices (BMPs)

Construction projects are required to implement the stormwater best management practices (BMP) on this page, as they apply to your project, all year long.

Materials & Waste Management



Non-Hazardous Materials

- ❑ Berm and cover stockpiles of sand, dirt or other construction material with tarps when rain is forecast or if not actively being used within 14 days.
- ❑ Use (but don't overuse) reclaimed water for dust control.

Hazardous Materials

- ❑ Label all hazardous materials and hazardous wastes (such as pesticides, paints, thinners, solvents, fuel, oil, and antifreeze) in accordance with city, county, state and federal regulations.
- ❑ Store hazardous materials and wastes in water tight containers, store in appropriate secondary containment, and cover them at the end of every work day or during wet weather or when rain is forecast.
- ❑ Follow manufacturer's application instructions for hazardous materials and be careful not to use more than necessary. Do not apply chemicals outdoors when rain is forecast within 24 hours.
- ❑ Arrange for appropriate disposal of all hazardous wastes.

Waste Management

- ❑ Cover waste disposal containers securely with tarps at the end of every work day and during wet weather.
- ❑ Check waste disposal containers frequently for leaks and to make sure they are not overfilled. Never hose down a dumpster on the construction site.
- ❑ Clean or replace portable toilets, and inspect them frequently for leaks and spills.
- ❑ Dispose of all wastes and debris properly. Recycle materials and wastes that can be recycled (such as asphalt, concrete, aggregate base materials, wood, gyp board, pipe, etc.)
- ❑ Dispose of liquid residues from paints, thinners, solvents, glues, and cleaning fluids as hazardous waste.

Construction Entrances and Perimeter

- ❑ Establish and maintain effective perimeter controls and stabilize all construction entrances and exits to sufficiently control erosion and sediment discharges from site and tracking off site.
- ❑ Sweep or vacuum any street tracking immediately and secure sediment source to prevent further tracking. Never hose down streets to clean up tracking.

Equipment Management & Spill Control



Maintenance and Parking

- ❑ Designate an area, fitted with appropriate BMPs, for vehicle and equipment parking and storage.
- ❑ Perform major maintenance, repair jobs, and vehicle and equipment washing off site.
- ❑ If refueling or vehicle maintenance must be done onsite, work in a bermed area away from storm drains and over a drip pan or drop cloths big enough to collect fluids. Recycle or dispose of fluids as hazardous waste.
- ❑ If vehicle or equipment cleaning must be done onsite, clean with water only in a bermed area that will not allow rinse water to run into gutters, streets, storm drains, or surface waters.
- ❑ Do not clean vehicle or equipment onsite using soaps, solvents, degreasers, or steam cleaning equipment.

Spill Prevention and Control

- ❑ Keep spill cleanup materials (e.g., rags, absorbents and cat litter) available at the construction site at all times.
- ❑ Inspect vehicles and equipment frequently for and repair leaks promptly. Use drip pans to catch leaks until repairs are made.
- ❑ Clean up spills or leaks immediately and dispose of cleanup materials properly.
- ❑ Do not hose down surfaces where fluids have spilled. Use dry cleanup methods (absorbent materials, cat litter, and/or rags).
- ❑ Sweep up spilled dry materials immediately. Do not try to wash them away with water, or bury them.
- ❑ Clean up spills on dirt areas by digging up and properly disposing of contaminated soil.
- ❑ Report significant spills immediately. You are required by law to report all significant releases of hazardous materials, including oil. To report a spill: 1) Dial 911 or your local emergency response number, 2) Call the Governor's Office of Emergency Services Warning Center, (800) 852-7550 (24 hours).

Earthmoving



- ❑ Schedule grading and excavation work during dry weather.
- ❑ Stabilize all denuded areas, install and maintain temporary erosion controls (such as erosion control fabric or bonded fiber matrix) until vegetation is established.
- ❑ Remove existing vegetation only when absolutely necessary, and seed or plant vegetation for erosion control on slopes or where construction is not immediately planned.
- ❑ Prevent sediment from migrating offsite and protect storm drain inlets, gutters, ditches, and drainage courses by installing and maintaining appropriate BMPs, such as fiber rolls, silt fences, sediment basins, gravel bags, berms, etc.
- ❑ Keep excavated soil on site and transfer it to dump trucks on site, not in the streets.

Contaminated Soils

- ❑ If any of the following conditions are observed, test for contamination and contact the Regional Water Quality Control Board:
 - Unusual soil conditions, discoloration, or odor.
 - Abandoned underground tanks.
 - Abandoned wells.
 - Buried barrels, debris, or trash.

Paving/Asphalt Work



- ❑ Avoid paving and seal coating in wet weather or when rain is forecast, to prevent materials that have not cured from contacting stormwater runoff.
- ❑ Cover storm drain inlets and manholes when applying seal coat, tack coat, slurry seal, fog seal, etc.
- ❑ Collect and recycle or appropriately dispose of excess abrasive gravel or sand. Do NOT sweep or wash it into gutters.
- ❑ Do not use water to wash down fresh asphalt concrete pavement.

Sawcutting & Asphalt/Concrete Removal

- ❑ Protect nearby storm drain inlets when saw cutting. Use filter fabric, catch basin inlet filters, or gravel bags to keep slurry out of the storm drain system.
- ❑ Shovel, absorb, or vacuum saw-cut slurry and dispose of all waste as soon as you are finished in one location or at the end of each work day (whichever is sooner!).
- ❑ If sawcut slurry enters a catch basin, clean it up immediately.

Concrete, Grout & Mortar Application



- ❑ Store concrete, grout, and mortar away from storm drains or waterways, and on pallets under cover to protect them from rain, runoff, and wind.
- ❑ Wash out concrete equipment/trucks offsite or in a designated washout area, where the water will flow into a temporary waste pit, and in a manner that will prevent leaching into the underlying soil or onto surrounding areas. Let concrete harden and dispose of as garbage.
- ❑ When washing exposed aggregate, prevent washwater from entering storm drains. Block any inlets and vacuum gutters, hose washwater onto dirt areas, or drain onto a bermed surface to be pumped and disposed of properly.

Landscaping



- ❑ Protect stockpiled landscaping materials from wind and rain by storing them under tarps all year-round.
- ❑ Stack bagged material on pallets and under cover.
- ❑ Discontinue application of any erodible landscape material within 2 days before a forecast rain event or during wet weather.

Painting & Paint Removal



Painting Cleanup and Removal

- ❑ Never clean brushes or rinse paint containers into a street, gutter, storm drain, or stream.
- ❑ For water-based paints, paint out brushes to the extent possible, and rinse into a drain that goes to the sanitary sewer. Never pour paint down a storm drain.
- ❑ For oil-based paints, paint out brushes to the extent possible and clean with thinner or solvent in a proper container. Filter and reuse thinners and solvents. Dispose of excess liquids as hazardous waste.
- ❑ Paint chips and dust from non-hazardous dry stripping and sand blasting may be swept up or collected in plastic drop cloths and disposed of as trash.
- ❑ Chemical paint stripping residue and chips and dust from marine paints or paints containing lead, mercury, or tributyltin must be disposed of as hazardous waste. Lead based paint removal requires a state-certified contractor.

Dewatering



- ❑ Discharges of groundwater or captured runoff from dewatering operations must be properly managed and disposed. When possible send dewatering discharge to landscaped area or sanitary sewer. If discharging to the sanitary sewer call your local wastewater treatment plant.
- ❑ Divert run-on water from offsite away from all disturbed areas.
- ❑ When dewatering, notify and obtain approval from the local municipality before discharging water to a street gutter or storm drain. Filtration or diversion through a basin, tank, or sediment trap may be required.
- ❑ In areas of known or suspected contamination, call your local agency to determine whether the ground water must be tested. Pumped groundwater may need to be collected and hauled off-site for treatment and proper disposal.

Storm drain polluters may be liable for fines of up to \$10,000 per day!



100% PLANS
NOT FOR CONSTRUCTION

BEST MANAGEMENT PRACTICES
DELAWARE STREET
SAFE ROUTES TO SCHOOL
CITY PROJECT NO. 46R022



Detailed Engineer's Estimate and Total Project Costs- Cycle 5 v1.2

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency: City of San Mateo

Date: 9/1/2020

Project Description: Construct a Class IV and III bikeways to enable safe access to two elementary schools.

Project Location: South Delaware Street from 19th Avenue to Pacific Boulevard

Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate: Jay Yu, Engineering Manager

License #: 85383

Engineer's Estimate and Cost Breakdown:

Engineer's Estimate (for Construction Items Only)

Cost Breakdown

Engineer's Estimate (for Construction Items Only)						ATP Eligible Costs/Items		ATP Ineligible Costs/Items		Corps/CCC to construct	
Item No.	Item	Quantity	Units	Unit Cost	Total Item Cost	%	\$	%	\$	%	\$
General <u>Overhead-Related</u> Construction Items											
	Mobilization	1	LS	\$ 52,418.00							
	Traffic Control	1	LS	\$ 81,000.00							
	Stormwater Protection Plan	1	LS	\$ 10,000.00							
	Construction Area Signs	1	LS	\$ 10,000.00							
			LS								
			LS								
			LS								
			LS								
			LS								
			LS								
General Construction Items											
11	Signs	1	LS	\$ 10,000.00	\$10,000	100%	\$10,000				
12	Striping	1	LS	\$ 434,800.00	\$434,800	100%	\$434,800				
13	AC Grind (6")	20000	SF	\$ 3.00	\$60,000	100%	\$60,000				
14	Curb and Gutter	100	LF	\$ 20.00	\$2,000	100%	\$2,000				
15	Median 12"	1200	SF	\$ 50.00	\$60,000	100%	\$60,000				
16	High Performance Delineator Buffer	800	EA	\$ 90.00	\$72,000	100%	\$72,000				
17	Speed Hump	10	EA	\$ 3,500.00	\$35,000	100%	\$35,000				
18	Traffic Control System	1	LS	\$ 300,000.00	\$300,000	100%	\$300,000				
19								100%			
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52								100%			
Subtotal of Construction Items:					\$1,127,218	\$1,127,218					
	Construction Item Contingencies (% of Construction Items):			10.00%	\$112,722	\$112,722					
Total (Construction Items & Contingencies) cost:					\$1,239,940	\$1,239,940					

Project Delivery Costs:

Type of Project Cost	Cost \$	ATP Eligible Costs	Non-participating Costs
Preliminary Engineering (PE)			

Detailed Engineer's Estimate and Total Project Costs- Cycle 5 v1.2

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency: City of San Mateo		Date: 9/1/2020	
Project Description:		Construct a Class IV and III bikeways to enable safe access to two elementary schools.	
Project Location:		South Delaware Street from 19th Avenue to Pacific Boulevard	
Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate:		Jay Yu, Engineering Manager	
License #:		85383	
Environmental Studies and Permits(PA&ED):	\$ 37,198	\$37,198	
Plans, Specifications and Estimates (PS&E):	\$ 213,189	\$213,189	
Total PE:	\$ 250,387	\$250,387	
Right of Way (RW)			
Right of Way Engineering:	\$ 10,000	\$10,000	
Acquisitions and Utilities:			
Total RW:	\$ 10,000	\$10,000	
Construction Engineering (CE)			
Construction Engineering (CE):	\$ 161,192	\$161,192	
(PE+RW+CE) Total Project Delivery:		\$421,579	\$421,579
Total Construction Costs:		\$1,239,940	\$1,401,132
Total Project Cost:		\$1,661,519	\$1,661,519

Documentation of Ineligible (Non-Participating) Costs:

The Engineer's logic and/or calculations for splitting costs between ATP-Eligible and Non-participating costs must be documented in this section of the Estimate form.

Separate logic is required for each item which is partly ineligible for ATP funding or is required for the construction of an ineligible item/element of the project.

Item #:	Description of Engineer's Logic: (See examples shown in the Instructions)



METROPOLITAN
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375 Beale Street, Suite 800
San Francisco, CA 94105
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www.mtc.ca.gov

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Association of Bay Area
Governments

Libby Schaaf
U.S. Department of Housing
and Urban Development

Andrew B. Premier
Executive Director

Alix Bockelman
Chief Deputy Executive Director

March 26, 2025

Matt Fabry, Director of Public Works
City of San Mateo
330 W 20th Avenue
San Mateo, CA 94403

Re: Delaware Street Safe Routes to School Corridor Project – Minor Scope Revision

Dear Director Fabry,

The Metropolitan Transportation Commission has received your minor scope revision request for the Delaware Street Safe Routes to School Corridor project in San Mateo County (PPNO: 2353), competitively selected in MTC's Regional Cycle 5 Active Transportation Program (ATP).

In March 2024, the California Transportation Commission (CTC) approved policies for any amendments to ATP projects. The adopted policies include required documentation of MTC's approval and rationale for approval of proposed scope changes.

Based on our evaluation of the documentation you provided, MTC staff understands that in order to maximize project delivery efficiencies with another concurrent active transportation-focused project, San Mateo plans to revise the project scope to deliver project elements originally planned for the Delaware Street Safe Routes to School Corridor project at the intersection of Delaware and 19th Avenue as a part of the US101/SR92 Fashion Island Blvd Mobility Hub project. The proposed scope change ensures that the original project benefits are preserved while allowing the concurrent project to construct safety improvements without interfering with the Delaware Street project.

The proposed scope change is estimated to result in a \$1,500,000 increase in cost. However, since the City committed to funding any cost increases with local funds and the modifications will maintain the benefits identified in the original application, MTC approves the scope revision request. MTC cautions San Mateo not to proceed with the project before securing necessary approvals from MTC, Caltrans, and the CTC, as such action may jeopardize ATP funding.

If you have any questions regarding MTC's position, please contact Karl Anderson, MTC's ATP Program Manager, at (415) 778-6645, or kanderson@bayareametro.gov.

TIP ID

SM-210006

PPNO

2353

Sincerely,



Theresa Romell
Director, Funding Policy and Programs

TR:KA

cc: Xi Zhang, Caltrans District 4 Division of Local Assistance
Teresa McWilliam, Caltrans HQ Division of Local Assistance
Anja Aulenbacher, California Transportation Commission

J:\PROJECT\Funding\ATP\Regional ATP\2021 rATP (Cycle 5)\Actions\San Mateo - Delaware St\Scope Change\Ltr-SanMateo_DelawareConcur_3-25-2025.docx

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 4.20, Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ACTIVE TRANSPORTATION PROGRAM – PROJECT SCOPE
AMENDMENT – FORT ORD REGIONAL TRAIL AND GREENWAY: CALIFORNIA
AVENUE SEGMENT PROJECT
RESOLUTION ATP-P-2425-13**

ISSUE:

Should the California Transportation Commission (Commission) approve a project scope amendment for the Cycle 6 Active Transportation Program (ATP) Transportation Agency for Monterey County (TAMC) – Fort Ord Regional Trail and Greenway: California Avenue Segment project (PPNO 3130A), in Monterey County?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve this project scope amendment for the Cycle 6 ATP Fort Ord Regional Trail and Greenway: California Avenue Segment project (PPNO 3130A), in Monterey County.

DISCUSSION:

In December 2022, the Commission approved the Small Urban and Rural component of the 2023 ATP, which included \$8,429,000 in funding for the Fort Ord Regional Trail and Greenway: California Avenue Segment project (PPNO 3130A), in Monterey County. This 1.8 mile segment of the Fort Ord Regional Trail and Greenway project (FORTAG) aims to close a gap in the regional multi-use path network and will connect disadvantaged communities in the City of Marina to elementary schools and the California State University Monterey Bay, planned affordable housing developments, future SURF! Busway and Bus Rapid Transit, health services, employment centers, and Fort Ord National Monument trails and open space. The FORTAG California Avenue segment will connect to Segment 2 of FORTAG, a 2.6 mile segment from Imjin Road to the Jerry Smith Corridor through Fort Ord open space.

During the design phase, geotechnical studies, topographic mapping, and environmental surveys were conducted, as further explained, which exposed challenges to the bridge that were not known at the time. In addition, at the time of the ATP application, there were no plans for improvements on Imjin Road. The City of Marina has since moved forward with roadway improvements immediately adjacent to the project location, a roundabout, sidewalk, bike lanes, shared use paths, and transit stops at Imjin Road and Imjin Parkway. The proposed midblock crosswalk would be adjacent to the new transit stop and would connect the FORTAG trail to this new pedestrian infrastructure, improving regional connectivity beyond what the bridge would have done.

Additionally, at the May 2025 Commission meeting, TAMC received approval for a 20-month time extension for the allocation of the Construction (CON) phase, extending the deadline to February 2027.

ANALYSIS:

The Department's analysis has deemed the TAMC request to be a significant scope change due to the replacement of a separated crossing with an at-grade crossing.

The revised scope includes the replacement/relocation of the originally proposed pedestrian overcrossing with an at-grade mid-block crossing with a High-intensity Activated crossWalk system and crossing improvement measures at two intersections. The relocation of the roadway crossing feature will facilitate access to a newly constructed roundabout with transit stops and the crossing improvements will enhance the safety of active transportation users. According to the revised cost estimates, the current construction costs are estimated at \$7,380,000, while the ATP programmed CON phase is \$6,920,000. The remaining funds, including the additional project costs, will be covered by TAMC.

ANALYSIS RECOMMENDATION:

Based on the Commission's scope change guidance, as well as the analysis of the proposed scope changes, the Department has determined that this scope change has the potential to increase overall active transportation users of the project as compared to the original scope; but the shift from a separated facility to an at-grade crossing will decrease the safety. Therefore, the Department supports the proposed scope change for this project.

BACKGROUND:

Resolution G-16-29 amended the ATP Guidelines to stipulate that any agency implementing an ATP project, present scope changes to the Department for consideration prior to allocation. Therefore, the Department will make a recommendation to the Commission for final approval with the understanding that scope changes that result in a decrease in active transportation benefits may result in removal from the program.

Attachments

Project Scope Change Request

Caltrans' Analysis and Recommendations

Resubmittal Date: 4/15/25

PROJECT NAME: Fort Ord Regional Trail and Greenway: California Avenue Segment

IMPLEMENTING AGENCY: Transportation Agency for Monterey County

ATP ID: ATP6-05-022R

FEDERAL PROJECT NO.: 6143(101)

PPNO: 3130A

DATE OF AGENCY/CT COORDINATION MEETING: 9/19/24

FIELD REVIEW DATE (major only):

APPROVED PROJECT DESCRIPTION: Construction of a 1.8 mile segment of the planned 28-mile FORTAG, including a protected intersection and bicycle and pedestrian overcrossing.

Purpose

This document serves as supplemental information to the Exhibit 25-D REQUEST FOR PROJECT SCOPE CHANGE (attached) completed by Transportation Agency for Monterey County (TAMC) and submitted to Caltrans on 1-30-25.

Caltrans' Recommendation(s)

As a result of Caltrans' review of the TAMC's Scope Change Request documentation and subsequent discussions with TAMC's staff, Caltrans recommends the following action:
APPROVE AS A MAJOR SCOPE CHANGE

Scope to Be Changed

The following is a numbered list of proposed scope changes:

1. Remove the proposed pedestrian overcrossing and replace/relocate with a mid-block crosswalk and HAWK system.
2. Adding 2 enhanced crossings on 8th street at
 - a. California Ave, and
 - b. Patton Parkway

Reason for the Scope Change

The agency is proposing to change the scope for the following reason(s):

1. The City of Marina is moving forward with the design of roundabout with sidewalks, bike lanes, shared used paths, and transit stops. The bridge would have-
 - a. hindered access to regional connectivity.
 - b. Created excessive negative impacts to endangered plants
2. The 2 crossing improvements will provide better access to the CSUMB campus.

Summary of Caltrans Analysis

Caltrans supports this request for the following reasons:

Proposed scope change's affect to benefits (Potential of the project to increase walking and biking as compared to the approved scope):

No change- The proposed scope change is expected to have the following impact on the project's potential to increase walking and biking as compared to the original scope.

1. The City of Marina has started improvements adjacent to the application's location, which includes adding a roundabout at Imjin Road/Parkway. The roundabout also includes transit stops. TAMC has relocated the trail alignment to improve access to the transit facilities.

The proposed roundabout will cause the vehicular speed to be reduced and a HAWK system will allow the non-motorized added safety at the at-grade crossing; while giving the non-motorized users easy access to the proposed transit stop. The addition of new transit stops may increase transit use, instead of the facility being used purely for recreation.

2. The 2 crossing improvements will provide better access to the CSUMB campus.

Proposed scope change's affect to benefits (Potential of the project to increase safety of pedestrians and bicyclists walking biking as compared to the approved scope):

Net Decrease - The proposed scope change is expected to have the following impact on the overall safety of pedestrians and bicyclists as compared to the original scope.

1. The originally proposed separated facility for non-motorized users would have offered the safest means of crossing vehicular traffic. The route in question has low traffic volume; but can experience higher traffic speeds. The use of a roundabout and HAWK system decrease non-motorized safety when compared to a fully separated facility; but will make transit access easier.
2. The 2 crossing improvements will provide increased non-motorized safety when compared to the current configuration.

Additional Comments

Caltrans concurs with the information provided in Attachment 1- Exhibit 25-D REQUEST FOR PROJECT SCOPE CHANGE. The agency has coordinated with Caltrans staff to provide the most accurate information possible.

Caltrans' Coordination with Requesting Agency

Via several email conversations

Impact to Project Cost

The project was awarded \$8.42M, including \$980,000 for Non-Infrastructure efforts. The current construction estimate is \$7.38M

Impact to Project Schedule

The agency will be requesting a Construction allocation time extension.

ATTACHMENTS

1. Exhibit 25-D REQUEST FOR PROJECT SCOPE CHANGE-Completed by Agency
2. Original plans with changes redlined
3. Original Detailed Engineer's Estimate with changes redlined



To: Dianna Garrett
District Local Assistance Engineer
Caltrans, Office of Local Assistance
50 Higuera St.
San Luis Obispo, CA 93401

Date: 10/17/2024
PPNO: 3130A
Federal PROJECT #: 6143(101)
ATP ID #: 0523000102

Project Name (Per CTC programming): Fort Ord Regional Trail and Greenway: California Avenue Segment

Approved Project Description (As submitted in Application): Construction of a 1.8-mile segment of the planned 28-mile FORTAG, including a protected intersection and bicycle and pedestrian overcrossing.

Approved Project Limits (As submitted in Application): FORTAG Segment 3 is located in the City of Marina on California Ave. from Carmel Avenue to 8th Avenue

☐ **For Federally Funded Projects:** Written MPO Concurrence (see Item #7)

Current FTIP/FSTIP Description: N/A

Current FTIP/FSTIP Limits: N/A

Dear Diana:

Consistent with the California Transportation Commission's (CTC) "Policy on Project Amendments and Advance Project Allocations", adopted on August 18, 2016, we request that Caltrans consider, and the CTC approve the proposed Project Amendments as documented below:

REQUIRED ELEMENTS (PER CTC'S PROJECT AMENDMENT POLICY):

1. An explanation of the proposed scope change;

The proposed scope change is to move from a pedestrian bridge over Imjin Road to an at-grade crossing at Imjin Road. In the ATP application, the pedestrian bridge was planned to be located at Imjin Road as per the Draft 30% FORTAG CSUMB North Loop Extension plans prepared by Alta, dated 04/13/2020. The bridge crossing will be replaced by a mid-block at grade crossing along Imjin Road which will include a pedestrian signal to stop vehicle traffic on Imjin Road when activated. At the time of the ATP application the bridge was determined to be the best solution, however, since the application was submitted, geotechnical studies, topographic mapping, and environmental surveys were conducted, as further explained below, which exposed challenges to the bridge that were not known at the time. In addition, at the time of the ATP application, there were no plans for improvements on Imjin Road. The City of Marina has since moved forward with the environmental clearance and design of roadway improvements immediately adjacent to the project location, a roundabout, sidewalk, bike lanes, shared use paths, and transit stops at Imjin Rd and Imjin Parkway. The proposed midblock crosswalk would be adjacent to the new transit stop and would connect the FORTAG trail to this new pedestrian infrastructure, improving regional connectivity beyond what the bridge would have done.

In addition to the originally scoped crossing improvements, the project now proposes two enhanced crossings on 8th Street to provide better access to CSUMB campus and enhanced crossings at California Avenue and Patton Parkway. See Appendix D for Regional Connectivity Map. See Appendix A for original plans and Appendix B for revised plans.

2. The reason for the proposed scope change:

The reason for the proposed scope change is due to the presence of Sand Gilia, a federally protected plant species, the topography on either side of the roadway, the sandy soils, the resulting engineering and environmental constraints, and the costs implications.

The project team met with the California Department of Fish and Wildlife to discuss the project and understand the agency's view on mitigation and to develop a mitigation strategy for the open space area adjacent to the proposed pedestrian bridge crossing. CDFW explained that impacting known occurrences of Sand Gilia if avoidable would not be acceptable and the agency considered the entire open space area as seed bank for Sand Gilia, and mitigation would be required.

The project team explored four alternative locations for the bridge that would not infringe upon the Gilia's critical 50-foot buffer zone and took into consideration the topography on either side of the roadway, the clearance requirements for Imjin Road, the sandy soils, and other engineering and environmental constraints. Additionally, geotechnical assessments revealed suboptimal soil conditions that would necessitate extensive and costly foundational support for the bridge abutments, further complicating the original plan.

The four alternatives included preliminary plans and cost estimates that were provided and discussed with the Transportation Agency for Monterey County (TAMC) and the City of Marina. All four alternatives would result in varying degrees of impact to the Gilia and exceeded the original cost estimate for the bridge and mitigation fees. After careful consideration, the decision was made to remove the pedestrian bridge from the project and replace it with an at-grade crossing due to high costs, poor soil conditions, challenging topography and the need to protect sensitive plant species. See Appendix D for bridge alternative plans.

The two enhanced crossings at California Ave. / Patton Parkway and California Ave. / 8th Street were added to provide connectivity to CSU Monterey Bay to the south and the existing bicycle facilities to the north.

3. The impact the proposed scope change would have on the overall cost of the project;

- The original overall project costs in the approval application equaled \$7,820,722.
- The current overall project costs based on the originally approved project scope equal \$14,788,722.
- The proposed overall project costs based on the proposed scope change equal \$7,386,344.
- See Appendix C for detailed engineer's estimates.

4. An estimate of the impact the proposed scope change would have on the potential of the project to increase walking and bicycling as compared to the benefits identified in the project application (increase or decrease in benefit);

Location/Element	Approved Scope	Proposed Change	Change in Walking/Bicycling Benefit
On Imjin Road from Imjin Parkway to 8 th Street	Pedestrian Bridge	At-grade crossing Pedestrian Hybrid Beacon System	Increase

The currently approved project scope includes a 150 LF pedestrian bridge. The soffit of the pedestrian bridge was required to be a minimum of 17 feet clear of the roadway elevation. As a result, each side of the pedestrian bridge requires long, steep (4.9% longitudinal slopes) approaches.

Conversely, the proposed scope removes the long and steep approaches, providing shorter connections to adjacent pedestrian and bicycling facilities, while offering a more accessible design for individuals of all ages and abilities, as well as connecting to regional transit stops and new bicycle and pedestrian infrastructure the City of Marina is currently constructing, thus increasing the benefit for walking and bicycling.

5. **An estimate of the impact the proposed scope change would have on the potential of the project to increase the safety of pedestrians and bicyclists as compared to the benefits identified in the project application (increase or decrease in benefit);**

Location/Element	Approved Scope	Proposed Change	Change in Safety Benefit
<i>On Imjin Road from Imjin Parkway to 8th Street</i>	<i>Pedestrian Bridge</i>	<i>At-grade crossing w/Hawk System</i>	<i>Equal to slight decrease</i>

The currently approved project scope includes a 150 LF pedestrian bridge. Since the elevated bridge and the road are separated, all conflict points between pedestrians, cyclists and vehicles would be eliminated.

With the proposed at-grade Pedestrian Hybrid Beacon system, vehicles are required to stop at the traffic light once the system is activated. While not eliminating conflict points, the proposed design will greatly reduce conflict points and provide a high level of safety for all users. The crossing will also be marked with high visibility crosswalk markings. Additionally, with the roundabout, the character of the road will be changing from a rural uncontrolled roadway to a more controlled traffic system.

6. **An explanation of the methodology used to develop the aforementioned estimates; and**

The original application portrayed the walking and biking needs and benefits:

Conflict Points: Several conflict points exist along the proposed project route including on California Ave. where the bike lane is within the roadway next to motor vehicles driving 40mph, at the California Ave. and Imjin Parkway intersection where two bike crashes occurred, and at Imjin road where the FORTAG crosses the road.

Inadequate facilities: 2 missing ADA curb ramps and 2 ramps require upgrades. 0.4-mile gap of Class II bike lanes on California Avenue, 0.5 miles of no pedestrian or Class II bike facilities on California Avenue between Imjin Parkway and 5th Ave.

How does project address inadequacies: The proposed project will provide an alternative facility for users with the addition of a 1.4 mile 12-foot multi-use path, installs 2 ADA curb ramps and 2 curb ramp upgrades, a protected intersection at California Ave. and Imjin Parkway, and a pedestrian overcrossing over Imjin Road.

The proposed scope change will continue to address inadequate facilities and conflicts points listed above. The change from a pedestrian bridge to an at-grade crossing at Imjin Road will provide a controlled crossing, thus reducing potential conflicts, but not completely eliminating potential conflicts like a grade separated facility. The project team has prepared a crossing control memo to inform the appropriate level of crossing control at this crossing. The crossing will feature a Pedestrian Hybrid Beacon system, providing full stop control. The pathway alignment is designed to provide a comfortable and convenient connection, but also an offset crossing to increase the awareness of the roadway crossing.

The project team produced alignments and profiles for the originally scoped pedestrian bridge, including provisions for minimizing impacts to critically sensitive species. To maintain 17' clear over the roadway, the bridge would require steep approaches. The proposed at-grade crossing will provide more accessible (less steep), and more direct connection to the new roundabout and bus stops at Imjin Parkway and the next segment of this trail, which will increase the likelihood that the trail is used for transportation rather than just for recreation.

In addition to the originally scoped crossing improvements, the project now proposes two enhanced crossings on 8th Street to provide better access to CSUMB campus and enhanced crossings at California Avenue and Patton Parkway.

7. **For projects programmed in the Metropolitan Planning Organization (MPO) component, evidence of MPO approval and the MPO rationale for their approval.**

No federal funds.

ADDITIONAL QUESTIONS:

8. Does this scope change require revalidation of your environmental document? No
If yes, what is the actual/estimated date of revalidation? _____

No, it follows the same alignment originally identified and reduces significantly the impacts to Sand Gilia and the natural environment.

9. Explain the additional public outreach efforts you have made with respect to this proposed scope change and provide a summary of the public response to these efforts:

Significant public outreach was conducted during the environmental clearance phase of the project. This outreach was focused more on the general routing and alignment and focused less on the specifics of the design. The 30% plans were not circulated to the public; therefore, this will not be a significant change to the community's expectations. The project is scoped to conduct various public meetings and stakeholder meetings, which will focus on the specifics of the design.

The project team has reached out to the City of Marina, California State University at Monterey Bay, and the FORTAG founders, to explain the scope change and all parties fully support the scope change.

REQUIRED Attachments: (check boxes of attached required documents)

- | | |
|--|--|
| <input checked="" type="checkbox"/> Original plans/workplan with changes highlighted | <input checked="" type="checkbox"/> Revised Detailed Engineer's Est. |
| <input checked="" type="checkbox"/> Revised plans/workplan | <input type="checkbox"/> Written MPO Concurrence (see Item #7) |
| <input checked="" type="checkbox"/> Original Detailed Engineer's Est. with changes highlighted | <input checked="" type="checkbox"/> Additional Revised Application Documentation |

Required revisions to the Project's Description and/or Limits:

The proposed Project Amendments documented above will require the following changes to the Project's Description and/or Limits:

Proposed changes to the Project Description: Construction of a 1.8-mile segment of the planned 28-mile FORTAG, including a protected intersection and at-grade pedestrian hybrid beacon.

Proposed changes to the Project Limits: N/A

For Federally Funded Projects:

Proposed changes to the FTIP/FSTIP Description: N/A

Proposed changes to the FTIP/FSTIP Limits: N/A

Project Delivery Status:

The following is a side-by-side comparison of the original project schedule and the current project schedule. The explanations for each milestone date change is listed below:

Original CTC Allocation Dates: (as programmed by the CTC when the application was approved for funding):

PA&ED: _____ PS&E: 23/24 R/W: _____ CON: 24/25 CON-NI: 23/24

Actual/Currently Anticipated CTC Allocation Dates: (at the time of this request)

PA&ED: _____ PS&E: 23/24 R/W: _____ CON: 25/26 CON-NI: 25/26

Explanation for milestone changes:

Original construction allocation has been pushed from 24/25 to 25/26 due to the bridge location and bridge type engineering and

cost alternative analysis. Additionally, the project schedule was changed to account for the completion of adjacent projects to ensure there are no conflicts with construction footprints and operations.

Local Agency Certification:

This Request for Scope Change has been prepared in accordance with the *Procedures for Administering Local Projects in the Active Transportation Program (ATP)*. I certify that the information provided in the document is accurate and correct. I understand that if the required information has not been provided this form will be returned and the request may be delayed. Please advise us as soon as the scope change has been approved.

You may direct any questions to:

_____ at _____
(name) (phone number)

Signature: Janneke Strause Title: Associate Transportation Planner Date: 10/24/2024

Agency/Commission: Transportation Agency for Monterey County

Attachments: Appendix A-D

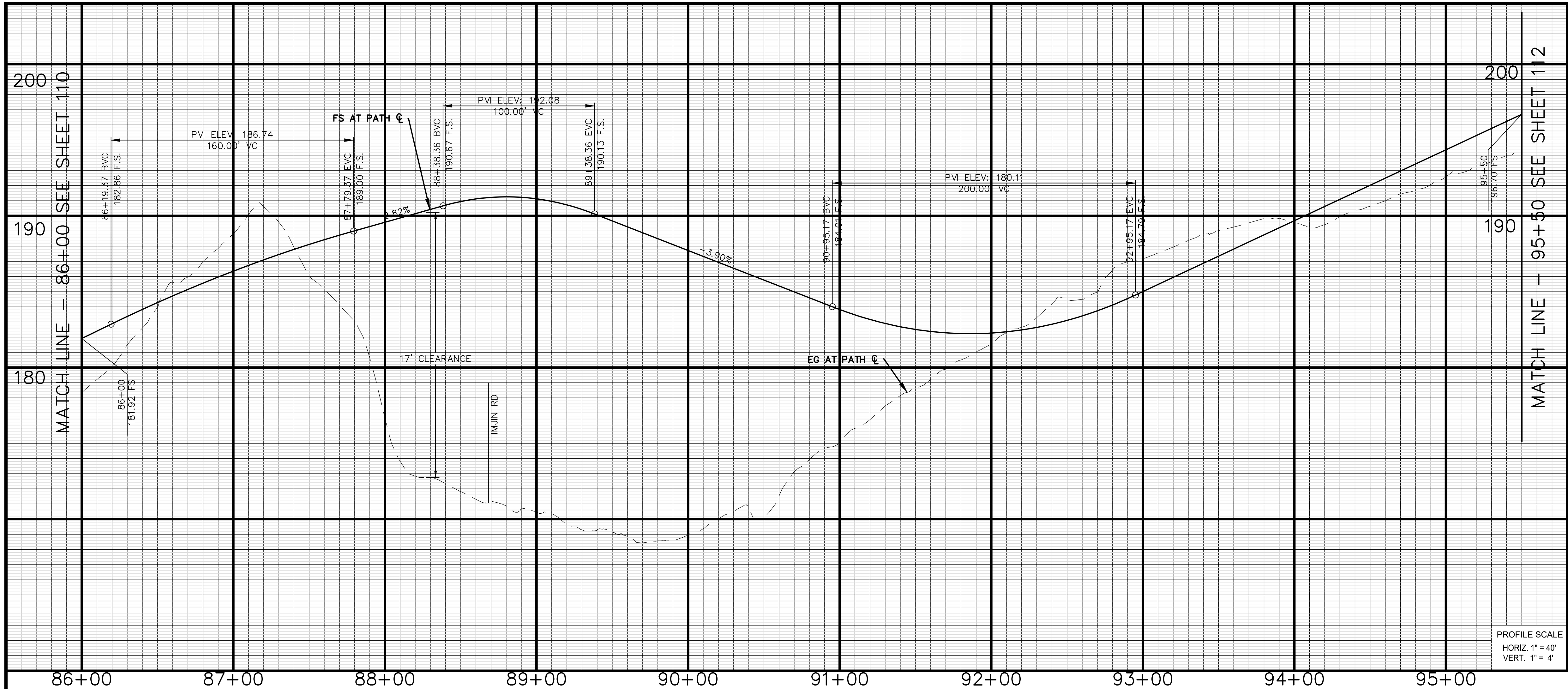
Distribution:

- (1) Original -DLAE
- (2) Copy – Division of Local Assistance, Headquarters ATP Program Manager
- (3) Copy – MPO/RTPA/County Transportation Commission

Appendices

Appendix A

Original plans with changes highlighted



CONSTRUCTION NOTES:

DISPOSITION NOTES

HATCH / LINE LEGEND

- NEW SHARED USE PATH (SEE TYPICAL SECTIONS)
- BRIDGE CROSSING
- PROPERTY LINE
- EXISTING ELECTRICAL
- EXISTING TELECOMMUNICATION
- EXISTING WATER
- EXISTING GAS
- CONTOUR LINE AT 1' INTERVAL

LINE / CURVE TABLE

#	RADIUS	LENGTH	DELTA/BRG	TANGENT
L1	—	64.75'	S64° 02' 33.55"E	—
C1	90.00'	44.72'	28° 28' 08"	22.83'
L2	—	223.06'	S35° 34' 25.63"E	—
C2	90.00'	116.92'	74° 25' 52"	68.35'
L3	—	307.03'	N69° 59' 42.27"E	—
C3	90.00'	50.61'	32° 13' 12"	25.99'
L4	—	210.23'	S77° 47' 05.58"E	—



KEY MAP



NO WORK SHALL BE DONE ON THIS SITE UNTIL BELOW AGENCY IS NOTIFIED OF INTENTION TO GRADE OR EXCAVATE.

Underground Service Alert
Call: TOLL FREE
811

TWO WORKING DAYS BEFORE YOU DIG

30% SUBMITTAL
NOT FOR CONSTRUCTION

alta
PLANNING + DESIGN
617 W. 7TH STREET, SUITE 1103
LOS ANGELES, CA 90017
p: 213.489.7443
www.altaplanning.com

PREPARED UNDER THE DIRECT SUPERVISION OF:

STEVEN FRIESON, R.C.E. NO. 42110

DATE:

SCALE AS SHOWN

F.B. NO. PAGE TO

DESIGNED DV/EK DATE 5/4/2020

DRAWN DV/EK DATE 5/4/2020

CHECKED VH DATE 5/11/2020

DATE REVISION BY

PROJECT:

FORTAG IMPROVEMENTS
RECONSTRUCTION

SUBMITTED

DATE

APPROVED

DATE

JOB NO.

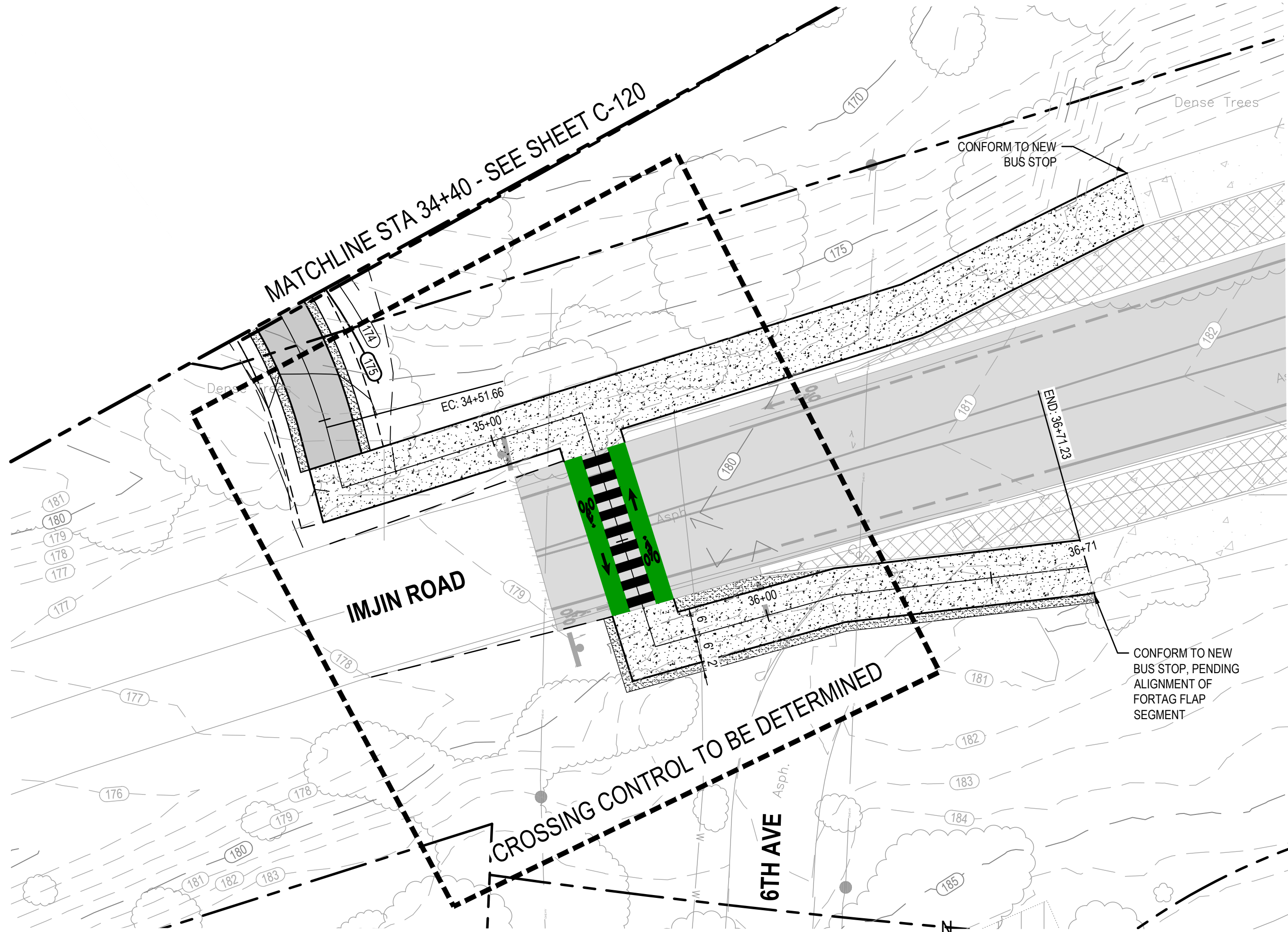
DRAWING NO.

SHEET 111 OF SHEET

TRANSPORTATION AGENCY FOR MONTEREY COUNTY
CIVIL ENGINEERING DEPARTMENT

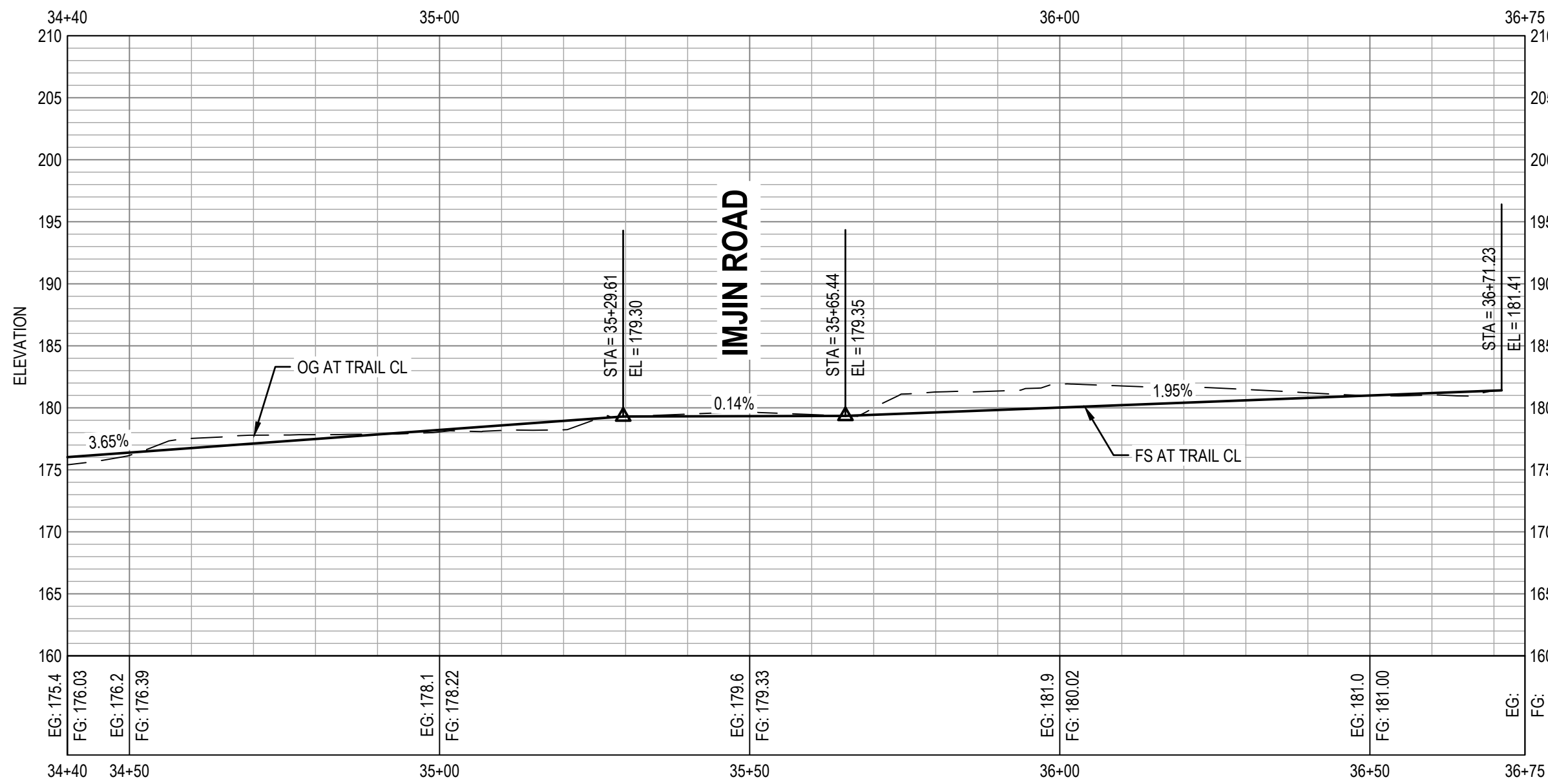
Appendix B

Revised plans



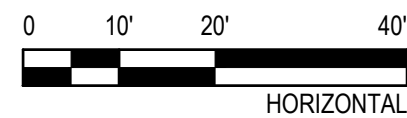
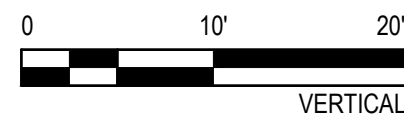
IMJIN RD TRAIL - PLAN VIEW

SCALE: 1" = 20'



IMJIN RD TRAIL - PROFILE VIEW

SCALE: 1" = 20' HORIZ, 1" = 10' VERT



GENERAL NOTES:

- ALL LOCATIONS OF UTILITIES SHOWN ARE APPROXIMATE AND DEPICT UTILITY LOCATIONS AT BEGINNING OF CONSTRUCTION. CONTRACTOR IS RESPONSIBLE TO FIELD VERIFY EXACT LOCATIONS OF ALL UTILITIES PRIOR TO ANY UNDERGROUND WORK.
- CONTRACTOR SHALL REPLACE ALL DISTURBED AREAS IN-KIND.
- FOR PAVEMENT AND TRAIL STRUCTURAL SECTIONS, SEE GENERAL SHEETS CROSS SECTIONS.

SHEET KEYNOTES

- FUTURE ROADWAY IMPROVEMENTS BY OTHERS.

LEGEND

- | | | | |
|--|--|--|---|
| | PAVEMENT
HOT MIX ASPHALT | | DG SHOULDER |
| | DETECTABLE WARNING
SURFACE | | MINOR CONCRETE |
| | SENSITIVE SPECIES,
VARIOUS, MAPPED 2024 | | SENSITIVE SPECIES, GLIA,
MAPPED 2024 |

35% PRELIMINARY

No.	Issue	Checked	Approved	Date
Author	C. PHELPS	Drafting Check	C. OTTO	Project Manager
Designer	C. PHELPS	Design Check	C. OTTO	Project Director
			L. VAN PARYS	

Bar is one inch on original size sheet
0 1"



GHD Inc.
655 Montgomery Street Suite 1010
San Francisco California 94111 USA
T 1 415 283 4970 F 1 415 283 4980

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Client FORTAG

Project CALIFORNIA AVENUE SEGMENT
FINAL DESIGN

Project No.
12603415

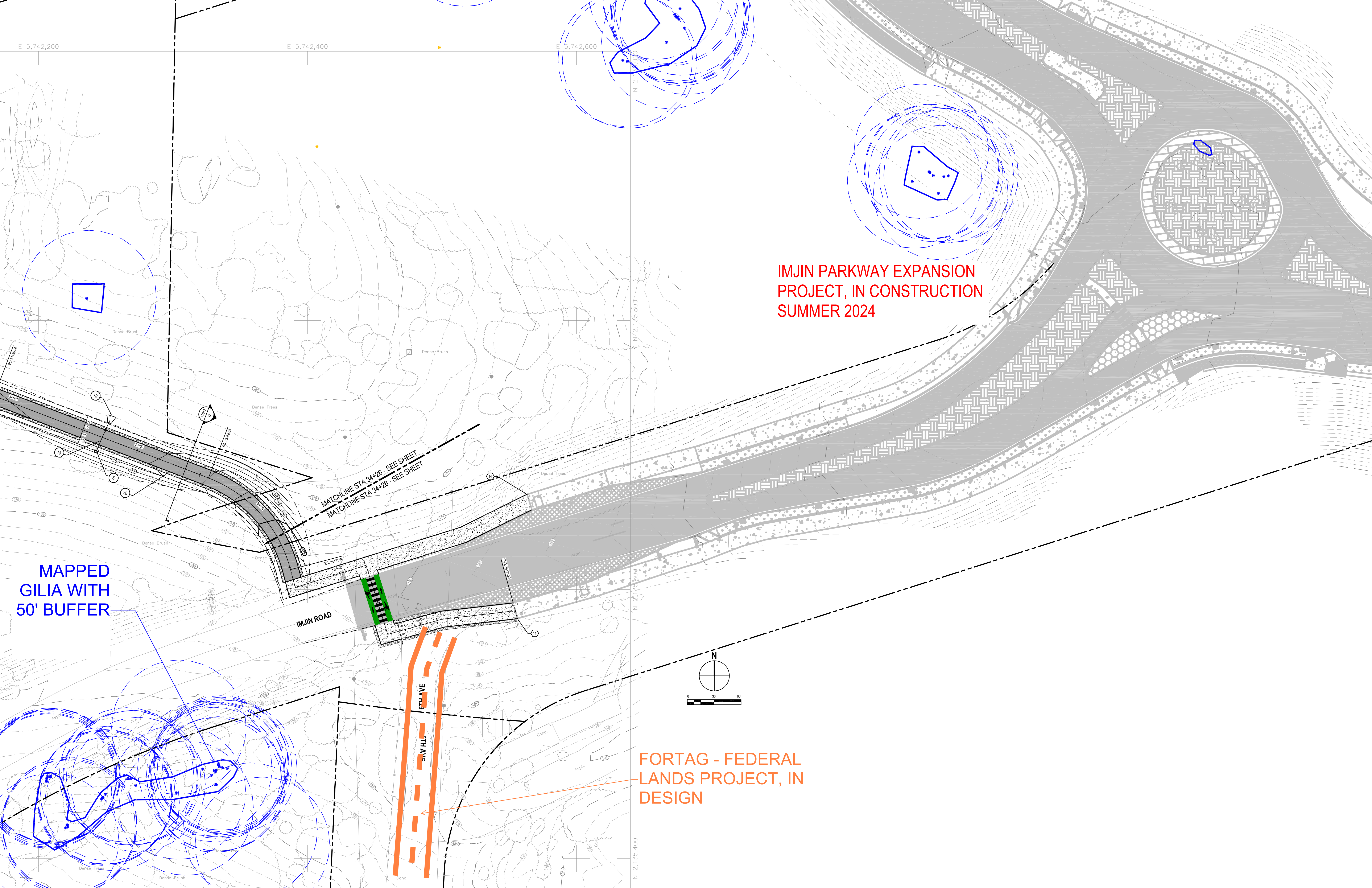
Date
JULY 17, 2024

Scale
AS SHOWN

Title IMPROVEMENT PLAN AND
PROFILE - 21

Size
ANSI D

Sheet No.
C-121
Sheet
32 of 34



IMJIN PARKWAY EXPANSION
PROJECT, IN CONSTRUCTION
SUMMER 2024

MAPPED
GILIA WITH
50' BUFFER

FORTAG - FEDERAL
LANDS PROJECT, IN
DESIGN

E 5,742,200

E 5,742,400

E 5,742,600

N 2,135,400

N 2,135,600

N 2,135,800

N 2,136,000

Appendix C

- + Original Engineer's Estimate**
- + Original Engineer's Estimate w/lowest cost bridge alternative**
- + Revised Engineer's Estimate**

FORTAG California Avenue Segment

ATP Cycle 5 - Scope Change Request

Cost Summary

Scope	Total Construction Cost	Detail
ATP Cycle 5 Application (Overpass Bridge)	\$7,820,722	Pg. 3-4
Original Scope (Overpass Bridge) with updated cost	\$14,788,722	Pg. 6-7
Updated Scope (at-grade crossing)	\$7,386,344	Pg. 9-11

ORIGINAL DETAILED
ENGINEER'S
ESTIMATE

Detailed Project Estimate and Total Project Costs- Cycle 6

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency: Transportation Agency for Monterey County

Date: 6/9/2022

Project Description: Fort Ord Regional Trail and Greenway: California Avenue Segment

Project Location: 5th Avenue/California Avenue from Imjin Road to Patton Parkway

Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate: Steve Grupico, PE

License #: 74607

Project Estimate and Cost Breakdown:

Project Estimate (for Construction Items Only)						Cost Breakdown					
						ATP Eligible Costs/Items		ATP Ineligible Costs/Items		Corps/CCC to construct	
Item No.	Item	Quantity	Units	Unit Cost	Total Item Cost	%	\$	%	\$	%	\$
General Overhead-Related Construction Items											
1	Mobilization	1	LS	\$286,000.00	\$286,000	100%	\$286,000				
2	Traffic Control	1	LS	\$191,000.00	\$191,000	100%	\$191,000				
3	Stormwater Protection Plan	1	LS	\$100,000.00	\$100,000	100%	\$100,000				
4	Permitting including Fees	1	LS	\$150,000.00	\$150,000	100%	\$150,000				
5	Clearing and Grubbing	3.6	AC	\$10,000.00	\$36,000	100%	\$36,000				
6	Environmental Protection and Mitigation	1	LS	\$100,000.00	\$100,000	100%	\$100,000				
7			LS					100%			
8			LS					100%			
9			LS					100%			
10			LS					100%			
General Construction Items											
11	Excavation	9370	CUYD	\$130.00	\$1,218,100	100%	\$1,218,100				
12	Demolition	1	LS	\$100,000.00	\$100,000	100%	\$100,000				
13	Misc Utilities	1	LS	\$65,000.00	\$65,000	100%	\$65,000				
14	ADA Curb Ramp	10	EA	\$8,000.00	\$80,000	100%	\$80,000				
15	8" CL II AB	1400	CY	\$85.00	\$119,000	100%	\$119,000				
16	4" HMA Pavement	3530	TON	\$150.00	\$529,500	100%	\$529,500				
17	Construct Overpass Bridge	1	LS	\$2,000,000.00	\$2,000,000	100%	\$2,000,000				
18	Signing and Striping	1	LS	\$140,000.00	\$140,000	100%	\$140,000				
19	Landscape and Irrigation Improvements	1	LS	\$120,000.00	\$120,000	100%	\$120,000				
20								100%			
21								100%			
22								100%			
23								100%			
24								100%			
25								100%			
26								100%			
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44								100%			
45								100%			
46								100%			
47								100%			
48								100%			
49								100%			
50								100%			
51								100%			
52								100%			
Subtotal of Construction Items:					\$5,234,600		\$5,234,600				

Construction Item Contingencies (% of Construction Items): 30.00% \$1,570,380

\$1,570,380

Total (Construction Items & Contingencies) cost: \$6,804,980

\$6,804,980

Project Delivery Costs:

Type of Project Cost		Cost \$			
		Preliminary Engineering (PE)		ATP Eligible Costs	Non-participating Costs
Environmental Studies and Permits(PA&ED):					
Plans, Specifications and Estimates (PS&E):		\$	1,328,278	\$1,328,278	
Total PE:		\$	1,328,278	\$1,328,278	"PE" costs / "CON" costs
					20% 25% Max

Detailed Project Estimate and Total Project Costs- Cycle 6

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency: Transportation Agency for Monterey County

Date: 6/9/2022

Project Description: Fort Ord Regional Trail and Greenway: California Avenue Segment

Project Location: 5th Avenue/California Avenue from Imjin Road to Patton Parkway

Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate: Steve Grupico, PE

License #: 74607

Right of Way (RW)

Right of Way Engineering:

Acquisitions and Utilities:

Total RW: \$ -

Total Pre-Construction Costs (PE+RW): \$1,328,278

Construction Engineering (CE)

Construction Engineering (CE): \$ 1,015,742

Total Construction Costs: \$7,820,722

\$1,015,742

--

"CE" costs / "CON" costs

15% **15% Max**

ATP Eligible Costs

Non-participating Costs

Total Project Cost: \$9,149,000

\$9,149,000

--

Documentation of Ineligible (Non-Participating) Costs:

The Engineer's logic and/or calculations for splitting costs between ATP-Eligible and Non-participating costs must be documented in this section of the Estimate form.

Separate logic is required for each item which is partly ineligible for ATP funding or is required for the construction of an ineligible item/element of the project.

Item #:	Description of Engineer's Logic: (See examples shown in the Instructions)

ORIGINAL DETAILED
ENGINEER'S ESTIMATE
W/LOWEST COST
BRIDGE ALTERNATIVE

Detailed Project Estimate and Total Project Costs- Cycle 6

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency:Transportation Agency for Monterey County

Date:6/9/2022

Project Description:Fort Ord Regional Trail and Greenway: California Avenue Segment

Project Location:5th Avenue/California Avenue from Imjin Road to Patton Parkway

Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate:Steve Grupico, PE

License #:74607

Project Estimate and Cost Breakdown:

Project Estimate (for Construction Items Only)

Cost Breakdown

ATP Eligible Costs/Items

ATP Ineligible Costs/Items

Corps/CCC to construct

Item No.	Item	Quantity	Units	Unit Cost	Total Item Cost	%	\$	%	\$	%	\$
General Overhead-Related Construction Items											
1	Mobilization	1	LS	\$286,000.00	\$286,000	100%	\$286,000				
2	Traffic Control	1	LS	\$191,000.00	\$191,000	100%	\$191,000				
3	Stormwater Protection Plan	1	LS	\$100,000.00	\$100,000	100%	\$100,000				
4	Permitting including Fees	1	LS	\$150,000.00	\$150,000	100%	\$150,000				
5	Clearing and Grubbing	3.6	AC	\$10,000.00	\$36,000	100%	\$36,000				
6	Environmental Protection and Mitigation	1	LS	\$100,000.00	\$100,000	100%	\$100,000				
7			LS					100%			
8			LS					100%			
9			LS					100%			
10			LS					100%			
General Construction Items											
11	Excavation	9370	CUYD	\$130.00	\$1,218,100	100%	\$1,218,100				
12	Demolition	1	LS	\$100,000.00	\$100,000	100%	\$100,000				
13	Misc Utilities	1	LS	\$65,000.00	\$65,000	100%	\$65,000				
14	ADA Curb Ramp	10	EA	\$8,000.00	\$80,000	100%	\$80,000				
15	8" CL II AB	1400	CY	\$85.00	\$119,000	100%	\$119,000				
16	4" HMA Pavement	3530	TON	\$150.00	\$529,500	100%	\$529,500				
17	Construct Overpass Bridge	1	LS	\$7,360,000.00	\$7,360,000	100%	\$7,360,000				
18	Signing and Striping	1	LS	\$140,000.00	\$140,000	100%	\$140,000				
19	Landscape and Irrigation Improvements	1	LS	\$120,000.00	\$120,000	100%	\$120,000				
20								100%			
21								100%			
22								100%			
23								100%			
24								100%			
25								100%			
26								100%			
27								100%			
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41								100%			
42								100%			
43								100%			
44								100%			
45								100%			
46								100%			
47								100%			
48								100%			
49								100%			
50								100%			
51								100%			
52								100%			
Subtotal of Construction Items:					\$10,594,600		\$10,594,600				
Construction Item Contingencies (% of Construction Items):				30.00%	\$3,178,380		\$3,178,380				
Total (Construction Items & Contingencies) cost:					\$13,772,980		\$13,772,980				
Project Delivery Costs:											
Type of Project Cost					Cost \$						
Preliminary Engineering (PE)											
Environmental Studies and Permits(PA&ED):							ATP Eligible Costs				
Plans, Specifications and Estimates (PS&E): \$					1,328,278		Non-participating Costs				
Total PE:					\$ 1,328,278		"PE" costs / "CON" costs				
					6		10% 25% Max				

9/23/2024

1 of 2

Detailed Project Estimate and Total Project Costs- Cycle 6

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency:	Transportation Agency for Monterey County	Date:	6/9/2022
Project Description:	Fort Ord Regional Trail and Greenway: California Avenue Segment		
Project Location:	5th Avenue/California Avenue from Imjin Road to Patton Parkway		
Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate:	Steve Grupico, PE	License #:	74607

Right of Way (RW)			
Right of Way Engineering:			
Acquisitions and Utilities:			
Total RW:	\$	-	
Total Pre-Construction Costs (PE+RW):	\$1,328,278	\$1,328,278	

Construction Engineering (CE)			
Construction Engineering (CE):	\$ 1,015,742	\$1,015,742	<div style="display: flex; justify-content: space-between;"> "CE" costs / "CON" costs 7% 15% Max </div>
Total Construction Costs:	\$14,788,722	\$14,788,722	

	ATP Eligible Costs	Non-participating Costs	
Total Project Cost:	\$16,117,000	\$16,117,000	

Documentation of Ineligible (Non-Participating) Costs:

The Engineer's logic and/or calculations for splitting costs between ATP-Eligible and Non-participating costs must be documented in this section of the Estimate form.

Separate logic is required for each item which is partly ineligible for ATP funding or is required for the construction of an ineligible item/element of the project.

Item #:	Description of Engineer's Logic: (See examples shown in the Instructions)

REVISED DETAILED
ENGINEER'S ESTIMATE
W/AT-GRADE CROSSING

FORTAG Cal Ave 35% Design Cost Estimate

GHD Project No 12603415

18-Aug-24



Item #	Item Description	UM	QTY	Unit Cost	Total Cost
Mobilization					
1	Mobilization	LS	1	\$298,700.00 \$	298,700
2	Temporary Traffic Control	LS	1	\$150,000.00 \$	150,000
3	Construction Area Signs	LS	1	\$40,000.00 \$	40,000
4	Utility Conflict Allowance	LS	1	\$25,000.00 \$	25,000
5	Temporary Construction Fencing	LS	1	\$50,000.00 \$	50,000
6	Tree Protection	LS	1	\$75,000.00 \$	75,000
7	Environmental Mitigation	LS	1	\$	-
Water Pollution Control					
8	Storm Water Pollution Prevention Plan (SWPPP)	LS	1	\$20,000.00 \$	20,000
9	Erosion & Sediment Control	LS	1	\$75,000.00 \$	75,000
Demo					
10	Remove AC Roadway	SF	11,600	\$7.00 \$	81,200
11	Remove AC Dike	LF	300	\$1.50 \$	450
12	Remove Curb and Gutter	LF	520	\$25.00 \$	13,000
13	Remove Concrete Sidewalk	SF	8,000	\$7.00 \$	56,000
14	Remove Concrete Cross Gutter	SF	200	\$10.00 \$	2,000
15	Remove Concrete Driveway	SF	-	\$	-
16	Remove Wall and Stairs	LF	20	\$25.00 \$	500
17	Remove Fence	LF	-	\$	-
18	Remove Tree	EA	3	\$1,000 \$	3,000
Earthwork					
19	Clearing and Grubbing	AC	6	\$15,000 \$	90,000
20	Roadway Excavation	CY	15,745	\$55.00 \$	865,975
21	Import Fill	CY	3,555	\$80.00 \$	284,400
22	Structural Excavation (Retaining Wall)	CY	120	\$160.00 \$	19,200
23	Structural Backfill (Retaining Wall)	CY	50	\$90.00 \$	4,500
Utilities					
24	Adjust Manhole to Grade	EA	6	\$2,000 \$	12,000
25	Adjust Valve to Grade	EA	4	\$1,500 \$	6,000
26	Storm Drain Inlet	EA	4	\$8,000 \$	32,000
27	Adjust Utility Box to Grade	EA	3	\$1,500 \$	4,500
28	4-inch Curb Drains	LF	60	\$85.00 \$	5,100
29	12-inch Storm Drain (HDPE)	LF	170	\$210.00 \$	35,700
30	18-inch Storm Drain Extension (HDPE)	LF	30	\$250.00 \$	7,500
31	Relocate Fire Hydrant	EA	4	\$8,000 \$	32,000
32	Relocate Flared End Section	EA	2	\$800.00 \$	1,600
33	Relocate RSP	SY	50	\$400.00 \$	20,000
34	Flared End Section	EA	1	\$150.00 \$	150
35	Rock Slope Protection (Class Light, Method B)	CY	3	\$400.00 \$	1,200
36	Rock Slope Protection Fabric	SF	75	\$5.00 \$	375

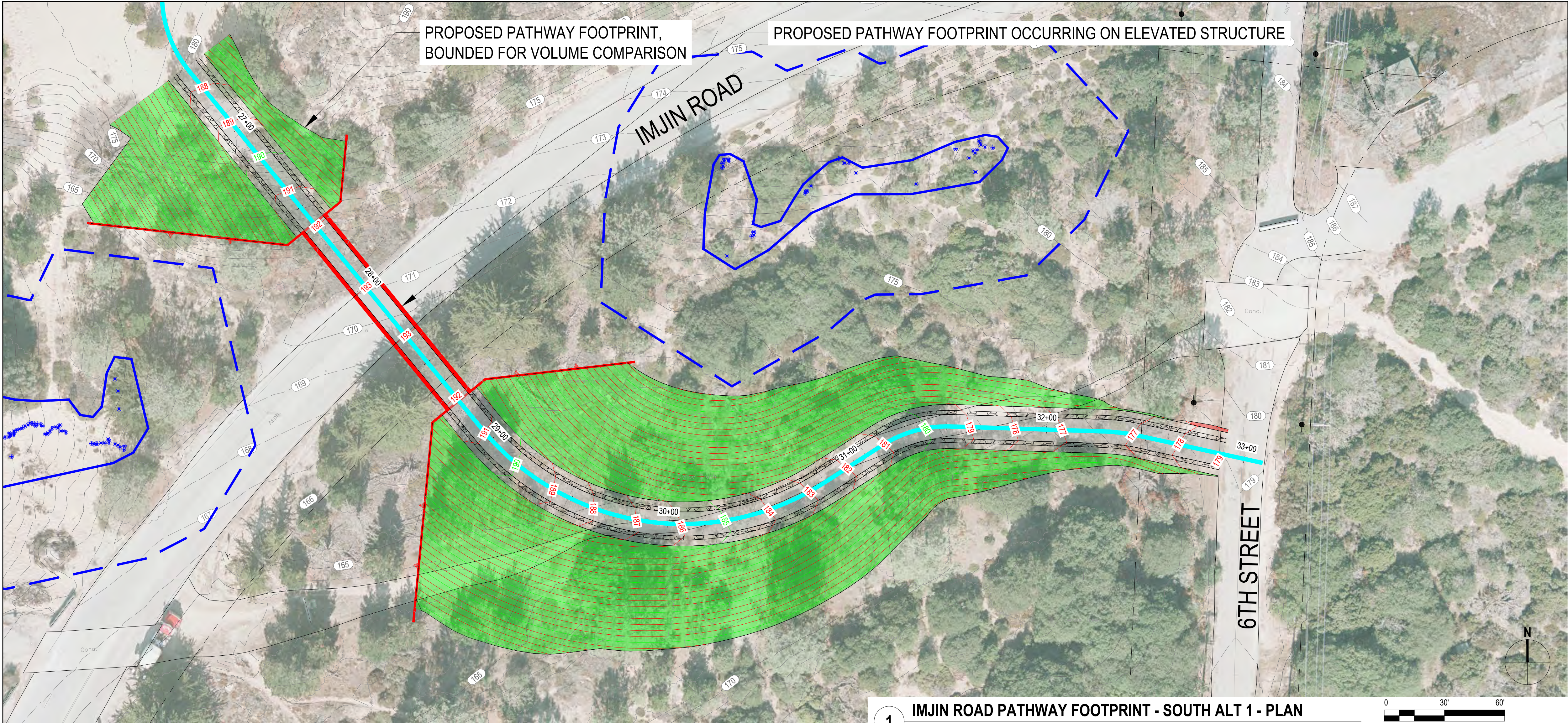
Item #	Item Description	UM	QTY	Unit Cost	Total Cost
Low Impact Development (LID)					
37	LID Area, Swale or Basin	SF	380	\$50.00	\$ 19,000
Concrete					
38	8-inch Median Curb	LF	150	\$55.00	\$ 8,250
39	Retaining Curb, 18" Ht	LF	40	\$175.00	\$ 7,000
40	Median Stamped Concrete	SF	400	\$20.00	\$ 8,000
41	Sidewalk	SF	5,000	\$16.00	\$ 80,000
42	PCC Curb Ramp	EA	13	\$12,500.00	\$ 162,500
43	PCC Bike Ramp	EA	2	\$8,500.00	\$ 17,000
44	Concrete Valley Gutter	SF	1,000	\$35.00	\$ 35,000
Paving					
45	Roadway HMA Paving (Type A) (F)	TN	130	\$330.00	\$ 42,900
46	Trail HMA Paving (Type A) (F)	TN	1,800	\$330.00	\$ 594,000
47	Driveway HMA Paving (F)	TN	60	\$330.00	\$ 19,800
48	Class 2 Aggregate Base, placed, compacted	CY	2,131	\$360.00	\$ 767,160
49	2-inch Grind - Variable Thickness HMA Overlay	TN	150	\$350.00	\$ 52,500
Structural					
50	3-foot High Retaining Wall	LF	100	\$850.00	\$ 85,000
Striping					
51	Signing and Striping	LS	1	\$120,000.00	\$ 120,000
Lighting					
52	Lighting	LS	1	\$300,000.00	\$ 300,000
Traffic Signals					
53	New RRFB - 8th Ave at Cal Ave and Cal Ave at Patton	EA	2	\$46,000.00	\$ 92,000
54	New HAWK - Imjin Road	LS	1	\$144,500.00	\$ 144,500
Landscaping					
55	Landscape Planting	SF	3,000	\$7.00	\$ 21,000
56	Landscape Irrigation	SF	-	\$8.00	\$ -
57	Landscape Concrete	SF	-	\$15.00	\$ -
58	Landscape Seat Wall	LF	-	\$300.00	\$ -
59	Landscape Amenities-Bench	EA	6	\$6,500.00	\$ 39,000
60	Landscape Amenities-Trash/Recycle Bin	EA	5	\$5,000.00	\$ 25,000
61	Landscape Amenities-Bike Rack	EA	5	\$2,600.00	\$ 13,000
62	Landscape Amenities-Hydration Station	EA	1	\$9,000.00	\$ 9,000
63	Landscape Amenities-Boulders	LS	1	\$20,000.00	\$ 20,000
64	Landscape Monument Signage	EA	2	\$25,000.00	\$ 50,000
65	Landscape Wayfinding Signage	EA	8	\$0.00	\$ -
66	Landscape Interpretive Signage	EA	2	\$5,000.00	\$ 10,000
67	Landscape Thermoplastic Pavement Marking	SF	-	\$12.00	\$ -
68	Fencing - Split Rail	LF	2,340	\$95.00	\$ 222,300
				Subtotal	\$ 5,275,960
69	Design Contingency	30%		\$	1,582,788
70	Cost Escalation - 2024-to-2026 mipoint of construcion	10%		\$	527,596
71				Construction Total	\$ 7,386,344
SF-Revised Total					

Item #	Item Description	UM	QTY	Unit Cost	Total Cost
72	<u>Program Costs</u>				
73	Design			Separate Budget	
74	Construction Management & Inspection			Separate Budget	
75	Real Estate and Environmental Mitigation			Separate Budget	
76	Administrative Costs			Separate Budget	

Appendix D

Additional Documentation

- + Bridge Alternative Plans**
- + Regional Connectivity Map**
- + Imjin Rd. At-Grade Crossing Tech Memo**

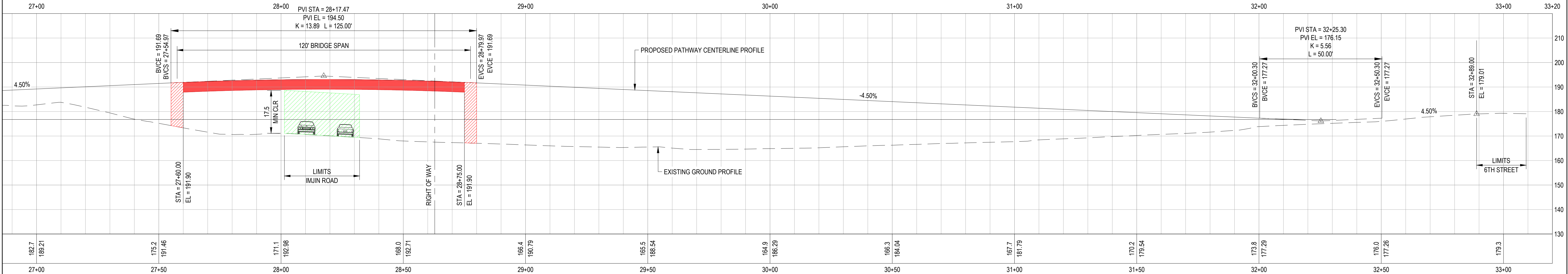


- SHEET GENERAL NOTES**
- THICK RED LINE REPRESENTS PROPOSED ELEVATED STRUCTURE CARRYING PROPOSED PATHWAY.
 - PROPOSED PATHWAY FOOTPRINT INCLUDES FINISH GRADE CONTOURS AND GREEN SHADING FOR FILL ZONE AND RED SHADING FOR CUT ZONE.
 - BLUE POLYGONS REPRESENT GILIA PLANT AREA BOUNDARIES. INSIDE THE PLANT AREA BOUNDARIES ARE BLUE DOTS THAT REPRESENT GILIA PLANT LOCATIONS PER RECENT FIELD STUDY. THE DASHED BLUE POLYGONS REPRESENT A 50' WIDE BUFFER ZONE.
 - YELLOW STRIPE REPRESENTS EXISTING HISTORICAL ROADWAY ALIGNMENT PER RECENT FIELD STUDY.

Alternative 1 Clear-Span Truss w/ Cast In Place Retaining Walls

PROPOSED PATHWAY FOOTPRINT GRADING.
INCLUDES ENGINEERED MATERIALS.
TOP FG WITH 3:1 SIDE SLOPE TO DAYLIGHT EG

SOUTH ALTERNATE 1A GRADING
16,516 cu. yd. NET FILL BOUNDED



30% PRELIMINARY

No.	Issue	Checked		Approved	Date
Author	C. PHELPS	Drafting Check	C. OTTO	Project Manager	S. GRUPICO
Designer	C. PHELPS	Design Check	S. GRUPICO	Project Director	##

Bar is one inch on
original size sheet

0 1"



GHD Inc.
655 Montgomery Street Suite 1010
San Francisco California 94111 USA
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Client **FORTAG**

Project **CALIFORNIA AVENUE SEGMENT
FINAL DESIGN**

Project No.
12603415

Date
MARCH 6, 2024

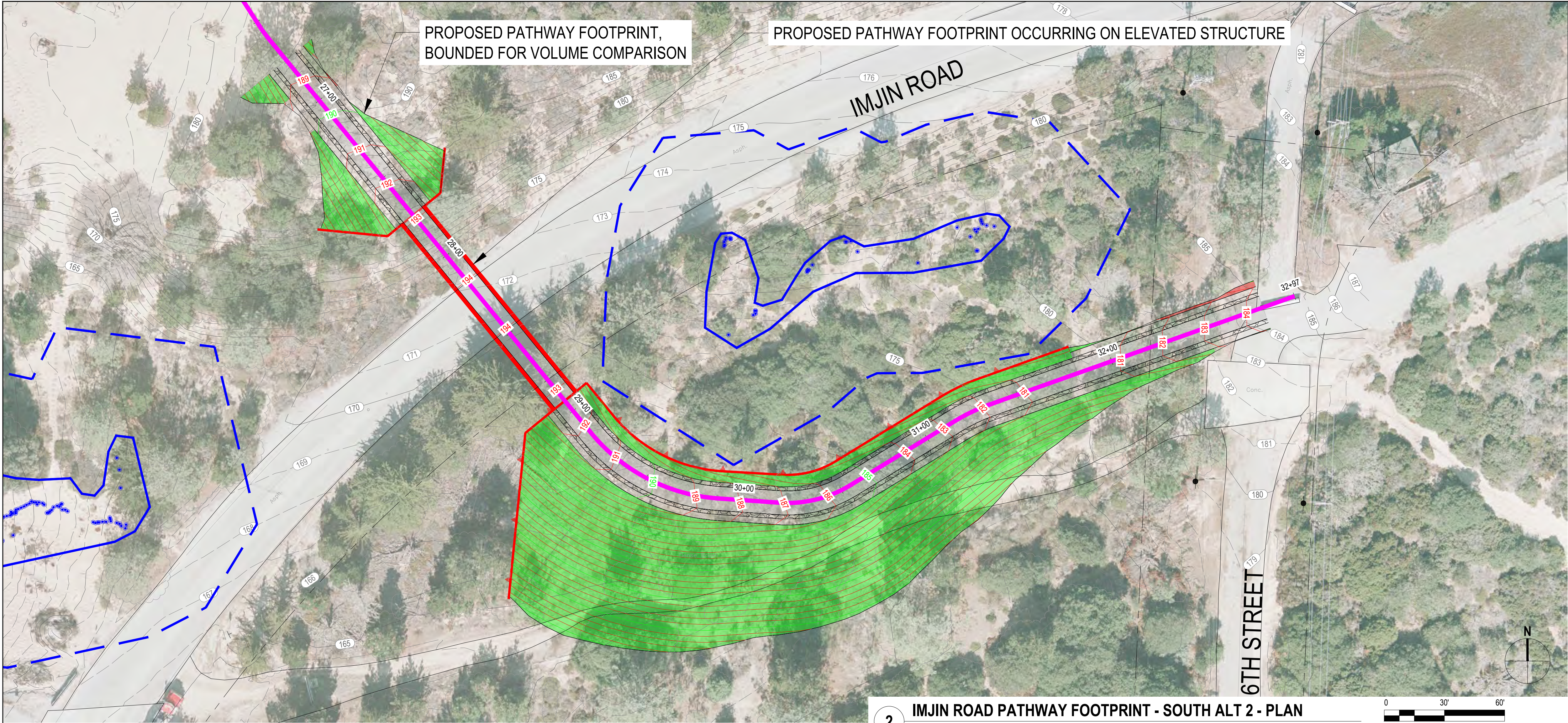
Scale
AS NOTED

Title **IMJIN RD PATHWAY FOOTPRINT
SOUTH ALTERNATE 1 PLAN AND
PROFILE**

Sheet No.
FIG 1

Size
ANSI D

Sheet
1 of 3



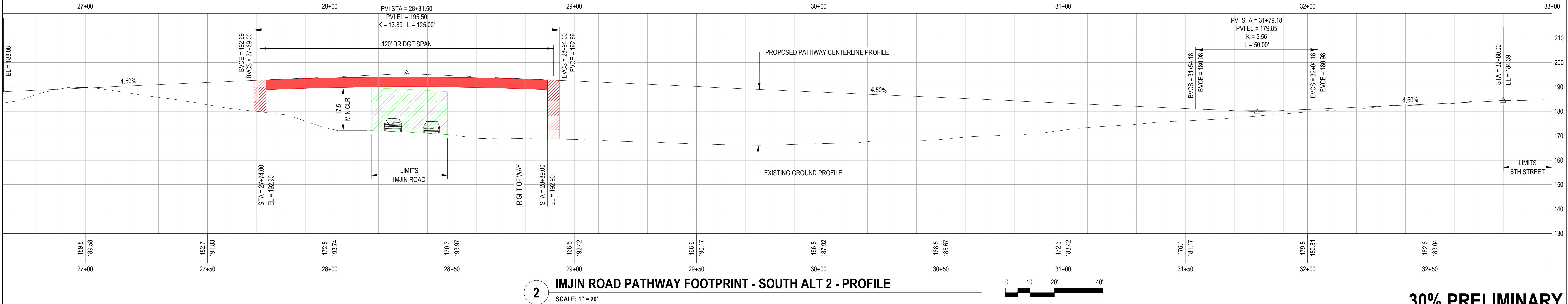
- SHEET GENERAL NOTES**
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 - PROPOSED PATHWAY FOOTPRINT INCLUDES FINISH GRADE CONTOURS AND GREEN SHADING FOR FILL ZONE AND RED SHADING FOR CUT ZONE.
 - BLUE POLYGONS REPRESENT GILIA PLANT AREA BOUNDARIES. INSIDE THE PLANT AREA BOUNDARIES ARE BLUE DOTS THAT REPRESENT GILIA PLANT LOCATIONS PER RECENT FIELD STUDY. THE DASHED BLUE POLYGONS REPRESENT A 50' WIDE BUFFER ZONE.
 - YELLOW STRIPE REPRESENTS EXISTING HISTORICAL ROADWAY ALIGNMENT PER RECENT FIELD STUDY.

Alternative 2

Clear-Span Truss W/ CIP Retaining Walls and Mechanically Stabilized Earth Wall

PROPOSED PATHWAY FOOTPRINT GRADING.
INCLUDES ENGINEERED MATERIALS.
TOP FG WITH 3:1 SIDE SLOPE TO DAYLIGHT EG

SOUTH ALTERNATE 2 GRADING
11,037 cu. yd. NET FILL BOUNDED



30% PRELIMINARY

No.	Issue	Checked	Approved	Date
Author	C. PHELPS	Drafting Check	C. OTTO	Project Manager
Designer	C. PHELPS	Design Check	S. GRUPICO	Project Director

Bar is one inch on original size sheet
0 1"

Bar is one inch on original size sheet
0 1"

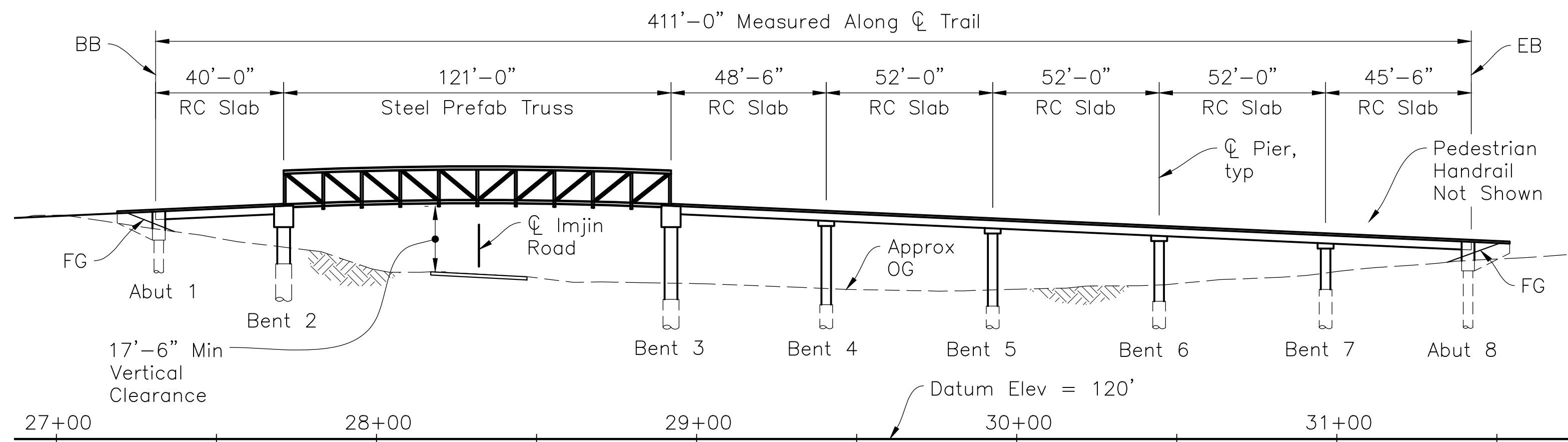
Bar is one inch on original size sheet
0 1"

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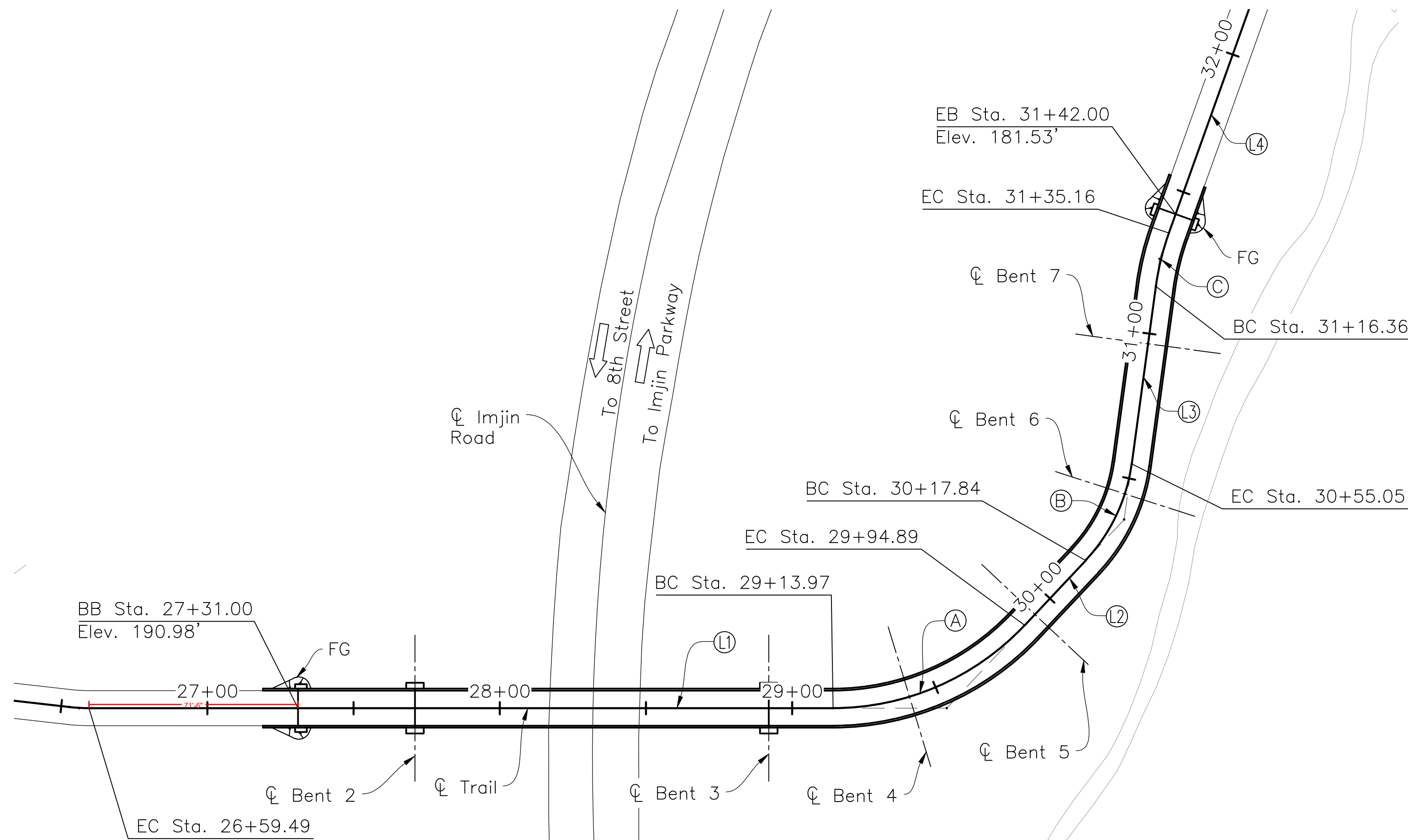
Client	FORTAG
Project	CALIFORNIA AVENUE SEGMENT FINAL DESIGN
Project No.	12603415
Date	MARCH 6, 2024
Scale	AS NOTED

Title	IMJIN RD PATHWAY FOOTPRINT SOUTH ALTERNATE 2 PLAN AND PROFILE
Sheet No.	FIG 2
Sheet	2 of 3

Alternative 4
Seven-Span Bridge (Steel Truss
W/ Concrete Approach Spans)



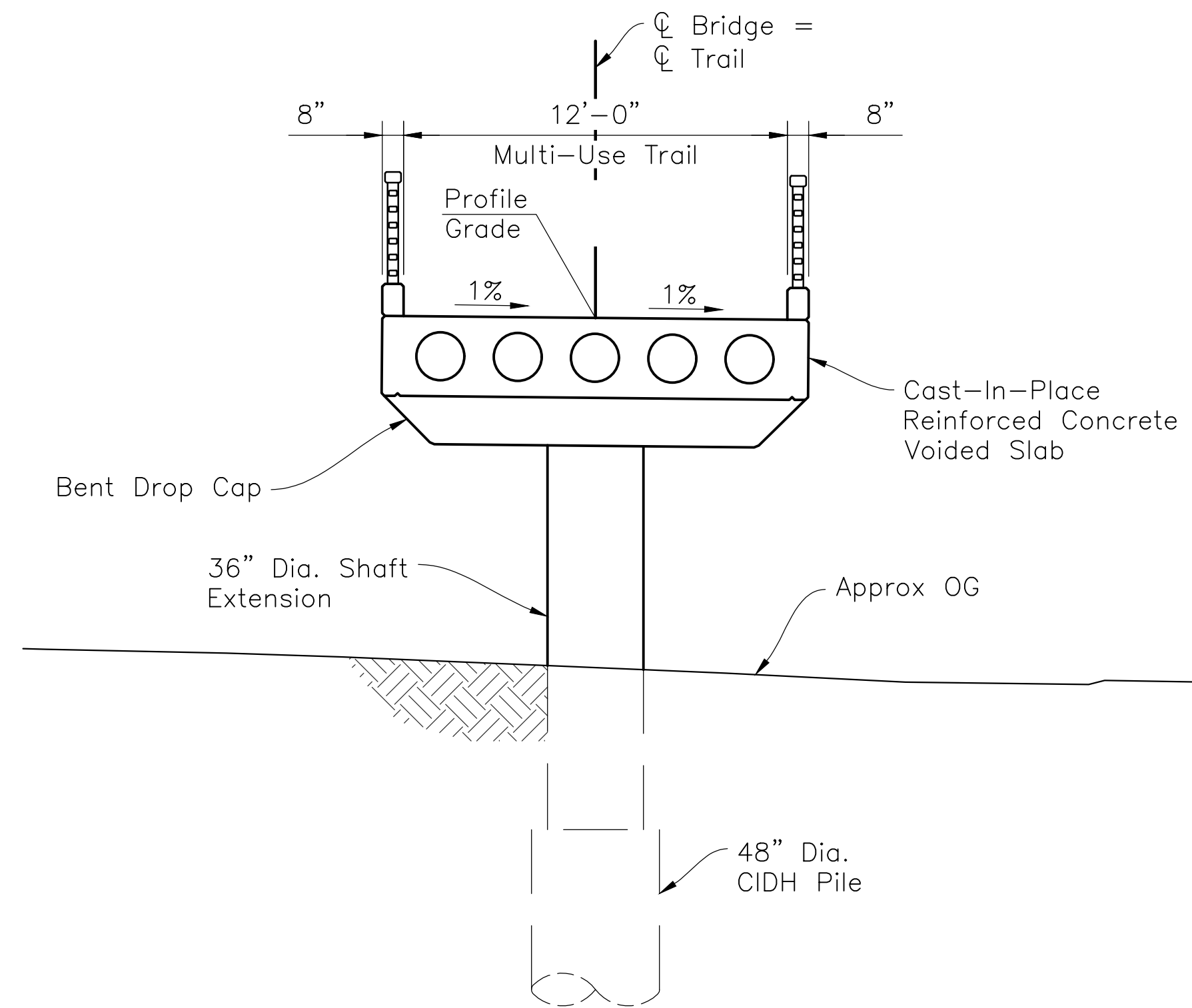
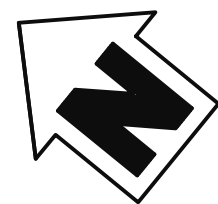
ELEVATION
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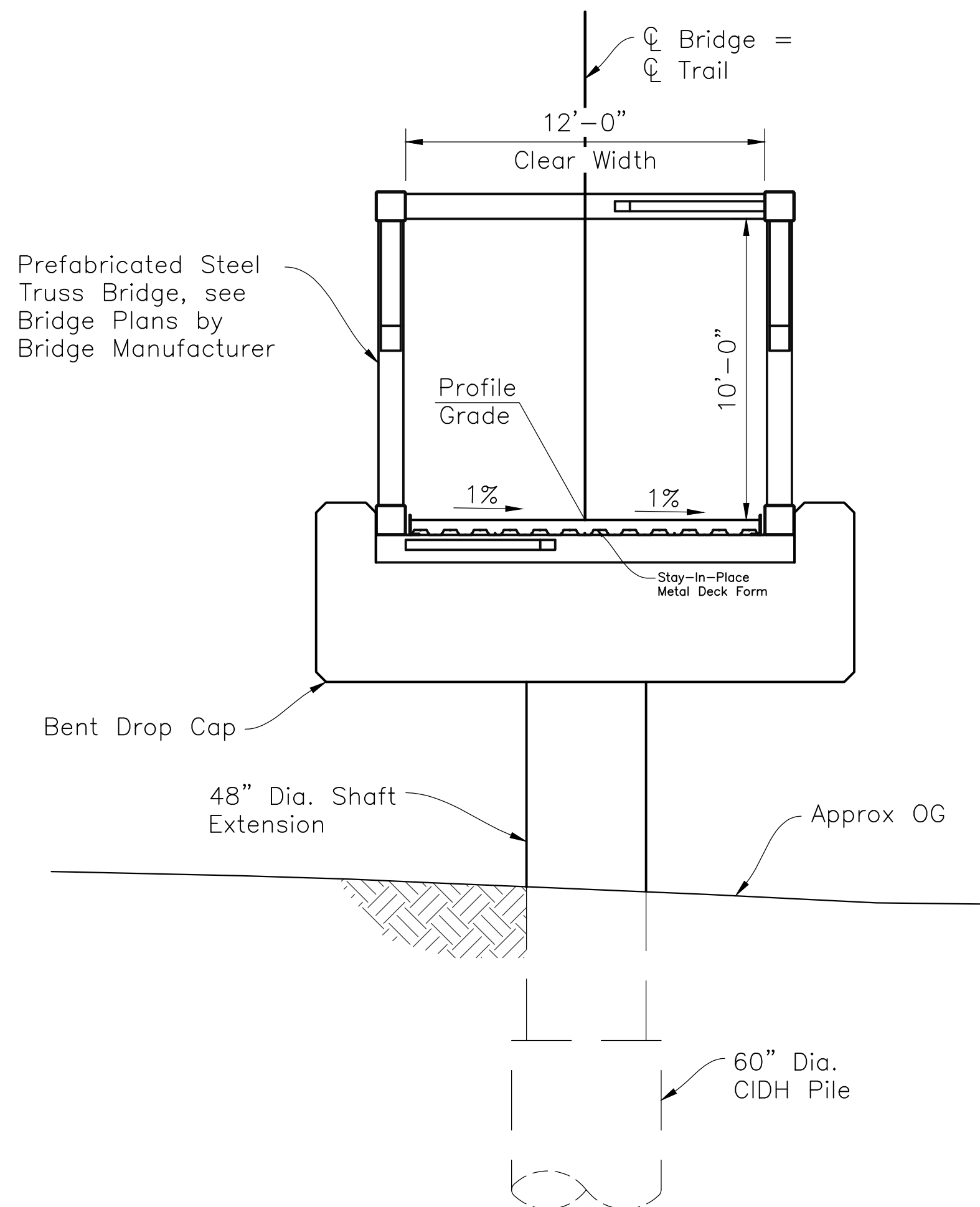
LINE DATA		
No.	BEARING	LENGTH
①	S39°15'30"E	254.47'
②	S85°59'42"E	22.95'
③	N58°28'23"E	61.31'
④	N70°26'20"E	160.75'

CURVE DATA			
No.	RADIUS	DELTA	LENGTH
①	90.00'	46°44'12"	80.92'
②	60.00'	35°31'55"	37.21'
③	90.00'	11°57'57"	18.80'

PLAN
1"=30'-0"

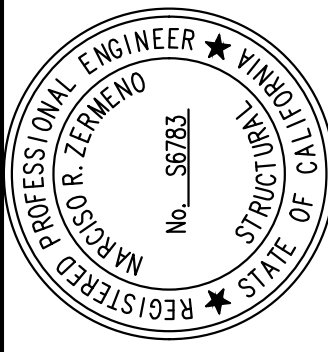


CONCRETE TYPICAL SECTION
1/4"=1'-0"



STEEL TYPICAL SECTION
1/4"=1'-0"

PLAN CHECK SET/NOT FOR CONSTRUCTION



DESIGNED BY:	NRZ
DRAWN BY:	DJF
CHECKED BY:	
DATE:	03/12/2024

REV.	DATE	DESCRIPTION

CORNERSTONE
structural engineering group

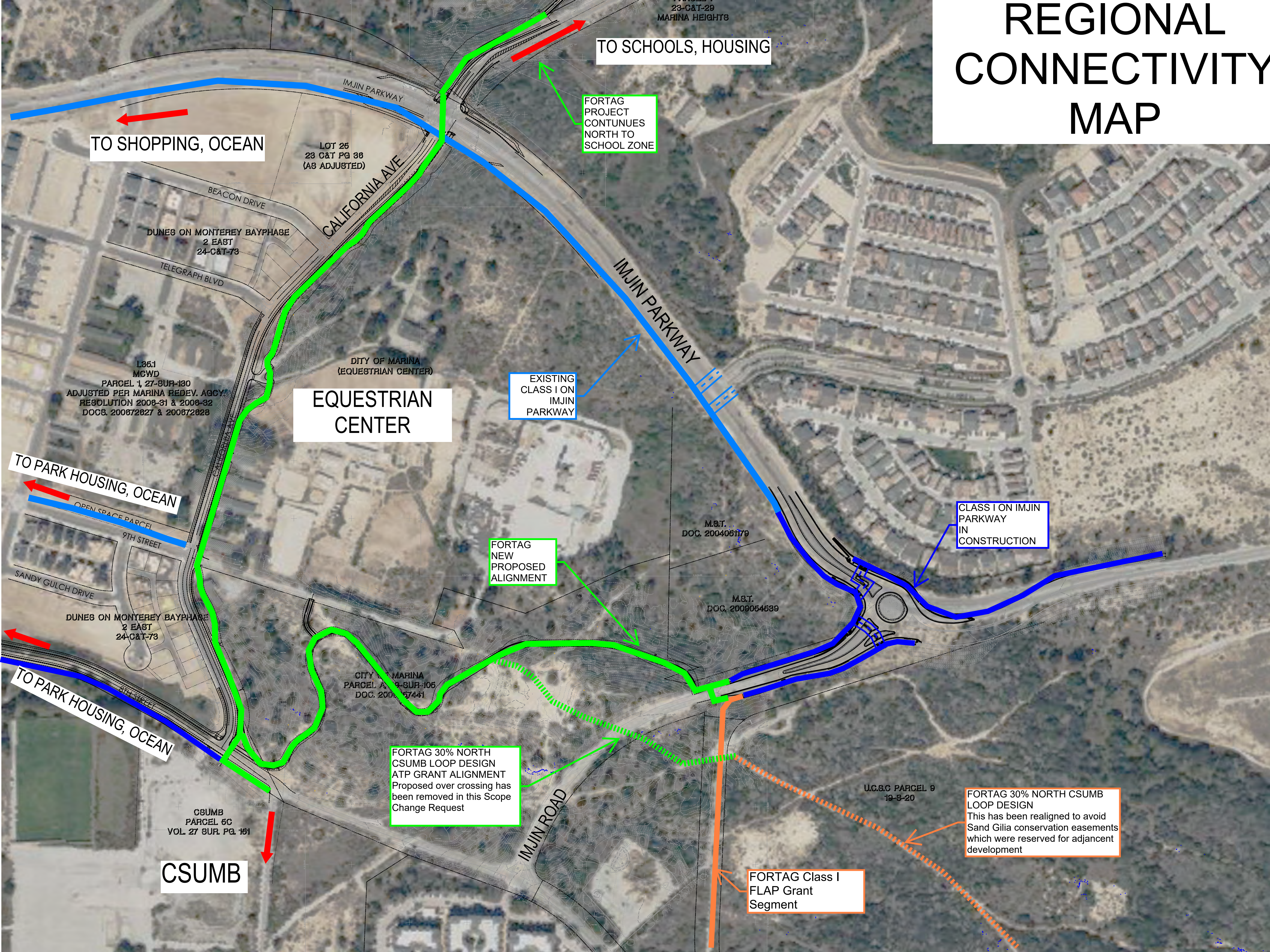
986 W Alluvial Ave - Suite 201
Fresno, California 93711
559.320.3200
fax 559.320.3201

GENERAL PLAN
FORTAG California Ave Segment
Over Imjin Road

California
Marina

SHEET NUMBER	S1
OF # SHEETS	4
General Plan	

REGIONAL CONNECTIVITY MAP



Technical Memorandum

October 17, 2024

To	Janneke Strauss, TAMC	Copy to	Michael Zeller, Laurie Williamson TAMC
From	Frank Penry, GHD	Project No.	12603415
Project Name	FORTAG CALIFORNIA AVE SGMT FINAL DESIGN AND ROW		
Subject	Imjin Road - At-Grade Pedestrian Crossing		

1. Introduction

GHD was retained by the Transportation Agency for Monterey County (TAMC) to complete the final design and right-of-way for the California Avenue segment of the Fort Ord Regional Trail and Greenway (FORTAG) project. FORTAG is proposed as a 12-foot wide continuously paved multiuse path that intends to connect Seaside, Marina, Del Rey Oaks, Monterey, and other unincorporated communities with California State University Monterey Bay, the Fort Ord National Monument, and the Monterey Bay Sanctuary Scenic Trail. This memorandum focuses specifically on the California Avenue segment in Marina which crosses Imjin Road just south of Imjin Parkway.

1.1 Purpose and Need

One of the values for FORTAG states that it should be 100% connected all the way around each loop. Initial plans at the Imjin Road crossing included a pedestrian overcrossing. After further review of the crossing location and cost estimates, it was determined that a pedestrian overcrossing facility would not be feasible at this location. To provide a safe and convenient connection across Imjin Road an at-grade pedestrian crossing is needed.

For the purposes of this study, pedestrian is used for all trail users crossing at the project location whether biking, walking, or rolling.

2. Existing Conditions

Imjin Road is an east-west local roadway that connects Imjin Parkway and 8th Street just north of California State University (CSU) Monterey Bay and spans approximately one-third mile. At the proposed crossing location, Imjin Road is a two-lane roadway with one 12-foot lane in each direction, a striped double-yellow centerline, and unpaved shoulders.

The posted speed limit on Imjin Road is 35mph. The City of Marina's 2018 Engineering and Traffic Survey did not cover Imjin Road, however the study indicates that the 85th percentile speed on similar roads in the vicinity

This Technical Memorandum is provided as an interim output under our agreement with . It is provided to foster discussion in relation to technical matters associated with the project and should not be relied upon in any way.

(such as California Avenue from Reindollar Avenue) is expected to be about 5-10 mph above the posted speed limit.

At the time of this study, a two-lane roundabout is under construction at the intersection of Imjin Road and Imjin Parkway as a part of the City of Marina's Imjin Parkway Expansion project. The project will construct bus stops on both the east and west-bound sides of Imjin Road near the proposed pedestrian crossing. The entry and exit curves for the roundabout end about 250' from the proposed pedestrian crossing on Imjin Road.

2.1 Sight Distance

For purposes of Sight Distance analysis under the prevailing speed and conditions, Stopping Sight Distance (SSD) analysis, Caltrans Highway Design Manual (HDM) Table 201.1, was used for required sight distance for the respective travel speeds in the vicinity of the proposed pedestrian crossing. These values include 250 feet for 35 mph (posted speed limit), 300 feet for 40 mph, and 360 feet for 45 mph and represent the distance required for a driver to see, react, and stop for an item or person in the roadway. As the posted speed limit on Imjin Road is 35 mph, the minimum sight distance at this location should be 250 feet. Figure 1 shows the limits of the minimum stopping sight distance in both directions from the crossing.

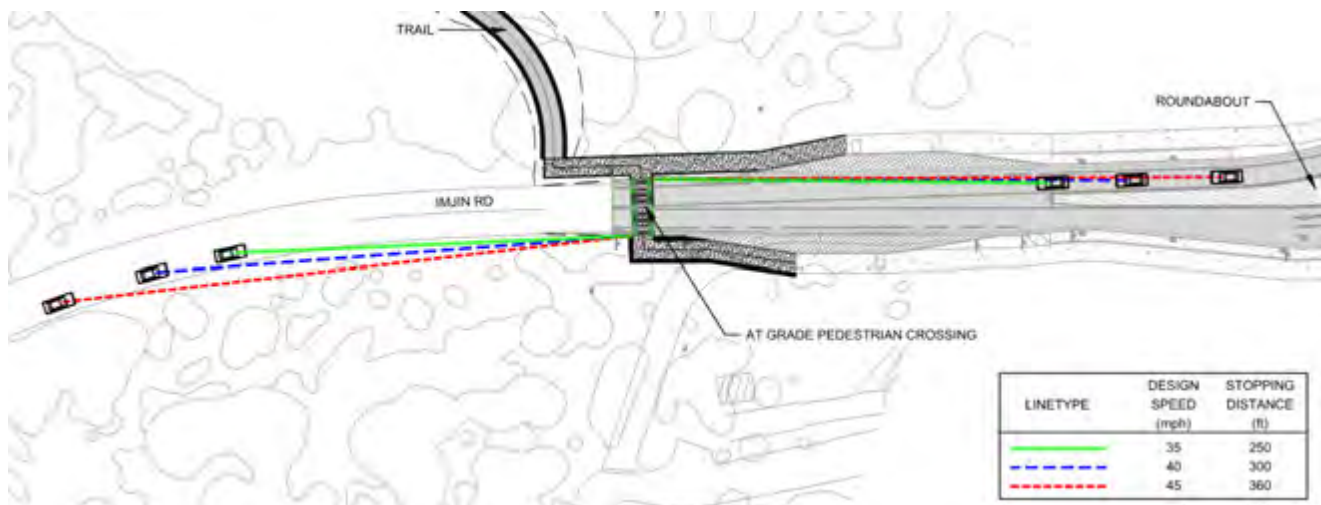


Figure 1 Minimum Stopping Sight Distance

As depicted in the figure, drivers traveling westbound from Imjin Parkway at the posted 35 mph speed limit or below will have adequate sight distance to the pedestrian crossing. Drivers traveling eastbound toward Imjin Parkway have more limited sight lines approaching the proposed crossing due to roadway curvature. However, the minimum sight distance is met when traveling at or below the posted speed limit. Considering that many drivers tend to travel over the speed limit, it should be noted that the sight distance will not be adequate in these scenarios.

3. Proposed Project – Pedestrian Hybrid Beacon

After reviewing the existing conditions and limitations of appropriate stopping sight distance of the proposed at-grade pedestrian crossing location, it was determined that a high visibility crossing or a rectangular rapid flashing beacon (RRFB) will not provide adequate warning protections for pedestrians and bicyclists crossing the roadway at this location. RRFBs and high visibility crossings are more effective in locations with clear sight lines to pedestrians and lower speeds. Although the posted speed limit is 35 mph and considered low enough for an RRFB, the sight lines to pedestrians waiting to cross are not adequate without the installation of curb bulbouts. Given the existing roadway width and the design of the new roundabout and bus stops to the east,

there would not be proper spacing to provide adequate curb bulb-outs. To ensure better protection for and visibility of the multimodal road users, a pedestrian hybrid beacon (PHB) was determined to be the best fit project for this crossing.

A pedestrian hybrid beacon is a specialized type of beacon used to warn and control traffic at an unsignalized location to assist pedestrians in crossing a street or highway at a marked crosswalk. Unlike a standard traffic signal, a pedestrian hybrid beacon lies dark while not in use and only illuminates when activated by a pedestrian. Pedestrian hybrid beacons are an intermediate option between a flashing beacon (i.e., a rectangular rapid flashing beacon (RRFB)) and a full traffic signal as they assign right of way and provide positive stop control. Additionally, PHBs allow motorists to proceed once pedestrians have cleared the crossing, therefore reducing vehicle delay in comparison to a standard traffic signal. Figure 2 shows the sequencing of the beacons when activated.

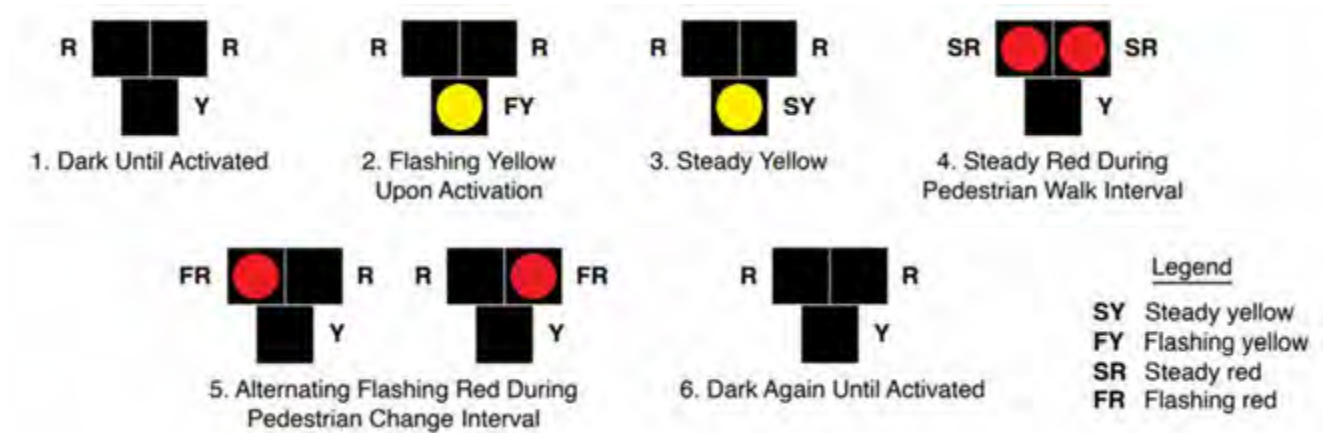


Figure 2 Sequence for a Pedestrian Hybrid Beacon

3.1 MUTCD Analysis

Chapter 4 of the latest edition of the California Manual on Uniform Traffic Control Devices (CA MUTCD) provides support, standards, options, and guidance for justifying the installation of a pedestrian hybrid beacon through the evaluation of warrants. Pedestrian hybrid beacons are specifically addressed in Chapter 4F where it states, "A pedestrian hybrid beacon may be considered for installation to facilitate pedestrian crossings at a location that does not meet traffic signal warrants (see Chapter 4C), or at a location that meets traffic signal warrants under Sections 4C.05 and/or 4C.06 but a decision is made to not install a traffic control signal."

3.1.1 Traffic Signal Warrant Analysis

For purposes of establishing whether a traffic signal is or is not warranted, below is a list of the traffic signal warrants found in Chapter 4C and whether the signal warrant is applicable or met at the project location.

- Warrant 1 – Eight Hour Vehicular Volume
 - **Not applicable**, no intersecting roadway/vehicles.
- Warrant 2 – Four Hour Vehicular Volume
 - **Not applicable**, no intersecting roadway/vehicles.
- Warrant 3 – Peak Hour
 - **Not applicable**, no intersecting roadway/vehicles.
- Warrant 4 – Pedestrian Volume
 - **Not applicable**, no pedestrian facilities currently connect to this location.

- Warrant 5 – School Crossing
 - **Not applicable**, schoolchildren (ages kindergarten to high school) crossing at this location is not the principal reason for installing the crossing.
- Warrant 6 – Coordinated Signal System
 - **Not applicable**, vehicle platooning not needed, Imjin Rd and Imjin Pkwy intersection being converted to roundabout.
- Warrant 7 – Crash Experience
 - **Not met**, no reported collisions at this location in the past five years that can be mitigated with a signal.
- Warrant 8 – Roadway Network
 - **Not applicable**, no intersecting roadway.
- Warrant 9 – Intersection Near a Grade Crossing
 - **Not applicable**, not located near a grade crossing.

3.1.2 Pedestrian Hybrid Beacon Analysis

As provided above, none of the traffic signal warrants are met. However, other than providing a baseline for the need of alternate traffic controls at the proposed location, a pedestrian hybrid beacon may still be considered for installation to facilitate pedestrian crossings. The following guidance for the installation of a pedestrian hybrid beacon is provided in Section 4F of the CA MUTCD. This guidance considers the crosswalk length, number of pedestrian crossings per hour, and total vehicles per hour along the roadway. The guidance, by way of the curves shown in Figures 4F-1 and 4F-2 in the CA MUTCD, are used for 85th percentile speeds either below or above 35 mph, respectively.

The proposed crossing is expected to be approximately 36 feet in crossing length and Imjin Road has volumes of approximately 500 total vehicles per hour. As shown in Figure 3, there would need to be approximately 250-300 pedestrians per hour to meet the warrant. While the trail is expected to draw many users, the trail may not see this many users for extended periods of time. However, it is possible the trail could see this many users at peak periods.

Figure 4F-2. Guidelines for the Installation of Pedestrian Hybrid Beacons on High-Speed Roadways

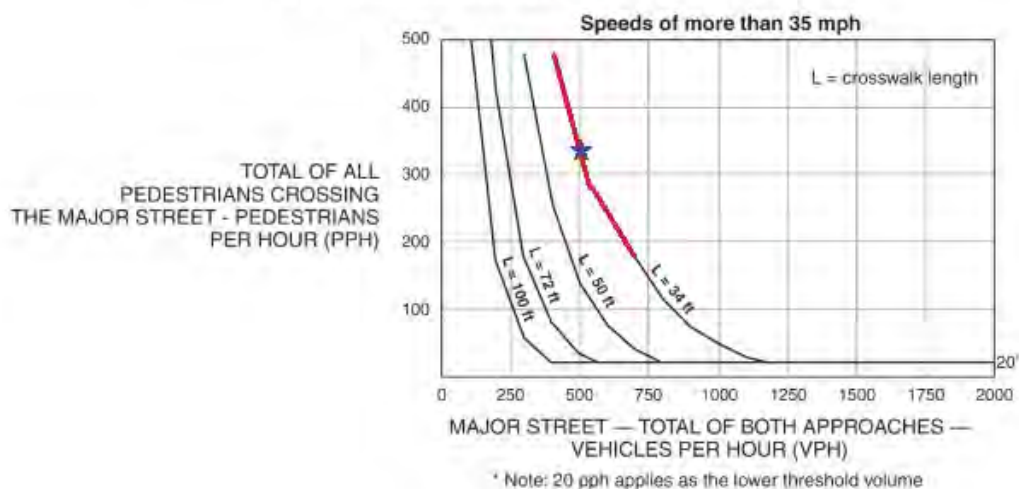


Figure 3 Pedestrian Hybrid Beacon Warrant

3.2 Engineering Judgement

As provided in the MUTCD, if the traffic control signal is not justified under the signal warrants of Chapter 4C and if gaps in traffic are not adequate to permit pedestrians to cross, or if the speed for vehicles approaching on the major street is too high to permit pedestrians to cross, or if pedestrian delay is excessive, the need for a pedestrian hybrid beacon should be considered on the basis of an engineering study that considers major-street volumes, speeds, widths, and gaps in conjunction with pedestrian volumes, walking speeds, and delay.

Given the roadway curvature and sight distance, particularly with the potential for higher than normal speeds along this stretch of roadway, a pedestrian hybrid beacon will provide better visibility for oncoming traffic as the mast arms span over the roadway and catch the driver's eye more easily than a RRFB on the shoulder. With the limited sight distance for eastbound vehicles, a PHB gives an earlier warning and will allow more space for a vehicle to begin braking. Additionally, PHBs provide drivers with clear indications of when to stop and when to cautiously proceed through the crossing that can reduce the possibility of vehicle-pedestrian collisions at this location where pedestrians have not been previously. Therefore, a pedestrian hybrid beacon should be installed at this location.

4. Recommendations

To provide pedestrians with a safe, visible crossing location, it is recommended that a Pedestrian Hybrid Beacon be installed at the proposed FORTAG crossing on Imjin Road. To enhance the PHB, an advanced warning flashing beacon should be considered in the eastbound direction due to the limited sight distance from the roadway curvature. Additionally, a speed table should be included as part of the crossing to change driver behavior through this area. A speed table will slow vehicles as they approach the new bus stops to the west of the proposed crossing providing increased protection for transit users at these stops as well as slow vehicles to a proper speed to navigate the new roundabout at the Imjin Road and Imjin Parkway intersection.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5h, Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **FISCAL YEAR 2025-26 ANNUAL ALLOCATION FOR LOCAL ASSISTANCE -
STATE FUNDS
RESOLUTION FM-24-02**

ISSUE:

Should the California Transportation Commission (Commission) approve the California Department of Transportation's (Department) request to allocate \$228,908,000 to administer the Department's Division of Local Assistance Fiscal Year 2025-26 State annual allocation subvention budget under the authority of the Commission?

RECOMMENDATION:

The Department recommends that the Commission approve the allocation of \$228,908,000 in State funds as an annual allocation for administering the Division of Local Assistance subvention budget for 2025-26, contingent upon passage of the Budget Act of 2025.

BACKGROUND:

The Department's Local Assistance Program administers the subvention budget under delegated authority from the Commission. The Department requests an annual allocation each year for these funds consistent with the Budget Act.

The guidelines for allocating, monitoring, and auditing of funds for Local Assistance projects are set forth in Commission Resolution G-99-25, which is based on Section 14529.1 of the California Government Code. Resolution G-01-08, approved by the Commission on February 22, 2001, delegates to the Department the authority to adjust allocations between expenditures and categories, and the Department reports to the Commission if transfers in or out of an expenditure category exceed 10 percent of its allocation.

FINANCIAL RESOLUTION:

Resolved, that \$228,908,000 be allocated from the Budget Act of 2025, Budget Act Item 2660-102-0042(1), contingent upon passage of the Budget Act of 2025.

**ANNUAL ALLOCATION
FISCAL YEAR 2025-26 FUNDS FOR LOCAL ASSISTANCE
(Dollars in Thousands)**

	State	Federal	Total
<u>2660-102-0042(1)</u>			
Surface Transportation Block Grant Program State Match and Exchange	\$75,682		\$75,682
Bridge Inspection & Seismic Retrofit State Match	497		497
Railroad Grade Separation	15,000		15,000
Railroad Grade Crossing Protection Maintenance¹	4,000		4,000
Miscellaneous Unassigned Local Programs	3,250		3,250
Freeway Service Patrol	25,479		25,479
Freeway Service Patrol – Senate Bill 1	25,000		25,000
Senate Bill 137 Exchange - HSIP	80,000		80,000
Total Local Programs	\$228,908		\$228,908

¹ Railroad Grade Crossing Protection Maintenance program was approved at the January 30-31, 2025, Commission meeting under Resolution FP-24-59.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5m. Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **FISCAL YEAR 2025-26 RIGHT OF WAY ANNUAL ALLOCATION
RESOLUTION FM-24-03**

ISSUE:

Should the California Transportation Commission (Commission) approve the annual allocation for Right of Way (RW) capital outlay for Fiscal Year 2025-26?

RECOMMENDATION:

The California Department of Transportation (Department) recommends the Commission approve Resolution FM-24-03 to allocate an annual allocation of \$125,000,000 for RW capital outlay funds for 2025-26, contingent upon passage of the Budget Act of 2025.

BACKGROUND:

Each year, the Department is required to obtain a RW Capital annual allocation from the Commission before it can deliver the RW needs for programmed projects in the Project Delivery Plan. The RW Capital annual allocation is used for State Highway Operation and Protection Program (SHOPP) and State Transportation Improvement Program (STIP) projects with RW Capital programming of \$10,000,000 or less. Projects with SHOPP or STIP RW Capital programming of \$10,000,000 or more will be individually allocated by the Commission. Additionally, the annual allocation is broken into the specific categories of costs during the Project Approval and Environmental Document (PA&ED) phase, costs exceeding programming, and inverse condemnation costs. The approval of this resolution will provide the Department the ability to deliver its RW commitments for 2025-26.

FISCAL YEAR (FY) 2025-26 RW CAPITAL ANNUAL ALLOCATION SUMMARY
(Dollars x 1,000)

Capital Projects	Total FY Annual Allocation Requested
STIP Capital Costs	\$6,912
STIP Costs During the PA&ED Phase ^{1 2}	\$10
STIP Costs Exceeding Programming ²	\$6,235
STIP Inverse Costs ²	\$1,001
Subtotal STIP	\$14,158
SHOPP Capital Costs	\$94,616
SHOPP Costs During the PA&ED Phase ^{1 2}	\$2,032
SHOPP Costs Exceeding Programming ²	\$12,829
SHOPP Inverse Costs ²	\$1,365
Subtotal SHOPP	\$110,842
Total Right of Way	\$125,000

Resolution G-19-01 requires the Department to present annually its RW Capital annual allocation plan to the Commission for review and acceptance. The Resolution states that the plan will be supported by a project list that includes at a minimum: district, county, route, expenditure authorization, planning programming number, project description, source programming document, year of RW Capital programming, total SHOPP and STIP programmed amount RW Capital, total SHOPP and STIP RW Capital commitments to date, and planned and actual RW Capital SHOPP and STIP expenditures for the fiscal year.

The 2025-26 RW Capital annual allocation plan is consistent with each fiscal year's Budget Act, as well as the SHOPP and STIP documents, approved by the Commission.

Consistent with Resolution G-19-01, the Department reviewed with Commission staff the proposed list of projects that comprise the requested 2025-26 RW Capital annual allocation. The Department will report to the Commission on a quarterly basis the status of the project list, including any projects added, and the balance of the annual allocation.

The chart below breaks down the RW Capital annual allocation by fund source and state budget fiscal year appropriation.

¹ Commitments made during the PA&ED phase are limited to 5 percent of the total annual allocation. It is estimated these costs will total approximately 1.6 percent of the total allocation.

² The proposed 2026 STIP Fund Estimate identifies a \$38 million fund reservation for unprogrammed RW Capital costs. The estimated commitments for 2025-26 stay within the fund reservation amount after STIP share adjustments are made at construction contract acceptance.

FINANCIAL RESOLUTION:

Resolved, that \$125,000,000 be allocated from the Budget Act of 2025, Budget Act Items 2660-301-0042, 2660-301-0890, 2660-302-0042, and 2660-302-0890 for the 2025-26 RW Capital Annual Allocation.

Program	Fund Source	FY 2025-26 Budget Authority (in thousands)
STIP	SHA-26301-0042	\$10,097
STIP	Federal-26301F-0890	\$4,061
	Subtotal STIP	\$14,158
SHOPP	SHA-26302-0042	\$99,589
SHOPP	Federal-26302F-0890	\$11,253
	Subtotal SHOPP	\$110,842
	Total	\$125,000

A copy of the detailed [RW Capital Plan](#) is available online.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5n., Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **FISCAL YEAR 2025-26 STATE HIGHWAY OPERATION AND PROTECTION
PROGRAM MINOR CONSTRUCTION PROGRAM ANNUAL ALLOCATION
RESOLUTION FM-24-04**

ISSUE:

Should the California Transportation Commission (Commission) approve Resolution FM-24-04 allocating an annual allocation amount of \$250,000,000 for the 2025-26 State Highway Operation and Protection Program (SHOPP) Minor Program?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve Resolution FM-24-04 allocating an annual allocation amount of \$250,000,000 (\$152,324,000 for construction capital and \$97,676,000 for capital outlay support) for the 2025-26 SHOPP Minor Program.

The 2025-26 annual allocation of \$250,000,000 amount is distributed as follows:

Construction Capital	
Minor A Projects	\$62,701,000
Minor B	\$56,414,000
Minor B Reservation	\$29,364,000
Right of Way	\$2,770,000
Day Labor	\$1,075,000
Capital Outlay Support	\$97,676,000
Total	\$250,000,000

(NOTE: A portion of the Minor B amount for each District is used at the District's discretion for unforeseen immediate needs and/or emergency project(s) that are unknown at this time.)

BACKGROUND:

At its October 2019 meeting, the Commission passed Resolution G-19-05a increasing the Minor Program to \$250,000,000, and delegating to the Department the sub-allocation and adjustment authority for minor capital construction projects. The resolution also requires the Department to present its Minor Program annually to the Commission for review and approval. In addition, the resolution states that the following project identifiers be included for each project allocations in the upcoming fiscal year:

- Project description, location, expenditure authorization number and performance measures.
- Program sum for day labor (material acquisitions only), right of way capital, construction capital and construction support.
- An annual allocation amount for the construction capital and support costs and right of way capital of Minor B projects.

In accordance with Resolution G-19-05a, the Department, in consultation with Commission staff, has reviewed the Minor Program project lists as shown on each of the attachments.

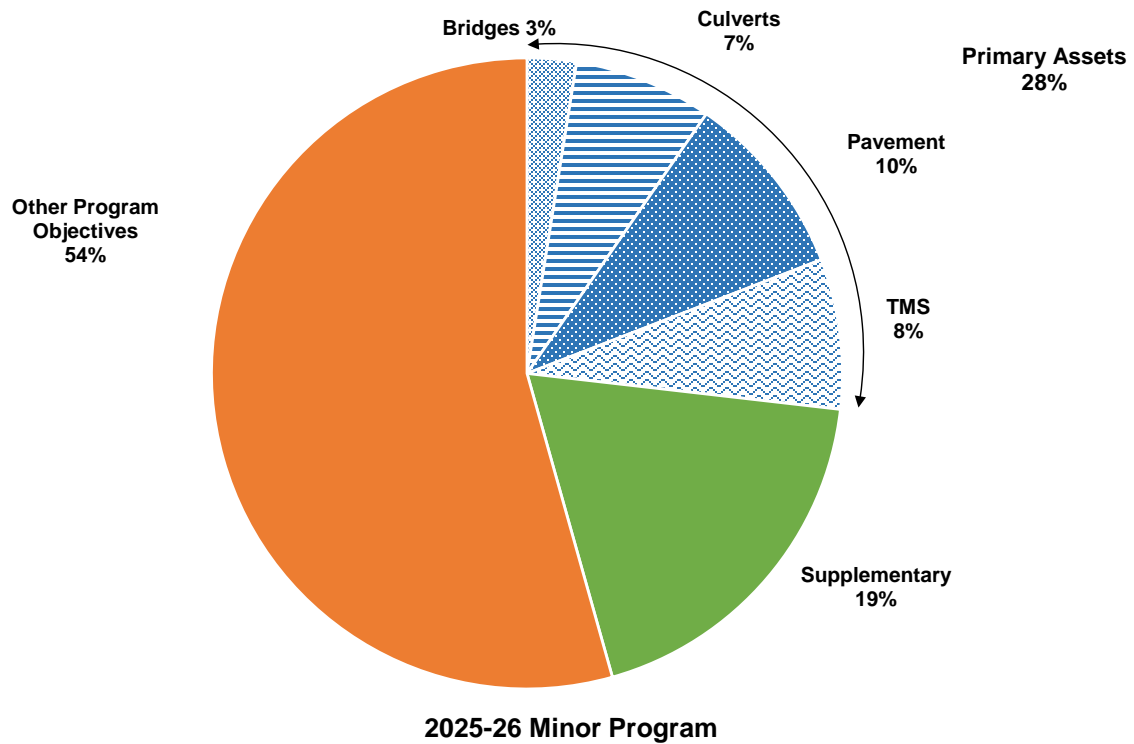
FINANCIAL RESOLUTION:

Resolved, that the amount of \$152,324,000 for construction be allocated from the Budget Act of 2025, Budget Act Items 2660-302-0042, 2660-302-0890 and 2660-303-0042, and \$97,676,000 for capital outlay support, as a lump sum amount for the Minor Program for 2025-26, contingent upon the passage of the Budget Act of 2025.

Attachments

**2025-26 STATE HIGHWAY OPERATION AND PROTECTION PROGRAM (SHOPP)
MINOR CONSTRUCTION PROGRAM
ANNUAL ALLOCATION**

Asset Class	Minor A		Minor B		Grand Total	
	Construction (Life of Project) (x\$1,000)	% of Total Construction	Construction (Life of Project) (x\$1,000)	% of Total Construction	Construction (Life of Project) (x\$1,000)	% of Total Construction
Bridges	\$1,830	3%	\$1,170	2%	\$3,000	3%
Culverts	\$3,219	5%	\$5,247	9%	\$8,466	7%
Pavement	\$2,950	5%	\$8,368	15%	\$11,318	10%
TMS	\$2,050	3%	\$7,112	13%	\$9,162	8%
Supplementary	\$11,365	18%	\$11,050	20%	\$22,415	19%
Other Program Objectives	\$41,287	66%	\$23,467	42%	\$64,754	54%
Grand Total	\$62,701	100%	\$56,414	100%	\$119,115	100%



2025-26 Minor A Project List
Districts 01-12
(x\$1,000)

No.	District	County	Route	Postmiles	Project Location/Description	EA	Project ID	Program Code	Activity Category	Construction (Life of Project)	Right of Way (Life of Project)	Support (Life of Project)	Total (Life of Project)	Performance Value	Performance Measure
1	01	HUM	96	R41.5/R41.9	Near Orleans, at 2.0 miles to 2.4 miles north of Peach Creek No. 04-0240. Install guardrail and perform vegetation control.	0N640	0124000087	201.015	Other Program Objectives	\$753	\$0	\$1,731	\$2,484	0.01	Annual fatal and serious injury collision(s)
2	01	MEN	1	59.7	In Fort Bragg, at Hare Creek Bridge No. 10-0175. Install new culvert.	0N030	0123000119	201.335	Other Program Objectives	\$1,076	\$225	\$2,744	\$4,045	3.3	Acre(s) treated/pollutant
3	01	HUM	101	77.3	In Eureka, at West Wabash Avenue. Add new parking spaces. Financial Contribution Only to the city of Eureka.	0J000	0118000145	201.353	Supplementary	\$1,250	\$384	\$2,406	\$4,040	1.0	Location(s)
4	02	SHA	299	18.6/19.0	In the city of Shasta, from 0.1 mile east of Second Street to Middle Creek Road. Construct Class 1 bike trail.	2H600	0216000150	201.400	Other Program Objectives	\$1,210	\$150	\$2,028	\$3,388	2,107.0	Bicycle and pedestrian infrastructure (linear feet)
5	02	TEH	273	14.47/15.0	In Redding, from 0.2 mile south of Wyndham Lane to Wyndham Lane. Install safety lighting. Install safety lighting.	0K800	0224000002	201.015	Other Program Objectives	\$900	\$0	\$1,182	\$2,082	0.01	Annual fatal and serious injury collision(s)
6	02	SHA	96	26.05	Near Cottage Grove, at various locations. Repair and replace culverts.	1K810	0224000168	201.151	Culvert(s)	\$969	\$71	\$893	\$1,933	5.0	Culvert(s) (ea)
7	02	SIS	36	16.1/16.4	Near Rosewood, from 7.1 miles west to 6.7 miles west of Bowman Road. Curve improvements.	0K750	0223000201	201.015	Other Program Objectives	\$1,250	\$162	\$1,000	\$2,412	0.02	Annual fatal and serious injury collision(s)
8	02	SHA	5	R16.7/R17.24	In Redding, from 0.8 mile north of Black Butte Overhead Bridge No. 02-0204 to 0.2 mile south of South Weed Undercrossing Bridge No. 02-0162. Remove and relocate rock to improve safety.	1K870	0224000175	201.015	Other Program Objectives	\$1,200	\$0	\$300	\$1,500	0.01	Annual fatal and serious injury collision(s)
9	03	SUT	50	71.0	Near Meyers, at Apache Avenue. Relocate mid-block crossing and install Rectangular Rapid Flash Beacons (RRFB).	0N910	0324000222	201.310	Other Program Objectives	\$800	\$50	\$524	\$1,374	1.0	Daily Person Hours of Delay (DPHD)
10	03	YUB	5	16.147	In the city of Sacramento, at the Pocket Road Overcrossing No. 24-0263. Pavement preservation.	0N820	0324000209	201.121	Pavement	\$600	\$0	\$250	\$850	0.0	Lane mile(s)
11	03	ED	20	1.1/1.7	In Marysville, at the intersection of 9th Street and B Street. Modify intersection.	3J960	0323000041	201.310	Other Program Objectives	\$1,100	\$0	\$880	\$1,980	70.0	Daily vehicle hour(s) of delay (DVHD)
12	03	NEV	80	15.661	In Truckee, 0.2 mile east of Bridge Street. Connect existing wash rack to drain to municipal sewer system.	4J550	0323000204	201.352	Supplementary	\$905	\$0	\$480	\$1,385	1.0	Location(s)
13	03	SAC	99	40.25	In Live Oak, at Pennington Road. Upgrade two-wire railroad preemption.	0J230	0319000243	201.310	Other Program Objectives	\$1,250	\$125	\$850	\$2,225	1.0	Daily vehicle hour(s) of delay (DVHD)
14	03	SAC	51	1.619/1.648	In the city of Sacramento, near B Street Underpass and from 0.2 miles west of Marconi Avenue to Fulton Avenue. Install new Closed-Circuit Television (CCTV) pole.	0N730	0324000176	201.315	TMS	\$1,000	\$0	\$600	\$1,600	3.0	Field element(s)
15	03	ED	50	48.7	Near Kyburz, 0.2 mile east of Kyburz Drive. Replacement of hydronic heating concrete pad.	4J430	0323000146	201.352	Supplementary	\$1,250	\$0	\$655	\$1,905	1.0	Location(s)
16	03	SIE	89	6.2/6.4	Near Sierraville, from 0.3 mile to 0.1 mile east of Cottonwood Road. Curve correction and install curve warning signs.	1N420	0325000024	201.015	Other Program Objectives	\$1,150	\$0	\$460	\$1,610	0.01	Annual fatal and serious injury collision(s)
17	03	YUB	20	1.24	In Marysville, at the District 3 Main Office at 703 B Street. Expand northside parking lot.	1N700	0325000086	201.352	Supplementary	\$1,250	\$0	\$500	\$1,750	0.0	Location(s)
18	03	ED	49	11.65	Near Diamond Springs, at Diamond Meadows Way. Enhance crosswalk and install Rectangular Rapid Flashing Beacons (RRFBs), additional intersection lighting, and Americans with Disabilities Act (ADA) curb ramp bulbouts.	1N980	0325000180	201.015	Other Program Objectives	\$695	\$0	\$417	\$1,112	0.03	Annual fatal and serious injury collision(s)
19	04	MRN	1	0.0/50.5	Near Stinson Beach and Marshall Beach, from Route 101 to Valley Ford Road at various locations. Install Rectangular Rapid Flash Beacons (RRFBs), curve warning signs, and speed feedback signs.	1X310	0424000307	201.015	Other Program Objectives	\$1,250	\$20	\$1,500	\$2,770	0.03	Annual fatal and serious injury collision(s)
20	04	SCL	101	31.7	In San Jose, at the Capitol Expressway Overcrossing No. 37-0218. Repair drainage, repair sinkhole, and install/improve underdrain.	3X690	0425000311	201.131	Other Program Objectives	\$1,250	\$20	\$640	\$1,910	2.0	Location(s)
21	04	SON	12	35.7	Near the city of Sonoma, at 0.1 mile north of Donald Street. Install Rectangular Rapid Flashing Beacons (RRFBs) and Americans with Disabilities Act (ADA) curb ramps.	3X710	0425000314	201.010	Other Program Objectives	\$840	\$5	\$201	\$1,046	0.05	Annual fatal and serious injury collision(s)

2025-26 Minor A Project List
Districts 01-12
(x\$1,000)

No.	District	County	Route	Postmiles	Project Location/Description	EA	Project ID	Program Code	Activity Category	Construction (Life of Project)	Right of Way (Life of Project)	Support (Life of Project)	Total (Life of Project)	Performance Value	Performance Measure
22	04	MRN	101	15.9/16.1	Near Novato, from 0.3 mile to 0.5 mile north of Miller Creek Road. Install High Friction Surface Treatment (HFST).	3X720	0425000315	201.010	Other Program Objectives	\$1,250	\$5	\$128	\$1,383	0.50	Annual fatal and serious injury collision(s)
23	05	MON	1	17.8	In the city of San Luis Obispo, at Highland Drive. Modify signals, install signs, and modify lighting standards.	1R440	0523000294	201.015	Other Program Objectives	\$1,250	\$0	\$1,950	\$3,200	0.02	Annual fatal and serious injury collision(s)
24	05	SCR	1	27.76/70.87	Near Big Creek Cove, from 0.3 mile south of Landels Hill Big Creek Reserve to 0.3 mile north of Riley Ranch Road. Rehabilitate drainage systems.	1N360	0521000006	201.151	Culvert(s)	\$1,250	\$40	\$3,200	\$4,490	6.0	Culvert(s) (ea)
25	05	MON	236	4.27	Near Boulder Creek, at Boulder Creek No. 36-0006. Bridge scour mitigation.	1P240	0522000004	201.111	Bridge	\$1,250	\$10	\$2,900	\$4,160	1.0	Bridge(s)
26	05	SLO	1	0.0/54.9	Near Big Sur, from the San Luis Obispo County line to 1.06 miles north of the Point Sur Naval Facility. Install signs.	1P260	0522000011	201.015	Other Program Objectives	\$600	\$0	\$1,500	\$2,100	0.37	Annual fatal and serious injury collision(s)
27	06	KIN	65	R0.0/0.8	Near Bakersfield, from 0.1 south to 0.1 north of Merle Haggard Drive; also on Route 99, from 1.01 miles north of Knudsen Drive to the Porterville Highway. Rehabilitate pavement and separation ramps.	1F450	0623000004	201.121	Pavement	\$1,100	\$22	\$1,600	\$2,722	0.0	Lane mile(s)
28	06	TUL	5	7.86/8.08	Near Coalinga, at Arroyo Pasajero Bridge No. 42-0412. Highway planting.	1C860	0621000188	201.210	Other Program Objectives	\$1,050	\$7	\$1,830	\$2,887	1.6	Acre(s)
29	06	FRE	43	2.2/2.3	In Corcoran, at the Sweet Canal. Install barrier.	1C340	0621000075	201.015	Other Program Objectives	\$1,100	\$5	\$1,270	\$2,375	0.01	Annual fatal and serious injury collision(s)
30	06	KER	190	20.9/21.2	Near Porterville, from 0.2 mile west to 0.1 mile east of Road 284. Rehabilitate roundabout.	1C710	0621000137	201.150	Other Program Objectives	\$1,218	\$180	\$3,040	\$4,438	1.0	Location(s)
31	06	TUL	58	T52.2/R52.4	Near Bakersfield, at the Route 58/99 Separation. Hardscape landscaping. Financial Contribution Only to the city of Bakersfield.	1G561	0625000046	201.210	Other Program Objectives	\$1,250	\$0	\$20	\$1,270	4.2	Acre(s)
32	06	KER	41	R26.46	In Fresno, at East Shields Avenue Undercrossing No. 42-0297/L/R. Construct Americans with Disabilities Act (ADA) compliant curb ramps and sidewalk. Financial Contribution Only to the city of Fresno.	1C041	0625000080	201.361	Supplementary	\$1,250	\$0	\$10	\$1,260	5.0	Curb ramp(s)
33	06	FRE	99	12.6/12.8	Near Pixley, from East Davis Street Overcrossing No. 46-0172 to East Court Avenue Overcrossing No. 46-0173. Upgrade bridge rail to improve sight distance.	1F750	0623000074	201.112	Bridge	\$580	\$7	\$1,146	\$1,733	0.01	Annual fatal and serious injury collision(s)
34	07	LA	5	35.0	In the city of Los Angeles, at Lankershim Boulevard Overcrossing. Repair slope erosion.	1Q960	0724000217	201.131	Other Program Objectives	\$950	\$0	\$550	\$1,500	1.0	Location(s)
35	07	LA	405	22.5/23.4	In Inglewood, 0.3 mile north of West Century Boulevard to W Manchester Boulevard. Install caps to existing abandoned sewer lines.	4V560	0725000040	201.999	Other Program Objectives	\$550	\$0	\$480	\$1,030	0.0	Centerline mile(s)
36	07	LA	405	29.94	In the city of Los Angeles, at the Sawtelle Maintenance Station (L5709) at 2532 Sawtelle Boulevard. Upgrade the facility to ensure compliance with Americans with Disabilities Act (ADA).	4V450	0722000220	201.352	Supplementary	\$1,250	\$0	\$950	\$2,200	1.0	Location(s)
37	07	LA	2	14.0/14.8	In the city of Los Angeles, from Clifford Street to 0.1 west mile of Rosebud Avenue. Install transverse sinusoidal rumble strips, a traffic ramp meter signal system, and install/upgrade signs.	4V570	0725000053	201.015	Other Program Objectives	\$1,250	\$20	\$600	\$1,870	0.43	Annual fatal and serious injury collision(s)
38	07	LA	10	43.006	In Pomona, 0.2 mile west of Route 57 connector exit. Construct drainage system.	4V580	0725000273	201.151	Culvert(s)	\$1,000	\$0	\$500	\$1,500	1.0	Culvert(s) (ea)
39	08	SBD	138	31.9/33.3	Near Crestline, from 0.5 miles east of Burnt Mill Road to 0.3 miles east of Old Mill Road. Regrade inside curve of the embankments on all four locations.	1P220	0823000110	201.015	Other Program Objectives	\$1,010	\$10	\$720	\$1,740	0.08	Annual fatal and serious injury collision(s)
40	08	SBD	74	42.8/43.1	In East Hemet, from Stanford Street to South Meridian Street. Correction of driveways and sidewalks to meet Americans with Disabilities Act (ADA) standards.	1P850	0824000133	201.361	Supplementary	\$1,010	\$75	\$960	\$2,045	0.0	Curb ramp(s)
41	08	RIV	58	R0.0/R33.0	Near Barstow, from the San Bernardino County line to Agate Road. Upgrade existing traffic management systems to fiber optic cables.	1P370	0824000204	201.315	TMS	\$1,050	\$10	\$820	\$1,880	1.1	Mile(s) of cable
42	08	SBD	138	R15.264	In San Bernardino, at the intersection of Routes 138 and 15 on the northbound onramp/offramp. Install traffic signal.	1P990	0824000205	201.310	Other Program Objectives	\$1,150	\$10	\$810	\$1,970	0.0	Daily Person Hours of Delay (DPHD)

**2025-26 Minor A Project List
Districts 01-12
(x\$1,000)**

No.	District	County	Route	Postmiles	Project Location/Description	EA	Project ID	Program Code	Activity Category	Construction (Life of Project)	Right of Way (Life of Project)	Support (Life of Project)	Total (Life of Project)	Performance Value	Performance Measure
43	08	SBD	18	32.9	In San Bernardino County, on Route 18 at various locations. Install barrier.	1P590	0824000006	201.015	Other Program Objectives	\$1,150	\$32	\$940	\$2,122	0.04	Annual fatal and serious injury collision(s)
44	08	SBD	210	R32.839	In Redlands, at the Lugonia Avenue Undercrossing No. 54-0931R. Construct sidewalk to meet Americans with Disabilities Act (ADA) standards.	1R400	0825000058	201.361	Supplementary	\$1,140	\$120	\$875	\$2,135	2.0	Curb ramp(s)
45	08	RIV	15	4.15/5.2	In Temecula, from 0.8 mile south to 0.2 mile north of Rancho California Overcrossing No. 56-0655. Replace highway planting, repair/replace irrigation system, and provide suitable erosion control.	1R750	0825000108	201.210	Other Program Objectives	\$1,230	\$10	\$1,030	\$2,270	3.2	Acre(s)
46	08	RIV	15	42.4/46.1	Near Corona, from the Corona Avenue Undercrossing No. 56-0697 to 0.2 mile north of the Detroit Street Overcrossing No. 56-0698. Rehabilitate highway planting and irrigation systems.	1R740	0825000109	201.210	Other Program Objectives	\$1,250	\$10	\$1,030	\$2,290	3	Acre(s)
47	08	RIV	15	R0.6/R1.4	Near Rainbow, from 0.6 mile to 1.4 miles north of the Riverside County line. Replace changeable message signs, open/close signs, underground cables, electronic equipment, and vehicle detection sensors.	1R780	0825000115	201.321	Supplementary	\$1,060	\$20	\$930	\$2,010	1.0	Location(s)
48	08	SBD	215	4.0	In Colton, 0.2 mile south of Route 10/215 Separation. Install a new decanting site.	1R800	0825000134	201.335	Other Program Objectives	\$1,128	\$10	\$1,010	\$2,148	0.0	Acre(s) treated/pollutant
49	09	MNO	395	66.0/68.0	In Lee Vining, from 4.5 miles north of Virginia Lakes Road to 1.9 miles south of Clearwater Creek. Widen unpaved shoulder and flatten slopes.	38270	0919000045	201.015	Other Program Objectives	\$1,250	\$0	\$400	\$1,650	0.02	Annual fatal and serious injury collision(s)
50	09	INY	395	115.019	In Bishop, at East Jay Street at the Caltrans District Office at 500 S Main Street. Replace main and supply waterlines.	39860	0923000036	201.353	Supplementary	\$1,000	\$20	\$520	\$1,540	1.0	Location(s)
51	09	KER	58	R94.2/R94.5	Near Tehachapi, at the Summit Overhead. Grind and overlay ramps with Hot Mix Asphalt (HMA).	42860	0925000087	201.121	Pavement	\$1,250	\$0	\$200	\$1,450	0.0	Lane mile(s)
52	10	CAL	4	R21.69/R21.82	Near Angels Camp, from 0.2 mile east of Gardner Lane to 0.1 mile west of Murphys Grade Road. Construct slow vehicle turnout.	1L800	1020000061	201.310	Other Program Objectives	\$1,100	\$0	\$1,620	\$2,720	50.6	Daily Person Hours of Delay (DPHD)
53	10	CAL	99	0.762/31.75	In San Joaquin, Stanislaus and Merced Counties, on Routes 4, 5, and 99 at various locations. Upgrade existing Remote Irrigation Control System communication components.	1Q640	1023000048	201.210	Other Program Objectives	\$1,157	\$0	\$1,378	\$2,535	0.0	Acre(s)
54	10	SJ	89	15.3/R20.1	Near Markleeville, from 0.5 mile north of Montgomery Street to Manzanita Lane. Remove and replace right-of-way security fencing.	1R340	1023000132	201.235	Other Program Objectives	\$1,170	\$0	\$1,895	\$3,065	0.01	Annual fatal and serious injury collision(s)
55	10	ALP	4	29.2/29.4	Near Angels Camp, from 0.06 east of Allen Lane to 0.04 west of Mitchler Street. Construct new sidewalk, curb ramps, and sidewalks, and upgrade curb ramps to Americans with Disabilities Act (ADA) standards.	1Q380	1023000008	201.400	Other Program Objectives	\$1,250	\$230	\$2,946	\$4,426	1,220.0	Bicycle and pedestrian infrastructure (linear feet)
56	12	ORA	90	0.5/12.8	In La Habra, Fullerton, Brea, Yorba Linda, and Anaheim, on Route 90 between Route 39 and Route 91. Replace existing traffic signal heads with yellow retroreflective plates.	0V660	1225000054	201.015	Other Program Objectives	\$1,000	\$0	\$300	\$1,300	0.33	Annual fatal and serious injury collision(s)
57	12	ORA	57	20.3/21.6	In Brea, at East Birch Street to 0.2 mile south of Tonner Canyon Road. Remove temporary concrete railing and replace with a concrete barrier, and upgrade crosswalk markings.	0V740	1225000069	201.015	Other Program Objectives	\$1,000	\$0	\$320	\$1,320	0.05	Annual fatal and serious injury collision(s)
58	12	ORA	73	23.8/R25.5	In Newport Beach and Irvine, northbound between McArthur Boulevard Overcrossing and at Campus Drive. Upgrade existing guardrail to current standards.	0V670	1225000060	201.015	Other Program Objectives	\$1,250	\$0	\$488	\$1,738	0.05	Annual fatal and serious injury collision(s)
Total										\$62,701	\$2,065	\$61,107	\$125,873		

2025-26 Minor B Project List
Districts 01-12
(x\$1,000)

No.	District	County	Route	Postmiles	Project Location/Description	EA	Project ID	Program Code	Activity Category	Construction (Life of Project)	Right of Way (Life of Project)	Support (Life of Project)	Total (Life of Project)	Performance Value	Performance Measure
1	01	HUM	101	R53.4/M53.9	In Rio Dell, from Scenic Way Undercrossing Bridge No. 04-0235L to Eel River Bridge No. 04-0016. Install pedestrian exclusion fence.	0P390	0125000001	201.015	Other Program Objectives	\$450	\$5	\$250	\$705	0.03	Annual fatal and serious injury collision(s)
2	01	HUM	101	R91.4/R91.5	Near McKinleyville, at School Road Overcrossing Bridge No. 04-0168. Provide multimodal facilities at interchange.	0P220	0124000205	201.400	Other Program Objectives	\$425	\$0	\$450	\$875	480.00	Bicycle and pedestrian infrastructure (linear feet)
3	01	HUM	101	R93.8/R93.9	Near McKinleyville, at Airport Road Undercrossing Bridge No. 04-0169L. Install sidewalk and crosswalk.	0P120	0124000175	201.400	Other Program Objectives	\$460	\$0	\$300	\$760	1,523.00	Bicycle and pedestrian infrastructure (linear feet)
4	01	HUM	96	38.17/38.54	Near Orleans, 0.5 mile east to 0.2 mile east of Red Cap Road. Pave shoulder and improve drainage.	0K160	0120000036	201.015	Other Program Objectives	\$430	\$2	\$625	\$1,057	0.01	Annual fatal and serious injury collision(s)
5	01	HUM	101	89.4	Near Korblex, at Boyd Draw Bridge No. 04-0085. Construct pedestrian path.	0K510	0120000108	201.015	Other Program Objectives	\$460	\$25	\$430	\$915	0.30	Annual fatal and serious injury collision(s)
6	01	HUM	101	111.21/111.24	Near Big Lagoon, 3.0 miles north of Big Lagoon Park Road. Replace culverts.	0P240	0124000211	201.151	Culvert(s)	\$440	\$0	\$400	\$840	2.00	Culvert(s) (ea)
7	01	LAK	20	5.31	Near Saratoga Springs, at 0.2 mile west of Witter Springs Road. Stabilize slope and install underdrain.	0N470	0124000045	201.150	Other Program Objectives	\$250	\$0	\$280	\$530	0.00	Location(s)
8	01	MEN	1	62.06	In Fort Bragg, at the Fort Bragg Maintenance Station. Install Electrical Vehicle (EV) charging station.	0P310	0124000218	201.352	Supplementary	\$460	\$10	\$280	\$750	1.00	Location(s)
9	01	MEN	20	20.0	Near Willits, at Road 900. Install radio repeater.	0P250	0124000212	201.352	Supplementary	\$250	\$0	\$300	\$550	0.00	Location(s)
10	02	SHA	299	33.04	Near Bella Vista, 1.4 miles east of Dry Creek Road at various locations. Repair and replace culverts.	1K790	0224000166	201.151	Culvert(s)	\$450	\$1	\$170	\$621	8.00	Culvert(s) (ea)
11	02	SIS	5	R44.1/R48.4	Near Yreka, from 0.2 mile south of Walters Road Overcrossing Bridge No. 02-0149 to 1.0 mile north of north Yreka Separation Bridge No. 02-0150. Install deer fencing.	1K800	0224000167	201.240	Other Program Objectives	\$461	\$8	\$186	\$655	0.00	Location(s)
12	02	SIS	97	27.0/52.1	Near Penoyar, from 0.7 mile west of Tennant-Bray Road to 0.3 mile west of Sheepy Creek Road near Dorris, at various locations. Repair and replace culverts.	0K520	0223000171	201.151	Culvert(s)	\$300	\$0	\$150	\$450	3.00	Culvert(s) (ea)
13	02	TEH	36	12.3/18.2	Near Rosewood, from 10.9 miles to 5.0 miles east of Bowman Road. Extend culverts.	1K370	0224000100	201.151	Culvert(s)	\$461	\$0	\$125	\$586	6.00	Culvert(s) (ea)
14	02	TRI	36	R28.68	Near Post Mountain, at the intersection of Route 3 and Route 36 at various locations. Repair and replace culverts.	1K780	0224000165	201.151	Culvert(s)	\$425	\$1	\$175	\$601	7.00	Culvert(s) (ea)
15	03	ED	89	17.0	Near Emerald Bay, 2.3 miles east of Cascade Road. Install Closed Circuit Television (CCTV).	3J090	0322000185	201.315	TMS	\$461	\$0	\$485	\$946	1.00	Field element(s)
16	03	ED	89	26.119	Near Tahoma, 0.1 mile north of California State Park Road. Install Rectangular Rapid Flash Beacons (RRFBs).	0N920	0324000223	201.310	Other Program Objectives	\$350	\$5	\$350	\$705	1.00	Daily Person Hours of Delay (DPHD)
17	03	PLA	28	0.4/0.6	In Tahoe City, from 0.1 mile west to 0.1 mile east of Grove Street. Install new traffic signals. Financial Contribution Only (FCO) to Placer County.	0N340	0324000093	201.310	Other Program Objectives	\$265	\$0	\$0	\$265	193.90	Daily Person Hours of Delay (DPHD)
18	03	PLA	28	4.1	In Cedar Flat, 0.2 mile south of Lardin Way. Install a pedestrian activated Rectangular Rapid Flash Beacon (RRFB).	3J28A	0325000146	201.015	Other Program Objectives	\$461	\$0	\$250	\$711	0.10	Annual fatal and serious injury collision(s)
19	03	PLA	28	5.85/5.98	Near Carnelian Bay, from 0.1 mile west of Onyx Street to Onyx Street. Replace concrete valley gutter/curb gutter to improve flow at drop inlet.	1N890	0325000145	201.151	Culvert(s)	\$461	\$0	\$250	\$711	1.00	Culvert(s) (ea)
20	03	SAC	51	1.2/1.44	In the city of Sacramento, from E Street Undercrossing to H Street Undercrossing. Grind pavement and replace polyester concrete overlay.	0N690	0324000172	201.121	Pavement	\$450	\$0	\$420	\$870	1.50	Lane mile(s)
21	04	ALA	580	R35.0/R38.0	In San Leandro, from the Foothill Boulevard Undercrossing No. 33-0334K to 0.2 mile west of the Keller Avenue Overcrossing No. 33-0340. Install end-of-queue detection for Variable Message Sign (VMS).	3X090	0425000192	201.015	Other Program Objectives	\$460	\$5	\$320	\$785	0.03	Annual fatal and serious injury collision(s)
22	04	ALA	880	R0.0/R33.5	Near Milpitas, from the Alameda County line to the 7th Street Undercrossing Bridge No. 33-0609 at various locations. Install safety signs.	2X690	0425000142	201.010	Other Program Objectives	\$350	\$5	\$320	\$675	0.02	Annual fatal and serious injury collision(s)
23	04	ALA	185	5.88	In San Leandro, 0.1 mile north of San Leandro Creek. Install Rectangular Rapid Flashing Beacons (RRFBs).	2X100	0425000042	201.015	Other Program Objectives	\$91	\$6	\$120	\$216	0.01	Annual fatal and serious injury collision(s)

2025-26 Minor B Project List
Districts 01-12
(x\$1,000)

No.	District	County	Route	Postmiles	Project Location/Description	EA	Project ID	Program Code	Activity Category	Construction (Life of Project)	Right of Way (Life of Project)	Support (Life of Project)	Total (Life of Project)	Performance Value	Performance Measure
24	04	MRN	101	12.2/12.5	Near Los Ranchitos, from Lincoln Avenue Undercrossing No. 27-0030 to 0.2 mile south of North San Pedro Road Undercrossing No. 27-0014. Improve fencing for wildlife.	3X390	0425000240	201.999	Other Program Objectives	\$325	\$1	\$301	\$627	0.00	Centerline mile(s)
25	04	SCL	101	R19.2/R21.6	In and near San Jose, from Coyote Creek to 0.3 mile north of Coyote Creek Road. Improve fencing for wildlife.	3X380	0425000239	201.999	Other Program Objectives	\$460	\$1	\$301	\$762	0.00	Centerline mile(s)
26	04	SCL	101	0.028/52.5	In Sargent, from the Santa Clara County line to Palo Alto, 0.3 mile north of Embarcadero Road Overcrossing No. 37-0150 at various locations. Install safety signs.	3X290	0425000228	201.010	Other Program Objectives	\$350	\$5	\$320	\$675	0.02	Annual fatal and serious injury collision(s)
27	04	SM	1	36.4	Near Montara, 0.1 mile north of 7th Street. Pavement improvements.	3X440	0425000245	201.120	Pavement	\$461	\$5	\$355	\$821	0.00	Lane mile(s)
28	05	MON	1	32.95	Near Big Sur, at 0.1 mile north of Esalen Canyon Hike. Replace culvert.	1P870	0522000120	201.151	Culvert(s)	\$333	\$0	\$110	\$443	1.00	Culvert(s) (ea)
29	05	MON	101	85.728/R91.88	In Salinas, 0.1 mile north of Airport Boulevard to just south of Russell Road. Replace pull boxes and wireless links.	1T040	0525000143	201.315	TMS	\$260	\$0	\$60	\$320	1.00	Field element(s)
30	05	SB	1	R33.36	Near Orcutt, at 1.2 miles south of Route 1/135 intersection. Replace culverts.	1T080	0525000147	201.151	Culvert(s)	\$250	\$5	\$75	\$330	2.00	Culvert(s) (ea)
31	05	SB	1	R35.787	In Santa Barbara and San Luis Obispo County, on Routes 1, 41, 46, 101, 150, and 166 at various locations. Upgrade signal equipment.	1T050	0525000144	201.310	Other Program Objectives	\$355	\$0	\$105	\$460	0.00	Daily Person Hours of Delay (DPHD)
32	05	SB	101	R14.2	In the city of Santa Barbara, at the Castillo Street Undercrossing No. 51-0204. Clean and restore drainage system to functional state.	1s560	0525000051	201.131	Other Program Objectives	\$461	\$5	\$140	\$606	1.00	Location(s)
33	05	SB	101	2.1/2.3	In the city of Carpinteria, 0.5 mile north of Ballard Avenue to 0.5 mile south of Casitas Pass Road. Extend shoulder and guardrail.	1T020	0525000141	201.015	Other Program Objectives	\$450	\$0	\$60	\$510	0.01	Annual fatal and serious injury collision(s)
34	05	SB	192	3.0/4.35	Near Santa Barbara, 0.1 mile west of Mission Canyon Road to 0.8 mile east of Hillcrest Road. Restore embankment, repair shoulder pavement damage and restore drainage profiles.	1T000	0525000139	201.131	Other Program Objectives	\$461	\$0	\$130	\$591	1.00	Location(s)
35	05	SB	1	11.05	Near Lompoc, at 0.9 mile north of Ytias Creek No. 51-0092. Repair culverts.	1T010	0525000140	201.151	Culvert(s)	\$200	\$5	\$70	\$275	3.00	Culvert(s) (ea)
36	05	SBT	101	0.0/R7.5	In Monterey and San Benito Counties, at various locations. Replace traffic signal and pedestrian signal heads.	1T030	0525000142	201.315	TMS	\$185	\$0	\$60	\$245	6.00	Field element(s)
37	05	SCR	1	R0.71	Near Watsonville, at Riverside Drive. Modify drainage.	1S210	0524000181	201.151	Culvert(s)	\$120	\$0	\$40	\$160	1.00	Culvert(s) (ea)
38	05	SCR	1	9.74	In Santa Cruz, on Routes 1, 129 and 152 at various locations. Upgrade traffic signals.	1T060	0525000145	201.315	TMS	\$280	\$0	\$60	\$340	9.00	Field element(s)
39	05	SCR	1	32.0/37.45	Near Swanton, 0.4 mile north of Scott Creek to the San Mateo County Line. Remove talus from catchment and repair rockfall barrier.	1Q990	0523000114	201.131	Other Program Objectives	\$388	\$0	\$130	\$518	1.00	Location(s)
40	05	SLO	101	16.146	In Pismo Beach, 0.1 mile south of 5 Cities Drive. Install travel time readers.	1T070	0525000146	201.315	TMS	\$70	\$0	\$30	\$100	7.00	Field element(s)
41	06	FRE	33	10.9/11.1	Near Coalinga, at Jacalitos Creek No. 42-0441. Replacement mitigation landscape planting.	43262	0622000206	201.210	Other Program Objectives	\$341	\$66	\$340	\$747	0.40	Acre(s)
42	06	FRE	33	14.23/14.89	In and near Coalinga, from Willow Springs Avenue to 0.1 mile south of Thompson Street. Construct Americans with Disabilities Act (ADA) compliant curb ramps and pedestrian path.	0Y430	0618000215	201.378	Supplementary	\$333	\$5	\$87	\$425	1.00	Curb ramp(s)
43	06	FRE	99	27.46	Near Fresno, at 0.7 mile south of West Shaw Avenue. Repair soundwall.	1F780	0623000089	201.131	Other Program Objectives	\$70	\$5	\$320	\$395	1.00	Location(s)
44	06	FRE	269	0/0.3	Near Avenal, at the Avenal Cutoff Road. Install safety lighting.	0V950	0617000073	201.010	Other Program Objectives	\$126	\$5	\$400	\$531	0.03	Annual fatal and serious injury collision(s)
45	06	KER	119	0.15/R10.46	In Taft, from Main Street/Gardnerfield Road to 1.3 miles east of Golf Course Road. Line and concrete invert pave culverts.	1J120	0625000124	201.151	Culvert(s)	\$388	\$0	\$156	\$544	5.00	Culvert(s) (ea)
46	06	KER	46	31.5/33.2	Near Lost Hills, at 0.2 mile east of Los Rios Street to 0.5 mile east of Buford Street. Highway planting.	1C840	0621000186	201.210	Other Program Objectives	\$305	\$3	\$230	\$538	2.00	Acre(s)
47	06	KER	99	26.7	Near Bakersfield, at the Calloway Canal No. 50-0265; also on Route 119, at the Weed Creek No. 50-0124 and the Broad Creek No. 50-0125. Repair bridges.	1A550	0620000068	201.110	Bridge	\$250	\$5	\$630	\$885	2.00	Bridge(s)
48	06	TUL	63	0.0/5.8	Near Tulare, from East Tulare Avenue to Avenue 264. Replace Americans with Disabilities (ADA) curb ramps.	1F530	0623000040	201.361	Supplementary	\$120	\$30	\$370	\$520	12.00	Curb ramp(s)

2025-26 Minor B Project List
Districts 01-12
(x\$1,000)

No.	District	County	Route	Postmiles	Project Location/Description	EA	Project ID	Program Code	Activity Category	Construction (Life of Project)	Right of Way (Life of Project)	Support (Life of Project)	Total (Life of Project)	Performance Value	Performance Measure
49	06	TUL	65	21.8	In Porterville, at West Linda Vista Avenue. Construct traffic diverter.	1A410	0620000006	201.010	Other Program Objectives	\$40	\$0	\$330	\$370	0.01	Annual fatal and serious injury collision(s)
50	06	TUL	99	39.7/51.8	In Visalia, from 0.7 mile north of Route 198 to Avenue 384. Rehabilitate drainage.	1E830	0622000096	201.150	Other Program Objectives	\$300	\$0	\$390	\$690	1.00	Location(s)
51	06	TUL	99	51.5/52.3	Near Kingsburg, from 0.3 mile south to 0.5 mile north of Avenue 384 Drive. Replace concrete pavement panels.	1G700	0624000155	201.121	Pavement	\$388	\$0	\$175	\$563	0.80	Lane mile(s)
52	07	LA	60	R22.2	In Diamond Bar, at westbound Lemon Avenue onramp. Construct drainage systems.	2Q040	0725000014	201.151	Culvert(s)	\$215	\$0	\$230	\$445	0.00	Culvert(s) (ea)
53	07	LA	60	R29.5	In Pomona, at South Garey Avenue. Rehabilitate existing drainage system.	1Q480	0723000058	201.151	Culvert(s)	\$200	\$0	\$300	\$500	1.00	Culvert(s) (ea)
54	07	LA	101	S0.91	In the city of Los Angeles, at 1st Street and South Pecan Street. Slope erosion control and fence repair.	2Q180	0725000232	201.210	Other Program Objectives	\$250	\$0	\$200	\$450	0.00	Acre(s)
55	07	LA	138	R48.52/53.561	In Palmdale, from East Palmdale Boulevard to 72nd Street East. Install traffic census stations.	1Q900	0724000210	201.315	TMS	\$250	\$15	\$250	\$515	2.00	Field element(s)
56	07	LA	170	R14.51/R14.78	In the city of Los Angeles, at the Riverside Drive/Tujunga Avenue Undercrossing. Rehabilitate highway planting.	2Q170	0725000223	201.210	Other Program Objectives	\$380	\$0	\$300	\$680	1.20	Acre(s)
57	07	LA	210	R18.3/R43.6	In Los Angeles County, on Routes 134 and 210 at various locations. Install/upgrade signs, striping, delineators, pavement markings, and retroreflective markers.	2Q060	0725000194	201.015	Other Program Objectives	\$400	\$0	\$235	\$635	4.17	Annual fatal and serious injury collision(s)
58	07	LA	91	6.298/R14.472	In Gardena, Crenshaw, Compton, and Long Beach, at South Figueroa Street to Downey Avenue. Install and upgrade signs, delineation, pavement markings, and retroreflective markers.	2Q080	0725000196	201.015	Other Program Objectives	\$320	\$0	\$235	\$555	2.10	Annual fatal and serious injury collision(s)
59	07	LA	101	9.4	In the city of Los Angeles, 0.2 mile south of Universal Studios Boulevard. Construct Design Pollution Prevention Infiltration Areas (DPPIAs).	1Q100	0722000208	201.335	Other Program Objectives	\$300	\$0	\$300	\$600	2.50	Acre(s) treated/pollutant
60	07	LA	110	2.875/28.1	In the city of Los Angeles, from West Harry Bridge Boulevard to South Ave 52. Install signs, channelizers, delineation, pavement markings, and retroreflective markers.	2Q120	0725000200	201.015	Other Program Objectives	\$410	\$0	\$250	\$660	1.04	Annual fatal and serious injury collision(s)
61	07	LA	213	0.31/8.449	In the cities of Los Angeles, Rancho Palos Verdes, Lomita, and Torrance, at West 19th Street to Torrance Boulevard. Install and upgrade signs, delineation, channelizers, pavement markings, and retroreflective markers.	2Q070	0725000195	201.015	Other Program Objectives	\$250	\$0	\$180	\$430	3.63	Annual fatal and serious injury collision(s)
62	07	LA	710	6.212/T31.761	In Los Angeles County, on Routes 2, 39 and 710 at various locations. Install/upgrade signs, delineators, pavement markings, retroreflective markers, and object markers.	2Q090	0725000197	201.015	Other Program Objectives	\$230	\$0	\$180	\$410	2.95	Annual fatal and serious injury collision(s)
63	07	LA	710	8.0/9.0	In Long Beach, from 27th Street to Wardlow Road. Rehabilitate highway planting.	2Q160	0725000222	201.210	Other Program Objectives	\$460	\$0	\$300	\$760	4.60	Acre(s)
64	07	LA	2	49.5	In Palmdale, at the Chilao Maintenance Station (L5708). Install pipes for water line.	2Q150	0725000221	201.352	Supplementary	\$250	\$0	\$200	\$450	1.00	Location(s)
65	07	LA	10	20.6/26.9	In the cities of Los Angeles, Alhambra, San Gabriel, and Rosemead, from 0.2 mile west of North Eastern Avenue Undercrossing to Rosemead Boulevard. Restore landscape and bioswales.	1Q070	0722000166	201.335	Other Program Objectives	\$330	\$0	\$520	\$850	2.40	Acre(s) treated/pollutant
66	07	LA	60	19.5/20.4	In City of Industry, at Fullerton Road and South Nogales Street. Restore stormwater quality Best Management Practices (BMPs) including bioswales and Austin Vault Sand Filter (AVSF).	1Q060	0722000165	201.335	Other Program Objectives	\$405	\$0	\$750	\$1,155	3.70	Acre(s) treated/pollutant
67	07	LA	405	23.5/23.7	In Inglewood, from Florence Avenue Overcrossing to northbound La Cienega Boulevard Overcrossing (PM 23.5/23.7); also in Los Angeles, at southbound on-ramp at La Tijera Boulevard (PM 24.13/24.27). Highway Planting Restoration.	2Q130	0725000219	201.210	Other Program Objectives	\$300	\$0	\$300	\$600	1.50	Acre(s)
68	07	LA	405	42.241/44.928	In the city of Los Angeles, from Sherman Way to Nordhoff Street. Install and upgrade signs and pavement markings.	2Q110	0725000199	201.015	Other Program Objectives	\$360	\$0	\$180	\$540	2.10	Annual fatal and serious injury collision(s)
69	07	VEN	1	2.63/9.7	In Malibu, 1.4 miles north of Yerba Buena Road to 0.5 mile south of Las Posas Road. Install signs, delineation, and rumble strips.	2Q100	0725000198	201.015	Other Program Objectives	\$230	\$0	\$175	\$405	1.64	Annual fatal and serious injury collision(s)
70	07	VEN	1	3.3	In Malibu, at 0.7 mile north of Deer Creek Road. Construct stairway.	1Q460	0723000008	201.235	Other Program Objectives	\$350	\$0	\$300	\$650	0.03	Annual fatal and serious injury collision(s)

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71	07	VEN	33	0.0/57.508	In Ventura County, on Routes 23, 33, and 150 at various locations. Install/upgrade signs, striping, and pavement markings.	1Q760	0723000159	201.015	Other Program Objectives	\$461	\$0	\$550	\$1,011	4.24	Annual fatal and serious injury collision(s)
72	07	VEN	126	3.1/3.3	In the city of Ventura, near South Kimball Road offramp. Repair broken V-ditch to improve roadside drainage.	2Q050	0725000186	201.151	Culvert(s)	\$350	\$0	\$230	\$580	0.00	Culvert(s) (ea)
73	08	RIV	10	R14.68	In Banning, 0.1 mile west of the East Ramsey Street - E10 Undercrossing No. 56-0328 at the Banning and Riverside Maintenance Stations. Install overhead lighting.	1R090	0825000061	201.352	Supplementary	\$400	\$10	\$640	\$1,050	2.00	Location(s)
74	08	RIV	91	7.4/7.6	In Riverside and San Bernardino Counties, on Routes 15, 60, 91 and 215 at various locations. Relocate service cabinets and pull boxes.	1R590	0825000087	201.315	TMS	\$350	\$10	\$610	\$970	10.00	Field element(s)
75	08	RIV	10	48.42	Near Desert Palms, 1.4 miles east of Cook Street eastbound onramp Bridge No. 56-0788S. Replacement of wind sensor, temperature visibility sensor, Closed Caption Television (CCTV), and permanent Edison Power.	1R570	0825000085	201.315	TMS	\$350	\$10	\$610	\$970	2.00	Field element(s)
76	08	RIV	74	12.7	In Lake Elsinore, at Lakeside High School Stadium Way. Add crosswalk markings.	1P580	0824000005	201.015	Other Program Objectives	\$400	\$10	\$660	\$1,070	0.03	Annual fatal and serious injury collision(s)
77	08	SBD	142	R1.7/2.0	Near Chino Hills, from 0.1 mile to 0.4 mile west of Fairway Drive. Install new guardrail.	1R000	0824000206	201.015	Other Program Objectives	\$460	\$10	\$650	\$1,120	0.04	Annual fatal and serious injury collision(s)
78	08	SBD	15	7.58	Near Fontana, at 0.2 mile north of Victoria Street Undercrossing No. 54-0965 at the Inland Empire Transportation Management Center (13892 Victoria Street). Replace deficient boilers and water heaters with electric equipment.	1R580	0825000086	201.355	Supplementary	\$350	\$20	\$610	\$980	1.00	Location(s)
79	08	SBD	142	5.267/5.536	In Chino Hills, from 0.1 mile west to 0.1 mile east of Pipeline Avenue. Correction of four non-compliant driveways to meet Americans with Disabilities Act (ADA) standards.	1N760	0823000046	201.361	Supplementary	\$420	\$10	\$910	\$1,340	0.00	Curb ramp(s)
80	08	SBD	18	49.203	In Big Bear Lake, at Knickerbocker Road. Implement a protected left-turn phase through signal modifications.	1R880	0825000141	201.315	TMS	\$410	\$20	\$610	\$1,040	1.00	Field element(s)
81	08	SBD	62	62.0/125.8	In Vidal Junction, at the intersection of Routes 62 and 95 at the Vidal Junction Maintenance Station. Relocate main switch gear box and generator and upgrade electrical point of connection.	1N990	0823000074	201.351	Supplementary	\$460	\$20	\$720	\$1,200	1.00	Location(s)
82	08	SBD	395	45.59	In Kramer Junction, 0.3 mile south of Twenty Mule Team Road at the Barstow Maintenance Stations. Install overhead lighting.	1R080	0825000064	201.352	Supplementary	\$400	\$10	\$660	\$1,070	2.00	Location(s)
83	09	INY	190	24.5/124.0	Near Death Valley, at the intersection of Route 190 and Route 136 to the Southern Entrance to Death Valley National Park. Replace signs.	42780	0925000074	201.170	Supplementary	\$461	\$0	\$120	\$581	175.00	Sign(s)
84	09	INY	190	140.659	In Inyo and Kern Counties, on Routes 14, 120, 127, 128, 190, 266 and 395 at various locations. Install loop detectors.	42790	0925000076	201.315	TMS	\$350	\$0	\$240	\$590	10.00	Field element(s)
85	09	KER	14	23.1	In Mojave, 0.5 mile south of Maury Avenue. Install Extinguishable Message Signs (EMS) system on mountain passes.	39890	0923000039	201.315	TMS	\$320	\$0	\$240	\$560	4.00	Field element(s)
86	09	KER	178	57.1/88.26	Near Weldon, from Kelso Valley Road to Route 14. Upgrade signs.	41160	0925000021	201.170	Supplementary	\$461	\$0	\$110	\$571	165.00	Sign(s)
87	09	MNO	120	R11.95	In Lee Vining, at 0.1 mile north of Vista Point Drive. Upgrade Transportation Management System (TMS) element Changeable Message Sign (CMS) structure and technology.	42730	0925000065	201.315	TMS	\$400	\$0	\$200	\$600	1.00	Field element(s)
88	09	MNO	167	0.24	In Inyo, Mono, and Kern Counties, on Routes 6, 89, 108, 120, 127, 158, 182, 190, and 395 at various locations. Install new loop detectors.	38530	0920000027	201.315	TMS	\$333	\$0	\$240	\$573	7.00	Field element(s)
89	10	AMA	104	R4.271	Near Plymouth, at 0.3 mile east of 5 Mile Drive. Install lighting.	1Q310	1022000265	201.015	Other Program Objectives	\$295	\$20	\$400	\$715	0.01	Annual fatal and serious injury collision(s)
90	10	AMA	26	3.41/3.6	Near Pioneer, from Deavers Road to 0.2 mile west of Roden Lane. Install curve warning signs.	1S750	1024000119	201.010	Other Program Objectives	\$264	\$0	\$280	\$544	0.04	Annual fatal and serious injury collision(s)
91	10	AMA	49	13.5	Near Drytown, at 0.03 mile north of Grelich Alley. Rehabilitate drainage system.	1M030	1023000150	201.151	Culvert(s)	\$193	\$5	\$290	\$488	0.00	Culvert(s) (ea)
92	10	CAL	4	35.6/37.4	Near Murphys, from 0.07 mile east of Coyote Canyon Road to just west Morris Road. Replace culverts and rehabilitate pavement.	1N240	1021000129	201.151	Culvert(s)	\$461	\$0	\$952	\$1,413	5.00	Culvert(s) (ea)

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93	10	CAL	26	19.3/19.34	Near Mokolunne Hill, on Routes 26 and 49 at various locations. Install signs.	1S740	1024000118	201.010	Other Program Objectives	\$281	\$0	\$280	\$561	0.34	Annual fatal and serious injury collision(s)
94	10	SJ	580	4.0/6.5	Near Tracy, from 0.1 mile east of Chrisman Road Overcrossing to 0.1 mile east of Tracy Boulevard Overcrossing. Repair and replace right-of-way security fencing.	1T650	1025000038	201.235	Other Program Objectives	\$285	\$0	\$60	\$345	0.01	Annual fatal and serious injury collision(s)
95	10	SJ	5	29.7/30.0	Near Stockton, at the March Lane Undercrossing. Install right-of-way security fencing at offramp and onramp.	1T180	1024000186	201.235	Other Program Objectives	\$443	\$0	\$260	\$703	0.01	Annual fatal and serious injury collision(s)
96	11	IMP	8	R0.435R	In Ocotillo, 1.6 miles south of Mountain Apings County Park. Install a new Changeable Message Sign (CMS) board.	3A947	1125000164	201.315	TMS	\$461	\$0	\$161	\$622	1.00	Field element(s)
97	11	IMP	8	R53.497	In Holtville, at Vanderlinden Road. Update Traffic Census System.	3A930	1125000136	201.315	TMS	\$460	\$0	\$161	\$621	2.00	Field element(s)
98	11	IMP	8	R89.5	Near Winterhaven, at the Winterhaven Agricultural Inspection Station at 3510 I-8, Winterhaven, Ca 92283. Repair pavement.	3A963	1125000184	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
99	11	IMP	8	R90.99	In Imperial County, on Route 8at the 03 All American Canal Bridge No. 58-0306R; also in San Diego County, on Route 15 at the San Luis Rey River Bridge No.58-0306R. Backfill erosion/ slope paving.	3A973	1125000197	201.110	Bridge	\$460	\$0	\$161	\$621	2.00	Bridge(s)
100	11	IMP	111	R2.211	In Calexico, at E 7th Street and Imperial Avenue. Install curb ramps, signs and striping to Americans with Disabilities Act (ADA) standards.	3A926	1125000130	201.361	Supplementary	\$350	\$0	\$123	\$473	4.00	Curb ramp(s)
101	11	IMP	86	1.657/1.805	Near Heber, 1.1 miles west Rockwood Road. Install new sidewalk, asphalt pavement, signs, and pavement markings.	3A921	1125000124	201.361	Supplementary	\$350	\$0	\$123	\$473	1.00	Curb ramp(s)
102	11	IMP	78	12.0	Near Borrego Springs, 1.1 miles west of the U.S. Border Patrol Station at CA-78 Borrego Springs, Ca 92004. Install a Changeable Message Sign (CMS) board.	3A889	1124000372	201.315	TMS	\$461	\$0	\$185	\$646	1.00	Field element(s)
103	11	IMP	78	27.3	In Brawley, 0.2 mile west of Whitlock Road. Replace guardrail.	3A949	1125000166	201.015	Other Program Objectives	\$350	\$0	\$123	\$473	0.10	Annual fatal and serious injury collision(s)
104	11	IMP	98	56.5	In Holtville, at the Caltrans Midway Maintenance Station. Install modular maintenance building.	3A964	1125000185	201.352	Supplementary	\$350	\$0	\$123	\$473	0.00	Location(s)
105	11	SD	5	R1.0/R55.0	In the cities of San Diego, Chula Vista, National City, Solana Beach, Encinitas, Carlsbad and Oceanside at Willow Rd to Wire Mountain Road. Install sign structures.	3A958	1125000179	201.170	Supplementary	\$350	\$0	\$122	\$472	30.00	Sign(s)
106	11	SD	5	R12.8/R19.2	In the city of San Diego, at South 32nd Street to Arista Street. Repair and replace down power poles.	3A961	1125000182	201.170	Supplementary	\$350	\$0	\$123	\$473	10.00	Lighting fixture(s)
107	11	SD	5	R28.454	In the city of San Diego, at onramp from westbound La Jolla Village Drive. Install sign structures and upgrade striping.	3A925	1125000128	201.170	Supplementary	\$350	\$0	\$123	\$473	3.00	Sign(s)
108	11	SD	5	R36.04	In the city of San Diego, at northbound exit ramp to Via de La Valle. Upgrade curb ramps to Americans with Disabilities Act (ADA) standards and sidewalks.	3A841	1124000303	201.361	Supplementary	\$405	\$0	\$142	\$547	2.00	Curb ramp(s)
109	11	SD	5	R36.23	In Del Mar, at the entrance ramp from eastbound Via De La Valle. Enhance pedestrian/bicycle pavement delineation and signing improvements for Active Transportation Programs.	3A946	1125000163	201.361	Supplementary	\$350	\$0	\$123	\$473	2.00	Curb ramp(s)
110	11	SD	5	R36.355/R36.555	In Del Mar, at Via De La Valle. Pavement rehabilitation.	3A918	1125000120	201.121	Pavement	\$350	\$0	\$123	\$473	1.00	Lane mile(s)
111	11	SD	5	R38.5/R60.01	In Encinitas, Carlsbad and Oceanside, at Manchester Avenue to 0.4 mile south of USMC Camp Pendleton. Repair and replace down power poles.	3A959	1125000180	201.170	Supplementary	\$350	\$0	\$123	\$473	13.00	Lighting fixture(s)
112	11	SD	5	R39.6	In Encinitas, at the westbound Birmingham Drive onramp. Replace guardrail.	3A951	1125000168	201.015	Other Program Objectives	\$350	\$0	\$123	\$473	0.10	Annual fatal and serious injury collision(s)
113	11	SD	5	R56.3R/R56.4R	In the city of San Diego, 0.1 mile south of the Santa Margarita River. Pavement rehabilitation.	3A343	1120000209	201.122	Pavement	\$388	\$0	\$155	\$543	0.40	Lane mile(s)
114	11	SD	5	R56.42R	In Oceanside, 0.1 miles south of the Santa Margarita River. Pavement rehabilitation.	3A796	1124000208	201.121	Pavement	\$381	\$0	\$133	\$514	0.00	Lane mile(s)
115	11	SD	5	R59.8	In Oceanside, at 0.3 mile north of the Aliso Creek Rest Area onramp. Pavement rehabilitation.	3A945	1125000160	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)

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116	11	SD	5	R67.6	Near Camp Pendleton North, at the San Diego County Weigh Station. Upgrade electronic sign board to current standards.	3A971	1125000195	201.321	Supplementary	\$350	\$0	\$123	\$473	0.00	Location(s)
117	11	SD	8	L1.46	In the city of San Diego, at the Home Avenue exit ramp. Install sign structures and upgrade striping.	3A977	1125000203	201.170	Supplementary	\$350	\$0	\$122	\$472	8.00	Sign(s)
118	11	SD	8	R66.5	Near Boulevard, at the Boulevard Maintenance Station. Install modular maintenance building.	3A936	1125000145	201.352	Supplementary	\$350	\$0	\$123	\$473	0.00	Location(s)
119	11	SD	15	M3.856/M6.419	In the city of San Diego, under Polk Avenue and Orange Avenue from 0.5 mile north of Manzanita Canyon Open Space to south of Camino del Rio South install soffit lighting in tunnel.	3A934	1125000142	201.170	Supplementary	\$460	\$0	\$161	\$621	19.00	Lighting fixture(s)
120	11	SD	15	M4.879	In the city of San Diego, under Polk Avenue and Orange Avenue at Teralta Park. Install soffit lighting in tunnel.	3A970	1125000192	201.170	Supplementary	\$460	\$0	\$161	\$621	19.00	Lighting fixture(s)
121	11	SD	15	R0.5	In the city of San Diego, at the Ocean View Boulevard offramp. Pavement rehabilitation.	3A953	1125000170	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
122	11	SD	15	R0.7	In the city of San Diego, at the Ocean View Boulevard onramp. Pavement rehabilitation.	3A940	1125000154	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
123	11	SD	15	R0.698	In the city of San Diego, at the onramp from Ocean View Boulevard. Pavement rehabilitation.	3A923	1125000126	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
124	11	SD	15	R0.773	In the city of San Diego, at the onramp to Ocean View Boulevard. Pavement rehabilitation.	3A929	1125000135	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
125	11	SD	15	R6.03	In the city of San Diego, at the Camino Del Rio South. Replace and upgrade guardrail.	3A919	1125000121	201.015	Other Program Objectives	\$350	\$0	\$123	\$473	0.10	Annual fatal and serious injury collision(s)
126	11	SD	15	R34.5	In Escondido, 0.3 mile north of Nutmeg Street. Replace guardrail.	3A969	1125000190	201.015	Other Program Objectives	\$350	\$0	\$122	\$472	0.10	Annual fatal and serious injury collision(s)
127	11	SD	94	R10.76R	Near Spring Valley, at Campo Road. Upgrade guardrail.	3A967	1125000188	201.015	Other Program Objectives	\$350	\$0	\$123	\$473	0.10	Annual fatal and serious injury collision(s)
128	11	SD	163	R11.28	In the city of San Diego, 0.5 mile north of Kearny Villa Road. Replace guardrail.	3A968	1125000189	201.015	Other Program Objectives	\$350	\$0	\$123	\$473	0.10	Annual fatal and serious injury collision(s)
129	11	SD	5	4.9/5.0	In the city of San Diego, on the northbound auxiliary lane from Palm Avenue to 0.3 mile south of Main Street. Rehabilitate pavement.	3A943	1125000158	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
130	11	SD	5	6.8	In Chula Vista, on the auxiliary lane from L Street to J Street. Rehabilitate pavement onramp.	3A939	1125000152	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
131	11	SD	5	6.25	In Chula Vista, 0.2 mile north from Palomar Avenue. Pavement rehabilitation.	3A938	1125000147	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
132	11	SD	5	9.1	In National City, at the Chula Vista Maintenance Station. Pavement rehabilitation.	3A972	1125000196	201.352	Supplementary	\$350	\$0	\$123	\$473	4.00	Location(s)
133	11	SD	11	0.745	In the city of San Diego, west of Sanyo Avenue. Update Traffic Census system.	3A950	1125000167	201.315	TMS	\$460	\$0	\$161	\$621	2.00	Field element(s)
134	11	SD	15	1.7	In the city of San Diego, at the northbound Market Street offramp. Rehabilitate pavement onramp.	3A954	1125000171	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
135	11	SD	15	1.9	In the city of San Diego, at the southbound Market Street exit ramp. Rehabilitate pavement onramp.	3A941	1125000155	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
136	11	SD	15	1.84	In the city of San Diego, at the northbound Market Street onramp. Rehabilitate pavement onramp.	3A955	1125000172	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
137	11	SD	15	2.1	In the city of San Diego, at the eastbound Route 94 Connector. Rehabilitate pavement onramp.	3A942	1125000157	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
138	11	SD	15	2.305	In the city of San Diego, at the westbound Route 94 Connector. Rehabilitate pavement onramp.	3A937	1125000146	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
139	11	SD	52	0.26	In the city of San Diego, at the eastbound onramp from La Jolla Parkway. Repair and upgrade guardrail.	3A927	1125000131	201.015	Other Program Objectives	\$350	\$0	\$123	\$473	0.10	Annual fatal and serious injury collision(s)
140	11	SD	54	1.97	Near Bonita, at Sweetwater Road. Upgrade lighting in the tunnel.	3A948	1125000165	201.170	Supplementary	\$460	\$0	\$161	\$621	1.00	Lighting fixture(s)
141	11	SD	94	2.86	In the city of San Diego, 0.3 mile east of 32nd Street at the southbound connector. Pavement rehabilitation.	3A944	1125000159	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
142	11	SD	94	3.42	In the city of San Diego, at the Home Avenue exit ramp. Install sign structures and upgrade striping.	3A928	1125000132	201.170	Supplementary	\$350	\$0	\$123	\$473	2.00	Sign(s)
143	11	SD	163	3.64	In the city of San Diego, at the El Centro offramp on right shoulder. Replace wall.	3A957	1125000174	201.150	Other Program Objectives	\$350	\$0	\$123	\$473	0.00	Location(s)
144	11	SD	805	7.1/7.2	In Chula Vista, at H Street. Roadside rehabilitation.	3A960	1125000181	201.210	Other Program Objectives	\$350	\$0	\$123	\$473	1.50	Acre(s)
145	11	SD	905	3.1R	In the city of San Diego, at the westbound Oro Vista Road offramp. Rehabilitate pavement onramp.	3A952	1125000169	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)

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146	11	SD	905	3.1R	In the city of San Diego, at the eastbound Oro Vista Road onramp. Rehabilitate pavement onramp.	3A956	1125000173	201.120	Pavement	\$350	\$0	\$123	\$473	0.00	Lane mile(s)
147	11	SD	905	3.656	In Chula Vista, at Main Street. Replace Highway Advisory Radios (HAR) technology components to reset life cycle and replace Vehicle To Infrastructure (VTIS) Road Side Unit (RSU) brackets.	3A924	1125000127	201.315	TMS	\$461	\$0	\$161	\$622	4.00	Field element(s)
148	11	SD	8	14.0/15.434	In El Cajon, from 1.3 miles north of El Cajon Boulevard to North Johnson Avenue. Rehabilitate bridge approach slab with foam injection.	3A974	1125000199	201.110	Bridge	\$460	\$0	\$161	\$621	7.00	Bridge(s)
149	11	SD	52	13.466	In Santee, at the westbound Exit Ramp to Mast Boulevard. Update curb ramps, sidewalks, signs and striping to Americans with Disabilities Act (ADA) standards.	3A935	1125000144	201.361	Supplementary	\$350	\$0	\$123	\$473	5.00	Curb ramp(s)
150	11	SD	805	12.234	In the city of San Diego, at Imperial Avenue. Update Changeable Message Sign (CMS) board.	3A920	1125000123	201.315	TMS	\$460	\$0	\$161	\$621	1.00	Field element(s)
151	12	ORA	22	R1.2	In Garden Grove, on westbound offramp at Valley View Street. Upgrade signal and safety lighting, replace dikes with curb and gutters, and place new striping.	0V770	1225000074	201.015	Other Program Objectives	\$115	\$0	\$80	\$195	0.02	Annual fatal and serious injury collision(s)
152	12	ORA	22	R10.7/R10.8	In Santa Ana, on eastbound connector from southbound Santa Ana Freeway and southbound Route 57. Enhance roadway signs and pavement delineation.	0V820	1225000082	201.015	Other Program Objectives	\$131	\$0	\$80	\$211	0.02	Annual fatal and serious injury collision(s)
153	12	ORA	90	0.5/12.6	In La Habra, Fullerton, Brea, Yorba Linda, and Anaheim, on Route 90 between Route 39 and route 91. Replace or install signs, upgrade crosswalks markings, and update striping.	0V700	1225000063	201.015	Other Program Objectives	\$360	\$0	\$144	\$504	0.85	Annual fatal and serious injury collision(s)
154	12	ORA	91	2.4	In Anaheim, on westbound offramp at Euclid Street. Enhance existing pavement delineation, signing, and signals to reduce number of wrong way collisions.	0V780	1225000075	201.015	Other Program Objectives	\$400	\$0	\$130	\$530	0.15	Annual fatal and serious injury collision(s)
155	12	ORA	133	1.5	In Laguna Beach, at Laguna Art College. Install rumble strips, upgrade signs, signal heads, and enhance pavement markings.	0V870	1225000098	201.015	Other Program Objectives	\$270	\$0	\$108	\$378	0.16	Annual fatal and serious injury collision(s)
156	12	ORA	261	1.3/3.2	In Irvine, from 0.3 mile south of Irvine Blvd and 0.3 mile north of Portola Parkway. Upgrade existing signing, install/ enhance pavement markers and enhance delineation for wrong-way driving.	0V730	1225000068	201.015	Other Program Objectives	\$410	\$0	\$164	\$574	0.14	Annual fatal and serious injury collision(s)
157	12	ORA	1	12.2/14.05	Near Laguna Beach to near Newport Beach, between School/State Park and Newport Coast Drive intersections. Install rumble strips and upgrade existing guardrail to current standards.	0V710	1225000066	201.010	Other Program Objectives	\$403	\$0	\$161	\$564	0.06	Annual fatal and serious injury collision(s)
158	12	ORA	1	20.3/21.0	In Newport Beach, at Balboa Boulevard to 61st Street. Install shoulder rumble strip for Class 2 bike lane and install vehicle speed feedback sign.	0V790	1225000076	201.015	Other Program Objectives	\$270	\$0	\$108	\$378	0.05	Annual fatal and serious injury collision(s)
159	12	ORA	5	32.8/33.3	In Santa Ana, near French Street to N Broadway. Upgrade existing signs, enhance pavement markings and upgrade traffic signals.	0V750	1225000071	201.015	Other Program Objectives	\$390	\$0	\$156	\$546	0.16	Annual fatal and serious injury collision(s)
160	12	ORA	57	11.08/R22.5	In Orange, Anaheim, Placentia, and Brea at Chapan Ave to 1.6 miles north of East Lambert Road. Replace all damaged fiber optic cables and upgrade Transportation Management System (TMS) elements.	0V760	1225000072	201.315	TMS	\$330	\$0	\$132	\$462	14.00	Field element(s)
161	12	ORA	241	36.5	In Anaheim, 0.2 mile south of Windy Ridge Road. Install/enhance pavement markers and enhance delineation for wrong-way driving.	0V720	1225000067	201.015	Other Program Objectives	\$360	\$0	\$144	\$504	0.34	Annual fatal and serious injury collision(s)
Total										\$56,414	\$389	\$37,600	\$94,402		

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.7a., Action Item

Prepared By: Tarek Tabshouri, Chief
Division of Aeronautics

Subject: **FISCAL YEAR 2025-26 AERONAUTICS ANNUAL ALLOCATION TO MATCH
FEDERAL AIRPORT IMPROVEMENT PROGRAM GRANTS
RESOLUTION FDOA-2025-01**

ISSUE:

Should the California Transportation Commission (Commission) approve the attached Resolution FDOA-2025-01, allocating \$2,500,000 for the Fiscal Year 2025-26 Aeronautics Program Annual Allocation to match Federal Airport Improvement Program (AIP) grants?

RECOMMENDATION:

The California Department of Transportation (Department) recommends the approval of Resolution FDOA-2025-01, allocating \$2,500,000 for the 2025-26 Aeronautics Program Annual Allocation to match Federal AIP grants.

BACKGROUND:

Each year the Commission approves an annual allocation to match Federal AIP grants. This allocation provides the authority for the Department to subvent matching funds to individual projects as requested by airport sponsors.

The Department provides Commission staff with quarterly reports on allocations for AIP matching grant funds. The reports show the sponsor's name, the airport name, a project description, as well as funded amounts and the project's district location.

The attached Resolution proposes to renew the delegation for the allocation of State funds to match the Federal AIP grants. Approval of this Resolution is contingent upon the passage of the Budget Act of 2025. This annual allocation request consists of \$1,500,000 as identified in the 2024 Aeronautics Fund Estimate, and \$1,000,000 of project savings from the 2021-22 annual allocation made by the Commission that remains available.

FINANCIAL RESOLUTION:

Resolved that Resolution FDOA-2025-01 is approved to authorize an annual allocation of \$2,500,000 to match Federal AIP grants in accordance with the following resolution.

Attachment:

Resolution FDOA-2025-01

CALIFORNIA TRANSPORTATION COMMISSION

Allocation of Funds to Match

Federal Airport Improvement Program Grants

Resolution FDOA-2025-01

- 1.1 WHEREAS, pursuant to Section 21683.1 of the California Public Utilities Code (PUC), the California Transportation Commission (Commission) is authorized to allocate funds for a portion of the local match for Federal Airport Improvement Program (AIP) grants to certain airports; and
- 1.2 WHEREAS, the Commission sponsored the legislation that enacted PUC section 21683.1 in order to maximize the amount of federal airport funds that can be allocated to California; and
- 1.3 WHEREAS, the Federal Aviation Administration (FAA) requires airport sponsors to certify that matching funds are available as a condition of accepting a federal grant; and
- 1.4 WHEREAS, in the June 2025 Commission meeting, the Commission approved the Capital Improvement Program which lists needed federal airport improvement projects from all funding sources;
- 2.1 NOW, THEREFORE BE IT RESOLVED, that, regarding Federal Airport Improvement Program grants made by the FAA to public entities in this State received by the California Department of Transportation (Department) from July 1, 2025, through June 30, 2026, with the approval of the Department, at such time as the federal grant offer is accepted by the public entity applicant, there is allocated to each applicant from the Aeronautics Account, five percent of that portion of the grant whose primary benefit is for general aviation in order to provide a part of the local match for the grant in accordance with the provisions of PUC section 21683.1 (b) until the total of all such allocations equals \$2,500,000; and
- 2.2 BE IT FURTHER RESOLVED, that the Department shall provide Commission staff with quarterly status reports on sponsors' matching fund applications that have been approved by the Department.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5a., Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR SHOPP DISTRICT MINOR PROJECT
RESOLUTION FP-24-94**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$1,250,000 for one District Minor project programmed in the State Highway Operation and Protection Program (SHOPP)?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation of \$1,250,000 for one District Minor SHOPP project.

BACKGROUND:

The attached vote list describes one District Minor SHOPP project totaling \$1,250,000. The Department is ready to proceed with this project, and is requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$1,250,000 be allocated from the Budget Act of 2024, Budget Act Item 2660-302-0042 for construction for one District Minor SHOPP project described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project #	Allocation Amount	County	Dist-Co-Rte	Postmile	Location Project Description	EA Project ID Program Adv Phase	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5a.(1)					Minor Projects			Resolution FP-24-94
1					Near Fresno, at the Shields Avenue	1C041	2024-25	
\$1,250,000					Undercrossing No. 42-0297R. Install/upgrade	0625000080	302-0042 SHA	\$1,250,000
					Americans with Disabilities Act (ADA) curb	SHOPP	20.20.201.361	
Fresno					ramps and sidewalks.			
06-Fre-41					This project will be funded from the District's	4FCO		
26.46					Minor Program			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5b.(1), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR SHOPP PROJECTS
RESOLUTION FP-24-95**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$367,528,000 for 25 projects programmed in the State Highway Operation and Protection Program (SHOPP)?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation of \$367,528,000 for 25 SHOPP projects.

BACKGROUND:

The attached vote list describes 25 SHOPP projects totaling \$367,528,000. The Department is ready to proceed with these projects, and is requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$315,392,000 be allocated from the Budget Act of 2023, Budget Act Items 2660-302-0890 and 2660-302-3290, and Non-Budget Act Item 2660-802-3290 for construction and \$52,136,000 for construction engineering for 25 SHOPP projects described on the attached vote list.

Attachment

2.5 Highway Financial Matters

2.5 Highway Financial Matters				PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description					
2.5b.(1)	SHOPP Projects					Resolution FP-24-95
1 \$2,233,000	In Arcata, from Sunset Avenue Overcrossing to Route 299. Financial Contribution Only (FCO) to City of Arcata to construct trail for enhanced safety and connectivity for bicyclists and pedestrians.			01-4856 SHOPP/23-24 CON ENG	505-3290 RMRA 20.10.201.400	\$0
Humboldt 01-Hum-101 86.942/88.272	Preliminary <u>Engineering</u>	<u>Budget</u>	<u>Expended</u>	CONST \$2,233,000 0124000054 3,4FCO 0N550	2017-18 802-3290 RMRA 20.20.201.400	\$2,233,000
	PA&ED	\$0	\$0			
	PS&E	\$0	\$0			
	R/W Sup	\$0	\$0			
<u>Performance Measure:</u> Planned: 1,850.0, Actual: 1,850.0 Bicycle and Pedestrian Infrastructure (linear feet) CEQA - MND, 12/22/2022 NEPA - CE, 03/13/2024; Re-validation 04/28/2025 Future consideration of funding approved under Resolution E-23-16; January 2023. Twelve month allocation time extension for CONST and CON ENG approved under Waiver 24-125; August 2024. Related non-SHOPP allocation for EA PPNO 01-2548 under Resolution FATP-2425-26; June 2025.						
2 \$66,891,000	In and near Ukiah, from Robinson Creek Bridge to Pomo Lane Undercrossing. Rehabilitate roadway, install vegetation control, upgrade guardrail, high tension cable median barrier, lighting, and Transportation Management System (TMS) elements, and apply High Friction Surface Treatment (HFST). This project will improve safety and ride quality.			01-4695 SHOPP/24-25 CON ENG \$5,000,000 CONST \$52,822,000 0117000237 3,4 0H570	505-3290 RMRA 001-0890 FTF 20.10.201.122 2017-18 802-3290 RMRA 2023-24 302-0890 FTF 20.20.201.122	\$573,000 <u>\$4,427,000</u> \$5,000,000 \$7,099,000 <u>\$54,792,000</u> \$61,891,000
Mendocino 01-Men-101 20.9/28.6	Preliminary <u>Engineering</u>	<u>Budget</u>	<u>Expended</u>			
	PA&ED	\$720,000	\$699,581			
	PS&E	\$2,365,000	\$2,311,009			
	R/W Sup	\$48,000	\$34,654			
<u>Performance Measure:</u> Planned: 25.0, Actual: 25.0 Lane mile(s) CEQA - CE, 04/21/2022; Re-validation 05/01/2025 NEPA - CE, 04/21/2022; Re-validation 05/01/2025 SB1 Baseline Agreement approval under Resolution SHOPP-P-2324-03B; December 2023.						
Performance Measure: Lane mile(s)						
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>	
Existing Condition	Lane mile(s)	0.0	22.9	2.1	25.0	
Post Condition	Lane mile(s)	25.0	0.0	0.0	25.0	

2.5 Highway Financial Matters

2.5 Highway Financial Matters				PPNO	Program/Year	Phase	Prgm'd Amount	Budget Year	Item # Fund Type	Amount by
Project No.	Allocation Amount	County	Location	Project Description	Project ID	Adv Phase	EA	Program Code	Fund Type	Fund Type
2.5b.(1)	SHOPP Projects				Resolution FP-24-95					
3	\$12,830,000	Shasta 02-Sha-299 88.1/89.5		Near Fall River Mills, from 3.0 miles west to 1.6 miles west of Glenburn Road. Construct 8-foot wide catchment area adjacent to roadway and install rockfall mitigation drapery systems, establish disposal sites, and rehabilitate drainage systems.	02-3784			505-3290 RMRA		\$326,000
					SHOPP/24-25		001-0890 FTF		<u>\$1,304,000</u>	
					CON ENG		20.10.201.407		\$1,630,000	
					\$1,630,000					
					CONST					
				Preliminary	\$9,746,000			2017-18		
				<u>Engineering</u>	0220000055			802-3290 RMRA		\$2,240,000
				PA&ED	3,4			2023-24		
				PS&E	1J290			302-0890 FTF		<u>\$8,960,000</u>
				R/W Sup				20.20.201.407		\$11,200,000
<u>Performance Measure:</u>										
Planned: 1.0, Actual: 1.0 Location(s)										
CEQA - CE, 02/02/2024; Re-validation 05/01/2025										
NEPA - CE, 02/02/2024; Re-validation 05/01/2025										
4	\$30,224,000	Nevada 03-Nev-20 20.0/41.287		Near Nevada City and Emigrant Gap, from 0.15 mile east of Dow Road to Placer County line (PM 20.0/41.287) and from Placer County line to Route 80 (PM 43.868/46.1); also in Placer County from Nevada County line to 0.2 mile east of Lake Spaulding Road (PM 41.287/43.868). Rehabilitate pavement and drainage systems, and upgrade guardrail, signs and Transportation Management System (TMS) elements. This project will extend the pavement service life and improve ride quality.	03-4003			505-3290 RMRA		\$312,000
					SHOPP/24-25		001-0890 FTF		<u>\$2,408,000</u>	
					CON ENG		20.10.201.121		\$2,720,000	
					\$2,720,000					
					CONST					
				Preliminary	\$23,840,000			2017-18		
				<u>Engineering</u>	0319000293			802-3290 RMRA		\$3,155,000
				PA&ED	3,4			2023-24		
				PS&E	0J520			302-0890 FTF		<u>\$24,349,000</u>
				R/W Sup				20.20.201.121		\$27,504,000
<u>Performance Measure:</u>										
Planned: 44.8, Actual: 44.8 Lane mile(s)										
CEQA - ND, 06/10/2024; Re-validation 05/02/2025										
NEPA - CE, 06/19/2024; Re-validation 05/02/2025										
Future consideration of funding approved under Resolution E-24-67; August 2024.										
Performance Measure: Lane mile(s)										

2.5 Highway Financial Matters

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type																								
2.5b.(1)	SHOPP Projects	Resolution FP-24-95																										
5 \$15,566,000 Sacramento 03-Sac-99 6.1/7.0	Near Elk Grove, from 0.1 mile north of Arno Road to 0.4 mile south of Dillard Road at Willow Slough Bridge No. 24-0046L/R. Replace existing south bridge, close the median gap between southbound and northbound bridges, widen shoulders, upgrade guardrail, and install Transportation Management System (TMS) elements. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	03-6936 SHOPP/24-25 CON ENG \$2,280,000 CONST \$11,100,000 0321000023 3,4 1J630	505-3290 RMRA 001-0890 FTF 20.10.201.110 2023-24 302-3290 RMRA 302-0890 FTF 20.20.201.110	\$311,000 <u>\$2,399,000</u> \$2,710,000 \$1,475,000 <u>\$11,381,000</u> \$12,856,000																								
<u>Performance Measure:</u> Planned: 2.0, Actual: 2.0 Bridge(s) CEQA - ND, 03/19/2024; Re-validation 05/01/2025 NEPA - CE, 03/19/2024; Re-validation 05/01/2025 Future consideration of funding approved under Resolution E-24-32; May 2024.																												
<table><tr><td colspan="6">Performance Measure: Bridge(s)</td></tr><tr><td></td><td><u>Unit</u></td><td><u>Good</u></td><td><u>Fair</u></td><td><u>Poor</u></td><td><u>Quantity</u></td></tr><tr><td>Existing Condition</td><td>Square feet</td><td>0.0</td><td>5,102.0</td><td>0.0</td><td>5,102.0</td></tr><tr><td>Post Condition</td><td>Square feet</td><td>4,472.0</td><td>2,648.0</td><td>0.0</td><td>7,120.0</td></tr></table>					Performance Measure: Bridge(s)							<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>	Existing Condition	Square feet	0.0	5,102.0	0.0	5,102.0	Post Condition	Square feet	4,472.0	2,648.0	0.0	7,120.0
Performance Measure: Bridge(s)																												
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>																							
Existing Condition	Square feet	0.0	5,102.0	0.0	5,102.0																							
Post Condition	Square feet	4,472.0	2,648.0	0.0	7,120.0																							
6 \$45,072,000 Marin 04-Mrn-37 14.5	Near Novato, at Petaluma River Bridge No. 27-0013. Rehabilitate bridge deck, upgrade railings, replace fender system, and mitigate bridge scour. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	04-0332C SHOPP/23-24 CON ENG \$7,030,000 CONST \$31,040,000 0419000019 3,4 2Q500	505-3290 RMRA 001-0890 FTF 20.10.201.110 2023-24 302-3290 RMRA 302-0890 FTF 20.20.201.110	\$951,000 <u>\$7,339,000</u> \$8,290,000 \$4,219,000 <u>\$32,563,000</u> \$36,782,000																								
<u>Performance Measure:</u> Planned: 1.0, Actual: 1.0 Bridge(s) CEQA - MND, 12/21/2022; Re-validation 05/27/2025 NEPA - CE, 12/21/2022; Re-validation 05/27/2025 Future consideration of funding approved under Resolution E-23-19; March 2023. Twelve month allocation time extension for CONST and CON ENG approved under Waiver 24-125; August 2024. As part of this allocation request, the Department is requesting to extend the completion of CONST and CON ENG an additional 12 months beyond the 36 month deadline.																												
<table><tr><td colspan="6">Performance Measure: Bridge(s)</td></tr><tr><td></td><td><u>Unit</u></td><td><u>Good</u></td><td><u>Fair</u></td><td><u>Poor</u></td><td><u>Quantity</u></td></tr><tr><td>Existing Condition</td><td>Square feet</td><td>0.0</td><td>0.0</td><td>146,830.0</td><td>146,830.0</td></tr><tr><td>Post Condition</td><td>Square feet</td><td>146,830.0</td><td>0.0</td><td>0.0</td><td>146,830.0</td></tr></table>					Performance Measure: Bridge(s)							<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>	Existing Condition	Square feet	0.0	0.0	146,830.0	146,830.0	Post Condition	Square feet	146,830.0	0.0	0.0	146,830.0
Performance Measure: Bridge(s)																												
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>																							
Existing Condition	Square feet	0.0	0.0	146,830.0	146,830.0																							
Post Condition	Square feet	146,830.0	0.0	0.0	146,830.0																							

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5b.(1)	SHOPP Projects	Resolution FP-24-95		
7 \$1,034,000	In American Canyon, at Rio Del Mar; also near American Canyon at 0.3 mile south of North Kelly Road (PM 5.1). Revegetation mitigation and monitoring for drainage rehabilitation project EA 4J410.	04-2914Q SHOPP/24-25 CON ENG \$201,000	505-3290 RMRA 20.10.201.151	\$201,000
Napa 04-Nap-29 1.7	Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	<u>Budget</u> \$0 \$150,000 \$20,000	<u>Expended</u> \$0 \$146,365 \$3,623	2023-24 302-3290 RMRA 20.20.201.151 \$833,000
<u>Performance Measure:</u> Planned: 0.0, Actual: 0.0 Culvert(s) (ea)				
CEQA - CE, 10/22/2019; Re-validation 03/17/2025 NEPA - CE, 10/22/2019; Re-validation 03/17/2025				
As part of this allocation request, the Department is requesting to extend the completion of CONST and CON ENG an additional 60 months beyond the 36 month deadline.				
8 \$766,000	In and near Yountville, at Dry Creek Bridge No. 21-0014 (PM 16.48), Perfume Creek Bridge No.21-0051 (PM 17.81), and California Drive Undercrossing No. 21-0047 (PM 19.04). Plant establishment and environmental mitigation and monitoring work for bridge rail upgrade project EA 0K630.	04-2917E SHOPP/24-25 CON ENG \$455,000 CONST \$265,000 0423000101 3,4 0K63A	505-3290 RMRA 20.10.201.112 2023-24 302-3290 RMRA 20.20.201.112	\$455,000 \$311,000
Napa 04-Nap-29 16.5/19.0	Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	<u>Budget</u> \$0 \$395,000 \$15,000	<u>Expended</u> \$0 \$131,865 \$11,164	
<u>Performance Measure:</u> Planned: 0.00, Actual: 0.00 Annual fatal and serious injury collision(s)				
CEQA - MND, 10/15/2020; Re-validation 04/17/2025 NEPA - CE, 10/19/2020; Re-validation 04/17/2025				

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5b.(1)	SHOPP Projects	Resolution FP-24-95		
9 \$3,763,000 Napa 04-Nap-121 13.4/20.7	Near Napa, from 2.8 miles north of Vichy Avenue to 1.4 miles south of Route 128 at various locations. Repair and restore slope washouts with Rock Slope Protection (RSP), erosion control, steel sheet piles, and drainage upgrades. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	04-2022D SHOPP/23-24 CON ENG \$1,361,000 CONST \$2,789,000 0418000153 3,4 0Q790	505-3290 RMRA 001-0890 FTF 20.10.201.131 2017-18 802-3290 RMRA 2023-24 302-0890 FTF 20.20.201.131	\$156,000 <u>\$1,205,000</u> \$1,361,000 \$276,000 <u>\$2,126,000</u> \$2,402,000
	<u>Performance Measure:</u> Planned: 5.0, Actual: 5.0 Location(s) CEQA - CE, 11/24/2022; Re-validation 05/01/2025 NEPA - CE, 11/24/2022; Re-validation 05/01/2025 Twelve month allocation time extension for CONST and CON ENG approved under Waiver 24-125; August 2024. Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.			
10 \$864,000 San Mateo 04-SM-280 R0.05	Near Menlo Park, at Alpine Road. Plant establishment, creek monitoring and biological monitoring work for bridge seismic restoration project EA 4J850. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	04-1498R SHOPP/24-25 CON ENG \$507,000 CONST \$520,000 0420000181 3,4 4J85A	505-3290 RMRA 20.10.201.113 2023-24 302-3290 RMRA 20.20.201.113	\$600,000 \$264,000
	<u>Performance Measure:</u> Planned: 0.0, Actual: 0.0 Bridge(s) CEQA - ND, 03/13/2020; Re-validation 03/27/2025 NEPA - CE, 03/13/2020; Re-validation 03/27/2025 As part of this allocation request, the Department is requesting to extend the completion of CONST and CON ENG an additional 12 months beyond the 36 month deadline. Future consideration of funding approved under Resolution E-20-26; May 2020.			

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5b.(1)	SHOPP Projects	Resolution FP-24-95		
11 \$906,000 Sonoma 04-Son-1 30.8/40.6	Near Fort Ross and Walsh Landing, from 2.2 miles south of Fort Ross Road to 0.7 mile north of Gerstle Cove Campground at various locations. Landscaping, plant establishment, monitoring and mitigation work for drainage rehabilitation project 1K730.	04-2919W SHOPP/24-25 CON ENG \$310,000 CONST \$820,000 0423000179	505-3290 RMRA 20.10.201.151 2023-24 302-3290 RMRA 20.20.201.151	\$310,000 \$596,000
	Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	<u>Budget</u> \$0 \$100,000 \$30,000	<u>Expended</u> \$0 \$90,012 \$2,758	
	<u>Performance Measure:</u> Planned: 0.0, Actual: 0.0 Culvert(s) (ea)			
	CEQA - MND, 05/07/2020; Re-validation 04/04/2025 NEPA - CE, 05/28/2020; Re-validation 04/04/2025			
	Future consideration of funding approved under Resolution E-20-48; June 2020.			
	As part of this allocation request, the Department is requesting to extend the completion of CONST and CON ENG an additional 16 months beyond the 36 month deadline.			
12 \$1,164,000 Sonoma 04-Son-1 41.2/54.6	Near Stewarts Point and Sea Ranch, from 1.7 miles south of Kruse Ranch Road to 0.2 mile north of Vantage Road. Landscaping, plant establishment, monitoring and mitigation work for drainage rehabilitation project 1K750.	04-2919X SHOPP/24-25 CON ENG \$625,000 CONST \$578,000 0423000177	505-3290 RMRA 20.10.201.151 2023-24 302-3290 RMRA 20.20.201.151	\$625,000 \$539,000
	Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	<u>Budget</u> \$0 \$605,000 \$50,000	<u>Expended</u> \$0 \$270,756 \$2,054	
	<u>Performance Measure:</u> Planned: 0.0, Actual: 0.0 Culvert(s) (ea)			
	CEQA - MND, 10/06/2020; Re-validation 04/16/2025 NEPA - CE, 10/09/2020; Re-validation 04/16/2025			
	Future consideration of funding approved under Resolution E-20-116; December 2020.			
	As part of this allocation request, the Department is requesting to extend the completion of CONST and CON ENG an additional 16 months beyond the 36 month deadline.			

2.5 Highway Financial Matters

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type																								
2.5b.(1)	SHOPP Projects	Resolution FP-24-95																										
13 \$6,210,000	Near Sebastopol, at Jones Creek Bridge No. 20-0094 (PM 19.90), Blucher Creek Bridge No. 20-0103 (PM 29.83) and Gossage Creek Bridge No. 20-0104 (PM 33.37). Upgrade bridge rails.	04-2028P SHOPP/23-24 CON ENG \$1,740,000	505-3290 RMRA 20.10.201.112	\$1,900,000																								
Sonoma 04-Son-116 19.9/33.4	Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	<u>Budget</u> \$1,380,000 <u>Expended</u> \$1,376,768 \$2,027,504 \$41,998	2023-24 302-3290 RMRA 20.20.201.112	\$4,310,000																								
<u>Performance Measure:</u> Planned: 430.00, Actual: 430.00 Linear feet rail CEQA - MND, 10/02/2023; Re-validation 04/25/2025 NEPA - CE, 09/29/2023; Re-validation 04/25/2025 Future consideration of funding approved under Resolution E-23-144; December 2023. Twelve month allocation time extension for CONST and CON ENG approved under Waiver 24-125; August 2024. As part of this allocation request, the Department is requesting to extend the completion of CONST and CON ENG an additional 12 months beyond the 36 month deadline.																												
<table><tr><td colspan="6">Performance Measure: Linear feet rail</td></tr><tr><td></td><td><u>Unit</u></td><td><u>Good</u></td><td><u>Fair</u></td><td><u>Poor</u></td><td><u>Quantity</u></td></tr><tr><td>Existing Condition</td><td>Linear feet rail</td><td>0.0</td><td>0.0</td><td>430.0</td><td>430.0</td></tr><tr><td>Post Condition</td><td>Linear feet rail</td><td>430.0</td><td>0.0</td><td>0.0</td><td>430.0</td></tr></table>					Performance Measure: Linear feet rail							<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>	Existing Condition	Linear feet rail	0.0	0.0	430.0	430.0	Post Condition	Linear feet rail	430.0	0.0	0.0	430.0
Performance Measure: Linear feet rail																												
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>																							
Existing Condition	Linear feet rail	0.0	0.0	430.0	430.0																							
Post Condition	Linear feet rail	430.0	0.0	0.0	430.0																							
14 \$120,000	Near Los Olivos, at Alamo Pintado Pedestrian Bridge No. 51-0076Y. Biological monitoring for bridge replacement project EA 1C410.	05-2391X SHOPP/24-25 CON ENG \$120,000	505-3290 RMRA 20.10.201.110	\$120,000																								
Santa Barbara 05-SB-154 R2.6	Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	<u>Budget</u> \$0 <u>Expended</u> \$0 \$0 \$0	0522000165 3 1C411																									
<u>Performance Measure:</u> Planned: 0.0, Actual: 0.0 Bridge(s) CEQA - MND, 01/21/2021; Re-validation 05/31/2022 NEPA - CE, 01/14/2021; Re-validation 05/31/2022 Future consideration of funding approved under Resolution E-21-21; March 2021. As part of this allocation request, the Department is requesting to extend the completion of CON ENG an additional 24 months beyond the 36 month deadline.																												

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5b.(1)	SHOPP Projects	Resolution FP-24-95		
15 \$43,000	In Morro Bay, at Toro Creek Bridge. Environmental mitigation (biological monitoring) for project EA 0L721.	05-0072C SHOPP/23-24 CON ENG \$37,000 0519000054 3 0L724	505-3290 RMRA 20.10.201.110	\$43,000
San Luis Obispo 05-SLO-1 32.6	Preliminary <u>Engineering</u> <u>Budget</u> <u>Expended</u> PA&ED \$0 \$0 PS&E \$0 \$0 R/W Sup \$0 \$0			
<u>Performance Measure:</u> Planned: 0.0, Actual: 0.0 Bridge(s) CEQA - MND, 06/22/2018; Re-validation 06/24/2020 NEPA - CE, 06/22/2018; Re-validation 06/24/2020 Future consideration of funding approved under Resolution E-18-94; August 2018. As part of this allocation request, the Department is requesting to extend the completion of CON ENG an additional 24 months beyond the 36 month deadline. Twelve month allocation time extension for CON ENG approved under Waiver 24-125; August 2024.				
16 \$969,000	Near Santa Margarita, from I Street to 0.31 mile east of Route 229. Mitigation project for landscaping for pavement project EA 1J970.	05-2845Y SHOPP/24-25 CON ENG \$418,000 CONST \$400,000 0522000175 3,4 1J971	505-3290 RMRA 20.10.201.121 2017-18 802-3290 RMRA 20.20.201.121	\$497,000 \$472,000
San Luis Obispo 05-SLO-58 1.8/7.2	Preliminary <u>Engineering</u> <u>Budget</u> <u>Expended</u> PA&ED \$0 \$0 PS&E \$748,000 \$362,783 R/W Sup \$400,000 \$19,647			
<u>Performance Measure:</u> Planned: 0.0, Actual: 0.0 Lane mile(s) CEQA - MND, 04/02/2023; Re-validation 02/14/2025 NEPA - CE, 06/26/2024; Re-validation 02/14/2025 Future consideration of funding approved under Resolution E-23-57; May 2023. As part of this allocation request, the Department is requesting to extend the completion of CON and CON ENG an additional 10 months beyond the 36 month deadline.				

2.5 Highway Financial Matters

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type																		
2.5b.(1)	SHOPP Projects	Resolution FP-24-95																				
17 \$10,889,000 Kern 06-Ker-46 51.40/R57.893	Near Wasco and Famoso, from 0.18 mile east of J Street to 0.10 mile east of Route 46/99 Separation. Rehabilitate pavement, upgrade guardrail, highway lighting, remove and replace concrete barrier, and install rumble strips. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	06-8005 SHOPP/24-25 CON ENG \$1,600,000 CONST \$13,165,000 0621000015 3,4 1A760	505-3290 RMRA 001-0890 FTF 20.10.201.121 2017-18 802-3290 RMRA 2023-24 302-0890 FTF 20.20.201.121	\$210,000 <u>\$1,620,000</u> \$1,830,000 \$1,039,000 <u>\$8,020,000</u> \$9,059,000																		
<u>Performance Measure:</u> Planned: 12.8, Actual: 12.8 Lane mile(s)																						
CEQA - CE, 10/17/2023; Re-validation 05/01/2025 NEPA - CE, 10/17/2023; Re-validation 05/01/2025																						
<div>Performance Measure: Lane mile(s)</div> <table><tr><td></td><td><u>Unit</u></td><td><u>Good</u></td><td><u>Fair</u></td><td><u>Poor</u></td><td><u>Quantity</u></td></tr><tr><td>Existing Condition</td><td>Lane mile(s)</td><td>0.0</td><td>12.8</td><td>0.0</td><td>12.8</td></tr><tr><td>Post Condition</td><td>Lane mile(s)</td><td>12.8</td><td>0.0</td><td>0.0</td><td>12.8</td></tr></table>						<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>	Existing Condition	Lane mile(s)	0.0	12.8	0.0	12.8	Post Condition	Lane mile(s)	12.8	0.0	0.0	12.8
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>																	
Existing Condition	Lane mile(s)	0.0	12.8	0.0	12.8																	
Post Condition	Lane mile(s)	12.8	0.0	0.0	12.8																	
18 \$16,700,000 Los Angeles 07-LA-72 0.0/6.5	In and near Whittier, from Valley Home Avenue to Lockheed Avenue. Financial Contribution Only (FCO) to the City of Whittier to relinquish roadway. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	07-5965 SHOPP/24-25 CON ENG CONST \$16,700,000 0722000199 3,4FCO 38700	505-3290 RMRA 20.10.201.160 2017-18 802-3290 RMRA 20.20.201.160	\$0 \$16,700,000																		
<u>Performance Measure:</u> Planned: 6.8, Actual: 6.5 Centerline mile(s)																						
CEQA - CE, 03/21/2022; Re-validation 04/21/2025																						

2.5 Highway Financial Matters

2.5 Highway Financial Matters				PPNO	Program/Year	Phase	Prgm'd Amount	Budget Year	Item # Fund Type	Amount by
Project No.	Allocation Amount	County	Location	Project ID	Adv Phase	EA	Program Code	Fund Type		Fund Type
Dist-Co-Rte			Project Description							
Postmile										
2.5b.(1) SHOPP Projects				Resolution FP-24-95						
19			Near Needles, from Route 40 to 0.5 mile north of Goffs Road.	08-3015T			505-3290 RMRA			\$1,810,000
\$10,076,000			Rehabilitate pavement and replace Asphalt Concrete (AC) dike and rumble strips. This project will extend the pavement service life and improve ride quality.	SHOPP/24-25			20.10.201.121			
				CON ENG						
				\$1,623,000			2017-18			
San Bernardino				CONST			802-3290 RMRA			\$8,266,000
08-SBd-95				\$7,158,000			20.20.201.121			
R57.208/64.5				0819000167						
			Preliminary							
			Engineering							
			PA&ED							
			PS&E							
			R/W Sup							

2.5 Highway Financial Matters

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type																								
2.5b.(1)	SHOPP Projects	Resolution FP-24-95																										
21 \$35,014,000 San Joaquin 10-SJ-4 19.5/38.1	Near Stockton, from Route 99 to Stanislaus County line (PM 38.059); also in Stanislaus County from San Joaquin County line to 0.5 mile east of Dunton Road (PM 0.0/7.2). Rehabilitate pavement, upgrade guardrail, construct rumble strips, upgrade facilities to Americans with Disabilities Act (ADA) standards, rehabilitate drainage systems, install Transportation Management System (TMS) elements, and upgrade bridge railing. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	10-3277 SHOPP/23-24 CON ENG \$5,250,000 CONST \$36,437,000 1017000178 3,4 1C050	505-3290 RMRA 001-0890 FTF 20.10.201.121 2017-18 802-3290 RMRA 2023-24 302-0890 FTF 20.20.201.121	\$602,000 <u>\$4,648,000</u> \$5,250,000 \$3,414,000 <u>\$26,350,000</u> \$29,764,000																								
	<u>Budget</u> \$3,126,000 \$3,171,100 \$657,000	<u>Expended</u> \$3,047,855 \$3,109,372 \$463,494																										
	<u>Performance Measure:</u> Planned: 50.8, Actual: 50.8 Lane mile(s) CEQA - MND, 02/03/2023; Re-validation 04/09/2025 NEPA - FONSI, 02/03/2023; Re-validation 04/09/2025 Future consideration of funding approved under Resolution E-23-62; May 2023. Amendment to time extension for an additional eleven months, for a total of eighteen months, for CONST and CON ENG approved under Waiver 25-43; March 2025. Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.																											
	<table><tr><td colspan="6">Performance Measure: Lane mile(s)</td></tr><tr><td></td><td><u>Unit</u></td><td><u>Good</u></td><td><u>Fair</u></td><td><u>Poor</u></td><td><u>Quantity</u></td></tr><tr><td>Existing Condition</td><td>Lane mile(s)</td><td>4.8</td><td>45.3</td><td>1.9</td><td>52.0</td></tr><tr><td>Post Condition</td><td>Lane mile(s)</td><td>50.8</td><td>0.0</td><td>0.0</td><td>50.8</td></tr></table>				Performance Measure: Lane mile(s)							<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>	Existing Condition	Lane mile(s)	4.8	45.3	1.9	52.0	Post Condition	Lane mile(s)	50.8	0.0	0.0	50.8
Performance Measure: Lane mile(s)																												
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>																							
Existing Condition	Lane mile(s)	4.8	45.3	1.9	52.0																							
Post Condition	Lane mile(s)	50.8	0.0	0.0	50.8																							
22 \$7,338,000 San Diego 11-SD-5 R66.2/R67.4	Near Oceanside, from 4.1 miles north of Las Pulgas Road to the northbound and southbound San Onofre Commercial Vehicle Enforcement Facilities (CVEFs). Upgrade CVEF scales, repair pavement, install new Weigh Station Message Sign (WSMS), automated lane closure system, and update signs and striping. Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	11-1385 SHOPP/24-25 CON ENG \$1,154,000 CONST \$5,337,000 1119000162 3,4 43082	505-3290 RMRA 20.10.201.321 2017-18 802-3290 RMRA 20.20.201.321	\$1,347,000 \$5,991,000																								
	<u>Budget</u> \$684,000 \$1,217,600 \$46,000	<u>Expended</u> \$634,452 \$1,168,702 \$12,779																										
	<u>Performance Measure:</u> Planned: 2.0, Actual: 2.0 Location(s) CEQA - CE, 11/30/2022; Re-validation 02/25/2025 NEPA - CE, 11/30/2022; Re-validation 02/25/2025																											

2.5 Highway Financial Matters

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type	
2.5b.(1)	SHOPP Projects	Resolution FP-24-95			
23 \$17,802,000	In and near Poway, from 0.2 mile north of Riverford Road to Ellie Lane. Rehabilitate culverts, enhance highway worker safety, upgrade lighting, and facilities to Americans with Disabilities Act (ADA) standards.	11-1314 SHOPP/23-24 CON ENG \$3,076,000	505-3290 RMRA 001-0890 FTF 20.10.201.151	\$418,000 \$3,229,000 \$3,647,000	
San Diego 11-SD-67 R4.1/15.9	Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	<u>Budget</u> \$1,440,000 <u>Expended</u> \$1,439,031 \$2,500,000 \$2,395,550 \$505,000 \$450,576	CONST \$11,910,000 1118000095 3,4 43031	2023-24 302-3290 RMRA 302-0890 FTF 20.20.201.151	\$1,624,000 \$12,531,000 \$14,155,000
<u>Performance Measure:</u> Planned: 34.0, Actual: 33.0 Culvert(s) (ea)					
CEQA - CE, 01/20/2023; Re-validation 04/25/2025 NEPA - CE, 01/20/2023; Re-validation 04/25/2025					
Twelve month allocation time extension for CONST and CON ENG approved under Waiver 24-125; August 2024.					
Concurrent Amendment under SHOPP Amendment 24H-012; June 2025					
Performance Measure: Culvert(s) (ea)					
	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Linear feet	0.0	1,566.0	1,697.7	3,263.7
Post Condition	Linear feet	3,263.7	0.0	0.0	3,263.7
24 \$11,400,000	In San Diego County, at Routes 8, 15, 78, and 805 at various locations. Remove and clear debris and vegetation. This project will utilize the Emergency Limited Bid contract method.	11-1610 SHOPP/24-25 CON ENG \$1,900,000	505-3290 RMRA 20.10.201.131	\$1,900,000	
San Diego 11-SD-Var Var	Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	<u>Budget</u> \$0 <u>Expended</u> \$0 \$9,500,000 1125000122 3,4 43278	CONST \$9,500,000 20.20.201.131	2017-18 802-3290 RMRA	\$9,500,000
<u>Performance Measure:</u> Planned: 5.0, Actual: 5.0 Location(s)					
Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.					

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5b.(1)	SHOPP Projects	Resolution FP-24-95		
25	In San Juan Capistrano, Mission Viejo, Laguna Niguel, and Irvine, from 0.4 mile north of Route 74 to Route 405. Rehabilitate pavement and drainage systems, upgrade lighting, enhance highway worker safety, replace overhead sign panels, construct bicycle and pedestrian improvements, construct a park and ride facility, and construct stormwater treatment Best Management Practices (BMPs). This project will extend the pavement service life and improve ride quality.	12-2676A	505-3290 RMRA	\$637,000
\$58,705,000		SHOPP/24-25	001-0890 FTF	<u>\$6,923,000</u>
Orange		CON ENG	20.10.201.121	\$7,560,000
12-Ora-5		\$6,353,000		
10.00/21.30		CONST		
		\$44,544,000	2017-18	
		1220000058	802-3290 RMRA	\$4,312,000
		3,4	2023-24	
		0S380	302-0890 FTF	<u>\$46,833,000</u>
			20.20.201.121	\$51,145,000
	<u>Preliminary Engineering</u>			
	<u>Budget</u>			
	<u>Expended</u>			
	PA&ED	\$4,550,000		\$4,266,429
	PS&E	\$5,555,000		\$2,378,192
	R/W Sup	\$195,000		\$0
	<u>Performance Measure:</u>			
	Planned: 136.4, Actual: 136.4 Lane mile(s)			
	CEQA - CE, 09/22/2023; Re-validation 03/04/2025			
	NEPA - CE, 09/22/2023; Re-validation 03/04/2025			
	SB1 Baseline Agreement approval under Resolution SHOPP-P-2425-01B; August 2024.			

Performance Measure: Lane mile(s)

	<u>Unit</u>	<u>Good</u>	<u>Fair</u>	<u>Poor</u>	<u>Quantity</u>
Existing Condition	Lane mile(s)	32.7	102.6	1.2	136.5
Post Condition	Lane mile(s)	136.4	0.0	0.0	136.4

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5b.(2), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR SHOPP PROJECTS**
PA&ED, PS&E AND R/W SUPPORT
RESOLUTION FP-24-96

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$57,507,000 for Project Approval and Environmental Document (PA&ED), Plans, Specifications, and Estimate (PS&E), and Right of Way (R/W) support for 43 phases programmed in the 2024 State Highway Operation and Protection Program (SHOPP)?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation of \$57,507,000 for 43 support phases in the SHOPP, as follows:

- \$33,991,000 for 35 SHOPP support phases and
- \$23,516,000 for 8 SHOPP Senate Bill 1 (SB 1) support phases.

The attached lists describe 43 SHOPP phases totaling \$57,507,000 for PA&ED, PS&E and R/W support costs that are ready for allocation.

BACKGROUND:

The 2024 SHOPP details both support and construction capital for rehabilitation projects on the State Highway System. The passage of SB 1 necessitates that the Department and the Commission establish baseline budgets for each phase of each project in the 2024 SHOPP and requires an allocation of each support phase on or after July 1, 2017.

FINANCIAL RESOLUTION:

Resolved, that \$57,507,000 be allocated for PA&ED, PS&E, and R/W support for SHOPP projects described on the attached vote lists.

Attachments

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2a)	Support Allocations for SHOPP Projects					Resolution FP-24-96	
1 01-DN-101 35.8/36.5	1147M 0121000070	Near Crescent City, from 0.3 mile south to 0.4 mile north of Smith River (Dr. Ernest M Fine Memorial) Bridge No. 01-0020. Mitigation project for onsite revegetation and monitoring for project EA 43640. <u>Program Code</u> 201.110 - Bridge Rehabilitation and Replacement <u>Performance Measure:</u> 0.0 Bridge(s) Future consideration of funding approved under Resolution E-20-21; March 2020. Twelve month allocation time extension for PS&E approved under Waiver 24-125; August 2024.	43641	25-26	PS&E	\$230,000	\$230,000
2 01-DN-101 18.4/19.3	1156 0122000105	Near Crescent City, from 1.9 miles south to 1.0 mile south of Mill Creek Campground Road. Improve roadway cross slope, widen shoulders, install guardrail, and place High Friction Surface Treatment. <u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.19 Annual fatal and serious injury collision(s) CEQA - CE, 5/5/2025	0M120	26-27	PS&E RW Sup	\$1,167,000 \$168,000	\$1,167,000 \$168,000
3 02-Sha-44 R26.94/R27.62	3921 0224000119	Near Shingletown, from 1.0 mile west to 0.3 mile west of Wilson Hill Road/Emigrant Trail. Widen shoulders, install centerline and shoulder rumble strips, install guardrail, and replace signs. <u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.38 Annual fatal and serious injury collision(s) Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.	1K540	27-28	PA&ED	\$820,000	\$820,000

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2a)	Support Allocations for SHOPP Projects					Resolution FP-24-96	
4 02-Teh-99 13.4/13.9	3919 0224000113	Near Los Molinos, from 0.1 mile south of Butler Street to 0.3 mile north of Taft Street. Widen roadway to construct two way left turn lane, and replace signs, fencing, and culverts. <u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.12 Annual fatal and serious injury collision(s) Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.	1K470	28-29	PA&ED	\$1,020,000	\$1,020,000
5 03-ED-50 48.8/70.0	3368 0324000235	Near Kyburz, Strawberry, and Meyers, from 0.1 mile west of Kyburz Drive to Chiapa Drive. Install curve warning signs, radar feedback signs, chevron signs, roadside signs, and centerline rumblestrips. <u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 3.60 Annual fatal and serious injury collision(s) Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.	0N970	27-28	PA&ED	\$540,000	\$540,000
6 03-Sac-50 Var	8788 0323000230	In Sacramento and Yolo Counties, on Routes 5, 50, 51, 80, and 99, at various locations. Install new trash netting and trash capture housing. <u>Program Code</u> 201.335 - Storm Water Mitigation <u>Performance Measure:</u> 184.0 Acre(s) treated/pollutant CEQA - CE, 5/5/2025	4J690	25-26	PS&E RW Sup	\$2,390,000 \$490,000	\$2,390,000 \$490,000

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2a)	Support Allocations for SHOPP Projects					Resolution FP-24-96	
7 03-Sac-160 L2.73/L6.976	7299 0325000005	Near Rio Vista, from 0.1 mile north of Sherman Island Cross Road to Three Mile Slough. Install centerline rumble strips, place striping, and upgrade guardrail. <u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.70 Annual fatal and serious injury collision(s) Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.	1N340	27-28	PA&ED	\$640,000	\$640,000
8 04-Sol-80 2.33	2922E 0423000325	In Vallejo, at westbound Route 780 Connector to eastbound Route 80. Install additional safety lights, upgrade concrete barrier, power source to flashing beacons, and replace top pavement layer with open-graded asphalt pavement. <u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.07 Annual fatal and serious injury collision(s) Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.	3Y870	27-28	PA&ED	\$877,000	\$877,000
9 04-Sol-220 3.2	2927B 0424000242	Near Howard Landing, at Steamboat Slough Ferry (J-Mack) Bridge No. 23-0037; also on Route 84 at Real McCoy II Ferry Bridge No. 23-0245 (PM2.49). Replace ferries. (Long Lead Project) <u>Program Code</u> 201.999 - Multiple Objective <u>Performance Measure:</u> 0.0 Centerline mile(s) Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.	1X070	30-31	PA&ED	\$4,000,000	\$4,000,000

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2a)	Support Allocations for SHOPP Projects					Resolution FP-24-96	
10 04-Son-116 36.92	2927T 0424000445	In Petaluma, at the intersection with Pine View Way. Construct intersection improvements. <u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.10 Annual fatal and serious injury collision(s) Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.	1X790	28-29	PA&ED	\$914,000	\$914,000
11 04-Son-121 9.54/10.36	2917J 0421000152	Near Schellville, from Ramal Road to 0.4 mile south of Napa Road. Realign 2 horizontal curves, widen shoulders, and construct centerline and shoulder rumble strips. <u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.25 Annual fatal and serious injury collision(s) Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.	2W230	28-29	PA&ED	\$3,651,000	\$3,651,000
12 05-Mon-1 62.97	2696 0516000163	Near Carmel-by-the-Sea, at the Garrapata Creek Bridge No. 44-0018. Replace bridge railing to meet current traffic safety standards. <u>Program Code</u> 201.112 - Bridge Rail Replacement and Upgrade <u>Performance Measure:</u> 0.01 Annual fatal and serious injury collision(s) Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.	1H800	29-30	PA&ED	\$2,600,000	\$2,600,000

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2a)	Support Allocations for SHOPP Projects					Resolution FP-24-96	
13 05-SCr-152 T2.5/0.7	3105 0522000174	In Watsonville, from Freedom Boulevard to Beck Street. Construct bulbouts and high visibility crosswalks and upgrade a pedestrian beacon.	1Q150	25-26	PS&E RW Sup	\$1,896,000 \$46,000	\$2,100,000 \$54,000
<u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.38 Annual fatal and serious injury collision(s) CEQA - CE, 4/22/2025							
14 06-Fre-180 M77.00/R77.70	8053 0623000212	Near the community of Yokuts Valley, from east of George Smith Road to Elwood Road. Oak tree mitigation and replacement planting for EA 1A460.	1A462	25-26	PS&E RW Sup	\$100,000 \$1,000	\$100,000 \$1,000
<u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.00 Annual fatal and serious injury collision(s) CEQA - CE, 4/9/2025							
15 06-Kin-41 28.4/R39.8	8092 0624000107	Near Stratford, from Nevada Avenue to Route 198. Environmental mitigation involving land protection in-perpetuity for pavement rehabilitation project 0W820.	0W822	25-26	PS&E RW Sup	\$80,000 \$250,000	\$80,000 \$250,000
<u>Program Code</u> 201.121 - Pavement Preservation (CAPM) <u>Performance Measure:</u> 0.0 Lane mile(s) CEQA - CE, 6/21/2022							

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2a)	Support Allocations for SHOPP Projects					Resolution FP-24-96	
16 07-LA-2 R17.0/26.72	5973 0721000254	In the cities of Los Angeles, Glendale, La Canada Flintridge, and Pasadena, from Verdugo Road to 0.3 mile west of Angeles Forest; also on Route 210, from 0.2 mile west of Wentworth Street to Washington Boulevard (PM R10.6/R23.5). Construct Best Management Practices (BMPs) to achieve statewide National Pollutant Discharge Elimination System (NPDES) permit compliance units for Total Maximum Daily Load (TMDL).	37930	25-26	PS&E RW Sup	\$2,156,000 \$31,000	\$2,547,000 \$31,000
<u>Program Code</u> 201.335 - Storm Water Mitigation <u>Performance Measure:</u> 83.9 Acre(s) treated/pollutant CEQA - CE, 12/17/2024 Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.							
17 07-LA-5 42.8/R43.6	6127 0723000251	In the city of Los Angeles, near San Fernando, from 0.2 mile north of Roxford Street Undercrossing to 0.2 mile south of San Fernando Road Overhead. Install High Friction Surface Treatment (HFST) and guardrail, rehabilitate pavement consisting of slab replacement, and improve drainage system.	39580	25-26	PS&E RW Sup	\$1,180,000 \$17,000	\$1,321,000 \$17,000
<u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.40 Annual fatal and serious injury collision(s) CEQA - CE, 04/01/2025 Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.							
18 08-Riv-10 R56.3/R56.6	3023K 0824000110	In Indio, from 0.3 mile to 0.6 mile west of Golf Center Parkway. Install guardrail, replace dike, and modify drainage system.	1P770	25-26	PS&E RW Sup	\$1,025,000 \$77,000	\$1,025,000 \$77,000
<u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.15 Annual fatal and serious injury collision(s) CEQA - CE, 4/14/2025							

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2a)	Support Allocations for SHOPP Projects					Resolution FP-24-96	
19 08-Riv-215 R10.89/R11.6	3025A 0824000209	In Murrieta, from 0.2 mile to 1.0 mile north of Los Alamos Road. Upgrade guardrail. <u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.15 Annual fatal and serious injury collision(s) Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.	1R020	26-27	PA&ED	\$1,130,000	\$1,130,000
20 10-SJ-26 10.7	3629 1023000030	In San Joaquin, Merced, and Stanislaus Counties, on Routes 26, 140 and 120 at various locations. Install new and replace existing pedestrian crosswalks and new rectangular rapid flashing beacons. <u>Program Code</u> 201.400 - Complete Streets <u>Performance Measure:</u> 384.0 Bicycle and pedestrian infrastructure (linear feet) CEQA - CE, 4/8/2025	1Q520	25-26	PS&E RW Sup	\$391,000 \$116,000	\$391,000 \$116,000
21 10-Sta-108 26.51	3660 1022000223	In Modesto, at the McHenry Avenue and Coralwood Road intersection. Construct signalized intersection to improve safety. <u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.08 Annual fatal and serious injury collision(s) CEQA - CE, 3/28/2025	1Q030	26-27	PS&E RW Sup	\$1,967,000 \$470,000	\$1,967,000 \$470,000

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2a)	Support Allocations for SHOPP Projects					Resolution FP-24-96	
22 10-Sta-120 8.9/9.5	3540 1021000168	Near Oakdale, east of Orange Blossom Road to east of Wamble Road. Widen roadway to install left-turn channelized lane.	1N330	25-26	PS&E	\$1,176,000	\$1,388,000
					RW Sup	\$149,000	\$149,000
		<u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.06 Annual fatal and serious injury collision(s) Future consideration of funding approved under Resolution E-25-31; May 2025.					
23 11-SD-76 R34.5/R34.7	1439 1121000121	Near Rincon, from 0.3 to 0.5 mile east of Rincon Ranch Road. Widen shoulder, construct retaining wall with safety barrier, remove boulders, and relocate and reconstruct drainage system to improve safety.	43126	26-27	PS&E	\$776,000	\$850,000
					RW Sup	\$374,000	\$420,000
		<u>Program Code</u> 201.010 - Safety Improvements <u>Performance Measure:</u> 0.06 Annual fatal and serious injury collision(s) CEQA - CE, 04/01/2025					

Component	No. of Phases	Total Amount
PA&ED	10	\$16,192,000
PS&E	13	\$15,556,000
R/W Sup	12	\$2,243,000
Total	35	\$33,991,000

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2b)	Support Allocations for SHOPP SB 1 Projects of Primary Asset Classes					Resolution FP-24-96	
1 02-Sis-263 54.5/56.4	3785 0220000059	Near Yreka, at Dry Gulch Bridge No. 02-0012 and Shasta River Bridge No. 02-0014. Rehabilitate bridges.	1J330	26-27	PS&E RW Sup	\$740,000 \$30,000	\$881,000 \$30,000
<u>Program Code</u> 201.110 - Bridge Rehabilitation and Replacement							
<u>Performance Measure:</u> 2.0 Bridge(s)							
CEQA - CE, 4/16/2025							
Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.							
2 04-Son-128 L4.859/24.761	2027A 0419000012	Near Geyserville and Calistoga, from Route 101 to Napa County line. Rehabilitate pavement, upgrade guardrail, and upgrade facilities to Americans with Disabilities Act (ADA) standards.	2Q430	26-27	PS&E RW Sup	\$4,892,000 \$151,000	\$4,892,000 \$151,000
<u>Program Code</u> 201.121 - Pavement Preservation (CAPM)							
<u>Performance Measure:</u> 41.3 Lane mile(s)							
CEQA - CE, 04/28/2025							
Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.							

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2b)		Support Allocations for SHOPP SB 1 Projects of Primary Asset Classes				Resolution FP-24-96	
3 09-Ker-178 92.0/R93.4	2685 0919000069	In and near Ridgecrest, from Redrock Inyokern Road to Clodt Road; also from 0.1 mile west of Mahan Street to the San Bernardino County line (99.0/104.6). Rehabilitate pavement and drainage systems, upgrade lighting and Transportation Management System (TMS) elements, replace guardrail and sign panels, upgrade facilities to Americans with Disabilities Act (ADA) standards, and construct Class 2 bike lanes as complete street elements.	38330	29-30	PS&E	\$4,400,000	\$4,400,000
		RW Sup			\$4,180,000	\$4,180,000	
		<p><u>Program Code</u> 201.121 - Pavement Preservation (CAPM)</p> <p><u>Performance Measure:</u> 23.8 Lane mile(s)</p> <p>Concurrent Amendment under SHOPP Amendment 24H-012; June 2025.</p> <p>Future consideration of funding approved under Resolution E-24-76; August 2024.</p> <p>As part of this allocation request, the Department is requesting to extend the completion of PS&E an additional 24 months beyond the 36 month deadline.</p> <p>As part of this allocation request, the Department is requesting to extend the completion of the R/W Sup phase an additional 24 months beyond the 72 month deadline.</p>					
4 12-Ora-39 0.0/8.5	3237 1218000084	In Huntington Beach and Westminster, from Route 1 to Route 22. Rehabilitate pavement by grinding and placing Rubberized Hot Mix Asphalt (RHMA), rehabilitate drainage systems, and upgrade curb ramps, crosswalks, and lighting.	0R360	26-27	PS&E	\$3,936,000	\$4,684,000
		RW Sup			\$3,612,000	\$4,298,000	
		<p><u>Program Code</u> 201.121 - Pavement Preservation (CAPM)</p> <p><u>Performance Measure:</u> 55.7 Lane mile(s)</p> <p>CEQA - CE, 04/24/2025</p>					

2.5 Highway Financial Matters

Project No. Dist-Co-Rte Postmile	PPNO Project ID	Location/Description	EA	Program Year	Phase	Programmed Amount	Allocation Amount
2.5b.(2b)	Support Allocations for SHOPP SB 1 Projects of Primary Asset Classes						Resolution FP-24-96

Component	No. of Phases	Total Amount
PA&ED	0	\$0
PS&E	4	\$14,857,000
R/W Sup	4	\$8,659,000
Total	8	\$23,516,000

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5b.(5), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: ALLOCATION FOR A SHOPP PROJECT
RESOLUTION FP-24-105

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$387,560,000 for the State Highway Operation and Protection Program (SHOPP) Major Damage (Permanent Restoration) project (PPNO 3636), in Mariposa County?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation of \$387,560,000 for the SHOPP Major Damage (Permanent Restoration) project (PPNO 3636), in Mariposa County.

BACKGROUND:

The attached vote list describes the SHOPP project totaling \$387,560,000. The Department is ready to proceed with this project, and is requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved that \$364,560,000 be allocated from the Budget Act of 2023, Budget Act Item 2660-302-0890 and Non-Budget Act Item 2660-802-3290 for construction and \$23,000,000 for construction engineering for the SHOPP project described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project No. Allocation Amount County Dist-Co-Rte Postmile	Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5b.(5)	SHOPP Project	Resolution FP-24-105		
1 \$387,560,000	Near El Portal and Yosemite National Park, from 1.2 miles west to 0.5 mile west of South Fork Merced River. <u>Outcome/Outputs</u> : Construct rock shed. This is a Construction Manager/General Contractor (CMGC) project.	10-3636 SHOPP/24-25 CON ENG \$23,000,000	505-3290 RMRA 001-0890 FTF 20.10.201.131	\$2,638,000 <u>\$20,362,000</u> \$23,000,000
Mariposa 10-Mpa-140 42.00/42.70	Preliminary <u>Engineering</u> PA&ED PS&E R/W Sup	<u>Budget</u> \$0 \$3,610,000 \$100,000	<u>Expended</u> \$0 \$3,303,540 \$33,255	2017-18 802-3290 RMRA 2023-24 302-0890 FTF 20.20.201.131
<u>Performance Measure:</u> Planned: 1.0, Actual: 1.0 Location(s)				
CEQA - EIR, 01/28/2014; Re-validation 04/15/2025 NEPA - EIS, 01/28/2014; Re-validation 04/15/2025				
Future consideration of funding approved under Resolution E-14-20; May 2014.				
Concurrent Amendment under SHOPP Amendment 24H-013; June 2025.				
As part of this allocation request, the Department is requesting to extend the completion of CONST and CON ENG an additional 44 months beyond the 36 month deadline.				

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5b.(3), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **RIGHT OF WAY CAPITAL ALLOCATION FOR A SHOPP PROJECT
RESOLUTION FP-24-97**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$11,276,000 for the Right of Way (RW) Capital phase for the State Highway Operation and Protection Program (SHOPP) Major Damage Restoration project (PPNO 2917N), on State Route (SR) 1, in San Mateo County?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation of \$11,276,000 for the RW Capital phase for the SHOPP Major Damage Restoration project (PPNO 2917N), on SR 1, in San Mateo County.

BACKGROUND:

Per Resolution G-19-01, projects with RW Capital programming of \$10 million or more, and that are programmed with RW Capital for Fiscal Year 2020-21 or later, must receive Commission approval for project level allocation.

This project is located in and near Pacifica, from 14th Street to Linda Mar Boulevard, in the County of San Mateo. This project will set up and fund an endowment to support the environmental mitigation activities and commitments made for the Devil's Slide Project, which was completed on March 26, 2013. This project provides biological monitoring and mitigation of the existing environmental assets, but no physical or engineering improvements will be made. The National Fish and Wildlife Foundation has been identified as the endowment manager/holder, and San Mateo Resource Conservation District shall be the easement manager/holder.

Although this allocation request is for the full \$11,276,000, the funds will be committed over multiple fiscal years.

FINANCIAL RESOLUTION:

Resolved, that \$11,276,000 be allocated from Non-Budget Act Item 2660-802-3290, for the RW Capital phase for the SHOPP project on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project No.	Location	PPNO	Budget Year	Amount by
Allocation Amount	Project Description	Program/Year	Item # Fund Type	Fund Type
County		Phase		
Dist-Co-Rte		Prgm'd Amount		
Postmile		Project ID		
		Adv Phase		
		EA	Program Code	
2.5b.(3)	SHOPP Right of Way Capital Project			Resolution FP-24-97
1	In and near Pacifica, from 14th Street to Linda Mar Boulevard.	04-2917N	2017-18	
\$11,276,000	<u>Outcome/Outputs:</u> Plant establishment mitigation for permanent restoration project EA 1123U.	SHOPP/24-25	802-3290 RMRA	\$11,276,000
		R/W	20.20.201.131	
		\$11,276,000		
San Mateo	Preliminary	0420000334		
04-SM-1	<u>Engineering</u>			
36.0/41.0	<u>Budget</u>			
	PA&ED			
	\$320,000			
	PS&E			
	\$532,000			
	<u>Expended</u>			
	\$310,195			
	\$207,424			
	<u>Performance Measure:</u>			
	Planned: 1.0, Actual: 1.0 Location(s)			
	CEQA - FEIR, 04/19/2002; Re-validation 11/08/2005			
	NEPA - EIS, 04/30/2002; Re-validation 11/08/2005			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5c.(3), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR LOCALLY-ADMINISTERED STIP PROJECTS
OFF THE STATE HIGHWAY SYSTEM
RESOLUTION FP-24-99**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$8,297,000 for nine locally-administered State Transportation Improvement Program (STIP) projects, off the State Highway System?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$8,297,000 for nine locally-administered STIP projects as follows:

2.5c.(3a) - \$4,839,000 for seven STIP projects.

2.5c.(3b) - \$3,458,000 for two STIP Planning, Programming, and Monitoring projects.

BACKGROUND:

The attached vote list describes nine locally-administered STIP projects totaling \$8,297,000. The local agencies are ready to proceed with these projects, and are requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$8,297,000 be allocated from the Budget Act of 2024, Budget Act Item 2660-101-0042 and 2660-101-0890 for nine locally-administered STIP projects described on the attached vote list.

Attachments

2.5 Highway Financial Matters

Project #		PPNO	Budget Year	
Allocation Amount		Program/Year	Item #	
Recipient	Project Title	Phase	Fund Type	Amount by
<u>RTPA/CTC</u>	Location	Prgm'd Amount	Program Code	Fund Type
District-County	Project Description	Project ID		

2.5c.(3a)

Locally-Administered STIP Projects Off the State Highway System

Resolution FP-24-99

1	Main Street, Patrick's Point Drive, and Westhaven Drive Pavement Rehabilitation. In Trinidad, on Main Street, Patricks Point Drive, and Westhaven Drive. Replace non-accessible sidewalks, driveways and curb ramps, rehabilitate asphalt concrete pavement, and install signage, striping and pavement markings.	01-4913 RIP/24-25 PS&E \$47,000 0124000206	2024-25 101-0042 SHA 20.30.600.620	\$47,000
\$47,000				
City of Trinidad				
<u>HCAOG</u>				
01-Humboldt				
<u>Outputs</u> Local road - rehabilitated Repair existing sidewalk Repair/Upgrade curb ramp Modify crosswalk Modify driveway		<u>Unit</u> Miles LF Each LF LF	<u>Total</u> 0.5 300 2 30 90	
CEQA - CE, 04/22/2025				
2	City of Susanville - Rehab FE. In the City of Susanville on Russell Avenue from Main Street to Paul Bunyan Road, Fair Drive from Main Street to Russell Avenue, Johnstonville Road from Skyline Road to Riverside Drive, S Lassen Street from Main Street to Susan River, Alexander Ave from Riverside Drive to Modoc Street, and Paiute Lane from Glenn Drive to Skyline Road. Rehabilitate roadway, repair or reconstruct surface drainage improvements, repair subgrade baserock structural failures and upgrade and construct ADA accessible pedestrian facilities.	02-2653 RIP/24-25 PS&E \$100,000 0225000136	2024-25 101-0042 SHA 20.30.600.621	\$100,000
\$100,000				
City of Susanville				
<u>LCTC</u>				
02-Lassen				
<u>Outputs</u> Repair existing sidewalk Repair/Upgrade curb ramp Roadway lane miles - new		<u>Unit</u> LF Each Miles	<u>Total</u> 285 28 1.9	
CEQA - CE, 04/29/2025				

2.5 Highway Financial Matters

Project #		PPNO	Budget Year	
Allocation Amount		Program/Year	Item #	
Recipient	Project Title	Phase	Fund Type	Amount by
<u>RTPA/CTC</u>	Location	Prgm'd Amount	Program Code	Fund Type
District-County	Project Description	Project ID		

2.5c.(3a)

Locally-Administered STIP Projects Off the State Highway System

Resolution FP-24-99

3	East 4th Street Rehab. East 4th Street between East Street and Josephine Street, in the City of Alturas. Rehabilitate the existing roadway with either removal and replacement of the existing pavement surface, or utilizing Full Depth Reclamation of the existing structural section. Other work will include curb ramp construction, curb, gutter and sidewalk upgrades, modifications to existing storm drain facilities, utility cover adjustment/replacement work, roadside signing, pavement delineation, and guard railing upgrades.	02-2644 RIP/24-25 PA&ED \$1,000 0225000137	2024-25 101-0042 SHA 20.30.600.621	\$1,000
\$1,000				
City of Alturas				
<u>MCTC</u>				
02-Modoc				
<u>Outputs</u>		<u>Unit</u>	<u>Total</u>	
Local Road - rehabilitated		Miles	0.08	
4	Beckwourth-Calpine Road Pavement Rehabilitation. From Sierra County Line to the intersection with Marble Hot Springs Road. Pavement grinding and overlay work for the construction phase for a paving contract and construction inspection and material testing by consultant.	02-2647 RIP/24-25 CONST \$1,516,000 0225000150	2024-25 101-0042 SHA 20.30.600.621	\$1,516,000
\$1,516,000				
Plumas County				
<u>PCTC</u>				
02-Plumas				
<u>Outputs</u>		<u>Unit</u>	<u>Total</u>	
Local Road - rehabilitated		Miles	7.6	
CEQA - CE, 01/27/2025				
Right of Way Certification: 4/29/2025				

2.5 Highway Financial Matters

Project #		PPNO		
Allocation Amount		Program/Year	Budget Year	
Recipient	Project Title	Phase	Item #	
<u>RTPA/CTC</u>	Location	Prgm'd Amount	Fund Type	Amount by
District-County	Project Description	Project ID	Program Code	Fund Type

2.5c.(3a)

Locally-Administered STIP Projects Off the State Highway System

Resolution FP-24-99

5	Ripple Creek Bridge Reconstruction. The goal of the project is to replace the existing bridge with a new bridge designed to meet the Caltrans Bridge Design Specifications and the AASHTO very low volume bridge standards. The County desires the most cost-effective solution, such as a prefabricated steel or concrete structure placed upon a simple foundation. Another important design consideration of the project is to minimize the environmental process by avoiding any work within the creek channel. Ideally the new bridge could just drop in place, with a slightly longer span than the old bridge, avoiding the creek envelope and wider than the existing structure to better align with the existing roadway, therefore not requiring approach roadways. Ripple Creek Bridge is located in northern Trinity County on Eagle Creek Loop Road approximately 1.5 miles northwest of SR 3.	02-2654	2024-25		
\$400,000		RIP/24-25	101-0042 SHA	\$350,000	
Trinity County		PS&E	20.30.600.620		
<u>TrinityCTC</u>		\$350,000			
02-Trinity		R/W	2024-25		
		\$50,000	101-0042 SHA	\$50,000	
		0225000013	20.30.600.620		
<u>Outputs</u>					
Local reconstructed					
bridge(s)/tunnel(s)					
	SQFT	336			
CEQA - CE, 03/03/2025					
<hr/>					
6	Cowell Boulevard Improvements. In the City of Davis, Cowell Boulevard between I-80 interchange and Research Park Drive: Improve 0.75 miles of Cowell Boulevard with pavement rehabilitation, narrowed traveled lanes, improved buffered and separated bikeways, enhanced bus stops, and improved intersection control and pedestrian crossings.	03-5328	2024-25		
\$75,000		RIP/24-25	101-0042 SHA	\$75,000	
City of Davis		PA&ED	20.30.600.620		
<u>SACOG</u>		\$75,000			
03-Yolo		0325000195			
<u>Outputs</u>					
Local road - rehabilitated					
		Miles	0.75		
<hr/>					

2.5 Highway Financial Matters

Project #		PPNO	Budget Year	
Allocation Amount		Program/Year	Item #	
Recipient	Project Title	Phase	Fund Type	Amount by
<u>RTPA/CTC</u>	Location	Prgm'd Amount	Program Code	Fund Type
District-County	Project Description	Project ID		

2.5c.(3a)

Locally-Administered STIP Projects Off the State Highway System

Resolution FP-24-99

7	Jamestown Safe, Healthy and Equitable Street Improvements. The project proposes a number of improvements. All three possible alternatives include transit stops, a park & ride facility, and sidewalks. Alternatives 1 & 2 propose to widen SR 49 to five lanes in project area. Alternative 3 does not propose any widening but instead adds operational improvements on SR 49 including signal interconnect and other geometric improvements to the Fifth Avenue, Rawhide, and Main Street intersections. The location of the widening is in Tuolumne County in the unincorporated community of Jamestown, CA from 0.2 miles south of Main Street to 0.2 miles north of Fifth Avenue. The anticipated Environmental Documents are Environmental Impact Report for CEQA and Environmental Assessment for NEPA.	10-3489	2024-25	
\$2,700,000		RIP/24-25	101-0890 FTF	\$2,700,000
		PA&ED	20.30.600.620	
Tuolumne County		\$2,700,000		
Transportation		1025000198		
Council				
<u>TuolumneCTC</u>				
10-Tuolumne				
<u>Outputs</u>		<u>Unit</u>	<u>Total</u>	
Pedestrian/Bicycle facilities constructed		Miles	1	

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5c.(3b)	Local STIP Planning, Programming and Monitoring Projects		Resolution FP-24-99	
1 \$33,000 Tahoe Regional Planning Agency <u>TRPA</u> 03-El Dorado	Planning, Programming, and Monitoring.	03-5650 RIP/24-25 CONST \$33,000 0325000235	2024-25 101-0042 SHA 20.30.600.670	\$33,000
2 \$3,425,000 Los Angeles County Metropolitan Transportation Authority <u>LACMTA</u> 07-Los Angeles	Planning, Programming and Monitoring.	07-9001 RIP/24-25 CONST \$3,425,000 0725000214	2024-25 101-0042 SHA 20.30.600.670	\$3,425,000

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5c.(6), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR LOCALLY-ADMINISTERED STIP PROJECTS
OFF THE STATE HIGHWAY SYSTEM
(ADVANCEMENT FISCAL YEAR 2025-26)
RESOLUTION FP-24-101**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$4,192,000 for 10 locally-administered State Transportation Improvement Program (STIP) Planning, Programming, and Monitoring (PPM) projects, off the State Highway System, programmed in Fiscal Year 2025-26?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$4,192,000 for 10 locally-administered STIP PPM projects, off the State Highway System, programmed in 2025-26.

BACKGROUND:

The attached vote list describes 10 locally-administered STIP PPM projects totaling \$4,192,000. The local agencies are ready to proceed with these projects, and are requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$4,192,000 be allocated from the Budget Act 2024, Budget Act Item 2660-101-0042 for 10 locally-administered STIP PPM projects described on the attached vote list.

Attachments

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5c.(6)	Local STIP Planning, Programming, and Monitoring Projects (Advancement FY 2025-26)		Resolution FP-24-101	
1 \$47,000 Modoc County Transportation Commission <u>MCTC</u> 02-Modoc	Planning, Programming, and Monitoring.	02-2051 RIP/25-26 CONST \$47,000 0225000141	2024-25 101-0042 SHA 20.30.600.670	\$47,000
2 \$75,000 Siskiyou County Transportation Commission <u>SCLTC</u> 02-Siskiyou	Planning, Programming and Monitoring.	02-2440 RIP/25-26 CONST \$75,000 0225000148	2024-25 101-0042 SHA 20.30.600.670	\$75,000
3 \$210,000 Placer County Transportation Planning Agency <u>PCTPA</u> 03-Placer	Planning, Programming, and Monitoring.	03-0L11 RIP/25-26 CONST \$210,000 0325000215	2024-25 101-0042 SHA 20.30.600.670	\$210,000
4 \$29,000 Sierra County Transportation Commission <u>SCTC</u> 03-Sierra	Planning, Programming, and Monitoring.	03-0L04 RIP/25-26 CONST \$29,000 0325000226	2024-25 101-0042 SHA 20.30.600.670	\$29,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5c.(6)	Local STIP Planning, Programming, and Monitoring Projects (Advancement FY 2025-26)		Resolution FP-24-101	
5 \$1,015,000 Sacramento Area Council of Governments <u>SACOG</u> 03-Various	Planning, Programming and Monitoring. This allocation combines 4 projects programmed in the 2025-2026 STIP: PPNO 0L41 (Yuba) for \$53,000 PPNO 0L37 (Yolo) for \$143,000 PPNO 1L53 (Sutter) for \$67,000 PPNO 0L30 (Sacramento) for \$752,000	03-VARIOUS RIP/25-26 CONST \$1,015,000 0325000230	2024-25 101-0042 SHA 20.30.600.670	\$1,015,000
6 \$58,000 Council of San Benito County Governments <u>AMBAG</u> 05-San Benito	Planning, Programming and Monitoring.	05-2043 RIP/25-26 CONST \$58,000 0525000223	2024-25 101-0042 SHA 20.30.600.670	\$58,000
7 \$294,000 San Luis Obispo Council of Governments <u>SLOCOG</u> 05-San Luis Obispo	Planning, Programming, and Monitoring.	05-0942 RIP/25-26 CONST \$294,000 0525000204	2024-25 101-0042 SHA 20.30.600.670	\$294,000
8 \$487,000 Ventura County Transportation Commission <u>VCTC</u> 07-Ventura	Planning, Programming and Monitoring.	07-9002 RIP/25-26 CONST \$487,000 0725000237	2024-25 101-0042 SHA 20.30.600.670	\$487,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5c.(6)	Local STIP Planning, Programming, and Monitoring Projects (Advancement FY 2025-26)		Resolution FP-24-101	
9 \$1,495,000 San Bernardino County Transportation Authority <u>SBCTA</u> 08-San Bernardino	Planning, Programming and Monitoring.	08-9811 RIP/25-26 CONST \$1,495,000 0825000149	2024-25 101-0042 SHA 20.30.600.670	\$1,495,000
10 \$482,000 San Joaquin Council of Governments <u>SJCOG</u> 10-San Joaquin	Planning Programming Monitoring.	10-7952 RIP/25-26 CONST \$482,000 1025000187	2024-25 101-0042 SHA 20.30.600.670	\$482,000

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 25-26, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5s.(4), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR A LOCALLY-ADMINISTERED SENATE BILL 1 LOCAL PARTNERSHIP PROGRAM (FORMULAIC) PROJECT OFF THE STATE HIGHWAY SYSTEM (ADVANCEMENT FISCAL YEAR 2025-26) RESOLUTION LPP-A-2425-32**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$311,000 for the locally-administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) Bowker Road Rehabilitation project (PPNO 1615), off the State Highway System, in Imperial County, programmed in Fiscal Year 2025-26?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$311,000 for the locally-administered SB 1 LPP (Formulaic) Bowker Road Rehabilitation project (PPNO 1615), off the State Highway System, in Imperial County, programmed in 2025-26.

BACKGROUND:

The attached vote list describes the locally-administered SB 1 LPP (Formulaic) project totaling \$311,000. The local agency is ready to proceed with this project, and is requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$311,000 be allocated from the Non-Budget Act Item 2660-601-3290 for the locally-administered SB 1 LPP (Formulaic) project described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type						
2.5s.(4)	Locally-Administered SB 1 LPP (Formulaic) Project Off the State Highway System (Advancement Fiscal Year 2025-26)		Resolution LPP-A-2425-32							
1 \$311,000 County of Imperial <u>SCAG</u> 11-Imperial	Bowker Road Rehabilitation. In the County of Imperial, near the City of Calexico, on Bowker Road from Jasper Road to Fawcett Road within the existing right of way of Bowker Road. Construct improvements to existing roadway including one-inch grinding of deteriorated pavement, asphalt concrete overlay pavement, street striping, and road shoulder backing. The project will rehabilitate approximately one mile of Bowker Road.	11-1615 LPP-F/25-26 CONST \$311,000 1125000231	2017-18 601-3290 RMRA 20.30.210.200	\$311,000						
<table><tr><td><u>Outputs</u></td><td><u>Unit</u></td><td><u>Total</u></td></tr><tr><td>Local Road - Rehabilitated</td><td>Miles</td><td>1</td></tr></table>					<u>Outputs</u>	<u>Unit</u>	<u>Total</u>	Local Road - Rehabilitated	Miles	1
<u>Outputs</u>	<u>Unit</u>	<u>Total</u>								
Local Road - Rehabilitated	Miles	1								
CEQA - CE, 03/27/2025										
Right of Way Certification: 4/23/2025										
Contribution from other sources: \$311,000.										

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6s.(4), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR A LOCALLY-ADMINISTERED MULTI-FUNDED SENATE
BILL 1 LOCAL PARTNERSHIP PROGRAM (FORMULAIC), STATE
TRANSPORTATION IMPROVEMENT PROGRAM, AND TRANSIT AND
INTERCITY RAIL CAPITAL PROGRAM PROJECT
RESOLUTION LPP-A-2425-35
RESOLUTION FP-24-102
RESOLUTION TIRCP-2425-73**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$727,267,000 for the locally-administered multi-funded Senate Bill 1 (SB 1) Local Partnership Program Formulaic (LPP) (Formulaic), State Transportation Improvement Program (STIP), and Transit and Intercity Rail Capital Program (TIRCP) East San Fernando Valley Transit Corridor project (PPNO 4296), in Los Angeles County?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$727,267,000 for the locally-administered multi-funded SB 1 LPP (Formulaic), STIP, and TIRCP East San Fernando Valley Transit Corridor project (PPNO 4296), in Los Angeles County.

BACKGROUND:

The attached vote list describes the locally-administered multi-funded SB 1 LPP (Formulaic), STIP, and TIRCP project totaling \$727,267,000. The local agency is ready to proceed with this project, and is requesting an allocation at this time; however, the allocation is contingent upon the passage of the 2025-26 Budget Act.

FINANCIAL RESOLUTION

Resolved, that \$727,267,000 be allocated from the Budget Acts of 2023 and 2025, Budget Act Items 2660-101-0046 and 2660-301-0046R and Non-Budget Act Item 2660-601-3290 for the locally-administered multi-funded SB 1 LPP (Formulaic), STIP, and TIRCP project described on the attached vote list.

Attachment

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6s.(4) Multi-Funded Senate Bill 1 - LPP (Formulaic), STIP and TIRCP Transit Project			Resolution LPP-A-2425-35 Resolution FP-24-102 Resolution TIRCP-2425-73	
1 \$727,267,000 Los Angeles County Metropolitan Transportation Authority <u>LACMTA</u> 07-Los Angeles	East San Fernando Valley Transit Corridor. Construct north-south transit corridor to improve transit service in eastern San Fernando Valley. <u>Outputs</u> Miles of new track New stations CEQA - EIR, 01/31/2021 Future consideration of funding approved under Resolution E-23-41; March 2023. Contribution from other sources: \$2,596,934,000. The current scope of work consists of a Progressive Design-Build (PDB) project delivery method. Cycle 3 TIRCP awards are for (2018:10) Los Angeles Region Transit System Integration and Modernization Program (East San Fernando Valley Transit Corridor) awarded on April 26, 2018 with funding split: GGRF \$90,897,000 and PTA \$114,102,000. In January 2023, this project was awarded \$600,000,000 in Cycle 6 General Fund Augmentation funding. LPP (Formulaic) and STIP Time Extensions for FY 23-24 CONST expire 06/30/2025. Concurrent TIRCP R/W allocation under Resolution TIRCP-2425-74; June 2025. Reimbursement schedule for the \$484,814,000 programmed amount is as follows: FY 24-25: \$25,000,000 FY 25-26: \$254,815,000 FY 26-27: \$204,999,000 As part of this allocation request, the local agency is requesting to extend the period of contract award an additional 6 months beyond the 6-month deadline. As part of this allocation request, the local agency is requesting to extend the period of project completion an additional 12 months beyond the 36-month deadline. <u>ALLOCATION IS CONTINGENT UPON THE PASSAGE OF THE 2025-26 BUDGET ACT.</u>	07-4296 LPP-F/23-24 CONST \$74,944,000 RIP/23-24 CONST \$167,509,000 TIRCP/24-25 CONST \$90,897,000 TIRCP/24-25 CONST \$114,102,000 TIRCP/24-25 CONST \$279,815,000 S 0725000269 T244GF	2017-18 601-3290 RMRA 30.10.724.100 2025-26 101-0046 PTA 30.10.070.625 2023-24 301-0046R GGRF 30.20.301.100 2025-26 101-0046 PTA 30.10.030.200 2023-24 301-0046R GF 30.20.301.100	\$74,944,000 \$167,509,000 \$90,897,000 \$114,102,000 \$279,815,000

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6s.(3), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR A LOCALLY-ADMINISTERED SENATE BILL 1
LOCAL PARTNERSHIP PROGRAM (COMPETITIVE) TRANSIT PROJECT
RESOLUTION LPP-A-2425-36**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$3,000,000 for the locally-administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Competitive) Southeast Los Angeles Transit Improvements Project (Bus Shelters) (PPNO LP011B), in Los Angeles County?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$3,000,000 for the locally-administered SB 1 LPP (Competitive) Southeast Los Angeles Transit Improvements Project (Bus Shelters) (PPNO LP011B), in Los Angeles County.

BACKGROUND:

The attached vote list describes the locally-administered SB 1 LPP (Competitive) Transit project totaling \$3,000,000. The local agency is ready to proceed with this project, and is requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$3,000,000 be allocated from Non-Budget Act Item 2660-601-3290 for the locally-administered SB 1 LPP (Competitive) Transit project described on the attached vote list.

Attachment

2.6 Mass Transportation Financial Matters

Project #	Allocation Amount	Project Title	PPNO	Budget Year	Amount by
Recipient		Location	Program/Year	Item #	Fund Type
<u>RTPA/CTC</u>			Phase	Fund Type	Amount by
District-County		Project Description	Prgm'd Amount	Program Code	Fund Type
			Project ID		
2.6s.(3)		Senate Bill 1 - LPP (Competitive) Transit Project		Resolution LPP-A-2425-36	
1					
\$3,000,000		Southeast Los Angeles Transit Improvements Project (Bus Shelters). Purchase and install bus shelters with real-time display and security lighting in NextGen Tier 1 and 2 high frequency corridors throughout I-710 South communities in Los Angeles County.	07-LP011B	2017-18	\$3,000,000
Los Angeles County			LPP-C/24-25	601-3290 RMRA	
Metropolitan			CONST	30.10.724.200	
transportation			\$3,000,000		
Authority			0725000268		
<u>LACMTA</u>			T654GA		
07-Los Angeles					
		<u>Outputs</u>	<u>Unit</u>	<u>Total</u>	
		Station Improvements	EA	100	
		CEQA - SE, 12/28/2023			
		Right of Way Certification: 1/31/2025			
		Contribution from other sources: \$3,000,000.			
		As part of this allocation request, the local agency is requesting to extend the period of contract award an additional 6 months beyond the 6-month deadline.			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5s.(5), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: ALLOCATION FOR A LOCALLY-ADMINISTERED MULTI-FUNDED
SENATE BILL 1 LOCAL PARTNERSHIP PROGRAM (FORMULAIC) AND
TRADE CORRIDOR ENHANCEMENT PROGRAM PROJECT ON THE STATE
HIGHWAY SYSTEM (ADVANCEMENT FISCAL YEAR 2025-26)
RESOLUTION LPP-A-2425-33
RESOLUTION TCEP-A-2425-24

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$87,731,000 for the locally-administered multi-funded Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) and SB 1 Trade Corridor Enhancement Program (TCEP) Interstate 10 Corridor Freight and Managed Lane Project: I-15 to Sierra Avenue, Contract 2A (PPNO 1325), on the State Highway System, in San Bernardino County, programmed in Fiscal Year 2025-26?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$87,731,000 for the locally-administered multi-funded SB 1 LPP (Formulaic) and SB 1 TCEP Interstate 10 Corridor Freight and Managed Lane Project: I-15 to Sierra Avenue, Contract 2A (PPNO 1325), on the State Highway System, in San Bernardino County, programmed in 2025-26.

BACKGROUND:

The attached vote list describes the locally-administered multi-funded SB 1 LPP (Formulaic) and SB 1 TCEP project vote box totaling \$87,731,000. The local agency is ready to proceed with this project, and is requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$87,731,000 be allocated from the Budget Act of 2024, Budget Act Items 2660-301-3291 and 2660-302-0890, and Non-Budget Act Item 2660-801-3290 for the locally-administered multi-funded SB 1 LPP (Formulaic) and SB 1 TCEP project described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project No. Allocation Amount Recipient RTPA/CTC County Dist-Co-Rte Postmile	Project Title Location Project Description Project Support Expenditures	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type									
2.5s.(5)	Locally-Administered Multi-Funded SB 1 LPP (Formulaic) and TCEP Project On the State Highway System (Advancement Fiscal Year 2025-26)		Resolution LPP-A-2425-33 Resolution TCEP-A-2425-24										
1 \$87,731,000 San Bernardino County Transportation Authority SBCTA San Bernardino 08-SBd-10 10.00/16.60	Interstate 10 Corridor Freight and Managed Lane Project: I-15 to Sierra Avenue, Contract 2A. The Contract 2A component of the Interstate 10 (I-10) Corridor Freight and Managed Lane Project will construct one lane in each direction for a total of 13.4 lane miles on I-10 from I-15 in Ontario to Sierra Ave in Fontana and approximately 0.35 lane miles of auxiliary lane going eastbound (EB) only, between Cherry Ave and Citrus Ave. The overall I-10 Corridor Freight and Managed Lane Project will ultimately provide one managed lane in each direction on I-10 from I-15 in Ontario to Pepper Ave in Colton, a total distance of 22.8 miles, connecting to the I-10 Corridor Contract 1 managed lanes currently under construction. The project will also construct four strategic auxiliary lane and ramp improvements: EB and WB auxiliary lanes between Riverside and Pepper Avenues and auxiliary lanes between EB Cherry Ave and Citrus Ave and EB Sierra Avenue to Cedar Avenue to improve truck mobility and safety.	08-1325 LPP-F/25-26 CONST \$12,731,000 TCEP-R/23-24 CONST \$75,000,000 0824000092 4CONL 1P710	2017-18 801-3290 RMRA 20.20.724.000 2024-25 301-3291 TCEA 302-0890 FTF 20.20.723.200	\$12,731,000 \$6,323,000 \$68,677,000 \$75,000,000									
<table><tr><td>Outputs</td><td>Unit</td><td>Total</td></tr><tr><td>Auxiliary lane constructed</td><td>Miles</td><td>0.35</td></tr><tr><td>HOV/HOT mainline constructed</td><td>Miles</td><td>13.4</td></tr></table>					Outputs	Unit	Total	Auxiliary lane constructed	Miles	0.35	HOV/HOT mainline constructed	Miles	13.4
Outputs	Unit	Total											
Auxiliary lane constructed	Miles	0.35											
HOV/HOT mainline constructed	Miles	13.4											
CEQA - EIR, 05/15/2017; Re-validation 05/15/2025 NEPA - EIS, 05/15/2017; Re-validation 05/15/2025 Future consideration of funding approved under Resolution E-17-68; October 2017. Concurrent addendum for the previously approved future consideration of funding under Resolution E-25-43A; June 2025. Right of Way Certification: 06/09/2025 Contribution from other sources: \$339,977,000. Time Extension for FY 23-24 CON expires June 30, 2025													

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5s.(3), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR LOCALLY-ADMINISTERED SENATE BILL 1 TRADE
CORRIDOR ENHANCEMENT PROGRAM PROJECTS OFF THE STATE
HIGHWAY SYSTEM
RESOLUTION TCEP-A-2425-25**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$38,713,000 for two locally-administered Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) projects, off the State Highway System?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$38,713,000 for two locally-administered SB 1 TCEP projects, off the State Highway System.

BACKGROUND:

The attached vote list describes two locally-administered SB 1 TCEP projects totaling \$38,713,000. The local agencies are ready to proceed with these projects, and are requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$38,713,000 be allocated from the Budget Act of 2022, Budget Act Item 2660-101-3291 for two locally-administered SB 1 TCEP projects described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project #		PPNO	Budget Year	
Allocation Amount		Program/Year	Item #	
Recipient	Project Title	Phase	Fund Type	Amount by
<u>RTPA/CTC</u>	Location	Prgm'd Amount	Program Code	Fund Type
District-County	Project Description	Project ID		

2.5s.(3)

Locally-Administered SB 1 TCEP Projects Off the State Highway System

Resolution TCEP-A-2425-25

1	Green Power Microgrid - Harbor Facilities Component.		04-2090S	2022-23	
\$16,507,000	Installation of solar infrastructure, battery energy storage systems, and 30 zero-emission vehicle chargers at the Harbor Facilities Center.		TCEP-R/24-25	101-3291 TCEA	\$9,904,000
Port of Oakland			CONST	20.30.210.320	
<u>MTC</u>			\$9,904,000		
04-Alameda	<u>Outputs</u>	<u>Unit</u>	<u>Total</u>	2022-23	
	Energy Storage System - Capacity	MWh	6.5	101-3291 TCEA	\$6,603,000
	Solar Capacity	MW	1	20.30.210.310	
	Number of AC charging ports	Each	30		
			\$6,603,000		
			0425000296		
	CEQA - ND, 12/04/2023				
	Future consideration of funding approved under Resolution E-24-44; May 2024.				
	Right of Way Certification: 04/24/2025				
	Contribution from other sources: \$7,284,000.				
	Baseline Agreement approved under Resolution TCEP-P-2324-07B; May 2024.				
	TCEP Programming Amendment to consolidate the CON programming from PPNOs 2090J, 2090L, and 2090M into 2090S and 2090T approved under Resolution G-25-30; May 2025.				
	As part of this allocation request, the Port of Oakland is requesting to extend the completion of Construction an additional 12 months beyond the 36-month deadline.				
	As part of this allocation request, the Port of Oakland is requesting to extend the period of contract award an additional 3 months beyond the 6-month deadline.				

2.5 Highway Financial Matters

Project #	Allocation Amount	Recipient	Project Title	PPNO	Budget Year	Amount by
<u>RTPA/CTC</u>			Location	Program/Year	Item #	Fund Type
District-County			Project Description	Phase	Fund Type	Amount by
				Prgm'd Amount	Program Code	Fund Type
				Project ID		
2.5s.(3)	Locally-Administered SB 1 TCEP Projects Off the State Highway System				Resolution TCEP-A-2425-25	
2	Green Power Microgrid - Seaport Terminals Component.					
\$22,206,000	Installation of 115 zero-emission vehicle chargers across five project site locations.			04-2090T	2022-23	
Port of Oakland				TCEP-R/24-25	101-3291 TCEA	\$13,324,000
<u>MTC</u>				CONST	20.30.210.320	
04-Alameda				\$13,324,000		
	<u>Outputs</u>	<u>Unit</u>	<u>Total</u>		2022-23	
	Number of AC charging ports	Each	115	TCEP-S/24-25	101-3291 TCEA	\$8,882,000
				CONST	20.30.210.310	
	CEQA - ND, 12/04/2023			\$8,882,000		
				0425000297		
	Future consideration of funding approved under Resolution E-24-44; May 2024.					
	Right of Way Certification: 04/24/2025					
	Contribution from other sources: \$9,798,000.					
	Baseline Agreement approved under Resolution TCEP-P-2324-07B; May 2024.					
	TCEP Programming Amendment to consolidate the CON programming from PPNOs 2090J, 2090L, and 2090M into 2090S and 2090T approved under Resolution G-25-30; May 2025.					
	As part of this allocation request, the Port of Oakland is requesting to extend the completion of Construction an additional 12 months beyond the 36-month deadline.					
	As part of this allocation request, the Port of Oakland is requesting to extend the period of contract award an additional 3 months beyond the 6-month deadline.					

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5s.(17), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR A LOCALLY-ADMINISTERED SENATE BILL 1 TRADE
CORRIDOR ENHANCEMENT PROGRAM PROJECT ON THE STATE
HIGHWAY SYSTEM
RESOLUTION TCEP-A-2425-26**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$33,500,000 for the locally-administered Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) SR-60/Potrero Boulevard Interchange Phase II project (PPNO 1321), on the State Highway System, in Riverside County?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$33,500,000 for the locally-administered SB 1 TCEP SR-60/Potrero Boulevard Interchange Phase II project (PPNO 1321), on the State Highway System, in Riverside County.

BACKGROUND:

The attached vote list describes the locally-administered SB 1 TCEP project totaling \$33,500,000. The local agency is ready to proceed with this project, and is requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$33,500,000 be allocated from the Budget Act of 2024, Budget Act Items 2660-301-3291 and 2660-302-0890 for the locally-administered SB 1 TCEP project described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project No.	Project Title	PPNO		
Allocation Amount	Location	Program/Year		
Recipient	Project Description	Phase		
RTPA/CTC	Project Support Expenditures	Prgm'd Amount		
County		Project ID	Budget Year	
Dist-Co-Rte		Adv Phase	Item # Fund Type	Amount by
Postmile		EA	Program Code	Fund Type

2.5s.(17)**Locally-Administered SB 1 TCEP Project On the State Highway System****Resolution TCEP-A-2425-26**

1	SR-60/Potrero Boulevard Interchange Phase II. on SR-60 between Jack Rabbit Trail & SR-60/I-10 Junction phase 2: new interchange on/off ramps. construct Westbound/Eastbound Exit Ramps (2 lanes) and Westbound/Eastbound Loop Entry Ramps (2 lanes) (entry ramps including HOV lane), realign Western Knolls Avenue, and remove Western Knolls Avenue Connection to SR-60.		08-1321	2024-25	
\$33,500,000			TCEP-S/24-25	301-3291 TCEA	\$3,843,000
			CONST	302-0890 FTF	<u>\$29,657,000</u>
			\$33,500,000	20.20.723.100	\$33,500,000
Riverside County			0800020445		
Trans Commission			4CONL		
<u>RCTC</u>			34142		
Riverside	<u>Outputs</u>	<u>Unit</u>	<u>Total</u>		
08-Riv-60	Interchange modification(s)	Each	1		
28.03/30.42	Auxiliary lane(s)	Miles	0.49		
	Bicycle lane mile(s)	Miles	0.75		
	Sidewalk mile(s)	Miles	0.89		
CEQA - MND, 03/01/2013; Re-validation 05/27/2025					
NEPA - FONSI, 05/12/2012; Re-validation 05/27/2025					
Future consideration of funding approved under Resolution E-13-42; May 2023.					
Concurrent addendum for the previously approved future consideration of funding under Resolution E-25-44A; June 2025.					
Right of Way Certification: 05/16/2025					
Contribution from other sources: \$24,500,000.					

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6s.(2), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR A LOCALLY-ADMINISTERED SENATE BILL 1
TRADE CORRIDOR ENHANCEMENT PROGRAM RAIL PROJECT
RESOLUTION TCEP-A-2425-27**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$30,528,000 for the locally-administered Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) San Dieguito Double Track Phase 1 Rail project (PPNO LP003), in San Diego County?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$30,528,000 for the locally-administered SB 1 TCEP San Dieguito Double Track Phase 1 Rail project (PPNO LP003), in San Diego County.

BACKGROUND:

The attached vote list describes the locally-administered SB 1 TCEP Rail project totaling \$30,528,000. The local agency is ready to proceed with this project, and is requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$30,528,000 be allocated from the Budget Act of 2022, Budget Act Item 2660-301-3291 for the locally-administered SB 1 TCEP Rail project described on the attached vote list.

Attachment

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6s.(2)	Senate Bill 1 - TCEP Rail Project		Resolution TCEP-A-2425-27	
1 \$30,528,000 San Diego Association of Governments <u>SANDAG</u> 11-San Diego	San Dieguito Double Track Phase 1. Construction of 0.8 miles of second track between Control Point (CP) Valley at Mile Post (MP) 242.2 in the City of Solano Beach to south of CP Del Mar at MP 243.9 in the City of Del Mar, including platform construction at the Del Mar Fairgrounds. <u>Outcome/Outputs:</u> Mile(s) of New Track CEQA - Exempt under PRC 21080(b)(12) and CCR, title 14 (CEQA Guidelines), section 15275; Pre-Empted, 49 CFR 10501(b), 12/15/2023. NEPA - FONSI, 05/15/2015 Right of Way Certification: 06/12/2025 Contribution from other sources: \$50,147,000. Time Extension for FY 22-23 CONST expires 06/30/2025.	11-LP003 TCEP-R/22-23 CONST \$22,845,000 TCEP-S/22-23 CONST \$7,683,000 S4 1125000234 R455GA	2022-23 301-3291 TCEA 30.20.723.200 2022-23 301-3291 TCEA 30.20.723.100	\$22,845,000 \$7,683,000

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5s.(15), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: ALLOCATION FOR A LOCALLY-ADMINISTERED MULTI-FUNDED
SENATE BILL 1 TRADE CORRIDOR ENHANCEMENT PROGRAM AND
INFRASTRUCTURE FOR REBUILDING AMERICA PROJECT ON THE STATE
HIGHWAY SYSTEM (ADVANCEMENT FISCAL YEAR 2025-26)
RESOLUTION TCEP-A-2425-29
RESOLUTION FP-24-104

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$56,400,000 for the locally-administered multi-funded Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) and Infrastructure for Rebuilding America (INFRA) Otay Mesa East Port of Entry Technology Package project (PPNO 0999J), on the State Highway System, in San Diego County, programmed in Fiscal Year 2025-26?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$56,400,000 for the locally-administered multi-funded SB 1 TCEP and INFRA Otay Mesa East Port of Entry Technology Package project (PPNO 0999J), on the State Highway System, in San Diego County, programmed in 2025-26

BACKGROUND:

The attached vote list describes the locally-administered multi-funded SB 1 TCEP and INFRA project totaling \$56,400,000. The local agency is ready to proceed with this project, and is requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved that \$56,400,000 be allocated from the Budget Acts of 2022 and 2024, Budget Act Items 2660-301-3291 and 2660-302-0890 for the locally-administered multi-funded SB 1 TCEP and INFRA project described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project No. Allocation Amount Recipient RTPA/CTC County Dist-Co-Rte Postmile	Project Title Location Project Description Project Support Expenditures	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5s.(15)	Locally-Administered Multi-Funded SB 1 TCEP and Infrastructure for Rebuilding America Project On the State Highway System (Advancement Fiscal Year 2025-26)		Resolution TCEP-A-2425-29 Resolution FP-24-104	
1 \$56,400,000	Otay Mesa East Port of Entry Technology Package. Near San Diego on Route 11 at 1.9 miles east of Sanyo Avenue Undercrossing. Otay Mesa East technology package to enhance efficiency and throughput at the Port of Entry and Commercial Vehicle Enforcement Facility.	11-0999J TCEP-S/24-25 CONST \$20,400,000	2022-23 301-3291 TCEA 20.20.723.100	\$20,400,000
San Diego Association of Governments <u>SANDAG</u> San Diego 11-SD-11 2.7/2.7	<u>Outputs</u> Border Crossing Improvements <u>Unit</u> <u>Total</u> Each 0.01 CEQA - EIR, 03/29/2012; Re-validation 03/27/2025 NEPA - EIS, 03/29/2012; Re-validation 04/22/2025 Future consideration of funding approved under Resolution E-12-35; June 2012. Right of Way Certification: 01/15/2025 Concurrent STIP Amendment under 24S-06; June 2025. Concurrent TCEP Programming Amendment under Resolution G-25-44; June 2025.	INFRA/25-26 CONST \$36,000,000 1125000107 4CONL 05643	2024-25 302-0890 FTF 20.20.400.409	\$36,000,000

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5s.(7), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR LOCALLY-ADMINISTERED SENATE BILL 1 SOLUTIONS
FOR CONGESTED CORRIDORS PROGRAM PROJECTS OFF THE STATE
HIGHWAY SYSTEM
RESOLUTION SCCP-A-2425-04**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$4,150,000 for three locally-administered Senate Bill 1 (SB 1) Solutions for Congested Corridors Program (SCCP) projects, off the State Highway System?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$4,150,000 for three locally-administered SB 1 SCCP projects, off the State Highway System.

BACKGROUND:

The attached vote list describes three locally-administered SB 1 SCCP projects totaling \$4,150,000. The local agencies are ready to proceed with these projects, and are requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$4,150,000 be allocated from the Budget Act of 2023, Budget Act Item 2660-109-0042 for three locally-administered SB 1 SCCP projects described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type								
2.5s.(7)	Locally-Administered SCCP Projects Off the State Highway System		Resolution SCCP-A-2425-04									
1 \$2,250,000 County of Santa Barbara <u>SBCAG</u> 05-Santa Barbara	Sheffield Drive to Ortega Hill Pedestrian and Bicycle Connector. Existing bicycle and pedestrian facilities along North Jameson Lane in Montecito will be connected to an existing Class I facility over Ortega Hill into the community of Summerland. <table><tr><td><u>Outputs</u></td><td><u>Unit</u></td><td><u>Total</u></td></tr><tr><td>Pedestrian/Bicycle facilities constructed</td><td>Miles</td><td>0.1</td></tr></table> CEQA - CE, 10/17/2023 Right of Way Certification: Pending Time Extension for FY 23-24 CONST expires on 06/30/2025.	<u>Outputs</u>	<u>Unit</u>	<u>Total</u>	Pedestrian/Bicycle facilities constructed	Miles	0.1	05-3199 SCCP/23-24 CONST \$2,250,000 0525000208	2023-24 109-0042 SHA 20.30.210.350 \$2,250,000			
<u>Outputs</u>	<u>Unit</u>	<u>Total</u>										
Pedestrian/Bicycle facilities constructed	Miles	0.1										
2 \$1,325,000 City of Santa Barbara <u>SBCAG</u> 05-Santa Barbara	Zero Emission Vehicle Infrastructure. The project will install Zero Emission Vehicle chargers in the City of Santa Barbara at two locations. Ten DC Fast Chargers (DCFC) will be installed at the Harbor parking lot, and four DCFC chargers will be installed at the Santa Barbara East Side Library. <table><tr><td><u>Outputs</u></td><td><u>Unit</u></td><td><u>Total</u></td></tr><tr><td>Facility Improvements</td><td>Each</td><td>44</td></tr><tr><td>DC charging ports</td><td>Each</td><td>14</td></tr></table> CEQA - CE, 11/07/2023 Right of Way Certification: 04/07/2025 Contribution from other sources: \$389,000. Time Extension for FY 23-24 CONST expires on 06/30/2025. Outputs have been updated for accuracy and have not changed.	<u>Outputs</u>	<u>Unit</u>	<u>Total</u>	Facility Improvements	Each	44	DC charging ports	Each	14	05-3202 SCCP/23-24 CONST \$1,325,000 0525000209	2023-24 109-0042 SHA 20.30.210.350 \$1,325,000
<u>Outputs</u>	<u>Unit</u>	<u>Total</u>										
Facility Improvements	Each	44										
DC charging ports	Each	14										

2.5 Highway Financial Matters

Project #		PPNO	Budget Year	
Allocation Amount		Program/Year	Item #	
Recipient	Project Title	Phase	Fund Type	Amount by
<u>RTPA/CTC</u>	Location	Prgm'd Amount	Program Code	Fund Type
District-County	Project Description	Project ID		

2.5s.(7)

Locally-Administered SCCP Projects Off the State Highway System

Resolution SCCP-A-2425-04

3	Zero Emission Vehicle Infrastructure. This project will install	05-3203	2023-24	
\$575,000	Zero Emission Vehicle chargers at two existing charging	SCCP/23-24	109-0042 SHA	\$575,000
	locations and one new location in Santa Barbara County. One	CONST	20.30.210.350	
County of Santa	DC Fast Charger will be installed at Lillie Avenue in	\$575,000		
Barbara	Summerland. Three chargers will be installed at Rincon Park,	0525000207		
<u>SBCAG</u>	where at least one of the three chargers will be a DC Fast			
05-Santa Barbara	Charger. At Santa Claus Lane, four chargers will be installed,			
	and these will be new installations. At least one of the four			
	chargers at Santa Claus Lane will be a DC Fast Charger.			
	<u>Outputs</u>	<u>Unit</u>	<u>Total</u>	
	Facility Improvements	Each	8	
	DC charging ports	Each	3	
	AC charging ports	Each	5	
	CEQA - CE, 10/16/2023			
	Right of Way Certification: 04/21/2025			
	Time Extension for FY 23-24 CONST expires on 06/30/2025.			
	Outputs have been updated for accuracy and have not changed.			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6s.(1), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR LOCALLY-ADMINISTERED SENATE BILL 1 SOLUTIONS
FOR CONGESTED CORRIDORS PROGRAM TRANSIT PROJECTS
RESOLUTION SCCP-A-2425-05**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$45,507,000 for two locally-administered Senate Bill 1 (SB 1) Solutions for Congested Corridors Program (SCCP) Transit projects?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$45,507,000 for two locally-administered SB 1 SCCP Transit projects.

BACKGROUND:

The attached vote list describes two locally-administered SB 1 SCCP Transit projects totaling \$45,507,000. The local agencies are ready to proceed with these projects, and are requesting an allocation at this time; however, the allocation is contingent upon the approval of a Budget Revision by the Department of Finance.

FINANCIAL RESOLUTION:

Resolved, that \$45,507,000 be allocated from the Budget Act of 2024, Budget Act item 2660-109-0042 for two locally-administered SB 1 SCCP Transit projects described on the attached vote list.

Attachment

2.6 Mass Transportation Financial Matters

Project #	Allocation Amount	Recipient	Project Title	PPNO	Budget Year	Amount by
			Location	Program/Year	Item #	Fund Type
			Project Description	Phase	Fund Type	Amount by
				Prgm'd Amount	Program Code	Fund Type
				Project ID		
2.6s.(1)			Senate Bill 1 - SCCP Transit Projects			Resolution SCCP-A-2425-05
1	\$25,507,000	Los Angeles County Metropolitan Transportation Authority LACMTA 07-Los Angeles	North San Fernando Valley Battery Electric Bus Project (Buses). Procurement of 75 Battery Electric Buses, needed to operate the North San Fernando Valley Transit Corridor Bus Enhanced Network. <u>Outputs</u> Rail car(s)/transit vehicle(s) CEQA - CE, 12/03/2020 Right of Way Certification: N/A Time Extension for FY 23-24 CONST expires 6/30/2025. As part of this allocation request, the local agency is requesting to extend the period of contract award an additional 6 months beyond the 6-month deadline.	07-6202 SCCP/23-24 CONST \$25,507,000 0725000250 T652GA	2024-25 109-0042 SHA 30.10.030.100	\$25,507,000
			<u>Unit</u> EA <u>Total</u> 75			
2	\$20,000,000	Los Angeles County Metropolitan Transportation Authority LACMTA 07-Los Angeles	Los Angeles Metro CORE Capacity & System Integration Project (Replacement-TPSS). Replacement of four existing Traction Power Substations located at Aviation, El Segundo, Douglas, and Hawthorne Yard, serving the original C (Green) Line alignment. <u>Outputs</u> Rail/Transit Equipment CEQA - SE, 11/16/2023 Right of Way Certification: 12/18/2023 Time Extension for FY 23-24 CONST expires 6/30/2025. As part of this allocation request, the local agency is requesting to extend the period of contract award an additional 6 months beyond the 6-month deadline. <u>ALLOCATION IS CONTINGENT UPON THE APPROVAL OF A BUDGET REVISION BY THE DEPARTMENT OF FINANCE.</u>	07-6210A SCCP/23-24 CONST \$20,000,000 0025000175 R617GA	2024-25 109-0042 SHA 30.10.030.100	\$20,000,000
			<u>Unit</u> EA <u>Total</u> 4			

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5s.(12), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: ALLOCATION FOR A LOCALLY-ADMINISTERED MULTI-FUNDED
SENATE BILL 1 LOCAL PARTNERSHIP PROGRAM (FORMULAIC) AND
SENATE BILL 1 SOLUTIONS FOR CONGESTED CORRIDORS PROGRAM
PROJECT OFF THE STATE HIGHWAY SYSTEM
RESOLUTION LPP-A-2425-34
RESOLUTION SCCP-A-2425-03

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$63,275,000 for the locally-administered multi-funded Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) and SB 1 Solutions for Congested Corridors Program (SCCP) Mid County Parkway: Ramona Expressway project (PPNO 1276), off the State Highway System, in Riverside County?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$63,275,000 for the locally-administered multi-funded SB 1 LPP (Formulaic) and SB 1 SCCP Mid County Parkway: Ramona Expressway project (PPNO 1276), off the State Highway System, in Riverside County.

BACKGROUND:

The attached vote list describes the locally-administered multi-funded SB 1 LPP (Formulaic) and SB 1 SCCP project vote box totaling \$63,275,000. The local agency is ready to proceed with this project, and is requesting an allocation at this time; however, the allocation is contingent upon the approval of a budget revision by the Department of Finance.

FINANCIAL RESOLUTION:

Resolved that \$63,275,000 be allocated from the Budget Act of 2024, Budget Act Item 2660-109-0042 and Non-Budget Act Item 2660-601-3290 for the locally-administered multi-funded SB 1 LPP (Formulaic) and SB 1 SCCP project described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type																								
2.5s.(12)	Locally-Administered Multi-Funded Senate Bill 1 LPP (Formulaic) and SCCP Project Off the State Highway System		Resolution LPP-A-2425-34 Resolution SCCP-A-2425-03																									
1 \$63,275,000 Riverside County Transportation Commission <u>RCTC</u> 08-Riverside	Mid County Parkway: Ramona Expressway. Mid County Parkway (MCP): Ramona Expressway - In unincorporated Riverside County: Construct 2 additional lanes on MCP alignment from approximately one mile east of Rider Street to Warren Road along Ramona Expressway and make safety improvements, including adding raised median with delineators, a new bridge over the San Jacinto River, bike lanes in each direction, modification of three signalized intersections, and a new wildlife crossing west of Bridge Street.	08-1276 SCCP/24-25 CONST \$44,500,000 LPP-F/24-25 CONST \$18,775,000 0823000062	2024-25 109-0042 SHA 20.30.210.350 2017-18 601-3290 RMRA 20.30.210.200	\$44,500,000 \$18,775,000																								
<table><tr><td><u>Outputs</u></td><td><u>Unit</u></td><td><u>Total</u></td></tr><tr><td>New local road bridge structure(s)/tunnel(s)</td><td>SQFT</td><td>121086</td></tr><tr><td>Intersection/Signal improvement(s)</td><td>Each</td><td>4</td></tr><tr><td>New curb ramp installed</td><td>Each</td><td>14</td></tr><tr><td>Install accessible pedestrian signal</td><td>Each</td><td>14</td></tr><tr><td>Bicycle lane mile(s)</td><td>Miles</td><td>17</td></tr><tr><td>Roadway lane mile(s) - new</td><td>Miles</td><td>17</td></tr><tr><td>Culvert(s)</td><td>LF</td><td>3000</td></tr></table> <p>CEQA - EIR, 04/15/2015; Re-validation 01/29/2025 NEPA - EIS, 04/15/2015; Re-validation 01/29/2025</p> <p>Future consideration of funding approved under Resolution E-19-66; June 2019.</p> <p>Right of Way Certification: Pending</p> <p><u>ALLOCATION IS CONTINGENT UPON APPROVAL OF A BUDGET REVISION BY THE DEPARTMENT OF FINANCE.</u></p>					<u>Outputs</u>	<u>Unit</u>	<u>Total</u>	New local road bridge structure(s)/tunnel(s)	SQFT	121086	Intersection/Signal improvement(s)	Each	4	New curb ramp installed	Each	14	Install accessible pedestrian signal	Each	14	Bicycle lane mile(s)	Miles	17	Roadway lane mile(s) - new	Miles	17	Culvert(s)	LF	3000
<u>Outputs</u>	<u>Unit</u>	<u>Total</u>																										
New local road bridge structure(s)/tunnel(s)	SQFT	121086																										
Intersection/Signal improvement(s)	Each	4																										
New curb ramp installed	Each	14																										
Install accessible pedestrian signal	Each	14																										
Bicycle lane mile(s)	Miles	17																										
Roadway lane mile(s) - new	Miles	17																										
Culvert(s)	LF	3000																										

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5w.(1), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR LOCALLY-ADMINISTERED ACTIVE TRANSPORTATION
PROGRAM PROJECTS OFF THE STATE HIGHWAY SYSTEM
RESOLUTION FATP-2425-26**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$64,166,000 for 28 locally-administered Active Transportation Program (ATP) projects, off the State Highway System?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$64,166,000 for 28 locally-administered ATP projects, off the State Highway System.

BACKGROUND:

The attached vote list describes 28 locally-administered ATP projects totaling \$64,166,000. The local agencies are ready to proceed with these projects, and are requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$64,166,000 be allocated from the Budget Acts of 2023 and 2024, Budget Act Items 2660-108-0890 and, 2660-108-3290 for 28 locally-administered ATP projects described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
1 \$3,658,000 City of Arcata <u>HCAOG</u> 01-Humboldt	<p>Arcata Annie and Mary Trail Connectivity Project. Class I trail along NCRA rail corridor and Caltrans' Giuntoli Lane and Sunset Avenue overpasses in north Arcata.</p> <p>Statewide</p> <p><u>Outcome/Outputs:</u> New bicycle and pedestrian improvements within the project limits and trailhead improvements. Construct 3.3 miles of Class I trail, 2200 linear feet Class II Bike lanes, 24 New ramps, 3 rectangular rapid flashing beacons, Reconstruct 2 ramps on two highway overpasses to provide safe connections to the trail and trailhead improvements.</p> <p>CEQA - MND, 12/22/2022 NEPA - CE, 03/13/2024; Re-validation 04/28/2025</p> <p>Future consideration of funding approved under Resolution E23-16; January 2023.</p> <p>Right of Way Certification: 04/29/2025</p> <p>Time Extension for FY 23-24 CONST expires 06/30/2025.</p> <p>Concurrent SHOPP Allocation under EA 01-0N550, PPNO 01-4856, Project ID 0124000054 under Resolution FP-24-95; June 2025.</p>	01-2548 ATP/23-24 CONST \$3,658,000 0121000102	2024-25 108-3290 RMRA 20.30.720.100	\$3,658,000
2 \$208,000 Modoc County <u>MCTC</u> 02-Modoc	<p>Surprise Valley School Safety and Community Connectivity Project. In the township of Cedarville, on Bonner Street from Patterson Street, to Main Street and along the North side on High Street, four blocks of Main Street and Ann Street. Construct/replace sidewalks, implement high visibility and raised crosswalks throughout a disadvantaged community connecting students and full community to school sites, park, and essential services.</p> <p>Small Urban and Rural</p> <p><u>Outcome/Outputs:</u> Construction of 11,000 linear feet of ADA compliant sidewalks, update seven crosswalks to become high visibility and raised, provide safe drop off and walking area for students.</p> <p>CEQA - CE, 03/31/2025</p>	02-2631 ATP/24-25 PS&E \$196,000 R/W \$12,000 0224000017	2024-25 108-3290 RMRA 20.30.720.100 2024-25 108-3290 RMRA 20.30.720.100	\$196,000 \$12,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
3 \$2,462,000 City of Redding <u>SRTA</u> 02-Shasta	Turtle Bay to Downtown Gap Completion Project. In the City of Redding, on Trinity Street from Center Street and along SR 44 from Continental Street to Sundial Bridge Drive. The project will construct non-motorized improvements including a paved pedestrian and bicycle multi-use path linking downtown Redding with the Turtle Bay Exploration Park and trails leading to the Eastside of Redding. Statewide <u>Outputs</u> Pedestrian/Bicycle facilities mile(s) constructed CEQA - CE, 02/01/2023 Right of Way Certification: 04/24/2025 PPNO 2588A is the infrastructure component to PPNO 2588B.	02-2588A ATP/24-25 CONST \$2,462,000 0219000109	2024-25 108-3290 RMRA 20.30.720.100	\$2,462,000
4 \$153,000 City of Redding <u>SRTA</u> 02-Shasta	Turtle Bay to Downtown Gap Completion Project. Coordinate and implement bicycle and pedestrian safety programs with schools and seniors along the corridor. Hold events and activities that encourage use of the new bike and pedestrian infrastructure. Statewide <u>Outcome/Outputs:</u> Pedestrian and bicycle safety curriculum for four different schools, one to two family bicycle and pedestrian events, four to six walk to school day events, one open street event, neighborhood maps. CEQA - CE, 02/01/2023 PPNO 2588B is the Non-Infrastructure component to PPNO 2588A.	02-2588B ATP/24-25 CONST \$153,000 0225000152	2024-25 108-3290 RMRA 20.30.720.100	\$153,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
5 \$420,000 City of Sacramento <u>SACOG</u> 03-Sacramento	Franklin Boulevard Complete Street - Phase 3. On Franklin Boulevard, between Fruitridge Road and 41st Avenue: Reduction in travel lanes from four to two (with center turn lane), addition of Class IV and buffered Class II bike facilities, pedestrian improvements, and signal modifications. Statewide <u>Outcome/Outputs:</u> Construct 4,500 feet of Class IV bike lanes/routes, 2 timing improvements, 5 crossing-surface improvements, 2 protected intersections, 930 feet of new sidewalk, 4 new curb ramps, 10 crosswalks, 16 ped-heads, 5 shortened crossing, 1 new traffic signal, 1 rectangular rapid flashing beacon, 2,400 feet of roadway segments lighting, 15 shade trees, reconstruct 11 ramps to standard, enhance 3 existing crosswalks, reconstruct/enhance 3,000 feet of existing sidewalk, and remove 4,600 feet of travel lane. CEQA - MND, 02/14/2020 Future consideration of funding approved under Resolution E-20-35; May 2020.	03-2221 ATP/24-25 R/W \$420,000 0324000300	2024-25 108-3290 RMRA 20.30.720.100 \$420,000	
6 \$1,401,000 City of San Mateo <u>MTC</u> 04-San Mateo	Delaware Street Safe Routes to School Corridor. Design/construction of Class IV separated bike lanes and bicycle boulevard, upgrade pedestrian facilities, and connections to existing facilities. MPO <u>Outcome/Outputs:</u> Construct 320 feet of Class II bike lanes, 1,857 feet of Class III bike routes, and 3,377 feet of Class IV bike lanes. Construct 5,534 feet of barrier protected sidewalk. At signalized intersections, enhance 27 existing crosswalks and improve timing of 5 pedestrian signals. CEQA - CE, 08/25/2022 Right of Way Certification: 03/13/2025 Concurrent ATP Scope Amendment under Resolution ATP-P-2425-11; June 2025.	04-2353 ATP/24-25 CONST \$1,401,000 0422000110	2024-25 108-3290 RMRA 20.30.720.100 \$1,401,000	

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
7 \$1,008,000 Monterey County <u>TAMC</u> 05-Monterey	Community and School Connections Through Active Transportation. Increase walking opportunities for students, seniors, and the community. Safe Routes to School activities, supporting safe walking to and from the school, library, and other neighborhood destinations. Educational safety campaign for motorists will be implemented. Develop community active transportation champions in community through civic engagement, trainings, and community organized stakeholder meetings. Small Urban and Rural <u>Outcome/Outputs:</u> 10 bicycle skills/safety classes, 30 community workshops/stakeholder meetings, 26 classroom/PE classes, two SRTS encouragement days, and one Regional Community Health and Wellness Conference. CEQA - CE, 03/11/2025 PPNO 3122B is the Non-Infrastructure component to PPNO 3122A.	05-3122B ATP/24-25 CONST \$1,008,000 0525000155	2024-25 108-3290 RMRA 20.30.720.100	\$1,008,000
8 \$911,000 Monterey County <u>TAMC</u> 05-Monterey	Chualar Community and School Connections Through Active Transportation. Increase walking opportunities for students, seniors, and the community. Safe Routes to School activities, supporting safe walking to and from the school, library, and other neighborhood destinations. Educational safety campaign for motorists will be implemented. Develop community active transportation champions in community through civic engagement, trainings, and community organized stakeholder meetings. Small Urban and Rural <u>Outcome/Outputs:</u> Two bicycle skills/safety classes, two pedestrian skills/safety classes, 30 community workshops/stakeholder meetings, 10 classroom/PE classes, one walking school bus program, eight SRTS encouragement days, and eight training sessions to implement the SRTS program. CEQA - CE, 03/11/2025 PPNO 3126B is the Non-Infrastructure component to PPNO 3126A.	05-3126B ATP/24-25 CONST \$911,000 0525000156	2024-25 108-3290 RMRA 20.30.720.100	\$911,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
9 \$507,000 City of Watsonville <u>SCCRTC</u> 05-Santa Cruz	<p>Safe Routes to Downtown Watsonville. Improve pedestrian and bike safety and provide or improve bike lanes, crosswalks, curb extensions, curb ramps, pavement markings, pedestrian islands, signage, and safety training.</p> <p>Statewide</p> <p><u>Outcome/Outputs:</u> Install 29,950 feet of Class III bike route, 500 feet of buffered Class II bike lanes, 21 new ADA ramps, 55 reconstructed ramps to ADA standard, 8 enhanced crosswalks at signalized intersections, 7 shortened crossings, 89 crossing-surface improvements, 7 advanced warning pavement markings, and 22 improvements to sight distance.</p> <p>CEQA - CE, 04/21/2025</p> <p>PPNO 3116A is the infrastructure component to PPNO 3116B.</p>	05-3116A ATP/24-25 PS&E \$507,000 0523000082	2024-25 108-3290 RMRA 20.30.720.100	\$507,000
10 \$1,122,000 City of Fresno <u>FCOG</u> 06-Fresno	<p>Cross, Walk & Roll! SRTS in Central Fresno. Located at the intersections of Chestnut & Weldon and First & Home. Project will install signals (scramble and high-intensity activated crosswalk (HAWK)) and establish a Bicycle and Pedestrian Safety Week education campaign at Ericson & Mayfair Elementary schools.</p> <p>MPO</p> <p><u>Outcome/Outputs:</u> Reconstruct 8 ramps to ADA standard, install 1 traffic signal, and 1 HAWK beacon.</p> <p>CEQA - CE, 01/26/2022 NEPA - CE, 07/06/2022</p> <p>Right of Way Certification: 4/16/2025</p> <p>PPNO 06-7088A is the infrastructure component to PPNO 06-7088B</p>	06-7088A ATP/24-25 CONST \$1,122,000 0622000044	2023-24 108-0890 FTF 20.30.720.100	\$1,122,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
11 \$21,000 City of Fresno <u>FCOG</u> 06-Fresno	<p>Cross, Walk & Roll! SRTS in Central Fresno. This project will provide bicycle and pedestrian safety education at two elementary schools. Both schools will receive educational materials for parents and students as part of an inaugural "Bicycle and Pedestrian Safety Week". Flyers, workbooks, banners and posters will be used to encourage, build knowledge and foster a culture of safety around active travel.</p> <p>MPO</p> <p><u>Outcome/Outputs:</u> Bike and pedestrian safety outreach materials, two parent educational meetings, Bike and pedestrian safety videos, classroom student educational materials and activities.</p> <p>Right of Way Certification: 4/16/2025</p> <p>PPNO 06-7088B is the non-infrastructure component to PPNO 06-7088A</p>	06-7088B ATP/24-25 CONST \$21,000 0625000224	2024-25 108-3290 RMRA 20.30.720.100	\$21,000
12 \$50,000 City of Fowler <u>FCOG</u> 06-Fresno	<p>Fremont Elementary/Marshall Elementary/Fowler High School Safe Routes to School. The project is located on Adams Avenue between 7th Street and Temperance Avenue. The project will construct sidewalks, pedestrian crossings, and paths to provide safe continuity and connectivity for pedestrians accessing Fremont Elementary, Marshall Elementary, and Fowler High School.</p> <p>MPO</p> <p><u>Outcome/Outputs:</u> Construct 2,011 linear feet of Class I Trail (over 8 feet wide), 1 bicycle/pedestrian bridge, 24 linear feet of sidewalk (over 8 feet wide), 2 ADA ramps, install 2,146 linear feet of Class I bike path, 1 pedestrian crossing-surface improvement at un-signalized intersection, and 2 intersection lighting.</p> <p>CEQA - CE, 04/30/2025</p>	06-8077 ATP/24-25 PS&E \$50,000 0624000262	2024-25 108-3290 RMRA 20.30.720.100	\$50,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
13 \$2,302,000 Kern County <u>KCOG</u> 06-Kern	<p>Norris Pedestrian and Railroad Safety Project. Located in Oildal Norris Road, from Melody Lane to Airport Drive: Construct Sidewalk; Norris Road from Airport Drive to North Chester Avenue: Construct Pedestrian Accessibility and Crossing Improvements.</p> <p>Statewide</p> <p><u>Outcome/Outputs:</u> Construct 3,772 linear feet of sidewalk, 11 ADA ramps, reconstruct 180 linear feet of sidewalk, 11 ramps to ADA standard, install 4 rectangular rapid flashing beacons, 2 crossing-surface improvements at un-signalized intersections, 2 intersection lighting, and enhance 9 existing crosswalks.</p> <p>CEQA - CE, 04/22/2024</p>	06-8027 ATP/24-25 R/W \$2,302,000 0624000278	2024-25 108-3290 RMRA 20.30.720.100	\$2,302,000
14 \$18,157,000 City of Los Angeles <u>LACMTA</u> 07-Los Angeles	<p>Liechty Middle and Neighborhood Elementary Schools Safety Improvement Project. 0.25-mile within 10th Street Elementary School, Esperanza Elementary School, Gratts Learning Academy for Young Scholars, Gratts Early Education Center, and Liechty Middle School in Central Los Angeles. Transform five of the City's most traffic-stressed schools through the implementation of safety improvements bicycle infrastructure, and speed-reduction measures.</p> <p>Statewide</p> <p><u>Outcome/Outputs:</u> Construct 10 new curb ramps and reconstruct 800 feet of sidewalk. Install 2,000 feet of Class II bike lanes, 9,300 feet of Class III bike lanes, 28 pedestrian signals, 17 new pedestrian beacons, 47 enhanced existing crosswalks, 81 crossing surface improvements, 1 roundabout, 6 speed humps, shade trees and roadway lighting.</p> <p>CEQA - CE, 06/09/2020 NEPA - CE, 11/28/2023</p> <p>Right of Way Certification: 4/23/2025</p> <p>Time Extension for FY 22-23 CONST expires 02/28/2025.</p> <p>As part of this allocation request, the City of Los Angeles is requesting to extend the completion of Construction an additional 12 months beyond the 36-month deadline.</p> <p>Concurrent Interim Timely Use of Funds Policy under Resolution G-25-51; June 2025.</p>	07-5576 ATP/22-23 CONST \$18,157,000 0720000016	2023-24 108-0890 FTF 20.30.720.100	\$18,157,000

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Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
15 \$3,366,000 City of Los Angeles <u>LACMTA</u> 07-Los Angeles	Alexandria Avenue Elementary School Neighborhood Safety Improvements Project. 0.25-mile vicinity of Alexandria Avenue Elementary School in the East Hollywood neighborhood within the City of Los Angeles. Transform one of the City's most traffic-stressed schools through the implementation of safety improvements, bicycle infrastructure, and speed-reduction measures. Statewide <u>Outcome/Outputs:</u> Construct 12 new curb ramps and reconstruct 3,100 feet of existing sidewalk. Install 1,670 feet of Class II bike lanes, 8,290 feet of Class III bike lanes, 28 pedestrian signals, a pedestrian beacon, 34 crossing surface improvements, 100 enhance existing crosswalks, 3 new rectangular rapid flashing beacons, 11 speed feedback signs, 6 speed humps and roadway lighting. CEQA - CE, 07/30/2020 NEPA - CE, 11/22/2023 Right of Way Certification: 4/23/2025 Time Extension for FY 22-23 CONST expires 02/28/2025. As part of this allocation request, the City of Los Angeles is requesting to extend the completion of Construction an additional 12 months beyond the 36-month deadline. Concurrent Interim Timely Use of Funds Policy under Resolution G-25-51; June 2025.	07-5586 ATP/22-23 CONST \$3,366,000 0720000015	2023-24 108-0890 FTF 20.30.720.100	\$3,366,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
16 \$9,000,000 City of Glendale <u>LACMTA</u> 07-Los Angeles	<p>Phase 1 of Citywide Pedestrian Master Plan. Disadvantaged areas of downtown Glendale on Wilson Avenue and Brand Boulevard, south Glendale on Colorado Street, San Fernando Road, Brand Boulevard, and west Glendale on San Fernando Road. Enhance seven corridors within the City of Glendale keeping in line with its City-Wide Pedestrian Master Plan.</p> <p>MPO</p> <p><u>Outcome/Outputs:</u> Construct 300 feet of new sidewalk and 900 feet of widen sidewalk. Reconstruct 5,300 feet of existing sidewalk and 96 curb ramps to standards. Install 2,000 feet of Class II bike lanes, 2,000 feet of Class IV bike lanes, 16 pedestrian beacons, 240 pedestrian signal heads, 33 speed feedback signs, 6 new bike boxes, roadway diet, roadway lighting, pedestrian amenities and 112 enhanced existing</p> <p>CEQA - CE, 03/13/2025</p> <p>Right of Way Certification: 02/27/2025</p> <p>Time Extension for FY 23-24 CONST expires 06/30/2025.</p>	07-6161 ATP/23-24 CONST \$9,000,000 0724000289	2024-25 108-3290 RMRA 20.30.720.100	\$9,000,000
17 \$1,525,000 San Gabriel Valley Council of Governments <u>LACMTA</u> 07-Los Angeles	<p>East San Gabriel Valley Sustainable Multimodal Improvement Project. Various segments in the cities of Azusa, Claremont, Covina, Glendora, La Verne, Pomona, and San Dimas. The project consists of a series of multimodal and active transportation improvements across seven cities in the San Gabriel Valley.</p> <p>MPO</p> <p><u>Outcome/Outputs:</u> Construct 20,304 feet of new sidewalk, 4,992 feet of widened sidewalk, 111 new curb ramp improvements, 2,640 feet of Class I trail and reconstruct 22 curb ramps. Install 51,875 feet of Class II bike lanes, 16,060 feet of Class IV bike lanes, 41 timing improvements, 20 crossing surface improvements, 6 rectangular rapid flashing beacons, 14 new crosswalks, 4 new traffic signals, and 7,501 feet of road diet.</p> <p>CEQA - CE, 09/17/2015 NEPA - CE, 03/18/2019; Re-validation 04/10/2024</p> <p>Right of Way Certification: 05/14/2025</p>	07-6173 ATP/24-25 CONST \$1,525,000 0724000149	2024-25 108-3290 RMRA 20.30.720.100	\$1,525,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
18 \$765,000 City of Paramount <u>LACMTA</u> 07-Los Angeles	West Paramount Utility Easement Multi-Use Path - Phase 1. The project is located between Orange Avenue and Garfield Avenue beginning 2,000 feet north of Rosecrans Avenue at the West Santa Ana Path and extending to Somerset Boulevard. The project includes a Class I multi-use path, lighting, fencing, landscaping, flashing beacons, and street furniture. MPO <u>Outcome/Outputs:</u> Construct 4,916 feet of Class I multi-use path. Install 4,916 of roadway lighting, 1 rectangular rapid flashing beacon, 5 bike racks, shade trees, and pedestrian amenities. CEQA - CE, 03/20/2025	07-6188 ATP/24-25 PS&E \$750,000 R/W \$15,000 0724000063	2024-25 108-3290 RMRA 20.30.720.100 2024-25 108-3290 RMRA 20.30.720.100	\$750,000 \$15,000
19 \$420,000 City of Twentynine Palms <u>SBCTA</u> 08-San Bernardino	Sullivan Road/Hatch Road Side Path/Bicycle Lanes. An east-west corridor located south of Twentynine Palms Highway (SR -62) on Sullivan Road and Hatch Road between Morongo Road and Amboy Road. Construct 2.1 mile of paved shoulders to provide Class II bike lanes and a pedestrian walking area. MPO <u>Outcome/Outputs:</u> The project will construct 2.1 miles (11,200 linear feet) of Class II bike lanes/pedestrian side path pavement shoulders, four speed feedback signs, and edge rumble strips to provide added safety for pedestrians and bicyclists. CEQA - MND, 04/21/2025 Concurrent consideration of funding under Resolution E-25-39; June 2025.	08-1302 ATP/24-25 PS&E \$220,000 R/W \$200,000 0824000149	2024-25 108-3290 RMRA 20.30.720.100 2024-25 108-3290 RMRA 20.30.720.100	\$220,000 \$200,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
20 \$581,000 San Bernardino County <u>SBCTA</u> 08-San Bernardino	Bloomington Area Schools Pedestrian Safety Improvements. In the unincorporated San Bernardino County community of Bloomington, in the areas surrounding both campuses of the Mary B. Lewis Elementary and Gerald A. Smith Elementary Schools. Installation of high visibility ladder-style crosswalks, ADA-compliant driveway, sidewalk, Rectangular Rapid Flashing Beacons (RRFB) and speed feedback signage in Bloomington area. MPO <u>Outcome/Outputs:</u> Installation of high visibility ladder-style crosswalks, ADA-compliant driveway, sidewalk, RRFB, speed feedback signage and the addition of painted curbs which will provide added comfort and safety for pedestrians in Bloomington area. CEQA - CE, 03/11/2025	08-1304 ATP/24-25 PS&E \$201,000 R/W \$380,000 0824000150	2024-25 108-3290 RMRA 20.30.720.100 2024-25 108-3290 RMRA 20.30.720.100	\$201,000 \$380,000
21 \$1,333,000 City of Stockton <u>SJCOG</u> 10-San Joaquin	Main and Market Complete Streets (Phase 1). The project is located in Stockton on Main Street, from California Street to the city limits near the Highway 99 crossing, and Market Street from California Street to Main Street. Install bike lane, sidewalk, ramp, and crosswalk improvements. MPO <u>Outcome/Outputs:</u> Construct 400 linear feet of sidewalk, 32 ADA ramps, reconstruct 1,000 linear feet of sidewalk, 10 ramps to ADA standard, install 7,000 linear feet of Class II bike lanes, 7,000 linear feet of roadway segment lighting, 32 intersection lighting, 7 bike boxes, enhance 33 crosswalks, perform 8 timing improvements, and remove 7,000 linear feet of travel lane for road diet. CEQA - CE, 04/17/2025 NEPA - CE, 04/18/2025 Time Extension for FY 23-24 PS&E expires 06/30/2025.	10-3611 ATP/23-24 PS&E \$1,333,000 1024000139	2024-25 108-3290 RMRA 20.30.720.100	\$1,333,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
22 \$53,000 City of Tracy <u>SJCOG</u> 10-San Joaquin	East Schulte Safety and Multimodal Community Corridor - MacArthur Phase. Along MacArthur Drive from Schulte Road to Mount Diablo Avenue. Project Improvements include roadway widening, a sidewalk gap closure, crossing enhancements, and Class II bike lanes. MPO <u>Outcome/Outputs:</u> Install 3,360 linear feet of Class II bike lanes, 3 timing improvements, construct 700 linear feet of sidewalk, 2 ADA ramps, widen 980 linear feet of sidewalk, and enhance 3 crosswalks.	10-3612 ATP/24-25 PA&ED \$53,000 1025000196	2024-25 108-3290 RMRA 20.30.720.100	\$53,000
23 \$2,834,000 San Diego Association of Governments <u>SANDAG</u> 11-San Diego	Central Avenue Bikeway - The Missing Link. Located in the City of San Diego, specifically in Kensington and City Heights, along Terrace Drive from Adams Avenue to Monroe Avenue and along Central Avenue from Monroe Avenue to Landis Street. Construct low-stress bike facilities, cul-de-sac, curb ramps, improved crosswalks, rectangular rapid flashing beacons, bike friendly intersections, pedestrian refuge islands, and intersection enhancements. Statewide <u>Outcome/Outputs:</u> Construct 269 linear feet of sidewalk, 4 ADA ramps, reconstruct 4 ramps to ADA standard, install 10,298 linear feet of Class III bike route, 330 linear feet of Class IV bikeway, 6 intersection lighting, and enhance 5 crosswalks. CEQA - CE, 04/27/2018 Right of Way Certification: 03/24/2025 Time extension for FY 23-24 CONST expires 06/30/2025	11-1489 ATP/23-24 CONST \$2,834,000 1123000181	2024-25 108-3290 RMRA 20.30.720.100	\$2,834,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
24 \$445,000 City of National City <u>SANDAG</u> 11-San Diego	24th Street Transit Center Connections. This project is located along East 30th Street between Hoover Avenue and Highland Avenue, and along Hoover Avenue between East 33rd Street and East 22nd Street. Construct separated bikes, curb extensions, traffic signal modifications with bike signals, continental crosswalks, and curb ramps. Statewide <u>Outcome/Outputs:</u> Install 10,875 linear feet of Class IV bikeway, 2 bike boxes, 2 crosswalks, 22 pedestrian heads, reconstruct 200 linear feet of sidewalk, 14 ramps to ADA standard, perform 6 timing improvements, and enhance 9 existing crosswalks. CEQA - CE, 03/26/2025	11-1490 ATP/24-25 PS&E \$445,000 1123000141	2024-25 108-3290 RMRA 20.30.720.100	\$445,000
25 \$380,000 City of National City <u>SANDAG</u> 11-San Diego	22nd Street Separated Bikeway. This project is located along East 22nd Street between Wilson Avenue and D Avenue in National City. Construct separated bikeways, curb extensions, a new traffic signal with bike signals, continental crosswalks, curb ramps, and new sidewalk. MPO <u>Outcome/Outputs:</u> Construct 400 linear feet of sidewalk, reconstruct 500 linear feet of sidewalk, 19 ramps to ADA standard, install 650 linear feet of Class I path, 2,350 linear feet of Class IV bikeway, 6 bike racks, 1 traffic signal, and 18 crossing surface improvements. CEQA - CE, 03/26/2025	11-1528 ATP/24-25 PS&E \$380,000 1124000082	2024-25 108-3290 RMRA 20.30.720.100	\$380,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
26 \$6,344,000 San Diego Association of Governments <u>SANDAG</u> 11-San Diego	Bayshore Bikeway Barrio Logan Phase 2: Intersection Safety Improvements. Along Harbor Drive at Sampson Street, Schley Street, and 28th Street in the City of San Diego, Community of Barrio Logan. Install advanced preemption systems, ADA compliant sidewalks, wider medians, pavement markings, chain link fence, and traffic signal modification. MPO <u>Outcome/Outputs:</u> Install 3 advance preemption system improvements, modify 1 traffic signal, widen 81 linear feet of sidewalk, and reconstruct 74 linear feet of sidewalk. CEQA - CE, 12/05/2023 Right of Way Certification: 04/15/2025 Time extension for FY 23-24 CONST expires 06/30/2025	11-1533 ATP/23-24 CONST \$6,344,000 1124000258	2024-25 108-3290 RMRA 20.30.720.100	\$6,344,000
27 \$4,610,000 City of Santa Ana <u>OCTA</u> 12-Orange	Raitt Street Protected and Buffered Bike Lane Project. Raitt Street from Warner Avenue to Washington Avenue in the City of Santa Ana. Raitt Street Class IV protected bike lane from St. Gertrude to Santa Ana Boulevard, Class II bike lane from Warner to Occidental, and Class III bicycle boulevard from Santa Ana Boulevard to Washington. MPO <u>Outcome/Outputs:</u> Install 6,800 feet of Class II bike lanes, 2,850 feet of Class III bike lanes, 1,900 feet of Class IV bike lanes, 2 new traffic signals, 2 new pedestrian beacons and a road diet. CEQA - CE, 01/23/2023 Right of Way Certification: 4/4/2025 Time Extension for FY 23-24 CONST expires 06/30/2025.	12-1306 ATP/23-24 CONST \$4,610,000 1222000051	2024-25 108-3290 RMRA 20.30.720.100	\$4,610,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(1)	Locally-Administered Active Transportation Program Projects Off the State Highway System		Resolution FATP-2425-26	
28 \$130,000 City of Buena Park <u>OCTA</u> 12-Orange	Dale / Whitaker Complete Streets Project. City of Buena Park: Dale Street between Malvern Avenue and Auto Center Drive; Whitaker Street between Stanton Avenue and eastern City limits. Construction funding to install Class IV and Class II bikeways, continental crosswalks, enhanced pavement markings, traffic calming features, and upgraded bus stop amenities. MPO <u>Outcome/Outputs:</u> Construct 50 feet of new sidewalk and a curb ramp. Reconstruct 19 curb ramps to standard. Install 6,800 feet of Class II bike lanes, 3,800 feet of Class IV bike lanes, 4 bicycle loop detectors, 5 new crosswalks, 2 new pedestrian beacons, 5 raised medians, pedestrian amenities, shade trees and a road diet. CEQA - CE, 04/15/2025	12-1316 ATP/24-25 R/W \$130,000 1224000020	2024-25 108-3290 RMRA 20.30.720.100	\$130,000

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5w.(2), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR A STATE-ADMINISTERED ACTIVE TRANSPORTATION
PROGRAM PROJECT ON THE STATE HIGHWAY SYSTEM
(ADVANCEMENT FISCAL YEAR 2025-26)
RESOLUTION FATP-2425-27**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$1,044,000 for the State-Administered Active Transportation Program (ATP) Los Alamos Connected Community Project (PPNO 3111A), on the State Highway System, in Santa Barbara County, programmed in Fiscal Year 2025-26?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve an allocation of \$1,044,000 for the State-Administered ATP Los Alamos Connected Community Project (PPNO 3111A), on the State Highway System, in Santa Barbara County, programmed in 2025-26.

BACKGROUND:

The attached vote list describes the State-Administered ATP project totaling \$1,044,000. The Department is ready to proceed with this project, and is requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$1,044,000 be allocated for construction engineering for the State-Administered ATP project described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project No. Allocation Amount Recipient RTPA/CTC County Dist-Co-Rte Postmile	Project Title Location Project Description Project Support Expenditures	PPNO Program/Year Phase Prgm'd Amount Project ID Adv Phase EA	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(2)	State-Administered Active Transportation Program Project On the State Highway System (Advancement Fiscal Year 2025-26)			Resolution FATP-2425-27
1 \$1,044,000 Department of Transportation <u>SBCAG</u> Santa Barbara 05-SB-135 0.00/0.73	Los Alamos Connected Community Project. On Route 135, from 0.1 miles south of US 101/135 separation. Construct pedestrian and bicycle infrastructure for 0.9 miles of SR 135 to connect residential areas to downtown community corridor and Olga Reed elementary school in Los Alamos. Small Urban and Rural <u>Outputs</u> Crosswalk New curb ramp installed Repair existing sidewalk Repair/Upgrade curb ramp Bicycle lane mile(s) Facility Improvements Sidewalk mile(s) CEQA - ND, 04/22/2025 Concurrent consideration of funding under Resolution E-25-38; June 2025.	05-3111A ATP/25-26 PS&E \$1,044,000 0523000033 1 1Q450	505-3290 RMRA 20.10.720.100	\$1,044,000
	<u>Unit</u> Each Each LF Each Miles Each Miles	<u>Total</u> 23 42 575 17 1.2 25 0.9		

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.5w.(3), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: **ALLOCATION FOR LOCALLY-ADMINISTERED ACTIVE TRANSPORTATION
PROGRAM PROJECTS OFF THE STATE HIGHWAY SYSTEM
(ADVANCEMENTS FISCAL YEAR 2025-26)
RESOLUTION FATP-2425-28**

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$1,319,000 for five locally-administered Active Transportation Program (ATP) projects, off the State Highway System, programmed in Fiscal Year 2025-26?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$1,319,000 for five locally-administered ATP projects, off the State Highway System, programmed in 2025-26.

BACKGROUND:

The attached vote list describes five locally-administered ATP projects totaling \$1,319,000. The local agencies are ready to proceed with these projects, and are requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$1,319,000 be allocated from the Budget Act of 2024, Budget Act Item 2660-108-3290 for five locally-administered ATP projects described on the attached vote list.

Attachment

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(3)	Locally-Administered Active Transportation Program Projects Off the State Highway System (Advancements Fiscal Year 2025-26)		Resolution FATP-2425-28	
1 \$222,000 Southern California Association of Governments <u>LACMTA</u> 07-Los Angeles	Covina Town Center Bicycle and Pedestrian Safety and Connectivity (Gap Closure) Quick-Build Roadway. Covina Town Center, including Valencia Place, 3rd Avenue, 2nd Avenue, Baranca Avenue, Edna Place, Front Street, San Bernardino Road, and Badillo Street. The quick-build project proposes the following safety elements in the Covina Town Center: bike lanes; separated bicycle lanes and bike lane curbing; bike boulevards; wayfinding/directional signs; bike boxes and green conflict striping; parkway landscaping and boxed street trees; Rectangular Rapid Flashing Beacons; temporary curb extension delineators; high visibility crosswalks and advanced stop bar markers; and solar lighting. MPO <u>Outcome/Outputs:</u> Improve safety, connectivity, and accessibility for people and walking to key destinations, including regional transit, local commercial, hospital, and city services. CEQA - CE, 03/12/2025 Right of Way Certification: 04/28/2025	07-6437 ATP/25-26 CONST \$222,000 0725000253	2024-25 108-3290 RMRA 20.30.720.100	\$222,000
2 \$284,000 Southern California Association of Governments <u>LACMTA</u> 07-Los Angeles	CSUDH Transportation Demand Management Plan: LA 28 and Beyond. Creation of a Transportation Demand Management (TDM) Plan, with a focus on active transportation, for California State University Dominguez Hills that will fully integrate the needs of thousands of commuters, primarily coming from disadvantage communities, along with the larger regional transit networks and partners relying on the campus as a key transit hub. MPO <u>Outcome/Outputs:</u> How to leverage the university's location and resources to build infrastructure and partnerships that maximize regional sustainable mobility for visitors traveling to/through campus.	07-6438 ATP/25-26 CONST \$284,000 0725000254	2024-25 108-3290 RMRA 20.30.720.100	\$284,000

2.5 Highway Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.5w.(3)	Locally-Administered Active Transportation Program Projects Off the State Highway System (Advancements Fiscal Year 2025-26)		Resolution FATP-2425-28	
3 \$803,000 Southern California Association of Governments <u>LACMTA</u> 07-Los Angeles	Norwalk Boulevard Vision Zero Quick-Build Pedestrian Safety. Norwalk Boulevard from Mines Boulevard to Broadway Avenue in Los Angeles County. The quick-build project will install temporary curb extensions using flexible posts and striping, leading pedestrian intervals, hardened centerlines, painted median islands with flexible posts and striping, and right edge striping. MPO <u>Outcome/Outputs:</u> Improve safety for people, especially those walking and biking, on one of the top 50 Collision Concentration Corridors in Los Angeles County. CEQA - CE, 03/25/2025 Right of Way Certification: 04/28/2025	07-6439 ATP/25-26 CONST \$803,000 0725000252	2024-25 108-3290 RMRA 20.30.720.100	\$803,000
4 \$5,000 City of Waterford <u>StanCOG</u> 10-Stanislaus	Waterford Safe Routes to School Project - Washington Road. Project is located on Washington Road between South Pasadena and South Reinway Avenue; east side of South Reinway; west side of South Pasadena. Install bike, sidewalk, ramp, and crosswalk improvements. MPO <u>Outcome/Outputs:</u> Construct 1,715 linear feet of sidewalk, reconstruct 5 ramps to ADA standard, install 2,880 linear feet of Class III bike route, 7 crosswalks, 2 streetlights, and enhance 1 crosswalk.	10-3688 ATP/25-26 PA&ED \$5,000 1025000199	2024-25 108-3290 RMRA 20.30.720.100	\$5,000
5 \$5,000 City of Waterford <u>StanCOG</u> 10-Stanislaus	Waterford Safe Routes to School Project - Welch Street. Project is located on Welch Street between Bentley Street and Tim Bell Road. Installation of bicycle lanes, sidewalks, ramps, streetlights, and crosswalks. MPO <u>Outcome/Outputs:</u> Construct 1,260 linear feet of sidewalk, reconstruct 510 linear feet of sidewalk, 4 ramps to ADA standard, install 6,000 linear feet of Class III bike route, 2 streetlights, and 7 crosswalks.	10-3689 ATP/25-26 PA&ED \$5,000 1025000200	2024-25 108-3290 RMRA 20.30.720.100	\$5,000

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.6g.(1), Action Item

Prepared By: Keith Duncan, Chief
Division of Budgets

Subject: ALLOCATION FOR TRANSIT AND INTERCITY RAIL CAPITAL
PROGRAM PROJECTS
RESOLUTION TIRCP-2425-74

ISSUE:

Should the California Transportation Commission (Commission) approve an allocation of \$487,122,000 for 17 Transit and Intercity Rail Capital Program (TIRCP) projects?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission approve an allocation of \$487,122,000 for 17 TIRCP projects.

BACKGROUND:

The attached vote list describes 17 TIRCP projects totaling \$487,122,000. The local agencies are ready to proceed with these projects, and are requesting an allocation at this time.

FINANCIAL RESOLUTION:

Resolved, that \$487,122,000 be allocated from the Budget Acts of 2023 and 2024, Budget Act Items 2660-101-0046 and 2660-301-0046R for 17 TIRCP projects described on the attached vote list.

Attachment

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program		Resolution TIRCP-2425-74	
1 \$813,000 Humboldt Transit Authority <u>HCAOG</u> 01-Humboldt	<p>(2022:12) Expanding Transit Services and Introducing Zero-Emission Fleets on California's North Coast (Service Bays). Update three existing maintenance bays to code, allowing for the maintenance and servicing of the fuel cell electric buses.</p> <p><u>Outcome/Outputs:</u> Updating existing bus infrastructure that will support zero-emission buses and help reduce greenhouse gas emissions.</p> <p>CEQA - SE, 12/05/2022</p> <p>Right of Way Certification: N/A</p> <p>As part of this allocation request, the local agency is requesting to extend the period of contract award an additional 6 months beyond the 6-month deadline.</p>	<p>01-CP101E TIRCP/24-25 CONST \$487,000</p> <p>TIRCP/24-25 CONST \$326,000 S 0025000165 T516GD</p>	<p>2023-24 301-0046R GGRF 30.20.301.100</p> <p>2024-25 101-0046 PTA 30.10.030.200</p>	<p>\$487,000</p> <p>\$326,000</p>
2 \$33,359,000 Sonoma-Marin Area Rail Transit District <u>MTC</u> 04-Sonoma	<p>(2018:26) SMART Larkspur to Windsor Corridor (Windsor to Healdsburg North Extension). Complete progressive design-build documents and begin project development services as well as Early Works Packages of the SMART Windsor to Healdsburg Rail and Pathway North Extension.</p> <p><u>Outcome/Outputs:</u> Increased ridership, reduction in greenhouse gas emissions and improved safety.</p> <p>CEQA - FEIR, 07/17/2009</p> <p>Future consideration of funding approved under Resolution E-09-56; July 2009.</p> <p>Right of Way Certification: 6/17/2024</p> <p>The project will be delivered using the progressive design-build method.</p> <p>In January 2023, this project was awarded \$34,338,000 in Cycle 6 General Fund Augmentation funding.</p> <p>Concurrent Allocation Amendment under Resolution TIRCP-2425-71; June 2025.</p>	<p>04-CP041A TIRCP/24-25 CONST \$33,359,000 S 0025000190 R390GD</p>	<p>2023-24 301-0046R GF 30.20.301.100</p>	<p>\$33,359,000</p>

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program	Resolution TIRCP-2425-74		
3 \$1,000,000 Sonoma County Transportation Authority <u>MTC</u> 04-Sonoma	<p>(2022:21) Sonoma Regional Bus and Rail Connectivity Improvements (Network Integration). Network integration efforts for facilities, public information and marketing, and system network improvements to develop connections to other corridors.</p> <p><u>Outcome/Outputs:</u> Improved transit facilities, system network, and public information and marketing resulting in ridership growth and reductions in green house gas emissions.</p> <p>CEQA - SE, 04/25/2025</p> <p>Right of Way Certification: N/A</p> <p>As part of this allocation request, the local agency is requesting to extend the period of contract award an additional 6 months beyond the 6-month deadline.</p>	<p>04-CP100N TIRCP/24-25 CONST \$600,000</p> <p>TIRCP/24-25 CONST \$400,000 S 0025000167 T649GA</p>	<p>2023-24 301-0046R GGFRF 30.20.301.100</p> <p>2024-25 101-0046 PTA 30.10.030.200</p>	<p>\$600,000</p> <p>\$400,000</p>

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program		Resolution TIRCP-2425-74	
4 \$1,726,000 Santa Cruz Metropolitan Transit District <u>SCCRTC</u> 05-Santa Cruz	<p>(2023:02) Capitol Corridor Emerging Market Access Program (Contactless EMV Readers). Implementation of Contactless EMV fare payment readers across a fleet of 122 buses, including 53 hydrogen fuel cell zero-emission buses.</p> <p><u>Outcome/Outputs:</u> Increased ridership and reduction of greenhouse gas emissions through instant fare collection using contactless EMV readers to allow payment by contactless bank cards or smart devices, speeding up passenger boarding and reducing dwell time on the fixed route buss fleet, creating a seamless ticketing system and multimodal transfer facilities, making it convenient to reach Capitol Corridor stations and destinations served by connecting transit without the need of a vehicle.</p> <p>CEQA - CE/SE, 06/16/2023</p> <p>Right of Way Certification: N/A</p> <p>In April 2023, TIRCP 2023:02 was awarded \$1,726,000 Cycle 6 General Fund New Project Funding.</p> <p>As part of this allocation request, the local agency is requesting to extend the period of contract award an additional 6 months beyond the 6-month deadline.</p> <p>As part of this allocation request, the local agency is requesting to extend the period of project completion an additional 12 months beyond the 36-month deadline.</p> <p>The local agency is requesting non-proportional spending of TIRCP funds. At completion, project expenditures will be reconciled to ensure that TIRCP funds have been expended proportionally.</p>	05-CP108A TIRCP/24-25 CONST \$1,726,000 S 0025000178 T653GA	2023-24 301-0046R GF 30.20.301.100	\$1,726,000

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program		Resolution TIRCP-2425-74	
5 \$375,000 Santa Cruz Metropolitan Transit District <u>SCCRTC</u> 05-Santa Cruz	<p>(2023:22) Santa Cruz Zero Emission Intercity Transit Service Expansion Project (Integrated Ticketing). Replacement of fareboxes to accommodate the mainly non-cash transit passengers and implementation of Cal-ITP ticketing services.</p> <p><u>Outcome/Outputs:</u> Increased ridership and reduced greenhouse gas emissions through instant fare collection with replacement of fareboxes, to begin shift away from primarily cash-paying riders to a digital system, allowing payment by contactless bank cards or smart devices.</p> <p>CEQA - CE/SE, 06/16/2023</p> <p>Right of Way Certification: N/A</p> <p>In April 2023, TIRCP 2023:22 was awarded \$375,000 Cycle 6 General Fund New Project Funding.</p> <p>As part of this allocation request, the local agency is requesting to extend the period of contract award an additional 6 months beyond the 6-month deadline.</p> <p>As part of this allocation request, the local agency is requesting to extend the period of project completion an additional 12 months beyond the 36-month deadline.</p> <p>The local agency is requesting non-proportional spending of TIRCP funds. At completion, project expenditures will be reconciled to ensure that TIRCP funds have been expended proportionally.</p>	05-CP128C TIRCP/24-25 CONST \$375,000 S 0025000174 T545GG	2023-24 301-0046R GF 30.20.301.100	\$375,000

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program		Resolution TIRCP-2425-74	
6 \$320,185,000 Los Angeles County Metropolitan Transportation Authority <u>LACMTA</u> 07-Los Angeles	<p>(2018:10) Los Angeles Region Transit System Integration and Modernization Program (East San Fernando Valley Transit Corridor). Acquisition of parcels along the alignment of the project.</p> <p><u>Outcome/Outputs</u>: Reduction of GHG emissions, travel time savings, improved public health, increased attractiveness of transit-served areas for the addition of local jobs and housing, and supports the implementation of sustainable communities strategies.</p> <p>CEQA - EIR, 01/31/2021</p> <p>Future consideration of funding approved under Resolution E-23-41; March 2023.</p> <p>Contribution from other sources: \$2,596,934,100.</p> <p>The current scope of work consists of a Progressive Design-Build (PDB) project delivery method.</p> <p>Concurrent Multi-Funded LPP (Formulaic), STIP, and TIRCP CONST Allocation under Resolutions LPP-A-2425-35, FP-24-102, and TIRCP-2425-73; June 2025.</p> <p>In January 2023, this project was awarded \$600,000,000 in Cycle 6 General Fund Augmentation funding.</p> <p><u>ALLOCATION IS CONTINGENT UPON THE PASSAGE OF THE 2025-26 BUDGET ACT.</u></p>	07-4296 TIRCP/23-24 R/W \$320,185,000 S 0025000199 T244GG	2023-24 301-0046R GF 30.20.301.100	\$320,185,000

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program		Resolution TIRCP-2425-74	
7 \$4,284,000 City of Pasadena <u>LACMTA</u> 07-Los Angeles	<p>(2023:04) Pasadena Transit System Legacy Project (Hydrogen Fueling Station). Construction of a hydrogen fueling station meant to fuel the City of Pasadena's hydrogen fuel cell bus fleet in Los Angeles County.</p> <p><u>Outcome/Outputs:</u> Charging infrastructure for battery electric vehicles as an alternative fuel source will produce zero greenhouse gas emissions, improved transit service, and thus an increase in ridership for the City of Pasadena.</p> <p>CEQA - SE, 04/25/2025</p> <p>Right of Way Certification: N/A</p> <p>On April 24, 2023, this project was awarded \$14,424,000 in TIRCP Cycle 6 General Fund New Project funding.</p> <p>This project will be delivered using the progressive design-build method.</p>	07-CP110 TIRCP/24-25 CONST \$4,284,000 S 0025000176 T559GD	2023-24 301-0046R GF 30.20.301.100	\$4,284,000
8 \$21,777,000 City of Santa Monica <u>LACMTA</u> 07-Los Angeles	<p>(2023:06) Ridership Growth, Workforce Development, and Zero-Emission Regional Connectivity (Route 3 Charging Infrastructure). Construction of charging infrastructure for zero emission battery electric buses, purchase of charging cabinets, and installation of charging dispensers. Chargers will be used for the City of Santa Monica's Big Blue Bus (BBB) Route 3 buses, in Los Angeles County.</p> <p><u>Outcome/Outputs:</u> Charging infrastructure for battery electric vehicles as an alternative fuel source will produce zero greenhouse gas emissions, improved transit service, and thus an increase in ridership for the City of Santa Monica.</p> <p>CEQA - CE, 04/24/2025</p> <p>Right of Way Certification: N/A</p> <p>As part of this allocation request, the local agency is requesting to extend the period of contract award an additional 6 months beyond the 6-month deadline.</p> <p>As part of this allocation request, the local agency is requesting to extend the period of project completion an additional 12 months beyond the 36-month deadline.</p> <p>On April 24, 2023, this project was awarded \$22,977,000 in TIRCP Cycle 6 General Fund New Project funding.</p>	07-CP112 TIRCP/24-25 CONST \$21,777,000 S 0025000168 T633GB	2023-24 301-0046R GF 30.20.301.100	\$21,777,000

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program		Resolution TIRCP-2425-74	
9 \$35,941,000 City of Santa Monica <u>LACMTA</u> 07-Los Angeles	<p>(2024:05) The POWER of Transportation: Clean Air, Access, and Opportunity (Route 2 Charging Infrastructure).</p> <p>Construction of charging infrastructure for zero emission battery electric buses, purchase of charging cabinets, and installation of charging dispensers. Chargers will be used for the City of Santa Monica's Big Blue Bus (BBB) Route 2 buses, in Los Angeles County.</p> <p><u>Outcome/Outputs:</u> Charging infrastructure for battery electric vehicles as an alternative fuel source will produce zero greenhouse gas emissions, improved transit service, and thus an increase in ridership for the City of Santa Monica.</p> <p>CEQA - CE, 04/24/2025</p> <p>Right of Way Certification: N/A</p> <p>As part of this allocation request, the local agency is requesting to extend the period of contract award an additional 6 months beyond the 6-month deadline.</p> <p>As part of this allocation request, the local agency is requesting to extend the period of project completion an additional 20 months beyond the 36-month deadline</p>	<p>07-CP148 TIRCP/24-25 CONST \$17,970,000</p> <p>TIRCP/24-25 CONST \$17,971,000 S 0025000169 T650GA</p>	<p>2023-24 301-0046R GGRF 30.20.301.100</p> <p>2024-25 101-0046 PTA 30.10.030.200</p>	<p>\$17,970,000</p> <p>\$17,971,000</p>
10 \$45,000,000 San Joaquin Regional Rail Commission <u>SJCOG</u> 10-San Joaquin	<p>(2024:21) Bridging Rail Initiatives, Technology, and Education (BRITE) (Stockton Diamond Grade Separation).</p> <p>Supports the execution of the Construction and Management agreements with Burlington Northern Santa Fe Railway (BNSF) and Union Pacific Railroad (UPRR), as well as related utility relocation agreements.</p> <p><u>Outcome/Outputs:</u> Improvement of freight rail efficiency, increased reliability, and reduction of greenhouse gas emission through decreased fuel consumption from idling locomotives.</p> <p>CEQA - FEIR, 06/24/2021 NEPA - FONSI, 07/26/2022</p> <p>Future consideration of funding approved under Resolution E-21-71; June 2021.</p> <p>The local agency is requesting non-proportional spending of TIRCP funds. At completion, project expenditures will be reconciled to ensure that TIRCP funds have been expended proportionally.</p>	<p>10-CP163A TIRCP/24-25 R/W \$22,500,000</p> <p>TIRCP/24-25 R/W \$22,500,000 S 0025000171 R645GB</p>	<p>2023-24 301-0046R GGRF 30.20.301.100</p> <p>2024-25 101-0046 PTA 30.10.030.200</p>	<p>\$22,500,000</p> <p>\$22,500,000</p>

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program	Resolution TIRCP-2425-74		
11 \$300,000 San Joaquin Regional Rail Commission <u>SJCOG</u> 10-San Joaquin	<p>(2024:21) Bridging Rail Initiatives, Technology, and Education (BRITE) (The Rail Academy of Central California (TRACC)). The Rail Academy of Central California (TRACC) Workforce Development Program will support the instruction of railroad industry courses, provide supplies, and facilitate engagement with potential students.</p> <p><u>Outcome/Outputs:</u> Education and economic opportunities to underserved communities in San Joaquin County and neighboring areas by providing a streamlined education program to prepare the community for high-paying job opportunities in the Railroad industry.</p> <p>CEQA - N/A</p> <p>Right of Way Certification: N/A</p> <p>The local agency is requesting non-proportional spending of TIRCP funds. At completion, project expenditures will be reconciled to ensure that TIRCP funds have been expended proportionally.</p>	<p>10-CP163D TIRCP/24-25 CONST \$150,000</p> <p>TIRCP/24-25 CONST \$150,000 S 0025000172 R645GC</p>	<p>2023-24 301-0046R GGRF 30.20.301.100</p> <p>2024-25 101-0046 PTA 30.10.030.200</p>	<p>\$150,000</p> <p>\$150,000</p>
12 \$1,500,000 Imperial County Transportation Commission <u>ICTC</u> 11-Imperial	<p>(2024:12) Connecting Vulnerable Communities: Calexico East Port of Entry (POE) Intermodal Transportation Center (ITC) & System Improvements Project (New Intermodal Transportation Center). Acquisition of private property to construct a new intermodal transportation center, including station amenities.</p> <p><u>Outcome/Outputs:</u> Improved travel experience at the Calexico East Port of Entry by making trips safer, easier, and more comfortable. Increased transit ridership and reduction of vehicle emissions that contribute to climate change.</p> <p>CEQA - SE, 05/14/2025</p>	<p>11-CP155A TIRCP/24-25 R/W \$750,000</p> <p>TIRCP/24-25 R/W \$750,000 S 0025000177 T561GB</p>	<p>2023-24 301-0046R GGRF 30.20.301.100</p> <p>2024-25 101-0046 PTA 30.10.030.200</p>	<p>\$750,000</p> <p>\$750,000</p>

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program		Resolution TIRCP-2425-74	
13 \$877,000 Anaheim Transportation Network <u>OCTA</u> 12-Orange	(2022:01) ATN FAST (Family of Advanced Solutions for Transit): Revolutionizing Transit for a Global Audience (Purchase 7 ZEV vans for EVE: New Airport Connection & Night Owl). Installation of contactless payment readers and acquisition of seven new all-electric shuttle vans for transit service between John Wayne Airport, Anaheim Convention Center, and Disneyland Resort in Orange County. <u>Outcome/Outputs:</u> Increased ridership through new and expanded service, reduced greenhouse gas emissions, improved integration with other transit services. CEQA - CE, 05/21/2023 Right of Way Certification: N/A Concurrent Allocation Amendment under Resolution TIRCP-2425-65; June 2025.	12-CP086 TIRCP/24-25 CONST \$526,000 TIRCP/24-25 CONST \$351,000 S 0025000183 T493GE	2023-24 301-0046R GGRF 30.20.301.100 2024-25 101-0046 PTA 30.10.030.200	\$526,000 \$351,000
14 \$313,000 Anaheim Transportation Network <u>OCTA</u> 12-Orange	(2022:01) ATN FAST (Family of Advanced Solutions for Transit): Revolutionizing Transit for a Global Audience (Purchase 10 (5 passenger) ZEV Microtransit for FRAN). Purchase of ten all-electric 5 passenger zero-emission vehicles to extend the Free Rides Around the Neighborhood (FRAN) service to Historic Orange and Chapman University in Orange County. <u>Outcome/Outputs:</u> Increased ridership through new and expanded service, reduced greenhouse gas emissions, improved integration with other transit services. CEQA - CE, 05/21/2023 Right of Way Certification: N/A Concurrent Allocation Amendment under Resolution TIRCP-2425-66; June 2025.	12-CP086A TIRCP/24-25 CONST \$188,000 TIRCP/24-25 CONST \$125,000 S 0025000185 T493GF	2023-24 301-0046R GGRF 30.20.301.100 2024-25 101-0046 PTA 30.10.030.200	\$188,000 \$125,000

2.6 Mass Transportation Financial Matters

Project # Allocation Amount Recipient <u>RTPA/CTC</u> District-County	Project Title Location Project Description	PPNO Program/Year Phase Prgm'd Amount Project ID	Budget Year Item # Fund Type Program Code	Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program		Resolution TIRCP-2425-74	
15 \$12,836,000 Anaheim Transportation Network <u>OCTA</u> 12-Orange	<p>(2022:01) ATN FAST (Family of Advanced Solutions for Transit): Revolutionizing Transit for a Global Audience (Purchase of 15 - 40' Zero-Emission Buses for ART Fleet Conversion). Purchase fifteen 40-foot zero-emission buses to expand service and complete full transition to an all-electric fleet in Orange County.</p> <p><u>Outcome/Outputs:</u> Increased ridership through new and expanded service, reduced greenhouse gas emissions, improved integration with other transit services.</p> <p>CEQA - CE, 05/21/2023</p> <p>Right of Way Certification: N/A</p> <p>Concurrent Allocation Amendment under Resolution TIRCP-2425-67; June 2025.</p>	<p>12-CP086C TIRCP/24-25 CONST \$7,702,000</p> <p>TIRCP/24-25 CONST \$5,134,000 S 0025000184 T493GG</p>	<p>2023-24 301-0046R GGRF 30.20.301.100</p> <p>2024-25 101-0046 PTA 30.10.030.200</p>	<p>\$7,702,000</p> <p>\$5,134,000</p>
16 \$3,836,000 Orange County Transportation Authority <u>OCTA</u> 12-Orange	<p>(2023:15) Central Mobility Loop (Garden Grove Hydrogen Fueling Station). Construction of a hydrogen fueling station to fuel zero-emission hydrogen fuel-cell buses on OCTA's fixed-route bus system, Orange County Bus 'OC Bus'.</p> <p><u>Outcome/Outputs:</u> Reduction of greenhouse gas emissions through investments in clean energy supporting vehicle infrastructure. Support clean vehicle technology, reduced vehicle miles traveled, and benefits priority populations.</p> <p>CEQA - CE, 02/09/2024</p> <p>Right of Way Certification: N/A</p> <p>As part of this allocation request, the local agency is requesting to extend the period of project award an additional 6 months beyond the 6-month deadline.</p> <p>This project will be delivered using the design-build method.</p> <p>On April 24, 2023, this project was awarded \$39,407,000 in TIRCP Cycle 6 General Fund New Project funding.</p>	<p>12-CP121 TIRCP/24-25 CONST \$3,836,000 S 0025000182 T618GE</p>	<p>2023-24 301-0046R GF 30.20.301.100</p>	<p>\$3,836,000</p>

Project #	PPNO	
Allocation Amount	Program/Year	Budget Year
Recipient	Phase	Item #
<u>RTPA/CTC</u>	Prgm'd Amount	Fund Type
District-County	Project ID	Program Code
		Amount by Fund Type
2.6g.(1)	Transit and Intercity Rail Capital Program	Resolution TIRCP-2425-74
17		
\$3,000,000	(2018:19) Valley Rail (Del Paso Siding Extension). Extends and upgrades the existing Del Paso Siding Track in Sacramento County, including a new Arcade Creek bridge, levee flood gate structures, and industrial spur improvements.	75-CP035F TIRCP/24-25 R/W \$1,330,000
San Joaquin Regional Rail Commission <u>SACOG</u> 75-Sacramento	<u>Outcome/Outputs:</u> Improved rail and transit connectivity, improved local and regional system integration, increased ridership, and improved safety.	2023-24 301-0046R GGRF 30.20.301.100
	CEQA - ND, 12/03/2020	2024-25 101-0046 PTA 30.10.030.200
	Future consideration of funding approved under Resolution E-20-130; December 2020.	\$1,670,000 S 0025000173 R484GX
	The local agency is requesting non-proportional spending of TIRCP funds. At completion, project expenditures will be reconciled to ensure that TIRCP funds have been expended proportionally.	

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(1), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR
STATE HIGHWAY OPERATION AND PROTECTION PROGRAM PROJECTS
WAIVER 25-95**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for 105 State Highway Operation and Protection Program (SHOPP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for 105 SHOPP projects, for the time periods as identified in the attachment.

BACKGROUND:

The Department will not be able to request an allocation of funds by the June 30, 2025, deadline. The attachment describes the details of the projects and the explanation for the delays.

The SHOPP Guidelines stipulate that the Commission may approve waivers to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u>	<u>No.</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u> <u>Months</u>	<u>Extended</u> <u>Deadline</u>	<u>Justification</u>
	1	01	Del Norte	101	M0.0/46.492	0K690	1142	0120000135	Culvert(s)	PS&E/ RW Sup	11	May 2026	In February 2023, six culverts were added to the project. These culverts are within the proposed project limits and in poor or critical condition. They were scheduled to be replaced by an adjacent emergency project. However, the emergency project was not able to replace them as anticipated, since they are situated in an environmentally sensitive area that required environmental technical studies. The most efficient, cost effective and expedient way to replace them was to add them to the scope of this project. These new locations required additional survey and preliminary design work, and since they were added to the project later in the phase, it delayed the environmental work. Despite the Department increasing the frequency of Project Development Team (PDT) meetings to help work through issues faster and help mitigate and minimize delays to the project, additional time is required to complete the Project Approval and Environmental Document (PA&ED) phase. The delay to PA&ED completion is delaying the start of the Plans, Specifications, and Estimate (PS&E) and Right of Way (R/W) Support phases. An 11-month allocation time extension is requested, delaying the PS&E and R/W Support (Sup) allocations to May 2026.
	2	01	Humboldt	101	R53.7/M54.2	0A111	2301B	0116000148	Bridge	Con/ Con Sup	4	Oct 2025	This project is a seismic retrofit and partial bridge replacement. Currently, Pacific Gas and Electric (PG&E) has a high-risk gas line in conflict with the project that requires relocation, drilling under the river, and a State Lands permit. The California Department of Fish and Wildlife (CDFW) is requiring a copy of the executed State Lands permit prior to issuing the final 1602 permit. The State Lands permit is scheduled to be heard at their June 2025 meeting. The project will be Ready to List (RTL) after receiving both permits. A 4-month allocation time extension is requested, delaying the Construction (CON) and CON Sup allocations to October 2025.
	3	01	Humboldt	299	R8.0/R9.2	0J801	2535	0120000032	Major Damage	Con/ Con Sup	4	Oct 2025	In February 2025, during the 401 Permit application process, the Department was informed that the standard filter fabric for Rock Slope Protection (RSP) at drainage outlets is no longer accepted by the North Coast Regional Water Quality Control Board. This required a new drainage design at several locations, updating costs and quantities, and delaying the final drainage design. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u>	<u>No.</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u> <u>Months</u>	<u>Extended</u> <u>Deadline</u>	<u>Justification</u>
	4	01	Mendocino	1	43.3/44.2	40110	4490	0100000154	Bridge	PS&E/ RW Sup	4	Oct 2025	Due to the complexity of the environmental document and sensitivity of the project area, the internal legal and technical reviews took longer than anticipated. In addition, concurrence from State Historic Preservation Office (SHPO) is taking longer than expected. Therefore, additional time is required to complete PA&ED. The delay to PA&ED completion is delaying the start of PS&E and R/W Support phase. A 4-month allocation time extension is requested, delaying the PS&E and R/W Sup allocations to October 2025.
	5	01	Mendocino	20	33.3/33.4	0E091	4587M	0121000081	Bridge	Con/ Con Sup	4	Oct 2025	This is a mitigation project for revegetation for a bridge replacement project. This project is scheduled to begin after the completion of the parent construction work. The completion of the parent construction project is anticipated to be in winter of 2025. However, the revegetation will be most successful if performed during the spring weather. Consequently, the project team decided to delay the start of revegetation work to spring of 2026, delaying the RTL until August 2025. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.
	6	01	Mendocino	101	10.7/11.2	0H140	4675	0117000115	Mandates	Con/ Con Sup	4	Oct 2025	This project underwent significant redesign due to the removal of the drainage rehabilitation portion and electrical elements. This was due to the local water utility district's inability to relocate utility conflicts due to funding constraints. In addition, PG&E requested 12 months to perform their required utility relocations prior to construction. PG&E has agreed to reduce their time in the spirit of partnership, but at extremely high risk. Therefore, to better coordinate with PG&E relocation efforts and lessen the possibility of right of way delay claims from the contractor, additional time is needed. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.
	7	02	Plumas	89	0.0/8.7	3H950	3709	0218000057	Roadway	PS&E/ RW Sup	6	Dec 2025	Due to the realignment of the roadway and raising of the road profile, an evaluation of the floodplain was performed. This evaluation found that the Water Surface Level (WSL) will be higher than the allowed threshold, triggering a revision to the Federal Emergency Management Agency (FEMA) flood map. A refined model and design are necessary to lower the WSL below the allowed threshold. Therefore, additional time is required to complete PA&ED. The delay to PA&ED completion is delaying the start of PS&E and R/W Sup phases. A 6-month allocation time extension is requested, delaying the PS&E and R/W Sup allocations to December 2025.

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u> <u>No.</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u> <u>Months</u>	<u>Extended</u> <u>Deadline</u>	<u>Justification</u>
8	02	Shasta	5	R14.8/R20.0	1J380	3790	0220000064	Collision	Con/ Con Sup	19	Jan 2027	This project is being combined for construction with a non-SHOPP project, EA 0H920/PPNO 02-3597. The non-SHOPP project is being delayed by 19 months due to Federal Highway Administration (FHWA) requesting modifications to the truck-only lane strategy. The new managed lane strategy necessitates updates to several key deliverables to proceed with the implementation, such as, revising traffic modeling and amending environmental documentation. The Department recommends delaying this project as well to keep two projects combined to ensure they are conforming to each other. A 19-month allocation time extension is requested, delaying the CON and CON Sup allocations to January 2027.
9	03	Butte	70	46.0/ 47.0	3H541	2293A	0322000244	Major Damage	Con Sup	4	Oct 2025	This is a mitigation project for a major damage restoration project. The parent project has not progressed enough to begin replanting and thus this project is not ready to start. A 4-month allocation time extension is requested, delaying the CON Sup allocation to October 2025.
10	03	Butte	162	15.6/18.5	0J440	2638	0319000285	Roadway	Con/ Con Sup	6	Dec 2025	The project requires a new Construction and Maintenance Agreement (C&M) with Union Pacific Railroad (UPRR) due to work that is being done within their right of way under the underpass. The project assumed UPRR approval could be completed with the Railroad Clearance memo. The need for a C&M was identified in November 2023 after UPRR reviewed the draft plans. Due to delays in subsequent UPRR reviews and design updates needed based on those reviews, completion of the C&M is delaying R/W Certification and RTL. A 6-month allocation time extension is requested, delaying the CON and CON Sup allocations to December 2025.
11	03	Placer	80	50.7	0J550	5141	0319000296	Facilities	Con/ Con Sup	4	Oct 2025	The project needs approval from State Fire Marshal (SFM) to proceed. The review and response to comments from SFM have delayed, thus delaying the project RTL. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u>	<u>No.</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u> <u>Months</u>	<u>Extended</u> <u>Deadline</u>	<u>Justification</u>
	12	04	Alameda	92	R2.4/6.8	0AA14	2031T	0419000445	Roadway	Con/ Con Sup	6	Dec 2025	Additional time is necessary to complete positive underground utility verifications, to apply those verifications into the final design plans, and to accommodate potential delays with the issuance of a Biological Opinion (BO) by the U.S. Fish and Wildlife Service (USFWS). Given the broad scope of this project including pavement rehabilitation, curb ramp upgrades, drainage and electrical enhancements, and guardrail installation, the risk of delays due to unidentified utility conflicts remains significant. Additionally, the the USFWS has advised the timeline for securing the BO remains subject to delay due to their current staffing workload and recent updates to federal regulatory guidance. A 6-month allocation time extension is requested, delaying the CON and CON Sup allocations to December 2025.
	13	04	Contra Costa	680	R0.0/R7.7	0AA24	2032P	0419000455	Roadway	Con/ Con Sup	6	Dec 2025	Additional time is necessary to complete positive underground utility verifications and to apply those verifications into the final design plans. Given the broad scope of this project including pavement rehabilitation, curb ramp upgrades, drainage and electrical enhancements, and guardrail installation, the risk of delays due to unidentified utility conflicts remains significant. A 6-month allocation time extension is requested, delaying the CON and CON Sup allocations to December 2025.
	14	04	Marin	1	28.4/28.6	0G642	0756K	0413000350	Bridge	Con/ Con Sup	12	Jun 2026	This project is in an environmentally sensitive rural area and requires an array of environmental permits including 404, 401, 1602, National Marine Fisheries Service, and USFWS BO, Incidental Take Permit, State Lands Permit, and Coastal Development Permit. Due to extensive coordination with regulatory agencies, environmental certification is delayed to June 2026. Additionally, this project includes 10 parcels to complete R/W Certification. Three parcels include utility relocations among PG&E, American Telephone & Telegraph Company, and North Marin Water District, three other parcels are currently in the condemnation process and one parcel is among PG&E for a redwood tree that is within the temporary overhead easement. The R/W Certification is delayed to May 2026. A 12-month allocation time extension is requested, delaying the CON and CON Sup allocations to June 2026.

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u>	<u>Extended</u>	<u>Justification</u>
<u>No.</u>										<u>Months</u>	<u>Deadline</u>	
15	04	Marin	101	3.4/3.7	2Y050	2917D	0423000061	Multiple Objective	PS&E/ RW Sup	7	Jan 2026	This project is both politically sensitive and technically complex. For Design, during the PA&ED phase, a fourth design alternative was introduced to align with the County's efforts to address flooding in Marin City. Additionally, Route 101 at this location lacks a feasible detour route, requiring the development of multiple stage construction scenarios. For R/W, a Permit to Enter (PTE) was not signed until February 2025 due to limited cooperation from the property owner which delayed the environmental studies at this location. For environmental, the introduction of the fourth design alternative altered the original project footprint and triggered the need for additional environmental studies. Additionally, a transition to a new environmental consultant support contractor further delayed the revision of the environmental document. As a result of these issues, completion of PA&ED phase is anticipated by October 2025. A 7-month allocation time extension is requested, delaying the PS&E and R/W Sup allocations to January 2026.
16	04	San Mateo	101	6.7/7.6	2J73A	2916W	0423000062	Bridge	Con Sup	12	Jun 2026	This biological monitoring mitigation project for the parent bridge and drainage replacement project is scheduled to begin at the end of construction of the parent project. The parent bridge and drainage replacement project is anticipated to complete construction in July 2026. A 12-month allocation time extension is requested, delaying the CON Sup allocation to June 2026.
17	04	Sonoma	116	21.6/R22.6	3Q640	2029L	0419000234	Collision	PS&E/ RW Sup	9	Mar 2026	Additional time is needed to complete all 18 PTE's from private property owners for environmental studies and tribal consultation. The Department has made efforts to expedite such completion including the exploration of multiple design variations to avoid impacting an existing gas station property and also made layout refinements based on survey data to ensure greater permit accuracy and mitigate further delay. PA&ED completion is anticipated in January 2026. A 9-month allocation time extension is requested, delaying the PS&E and R/W Sup allocations to March 2026.

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u>	<u>No.</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u> <u>Months</u>	<u>Extended</u> <u>Deadline</u>	<u>Justification</u>
	18	05	San Luis Obispo			1K680	2920	0518000234	Facilities	Con/ Con Sup	12	Jun 2026	The project alternative for constructing the new maintenance station included drilling a groundwater well and installing an on-site septic system, however, the test wells showed little to no groundwater, and multiple percolation tests failed. Additional time is needed to move forward with a phased approach to build only a portion of the maintenance station meanwhile annexation by the city of the project site will facilitate access to the City of San Luis Obispo utilities and enable the full build. The additional time will be used to obtain SFM approval, finalize PS&E documents to reflect reduced scope, and RTL the project in time for an allocation in June 2026. A 12-month allocation time extension is requested, delaying the CON and CON Sup allocations to June 2026.
	19	06	Fresno	41	R0.0/R6.202	1A790	7064	0620000077	Roadway	Con/ Con Sup	4	Oct 2025	The project is intended to be combined with the non-SHOPP project EA 06-0S370. With that, the non-SHOPP project is experiencing delays in securing all the funding necessary in order for it to be ready to request the construction allocation. Furthermore, that project has delays in achieving R/W Certification 3W due to utility relocation plans that are not approved, and the utility agreements still need to be finalized. The anticipated R/W Cert 3W is June 30, 2025, in time for an allocation in October 2025. A 4-month allocation time extension is requested, delaying the CON and Con Sup allocations to October 2025.
	20	06	Kern	5	0.7/1.1	0X570	6976	0618000101	Roadside	Con/ Con Sup	4	Oct 2025	The project has experienced longer than anticipated review and approval from the Regional Water Quality Control Board (RWQCB) for an amended waste discharge permit. The permit was submitted in December 2024 with the expectation that it would be obtained in 1 to 2 months. The Department continues to work with the RWQCB with the expectation that permit will be obtained and RTL achieved in time for a construction allocation in October 2025. A 4-month allocation time extension is requested, delaying the construction and construction support allocations to October 2025.
	21	06	Kern	33	14.4/17.9	0W830	6950	0617000305	Roadway	PS&E/ RW Sup	6	Dec 2025	The project is experiencing delay in PA&ED requiring additional time to analyze a cost prohibitive estimate beyond the programmed amount. The time will be needed to analyze alternatives including reducing scope to a Capital Preventative Maintenance (CAPM) pavement strategy delaying PA&ED. A 6-month allocation time extension is requested, delaying the PS&E and R/W Sup allocations to December 2025.

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u> <u>Months</u>	<u>Extended</u> <u>Deadline</u>	<u>Justification</u>
22	06	Kern	223	R20.1/21.3	0Y150	6984	0619000002	Roadway	Con/ Con Sup	12	Jun 2026	The project has a continued impact from the discovery that a parcel is on the Hazardous Waste and Substances Sites (Cortese) List, requiring an elevated environmental clearance, extensive coordination, and a public hearing. Additionally, 20 of the 24 parcel acquisitions are outstanding delaying R/W certification and RTL. A 12-months allocation time extension is requested, delaying the CON and CON Sup to June 2026.
23	07	Los Angeles	2	R18.7/R18.7	37310	5781	0720000248	Facilities	Con/ Con Sup	12	Jun 2026	This project is delayed to satisfy a state requirement and for efficiency purposes. Senate Bill (SB) 1203 requires net-zero emission of greenhouse gases for State agency operations by January 2035. In response, the Department has chosen to add decarbonization to this project for the replacement of mechanical equipment at the Los Angeles Regional Transportation Management Center to avoid future throw-away costs. For additional cost efficiency purposes, this project will be combined at construction with SHOPP TMC Facility project EA 37300 in Fiscal Year 2025-26. A 12-month allocation time extension is requested, delaying the CON and CON Sup allocations to June 2026.
24	07	Los Angeles	101	17.1/25.5	36300	5640	0719000310	Mobility	Con/ Con Sup	4	Oct 2025	The necessary utility potholing for the project has been delayed due to delays that were experienced in finalizing the new District potholing contract. Additionally, the City of Los Angeles requires both a permit for any potholing activities and the submission of traffic control plans, which will cause further delays. As a result, the project team will need additional time to update the PS&E package and revise the design to avoid utility conflicts. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.
25	07	Los Angeles	110	17.98	39010	6023	0722000333	Bridge	PS&E/ RW Sup	7	Jan 2026	The PA&ED phase has been delayed due to the need for an Advanced Planning Study (APS), which was not included in the original schedule. This study, recently completed, was essential for conducting a comprehensive seismic analysis and finalizing the bridge repair strategy, including identifying necessary strengthening requirements. As a result, the Project Report requires additional time to complete, as several functional area deliverables were affected by the outcomes of the APS. A 7-month allocation time extension is requested, delaying the PS&E and R/W Sup allocations to January 2026.

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u> <u>No.</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u> <u>Months</u>	<u>Extended</u> <u>Deadline</u>	<u>Justification</u>
26	07	Los Angeles	134	0.0/R13.3	34710	5375	0718000094	Roadway	Con/ Con Sup	9	Mar 2026	This project has absorbed additional work from the SHOPP Pavement Preservation project EA 31170, an earlier construction project that was unable to complete its guardrail upgrades due to underground utility conflicts. Redesigning around existing underground utilities, salvaging unfinished guardrail equipment, and transferring standards from 2018 to 2024 standards has been extremely complex and requires more time than originally anticipated. As a result, a 9-month allocation time extension is requested, delaying the CON and CON Sup allocations to March 2026.
27	07	Los Angeles	170	R17.6	37190	5743	0720000155	Mandates	Con	4	Oct 2025	An electrical service request submitted to the Los Angeles Department of Water and Power (LADWP) Power Division, took longer than expected to process due to the agency's staffing shortages and the added complexity of consolidating Valley Plaza Park's existing electrical services with those proposed for the project. A 4-month allocation time extension is requested, delaying the CON allocation to October 2025.
28	07	Los Angeles	170	R18.0	37290	5806	0720000234	Mandates	Con	4	Oct 2025	An electrical service request submitted to the Los Angeles Department of Water and Power (LADWP) Power Division, took longer than expected to process due to the agency's staffing shortages and the added complexity of consolidating Valley Plaza Park's existing electrical services with those proposed for the project. A 4-month allocation time extension is requested, delaying the CON allocation to October 2025.
29	07	Los Angeles	405	25.9/43.8	36330	5643	0719000313	Mobility	Con/ Con Sup	4	Oct 2025	Necessary utility verification for the project has been delayed due to finalization of the new district utility verification contract. Additionally, the City of Los Angeles requires both a permit for any utility verification activities and the submission of traffic control plans. As a result, the project team will need additional time to update the PS&E package and revise the design to avoid utility conflicts. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.
30	07	Ventura	1	7.8/9.7	37960	6217	0721000246	Mandates	PS&E/ RW Sup	4	Oct 2025	Unanticipated percolation testing, required to evaluate the suitability of stormwater Best Management Practices (BMP) locations along State Route (SR) 1 in Ventura County, caused delays in the approval of the environmental document. Additionally, the testing required clearance from the Coastal Commission, which could only be granted after cultural studies were completed to assess potential archaeological resources within the corridor. A 4-month allocation time extension is requested, delaying the PS&E and R/W Sup allocations to October 2025.

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u>	<u>No.</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u> <u>Months</u>	<u>Extended</u> <u>Deadline</u>	<u>Justification</u>
	31	07	Ventura	23	22.5/22.9	37630	5801	0721000094	Culvert(s)	Con/ Con Sup	9	Mar 2026	In March 2025, the conflict between the proposed concrete box culvert and existing high pressure gas line was identified. The project team determined that utility relocation is not a viable option and proceeded with the revision to the design for a concrete lined v-ditch. This change requires a complete overhaul of the PS&E package, requiring additional time. A 9-month allocation time extension is requested, delaying the CON and CON Sup allocations to March 2026.
	32	08	Riverside	10	R58.0	1L060	3014U	0819000141	Facilities	Con/ Con Sup	4	Oct 2025	This project involves building work and requires a SFM permit. Plans were submitted to the SFM as early as October 2024 with reviews expected to take six weeks. However, updated requirements now require a 30-week review. The permit is expected no later than June 2025. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.
	33	08	Riverside	371	67.1/67.4	1N530	3021J	0823000053	Collision	PS&E/ R/W Sup	12	Jun 2026	Due to a recent discovery of cultural resources within the project limits, the environmental document has been elevated to a higher level environmental document. In consultation with the Cahuilla Indian Tribe, several studies will need to be conducted to complete this higher level document, including historic property surveys and archaeological surveys. The Department anticipates the PA&ED phase to be completed in April 2026. A 12-month allocation time extension is requested, delaying the PS&E and R/W Sup allocations to June 2026.
	34	08	San Bernardino	38	0.0/1.3	1K940	3014N	0819000130	Roadway	Con/ Con Sup	4	Oct 2025	This project is experiencing delay due to a utility conflict. The utilities are fire hydrants and meters, owned by City of Redlands and requires a utility agreement. The utility agreement has since been completed and is currently under review by the Department. R/W Cert and RTL is anticipated to be completed June 2025. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.
	35	08	San Bernardino	210	R24.0	1M560	3019V	0821000140	Collision	Con/ Con Sup	4	Oct 2025	This project is experiencing delay due to R/W acquisition. A RON was approved at the May 2025 Commission meeting, and thereafter a R/W Certification is being submitted for approval in June 2025. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u>	<u>Extended</u>	<u>Justification</u>
<u>No.</u>										<u>Months</u>	<u>Deadline</u>	
36	10	Alpine	4	26.8/31.2	1F720	3488	1017000180	Culvert(s)	Con/ Con Sup	12	Jun 2026	The project continues to experience delay in parcel acquisitions due to vacant parcels and from the winter closure of SR 4 in Alpine County. Reopening of the route is anticipated near Memorial Day. Outstanding are 14 of the 16 parcels including one from the United States Forest Service (USFS) which requires a special use permit. The permit process began in November 2024 and expected to be completed by November 2025. Meanwhile the condemnation packages will be submitted at the October 2025 Commission meeting, R/W Cert 3W by March 2026, and RTL by April 2026 in time for an allocation in June 2026. A 12-month allocation time extension is requested, delaying the CON and CON Sup allocations to June 2026.
37	10	Merced	140	16.0/16.5	1H020	3438	1017000175	Mobility	Con/ Con Sup	9	Mar 2026	The project is experiencing a delay in obtaining the utility agreement with Merquin Irrigation District (MID) which began in July 2024 and continued in January 2025 for the determination of liability. The MID approval of the preliminary engineering agreement is anticipated by April 2025. Then, the relocation is anticipated to be completed 5 months thereafter in time for an RTL in late December 2025. A 9-month allocation time extension is requested, delaying the CON and CON Sup allocations to March 2026.
38	10	San Joaquin	120	10.82	1Q010	3620	1022000221	Collision	PS&E/ RW Sup	4	Oct 2025	The project received the Storm Water Data Report in March 2025 and has since experienced delay in obtaining necessary R/W to accommodate the needed Treatment Best Management Practices (TBMPs). The TBMPs are substantial in size and require construction outside current right of way limits. Additional time is needed to revise mapping and complete additional environmental studies with a target completion date by August 2025. A 4-month allocation time extension is requested, delaying PS&E and R/W Sup allocations to October 2025.
39	10	Stanislaus	33	18.6/19.2	1N340	3560	1021000169	Collision	PS&E/ RW Sup	6	Dec 2025	The project is experiencing delay in completing PA&ED due to the Safety Index (SI) not meeting the minimum threshold. Additional time is needed to explore viable options to avoid the no-build alternative. A 6-month allocation time extension is requested, delaying the PS&E and R/W Sup allocations to December 2025.
40	11	San Diego	5	R49.3/R53.2	43100	1386	1119000214	Mandates	Con/ Con Sup	4	Oct 2025	During 60 percent constructability review, additional drainage and Intersection Control Evaluation studies were found to be required for the reconfiguration of the Mission Avenue intersection and T-ups. Also, additional utility verification on Mission Avenue required unexpected time to resolve a utility conflict, further delaying PS&E. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.

Time Extension/Waiver - Project Construction and Support Allocation Deadline

<u>Index</u>	<u>No.</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested</u> <u>Months</u>	<u>Extended</u> <u>Deadline</u>	<u>Justification</u>
	41	11	San Diego	76	R0.0/R8.1	43091	1390	1119000198	Roadway	Con/ Con Sup	7	Jan 2026	The drainage strategy for this project was changed from cast-in-place pipe to replacement of existing culvert. This change in strategy resulted in an additional permit. In addition, during the design phase, multiple curb ramp locations were redesigned due to conflict with the existing electrical features. Additional coordination is ongoing with the utility company for the relocation of a utility pedestal. Therefore, a 7-month allocation time extension is requested, delaying the CON and CON Sup allocations to January 2026.
	42	11	San Diego	76	R17.5/52.2	43027	1307	1118000084	Culvert(s)	Con/ Con Sup	4	Oct 2025	A large number of culverts located in rural and remote areas has resulted in additional time needed to complete PS&E and accompanying Environmental and R/W deliverables. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.
	43	11	San Diego	79	11.2/44.7	43081	1384	1119000161	Roadway	Con/ Con Sup	4	Oct 2025	Re-validation of the environmental document has taken additional time to complete due to delayed completion of cultural surveys and studies as well as delayed completion of regulatory agency consultation and the environmental commitments record. These efforts have resulted in additional time needed to reach RTL. A 4-month allocation time extension is requested, delaying the CON and CON Sup allocations to October 2025.
	44	12	Orange	5	R28.9/44.382	0Q950	2833C	1218000006	Mobility	PS&E	7	Jan 2026	This project is Department's first sponsored, implemented, and operated priced managed lanes project. There is an increased complexity of the environmental document which requires a more involved and lengthy development. In addition, the preferred alternative requires Vehicle Miles Traveled mitigation. The Department is performing the needed analysis and requires additional time to conduct stakeholder meetings. Therefore, PA&ED is taking longer than expected. The delay of PA&ED completion is affecting the start of the PS&E phase. A 7-month allocation time extension is requested, delaying the PS&E allocation to January 2026.

Time Extension/Waiver - Project Construction and Support Allocation Deadline
(For 2 months until August 2025)

<u>Index No.</u>	<u>District</u>	<u>County</u>	<u>Route</u>	<u>Postmile</u>	<u>EA</u>	<u>PPNO</u>	<u>Project ID</u>	<u>Program</u>	<u>Phase</u>	<u>Requested Months</u>	<u>Extended Deadline</u>
1	01	Del Norte	101	39.6/39.9	0M850	1157	0123000076	Collision	PS&E/ RW Sup	2	Aug 2025
2	01	Del Norte	197	M3.2/4.5	0N290	1161	0123000202	Mobility	Con/ Con Sup	2	Aug 2025
3	01	Humboldt	254	4.3/4.7	0H801	2502M	0122000142	Major Damage	Con/ Con Sup	2	Aug 2025
4	01	Mendocino	1	59.8/62.1	0B220	4546	0112000110	Mandates	Con/ Con Sup	2	Aug 2025
5	02	Siskiyou	161	4.5/9.1	0J550	3754	0219000135	Roadway	Con/ Con Sup	2	Aug 2025
6	02	Trinity	3	67.7/T85.06	0J760	3768	0219000157	Roadway	Con/ Con Sup	2	Aug 2025
7	03	Colusa	5	R24.3	0J460	2975	0319000287	Roadside	Con/ Con Sup	2	Aug 2025
8	03	El Dorado	50	18.7/21.9	0J400	3337	0319000280	Roadway	Con/ Con Sup	2	Aug 2025
9	03	El Dorado	50	39.7/58.9	1J160	3341	0320000113	Roadway	Con/ Con Sup	2	Aug 2025
10	03	Placer	80	33.0/44.9	0J410	5140	0319000281	Roadway	Con/ Con Sup	2	Aug 2025
11	03	Placer	89	13.1/21.667	1J170	5292	0320000114	Roadway	Con/ Con Sup	2	Aug 2025
12	03	Sacramento	5	1.2/3.1	4J900	5884	0323000287	Collision	PS&E/ RW Sup	2	Aug 2025
13	03	Sutter	20	15.8/16.1	3J680	8146	0322000275	Collision	Con/ Con Sup	2	Aug 2025
14	04	Alameda	24		0AA80	2095C	0419000568	Mobility	Con/ Con Sup	2	Aug 2025
15	04	Alameda	80	2.4/R7.0	0Y660	2914S	0422000317	Collision	Con/ Con Sup	2	Aug 2025
16	04	Alameda	123	2.09/2.72	2Y260	2917G	0423000091	Complete Streets	PS&E/ RW Sup	2	Aug 2025
17	04	Contra Costa	4	R29.0/R44.4	0AA19	2033N	0419000450	Roadway	Con/ Con Sup	2	Aug 2025
18	04	Contra Costa	24	R0.01	0J540	0480B	0414000011	Bridge	PS&E/ RW Sup	2	Aug 2025

Time Extension/Waiver - Project Construction and Support Allocation Deadline
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19	04	Marin	101	8.0/15.3	4AC70	2035D	0420000148	Roadway	Con/ Con Sup	2	Aug 2025
20	04	Napa	12		4AC90	2304A	0420000150	Collision	Con/ Con Sup	2	Aug 2025
21	04	San Francisco	35	2.055/2.147	3AC80	2034N	0420000134	Collision	Con/ Con Sup	2	Aug 2025
22	04	Santa Clara	101	0.028/3.2	1W190	2913E	0420000344	Collision	Con/ Con Sup	2	Aug 2025
23	04	Santa Clara	152	12.9/21.8	0W700	2909Q	0420000280	Collision	Con/ Con Sup	2	Aug 2025
24	04	Solano	80	1.8/5.7	2Q640	2031H	0419000033	Mandates	Con/ Con Sup	2	Aug 2025
25	04	Sonoma	12		4AC40	2035C	0420000142	Collision	Con/ Con Sup	2	Aug 2025
26	05	Monterey	1	44.3	1P210	3075	0521000188	Major Damage	Con/ Con Sup	2	Aug 2025
27	05	Monterey	1	60.0	1Q160	3164	0522000177	Bridge	Con/ Con Sup	2	Aug 2025
28	05	Monterey	101	R28.2/100.3	1J890	2797	0518000084	Culvert(s)	Con/ Con Sup	2	Aug 2025
29	05	Santa Barbara	101	R56.1/ 65.0	1M100	2726	0519000066	Roadway	Con/ Con Sup	2	Aug 2025
30	05	Santa Barbara	154	R5.7/R6.2	1H311	2674Y	0522000058	Mobility	Con/ Con Sup	2	Aug 2025
31	05	Santa Cruz	9	6.3/7.2	1M400	3012	0519000136	Collision	Con/ Con Sup	2	Aug 2025
32	06	Fresno	99	23.3	0X430	6973	0618000065	Facilities	Con/ Con Sup	2	Aug 2025
33	06	Fresno	168	49.1/49.5	1A090	7061	0620000065	Major Damage	Con/ Con Sup	2	Aug 2025
34	06	Fresno	168	R8.13/45.8	0X220	6992	0618000041	Culvert(s)	Con/ Con Sup	2	Aug 2025
35	06	Kern	5	4.4/10.2R	0W920	6980	0618000063	Roadway	Con/ Con Sup	2	Aug 2025
36	06	Kern	223	1.85/10.4	1C060	3019	0621000014	Roadway	Con/ Con Sup	2	Aug 2025

Time Extension/Waiver - Project Construction and Support Allocation Deadline
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37	06	Tulare	245	0.0/ 33.0	0X070	6959	0618000016	Culvert(s)	Con/ Con Sup	2	Aug 2025
38	07	Los Angeles	10	19.1	37260	5746	0720000169	Mandates	Con/ Con Sup	2	Aug 2025
39	07	Los Angeles	14	R24.8/33.8	36380	5646	0719000316	Mobility	Con/ Con Sup	2	Aug 2025
40	07	Los Angeles	47	R0.0/3.6	36250	5636	0719000307	Mobility	Con/ Con Sup	2	Aug 2025
41	07	Los Angeles	57	R4.2/R12.3	37430	5789	0721000047	Roadway	Con/ Con Sup	2	Aug 2025
42	07	Los Angeles	60	9.465/19.521	38910	5991	0722000264	Collision	PS&E/ RW Sup	2	Aug 2025
43	07	Los Angeles	101	31.1/38.19	36960	5704	0719000376	Roadway	Con/ Con Sup	2	Aug 2025
44	07	Los Angeles	101	7.1	36060	5616	0719000275	Bridge	Con/ Con Sup	2	Aug 2025
45	07	Los Angeles	110	24.0/31.9	37530	5795	0721000064	Culvert(s)	Con/ Con Sup	2	Aug 2025
46	07	Los Angeles	210	R11.0/R11.5	36180	5628	0719000292	Culvert(s)	Con/ Con Sup	2	Aug 2025
47	07	Los Angeles	210	R43.3/R46.0	36870	5697	0719000367	Roadway	Con/ Con Sup	2	Aug 2025
48	07	Ventura	1	28.15	3230U	6406	0724000254	Collision	Con/ Con Sup	2	Aug 2025
49	07	Ventura	101	2.5/13.9	36320	5642	0719000312	Mobility	Con/ Con Sup	2	Aug 2025
50	08	Riverside	10		1F561	3004P	0816000026	Roadside	Con Sup	2	Aug 2025
51	08	Riverside	10	R0.0/R4.4	1J650	3011W	0818000089	Roadway	Con/ Con Sup	2	Aug 2025
52	08	Riverside	10	R14.6/R16.3	1K960	3014Q	0819000122	Mobility	Con/ Con Sup	2	Aug 2025
53	08	Riverside	10	R4.4/8.2	1J640	3011V	0818000088	Roadway	Con/ Con Sup	2	Aug 2025
54	08	Riverside	15	38.95/39.4	1N610	3021L	0823000025	Collision	PS&E/ RW Sup	2	Aug 2025

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55	08	San Bernardino	15	0.0/37.4	1L810	3017L	0820000160	Collision	Con/ Con Sup	2	Aug 2025
56	08	San Bernardino	395	4.8/7.6	1L890	3017R	0821000009	Collision	Con	2	Aug 2025
57	10	Merced	99	R0.6	1J510	3493	1020000191	Mobility	Con/ Con Sup	2	Aug 2025
58	10	San Joaquin	4	0.0/8.9	1F320	3517	1016000047	Roadway	Con/ Con Sup	2	Aug 2025
59	12	Orange	5	21.3/R25.8	0S051	2590A	1222000012	Roadway	Con/ Con Sup	2	Aug 2025
60	12	Orange	5	R25.8/30.3	0S052	2590B	1222000013	Roadway	Con/ Con Sup	2	Aug 2025
61	12	Orange	241	24.5/35.7	0T730	5498	1222000083	Major Damage	PS&E	2	Aug 2025

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8b.(1), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF CONTRACT AWARD FOR STATE HIGHWAY OPERATION AND PROTECTION PROGRAM PROJECTS, ON THE STATE HIGHWAY SYSTEM, PER SHOPP GUIDELINES WAIVER 25-96**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of contract award for three State Highway Operation and Protection Program (SHOPP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of contract award for three SHOPP projects, for the time periods as identified in the attachment.

BACKGROUND:

In December 2024 and January 2025, the Commission allocated \$180,942,000 in Construction (CON) Capital for three SHOPP projects. In accordance with the SHOPP Guidelines (Guidelines), the deadline to award contracts for projects allocated in December 2024 and January 2025 is June 30, 2025 and July 31, 2025, respectively. The Department is requesting time extensions for the period of contract award. The attachment shows the details of each project and the delays that have resulted in these time extension requests.

Current Guidelines stipulate that the agency implementing a project request a time extension if the project will not be awarded within one Commission meeting after expiration. Expiration of CON funds occurs six months after a project receives its CON allocation. The Commission may approve waivers to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Contract Award for
State Highway Operation and Protection Program**

Project Number: 1
County: Alameda
District: 4
PPNO: 2027J
EA: 2Q740
Route: 880
Allocation Amount: \$70,100,000
Phase: Construction
Allocation Date: 12/06/2024
Allocation Resolution: FP-24-42
Number of Months Requested: 4 Months
Extended Deadline: 10/31/2025
Department Recommendation: Support

SHOPP Mobility project

The Department requests a 4-month time extension for the period of contract award for the Construction (CON) phase of the SHOPP Mobility project. The Department experienced delays in awarding the project.

The project is on Routes 80,101, 880, and 980 at various locations in Alameda, San Francisco, and San Mateo counties. The project proposes to install and upgrade Transportation Management System (TMS) elements including Closed Circuit Television (CCTV) cameras, Vehicle Detection Systems, Changeable Message Signs, ramp meters, and fiber optic cable.

The Department received the CON phase allocation in December 2024 and planned to award the project by June 30, 2025. The advertisement and subsequent award were delayed due to coordination efforts with overlapping projects, and redesign of proposed signage systems.

This project involves installing a fiber optic system and upgrading the TMS across multiple counties. After receiving the construction allocation, one of the overlapping projects was found to include bridge joint repairs that would sever the fiber optic system being installed under this proposed project. Other conflicts were identified with the proposed sign structure on the Bay Bridge. The sign structure design had to be modified to avoid bridge cabling. Since the project was designed using an Architectural and Engineering consultant and they are the Engineer of Record, this required additional coordination for the revisions to be made. This resulted in a six week delay since the contract with the consultant had expired and required reissuance. The Department addressed these issues and advertised the project on April 28, 2025. Since the project requires specialized contractors, an extended advertisement period was used and bid opening was scheduled for mid-June. However, the project received over 45 bidder inquiries, with the majority regarding the fiber optic line installation. As a result, the Department will be issuing addenda to provide installation clarity. To provide bidders

time to adjust their bids, the bid opening is now scheduled for July 31, 2025. Therefore, additional time to award the project is being requested.

Therefore, the Department is requesting a 4-month time extension to award a contract from June 30, 2025 to October 31, 2025.

Project Number: 2
County: Alameda
District: 4
PPNO: 0064A
EA: 15500
Route: 80
Allocation Amount: \$110,667,000
Phase: Construction
Allocation Date: 12/06/2024
Allocation Resolution: FP-24-42
Number of Months Requested: 5 Months
Extended Deadline: 11/30/2025
Department Recommendation: Support

SHOPP Mobility project

The Department requests a 5-month time extension for the period of contract award for the CON phase of the SHOPP Mobility project. The Department experienced delays in awarding the project.

The project is located on Interstate 80 (I-80), I-580, and I-980 in Alameda, Contra Costa, and Solano Counties, at various locations, also on local streets from I-980 to the District 4 Transportation Management Center. The project proposes to install 26 miles of fiber optic cable, three CCTVs, one Variable Message Sign, four VDS, and upgrade TMS equipment.

The Department received the CON phase allocation in December 2024 and planned to award by June 30, 2025. However, a portion of the project, situated between the San Francisco Bay Bridge toll plaza and Ashby Avenue, required revisions to avoid conflicts with an overlapping Middle Mile Broadband (MMBN) project. As part of the scope, this project proposes to install fiber optic cable throughout the project limits. Since the MMBN project will be in the same location, the MMBN contractor will install the conduit and pull boxes in a single trench for better efficiency and less risk of construction conflicts. The change required modification to the Plans, Specifications, and Estimate (PS&E) package and delayed submittal for advertisement to April 18, 2025. The project advertised in mid-May, bid opening is scheduled for June 18, 2025, and award is expected by August 2025. In the event that there are delays to the bid opening due to bidder inquiries or bid protests leading to delay of contract award, additional time is being requested to award by November 30, 2025.

Therefore, the Department is requesting a 5-month time extension to award a contract from June 30, 2025 to November 30, 2025.

Project Number: 3
County: San Diego
District: 11
PPNO: 1478
EA: 42651
Route: 5
Allocation Amount: \$175,000
Phase: Construction
Allocation Date: 01/31/2025
Allocation Resolution: FP-24-54
Number of Months Requested: 4 Months
Extended Deadline: 11/30/2025
Department Recommendation: Support

SHOPP Sustainability and Miscellaneous project

The Department requests a 4-month time extension for the period of contract award for the CON phase of the SHOPP Sustainability and Miscellaneous project. The Department experienced delays in awarding the project.

The project is located on I-5, In the city of San Diego, at 0.2 mile south of Carmel Valley Road. The project proposes to perform biological plant establishment for a parent project (EA 11-42650).

The Department received the CON phase allocation in January 2025 to proceed through the award process. Since this project's programmed and allocated amount is below the \$461,000 threshold, the advertisement and award is processed through the Department's Procurement and Contracts Division. The advertisement of this project is taking longer than anticipated due to an increase in volume of projects moving through the end of the fiscal year. The project is currently being processed and additional time is needed to advertise and award the project.

Therefore, the Department is requesting a 4-month time extension to award a contract from July 31, 2025 to November 30, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8c.(1), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT COMPLETION FOR
STATE-ADMINISTERED STATE HIGHWAY OPERATION AND PROTECTION
PROGRAM PROJECTS, ON THE STATE HIGHWAY SYSTEM, PER SHOPP
GUIDELINES
WAIVER 25-97**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project completion for six State Highway Operation and Protection Program (SHOPP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project completion for six SHOPP projects, for the time periods as identified in the attachment.

BACKGROUND:

In May 2021, January 2022, March 2022, and May 2022, the Commission allocated \$252,884,000 in Construction Capital (CON/CON Support) for the attached six SHOPP project phases. These projects were awarded in May 2021, June 2022, and July 2022. In accordance with the SHOPP Guidelines (Guidelines), the deadline to complete Construction Contract Acceptance for projects awarded in May 2021, June 2022, and July 2022, is May 31, 2024, June 30, 2025, and July 31, 2025, respectively. Because the Department will not meet these deadlines, the Department is requesting time extensions to complete the CON phase. The attachment shows the details of the projects and the delays that have resulted in these time extension requests.

Current Guidelines stipulate that the agency implementing a project request a time extension if the project will not be completed within 36 months after award. The Commission may approve waivers to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Project Completion for
State Highway Operation and Protection Program**

Project Number: 1
Applicant: Department of Transportation
County: Sacramento
District: 3
PPNO: 6402C
EA: 3F073
Route: 51
Allocation Amount: \$61,691,000
Phase: CON/CON Support
Allocation Date: 05/19/2022
Allocation Resolution: FP-21-83
Award Date: 06/29/2022
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

SHOPP Bridge Preservation project

The Department requests a 20-month time extension to complete the Construction (CON) phase for the SHOPP Bridge Preservation project. The Department has experienced unexpected delays in completing construction.

The project is located on State Route (SR) 51, in the City of Sacramento, from north B Street Underpass to north of Exposition Boulevard Overcrossing at the American River Bridge No. 24-0133. The project will construct the foundation for widening the American River Bridge.

The parent project (EA 03-3F070) utilized a child project (EA 03-3F073) to separate the mobilization and demobilization costs. When the Construction Manager/General Contractor (CMGC) was awarded, the parent project did not include the mobilization/demobilization costs from the child project. The child project's mobilization and demobilization costs are still needed even after the parent project is completely constructed. These items need to be available to the CMGC on the parent project if the child project reaches Construction Contract Acceptance (CCA). The child project contractor is listed as a Disadvantage Business Enterprise (DBE). Should the child project reach CCA, the parent project's DBE commitments cannot be transferred between projects, even if they are related. The additional time will allow for the child project timelines to align with the parent project allowing the CMGC to meet their DBE commitments and avoid deductions and revenue loss.

Therefore, the Department is requesting a 20-month time extension to allow sufficient time for the Department to complete construction activities from June 30, 2025 to February 28, 2027.

Project Number: 2
Applicant: Department of Transportation
County: Sonoma
District: 4
PPNO: 0482T
EA: 0J300
Route: 1
Allocation Amount: \$18,857,000
Phase: CON/CON Support
Allocation Date: 01/27/2022
Allocation Resolution: FP-21-49
Award Date: 07/08/2022
Number of Months Requested: 6 Months
Extended Deadline: 01/31/2026
Department Recommendation: Support

SHOPP Major Damage Restoration project

The Department requests a 6-month time extension to complete the CON phase for the SHOPP Major Damage Restoration project. The Department has experienced unexpected delays in completing construction.

This project is located on SR 1, near Jenner, from north of Myers Grade Road. The project will perform permanent restoration of roadway slip outs.

The permanent restoration project was awarded on July 8, 2022, and was anticipated to be completed in June 2025. However, construction began a few months later than expected in September 2022, due to the additional biological monitoring required through the winter months to ensure biological permit compliance. Once construction began, heavy storms in December 2022 and January 2023, caused additional embankment and roadway damage and required immediate action to keep the open lane of the existing highway open to public traffic. The existing embankment from the northbound lane had slipped in multiple locations and required removal and disposal of the slide material. To mitigate the movement, a series of sheet piles were installed. These sheet piles were in direct conflict with an existing drainage inlet and culvert. To positively locate the culvert and continue with the work, potholing was required resulting in a slight delay to the project. The drainage inlet was removed, necessitating the installation of bioswales to remain in compliance with the 401 permit requirements. As the result of the new scope to address the new slip out, additional coordination with permitting agencies is required. The Department will need to continue coordination with the regulatory agencies to update the Coastal Permits, 404, 401, and Biological Opinion. Additional time is needed to complete the remaining work, address any punch list items and CCA the project.

Therefore, the Department is requesting a 6-month time extension to allow sufficient time to complete construction from July 31, 2025 to January 31, 2026.

Project Number: 3
Applicant: Department of Transportation
County: Santa Barbara
District: 5
PPNO: 2426A
EA: 1C822
Route: 101
Allocation Amount: \$100,762,000
Phase: CON/CON Support
Allocation Date: 05/12/2021
Allocation Resolution: FP-20-79
Award Date: 05/26/2021
Number of Months Requested: 5 Months
Extended Deadline: 10/31/2025
Department Recommendation: Support

SHOPP Roadway Rehabilitation project

The Department requests a 5-month time extension to complete the CON phase for the SHOPP Roadway Rehabilitation project. The Department has experienced unexpected delays in completing construction.

This project is located on United States Highway (US) 101, in and near Summerland, from south of South Padaro Lane Undercrossing to north of Padaro Lane Overcrossing. The project will rehabilitate roadway.

In January 2025, project construction revealed a safety risk where vehicles on Frontage Road Via Real may run off the road and into northbound US 101 lanes. A barrier is needed for traveler safety, but underground utilities complicate permanent installation and would require approvals from multiple utility companies. The Department considered installing a temporary barrier as it works on a permanent solution with the utility companies. The County of Santa Barbara (County) approved temporary k-rail placement and end treatment measures along Frontage Road Via Real. This temporary solution will remain until the County can relocate utilities to allow installation of a permanent barrier.

Additionally, homeowner associations raised concerns about noise from the existing rumble strip. To mitigate noise, the Department replaced it with a quieter sinusoidal strip which is positioned three feet farther from the road. These adjustments caused a 5-month project delay. Completion is now expected by October 2025. This project has a concurrent time extension request for the period of project completion under PPNO 7101D, on this month's Commission agenda.

Therefore, the Department is requesting a 5-month time extension to allow sufficient time to complete construction from May 31, 2025 to October 31, 2025.

Project Number: 4
Applicant: Department of Transportation
County: Madera
District: 6
PPNO: 6857
EA: 0V120
Route: 99
Allocation Amount: \$32,260,000
Phase: CON/CON Support
Allocation Date: 01/27/2022
Allocation Resolution: FP-21-49
Award Date: 06/22/2022
Number of Months Requested: 4 Months
Extended Deadline: 10/31/2025
Department Recommendation: Support

SHOPP Bridge Preservation project

The Department requests a 4-month time extension to complete the CON phase for the SHOPP Bridge Preservation project. The Department has experienced unexpected delays in completing construction.

This project is located on SR 99, near the City of Madera, at Cottonwood Creek Bridge No. 41-065R, No. 41-0065L and No. 41-0065S. The project will replace bridges and mitigate corrosion by chloride latent concrete.

The project was awarded in June 2022 and construction activities began in October 2022. The project has several locations involving bridges. The project requires approval from the Madera Irrigation District (MID) prior to working in the Cottonwood Creek channel. MID has work restrictions that start in October and end in April of the following year, unless approval is obtained from MID to work in the channel during that period. The contractor applied for and received permission from MID in the winter of 2022-23. However, the project had already entered winter suspension through spring 2023. The project was further delayed due to the contractor having to procure a substitute electrical DBE sub-contractor due to bankruptcy. The process of finding a qualified subcontractor took an additional 10 weeks. The project has since commenced work and construction activities have been accelerated where possible to mitigate for the delays. However, additional time is needed to complete the remaining work.

Therefore, the Department is requesting a 4-month time extension to allow sufficient time to complete construction from June 30, 2025 to October 31, 2025.

Project Number: 5
Applicant: Department of Transportation
County: Los Angeles
District: 7
PPNO: 5237
EA: 33960
Route: 14
Allocation Amount: \$31,816,000
Phase: CON/CON Support
Allocation Date: 03/17/2022
Allocation Resolution: FP-21-60
Award Date: 07/06/2022
Number of Months Requested: 8 Months
Extended Deadline: 03/31/2026
Department Recommendation: Support

SHOPP Major Damage Restoration project

The Department requests an 8-month time extension to complete the CON phase for the SHOPP Major Damage Restoration project. The Department has experienced unexpected delays in completing construction.

This project is located on SR 14, in Santa Clarita, from north of Placerita Canyon Road to south of Golden Valley Road. The project will regrade slopes, construct drainage ditches, and install erosion control system.

The project was awarded in July 2022 and was anticipated to be completed by July 2025, including the 1-year erosion control establishment period. Construction was scheduled to begin in September 2022; however, the project was suspended for six months since one of the subcontractors lost its bonding, and the contractor had to procure a new sub. Construction had since begun, and in February 2024, the project site was impacted by heavy storms. The storms damaged over half of the installed turf reinforcement mat. The mat required reconstruction resulting in a 5-month delay. The project has since commenced work and construction activities have been accelerated where possible to mitigate for the delays. The remaining work includes erosion control, k-rail removal, slurry seal and a few punch list items. Once the final work items are completed, the project will undergo final inspection, including review by the State Water Resources Control Board for the revegetation. Additional time is needed to complete these tasks.

Therefore, the Department is requesting an 8-month time extension to allow sufficient time to complete construction from July 31, 2025 to March 31, 2026.

Project Number: 6
Applicant: Department of Transportation
County: San Bernardino
District: 8
PPNO: 0184C
EA: 0G691
Route: 18
Allocation Amount: \$7,498,000
Phase: CON/CON Support
Allocation Date: 01/27/2022
Allocation Resolution: FP-21-49
Award Date: 06/13/2022
Number of Months Requested: 4 Months
Extended Deadline: 10/31/2025
Department Recommendation: Support

SHOPP Roadway Preservation project

The Department requests a 4-month time extension to complete the CON phase for the SHOPP Roadway Preservation project. The Department has experienced unexpected delays in completing construction.

This project is located on SR 18, in and near the City of San Bernardino, from 48th Street to SR 138. The project will repair, reline, and replace culverts.

The project was awarded in June 2022 and construction began soon after. The repair strategies vary by system and include relining, repairing or full replacements. The project encountered delays during work on a 60-inch diameter culvert. During excavation and drilling operations for this system, the contractor encountered unanticipated hard rock and higher than expected groundwater. The presence was unforeseen and caused delays to operations. To mitigate for the groundwater intrusion, shoring plans had to be designed and implemented under a contract change order. As part of the increased efforts, the planned access road needed to be extended for additional equipment and ease of construction. This required additional efforts not planned for and resulted in a 4-month delay to the project. Construction has resumed, however additional time is needed to complete the remaining work.

Therefore, the Department is requesting a 4-month time extension to allow sufficient time to complete construction from June 30, 2025 to October 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8d.(1), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT DEVELOPMENT
EXPENDITURE FOR STATE HIGHWAY OPERATION AND PROTECTION
PROGRAM PROJECTS, ON THE STATE HIGHWAY SYSTEM, PER SHOPP
GUIDELINES
WAIVER 25-98**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project development expenditure for 17 State Highway Operation and Protection Program (SHOPP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project development expenditure for 17 SHOPP projects, for the time periods as identified in the attachment.

BACKGROUND:

In May, June and August 2022, the Commission allocated \$51,274,000 in project development support for 17 SHOPP project phases listed in the attachment. In accordance with SHOPP Guidelines (Guidelines), the deadline to complete the Plans, Specifications, and Estimate (PS&E) and Project Approval and Environmental Document (PA&ED) phases for projects allocated in May, June, and August 2022 is May 31, 2025, June 30, 2025, and August 31, 2025, respectively.

Current Guidelines stipulate that the implementing agency request an expenditure time extension if the project phase will not be completed within 36 months of allocation for the PS&E and PA&ED phases. The Commission may approve waivers to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Project Development Expenditure for
State Highway Operation and Protection Program**

Project Number: 1
Applicant: Department of Transportation
County: Mendocino
District: 1
PPNO: 4695
EA: 0H570
Route: 101
Allocation Amount: \$1,150,000
Remaining Balance: \$42,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 6 Months
Extended Deadline: 12/31/2025
Department Recommendation: Support

SHOPP Roadway Preservation project

The Department requests a 6-month time extension for the period of project development expenditure for the Plans, Specifications, and Estimate (PS&E) phase of the SHOPP Roadway Preservation project. The Department experienced unexpected delays in completing this project's preconstruction phase.

This project is located on State Route (SR) 101, in and near Ukiah, from north of Robinson Creek Bridge to Pomo Lane Undercrossing. The project proposes to rehabilitate the roadway, install vegetation control, upgrade guardrail, lighting, and Transportation Management System (TMS) elements, and apply High Friction Surface Treatment.

During the PS&E phase, several changes were required to be incorporated into the plans. These changes included a change in material recommendation and standard plan updates. The project's pavement strategy was developed using the R-Value method for the pavement deflection study. However, recent changes to the Highway Design Manual required replacement of this method with the Empirical Method. The recommendation was re-issued using this new method and resulted in a different structural section recommendation. In addition, the 20-year design strategy was altered to use a 40-year strategy. These changes resulted in redesigning the facility since the structural section changed. The change in the structural section also affected the vertical clearance at the structures, and adjustments had to be made to other appurtenances as a result.

In addition, the project includes several transition barriers throughout the project. Adjustments to the plans were required due to standard plan updates for concrete barrier transitions at 41 locations. These changes were not anticipated and required additional time to develop individualized plans for these barriers. This triggered the need for traffic handling plans and long-form stormwater data report, neither of which were included in the original workplan.

Therefore, the Department is requesting a 6-month project development expenditure time extension to allow enough time to complete the PS&E phase from June 30, 2025 to December 31, 2025.

Project Number: 2
Applicant: Department of Transportation
County: Contra Costa
District: 4
PPNO: 2906A
EA: 1Q720
Route: 680
Allocation Amount: \$9,620,000
Remaining Balance: \$164,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 9 Months
Extended Deadline: 03/31/2026
Department Recommendation: Support

SHOPP Mobility project

The Department requests a 9-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Mobility project. The Department experienced unexpected delays in completing this project's preconstruction phase.

This project is located on Interstate 580 (I-580), I-680, and I-780, in Alameda, Contra Costa, and Solano counties, at various locations. The project proposes to install and upgrade fiber optic cable, ramp metering systems, and TMS elements, and widen ramps to provide High-Occupancy Vehicle bypass ramp lanes.

The PS&E phase for the project was allocated in June 2022 and has a deadline of June 2025. In June 2023, the project was split into two child projects: EA 04-1Q721 and EA 04-1Q722. Since the split occurred after PS&E had begun, the entire allocated PS&E budget remains under the parent project.

The first child project (EA 04-1Q721) was planned for delivery in Fiscal Year 2023-24. However, due to a delay in obtaining railroad permits from multiple railroad companies, a 9-month allocation time extension was requested and approved at the August 2024 Commission meeting. The project achieved Ready to List (RTL) in January 2025 and was advertised in May 2025, with bid opening scheduled for July 3, 2025. Contract approval is scheduled for October 2, 2025. The project's PS&E requires additional time to complete tasks such as bid opening, contract award, contract approval, and to address any unforeseen issues that may arise during the advertisement and award process.

The second child project (EA 04-1Q722), is scheduled to RTL in March 2026. The Department is currently processing a change to convert the project to a Financial Contribution Only (FCO). The FCO will be contributed to the Contra Costa Transportation Authority to integrate the scope into their overlapping local project. As a result, the PS&E budget under the parent project will not be used for this project and a time extension to complete the phase is not being considered. The additional time is only required for the first child project to complete the PS&E phase.

Therefore, the Department is requesting a 9-month project development expenditure time extension to allow enough time to complete the PS&E phase from June 30, 2025 to March 31, 2026.

Project Number: 3
Applicant: Department of Transportation
County: Santa Barbara
District: 5
PPNO: 3015
EA: 1M440
Route: 101
Allocation Amount: \$3,598,000
Remaining Balance: \$1,114,000
Phase: Project Approval and Environmental Document
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

SHOPP Major Damage Restoration project

The Department requests a 20-month time extension for the period of project development expenditure for the Project Approval and Environmental Document (PA&ED) phase of the SHOPP Major Damage Restoration project. The Department experienced unexpected delays in completing this project's preconstruction phase.

This project is on SR 101, near Gaviota and Las Cruces, from north of Beckstead Overcrossing to 1.2 miles north of SR 1. The project proposes to stabilize the roadway shoulder, replace failed retaining walls, and improve a fish passage.

This project was programmed as a long lead and PA&ED was allocated in June 2022. At the time of programming extensive topographic survey work in vegetated and steep sloped creek channels was expected with basic hydraulic studies.

After beginning the PA&ED phase, the extent of the required hydraulic studies needed were determined to be a Geomorphology Study and Report for the entire range of the creek corridor, Fish Passage Alternative Analysis and Report, and a Floodplain Study. These studies cannot be completed by the Department, requiring District 5 to hire a consultant. The full extent of data collection and time needed for modeling and analysis was unknown during preparation of the Project Initiation Document and requires more time than originally anticipated and requested.

During the topographic survey work needed for the hydraulic studies, significant storms impacted the project site and altered the creek's path in 2023-2024. The storms impacted the survey delivery schedule due to having to start and stop the surveys numerous times. The roadway within the project limits was also impacted and required an emergency restoration project to be completed before any work on this project could resume. The topographic survey work was completed in 2024. After these storm events new survey and study information was needed, and the hydraulic model and analysis process had to start over.

Adding to the delay was obtaining California Department of Fish and Wildlife (CDFW) comments and concurrence of the Geomorphology Report and Fish Passage Analysis. The Department's hydraulics and biology staff, and consultant worked closely to provide timely information to CDFW, but the CDFW response times were longer than anticipated. CDFW's fish passage criteria evolved during the last set of comments. The hydraulic and consultant team evaluated the impact of the criteria changes. After evaluation it was determined and agreed upon that the project could move forward to avoid incurring further delay, and that more hydraulic analysis would be required during the PS&E phase to complete the additional analysis and finalize the design refinement with the new criteria. As a result, the hydraulic studies are still in the process of being completed. Once the hydraulic studies are completed, and alternatives are reviewed and consensus received by CDFW and National Marine Fisheries Service, the Department's environmental team can resume preparation of the Draft Environmental Impact Report (EIR) and complete the public circulation and Final EIR approval process.

Therefore, the Department is requesting a 20-month project development expenditure time extension to allow enough time to complete the PA&ED phase from June 30, 2025 to February 28, 2027.

Project Number: 4
Applicant: Department of Transportation
County: Madera
District: 6
PPNO: 7005
EA: 0Y180
Route: 145
Allocation Amount: \$2,887,000
Remaining Balance: \$23,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-89
Number of Months Requested: 6 Months
Extended Deadline: 12/31/2025
Department Recommendation: Support

SHOPP Roadway Preservation project

The Department requests a 6-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Roadway Preservation project. The Department experienced unexpected delays in completing this project's preconstruction phase.

This project is on SR 145, In and near the city of Madera, from south of Avenue 13 to 1.0 mile north of Tozer Street. The project proposes to rehabilitate pavement, install TMS elements, construct rumble strips, replace signs, rehabilitate drainage systems, install street lighting, construct curb extensions/bulb-outs, install or improve transit shelters, plant trees, and restripe roadway to install bike lanes as complete streets elements.

The project's scope is in close proximity to the Union Pacific Railroad. Initial coordination was focused on obtaining a Construction and Maintenance (C&M) agreement. However, after numerous meetings and modifications to the project plans, it was determined that a Maintenance Consent Letter (MCL) would be more appropriate. The Department pursued the MCL, and it was approved on April 1, 2025. Initially pursuing a C&M Agreement and then an MCL was not anticipated and resulted in a delay to the project.

In addition, coordination with Pacific Gas and Electric (PG&E) took longer than expected. The project scope included adjusting three utility vaults and coordination with PG&E took longer than expected to determine responsibility. After reviewing the plans and coordinating with PG&E, the adjustments were avoided by modifying the plans. The project's PS&E requires additional time to complete tasks including bid opening, contract award, contract approval, and addressing any unforeseen issues that may arise during the advertisement and award process.

Therefore, the Department is requesting a 6-month project development expenditure time extension to allow enough time to complete the PS&E phase from June 30, 2025 to December 31, 2025.

Project Number: 5
Applicant: Department of Transportation
County: Los Angeles
District: 7
PPNO: 5364
EA: 34610
Route: 1
Allocation Amount: \$3,204,000
Remaining Balance: \$98,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 4 Months
Extended Deadline: 10/31/2025
Department Recommendation: Support

SHOPP Bridge Preservation project

The Department requests a 4-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Bridge Preservation project. The Department experienced unexpected delays in completing this project's preconstruction phase.

This project is located on SR 1, in Long Beach, at Los Angeles River Bridge No. 53-0241 and De Forest Avenue Undercrossing No. 53-1047. The project proposes to perform seismic retrofit, upgrade bridge rails and lighting, and upgrade facilities to ADA standards.

The project involves accessing the Los Angeles River, which falls under the jurisdiction of both the LA County Flood Control District and the United States Army Corps of Engineers (USACE). Due to several levee walls located on both sides of the bridge bents, accessing the bents with construction equipment for retrofitting has been challenging. The Department explored multiple design strategies to meet the USACE 408 permit requirements, which resulted in additional effort and extended the time needed to finalize the PS&E package. The project achieved RTL in December 2024 and was advertised for bidding in April 2025.

The current bid opening is scheduled for June 2025. Following the bid opening, the Department requires sufficient time to evaluate Disadvantaged Business Enterprise good faith efforts and to address any potential bid protests. Therefore, the project award is targeted for August 2025, with contract approval anticipated by October 8, 2025. The additional time is required to provide adequate time to complete all necessary processes, including document processing, bid protest resolution, contract award, and contract approval.

Therefore, the Department is requesting a 4-month project development expenditure time extension to allow enough time to complete the PS&E phase from June 30, 2025 to October 31, 2025.

Project Number: 6
Applicant: Department of Transportation
County: Los Angeles
District: 7
PPNO: 5484
EA: 35320
Route: 405
Allocation Amount: \$5,664,000
Remaining Balance: \$48,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 8 Months
Extended Deadline: 02/28/2026
Department Recommendation: Support

SHOPP Roadway Preservation project

The Department requests an 8-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Roadway Preservation project. The Department experienced unexpected delays in completing this project's preconstruction phase.

This project is on SR 405, in and near the cities of Long Beach, Signal Hill, Los Angeles, and Carson, from the Orange County line to south of SR 110. The project proposes to rehabilitate pavement, upgrade signs, rehabilitate bridge, upgrade lighting, improve safety, rehabilitate TMS elements, replace copper cabling with fiber, rehabilitate culverts, replace landscape and irrigation system, rock blanket paving, minor concrete paving, grind concrete pavement, construct maintenance vehicle pullouts, and upgrade facilities to Americans with Disabilities Act (ADA) standards.

In January 2025, when the contract was advertised, the Department received 210 bidder inquiries from 13 potential contractors. Based on these inquiries, the Department issued two addenda to address the discrepancies and inconsistencies that were encountered during the bidders' review process. The addenda revised approximately 200 plan sheets, including layouts, typical cross sections, traffic signing and striping, specifications, and bid quantities. In March 2025, bids were opened, and three bids were received. The bids were analyzed for validation, and they were found to be non-responsive, front loaded, or too high to award to the lowest responsive bidder. Therefore, the Department decided to reject all the bids and re-advertise the contract after it is repackaged. The repackaging of the PS&E will require incorporating approximately 200 addenda sheets, updating the PS&E to meet the mid-cycle standard plans released in April 2025, revising the PS&E to use the latest compliant temporary barrier, and upgrading the electrical pull boxes to prevent vandalism and theft. The Department will also make plan changes to address several bidder inquiries regarding the construction of the overhead signs behind existing soundwalls. There is a concurrent supplemental funds request on this month's Commission agenda.

Therefore, the Department is requesting an 8-month project development expenditure time extension to accommodate these changes and ensure sufficient time to complete the PS&E phase though contract award from June 30, 2025 to February 28, 2026.

Project Number: 7
Applicant: Department of Transportation
County: Los Angeles
District: 7
PPNO: 5491
EA: 35420
Route: 1
Allocation Amount: \$2,960,000
Remaining Balance: \$1,620,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 14 Months
Extended Deadline: 08/31/2026
Department Recommendation: Support

SHOPP Roadway Preservation project

The Department requests a 14-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Roadway Preservation project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is on SR 1, in and near Malibu, from north of Serra Road to the Ventura County line. The project proposes to rehabilitate pavement, upgrade facilities to ADA standards, and upgrade guardrail.

The project was scheduled to advertise in December 2025, have bid opening in February 2026, and award in April 2026. However, the schedule was revised due to additional time needed to incorporate safety aspects into the roadway rehabilitation scope. In August 2024, the project received a 19-month construction allocation extension through January 2026, due to the addition of safety improvements identified during a Road Safety Audit (RSA) performed in coordination with multiple stakeholders following a recent fatal collision in the area. The RSA was conducted within the project limits and completed in March 2024.

Although the Department aimed to accelerate project delivery and planned to request construction allocation in October 2025 and award the project by January 2026, progress was further delayed by the Palisades and Malibu wildfires. These fires restricted access to the project site and caused delays in critical survey and site investigation work necessary for finalizing the PS&E package. The Department plans to achieve RTL status in November 2025, request construction allocation in January 2026, award the construction contract in July 2026, and approve the contract in August 2026.

Therefore, the Department is requesting a 14-month project development expenditure time extension to allow enough time to complete the PS&E phase from June 30, 2025 to August 31, 2026.

Project Number: 8
Applicant: Department of Transportation
County: Ventura
District: 7
PPNO: 5515
EA: 35590
Route: 34
Allocation Amount: \$3,983,000
Remaining Balance: \$376,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 13 Months
Extended Deadline: 07/31/2026
Department Recommendation: Support

SHOPP Roadway Preservation project

The Department requests a 13-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Roadway Preservation project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is on SR 34, in and near Camarillo, from 0.5 mile east of Rice Avenue to SR 118. The project proposes to rehabilitate pavement, upgrade guardrail, install complete street elements, and upgrade facilities to ADA standards.

This project's PS&E phase was allocated in June 2022. The project has encountered several unforeseen delays impacting the completion of the PS&E phase. Initially, the Department's potholing contract execution was delayed by eight months due to a protest that resulted in a re-bid. Typically, the new potholing contract is executed three to four months prior to the existing contract expiration in June 2024 to commence work by July 1, 2024. However, the new contract was not executed until early October 2024, and the new contractor did not commence work until mid-October 2024. This caused a 4-month delay to begin potholing operations to verify underground utility locations.

With the completion of the potholing activities, a new reinforced concrete box culvert was found to conflict with an existing 30-inch waterline. The design of the 2-foot by 6-foot by 1,910-foot-long culvert was required to be repositioned along its length by approximately 8 to 30 feet to avoid the conflict with the waterline. As a result, the drainage plans and estimates required adjustment.

Since the system's alignment was revised to avoid the waterline, the Department amended the task order for the utility consultant to allow for additional potholing and update the utility plan sheets. This required an additional 39 potholes to identify existing utilities along the new alignment of the culvert. In addition to potholing, the effort required additional surveying for the utility plan sheets. Additionally, the January 2025 Los Angeles fires necessitated utility companies, such as Southern California Edison and Frontier, to redirect resources toward fire restoration and recovery efforts further delaying the project.

The Department and utility companies are now working towards delivering relocation plans by October 2025 and aiming to obtain Right of Way (RW) Certification by November 2025. This schedule will facilitate achieving RTL in November 2025, requesting construction funds allocation in January 2026, advertisement in March 2026, bid opening in May 2026, and reaching contract approval by July 2026.

Therefore, the Department is requesting a 13-month project development expenditure time extension to allow enough time to complete the PS&E phase from June 30, 2025 to July 31, 2026.

Project Number: 9
Applicant: Department of Transportation
County: Los Angeles
District: 7
PPNO: 5625
EA: 36150
Route: 1
Allocation Amount: \$2,156,000
Remaining Balance: \$810,000
Phase: Project Approval and Environmental Document
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 4 Months
Extended Deadline: 10/31/2025
Department Recommendation: Support

SHOPP Roadway Preservation project

The Department requests a 4-month time extension for the period of project development expenditure for the PA&ED phase of the SHOPP Roadway Preservation project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is located on SR 1, in and near the cities of Santa Monica, Los Angeles, and Malibu, from Colorado Avenue to south of Cross Creek Road. The project proposes to rehabilitate pavement, upgrade guardrail, install TMS elements, and culverts, and upgrade facilities to ADA standards.

This project's PA&ED phase was allocated in June 2022. The project has previously received PS&E and RW support allocation time extensions until June 30, 2025, and is on target to complete the PA&ED phase as scheduled by October 31, 2025. Additional time is needed for the PA&ED phase due to unforeseen circumstances.

The Palisades fire necessitated the postponement of the public outreach meeting to late March and early April 2025. This delay in community engagement has impacted the timeline for addressing public comments and safety concerns. Although the environmental clearance through the California Environmental Quality Act will be achieved by August 2025, on-going PA&ED activities require additional time to complete. In addition, the Project Report still needs to be finalized by July or early August. These activities include on-going consultant task orders finalizing site investigation activities, including hazardous waste site testing, photogrammetry surveys, and geotechnical drilling. These activities are still in progress and will conclude by October 2025. While these tasks are not required to complete the PA&ED phase, their uninterrupted continuation is essential for comprehensive project development. The additional time is being requested to adequately address public safety concerns and complete on-going investigations that were delayed by four months. There is a concurrent greater than 120 allocation request on this month's Commission agenda.

Therefore, the Department is requesting a 4-month project development expenditure time extension to allow enough time to complete the PA&ED phase from June 30, 2025 to October 31, 2025.

Project Number: 10
Applicant: Department of Transportation
County: Los Angeles
District: 7
PPNO: 5701
EA: 36930
Route: 110
Allocation Amount: \$1,245,000
Remaining Balance: \$562,000
Phase: Project Approval and Environmental Document
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 2 Months
Extended Deadline: 08/31/2025
Department Recommendation: Support

SHOPP Collision Reduction project

The Department requests a 2-month time extension for the period of project development expenditure for the PA&ED phase of the SHOPP Collision Reduction project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is located on SR 110, in the cities of Los Angeles and South Pasadena, at Avenue 43 Bridge and Arroyo Seco Bridge. The project proposes to upgrade bridge railing.

This project's PA&ED phase was allocated in June 2022. The project encountered unexpected delays due to the need for extensive coordination with the State Historic Preservation Officer (SHPO). The level of coordination with the SHPO was more involved than originally anticipated and resulted in a slight delay to the project. This coordination was essential to develop a context-sensitive barrier design on the structure that minimizes impacts to the scenic and historic elements of the highway.

Additionally, the preparation of the Draft Initial Study/Environmental Assessment (IS/EA) took longer than anticipated to complete the required technical studies. The Draft Environmental Document (DED) was released for public circulation on February 11, 2025. The project team is currently working on the Final IS/EA, which will include responses to comments received during the public review period and identify the preferred alternative. The target date for completing the PA&ED phase is August 15, 2025. The PS&E is programmed in FY 2025-26 and the allocation will be requested at the August Commission meeting. This project will be combined with EA 07-37130 at voting.

Therefore, the Department is requesting a 2-month project development expenditure time extension to allow enough time to complete the PA&ED phase from June 30, 2025 to August 31, 2025.

Project Number: 11
Applicant: Department of Transportation
County: Los Angeles
District: 110
PPNO: 5740
EA: 37130
Route: 110
Allocation Amount: \$1,795,000
Remaining Balance: \$283,000
Phase: Project Approval and Environmental Document
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 2 Months
Extended Deadline: 08/31/2025
Department Recommendation: Support

SHOPP Collision Reduction project

The Department requests a 2-month time extension for the period of project development expenditure for the PA&ED phase of the SHOPP Collision Reduction project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is on SR 110, in the city of Los Angeles, near the neighborhood of Cypress Park, at the N110-N5 Connector Sidehill Viaduct Bridge. The project proposes to modify the bridge structure to support shoulder widening and a new concrete barrier railing, and upgrade the overhead sign structure, sign panels, and safety devices.

This project's PA&ED phase was allocated in June 2022. The project encountered unexpected delays due to the need for extensive coordination with the SHPO. The level of coordination with the SHPO was more involved than originally anticipated and resulted in a slight delay to the project. This coordination was essential to develop a context-sensitive barrier design on the structure that minimizes impacts to the scenic and historic elements of the highway.

Additionally, the preparation of the Draft IS/EA took longer than anticipated to complete the required technical studies. The DED was released for public circulation on February 11, 2025. The project team is currently working on the Final IS/EA, which will include responses to comments received during the public review period and identify the preferred alternative. The target date for completing the PA&ED phase is August 15, 2025. The PS&E is programmed in FY 2025-26 and the allocation will be requested at the August Commission meeting. This project will be combined with EA 07-36930 at voting.

Therefore, the Department is requesting a 2-month project development expenditure time extension to allow enough time to complete the PA&ED phase from June 30, 2025 to August 31, 2025.

Project Number: 12
Applicant: Department of Transportation
County: Mono
District: 9
PPNO: 2645
EA: 37430
Route: 395
Allocation Amount: \$2,304,000
Remaining Balance: \$632,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 8 Months
Extended Deadline: 02/28/2026
Department Recommendation: Support

SHOPP Mandates project

The Department requests an 8-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Mandates project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is on SR 395, near Lee Vining from SR 120 to Picnic Grounds Road. The project proposes to upgrade facilities to ADA standards, rehabilitate drainage, replace signs and guardrail, and make improvements to accommodate bicycles.

This project's PS&E phase was allocated in June 2022 and required utility relocation work. While performing the utility potholing studies that were completed in March 2024, it was unexpectedly discovered that the local water and sewer lines were in poor condition. This finding was not anticipated during the project's initial planning stages. Given that the existing water line would likely be damaged or destroyed during construction, the Department decided, for the benefit of the project and the community, to include the replacement of both the water and sewer lines within the project scope. To facilitate this, the Department initiated a cooperative agreement with Mono County (County), acting on behalf of the Lee Vining Public Utility District, for the utility district to design and fund the capital and support costs associated with the water and sewer line replacements during construction. The cooperative agreement is currently out for signature by the County, and a draft PS&E has been received. However, additional coordination is required for right-of-way acquisitions, approvals and variances of County ordinances, permits, approval of landscape designs, and the negotiation of maintenance agreements with the County. This coordination is complex and is taking more time and consideration than originally anticipated. This extension will provide the necessary time to complete the required coordination and ensure the successful implementation of the water and sewer line replacements within the project by February 2026. There is a concurrent allocation time extension amendment request for the Construction phase on this month's Commission agenda.

Therefore, the Department is requesting an 8-month project development expenditure time extension to allow enough time to complete the PS&E phase from June 30, 2025 to February 28, 2026.

Project Number: 13
Applicant: Department of Transportation
County: Merced
District: 10
PPNO: 3439
EA: 0V690
Route: 59
Allocation Amount: \$1,962,000
Remaining Balance: \$57,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 08/18/2022
Allocation Resolution: FP-21-11
Number of Months Requested: 10 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

SHOPP Roadway Preservation project

The Department requests a 10-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Roadway Preservation project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is on SR 59 near Merced, from Route 152 to Reilly Road. The project proposes to rehabilitate pavement, upgrade guardrail and lighting, replace sign panels, construct rumble strips, rehabilitate drainage, replace one bridge, and rehabilitate two bridge decks.

The replacement of Mariposa Creek Bridge was the critical path on this project. The early PS&E work established elevations and completed design of the bridge, which required hydraulic, structure hydraulic, and geotechnical studies. Some of this work was performed through task orders which required resources above what was initially anticipated.

With the flooding events in early 2023, the Department had to reconfirm the established elevations and address the drainage of floodwater surrounding the Mariposa Creek Bridge. The flooding events also helped identify other areas within the project limits that are required to be addressed. This work was not anticipated and required additional hydraulic studies and recommendations to finalize the design.

With several factors affecting the resources, G-12 funds for the PS&E phase were requested in October 2023. It was anticipated that the project would reach RTL within the 12-month allocation time extension for the Construction phase. However, after receiving the G-12 funding, the Department experienced delays in acquiring the 408 permit, which resulted in an amended 7-month allocation time extension to reach RTL. The Department was notified by the USACE that PG&E is required to apply for a 408 permit separately. As a result, the Department's Environmental and Hydraulics office coordinated the permit application efforts with PG&E and the Central Valley Flood Protection Board to facilitate the separate permit applications for this project and provide information as needed. This required additional resources beyond what was anticipated.

The PS&E package was developed utilizing the standard plans and specifications from earlier versions. Since the project was delayed by 19 months, the PS&E package was revisited and updated to the most recent release of standard plans and specifications. There is a concurrent supplemental funds request on this month's Commission agenda.

Therefore, the Department is requesting a 10-month project development expenditure time extension to allow enough time to complete the PS&E phase from August 31, 2025 to June 30, 2026.

Project Number: 14
Applicant: Department of Transportation
County: Calaveras
District: 10
PPNO: 3288
EA: 0X740
Route: 12
Allocation Amount: \$2,396,000
Remaining Balance: \$61,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 4 Months
Extended Deadline: 10/31/2025
Department Recommendation: Support

SHOPP Collision Reduction project

The Department requests a 4-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Collision Reduction project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is on SR 12, near San Andreas, from west to east of North Fork Calaveras Creek Bridge. The project proposes to replace the bridge and realign the roadway.

The project's PS&E phase was allocated in June 2022 and has a current deadline to complete the phase by June 30, 2025. The project achieved RTL in April 2025 and received its construction fund allocation in May 2025, a few months later than anticipated. Reaching RTL was delayed due to complications with utility relocation efforts that have since been resolved. The coordination efforts with the utility owners took slightly longer than anticipated. The project is scheduled to be advertised by June 30, 2025, award by September 2025, and reach contract approval by October 2025.

Therefore, the Department is requesting a 4-month project development expenditure time extension to allow enough time to complete the PS&E phase from June 30, 2025 to October 31, 2025.

Project Number: 15
Applicant: Department of Transportation
County: Merced
District: 10
PPNO: 3438
EA: 1H020
Route: 140
Allocation Amount: \$1,766,000
Remaining Balance: \$280,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 08/18/2022
Allocation Resolution: FP-22-11
Number of Months Requested: 10 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

SHOPP Mobility project

The Department requests a 10-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Mobility project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is on SR 140, near Hilmar, at the intersection of SR 140 and SR 165. The project proposed to construct a roundabout.

This project's PS&E phase was allocated in August 2022 and has a current deadline of August 31, 2025. The project impacts Merquin Irrigation District (MID) facilities and requires a utility agreement between MID and the Department. To help facilitate the agreement, MID requested a preliminary engineering agreement to develop the relocation plan and estimate. MID has provided a schedule that indicates completing the relocation plan and obtaining a contractor to relocate the utilities will take approximately 5 months. As a result, the targeted RTL date for this project is October 2025. The project's PS&E phase requires additional time to accommodate coordination with MID, and additional time to complete tasks such as bid opening, contract award, contract approval, and to address any unforeseen issues that may arise during the advertisement and award process.

Therefore, the Department is requesting a 10-month project development expenditure time extension to allow enough time to complete the PS&E phase and award the project from August 31, 2025 to June 30, 2026.

Project Number: 16
Applicant: Department of Transportation
County: Stanislaus
District: 10
PPNO: 3526
EA: 1H230
Route: 4
Allocation Amount: \$2,058,000
Remaining Balance: \$281,000
Phase: Project Approval and Environmental Document
Allocation Date: 05/19/2022
Allocation Resolution: FP-21-79
Number of Months Requested: 13 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

SHOPP Bridge Preservation project

The Department requests a 13-month time extension for the period of project development expenditure for the PA&ED phase of the SHOPP Bridge Preservation project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is on SR 4, near Copperopolis, at Hoods Creek Bridge. The project proposes to replace the bridge.

The PA&ED phase has experienced delays due to unforeseen environmental and cultural resource requirements identified during the environmental work. One of the primary causes of the delay was the unanticipated need for a potential Incidental Take Permit and a formal Section 7 consultation related to the California Tiger Salamander. This required extensive coordination with the United States Fish and Wildlife Service and the CDFW. The presence of suitable habitat within the project area for the Tiger Salamander triggered the need for additional biological surveys to be performed prior to obtaining the permit. In addition to the biological considerations, the potential presence of an archaeological site required a Phase 2 archaeological survey, further delaying the project. The environmental and cultural requirements have collectively delayed the completion of the DED and impacted the overall PA&ED schedule. The remaining work is anticipated to be completed by June 2026.

Therefore, the Department is requesting a 13-month project development expenditure time extension to allow enough time to complete the PA&ED phase from May 31, 2025 to June 30, 2026.

Project Number: 17
Applicant: Department of Transportation
County: Alpine
District: 10
PPNO: 3433
EA: 1H560
Route: 88
Allocation Amount: \$2,526,000
Remaining Balance: \$172,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/30/2022
Allocation Resolution: FP-21-92
Number of Months Requested: 6 Months
Extended Deadline: 12/31/2025
Department Recommendation: Support

SHOPP Facilities project

The Department requests a 6-month time extension for the period of project development expenditure for the PS&E phase of the SHOPP Facilities project. The Department experienced unexpected delays in completing this project's preconstruction phase.

The project is on SR 88, near Kirkwood, at the Caples Lake Maintenance Station. The project proposes to reconstruct a dormitory and sand shed structures, and rehabilitate a generator building.

The project began design in June 2022 and is nearing completion. During design, facility use requirements necessitated adjustment to the size of the proposed structures. These adjustments impacted and required adjustments to site grading, electrical, plumbing, and ventilation designs. In addition, since the project proposed to construct new facilities, approval from the Office of State Fire Marshal (SFM) was required. This process took longer than anticipated as the SFM required plan modifications to the fire suppression system for the generator building. Final approval from the SFM was received in March 2025. The design package is currently being finalized, the cost estimate is moving through certification, and RTL is expected to be completed in early May 2025. The engineer's estimate is trending higher than programmed and may require supplemental funding. The Department is currently analyzing the issue and requires additional time to prepare and submit the funding request, and advertise and complete the award process. There is a concurrent greater than 120 allocation request on this month's Commission agenda.

Therefore, the Department is requesting a 6-month project development expenditure time extension to allow enough time to request funding to complete the PS&E phase and award the project from June 30, 2025 to December 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(2), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR A
LOCALLY-ADMINISTERED STATE TRANSPORTATION IMPROVEMENT
PROGRAM PROJECT, ON THE STATE HIGHWAY SYSTEM, PER STIP
GUIDELINES
WAIVER 25-99**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the locally-administered State Transportation Improvement Program (STIP) I-280/Wolfe Road Interchange Improvement project (PPNO 2091M), in Santa Clara County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of project allocation for the locally-administered STIP I-280/Wolfe Road Interchange Improvement project (PPNO 2091M), in Santa Clara County, for the time period as identified in the attachment.

BACKGROUND:

In March 2024, the Commission programmed \$10,000,000 for the locally-administered STIP I-280/Wolfe Road Interchange Improvement project (PPNO 2091M). The responsible agency will not be able to request an allocation for funds by the June 30, 2025 deadline. The attachment describes the details of the project and explanation for the delay.

Current STIP Guidelines adopted by the Commission, stipulate that funds programmed are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the timely use of funds deadlines, one-time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Project Allocation
State Transportation Improvement Program**

Project Number: 1
Applicant: Santa Clara Valley Transportation Authority
County: Santa Clara
District: 4
PPNO: 2091M
Allocation Amount: \$10,000,000
Phase: Construction
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

I-280/Wolfe Road Interchange Improvement project

Santa Clara Valley Transportation Authority (VTA) requests a 12-month time extension for the period of project allocation for the Construction (CON) phase of the I-280/Wolfe Road Interchange Improvement project. VTA has experienced multiple legal challenges during the Right of Way (RW) phase.

The project will reconstruct the Interstate 280 (I-280) / Wolfe Road interchange in Cupertino in Santa Clara County, on I-280 from 0.2 miles north of Tantau Avenue Overcrossing to 0.1 mile south of Blaney Avenue Overcrossing.

This project's RW challenges include RW acquisitions, including permanent and temporary construction easement from four properties. The condemnation process began in April 2023 and is ongoing. Santa Clara VTA secured the Order of Procession from two of four properties in late 2024. The Right to take trials for these properties are still in progress and are estimated to take an additional three to six months to complete.

Additionally, Santa Clara VTA has not obtained a Final Order in Condemnation for two of four properties. These are expected within three to six months. Therefore, RW Certification is delayed and is expected to be completed by March 2026. Given these complexities, additional time is necessary to ensure all RW acquisitions and certification is completed. The Ready to List revised date is April 2026. Additional time is required to accommodate these delays and ensure the project's successful execution.

Therefore, Santa Clara VTA is requesting a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(3), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR
LOCALLY-ADMINISTERED STATE TRANSPORTATION IMPROVEMENT
PROGRAM PROJECTS, OFF THE STATE HIGHWAY SYSTEM, PER STIP
GUIDELINES
WAIVER 25-100**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for seven locally-administered State Transportation Improvement Program (STIP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for seven locally-administered STIP projects, for the time periods as identified in the attachment.

BACKGROUND:

The agencies will not be able to request an allocation of funds by the June 30, 2025 deadline. The attachment shows the details of the projects and the explanations for the delays. The respective agencies are requesting extensions, and the regional planning agencies concur.

Current STIP Guidelines adopted by the Commission, stipulate that funds that are programmed for all components of local grant projects are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Project Allocation
State Transportation Improvement Program**

Project Number: 1
Applicant: Humboldt County
County: Humboldt
District: 1
PPNO: 2577
Allocation Amount: \$870,000
Phase: Construction
Number of Months Requested: 6 Months
Extended Deadline: 12/31/2025
Department Recommendation: Support

Rehabilitation, Overlay, and Bicycle Improvements on Myrtle Avenue (F3K300) project

Humboldt County (County) requests a 6-month time extension for the period of project allocation for the Construction (CON) phase of the Rehabilitation, Overlay, and Bicycle Improvements on Myrtle Avenue (F3K300) project. The County experienced delays during the design phase.

The project proposes to rehabilitate pavement, install an overlay, install bicycle markings, and striping.

The project faced delays due to the community's concerns about bicycling safety, adjustments to the advertising window, and cost savings. The County needs to redesign the project to address the local bicycling community's concerns. The redesign will modify project limits to include postmiles (PM) 1.57 to PM 2.68. The new design will incorporate project-specific survey data. The County anticipates that the redesign will take three months. In addition, the County needs to adjust the project's advertising window to schedule construction to coincide with the adjacent local school's summer break. The current construction window for the project is between June and August 2025. By extending the construction allocation to October 2025, the County can strategically bid the project during the winter months, capitalizing on potential cost savings and increased contractor availability. Furthermore, the County plans to await the collection of new local voter-approved tax revenues so that the funds can be used to expand project limits. The expanded project, which will utilize local and state tax revenues to assist in constructing the project from PM 1.57 to PM 2.68, is crucial to the project's success. This step will be finalized within three months. The County is committed to completing the project and requires additional time to proceed accordingly.

Therefore, the County is requesting a 6-month time extension for the period of project allocation for the CON phase from June 30, 2025 to December 31, 2025.

Project Number: 2
Applicant: City of Etna
County: Siskiyou
District: 2
PPNO: 2595
Allocation Amount: \$681,000
Phase: Construction
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

Main Street Rehabilitation project

The City of Etna (City) requests a 20-month time extension for the period of project allocation for the CON phase of the Main Street Rehabilitation project. The City encountered delays due to a conflict with another project.

The project proposes to rehabilitate the roadway, including the replacement of the failed base section.

The City has recently secured funding through the California Public Utilities Commission's Rule 20A to convert Main Street into an underground power district. The construction of the underground power lines is expected to be completed by the summer of 2026. The City needs to postpone the Main Street Rehabilitation project until Pacific Power's work is completed to avoid damage to a newly paved street. Since the construction schedule provided by Pacific Power is tentative, the City is taking precautions and requesting the maximum time allowed to incorporate as-built conditions into the road rehabilitation plans.

Therefore, the City is requesting a 20-month time extension for the period of project allocation for the CON phase from June 30, 2025 to February 28, 2027.

Project Number: 3
Applicant: City of Portola
County: Plumas
District: 2
PPNO: 2646
Allocation Amount: \$91,000
Phase: Plans, Specifications, and Estimate
Number of Months Requested: 9 Months
Extended Deadline: 03/31/2026
Department Recommendation: Support

Rio Grande Street Pavement Rehabilitation project

The City of Portola (City) requests a 9-month time extension for the period of project allocation for the Plans, Specifications, and Estimate (PS&E) phase of the Rio Grande Street Pavement Rehabilitation project. The City experienced unforeseen delays due to responding to another urgent project.

The project proposes to rehabilitate pavement. Rehabilitation of the pavement involves full-depth recycling of the existing pavement and base, construction of new Americans with Disabilities Act curb ramps, concrete curb and gutter, concrete valley gutters, and drainage upgrades.

The project experienced unforeseen delays due to a shift in staffing resources, driven by an urgent need to secure funding for critical repairs to the Gulling Street Bridge. This bridge is the only river crossing for a 14-mile stretch, providing vital access to a regional hospital and a high school. The primary concern with the bridge is the scour-erosion of the riverbed, which undermines the spread footing of Pier 3. An underwater inspection conducted by the Department in September 2023 revealed that the footing was undermined approximately 14 inches below its base and 12 inches toward the pier centerline on the upstream side. This significantly compromised the bearing capacity of the pier footing, reducing it from an original 14 Kips per Square Foot (KSF) to 4.1 KSF, as determined in the December 2023 geotechnical assessment. Continued scour could cause pier rotation and deck damage, rendering the bridge unusable. In response to this urgent safety concern, the City shifted its focus to securing funding for repairs, which resulted in the reassessment of all ongoing projects.

It was recently determined that the project's funding could not be reassigned to the Gulling Street Bridge. In January 2025, the City Council approved \$65,000 for a temporary monitoring system to safeguard the bridge while pursuing long-term repair funding. The City has applied for Federal Emergency Management Agency assistance for storm-related damages incurred in 2023; however, the request is still under review. The City is prepared to move forward with the Rio Grande Street Pavement Rehabilitation project. However, due to the emergency response required for the Gulling Street Bridge project, the City needs additional time.

Therefore, the City is requesting a 9-month time extension for the period of project allocation for the PS&E phase from June 30, 2025 to March 31, 2026.

Project Number: 4
Applicant: City of Sacramento
County: Sacramento
District: 3
PPNO: 2218
Allocation Amount: \$2,612,000
Phase: Construction
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Pedestrian Crossing at R and 29th/30th Street project

The City of Sacramento (City) requests a 12-month time extension for the period of project allocation for the CON phase of the Pedestrian Crossing at R and 29th/30th Street project. The City experienced unforeseen delays during the preliminary engineering phase.

The project proposes to construct bulb-outs, interconnect the signals with light rail operations, signalize intersections, and relocate existing crossing signals.

During the preliminary engineering phase, it was discovered that modifying the existing light rail infrastructure to accommodate advanced preemption of traffic signals would require adding specialized rail design capabilities to the project's consulting team, which would be an additional workload for the team. Specifically, the initially assumed method of underground conduit and overhead wire connections needed for preemption became infeasible due to existing conditions and the required distance for safe design clearance. It was also determined that the modifications to the light rail infrastructure would increase the project's cost by approximately \$1,000,000, funded by the City. The time needed to locate and procure a specialized rail engineering consultant, the duration necessary to identify funding for the addition of the rail engineering consultant, and the increase in rail infrastructure construction costs resulted in a 12-month delay, necessitating a time extension.

Therefore, the City is requesting a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

Project Number: 5
Applicant: City of Rancho Cordova
County: Sacramento
District: 3
PPNO: 5604
Allocation Amount: \$5,500,000
Phase: Construction
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

White Rock Road- 0.5 Mile East of Rancho Cordova Parkway to Easterly City Limits project

The City of Rancho Cordova (City) requests a 20-month time extension for the period of project allocation for the CON phase of the White Rock Road- 0.5 Mile East of Rancho Cordova Parkway to Easterly City Limits project. The City experienced delays during the National Environmental Policy Act (NEPA) approvals and Right of Way (RW) Certifications.

The project proposes to widen a road from two to four lanes and construct center medians, landscaping, bike lanes, drainage, and roads.

The project requires compliance with federal air quality regulations, particularly in areas designated as nonattainment or maintenance for the National Ambient Air Quality Standards. This compliance was secured under NEPA approvals in 2016 and revalidated in 2020. In October 2024, the City was notified that the project needed another NEPA revalidation and an updated Air Conformity determination. In March 2025, the project received Air Conformity concurrence. This delay prevented the City from submitting the RW Certification documents, as the first requirement was NEPA Clearance. In addition, the delay in the Air Quality Conformity concurrence prevented the City from submitting the Federal Authorization to release the Congressional Direct Spending Funds supported by Congressman Bera's office and allocated as part of the federal budget.

Also, securing concurrence through the Sacramento Area Council of Governments Project Level Conformity Group added an additional five months of coordination effort related to the Federal Highway Administration (FHWA) and the United States Environmental Protection Agency. This coordination effort impacted and shifted subsequent tasks in the project, resulting in significant delays. The City plans to secure the NEPA Clearance by June 2025, RW Certification by August 2025, and finalize Federal Authorization by November 2025. The additional time will allow the City to adjust should any of the steps require more time and for any other potential unforeseen challenges.

Therefore, the City is requesting a 20-month time extension for the period of project allocation for the CON phase from June 30, 2025 to February 28, 2027.

Project Number: 6
Applicant: Sonoma Marin Area Rail Transit District
County: Sonoma
District: 4
PPNO: 2376
Allocation Amount: \$6,097,000
Phase: Construction
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

SMART Pathway/Great Redwood Trail - Santa Rosa (Guerneville Road to Airport Boulevard) project

The Sonoma Marin Area Rail Transit District (Agency) requests a 20-month time extension for the period of project allocation for the CON phase of the SMART Pathway/Great Redwood Trail - Santa Rosa (Guerneville Road to Airport Boulevard) project. The Agency experienced delays during the federal grant process.

The project proposes to construct a 4.7-mile, 8 to 10-foot-wide, Class I nonmotorized pathway in and along the railroad right of way, directly connecting the surrounding neighborhood to bicycle facilities and the Sonoma County Airport stations.

The project was awarded construction funds through two federal agencies in 2023 and 2024. Since the awards, the completion of NEPA and the execution of the two federal grant agreements have not progressed. There is no clear timeline from federal partners regarding when the grants will be successfully executed. Nevertheless, the Agency remains committed to delivering the project and cannot move forward until the awarded federal grants are fully secured in executed grant agreements.

The first grant is a \$2,000,000 FHWA formula Congestion Mitigation and Air Quality grant programmed through the Sonoma County Transportation Authority and the Metropolitan Transportation Commission's One Bay Area Grant process for the Agency. The grant was then transferred and obligated to the Federal Transit Administration (FTA), which serves as the lead agency for execution and delivery.

The second grant is a \$7,000,000 discretionary FHWA Safe Streets for All grant, awarded in September 2024. For four months, FHWA discussed transferring the grant to the FTA to combine it with the first federal grant. Both federal grants must undergo a federal environmental clearance process to complete prior to grant execution, in addition to the agreement routing and approval process. The clearance and approval process for the FTA funds began in September 2023.

The Agency cannot proceed without the execution of the existing awarded grants. The Agency continues to collaborate with federal partners, maintaining regular communication regarding

grant status, to ensure that this critical project is delivered successfully. The Agency requests additional time to extend the allocation deadline.

Therefore, the Agency is requesting a 20-month time extension for the period of project allocation for the CON phase from June 30, 2025 to February 28, 2027.

Project Number: 7
Applicant: Riverside County
County: Riverside
District: 8
PPNO: 1334
Allocation Amount: \$8,800,000
Phase: Right of Way
Number of Months Requested: 14 Months
Extended Deadline: 08/31/2026
Department Recommendation: Support

I-10 Bypass project

Riverside County (County) requests a 14-month time extension for the period of project allocation for the RW phase of the I-10 Bypass project. The County experienced unforeseen delays during coordination with tribal lands.

The project proposes to construct a bridge crossing and 2 lanes of an ultimate 4-lane roadway to provide a bypass network facility.

As part of the Project Approval & Environmental Document phase for the project, various alternatives were considered, and the preferred alternative will pass directly through the Morongo Band of Mission Indians tribal lands. Because of this, extensive coordination with the tribe was necessary to ensure compliance with tribal laws, regulations, and cultural sensitivities. Collaborating with a sovereign tribal government requires a diplomatic, thorough, and cooperative approach, which has taken significantly longer than initially anticipated, resulting in a 14-month delay.

The County recognized that moving forward with the Consultant Services Agreement for the PS&E phase without the finalized Memorandum of Understanding (MOU) with the tribe could have negative implications. It could be perceived negatively by the tribe and risk undermining the trust and collaborative relationship that has been carefully established throughout the planning process. Therefore, the County adopted a respectful and deliberate approach by pausing further project advancement until a mutual agreement was reached.

The ongoing negotiations, which took 14 months, caused the start of the PS&E phase to be delayed. This delay has also postponed the start of the RW phase, as the development of the RW phase requirements map relies on progress made during the PS&E phase. In addition, as the County advances with the PS&E phase and begins securing necessary environmental permits, further modifications to the RW needs are expected. Permit conditions, particularly those related to biological, cultural, and water resource protections, may necessitate design refinements such as avoidance areas, access adjustments, or expanded buffer zones. These modifications could directly affect the final RW phase's footprint, requiring further coordination, updates to the RW map, and refinement of cost estimates.

The County will continue to collaborate closely with permitting agencies and the tribe throughout the PS&E phase to ensure that the RW phase planning remains responsive to evolving design and environmental considerations. Once the final RW phase requirements map is established, the County will create a detailed cost estimate and seek allocation for the RW phase, which is anticipated to begin in August 2026. However, due to the complexity of the project, the County needs additional time to proceed accordingly.

Therefore, the County is requesting a 14-month time extension for the period of project allocation for the RW phase from June 30, 2025 to August 31, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8b.(2), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF CONTRACT AWARD FOR A
LOCALLY-ADMINISTERED STATE TRANSPORTATION IMPROVEMENT
PROGRAM PROJECT, OFF THE STATE HIGHWAY SYSTEM, PER STIP
GUIDELINES
WAIVER 25-101**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of contract award for the locally-administered State Transportation Improvement Program (STIP) Wescott Road Rehabilitation project (PPNO 3185), in Colusa County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of contract award for the locally-administered STIP Wescott Road Rehabilitation project (PPNO 3185), in Colusa County, for the time period as identified in the attachment.

BACKGROUND:

In January 2025, the Commission allocated \$4,200,000 to the Construction phase of the locally-administered STIP Wescott Road Rehabilitation project (PPNO 3185). In accordance with STIP Guidelines (Guidelines), the deadline to award contracts for projects allocated in January 2025 is July 31, 2025. The City of Colusa is requesting a time extension for the period of contract award. The attachment describes the details of the project and the explanation for the delays.

Current Guidelines stipulate that the agency implementing a project request a time extension if the project will not be awarded within six months of allocation. The Commission may approve a waiver to the Timely Use of Funds deadline, one time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Contract Award
State Transportation Improvement Program**

Project Number: 1
Applicant: City of Colusa
County: Colusa
District: 3
PPNO: 3185
Allocation Amount: \$4,200,000
Phase: Construction
Allocation Date: 01/31/2025
Allocation Resolution: FP-24-58
Number of Months Requested: 20 Months
Extended Deadline: 03/31/2027
Department Recommendation: Support

Wescott Road Rehabilitation project

The City of Colusa (City) requests a 20-month time extension for the period of contract award for the Construction (CON) phase of the Wescott Road Rehabilitation project. The City has experienced unforeseen delays during the award process.

The project proposes to rehabilitate existing pavement, repair drainage, and install and upgrade the Americans with Disabilities Act (ADA) improvements including handicap ramps, sidewalks, and crosswalks.

The project plans were completed nearly a decade ago, and since then, the state, local, and ADA standards have been updated. Initially, in 2014, the roadway had a Pavement Condition Index (PCI) of 65. Today, according to the 2022 traffic analysis, the PCI at the job site averages around 20. The means and methods must be revised to implement a design that meets the roadway conditions by updating the plans and specifications. Additionally, much of the sidewalks along the project site were in better condition in 2014; however, upon visual inspection, further demolition, removal, and replacement of sidewalks need to be incorporated into the design and bid documents. ADA and the City standards have also changed for the project. In 2024, the City adopted new standard construction details, which should be integrated into the design and bid documents. For bidding purposes, the bid documents must reflect these changes, including the quantities and takeoffs. The City Council has directed staff to bid the project and commence construction as quickly as possible. However, due to these necessary updates, the City needs additional time to complete the tasks and proceed accordingly.

Therefore, the City is requesting a 20-month time extension for the period of contract award for the CON phase from July 31, 2025 to March 31, 2027.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8d.(2), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT DEVELOPMENT
EXPENDITURE FOR LOCALLY-ADMINISTERED STATE TRANSPORTATION
IMPROVEMENT PROGRAM PROJECTS, OFF THE STATE HIGHWAY SYSTEM,
PER STIP GUIDELINES
WAIVER 25-102**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project development expenditure for five locally-administered State Transportation Improvement Program (STIP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project development expenditure for five locally-administered STIP projects, for the time periods as identified in the attachment.

BACKGROUND:

In March, May, and June 2023, the Commission allocated \$6,775,000 for the five locally-administered STIP projects identified in the attachment. The responsible agencies were unable to complete the phase within the expenditure period. The attachment describes the details of the projects and the explanations for the delays.

Current STIP Guidelines, adopted by the Commission stipulate that funds programmed for all components of local grant projects are available for expenditure only until the end of the second Fiscal Year following the year of allocation. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Project Development Expenditure
State Transportation Improvement Program**

Project Number: 1
Applicant: Mendocino County
County: Mendocino
District: 1
PPNO: 4590
Allocation Amount: \$1,800,000
Remaining Balance: \$1,750,134
Phase: Plans, Specifications, and Estimate
Allocation Date: 05/18/2023
Allocation Resolution: FP-22-74
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

North State Street Intersection Improvements: Phase 1 project

The Mendocino County (County) requests a 20-month time extension for the period of project development expenditure for the Plans, Specifications, and Estimate (PS&E) phase of the North State Street Intersection Improvements: Phase 1 project. The County experienced unforeseen delays during the design phase.

The project proposes to install medians, landscape and aesthetic features, and roundabouts at the intersections of Kuki Lane and the southbound United States Highway 101 interchange.

The project is significantly constrained by the existing infrastructure, including state highway ramps, local roads and businesses, utilities, the storm drain system, and limited roadway right of way. Design changes have been necessary to accommodate stakeholder input, with delays in the design process primarily due to the time required for stakeholders to respond. The project requires new Right of Way (RW), changes to the Department's RW and access control, realignment of roads and ramps, utility relocations, and modifications to private property access. Extensive coordination and multiple design iterations were necessary to resolve these issues. To prevent future litigation, the County has provided stakeholders with extended review periods to address unforeseen impacts. The environmental documents and technical studies also required additional review time. Complex utility and drainage issues have necessitated more research and analysis than initially anticipated. The County needs more time to proceed accordingly.

Therefore, the County is requesting a 20-month time extension for the period of project development expenditure for the PS&E phase from June 30, 2025 to February 28, 2027.

Project Number: 2
Applicant: City of Susanville
County: Lassen
District: 2
PPNO: 2480
Allocation Amount: \$300,000
Remaining Balance: \$294,597
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/29/2023
Allocation Resolution: FP-22-100
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Riverside Drive Reconstruction & Class I Ped-Bike lane project

The City of Susanville (City) requests a 12-month time extension for the period of project development expenditure for the PS&E phase of the Riverside Drive Reconstruction & Class I Ped-Bike lane project. The City experienced unforeseen delays in securing engineering services.

The project proposes to construct a separate Class I bicycle and pedestrian path with a pedestrian bridge, lighting, and signage.

The City has faced unexpected delays in securing engineering services for the pedestrian bridge abutments, piers, and footings. In January 2024, the City-owned golf course clubhouse experienced flooding, requiring engineering staff to secure abatement contractors and oversee remediation work. This emergency led to a 6-month setback in the project's progress. In November 2024, the St. Francis Hotel in Susanville sustained fire damage, prompting engineering staff to obtain a structural engineer's report and oversee summary abatement work to address immediate hazards. Due to the property owner's inability to complete the full abatement, the City was forced to assume responsibility for the remaining work. Engineering staff secured hazardous materials inspection services and prepared bid documents and specifications for the demolition of the remaining structure, resulting in an additional 6-month delay to the project. The City anticipates receiving proposals in the coming weeks for the required engineering services to be obtained. However, the final design work is not expected to be finalized until April 2026.

Therefore, the City is requesting a 12-month time extension for the period of project development expenditure for the PS&E phase from June 30, 2025 to June 30, 2026.

Project Number: 3
Applicant: City of Mt. Shasta
County: Siskiyou
District: 2
PPNO: 2594
Allocation Amount: \$100,000
Remaining Balance: \$100,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 05/18/2023
Allocation Resolution: FP-22-74
Number of Months Requested: 9 Months
Extended Deadline: 03/31/2026
Department Recommendation: Support

Lake Street Reconstruction project

The City of Mt. Shasta (City) requests a 9-month time extension for the period of project development expenditure for the PS&E phase of the Lake Street Reconstruction project. The City experienced unforeseen delays during the project solicitation process.

The project proposes to rehabilitate existing pavement, sections of curb, gutter, and sidewalks.

The City is dedicated to managing two separate projects on Lake Street. One is this STIP project, and the other is the Complete Streets Visioning project, called Lake Street 4 All. This project is funded through the Sustainable Communities Planning Grant via the Office of Regional and Community Planning. The City's original plan was to hire a design firm for the STIP project in the fall of 2023. However, in August 2023, the City was awarded a Sustainable Communities Planning Grant of \$140,000 to conceptualize more comprehensive complete streets components for Lake Street. The City promptly hired a planning and engineering team for the Lake Street 4 All project in August 2024. This project is well underway, and conceptual designs are being developed. In an effort to design these projects in concert, the City assumed that the planning and design team for the Lake Street 4 All project could design the STIP project. On April 17, 2025, the City identified that a separate solicitation process would need to occur for the STIP project to comply with California Government Code 4527 and public contracting code, which will pose a risk to the PS&E funds with a June 30, 2025, deadline. The City is dedicated to resolving this issue and needs additional time to move forward appropriately.

Therefore, the City is requesting a 9-month time extension for the period of project development expenditure for the PS&E phase from June 30, 2025 to March 31, 2026.

Project Number: 4
Applicant: Siskiyou County
County: Siskiyou
District: 2
PPNO: 2603
Allocation Amount: \$75,000
Remaining Balance: \$50,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/29/2023
Allocation Resolution: FP-22-100
Number of Months Requested: 6 Months
Extended Deadline: 12/31/2025
Department Recommendation: Support

Jackson Ranch Road Rehabilitation project

The Siskiyou County (County) requests a 6-month time extension for the period of project development expenditure for the PS&E phase of the Jackson Ranch Road Rehabilitation project. The County experienced unforeseen delays during the design phase.

The project proposes to restore five miles of existing roadway by pulverizing existing asphalt, shaping and re-compacting, overlaying, and striping.

The PS&E phase has been delayed six months due to unforeseen circumstances of the Klamath River Renewal Corporation Dam Removal project. It required unexpected engineering staff time due to the impacts on roadways, transportation, and encroachment permits, as well as multiple atmospheric events that caused flooding and raised concerns across the County. The County needs additional time to ensure the successful completion of the design and construction bid package for construction in summer 2026.

Therefore, the County is requesting a 6-month time extension for the period of project development expenditure for the PS&E phase from June 30, 2025 to December 31, 2025.

Project Number 5
Applicant: City of Richmond
County: Contra Costa
District: 4
PPNO: 2025H
Allocation Amount: \$4,500,000
Remaining Balance: \$4,493,669
Phase: Right of Way
Allocation Date: 03/23/2023
Allocation Resolution: FP-22-61
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

I-80/Central Avenue - Phase 2 (Local Road Realignment) project

The City of Richmond (City) requests a 20-month time extension for the period of project development expenditure for the RW phase of the I-80/Central Avenue - Phase 2 (Local Road Realignment) project. The City experienced unforeseen delays due to coordination with Pacific Gas and Electric Company (PG&E).

The project proposes to reconstruct a roadway, underground all utilities at the project site, construct a new sidewalk, and a bus stop.

The project faced delays mainly because of the PG&E implementation of Rule 20A within the project limits. Rule 20A is a provision within PG&E's Electric Tariff Rule 20 that allows PG&E to allocate funds to cities and counties to assist with the cost of undergrounding electric facilities. The completion process has been delayed by nearly two years due to staffing constraints and the project's position on PG&E's priority list. The City has been actively coordinating with PG&E to elevate the project's priority. PG&E has recognized the City's request. As a result, the project has been moved higher on their list, with tentative construction now anticipated to be completed in early 2027. The City needs additional time to ensure the successful completion of the project.

Therefore, the City is requesting a 20-month time extension for the period of project development expenditure for the RW phase from June 30, 2025 to February 27, 2027.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(4), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR A
LOCALLY-ADMINISTERED MULTI-FUNDED STATE TRANSPORTATION
IMPROVEMENT PROGRAM, TRANSIT AND INTERCITY RAIL CAPITAL
PROGRAM, AND PROPOSITION 1B INTERCITY RAIL IMPROVEMENT PROGRAM
PROJECT, PER STIP GUIDELINES
WAIVER 25-103**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the locally-administered multi-funded State Transportation Improvement Program (STIP), Transit and Intercity Rail Capital Program (TIRCP), and Proposition 1B (Prop 1B) Intercity Rail Improvement (IRI) Program Central Coast Layover Facility Rail project (PPNO 2195), in San Luis Obispo County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of project allocation for the locally-administered multi-funded STIP, TIRCP, and Prop 1B IRI Program Central Coast Layover Facility Rail project (PPNO 2195), in San Luis Obispo County, for the time period as identified in the attachment.

BACKGROUND:

The Los Angeles – San Diego – San Luis Obispo (LOSSAN) Rail Corridor Agency will not be able to request an allocation of STIP programmed funds by the June 30, 2025 deadline. The attachment shows the details of the project and the explanation for the delays. The TIRCP and Prop 1B IRI funds do not require an allocation time extension at this time.

Current STIP Guidelines stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the Timely Use of Funds deadline, one-time only, for up to 20 months.

Attachment

“Improving lives and communities through transportation.”

**Time Extension/Waiver – Project Allocation
Multi-Funded STIP, TIRCP, and Prop 1B IRI**

Project Number: 1
Applicant: Los Angeles – San Diego – San Luis Obispo Rail Corridor Agency
County: San Luis Obispo
District: 75
PPNO: 2195
Allocation Amount: STIP \$9,000,000
Phase: Construction
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

Central Coast Layover Facility

The Los Angeles – San Diego – San Luis Obispo (LOSSAN) Rail Corridor Agency (Agency) is requesting a 20-month time extension for the period of project allocation for the Construction (CON) phase of the Central Coast Layover Facility Rail Project. This extension request applies only to the State Transportation Improvement Program (STIP) portion of the project. The Agency experienced delays acquiring the land necessary for the facility.

The proposed project involves expanding the existing Amtrak layover facility in San Luis Obispo to increase overnight train storage capacity, enhance maintenance capabilities, and meet the objectives that align with program goals and the California State Rail Plan. The expansion includes the relocation and construction of a new maintenance and layover facility south of the San Luis Obispo station, which is in a vacant yard owned by the Union Pacific Railroad (UPRR) that the Department will purchase. This facility will accommodate additional and longer trains and allow for movement between the station and maintenance area without disrupting mainline passenger or freight operations.

The project also seeks to integrate this new facility with the City of San Luis Obispo's vision for their Roundhouse District (District), which is a City designated zone that this project falls within. In coordination with the City of San Luis Obispo, the project will conform to this District's ordinances to provide a pedestrian and bike trail that connects the District to the station while serving as a buffer between this facility and nearby development. This expanded facility supports potential future service extensions to Northern California, contingent on further infrastructure investments along the Central Coast.

The primary cause of this requested time extension is the delay in land purchase. The Agency is working closely with the Department to help facilitate the purchase from UPRR; a process that is now expected to take approximately 24 months due to required procedural steps. Since this timeline is excessive, the Agency has decided to purchase an interim lease agreement

with UPRR, to stay within the requested 20-month extension, and be able to allocate for construction, while the actual purchase is being finalized. A draft lease is expected from UPRR in the next couple of months, though details such as insurance requirements and exact property boundaries still need to be determined.

In summary, the additional time requested is due to certain delays and unanticipated lengthened timelines as outlined below:

- Delays in obtaining Right of Entry Permits from UPRR. Four permits were required. The total cumulative delay was 10 months; pushing back the initial land purchase coordination with the Department to May 2024.
- Schedule and perform field review by the Department – 2 months. (July 2024)
- Per the Department a Phase II Environmental Site Assessment is required for purchase. Time to procure consultant – 3 months (October 2024)
- Time for consultant to gain Right of Entry to perform Phase II investigations – 8 months (May 2025)
- Time to complete Phase II analysis – 1 month (July 2025)
- Time to update Appraisal and perform property boundary survey – 2 months (September 2025)
- Time to negotiate lease agreement terms and procure insurance – 5 months (February 2026)
- Time to finalize agreement and execute – 2 months (April 2026)
- Time to procure contractor – 8 months (December 2026)
- Time to allocate for construction – 2 months (February 2027)

Therefore, the Agency is requesting a 20-month time extension for the period of project allocation for the CON phase from June 30, 2025 to February 28, 2027.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(5), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR
A STATE-ADMINISTERED MULTI-FUNDED SENATE BILL 1 LOCAL
PARTNERSHIP PROGRAM (FORMULAIC) AND STATE TRANSPORTATION
IMPROVEMENT PROGRAM PROJECT, ON THE STATE HIGHWAY SYSTEM,
PER LPP (FORMULAIC) AND STIP GUIDELINES
WAIVER 25-104**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the State-Administered multi-funded Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) and State Transportation Improvement Program (STIP) Excelsior Expressway project (PPNO 6705), in Kings and Fresno counties, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for the State-Administered multi-funded SB 1 LPP (Formulaic) and STIP Excelsior Expressway project (PPNO 6705), in Kings and Fresno counties, for the time period as identified in the attachment.

BACKGROUND:

The Department will not be able to request allocation for funds by the June 30, 2025 deadline. The attachment shows the details of the project and the explanation for the delays.

Current SB 1 LPP (Formulaic) and STIP Guidelines adopted by the Commission stipulate that funds programmed for all components are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months for SB 1 LPP and 20 months for STIP.

Attachment

**Time Extension/Waiver – Project Allocation
Multi-Funded LPP (Formulaic) and STIP**

Project Number: 1
Applicant: Department of Transportation
County: Kings and Fresno
District: 6
PPNO: 6705
Allocation Amount: \$37,164,000 (LPP (Formulaic): \$4,164,000 and STIP: \$33,000,000)
Phase: Construction
Number of Months Requested: 4 Months
Extended Deadline: 10/31/2025
Department Recommendation: Support

Excelsior Expressway

The Department requests a 4-month time extension for the period of project allocation for the Construction (CON) phase of the Excelsior Expressway project. The Department has experienced delays in completing this project's Right of Way (RW) phase.

This project will widen State Route 41 from a 2-lane conventional highway to a 4-lane expressway in Kings and Fresno counties about 6 miles north of Lemoore, from 0.3 miles north of the Excelsior Avenue Undercrossing to 1.0 mile north of Elkhorn Avenue.

The delays in obtaining RW Certification stem from utility coordination issues, specifically with Pacific Gas and Electric (PG&E). Initial utility relocation plans were not received from PG&E until April 1, 2025, significantly later than anticipated. Since then, two rounds of revisions have been required, and the utility relocation plans and agreements are currently in the final stages of completion. These delays have impacted the project's ability to obtain RW Certification on schedule. To ensure the project remains eligible for funding and can proceed without further delay once certification is received, the Department is requesting more time to obtain RW Certification. Approval of this extension will allow sufficient time to finalize all necessary documentation and proceed with allocation and advertisement immediately upon achieving Ready to List. Additionally, this project is being combined with an existing State Highway Operation and Protection Program (SHOPP) funded project. Due to the complexities of the funding portfolio, additional time is required to finalize funding and cost updates. This requires thorough coordination with multiple funding partners to ensure accuracy and compliance with LPP (Formulaic), STIP and SHOPP guidelines.

Therefore, the Department is requesting a 4-month time extension for the period of project allocation for the CON phase from June 30, 2025 to October 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8b.(3), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF CONTRACT AWARD FOR
A STATE-ADMINISTERED MULTI-FUNDED SENATE BILL 1 LOCAL
PARTNERSHIP PROGRAM (FORMULAIC AND COMPETITIVE) AND STATE
TRANSPORTATION IMPROVEMENT PROGRAM PROJECT, ON THE STATE
HIGHWAY SYSTEM, PER LPP (FORMULAIC AND COMPETITIVE) AND STIP
GUIDELINES
WAIVER 25-105**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of contract award for the State-Administered multi-funded Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic and Competitive) and State Transportation Improvement Program (STIP) Oakland Alameda Access Project (PPNO 0044D), in Alameda County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of contract award for the State-Administered multi-funded SB 1 LPP (Formulaic and Competitive) and STIP Oakland Alameda Access Project (PPNO 0044D), in Alameda County, for the time period as identified in the attachment.

BACKGROUND:

In December 2024, the Commission allocated \$67,625,000 to the Construction phase of the State-Administered multi-funded SB 1 LPP (Formulaic and Competitive) and STIP Oakland Alameda Access Project (PPNO 0044D). In accordance with SB 1 LPP (Formulaic and Competitive) and STIP Guidelines (Guidelines), the deadline to award contracts for projects allocated in December 2024 is June 30, 2025. The Department is requesting a time extension for the period of contract award. The attachment shows the details of the project and the delays that have resulted in this time extension request.

Current Guidelines stipulate that the agency implementing a project request a time extension if the project is not awarded within six months of allocation. The Commission may approve waivers to the Timely Use of Funds deadline, one-time only, for up to 12 months for SB 1 LPP (Formulaic and Competitive) and up to 20 months for STIP.

Attachment

**Time Extension/Waiver – Contract Award
Multi-Funded SB 1 LPP (Formulaic and Competitive) and STIP**

Project Number: 1
Applicant: Department of Transportation
County: Alameda
District: 4
PPNO: 0044D
EA: 0G360
Route: 260 and 880
Allocation Amount: \$67,625,000 (LPP (Competitive): \$25,000,000, LPP (Formulaic): \$7,281,000
and STIP: \$35,344,000)
Phase: Construction
Allocation Date: 12/06/2024
Allocation Resolution: LPP-A-2425-14 and FP-24-46
Number of Months Requested: 9 Months
Extended Deadline: 03/31/2026
Department Recommendation: Support

Oakland Alameda Access Project

The Department requests a 9-month time extension for the period of contract award for the Construction (CON) phase of the Oakland Alameda Access Project. The Department has experienced delays in awarding the project.

This project is located on State Route (SR) 260 and Interstate 880 (I-880), between 5th Avenue Overhead and 5th and 6th Street Viaduct within the cities of Oakland and Alameda in Alameda County. The project will improve access along I-880 and in and around the Webster and Posey Tubes, downtown Oakland, and the City of Alameda. Proposed improvements also include implementation of follow-up landscaping work to restore removed vegetation required by the highway improvements.

The project achieved Ready to List (RTL) in October 2024 and was initially scheduled to advertise in January 2025, with bids opening in February 2025. However, due to delays related to updating the RTL package and associated coordination efforts, the advertisement occurred in April 2025, and the bid opening was revised to October 2025.

The RTL package required updates to comply with the newly issued 2024 Department Construction Contract Standards, released on October 21, 2024. The project includes numerous plan sheets, specifications, and technical reports. Updating these documents to reflect new standards required extensive review by multiple functional units within the Department.

Additionally, the project involves significant improvements to local streets within the jurisdiction of the cities of Oakland and Alameda, which will be responsible for maintaining these improvements. As a result, the updated Plans, Specifications, and Estimate (PS&E) package also needed review and approval from the cities. Because the cities follow American Public

Works Association specifications, incorporating local design elements into the Department's contract documents necessitated the development and inclusion of several non-standard special provisions. This added another layer of review, approval, and coordination between the Department and the cities.

The need to update the RTL package for the new standards and the extensive coordination with the local agencies contributed to a 4-month delay in the project schedule, pushing advertisement to April 2025. Due to the project's size and complexity, additional time is being requested to allow for any unforeseen issues with the advertisement, bidder inquiries, possible addenda, and bid protests that may arise.

Therefore, the Department is requesting a 9-month time extension for the period of contract award for the CON phase from June 30, 2025 to March 31, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8d.(3), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT DEVELOPMENT
EXPENDITURE FOR A LOCALLY-ADMINISTERED MULTI-FUNDED SENATE
BILL 1 LOCAL PARTNERSHIP PROGRAM (FORMULAIC) AND STATE
TRANSPORTATION IMPROVEMENT PROGRAM PROJECT, ON THE STATE
HIGHWAY SYSTEM, PER LPP (FORMULAIC) AND STIP GUIDELINES
WAIVER 25-106**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project development expenditure for the locally-administered multi-funded Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) and State Transportation Improvement Program (STIP) State Route 132 West Freeway/Expressway Phase II Project (PPNO 0944A), in Stanislaus County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of project development expenditure for the locally-administered multi-funded SB 1 LPP (Formulaic) and STIP State Route 132 West Freeway/Expressway Phase II Project (PPNO 0944A), in Stanislaus County, for the time period as identified in the attachment.

BACKGROUND:

In May 2023, the Commission allocated \$5,495,000 to the Right of Way (RW) phase of the locally-administered multi-funded SB 1 LPP (Formulaic) and STIP State Route 132 West Freeway/Expressway Phase II Project (PPNO 0944A). In accordance with the LPP (Formulaic) and STIP Guidelines (Guidelines), the deadline to complete the RW phase for projects allocated in May 2023 is June 30, 2025. The projects will not be able to meet the deadline, and Stanislaus Council of Governments is requesting an expenditure time extension for the

RW phase. The attachment shows the details of the project and the delays that have resulted in the time extension request.

Pursuant to the Guidelines, funds allocated for project development costs must be expended by the end of the second fiscal year following the fiscal year in which the funds were allocated. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Project Development Expenditure
Multi-Funded SB 1 LPP (Formulaic) and STIP**

Project Number: 1
Applicant: Stanislaus Council of Governments
County: Stanislaus
District: 10
PPNO: 0944A
EA: 40352
Route: 99 and 132
Allocation Amount: \$5,495,000 (LPP (Formulaic): \$3,217,000, STIP: \$2,278,000)
Remaining Balance: \$32,000
Phase: Right of Way
Allocation Date: 05/18/2023
Allocation Resolution: LPP-A-2223-36 and FP-22-81
Number of Months Requested: 12 Months
Extended Deadline: 09/30/2026
Department Recommendation: Support

State Route 132 West Freeway/Expressway Phase II Project

The Stanislaus Council of Governments (StanCOG) requests a 12-month time extension for the period of project development expenditure for the Right of Way (RW) phase of the State Route 132 West Freeway/Expressway Phase II Project. In August 2023, the project received an allocation amendment for an additional three months for project development expenditure under the Coronavirus Response and Relief Supplemental Appropriations Act of 2021, extending the deadline to from June 30, 2025 to September 30, 2025. StanCOG experienced unexpected delays in completing the project's RW phase.

The project is located on State Route (SR) 99 and SR 132. This project will add two lanes to the existing 2-lane access-controlled facility to create a 4-lane expressway, from Needham Street on the east side of SR 99 to Dakota Avenue. The project includes construction of freeway-to-freeway connectors for eastbound SR 132 to southbound SR 99 and northbound SR 99 to westbound SR 132. The project will also include construction of a partial interchange at Carpenter Road.

The project's RW phase requires additional time due to several critical factors that have emerged since reaching key milestones. Although the PS&E phase reached the 65 percent completion milestone in April 2024 with the RW needs established and RW acquisition documents finalized in July 2024, the process has proven lengthier than anticipated. The appraisal phase began in August 2024 on the 50 properties involved. Since then, several property owners have requested to obtain independent appraisals. Additionally, parcels involving relocations need sufficient time for impacted owners to find and secure suitable replacement properties. In addition, a major component of the project includes acquiring RW from the City of Modesto's Corporation Yard. This facility houses essential services including the 911 call center. The City of Modesto has requested additional time to adequately and carefully plan the

relocation of these services to avoid disruptions. StanCOG anticipates addressing and resolving the issues and completing the phase by June 2026.

Therefore, StanCOG is requesting a 12-month time extension for the period of project development expenditure to allow enough time to finalize the RW phase, from September 30, 2025 to September 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(6), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR
LOCALLY-ADMINISTERED SENATE BILL 1 LOCAL PARTNERSHIP PROGRAM
(FORMULAIC) PROJECTS, OFF THE STATE HIGHWAY SYSTEM, PER LPP
(FORMULAIC) GUIDELINES
WAIVER 25-107**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for two locally-administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of project allocation for two locally-administered SB 1 LPP (Formulaic) projects, for the time periods as identified in the attachment.

BACKGROUND:

The local agencies will not be able to request allocation for funds by the June 30, 2025, deadline. The attachment shows the details of the projects and the explanation for the delays. The respective agencies are requesting extensions, and the regional planning agencies concur.

Current LPP (Formulaic) Guidelines adopted by the Commission, stipulate that funds programmed for all components of local grant projects are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the Timely Use of Funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Allocation
Local Partnership Program (Formulaic)**

Project Number: 1
Applicant: City of Sacramento
County: Sacramento
District: 3
PPNO: 2222
Allocation Amount: \$809,000
Phase: Construction
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Traffic Signal Safety project

The City of Sacramento (City) requests a 12-month time extension for the period of project allocation for the Construction (CON) phase of the Traffic Signal Safety project. The City experienced unforeseen delays due to the inclusion of permanent easement acquisition.

The project proposes to construct intersection improvements at nine separate locations throughout the city.

During the preliminary design phase of the Project Approval and Environmental Document (PA&ED), topographic survey and right of way mapping were conducted, indicating that permanent easement acquisition would be necessary at two locations, along with temporary construction easements for seven parcels. The needs for property rights were unforeseen conditions discovered during preliminary engineering that warranted a supplemental agreement for additional professional services and identification of additional funding. Following the approval of the supplemental agreement, the City ordered preliminary title reports, and prepared plats and legal descriptions for the future acquisitions. This process resulted in a 4-month delay. Additionally, the project was presented to the City's Disability Advisory Commission and Active Transportation Commission. The feedback from both Commissions necessitated further unanticipated traffic analysis and engineering design to consider other control types. Consequently, the project development team reevaluated the types of signal control resulting in a 3-month delay.

Furthermore, the City did not anticipate significant stakeholder and community interest that would necessitate changes to the project's scope. Representatives from Sacramento Country Day School and Sacramento New Technology High School made land use decisions affecting circulation patterns that led to alterations in the project scope. These decisions resulted in upgraded bus stops, an additional sidewalk, and improvements to comply with accessibility standards for a new transit program. Additionally, the City needed to incorporate pavement rehabilitation at various locations not initially included in the project. Upon evaluating pavement conditions, it became evident that rehabilitation was necessary at sites where advanced traffic

signal loop detection would be installed. This was unforeseen and recognized only after the loop detection was designed and the pavement conditions were assessed. The City was then required to find a funding source for the improvements associated with the unanticipated design and construction costs linked to pavement rehabilitation, bus stop upgrades, and sidewalk enhancements. An additional \$800,000 from local funds was added to the project for pavement rehabilitation at multiple locations. This coordination resulted in a 5-month delay. The City is committed to proceeding with the project as planned; however, it requires additional time.

Therefore, the City is requesting a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

Project Number: 2
Applicant: San Bernardino County
County: San Bernardino
District: 8
PPNO: 1342
Allocation Amount: \$263,000
Phase: Plans, Specifications, and Estimate
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

National Trails Highway Bridge Replacements, Amboy to Kelbaker project

The San Bernardino County (County) requests a 12-month time extension for the period of project allocation for the Plans, Specifications, and Estimate (PS&E) phase of the National Trails Highway Bridge Replacements, Amboy to Kelbaker project. The County experienced unforeseen delays due to various reasons.

The project proposes to replace three bridges on National Trails Highway between 0.96 miles east of Amboy Road and 0.55 miles east of Kelbaker Road.

The County received an allocation for the PA&ED phase in August 2024. In November 2024, the County entered into an agreement with Dokken Engineering for the professional engineering and environmental services related to the project. Due to unresponsive property owners, the County encountered difficulties obtaining Potential Toxic Elements (PTE) necessary for the environmental and biological surveys. With the PTEs in place, Dokken Engineering is currently working on the 35 percent design and will soon begin the environmental and biological surveys.

In addition, as the project has federal funds programmed for construction, the Mojave Sanctions imposed by the United States Environmental Protection Agency (EPA) on the Mojave Desert Air Quality Management District may cause significant delays in obtaining these funds. These sanctions grant the EPA the authority to suspend federal approval of projects and award certain grants for non-exempt transportation projects, making them a key factor in the project's current situation. All federal approvals will cease on August 1, 2025, and will not resume until an EPA-approved California State Implementation Plan (SIP) is in place unless the EPA approves a revised SIP by July 31, 2025. The County needs additional time in the event environmental clearance cannot be completed by July 31, 2025. The County is at risk of losing federal funds, as the delay to the Mojave Sanctions may last up to a year.

Therefore, the County is requesting a 12-month time extension for the period of project allocation for the PS&E phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(7), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR
LOCALLY-ADMINISTERED SENATE BILL 1 LOCAL PARTNERSHIP PROGRAM
(COMPETITIVE) TRANSIT COMPONENTS, PER LPP (COMPETITIVE)
GUIDELINES
WAIVER 25-108**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for three locally administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Competitive) Southeast Los Angeles Transit Improvements Project components, in Los Angeles County, for the time periods as identified in the attachment?

RECOMMENDATION:

The Department of Transportation recommends that the Commission extend the period of project allocation for three locally administered SB 1 LPP (Competitive) Southeast Los Angeles Transit Improvements Project components, in Los Angeles County, for the time periods as identified in the attachment.

BACKGROUND:

The Los Angeles County Metropolitan Transportation Authority will not be able to request an allocation of programmed funds by the June 30, 2025, deadline. The attachment shows the details of the projects and the explanations for the delays.

Current LPP (Competitive) Guidelines stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Allocation
Local Partnership Program (Competitive)**

Project Number: 1
Applicant: Los Angeles County Metropolitan Transportation Authority
County: Los Angeles
District: 7
PPNO: LP011C
Allocation Amount: \$250,000
Phase: Construction
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Southeast Los Angeles Transit Improvements Project (Lighting)

The Los Angeles County Metropolitan Transportation Authority (Metro) requests a 12-month time extension for the period of project allocation for the Construction (CON) phase of the Southeast Los Angeles Transit Improvements Project (Lighting) component. Metro has experienced delays in procuring design services for the Termini/Layover component.

Construction of the lighting upgrades will begin after the completion of bus shelters with real-time displays, which is one of the four components awarded under this project. The Lighting component will help to improve bus service and enhance customer amenities in NextGen Tier 1 and 2 high frequency corridors throughout the Interstate 710 South communities in southeast Los Angeles County through the purchase and installation of solar-powered lighting upgrades.

Metro continues collaboration with eleven local jurisdictions to negotiate and execute Memorandums of Understanding (MOUs) that define the scope of work for proposed bus shelters with real-time display upgrades at 100 stops without existing shelters. Extensive project portfolios and limited staffing capacity among Metro and local partners are the factors that contributed to delays in necessary coordination for reaching contract award. Additionally, Metro in coordination with 11 local partners took more time than estimated to complete selection of sites for lighting following the selection of sites for shelters (which will include lighting). Now that shelters' site selection is nearly completed Metro and partners are shifting focus toward the lighting project which Metro expects will be a faster process since lighting amenities are easier to procure and install than actual bus shelters and real-time displays.

Metro requires executed MOUs with each of 11 jurisdictions before contract award for proposed bus shelters with real-time displays upgrades, and expects to complete MOU negotiations, including several layers of reviews, by December 31, 2025. As each respective

MOU is finalized, including selection of the shelters' location, Metro and its local partners will be able to issue authorization to proceed with procurement and installation of the lighting amenity upgrades after the bus shelters are installed.

Therefore, Metro is requesting a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

Project Number: 2
Applicant: Los Angeles County Metropolitan Transportation Authority
County: Los Angeles
District: 7
PPNO: LP011D
Allocation Amount: \$4,000,000
Phase: Construction
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Southeast Los Angeles Transit Improvements Project (Termini/Layover)

The Los Angeles County Metropolitan Transportation Authority (Metro) requests a 12-month time extension for the period of project allocation for the CON phase of the Southeast Los Angeles Transit Improvements Project (Termini/Layover) component. Metro has experienced delays in procuring design services.

The Termini/Layover component will implement civil works at the Norwalk, Artesia, and Compton Stations' mobility hubs. The civil works will include adding shelters and modifying existing curb and sidewalk to create more space for layovers at the mobility hubs.

Metro is currently working to procure design services despite previous delays caused by limited staffing capacity with extensive project portfolios at Metro. Metro staff working on this project were also allocated responsibility for major special events service planning for the 2026 World Cup and 2028 Olympic and Paralympic Games, which limited time available to move forward on procuring design services. Since September 2024, the challenges causing delay have been resolved with the placement of additional staff to prioritize the work required to procure design services by the end of September 2025, which includes working internally to schedule a task order within an on-call contract. Once a consultant is onboard, Metro estimates design completion by June 30, 2026, and subsequently award the contract for construction by December 31, 2026. The additional time requested will allow Metro to complete this critical phase without compromising the quality or integrity of the project.

Therefore, Metro is requesting a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

Project Number: 3
Applicant: Los Angeles County Metropolitan Transportation Authority
County: Los Angeles
District: 7
PPNO: LP011E
Allocation Amount: \$7,250,000
Phase: Construction
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Southeast Los Angeles Transit Improvements Project (ZEB Charging)

The Los Angeles County Metropolitan Transportation Authority (Metro) requests a 12-month time extension for the period of project allocation for the CON phase of the Southeast Los Angeles Transit Improvements Project (ZEB Charging) component. Metro experienced delays in preparing for contract award due to the extra time taken to determine locations for the installation of Zero-Emission Bus (ZEB) charging masts.

The component proposes the purchase and installation of ZEB charging masts in NextGen Tier 1 and 2 high frequency corridors throughout the I-710 South communities in southeast Los Angeles County to improve bus service with speed and reliability infrastructure and enhanced customer amenities.

In early 2024, Metro determined that the ZEB charging masts were no longer needed at Compton Station as originally proposed in the project application. In June 2024, Metro implemented a revised service plan extending the existing route to Downey Depot, based on higher ridership travel patterns. The new installation locations will be split between Norwalk Station (six chargers) and Artesia Station (four chargers); and Metro is currently working with the Department to amend the scope accordingly. The reason for the change in installation locations was because ridership travel patterns to Compton Station failed to increase to a reasonable level that would warrant placement of ZEB charging masts, so Metro extended the route to the Downey Depot Transit Center where ridership is higher. Metro estimates base contract award in November 2025, and an options contract award by June 30, 2026, which will carry the scope for this component's procurement and installation of ZEB chargers. Metro is currently estimating construction to begin before the end of December 2026.

Therefore, Metro is requesting a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(8), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR A
LOCALLY-ADMINISTERED SENATE BILL 1 LOCAL PARTNERSHIP PROGRAM
(COMPETITIVE) PROJECT, PER LPP (COMPETITIVE) GUIDELINES
WAIVER 25-109**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the locally-administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Competitive) Gold Line Light Rail Platform Modifications - Phase 2 project (PPNO LP008), in Sacramento County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for the locally-administered SB 1 LPP (Competitive) Gold Line Light Rail Platform Modifications - Phase 2 project (PPNO LP008), in Sacramento County, for the time period as identified in the attachment.

BACKGROUND:

In June 2023, the Commission programmed \$10,000,000 for the Construction phase of the locally-administered SB 1 LPP (Competitive) Gold Line Light Rail Platform Modifications - Phase 2 project (PPNO LP008). The responsible agency will not be able to request an allocation for funds by the June 30, 2025 deadline. The attachment describes the details of the project and the explanation for the delay.

Current LPP Guidelines adopted by the Commission stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Allocation
Local Partnership Program (Competitive)**

Project Number: 1
Applicant: Sacramento Regional Transit District
County: Sacramento
District: 3
PPNO: LP008
Allocation Amount: \$10,000,000
Phase: Construction
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Gold Line Light Rail Platform Modifications - Phase 2

The Sacramento Regional Transit District (SacRT) requests a 12-month time extension for the period of project allocation for the Construction (CON) phase of the Gold Line Light Rail Platform Modifications - Phase 2 project. SacRT has experienced unexpected delays during the design of the project.

The project proposes to complete the conversion of 29 existing light rail stations to accommodate newly acquired low-floor light rail vehicles. Station modifications include adjusting station platforms to an eight-inch level above the top of the rail, adjusting furniture, equipment, and signage to the new platform height, improving drainage and landscaping, modifying entrance and exit ramps, retaining walls and curbs, and installing detectable warning surface and direct guidance tiles.

The project experienced delays during the testing and burn in of the Light Rail Vehicles (LRVs). It was discovered that the Train to Wayside Communications (TWC) signal system, which is crucial for train signaling, control, and data transmission, was not communicating with the train. Additionally, it was discovered that the route selector would require SacRT staff to deboard the LRVs and manually change the route switch. This is a safety issue for staff and SacRT must redesign portions of the light rail platforms to accommodate this. SacRT must relocate the TWC signaling and equipment, as well as and the route selector switch and equipment, in the design.

Additionally, due to the light rail platforms needing to be increased by eight inches, it was discovered that multiple trees needed to be removed to accommodate the original scope design.

Finally, the 23rd Street station was discovered to not be long enough for boarding for people with disabilities. This led to the 23rd Street station needing spur track removal and extensive redesign to accommodate the new 3-car trains so people with disabilities may board, the introduction of additional detectable warning surface and direct guidance tiles to be added to the platform, and the relocation of wayfinding signage and the braille signs. With these changes occurring, the completion of the design work has been extended to June 2025.

Therefore, SacRT is requesting a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8b.(4), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: REQUEST TO EXTEND THE PERIOD OF CONTRACT AWARD FOR A
LOCALLY- ADMINISTERED LOCAL PARTNERSHIP PROGRAM (COMPETITIVE)
TRANSIT PROJECT, PER LPP (COMPETITIVE) GUIDELINES
WAIVER 25-110

ISSUE:

Should the California Transportation Commission (Commission) extend the period of contract award for the locally-administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Competitive) SamTrans Emission Zero Project (PPNO LP010), in San Mateo County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of contract award for the locally-administered SB 1 LPP (Competitive) SamTrans Emission Zero Project (PPNO LP010), in San Mateo County, for the time period as identified in the attachment.

BACKGROUND:

In January 2025, the Commission allocated \$14,824,000 for the Construction phase of the locally-administered SB1 LPP (Competitive) SamTrans Emission Zero Project (PPNO LP010). The responsible agency has been unable to award the contract per the Timely Use of Funds (TUF) deadline. The attachment describes the details of the project and the explanation for the delays.

Current LPP Guidelines stipulate that the agency implementing a project request a time extension if the project will not be completed within the specified timeline. Per the TUF policy, an extension can be requested for up to 12 months for each delivery milestone, where permissible by statute.

Attachment

**Time Extension/Waiver – Contract Award
Local Partnership Program (Competitive)**

Project Number: 1
Applicant: San Mateo County Transit District
County: San Mateo
District: 4
PPNO: LP010
Allocation Amount: \$14,824,000
Phase: Construction
Allocation Date: 01/31/2025
Allocation Resolution Number: LPP-A-2425-17
Number of Months Requested: 12 Months
Extended Deadline: 07/31/2026
Department Recommendation: Support

SamTrans Emission Zero Project

The San Mateo County Transit District (SamTrans) requests a 12-month time extension for the period of contract award for the Construction (CON) phase of the SamTrans Emission Zero Project. SamTrans has experienced delays in awarding the contract.

The project is located in San Mateo County. The purpose of this project is for the construction of electric charging infrastructure at the SamTrans South Base Maintenance and Operations Facility. The project will construct core transit capital development improvements, including procurement and installation of a power distribution network scalable for future battery-electric bus expansion.

During the CON phase, the contract bid came in \$14,000,000 higher than anticipated, particularly for the cost of equipment and materials. Due to design changes to accommodate Pacific Gas and Electric Company requirements and contractors expecting tariffs to take effect. SamTrans is in the process of obtaining additional local funds to cover the unanticipated cost increase and is requesting 12-months to ensure adequate funding is in place before a contract is awarded. The extension will allow for proper coordination with board schedules and procurement requirements.

This extension will allow SamTrans to complete all ongoing negotiations for contract award. Therefore, SamTrans is requesting a 12-month time extension for the period of contract award for the CON phase from July 31, 2025 to July 31, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8c.(2), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT COMPLETION FOR A
STATE-ADMINISTERED SENATE BILL 1 LOCAL PARTNERSHIP PROGRAM
(FORMULAIC) PROJECT, ON THE STATE HIGHWAY SYSTEM,
PER LPP (FORMULAIC) GUIDELINES
WAIVER 25-111**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project completion for the State-Administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) San Benito Route 156 Improvement Project (PPNO 0297), in San Benito County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project completion for the State-Administered SB 1 LPP (Formulaic) San Benito Route 156 Improvement Project (PPNO 0297), in San Benito County, for the time period as identified in the attachment.

BACKGROUND:

In June 2021, the Commission allocated \$2,500,000 in Construction Capital for the State-Administered SB 1 LPP (Formulaic) San Benito Route 156 Improvement Project (PPNO 0297). The project was awarded in June 2022. In accordance with SB 1 LPP (Formulaic) Guidelines (Guidelines), the deadline to complete the Construction Contract Acceptance for projects awarded in June 2022 is June 30, 2025. The Department is requesting a time extension to complete construction. The attachment shows the details of the project and the delays that have resulted in the time extension request.

Current Guidelines stipulate that the agency implementing a project request a time extension if the project will not be completed within 36 months after award. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Project Completion for
Senate Bill 1 Local Partnership Program (Formulaic)**

Project Number: 1
Applicant: Department of Transportation
County: San Benito
District: 5
PPNO: 0297
EA: 34490
Route: 156
Allocation Amount: \$2,500,000
Phase: Construction
Allocation Date: 06/24/2021
Allocation Resolution: LPP-A-2021-26
Award Date: 06/10/2022
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

San Benito Route 156 Improvement Project

The Department requests a 12-month time extension to complete the Construction phase of the San Benito Route 156 Improvement Project. The Department has experienced unexpected delays in completing construction.

The project is located in San Juan Bautista, from San Canyon Road (The Alameda) to east of Fourth Street. The project will widen the 2-lane facility to 4 lanes.

The project began construction in June 2022 and anticipated reaching Construction Contract Acceptance (CCA) by September 2025. However, the deadline was revised to September 2026 to account for the one-year plant establishment. The State Transportation Improvement Program (STIP) Construction Capital and Construction Support funds were allocated in October 2020 and the LPP (Formulaic) Construction Capital funds were allocated in June 2021. The STIP funds received an additional 12-months to complete the Construction phase during the time of allocation for a total of 48 months. However, the LPP (Formulaic) funds did not receive the 12 months at allocation. To help align the funding streams, additional time is being requested. The Commission previously approved non-proportional spending on this project, as a result, the LPP (Formulaic) funds are anticipated to be expended within the next 12 months.

Therefore, the Department is requesting a 12-month time extension to allow sufficient time to achieve CCA from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8d.(4), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT DEVELOPMENT
EXPENDITURE FOR A STATE-ADMINISTERED SENATE BILL 1 LOCAL
PARTNERSHIP PROGRAM (FORMULAIC) PROJECT, ON THE STATE HIGHWAY
SYSTEM, PER LPP (FORMULAIC) GUIDELINES
WAIVER 25-112**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project development expenditure for the State-Administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) 805/15 Transit Priority Connectors project (PPNO 1500), in San Diego County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project development expenditure for the State-Administered SB 1 LPP (Formulaic) 805/15 Transit Priority Connectors project (PPNO 1500), in San Diego County, for the time period as identified in the attachment.

BACKGROUND:

In June 2023, the Commission allocated \$7,200,000 in project development support for the State-Administered SB 1 LPP (Formulaic) 805/15 Transit Priority Connectors project (PPNO 1500) identified in the attachment. In accordance with the SB 1 LPP (Formulaic) Guidelines (Guidelines), the deadline to complete the Plans, Specifications, and Estimate (PS&E) phase for projects allocated in June 2023 is June 30, 2025. The project will not be able to meet the deadline, and the Department is requesting an expenditure time extension for the PS&E phase. The attachment shows the details of the project and the delays that have resulted in the time extension request.

Pursuant to the Guidelines, funds allocated for project development costs must be expended by the end of the second fiscal year following the fiscal year in which the funds were allocated. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Development Expenditure
Local Partnership Program (Formulaic)**

Project Number: 1
Applicant: Department of Transportation
County: San Diego
District: 11
PPNO: 1500
EA: 2T366
Route: 805
Allocation Amount: \$7,200,000 (PS&E: 6,450,000, and R/W Support: \$750,000)
Remaining Balance: \$2,800,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 06/29/2023
Allocation Resolution: LPP-A-2223-44
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

805/15 Transit Priority Connectors project

The Department requests a 12-month time extension for the period of project development expenditure for the Plans, Specifications, and Estimate (PS&E) phase of the 805/15 Transit Priority Connectors project. The Department experienced unexpected delays in completing the project's PS&E phase.

The project is located on Interstate 805 (I-805) at the I-805 and I-15 Transit Priority Connectors, in San Diego County. The project will construct transit connectors on I-15 from the I-805 and I-15 separation to north of the I-805 and I-15 separation and on the I-805 from Market Street to Landis Street.

During the PS&E phase, the construction funding was delayed. As a result, additional time is needed to continue the PS&E phase, which is currently on hold. The remaining funds are being reserved to update and finalize the PS&E package once construction funding is secured.

The Department anticipated to secure construction funding before the Ready to List milestone, as this project is a regional priority. However, the project did not receive the Solutions for Congested Corridors Program funding that the Department and Region applied for. The Department now anticipates identifying new funding sources by the end of 2025, which will allow the PS&E phase to be completed by June 2026. There are no impacts anticipated to the future phases.

Therefore, the Department is requesting a 12-month time extension for the period of project development expenditure to allow enough time to finalize the PS&E phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(9), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR A
LOCALLY-ADMINISTERED MULTI-FUNDED SENATE BILL 1 LOCAL
PARTNERSHIP PROGRAM (FORMULAIC) AND SENATE BILL 1 SOLUTIONS FOR
CONGESTED CORRIDORS PROGRAM PROJECT, OFF THE STATE HIGHWAY
SYSTEM, PER LPP (FORMULAIC) AND SCCP GUIDELINES
WAIVER 25-113**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the locally-administered multi-funded Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) and SB 1 Solutions for Congested Corridors Program (SCCP) Mid County Parkway: Ramona Expressway project (PPNO 1276), in Riverside County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for the locally-administered multi-funded SB 1 LPP (Formulaic) and SB 1 SCCP Mid County Parkway: Ramona Expressway project (PPNO 1276), in Riverside County, for the time period as identified in the attachment.

BACKGROUND:

The local agency will not be able to request allocation for funds by the June 30, 2025, deadline. The attachment shows the details of the project and the explanation for the delays. The respective agency requests an extension, and the regional planning agency concurs.

Current SB 1 LPP (Formulaic) and SB 1 SCCP Guidelines adopted by the Commission stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the Timely Use of Funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Allocation
Multi-Funded SB 1 LPP (Formulaic) and SB 1 SCCP**

Project Number: 1
Applicant: Riverside County Transportation Commission
County: Riverside
District: 8
PPNO: 1276
Allocation Amount: \$63,275,000 (LPP (Formulaic): \$18,775,000 and SCCP: \$44,500,000)
Phase: Construction
Number of Months Requested: 2 Months
Extended Deadline: 08/31/2025
Department Recommendation: Support

Mid County Parkway: Ramona Expressway project

The Riverside County Transportation Commission (Agency) requests a two-month time extension for the period of project allocation for the Construction (CON) phase of the Mid County Parkway: Ramona Expressway project. The Agency is experiencing delays with the Right of Way (RW) Certification.

The project proposes to construct two additional lanes on the Model Context Protocol alignment, with bike lanes in each direction, a new bridge, and implement safety improvements, including the addition of a raised median with delineators, modifications to three signalized intersections, and a new wildlife crossing.

The Agency struggled to coordinate timely with the Eastern Municipal Water District (EMWD) to get feedback on the utility agreements and plans which delayed the RW Certification delivery. This set back the project timeline by one month. The EMWD Committee approved the agreements on March 27, 2025, followed by final approval from the EMWD Board on April 7, 2025. The Agency's approval was granted at the RCTC meeting on April 9, 2025.

The Agency, EMWD, respective Boards, and legal teams have approved the language in the utility agreements. The Department reviewed the draft RW Certification and noted non-standard language was included in the utility agreements. The use of non-standard clauses requires further review and approval from the local district utility coordinator and the Department. This RW Certification review is expected to take 45 days, which is the standard timeframe for RW Certification, further delaying the process.

The Agency seeks approval to use the non-standard clauses, asserting the clauses do not pose any risk to the Department's policies and procedures, nor create any financial liability for the Department or the Federal Highway Administration, as no federal or state funds will reimburse EMWD. The proposed clauses are general in nature and do not impact statewide

policies, thereby ensuring compliance. The Agency expects to receive RW Certification by mid-June 2025.

Therefore, the Agency requests a two-month time extension for the period of project allocation for the CON phase from June 30, 2025, to August 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(10), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR A
LOCALLY-ADMINISTERED MULTI-FUNDED SENATE BILL 1 SOLUTIONS FOR
CONGESTED CORRIDORS PROGRAM AND STATE TRANSPORTATION
IMPROVEMENT PROGRAM RAIL PROJECT, PER SCCP AND STIP
GUIDELINES
WAIVER 25-114

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the locally-administered multi-funded Senate Bill 1 (SB1) Solutions for Congested Corridors Program (SCCP) and State Transportation Improvement Program (STIP) Leesdale Passing Siding Rail project (PPNO 9887), in Ventura County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of project allocation for the locally-administered multi-funded SB 1 SCCP and STIP Leesdale Passing Siding Rail project (PPNO 9887), in Ventura County, for the time period as identified in the attachment.

BACKGROUND:

The Los Angeles – San Diego – San Luis Obispo (LOSSAN) Rail Corridor Agency will not be able to request an allocation of programmed funds by the June 30, 2025, deadline. The attachment shows the details of the project and the explanation for the delays.

Current STIP Guidelines stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the STIP Timely Use of Funds (TUF) deadline, one time only, for up to 20 months.

“Improving lives and communities through transportation.”

Current SB1 SCCP Guidelines stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the SB 1 SCCP TUF deadline, one time only, for up to 12 months. The agency requests a one-time exception to SB1 SCCP TUF Policy for an additional eight months, for a total of 20 months for project allocation.

Attachment

**Time Extension/Waiver – Project Allocation
Multi-Funded SB 1 SCCP and STIP**

Project Number: 1

Applicant: Los Angeles – San Diego – San Luis Obispo (LOSSAN) Rail Corridor Agency

County: Ventura

District: 75

PPNO: 9887

Allocation Amount: \$63,500,000 (SCCP \$43,500,000 STIP \$20,000,000)

Phase: Construction

Number of Months Requested: 20 Months

Extended Deadline: 02/28/2027

Department Recommendation: Support

Leesdale Passing Siding

The Los Angeles – San Diego – San Luis Obispo (LOSSAN) Rail Corridor Agency (Agency) requests a 20-month time extension for the period of project allocation for the Construction (CON) phase of the Leesdale Passing Siding Rail project. The Agency experienced delays during the Plans, Specifications, and Estimate (PS&E) phase.

The project will upgrade and extend existing siding to eliminate the need for passenger trains to wait on a regular basis at the Oxnard station. This will also serve future needs to expand the Oxnard station to two platforms.

The project is experiencing significant delays in the Plans, Specifications, and Estimates (PS&E) phase due to coordination required with Union Pacific Railroad (UPRR) and the California Public Utilities Commission (CPUC). Currently, the PS&E phase is approximately 30 percent complete, with both structural and non-structural plans submitted to UPRR for review. Although the Agency anticipated completion of the PS&E phase by December 2026, there are additional delays and challenges associated with the project:

UPRR:

- Right-of-Entry Permit: Although some of the permits were processed concurrently each of the four necessary permits exceeded initial projections by an average of four months, resulting in significant delays to critical project activities (e.g., survey, utility potholing, geotechnical borings, and environmental investigations). As a result, the cumulative impact on the project schedule was approximately eight months.
- Design Review: UPRR's design and resubmittal review periods have been significantly longer compared to past projects, an additional two to four months at each milestone.

One reason for these delays is the increased number of projects UPRR must review in a short period. Additionally, UPRR updated its design review process and limited communications to email-only to review and resolve comments without meetings. This new process is further slowing the review process because questions must be in writing and cannot be sent directly to the reviewer. Which is a new process within the last several years. The questions go into a queue, then to the reviewer. This can take one to two weeks; the reviewer can take several weeks to respond. If there is a follow-up question, the process starts over. If it is still unresolved, then a meeting is scheduled, and it can take several weeks before the meeting happens. Previously a phone call with the reviewer could be done to resolve the comment on the same day. The UPRR design approval delays have impacted the timely initiation of the CPUC review process.

- Aging and Non-Standard Existing Infrastructure: UPRR's current standards are more stringent than when the existing infrastructure was built. For instance, track separation has increased from 15 to 20 feet, and bridge clearance requirements above high water marks have also increased. These changes created significant design challenges for new infrastructure adjacent to the existing facilities and required prolonged negotiations with UPRR to obtain approval for variances. The project includes three new bridges and nine culvert extensions, adding an additional two to four months to the project schedule, due to coordination with UPRR and the local flood control district.
- Potential Right of Way (ROW) Encroachment: Survey mapping revealed a wrought iron fence encroached up to 10 feet into the railroad right-of-way near Rice Avenue, creating design and real estate issues to delay the project. Resolving this encroachment issue will require complex coordination with UPRR's Real Estate and other departments, potentially adding six to nine months to the schedule. The encroachment permit issue also affects the planned fiber optic relocation, as minimum clearance requirements must be maintained between the railroad tracks and the fence. Lumen, the fiber optics owner, reports that special design considerations and variances will be needed to relocate the existing fiber optics lines, potentially adding another three to four months to the project schedule.

CPUC

- Field diagnostics are essential for at-grade crossing design and safety measures. The CPUC field diagnostic meeting is postponed due to pending design revisions and necessary updates to the diagnostic exhibits. Once these exhibits are finalized and approved by UPRR, the meeting with CPUC can be rescheduled within one to two months. However, given the number of essential stakeholders and staff whose input is critical, coordinating this meeting requires lead time. In a rapid-response situation, this can create scheduling challenges beyond the Agency's control, which may introduce additional delays.

OTHERS

- **Rice Avenue Grade Separation Project Coordination Impacts:** The Rice Avenue Grade Separation project, located within the same corridor, has presented unanticipated challenges involving multiple stakeholders including the City of Oxnard, County of Ventura, Flood Control District, private utilities including Lumen and PG&E, and the CPUC. In addition, unexpected design conflicts have emerged related to the installation of a new water line and modifications to the existing fiber optic infrastructure, requiring additional coordination, plan reviews, and approvals from the various stakeholders. While the exact impact on the schedule is difficult to quantify, this added complexity demands significant attention and critical resources, ultimately slowing the overall design progress and will result in a delay.
- **Wood Road Crossing Vehicle Safety Concerns:** Safety issues were identified during preliminary design at the Wood Road at-grade crossing, where the steep grade poses a risk to low-profile vehicles getting high-centered on the main line and a siding track. This required a full redesign, including removing the existing siding—originally planned for refurbishment—and reconfiguring the project's east-end termination. These changes will add at least one to two months to the project schedule.

Despite these setbacks, design consultant Zephyr Rail is moving ahead with tasks not dependent on UPRR or CPUC, using parallel processing to maintain schedule momentum.

At present, the Agency still has resubmittals to the UPRR and the other stakeholders for 30 percent and regular submittals at 60 percent, 90 percent, and 100 percent design.

The following is a summary of the expected delays explained above:

- Right of Entry Permits to perform field work – 8 months
- UPRR Design Reviews – 8 months
 - 10 percent submittal – 2 months
 - 25 percent submittal – 2 months
 - 30 percent submittal – 2 months
 - 60 & 90 percent submittals – 2 months; these could be reviewed concurrently
 - 100 percent submittal – Typically no formal review is required
- Wood Road re-design, siding and siding termination redesign – 2 months
- Rice Avenue Grade Separation resolving design conflict with utilities, potential CPUC meeting delays, and other potential delays with aging infrastructure and ROW encroachments – 2 months (this 2-month estimation is based on running the individual activities in parallel with other activities).

Combined, these amount to approximately 20 months of expected delays. UPRR has directed Zephyr to review the schedule and incorporate these delays, along with mitigation strategies, to minimize overall impacts to the project schedule.

Therefore, the Agency requests a 20-month time extension for the period of project allocation for the CON phase from June 30, 2025 to February 28, 2027.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8c.(3), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT COMPLETION FOR A STATE-ADMINISTERED MULTI-FUNDED SENATE BILL 1 SOLUTIONS FOR CONGESTED CORRIDORS PROGRAM AND STATE TRANSPORTATION IMPROVEMENT PROGRAM PROJECT, ON THE STATE HIGHWAY SYSTEM, PER SCCP AND STIP GUIDELINES**
WAIVER 25-115

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project completion for the State-Administered multi-funded Senate Bill 1 (SB 1) Solutions for Congested Corridors Program (SCCP) and State Transportation Improvement Program (STIP) South Coast 101 HOV-Padaro (Segment 4B) project (PPNO 7101D), in Santa Barbara County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project completion for the State-Administered multi-funded SB 1 SCCP and STIP, South Coast 101 HOV-Padaro (Segment 4B) project (PPNO 7101D), in Santa Barbara County, for the time period as identified in the attachment.

BACKGROUND:

In May 2021, the Commission allocated \$67,520,000 in Construction Capital and Support for the State-Administered multi-funded SB 1 SCCP and STIP South Coast 101 HOV-Padaro (Segment 4B) project (PPNO 7101D). The project was awarded in May 2021. In accordance with the SB 1 SCCP and STIP Guidelines, the deadline to complete Construction Contract Acceptance for projects awarded in May 2021 is May 31, 2024. At the time of allocation, the project received an additional 14 months beyond the 36 months extending the deadline to July 31, 2025. Because the Department will not meet this deadline, the Department is requesting a time extension to complete construction. The attachment shows the details of the project and the delays that have resulted in the time extension request.

Current STIP and SB 1 SCCP Guidelines stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the STIP and SB 1 SCCP Timely Use of Funds deadline, one time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Project Completion for
Multi-funded SB 1 SCCP and STIP**

Project Number: 1
Applicant: Department of Transportation
County: Santa Barbara
District: 5
PPNO: 7101D
EA: 0N702
Route: 101
Allocation Amount: \$67,520,000 (SCCP: \$63,310,000 and STIP: \$4,210,000)
Phase: Construction
Allocation Date: 05/13/2021
Allocation Resolution: SCCP-A-2021-05 and FP-20-80
Award Date: 05/26/2021
Number of Months Requested: 5 Months
Extended Deadline: 12/31/2025
Department Recommendation: Support

South Coast 101 HOV-Padaro (Segment 4B) project

The Department requests a five-month time extension to complete the Construction (CON) phase of the South Coast 101 HOV-Padaro (Segment 4B) project. The Department has experienced unexpected delays in completing construction. At the time of allocation, the project received an additional 14 months beyond the 36 months extending the deadline to July 31, 2025. This project is combined with State Highway Operation and Protection Program project EA 1C822 for construction under EA 0N72U.

The project is located on United States Highway (US) 101, in Santa Barbara County, near Carpinteria and Summerland from south of Padaro Lane Undercrossing to north of Padaro Lane Overcrossing. The project will construct high occupancy vehicle lanes.

In January 2025, project construction revealed a safety risk where vehicles on Frontage Road Via Real may run off the road and into northbound US 101 lanes. A barrier is needed for traveler safety, but underground utilities complicate permanent installation and would require approvals from multiple utility companies. The Department considered installing a temporary barrier as it works on a permanent solution with the utility companies. The County of Santa Barbara (County) approved temporary k-rail placement and end treatment measures along Frontage Road Via Real. This temporary solution will remain until the County can relocate utilities to allow installation of a permanent barrier.

Additionally, homeowner associations raised concerns about noise from the existing rumble strip. To mitigate noise, the Department replaced it with a quieter sinusoidal strip which is positioned three feet farther from the road. These adjustments caused a 5-month project delay. Completion is now expected by December 2025. There is a concurrent project completion time extension for the SHOPP project EA 1C822 on this month's Commission agenda.

Therefore, the Department is requesting a 5-month time extension to allow sufficient time to reach Construction Contract Acceptance from July 31, 2025 to December 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(11), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR A
LOCALLY-ADMINISTERED MULTI-FUNDED SENATE BILL 1 SOLUTIONS FOR
CONGESTED CORRIDORS PROGRAM AND ACTIVE TRANSPORTATION
PROGRAM PROJECT, ON THE STATE HIGHWAY SYSTEM,
PER SCCP AND ATP GUIDELINES
WAIVER 25-11

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the locally-administered multi-funded Senate Bill 1 (SB 1) Solutions for Congested Corridors Program (SCCP) and Active Transportation Program (ATP) East Bay Greenway Multimodal, North Segment Project Phase 1 (PPNO 2364), in Alameda County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for the locally-administered multi-funded SB 1 SCCP and ATP East Bay Greenway Multimodal, North Segment Project Phase 1 (PPNO 2364), in Alameda County, for the time period as identified in the attachment.

BACKGROUND:

The local agency will not be able to request allocation for funds by the June 30, 2025, deadline. The attachments show the details for the project and the explanation for the delays. The respective agency is requesting an extension, and the regional planning agency concurs.

Current SB 1 SCCP Guidelines stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the SCCP Timely Use of Funds deadline, one-time only, for up to 12 months.

Current SB 1 ATP Guidelines stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the ATP Timely Use of Funds deadline, one-time only, for up to 20 months.

Attachment

**Time Extension/Waiver – Project Allocation
Multi-Funded SB 1 SCCP and ATP**

Project Number: 1
Applicant: Alameda County Transportation Commission
County: Alameda
District: 4
PPNO: 2364
Allocation Amount: \$58,875,000 (SCCP: \$39,375,000 and ATP: \$19,500,000)
Phase: Construction
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

East Bay Greenway Multimodal, North Segment Project Phase 1

The Alameda County Transportation Commission (ACTC) requests a 12-month time extension for the period of project allocation for the Construction (CON) phase of the East Bay Greenway Multimodal - North Segment Project Phase 1. ACTC is experiencing delays with environmental clearance.

The project proposes to construct Class I shared use paths, Class IV protected bikeways, protected intersection treatments, and make improvements for pedestrian crossing safety, accessibility, and bus stop enhancements to improve speed and passenger comfort through the Cities of Oakland and San Leandro.

The project is experiencing delays with the National Environmental Policy Act (NEPA) clearance, specifically with the Extended Phase 1 archaeological testing requirement. This testing requires additional local funds for the Project Approval and Environmental Document phase, consultant agreement amendments, extended coordination with the Department's Division of Local Assistance to develop a testing plan, and city permits before ACTC could begin to test and document findings. NEPA certification delays also stall Right of Way activities. NEPA clearance is anticipated in November 2025. The California Environmental Quality Act clearance is complete. ACTC has continued to advanced project design, plans are currently at 65 percent and it is anticipated to be at 95 percent in June 2025.

Therefore, the ACTC request a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(12), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR
LOCALLY-ADMINISTERED SENATE BILL 1 SOLUTIONS FOR CONGESTED
CORRIDORS PROGRAM TRANSIT COMPONENTS, PER SCCP GUIDELINES
WAIVER 25-117**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for two locally-administered Senate Bill 1 (SB 1) Solutions for Congested Corridors Program (SCCP) Transit components, in Los Angeles County, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of project allocation for two locally-administered SB 1 SCCP Transit components, in Los Angeles County, for the time periods as identified in the attachment.

BACKGROUND:

The Los Angeles County Metropolitan Transportation Authority will not be able to request an allocation for funds by the June 30, 2025, deadline. The attachment shows the details of the components and the explanation for the delay.

Current SB 1 SCCP Guidelines stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the Timely Use of Funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Allocation
Solutions for Congested Corridors Program**

Project Number: 1
Applicant: Los Angeles County Metropolitan Transportation Authority
County: Los Angeles
District: 7
PPNO: 6203
Allocation Amount: \$5,320,000
Phase: Construction
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Venice Blvd. Bus Speed and Reliability (Bus Boarding Islands and Bus Pads)

The Los Angeles County Metropolitan Transportation Authority (Metro) requests a 12-month time extension for the period of project allocation for the Construction (CON) phase of the Venice Blvd. Bus Speed and Reliability (Bus Boarding Islands and Bus Pads) component. Metro has experienced delays in completing the design phase.

In June 2023, the Commission adopted the project to improve bus service in the Interstate 405 (I-405) community corridor. As one of four components awarded, the Bus Boarding Islands and Bus Pads component will support Metro and the State's goals and policies to reduce greenhouse gas emissions, increase transit ridership, integrate non-vehicle modes, and increase safety by constructing 38 bus boarding islands and bus pads along Venice Boulevard between Fairfax Avenue and Inglewood Boulevard.

Metro has experienced delays in completing the design phase for this component due to limitations in staffing resources, which temporarily slowed progress on key project activities. In December 2022, Metro determined the relatively new Speed and Reliability Tactical Transit Engineering Department (SRTTED) would deliver this project, among other high priority initiatives at Metro. Initially, the SRTTED was staffed with one individual with the intention of adding additional staff as needed. When the project was programmed in June 2023 the process of staffing up had already begun, and Metro anticipated the SRTTED would be fully staffed by the construction allocation deadline and able to meet the original project schedule. Due to lack of qualified candidates after several rounds of interviews during the hiring process, the schedule was delayed by 14 months, until Metro was able to hire a qualified candidate.

Since onboarding staff in August 2024, SRTTED has been able to make up for approximately two months of the delay by completing briefing presentations to relevant City of Los Angeles agencies including the Los Angeles Department of Transportation, overseeing geometric design, vehicle operations and lane configurations, and Los Angeles Public Works Bureau of

Engineering, overseeing civil design, utilities and permits. Further SRTTED developed high-level conceptual designs for all 38 bus boarding island locations in coordination with Metro Operations and Service Planning and Stops and Zones, to ensure the conceptual design meets the standards for bus operations. To assist with the Plans, Specifications, and Estimate (PS&E) phase a consultant will be utilized. The goal was to do all the necessary internal background work so once a design firm is chosen, notice to proceed can immediately follow.

Metro is on schedule to begin PS&E in August 2025. Design firms have been solicited and interviews are currently being conducted. Once a firm is selected, Metro anticipates PS&E completion by the end of March 2026, and request the CON allocation by June 2026. The ensuing CON phase scheduled to begin with a contractor onboard by the end of September 2026. Metro is working in good faith to deliver this project by June 2028 and has already taken concrete steps toward that end. While the 14-month delay was unfortunate and unforeseen due to a lack of qualified candidates, Metro has done their best to get the project back on schedule.

Therefore, Metro requests a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

Project Number: 2
Applicant: Los Angeles County Metropolitan Transportation Authority
County: Los Angeles
District: 7
PPNO: 6204
Allocation Amount: \$1,173,000
Phase: Construction
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Venice Blvd. Bus Speed and Reliability (Real-Time Passenger Info and Bus Shelters)

The Los Angeles County Metropolitan Transportation Authority (Metro) requests a 12-month time extension for the period of project allocation for the CON phase of the Venice Blvd. Bus Speed and Reliability (Real-Time Passenger Info and Bus Shelters) component. Metro has experienced delays in completing for the design phase.

In June 2023, the Commission adopted the project to improve bus service in the I-405 community corridor. As one of four components awarded, the Real-Time Passenger Info and Bus Shelters component will support Metro and the State's goals and policies to reduce greenhouse gas emissions, increase transit ridership, integrate non-vehicle modes and increase safety by constructing 27 bus shelters equipped with real-time information displays along Venice Boulevard between Fairfax Avenue and Inglewood Boulevard. Many of the bus shelters and real-time information displays share the same location where new bus boarding islands and bus pads will be constructed, and in those cases the shelters and information displays' CON will begin upon completion of the boarding islands and bus pads.

Metro has experienced delays in completing the design phase for this component due to limitations in staffing resources, which temporarily slowed progress on key project activities. In December 2022, Metro determined the relatively new SRTTED would deliver this project, among other high priority initiatives at Metro. Initially, the SRTTED was staffed with one individual with the intention of adding additional staff as needed. When the project was programmed in June 2023 the process of staffing up had already begun, and Metro anticipated the SRTTED would be fully staffed by the construction allocation deadline and able to meet the original project schedule. Due to lack of qualified candidates after several rounds of interviews during the hiring process, the schedule was delayed by 14 months, until Metro was able to hire a qualified candidate.

Since onboarding staff in August 2024, SRTTED has been able to make up for approximately two months of the delay by completing briefing presentations to relevant City of Los Angeles agencies including the Los Angeles Department of Transportation, overseeing geometric

design, vehicle operations and lane configurations, and Los Angeles Public Works Bureau of Engineering, overseeing civil design, utilities and permits. Further SRTTED developed high-level conceptual designs for all 38 bus boarding island locations in coordination with Metro Operations and Service Planning and Stops and Zones, to ensure the conceptual design meets the standards for bus operations. To assist with the PS&E phase a consultant will be utilized. The goal was to do all the necessary internal background work so once a design firm is chosen, notice to proceed can immediately follow.

Metro is on schedule to begin PS&E in August 2025. Design firms have been solicited and interviews are currently being conducted. Once a firm is selected, Metro anticipates PS&E completion by the end of March 2026, and request the CON allocation by June 2026. The ensuing CON phase scheduled to begin with a contractor onboard by the end of September 2026. Metro is working in good faith to deliver this project by June 2028 and has already taken concrete steps toward that end. While the 14-month delay was unfortunate and unforeseen due to a lack of qualified candidates, Metro has done their best to get the project back on schedule. As Metro works through the procurement process for PS&E phase consultants, it is also in the process of developing a Memorandum of Understanding with the City of Los Angeles to inform the construction contract for the Real-Time Passenger Info and Bus Shelters component.

Approval of this request for project allocation time extension will ensure that Metro is able to maximize the impact of the Solutions for Congested Corridors Program award by allowing them to follow a structured and effective timeline while maintaining compliance with all necessary regulations and standards. CON for boarding islands, bus pads, bus shelters, and real-time displays are estimated for completion by the end of June 2028.

Therefore, Metro is requesting a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(13), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR A
LOCALLY-ADMINISTERED SENATE BILL 1 SOLUTIONS FOR CONGESTED
CORRIDORS PROGRAM PROJECT, OFF THE STATE HIGHWAY SYSTEM, PER
SCCP GUIDELINES
WAIVER 25-118**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the locally-administered Senate Bill 1 (SB 1) Solutions for Congested Corridors Program (SCCP) Light Rail Realignment (Sacramento Valley Light Rail Station Realignment) component (PPNO CP094H), in Sacramento County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for the locally-administered SB 1 SCCP Light Rail Realignment (Sacramento Valley Light Rail Station Realignment) component (PPNO CP094H), in Sacramento County, for the time period as identified in the attachment.

BACKGROUND:

In June 2023, the Commission programmed \$25,000,000 for the locally-administered SB 1 SCCP Light Rail Realignment (Sacramento Valley Light Rail Station Realignment) component (PPNO CP094H). The responsible agency will not be able to request an allocation for funds by the June 30, 2025 deadline. The attachment describes the details of the project and the explanation for the delays.

Current SB 1 SCCP Guidelines adopted by the Commission stipulate that programmed funds are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the Timely Use of Funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Allocation
Solutions for Congested Corridors Program**

Project Number: 1
Applicant: Sacramento Regional Transit District
County: Sacramento
District: 3
PPNO: CP094H
Allocation Amount: \$25,000,000
Phase: Construction
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

Light Rail Realignment (Sacramento Valley Light Rail Station Realignment)

The Sacramento Regional Transit District (SacRT) requests a 20-month time extension for the period of project allocation for the Construction (CON) phase of the Light Rail Realignment (Sacramento Valley Light Rail Station Realignment) component. SacRT has experienced unexpected delays during the design of the project.

The project proposes to relocate the existing east-west oriented SacRT Sacramento Valley Station on H Street in Sacramento, to a new north-south oriented station west of 5th Street, add double tracking, removed existing storage track, and add storage track north of the new station. The relocation is due to conflict with the City of Sacramento's proposed drain facilities, a transit pick up and drop off site, and a bike pathway. Additionally, the relocation is also needed for the proposed expansion of light rail service to the Sacramento International Airport.

The project experienced unexpected delays due to the City of Sacramento changing their Sacramento Valley Station master plan, which resulted in SacRT needing to complete a California Environmental Quality Act (CEQA) addendum to their previously approved Initial Study and Mitigated Negative Declaration. Additionally, more time was needed to complete the CEQA addendum due to recent CEQA updates that required engagement and consultation regarding tribal resources. This CEQA addendum work took an additional six months. The CEQA addendum was adopted in June 2024. SacRT then proceeded with the Request for Proposal to award a design contract for the project. Extended negotiations due to required financial review, and discussions over awardee's proposed exceptions to SacRT's contract terms resulted in a four month delay that pushed contract award, execution and notice to proceed to November 2024. Initially, the design contractor estimated that the design would take 18 months. During design, the design contractor informed SacRT that an additional two months would be required to complete the design due to extensive coordination with the City of Sacramento, which was not initially anticipated by SacRT. Lastly, in March 2025 the Shingle Springs Band of Miwok Indians requested archaeological presence during the design

work to locate underground utilities, in addition to the anticipated archeological presence during the CON phase. SacRT anticipates this to add six months to the design timeline. SacRT is committed to meeting the February 28, 2027 timeline to complete all pre-construction phases and request an allocation for the CON phase for the Light Rail Realignment (Sacramento Valley Light Rail Station Realignment) component.

Therefore, SacRT is requesting a 20-month time extension for the period of project allocation for the CON phase from June 30, 2025 to February 28, 2027.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(14), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR A
STATE-ADMINISTERED MULTI-FUNDED SENATE BILL 1 TRADE
CORRIDOR ENHANCEMENT PROGRAM PROJECT AND STATE
TRANSPORTATION IMPROVEMENT PROGRAM, ON THE STATE HIGHWAY
SYSTEM, PER TCEP AND STIP GUIDELINES
WAIVER 25-119

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the State-Administered multi-funded Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) and State Transportation Improvement Program (STIP) RT 49 Corridor Improvements, La Barr–McKnight, SB Truck Climbing Lane project (PPNO 4117), in Nevada County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for the State-Administered multi-funded SB 1 TCEP and STIP RT 49 Corridor Improvements, La Barr–McKnight, SB Truck Climbing Lane project (PPNO 4117), in Nevada County, for the time period as identified in the attachment.

BACKGROUND:

In March 2024, the Commission programmed \$21,000,000 in Construction Capital and Support for the State-Administered multi-funded SB1 TCEP and STIP RT 49 Corridor Improvements, La Barr–McKnight, SB Truck Climbing Lane project (PPNO 4117). The Department will not be able to request an allocation for funds by the June 30, 2025 deadline. The attachment describes the details of the project and explanation for the delays.

Current SB 1 TCEP and STIP Guidelines adopted by the Commission stipulate that funds programmed for all components are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months for SB 1 TCEP and 20 months for STIP.

Attachment

**Time Extension/Waiver – Project Allocation
Multi-Funded SB 1 TCEP and STIP**

Project Number: 1
Applicant: Department of Transportation
County: Nevada
District: 3
PPNO: 4117
Allocation Amount: \$21,000,000 (TCEP: \$14,615,000 and STIP: \$6,385,000)
Phase: Construction and Construction Support
Number of Months Requested: 6 Months
Extended Deadline: 12/31/2025
Department Recommendation: Support

RT 49 Corridor Improvements, La Barr–McKnight, SB Truck Climbing Lane project

The Department requests a 6-month time extension for the period of project allocation for the Construction (CON) phase of the RT 49 Corridor Improvements, La Barr–McKnight, SB Truck Climbing Lane project. The Department has experienced unexpected delays in completing this project's Ready to List (RTL) Package.

The RT 49 Corridor Improvements, La Barr–McKnight, SB Truck Climbing Lane project proposes improvements from Post Mile R10.8 to R13.3 on SR 49. The project focuses on goods movement, safety, operations, evacuation egress, reducing freight-related emissions in adjacent Assembly Bill 1550 communities, minimizing community impacts related to Interstate 80 detours, providing adequate access to Zero-Emission Bus transit infrastructure, and supporting multimodal mobility.

Two Resolutions of Necessity (RON) determinations were approved at the December 2024 Commission meeting, and a final RON package recommended at the May 2025 Commission meeting. The RON package included a longer right-of-way acquisition period due to the parcel owners' limited availability stemming from their restricted circumstances, which delayed the RON submittal until the May 2025 Commission meeting. These RON packages have, in turn, delayed final utility relocation plan approvals and execution of utility agreements. As a result, submission of the Certification 3W memorandum for Department Headquarters and Federal Highway Administration review and approvals has been delayed. These factors have pushed the project's Ready to List (RTL) date to early October 2025.

Therefore, the Department is requesting a 6-month time extension for the period of project allocation for the CON phase from June 30, 2025 to December 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(15), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR A
STATE-ADMINISTERED MULTI-FUNDED SENATE BILL 1 TRADE CORRIDOR
ENHANCEMENT PROGRAM AND STATE TRANSPORTATION IMPROVEMENT
PROGRAM PROJECT, ON THE STATE HIGHWAY SYSTEM,
PER TCEP AND STIP GUIDELINES
WAIVER 25-120

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the State-Administered multi-funded Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) and State Transportation Improvement Program (STIP) Fix 5 Cascade Gateway / I-5 Improvements Shasta project (PPNO 3597), in Shasta and Tehama Counties, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for the State-Administered multi-funded SB 1 TCEP and STIP Fix 5 Cascade Gateway / I-5 Improvements Shasta project (PPNO 3597), in Shasta and Tehama Counties, for the time period as identified in the attachment.

BACKGROUND:

The Department will not be able to request allocation for funds by the June 30, 2025, deadline. The attachment shows the details of the project and the explanation for the delays that have resulted in this time extension request.

Current SB 1 TCEP and STIP Guidelines adopted by the Commission stipulate that funds programmed for all components of local grant projects are available for allocation only until the end of the fiscal year programmed. The Commission may approve waivers to the Timely Use of Funds deadline, one-time only, for up to 12 months for SB 1 TCEP, and 20 months for STIP.

Attachment

**Time Extension/Waiver – Project Allocation
Multi-Funded SB 1 TCEP and STIP**

Project Number: 1
Applicant: Department of Transportation
County: Shasta and Tehama
District: 2
PPNO: 3597
Allocation Amount: \$75,480,000 (TCEP: \$70,399,000 and STIP: \$5,081,000)
Phase: Construction and Construction Support
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

Fix 5 Cascade Gateway / I-5 Improvements Shasta

The Department requests a 20-month time extension for the period of project allocation for the Construction (CON) phase of the Fix 5 Cascade Gateway / I-5 Improvements Shasta project. The Department is experiencing delays with the design phase and environmental clearance.

The project proposes to add a third through lane in the median as a “truck only” lane in each direction, construct four auxiliary lanes, widen seven bridges, install an Advanced Lane Management System in the northbound direction including three overhead sign structures, and install two 350 kw Electric Vehicle Chargers for trucks and other vehicles.

On April 1, 2025, during the design phase review, the Federal Highway Administration (FHWA) requested modifications to the “truck-only” lane strategy. The new managed lane strategy requested by FHWA requires updates to several key deliverables including traffic modeling to proceed with traffic operations and safety requirements. The new managed lane strategy will be a “managed lane for part-time truck only operation”. In addition, the requested changes to the project require amendments to multiple documents such as environmental documentation, estimate certification, and specifications to reflect revised modeling and the new implementation strategy that cannot be completed by end of the fiscal year. After traffic modeling outcomes, the Initial Study may elevate to an Environmental Impact Report, requiring up to 20 months to complete. This work will proceed concurrently with other updates to project documentation.

Therefore, the Department is requesting a 20-month time extension for the period of project allocation for the CON phase from June 30, 2025 to February 28, 2027.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(16), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR
A STATE-ADMINISTERED MULTI-FUNDED SENATE BILL 1 TRADE CORRIDOR
ENHANCEMENT PROGRAM AND STATE TRANSPORTATION IMPROVEMENT
PROGRAM PROJECT, ON THE STATE HIGHWAY SYSTEM, PER TCEP AND
STIP GUIDELINES
WAIVER 25-121**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for the State-Administered multi-funded Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) and State Transportation Improvement Program (STIP) Five Cities Multimodal Transportation Network Enhancement Project (PPNO 2653), in San Luis Obispo County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for the State-Administered multi-funded SB 1 TCEP and STIP Five Cities Multimodal Transportation Network Enhancement Project (PPNO 2653), in San Luis Obispo County, for the time period as identified in the attachment.

BACKGROUND:

In March 2018 and June 2023, the Commission programmed a combined total of \$62,350,000 in Construction Support and Capital for the State-Administered multi-funded Five Cities Multimodal Transportation Network Enhancement Project (PPNO 2653). The Department will not be able to request allocation for funds by the June 30, 2025 deadline. The attachment shows the details of the project and the explanation for the delays.

Current SB 1 TCEP and STIP Guidelines adopted by the Commission stipulate that funds programmed for all components are available for allocation only until the end of the fiscal year

programmed. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months for SB 1 TCEP and 20 months for STIP.

Attachment

**Time Extension/Waiver – Project Allocation
Multi-Funded SB 1 TCEP and STIP**

Project Number: 1
Applicant: Department of Transportation
County: San Luis Obispo
District: 5
PPNO: 2653
Allocation Amount: \$62,350,000 (TCEP: \$61,294,000 and STIP: \$1,056,000)
Phase: Construction Support and Construction Capital
Number of Months Requested: 12 Months
Extended Deadline: 6/30/2026
Department Recommendation: Support

Five Cities Multimodal Transportation Network Enhancement Project

The Department requests a 12-month time extension for the period of project allocation for the Construction (CON) phase of the Five Cities Multimodal Transportation Network Enhancement Project. The Department has experienced delays in completing this project's Plans, Specifications, and Estimates (PS&E) phase.

The project is located in San Luis Obispo County, in and near the City of Pismo Beach, extending from 0.2 mile south of the Pismo Overhead to 0.2 mile north of the North Avila Road Overcrossing. The project proposes to convert the existing truck climbing lane into a number 2 lane, replace the inside shoulder with a part-time travel lane, and construct a park-and-ride lot equipped with Electric Vehicle charging stations.

This project was initially scheduled to receive its CON phase allocation at the June 2025 Commission meeting. However, additional time is needed to incorporate refinements identified during the Cost Estimate Certification process. The District Office of Cost Estimate Review (OCER) identified additional refinements needed in the stage construction details. These refinements required the inclusion of additional cross-sections, which led to updates to the Engineer's Estimate and the incorporation of ramp lane closure charts into the Standard Special Provisions. The District Design team is actively finalizing these updates for review by District OCER, as well as District Construction and Traffic Operations, to obtain Cost Estimate Certification and proceed to Ready to List status. Additionally, this project has experienced estimated cost increases due to anticipated rises in temporary traffic control, stage construction, traffic management, median barriers and components associated with the mobility hub. These cost updates necessitate additional time for comprehensive coordination with multiple funding partners to secure the additional funding.

Therefore, the Department is requesting a 12-month time extension for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(18), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR
STATE-ADMINISTERED SENATE BILL 1 TRADE CORRIDOR
ENHANCEMENT PROGRAM PROJECTS, OFF THE STATE HIGHWAY SYSTEM,
PER TCEP GUIDELINES
WAIVER 25-123**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for two State-Administered Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for two State-Administered SB 1 TCEP projects, for the time periods as identified in the attachment.

BACKGROUND:

The local agencies will not be able to request allocation for funds by the June 30, 2025, deadline. The attachment shows the details of the projects and the explanations for the delays. The respective agencies are requesting time extensions, and the regional planning agencies concur.

Current TCEP Guidelines adopted by the Commission, stipulate that funds programmed for all components of local grant projects are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the Timely Use of Funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Allocation
Trade Corridor Enhancement Program**

Project Number: 1
Applicant: Department of Transportation (Modified)
County: Kern
District: 6
PPNO: 6240A
Allocation Amount: \$4,683,000
Phase: Construction
Number of Months Requested: 4 Months
Extended Deadline: 10/31/2025
Department Recommendation: Support

EV Oasis South B - TA Wheeler Ridge project

The Department along with bp Products North America requests a 4-month time extension for the period of project allocation for the Construction (CON) phase of the EV Oasis South B - TA Wheeler Ridge project. The Department experienced delays due to supply chain constraints.

The project proposes to construct and install medium and heavy-duty electric chargers and infrastructure.

The project has faced delays due to unforeseen circumstances related to supply chain constraints affecting the delivery of the Megawatt Charging System (MCS) hardware. Several factors are impacting the delivery of the HYC1000 DC fast-charging equipment. As new equipment, it has high demand and limited supply, causing additional delays at the factory level. Updated lead times from the manufacturer indicate that the delivery of MCS units is not expected until August 2026. The hardware is essential to the site's high-power charging configuration and cannot be substituted without compromising the approved outcomes. In parallel, the Department needs additional time to complete the 100 percent Construction Documents (CD100), utility review and permitting, and MCS equipment delivery, all of which are necessary for obtaining utility approvals and proceeding with permitting and construction. These delays are directly affecting the construction timeline. There is a concurrent Project Scope Amendment on this month's Commission agenda.

Therefore, the Department along with bp Products North America are requesting a 4-month time extension for the period of project allocation for the CON phase from June 30, 2025 to October 31, 2025.

Project Number: 2
Applicant: Department of Transportation (Modified)
Counties: San Bernardino
District: 8
PPNO: 6199A
Allocation Amount: \$4,683,000
Phase: Construction
Number of Months Requested: 4 Months
Extended Deadline: 10/31/2025
Department Recommendation: Support

EV Oasis South A - Petro Ontario project

The Department along with bp Products North America requests a 4-month time extension for the period of project allocation for the CON phase of the EV Oasis South A - Petro Ontario project. The Department experienced unforeseen delays due to supply chain constraints.

The project proposes to construct and install medium and heavy-duty electric chargers and infrastructure for heavy-duty trucks in various locations.

The project has faced delays due to unforeseen circumstances related to supply chain constraints affecting the delivery of the MCS hardware. Several factors are impacting the delivery of the HYC1000 DC fast-charging equipment. As new equipment, it has high demand and limited supply, causing additional delays at the factory level. Updated lead times from the manufacturer indicate that the delivery of MCS units is not expected until August 2026. The hardware is essential to the site's high-power charging configuration and cannot be substituted without compromising the approved outcomes. In parallel, the Department needs additional time to complete CD100, utility review and permitting, and MCS equipment delivery, all of which are necessary for obtaining utility approvals and proceeding with permitting and construction. These delays are directly affecting the construction timeline. There is a concurrent Project Scope Amendment on this month's Commission agenda.

Therefore, the Department along with bp Products North America are requesting a 4-month time extension for the period of project allocation for the CON phase from June 30, 2025 to October 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(21), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR
LOCALLY-ADMINISTERED ACTIVE TRANSPORTATION PROGRAM PROJECTS,
OFF THE STATE HIGHWAY SYSTEM, PER ATP GUIDELINES
WAIVER 25-126**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for 44 locally-administered Active Transportation Program (ATP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of allocation for 44 locally-administered ATP projects, for the time periods as identified in the attachment.

BACKGROUND:

The local agencies will not be able to request allocation for funds by the June 30, 2025, deadline. The attachment shows the details of the projects and the explanations for the delays. The respective agencies are requesting an extension, and the regional planning agencies concur.

Current ATP Guidelines adopted by the Commission stipulate that funds programmed for all components of local grant projects are available for allocation only until the end of the fiscal year. The Commission may approve a waiver to the Timely Use of Funds deadline, one-time only, for up to 20 months.

Attachment

Time Extension/Waiver – Project Allocation Active Transportation Program

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
1	City of Redding	Victor Ave and Cypress Ave Active Transportation (VCAT) Project	02-2606A	CON	20	02/28/2027	\$7,735,000	The project has experienced significant delays with the environmental process and anticipated right of way acquisitions. There was an initial delay with the consultant selection due to the Disabled Business Enterprise requirement that resulted in a dispute and a reconsideration hearing. A consultant was selected in October 2022. In early 2023, field surveys, and archaeological/biological background research began. During National Environmental Policy Act (NEPA) evaluation, it was determined that coordination for historical significance would be required on several residential foundations and there was a late addition of a Phase I Environmental Site Assessment. The project was also further delayed due to revisions needed to the cultural studies required from a newly adopted federal Programmatic Agreement for Section 106. In addition to the environmental delays, the City needs to obtain four permanent acquisitions and 25 Temporary Construction Easements (TCE), and relocate overhead power and communication facilities. The City is requesting the maximum time allowed to finalize the environmental document and Right of Way (RW) Certification. There is a concurrent project allocation time extension request for the Construction Non-Infrastructure (CON-NI) phase under this same item on this month's Commission agenda.
2	City of Redding	Victor Ave and Cypress Ave Active Transportation (VCAT) Project	02-2606B	CON-NI	18	12/31/2026	\$87,000	The project has experienced delays significant delays with the environmental process and anticipated right of way acquisitions. These delays are preventing the City from requesting allocation for the Construction (CON) phase. The pedestrian and bicycle counts are to be performed before and after construction. These counts need to be performed within a reasonable amount of time prior to and after completion of the project construction. In addition, there are bicycle events planned that will begin once the facilities are in place for students and residents to learn how to use properly. There is a concurrent project allocation time extension request for the CON phase under this same item on this month's Commission agenda.
3	City of Redding	Butte Street Boogie Network Project	02-2629A	PS&E	20	02/28/2027	\$834,000	The City understood the property for the trail is City owned and there would not be any right of way issues. However, during the development of the project it was discovered that although the City has full ownership of the property, portions of the project are within the Benton Airport Layout Plan (ALP). Since the airport receives federal grants for their operations, any ground disturbance or alteration in the ALP requires Federal Aviation Administration (FAA) and NEPA review. In addition, coordination with FAA and NEPA review is done through a City airport consultant, and the City's engineering division has limited ability to control the work. The City is anticipating a lengthy delay and therefore is requesting the maximum allowable time.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
4	City of Redding	Victor Improvement Project	02-2632	PS&E	20	02/28/2027	\$1,045,000	The City is coordinating design with the separate adjacent ATP Victor Ave and Cypress Ave Active Transportation (VCAT) Project (PPNO 2606). The Victor Improvement Project (PPNO 2632) has encountered the same delays to environmental as the VCAT project (PPNO 2606), due to revisions needed to the cultural studies required from a newly adopted federal Programmatic Agreement for Section 106. The City has completed topographic survey for the project, as well as submitted the Preliminary Environmental Study (PES). City staff are currently finalizing the PES to address the Department's comments. The Request for Proposal (RFP) to select and contract with an environmental consultant is also in process for the environmental planning and permitting. At this time, the City estimates receiving NEPA clearance by December 2026. In addition, the City is preparing the project for 30 percent design plans in coordination with the VCAT Project (PPNO 2606). The City is requesting the additional time to receive NEPA clearance. The City is requesting the maximum time allowed in the event there are further unanticipated delays.
5	Sacramento County	Elkhorn Boulevard Complete Streets Project	03-1817A	PS&E	12	06/30/2026	\$427,000	In January 2025, the County was notified that an Initial Site Assessment (ISA) Technical Memo and an Aerially Deposited Lead report, per the PES, prior to the issuance of the NEPA Categorical Exclusion was needed. The County is in the process of executing a contract with a consultant to help prepare and complete the ISA Technical Memo. Once the consultant has been approved, the County will coordinate with the Planning and Environmental review team and the consultant. The County anticipates completion and receiving environmental clearance by June 2026. There is a concurrent project allocation time extension request for the RW phase under this same item on this month's Commission agenda.
6	Sacramento County	Elkhorn Boulevard Complete Streets Project	03-1817A	RW	12	06/30/2026	\$354,000	In January 2025, the County was notified that an ISA Technical Memo and an Aerially Deposited Lead report, per the PES, prior to the issuance of the NEPA Categorical Exclusion was needed. The County is in the process of executing a contract with a consultant to help prepare and complete the ISA Technical Memo. Once the consultant has been approved, the County will begin coordination with the Planning and Environmental review team and the consultant. The County anticipates completion and receiving environmental clearance by June 2026. Once environmental clearance has been approved the County will be able to proceed with the RW phase. There is a concurrent project allocation time extension request for the PS&E phase under this same item on this month's Commission agenda.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
7	City of Sacramento	Envision Broadway in Oak Park	03-2220	PS&E	12	06/30/2026	\$1,101,000	The project has experienced delays with environmental and stakeholder coordination that has led to additional evaluations. The City was delayed seven months in submitting the PES to the Department due to a Metropolitan Transportation Improvement Program modification prior to the Department's review. The PES was submitted to the Department in May 2024, and received approval in December 2024. As part of the multi-modal design process, the City worked with the Sacramento Regional Transit District to relocate bus stops to accommodate the design. It was determined the new bus stop locations required right of way acquisition. Additional time was needed to confirm the new bus stop locations and right of way needs, so that the environmental documents would appropriately capture the footprint of the project. The bus stop relocations have been finalized and the City is working on the right of way acquisition. The right of way acquisitions for the bus stops triggered the need for additional technical studies that were not originally scoped or anticipated. In addition, there were several design elements which required coordination with community organizations to address pedestrian safety concerns. The City anticipates resolving these delays by June 2026.
8	City of Rancho Cordova	Zinfandel Drive Bicycle and Pedestrian Overcrossing	03-5406	CON	20	02/28/2027	\$19,956,000	The project experienced delays due to extended and repeated reviews required by the Department of the environmental document and project report. As part of the environmental and project report coordination, the comments have required additional time to address each set thoroughly and ensure that all feedback is fully incorporated into the project documents. In addition, the project has experienced delays due to unforeseen cost increases. These escalations are largely attributable to rising construction costs, changes in material prices, and updated City requirements. As the project's costs increased, the City needed to evaluate its funding strategies and secured an additional \$5,000,000 for the CON phase.
9	City of West Sacramento	West Capitol Avenue Regional Connection Bicyclist and Pedestrian Safety Improvements	03-5448	CON	18	12/31/2026	\$735,000	The project required a scope change request due to a Department Encroachment Permit being denied for improvements within State right of way. The scope change request was submitted to the Department in February 2025 and is currently in review. The City has applied for an Affordable Housing and Sustainable Communities Grant and is anticipating approval by December 2025. The City expects to be at 100 percent design by summer of 2026 and will submit the request for the CON phase allocation by the end of 2026.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
10	City of Grass Valley	SR 174/49/20 Roundabout and Active Transportation Safety Project	03-6124	PS&E	15	09/30/2026	\$900,000	The original ATP application originally requested State only funds; however, federal funds were included due to the Congested Mitigation Air Quality (CMAQ) program funds on the CON phase as leveraging funds. This City deliberated about a possible reallocation of funds to an earlier phase to streamline and reduce costs for the CON phase and ultimately led to the federalization of the whole project. This required a Cooperative Agreement which delayed the Project Approval and Environmental Document (PA&ED) phase allocation by eight months. The project was further delayed when the PA&ED phase was inadvertently identified as a State Capitol project and not routed to the Federal Highway Administration (FHWA) for approval. The City received approval from FHWA in September 2024. Afterwards, an RFP was advertised, and the consultant selection was approved by the City in March 2025. The City is requesting the additional time due to the unanticipated delays that has set the project behind schedule. There is a concurrent project allocation time extension request for the RW phase under this same item on this month's Commission agenda.
11	City of Grass Valley	SR 174/49/20 Roundabout and Active Transportation Safety Project	03-6124	RW	15	09/30/2026	\$225,000	The original ATP application originally requested State only funds; however, federal funds were included due to the CMAQ program funds on the CON phase as leveraging funds. This City deliberated about a possible reallocation of funds to an earlier phase to streamline and reduce costs for the CON phase and ultimately led to the federalization of the whole project. This required a Cooperative Agreement which delayed the PA&ED phase allocation by eight months. The project was further delayed when the PA&ED phase was inadvertently identified as a State Capitol project and not routed to the FHWA for approval. The City received approval from FHWA in September 2024. Afterwards, an RFP was advertised, and the consultant selection was approved by the City in March 2025. An expanded level of design is required due to project's proximity to the Department's right of way. These concerns include the modification of the signalized treatment of a nearby intersection affecting the roundabout design, mitigation treatments for the modification of parking at the park-n-ride facility in the center of the project, and the evaluation of whether to prepare a Design Engineering Evaluation Report or a Project Report. The City is currently coordinating the design with the Department. The City is requesting the additional time due to the unanticipated delays that has set the project behind schedule. There is a concurrent project allocation time extension request for the PS&E phase under this same item on this month's Commission agenda..

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
12	Town of Paradise	Go Paradise: Skyway Link Project	03-6129	PS&E	20	02/28/2027	\$715,000	The RFP for the PA&ED phase was advertised in September 2023 and was approved in September 2024. As a result of the 2018 Camp Fire, a corresponding evacuation route widening effort project is a part of the Go Paradise Skyway Link Project (PPNO 6129). The Town has brought additional Community Development Block Grant Disaster Recovery funds allocated through the federal Housing and Urban Development (HUD) Department and administered by the California Housing and Community Development Department to fully leverage a new total project cost of \$28 million on the now combined projects. HUD does not typically oversee road projects therefore their NEPA documentation and process have not been clear on how to move forward for roadway projects. The Town is requesting the additional maximum time allowed to complete the environmental requirements. There is a concurrent project allocation time extension request for the RW phase under this same item on this month's Commission agenda.
13	Town of Paradise	Go Paradise: Skyway Link Project	03-6129	RW	20	02/28/2027	\$358,000	The RFP for the PA&ED phase was advertised in September 2023 and was approved in September 2024. As a result of the 2018 Camp Fire, a corresponding evacuation route widening effort project is a part of the Go Paradise Skyway Link Project (PPNO 6129). The Town has brought additional Community Development Block Grant Disaster Recovery funds allocated through the federal HUD Department and administered by the California Housing and Community Development Department to fully leverage a new total project cost of \$28 million on the now combined projects. HUD does not typically oversee road projects therefore their NEPA documentation and process have not been clear on how to move forward for roadway projects. Preliminary surveying and geotechnical are complete and the consultant is working on the preliminary design to determine what right of way will be required. It is anticipated that right of way in fee, TCE and Permit to Enter and Construct will be required. There have also been utilities identified that will be in conflict as the Town moves further into design. The identified utilities have been notified. The Town is requesting the additional maximum time allowed to complete the environmental requirements. There is a concurrent project allocation time extension request for the PS&E phase under this same item on this month's Commission agenda.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
14	City of Berkeley	Addison Street Bicycle Boulevard Extension Project	04-2356	PS&E	12	06/30/2026	\$529,000	The City received its PA&ED phase allocation in May 2024. The City has been engaged with an intersecting Alameda County Transportation Commission (ACTC) project and a local quick build project, within the Addison Street Bicycle Boulevard Extension Project (PPNO 2356) (Addison Street) limits. The ACTC project is a larger multimodal project within Alameda and Contra Costa Counties and has been in development since the ATP Cycle 6 application. Additionally, the City Council and community have requested nearterm improvements at one of the project intersections. As a result of these related efforts, City staff needed to undertake additional coordination to ensure there are no overlapping efforts with the projects. At the City's expense, the City has developed a quick build pilot project to test the configuration of the project. This is necessary per the recommendations of the City Council adopted Bicycle Plan, Vision Zero Action Plan, and in response to Council member and constituent requests to improve crossing safety along one of the City's Vision Zero High Injury Streets. The results of this pilot project needs to be integrated into both the PA&ED and PS&E phases of the Addison Street. The quick build consists of temporary pavement markings and traffic delineator posts to create a pedestrian and bicycle median refuge. The ATP funded project will study the temporary design and evaluate it as part of public engagement, with the goal of making it permanent, per the ATP grant scope of work.
15	City of Berkeley	Washington Elementary and Berkeley High Safe Routes to School Project	04-2366	PS&E	12	06/30/2026	\$150,000	The City has been engaged with preliminary outreach and coordination with the Washington Elementary School administration to discuss school bus stop improvements within the project area. At that time, City staff developed preliminary concepts for changes to bus stop curb painting and signs. The City first contacted the elementary school principal in the fall of 2022. The project was delayed due to a lack of response from the principal. Unbeknownst to the City, that was a result of a staffing change at the elementary school. The elementary school has a new principal and the preliminary concepts for the project are now moving forward. Currently there are project team meetings scheduled through June 2025. The City plans to have the engagement with the school community completed by September 2025, and environmental complete by June 2026.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
16	City of San Pablo	Broadway-El Portal Safe Routes (BESR) Project	04-2369	PS&E	12	02/28/2027	\$1,295,000	In June 2024, the City received its PA&ED allocation. At the time, the project shared similar improvements and overlapping segments with a separate federal Carbon Reduction Grant. In May 2024, the City began discussions with the Department and the Metropolitan Transportation Commission to combine the two projects to maximize funding and ensure cost savings for both projects. However, in November 2024, it was decided that a clear accounting of the projects would not be feasible. The project no longer has overlapping segments and will continue as originally approved. The shift in approach required additional time to set up the PA&ED phase. In addition, in October 2024, the City was notified that the Master Agreement for the project needed to be renewed. The Master Agreement and the Program Supplement Agreement was fully executed in March 2025. The City anticipates to complete the PA&ED phase and request PS&E allocation by February 2027.
17	City of Santa Maria	Active Santa Maria Safe Routes to School Corridor Improvements	05-3117	PS&E	18	12/31/2026	\$440,000	As part of preparation of environmental document, the City is requesting right of entry (ROE) on land where the trail portion of the project is anticipated to be constructed to perform soil testing. The City has received unanticipated opposition to this request from the property owner, due to property owner's concerns regarding liability of potential contaminant rehabilitation that they would be responsible for if discovered during the City's soil sampling. The City is continuing negotiations with the property owner to determine conditions that will appease the owner's concerns regarding their potential liability discovered by the environmental process. The City is proceeding with the soil sampling on portions of the proposed trail that do not require ROE from the property owner. Time is needed for this process as the property owner's counsel will need to review actions along each step of this process. Accordingly, the City's counsel will need time to prepare the appropriate documentation to protect the City's interests. It is unknown how long these negotiations may take, and response times to both verbal and written communications have taken several weeks. The City is requesting the additional time to accommodate these negotiations and the required testing time, which will impact delivery of completed environmental document. There is a concurrent project allocation time extension request for the RW phase under this same item on this month's Commission agenda.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
18	City of Santa Maria	Active Santa Maria Safe Routes to School Corridor Improvements	05-3117	RW	18	12/31/2026	\$600,000	As part of preparation of the environmental document, the City is requesting ROE on land where the trail portion of the project is anticipated to be constructed to perform soil testing. The City has received unanticipated opposition to this request from the property owner, due to property owner's concerns regarding liability of potential contaminant rehabilitation that they would be responsible for if discovered during the City's soil sampling. The City is continuing negotiations with the property owner to determine conditions that will appease the owner's concerns regarding their potential liability discovered by the environmental process. The City is proceeding with the soil sampling on portions of the proposed trail that do not require ROE from the property owner. Time is needed for this process as the property owner's counsel will need to review actions along each step of this process. Accordingly, the City's counsel will need time to prepare the appropriate documentation to protect the City's interests. It is unknown how long these negotiations may take, and response times to both verbal and written communications have taken several weeks. The City is requesting the additional time to accommodate these negotiations and the required testing time, which will impact delivery of completed environmental document. There is a concurrent project allocation time extension request for the PS&E phase under this same item on this month's Commission agenda.
19	Monterey County	Community and School Connections Through Active Transportation	05-3122A	PS&E	15	09/30/2026	\$450,000	In July 2024, the project experienced delays with NEPA clearance due to the discovery of two potential cultural resources that are close to the project location. The County provided the preliminary maps to the Department in September 2024. It could not be determined if there were Area of Potential Effects (APE). The County is required to assess the APE and conduct a Historic Property Survey Report (HPSR) and Archaeological Survey Report (ASR). The County is requesting the additional time to complete the reports, Department review, and for the County to provide comment to receive environmental clearance. There is a concurrent project allocation time extension request for the RW phase under this same item on this month's Commission agenda.
20	Monterey County	Community and School Connections Through Active Transportation	05-3122A	RW	15	09/30/2026	\$38,000	The project experienced delays with NEPA clearance due to the discovery of two potential cultural resources that are close to the project location. It could not be determined there were no APE. The County is required to assess the APE and conduct a HPSR and ASR. The County is requesting the additional time to complete the reports, Department review, and for the County to provide comment to receive environmental clearance. Once environmental clearance has been received, the County will be able to begin the RW phase. There is a concurrent project allocation time extension request for the PS&E phase under this same item on this month's Commission agenda.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
21	San Luis Obispo County	Morro Bay to Cayucos Multi-Use Trail Gap Closure Project	05-3124	CON	20	02/28/2027	\$7,406,000	The originally proposed construction schedule for the project significantly conflicts with the nearby On Toro Creek Southbound Bridge Replacement Project (EA 05-1R100) (Bridge Replacement project). The Morro Bay to Cayucos Multi-Use Trail Gap Closure Project (PPNO 3124) is located within the Department's right of way. The Department has informed the County that construction staff will not be permitted at the site between March 2026 and March 2027. The County will not have time after the CON phase allocation to award a contract and the awardee to complete the project before the Department starts construction in March 2026.
22	City of King City	San Antonio Drive Path & Safe Routes to Schools	05-3131A	PS&E	20	02/28/2027	\$1,050,000	The project experienced delays during the environmental process due to studies requested by the State Historic Preservation Office (SHPO) to adequately determine the Finding of No Adverse Effect. The City will need to complete a HPSR, APE, and ASR. The City has conducted public outreach, intersection control evaluations, and is currently at 30 percent design. The City is requesting the additional time to complete the reports, Department review, and for the City to provide comment to receive environmental clearance. There is a concurrent project allocation time extension request for the RW phase under this same item on this month's Commission agenda.
23	City of King City	San Antonio Drive Path & Safe Routes to Schools	05-3131A	RW	20	02/28/2027	\$12,000	The project experienced delays during the environmental process due to studies requested by the SHPO to adequately determine the Finding of No Adverse Effect. The City will need to complete a HPSR, APE, and ASR. The City has conducted public outreach, intersection control evaluations, and is currently at 30 percent design. The City is requesting the additional time to complete the reports, Department review, and for the City to provide comment to receive environmental clearance. Once environmental clearance has been received, the City will be able to begin the RW phase. There is a concurrent project allocation time extension request for the PS&E phase under this same item on this month's Commission agenda.
24	City of Salinas	Harden Parkway Path & Safe Routes to School project	05-3133A	PS&E	15	09/30/2026	\$1,575,000	The project experienced delays during the environmental process due to studies requested by the Department. The City will need to complete Hazardous Materials, Noise and Traffic Memos, HPSR, ASR, and a Section 4F. The RFP was advertised in February 2025, and the environmental consultant selection was approved in April 2025. The City is requesting the additional time to complete the reports, Department review, and for the City to provide comment to receive environmental clearance.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
25	Tulare County	Tipton Sidewalk Improvements Project	06-7093	CON	14	08/31/2026	\$1,218,000	The County has experienced delays with final design and right of way, which have significantly impacted the project timeline. During the final design phase in early 2024, it became evident that additional improvements were necessary to enhance safety and accommodate existing site conditions. One of the most significant changes was the incorporation of diagonal parking along Tipton Elementary School's frontage, replacing the originally planned parallel parking. The revised design improves traffic flow, pedestrian access, and safety while maintaining adequate parking. However, because the proposed improvements extend into school property, additional coordination with the school district has been required, delaying the right of way process. Additionally, it was estimated that only eight parcels would require right of way coordination, but this number increased to 23 due to necessary design refinements completed in early 2024. Two of those parcels require acquisition, while the remaining parcels involve encroachments and TCEs. The increased number of impacted properties has extended the time required for property owner notifications, negotiations, and necessary approvals. The project has also faced more extensive utility conflicts than initially anticipated. The final design will require relocation of fire hydrants, light poles, and underground utilities. The final design confirmed in late 2024, that four utility providers (American Telephone and Telegraph, Tipton Community Services District, Southern California Gas Company, and Southern California Edison) will be impacted. Final design efforts are ongoing and cannot be finalized until all utility conflicts are resolved and right of way needs are secured.
26	City of Bakersfield	H Street Corridor (SR-204 to Hwy 58)	06-8070	CON	20	02/28/2027	\$3,154,000	The project is funded through the ATP, State and federal, in addition to the CMAQ program. As a result, the project is going through a stringent federal approval process. The PES was initially approved in January 2025. In March 2025, it was determined by the Department that a Hot Spot Analysis and an interagency consultation approval was needed and ultimately approved in April 2025. The RW Certification is pending as the City is in discussions with Pacific Gas and Electric (PG&E) with liability of adjustments to valves, boxes, and vaults. Despite constant pressure from the City to obtain a Liability Letter, PG&E has not been receptive to the City's responsibility to adhere to the Department's TUF guidelines. Due to the uncertainty of a timely response from PG&E, in addition to any comments or concerns they may have, the City is requesting the maximum time allowed to resolve this issue.
27	City of Porterville	Tule River Tribe Complete Streets and Two Pedestrian Bridges Project	06-8084B	CON-NI	12	06/30/2026	\$40,000	The CON phase was approved for a project allocation time extension at the May 2025 Commission meeting due to delays in the PA&ED and PS&E phases. The City was not the original implementing agency at the time of programming. The City was not familiar with the project or any of its components until the documents for the authorization of PA&ED phase were received for the City to submit, by the original implementing agency, Tulare County Association of Governments. The City made a selection and was approved at the March 2025 City Council meeting. The CON-NI phase of this project is planned to coincide with the design of the project and CON phase. This will allow the City to incorporate the design elements into the public outreach efforts. The additional time will allow the City to complete the PA&ED phase and request allocation for PS&E phase by June 2026.

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28	City of Los Angeles	SRTS Carver Middle, Ascot Avenue and Harmony Elementary Schools Project	07-5859	CON	20	02/28/2027	\$4,939,000	At the May 2024 meeting, the Commission approved a 12-month project development expenditure and project allocation time extension for the PS&E phase, respectively. The City was approved for its PS&E phase allocation at the May 2025 Commission meeting. The project was affected by the atmospheric river winter storms in 2023 and 2024, that prompted both State and Federal Emergency Declarations. The project was further delayed early in January 2025, by the Eaton and Palisades fires that also prompted State and Federal Emergency Declarations. Due to the devastating fires, the extent of delays to the project is still not known. The delays of these phases have pushed out the project's design timeline. The City is requesting a 36-month project allocation time extension to align the CON allocation with the current PS&E timeline and preserve the project's ability to request a one-time expenditure extension for PS&E if necessary, as permitted in the TUF guidelines.
29	City of Los Angeles	SRTS Panorama City Elementary School Project	07-5866	CON	20	02/28/2027	\$5,064,000	At the May 2024 meeting, the Commission approved a 12-month project development expenditure and project allocation time extension for PS&E phase, respectively. The City was approved for its PS&E phase allocation at the May 2025 Commission meeting. The project was affected by the atmospheric river winter storms in 2023 and 2024, that prompted both State and Federal Emergency Declarations. The project was further delayed early in January 2025, by the Eaton and Palisades fires that also prompted State and Federal Emergency Declarations. Due to the devastating fires, the extent of delays to the project is still not known. The delays of these phases have pushed out the project's design timeline. The City is requesting a 36-month project allocation time extension to align the CON allocation with the current PS&E timeline and preserve the project's ability to request a one-time expenditure extension for PS&E if necessary, as permitted in the TUF guidelines.
30	Los Angeles County	Rosewood/West Rancho Dominguez Vision Zero Traffic Safety Enhancement Project	07-6157	PS&E	20	02/28/2027	\$1,150,000	Before preliminary design began, the County performed a topographic survey for the project. The survey was extensive and required more time than is typical. After the survey was completed, it was found that several locations initially planned for improvements were not feasible. The proposed improvements could not be constructed at these locations due to conflicts with adjacent driveways, drainage structures, and overlapping improvements with other nearby projects. Los Angeles County Public Works evaluated new locations to ensure that the project provides a meaningful improvement to community safety. The new locations selected as replacements are still within the project limits of the approved ATP application. The first community outreach is planned for summer 2025, and every six months thereafter until completion of environmental and final design. The completion of the environmental assessment has been delayed until revised preliminary plans are developed with enough information to confidently assess the project's environmental impacts. The California Environmental Quality Act (CEQA) clearance is anticipated in November 2026. The project will also require additional time to process the baseline agreement prior to requesting the PS&E phase allocation.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
31	Los Angeles County	Walnut Park Pedestrian Plan Implementation	07-6159	PS&E	20	02/28/2027	\$262,000	The project is in primarily commercial areas. County staff spent extensive time to perform field investigation and analysis to determine the feasibility of the proposed locations and improvements based on the existing conditions, and how their implementation might affect access for businesses. The feasibility study was completed with some adjustments from the original scope and will be presented to the community for feedback. The completion of the environmental assessment has been delayed until preliminary plans are developed with enough information to confidently assess the project for environmental impacts. The first community outreach is planned for Summer 2025, and every six months thereafter until completion of environmental and final design.
32	City of Signal Hill	E. Burnett Street Historical District Pedestrian and Bike Enhancement Project	07-6169	PS&E	4	10/31/2025	\$306,000	There were delays in finalizing the environmental analysis report. Additional time was needed to substantiate the applicability of a Class 1 Categorical Exemption for the project, including a thorough evaluation of potential exceptions under CEQA Guidelines Section 15300.2. The environmental review has now confirmed that no exceptions apply and that the Class 1 exemption is appropriate, the document is currently undergoing final internal processing and is scheduled to be filed with the Los Angeles County Recorder's Office in the coming weeks. Although the PA&ED phase is nearly complete, the CEQA documentation was not finalized in time to meet the deadline for submitting materials for the June 2025 Commission meeting. All required documents will be ready to meet the June 16, 2025 deadline to request the PS&E phase allocation for the August 2025 Commission meeting.
33	City of Cudahy	Salt Lake Avenue Pedestrian Accessibility Project	07-6171	PS&E	12	06/30/2026	\$632,000	The City advertised the RFP in October 2024, and only received one proposal. The agency decided to re-advertise the RFP to receive at least three proposals. The second RFP was advertised in January 2025, and the agency is currently reviewing two proposals and will select a consultant in May 2025. It is anticipated that the environmental determination will be a Categorical Exemption, which will require the preparation of a Notice of Exemption. The City is requesting the additional time to receive approval for the consultant selection and complete the environmental submissions.
34	City of Simi Valley	Arroyo Simi Greenway Phase 5 AP#2	07-6186	CON			\$659,000	The City submitted a revised Electronic Project Programming Request to the Ventura County Transportation Commission and the Southern California Association of Governments to correct the programming for the PS&E, RW, and CON phases, however it was not received by the Department. As a result, the action to move the programmed fiscal years for the CON phase was not captured by the Commission. At the June 2024 Commission meeting a time extension for the PS&E and RW phases was approved and subsequently the phases were approved for project allocation at the January 2025 Commission meeting. However, the CON phase fiscal year programming remains misaligned with the updated project delivery schedule. Recently, the City noticed the programming oversight and promptly requested a time extension following current timely use of funds provisions.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
35	City of Fontana	Foothill Boulevard Active Transportation Improvement	08-1295	CON	20	02/28/2027	\$4,925,000	The project improvements require construction and right of way acquisitions along State Route 66 and Malaga Bridge/Pumphouse, both designated as historical by the City of Fontana and Section 106 of the National Historic Preservation Act. Right of way acquisition and 100 percent PS&E cannot be prepared and approved until environmental clearance has been approved. The City coordinated with the Department and the SHPO and it was determined the project would have an adverse effect on historic properties. As a result, the environmental document must be elevated to an Environmental Impact Statement/Environmental Assessment, causing significant delays. Due to the sensitive nature of the historic properties and environmental review involved, the City is requesting the maximum time allowed to complete review and receive approval.
36	City of Montclair	Montclair SRTS Implementation Project	08-1305A	CON	12	06/30/2026	\$76,000	The City received it's PS&E phase allocation in June 2024. The City began its efforts to prepare and solicit for engineering design services and made a selection in November 2024. The project encountered delays due to lack of found monumentation. To correct for delay in schedule the consultant overlapped base construction drawings with topographic survey. The survey was completed in May 2025 and the design is scheduled to be completed by early 2026 with the first review is anticipated in August 2025. The City is requesting the additional time to complete the design and reviews.
37	City of Montclair	Montclair SRTS Implementation Project	08-1305B	CON-NI	12	06/30/2026	\$76,000	This phase requires coordination with three local schools. The City will implement an educational and encouragement program at each of the schools to complement the infrastructure improvements. The planned activities include school rodeos, walking events, crosswalk demonstration projects, and before/after walking and biking audits. These programs will be conducted during and after construction. This has delayed the preparation of materials and the activity planning component. The additional time will provide flexibility for activity and educational outreach planning while the City completes the PS&E phase. The educational outreach will be conducted during and after construction.
38	City of Stockton	Main and Market Complete Streets (Phase 1)	10-3611	CON	20	02/28/2027	\$6,893,000	The City has encountered delays obtaining accurate traffic data due to the COVID-19 Pandemic, the need for additional public outreach, and securing environmental clearance and final approval of the Precise Road Plan that governs the broader corridor. Expanded public outreach efforts were necessary to address stakeholder concerns and ensure thorough compliance with NEPA and CEQA requirements. In March 2024, the Commission approved a request to extend the PS&E phase and has a concurrent allocation request on this month's Commission agenda. The City received CEQA and NEPA clearance in April 2025. As a result of the delays to the PS&E phase, additional time is needed to complete the PS&E phase. The City anticipates completing the PS&E phase and obtaining RW Certification by December 2026.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
39	City of Ceres	Building on Active Transportation Connectivity and Access within the City	10-3613	PS&E	20	02/28/2027	\$256,000	The City is on the Department's Do Not Authorize List and is unable to request further allocations until their Fiscal Year 2023 and 2024 Single Audit Reports (SAR) have been completed. The City has been working on their 2023 SAR since the end of 2023. At that time, the City was without a Finance Director and had implemented a new financial system but despite best efforts, staff was unable to successfully reconcile. The City hired a consultant, but even with the specialized expertise, it took six weeks to go through one month of records. This situation is unprecedented for the City but they are now in a position to complete their 2023 SAR by December 2025 and immediately begin the 2024 SAR. The City has since hired a Finance Director and is hopeful they will not require the full extension period. The City's financial accounting has a new administration, and is confident it has untangled the previous administration's accounting. In addition, the consultant will be staying on through the completion of the 2024 SAR to ensure the City is on solid ground again. The City is requesting the maximum time allowed to have the flexibility to ensure compliance without risking project funding.
41	City of Ceres	Improving Pedestrian Safety on Central Ave and Hackett Road Corridor	10-3617	RW	6	12/31/2026	\$637,000	The City is in the process of finalizing the PA&ED phase. However, the City is on the Department's Do Not Authorize List and is unable to request further allocations until their Fiscal Year 2023 and 2024 SARs have been completed. The City has been working on their 2023 SAR since the end of 2023. At that time, the City was without a Finance Director and had implemented a new financial system but despite best efforts, staff was unable to successfully reconcile. The City hired a consultant, but even with the specialized expertise, it took six weeks to go through one month of records. This situation is unprecedented for the City but they are now in a position to complete their 2023 SAR by December 2025 and immediately begin the 2024 SAR. The City has since hired a Finance Director and is hopeful they will not require the full extension period. The City's financial accounting has a new administration, and is confident it has untangled the previous administration's accounting. In addition, the consultant will be staying on through the completion of the 2024 SAR to ensure the City is on solid ground again. There is a concurrent project allocation time extension request for the PS&E phase under this same item on this month's Commission agenda.
42	City of Chula Vista	Bayshore Bikeway Segment 6A	11-1522	PS&E	12	09/30/2026	\$150,000	The City received its PA&ED phase allocation in January 2024, with the intention of completing the project by the end of December 2025. However, the City experienced unexpected and no advance notice of the retirement of the City's Department of Engineering Director, Capitol Improvements Project Director, and Senior Engineer. In addition, the City has three associate engineer vacancies. The City has been diligently recruiting and strategizing methods to maintain project schedules and increase project output. The City's updated project schedule anticipates selecting a consultant for the PA&ED phase by August 2025, and receiving environmental clearance by May 2026. The City is requesting the additional time to begin and complete the PA&ED phase and request its PS&E project allocation by June 2026.

Project Number	Agency Name	Project Title	District PPNO	Phase	Requested Months	Extended Deadline	Amount	Justification
43	City of Lemon Grove	Connect Main Street Phases 4 thru 6	11-1525	PS&E	20	02/28/2027	\$1,194,000	In January 2024, the City suffered significant damage from unprecedented storms, which resulted in the Governor declaring a State of Emergency (Proclamation CDAA 2024-04). The emergency incident required staff to shift its focus and resources to addressing four storm drain capital improvement projects resulting from damage and claims, creating an interruption in completing the PA&ED phase. The City is currently in the planning process of developing the RFP and specifying the scope of services for consultant selection for the PA&ED phase. This time extension will allow the City to complete the PA&ED phase and request PS&E by March 2027.
44	City of Buena Park	Dale / Whitaker Complete Streets Project	12-1316	CON	20	12/31/2026	\$3,473,000	The project experienced delays with the permitting from BNSF Railroad (BNSF), the City of Fullerton, and the Orange County Flood Control District (OCFCD). A Roadway Resurfacing Permit is needed from BNSF. Coordination with BNSF has started and the City is waiting to hear if 65 percent plans will be required for permitting. If permitting is needed, the City anticipates submitting those plans by June 2025. Encroachment permits are also needed from the City of Fullerton and the OCFCD. Both are requiring review of 65 percent plan. The meetings are ongoing. Additionally, a new Southern California Edison irrigation meter will be required as part of the landscaping and irrigation improvements. The current schedule anticipates several months to complete the right of way activity. Due to the agencies involved, there will likely be multiple rounds of review. The City has a concurrent RW phase allocation on this month's Commission agenda. The City anticipates finalizing PS&E and with approved right of way activities to request CON phase allocation by December 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8b.(5), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF CONTRACT AWARD FOR
LOCALLY-ADMINISTERED ACTIVE TRANSPORTATION PROGRAM PROJECTS,
OFF THE STATE HIGHWAY SYSTEM, PER ATP GUIDELINES
WAIVER 25-127**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of contract award for four locally-administered Active Transportation Program (ATP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of contract award for four locally-administered ATP projects, for the time periods as identified in the attachment.

BACKGROUND:

In December 2024, January 2025, and March 2025, the Commission allocated \$12,212,000 for four locally-administered ATP projects identified in the attachment. The responsible agencies have been unable to award the contracts within six months of allocation, per ATP Guidelines (Guidelines). The attachment describes the details of the projects and the explanations for the delays. The respective agencies request an extension, and the planning agencies concur.

Current Guidelines adopted by the Commission, stipulate that the agencies implementing a project may request a time extension if the project will not be awarded within six months of allocation. The Commission may approve a waiver to the Timely Use of Funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Contract Award
Active Transportation Program**

Project: 1
Applicant: County of Santa Barbara
County: Santa Barbara
PPNO: 2992
District: 5
Allocation Amount: \$2,616,000
Phase: Construction
Allocation Date: 12/06/2024
Allocation Resolution: FATP-2425-11
Number of Months Requested: 10 Months
Extended Deadline: 04/30/2026
Department Recommendation: Support

Modoc Road Multimodal Path Gap Closure (Phase II)

The County of Santa Barbara (County) requests a 10-month time extension for the period of contract award for the Construction (CON) phase of the Modoc Road Multimodal Path Gap Closure (Phase II) project. The City has experienced delays with the project due to weather and resource impacts.

The project proposes to construct a multi-use path along Modoc Road.

The County experienced significant storm damage during the 2023-24 and 2024-25 winter seasons, that resulted in dozens of large disaster sites. The work on damage repair is still ongoing. This has impacted the County's ability to assign resources to the project resulting in delays with the contract bid process. As a result, the plans and specifications need to be updated to address material cost increases. The County is requesting the additional time to update the plans to prepare for the contract bid and award process. There is a concurrent project development expenditure time extension request for the Plans, Specifications, and Estimate phase on this month's Commission agenda.

Therefore, the County is requesting a 10-month time extension for the period of contract award for the CON phase from June 30, 2025 to April 30, 2026.

Project: 2
Applicant: City of El Monte
County: Los Angeles
PPNO: 6042
District: 7
Allocation Amount: \$4,334,000
Phase: Construction
Allocation Date: 01/31/2025
Allocation Resolution: FATP-2425-13
Number of Months Requested: 6 Months
Extended Deadline: 01/31/2026
Department Recommendation: Support

Traffic Calming for Parkway Dr/Denholm Dr

The City of El Monte (City) requests a 6-month time extension for the period of contract award for the CON phase of the Traffic Calming for Parkway Dr/Denholm Dr project. The City has experienced delays in construction programming.

The project proposes to construct a Class III, Class IV, landscape buffer, crosswalk, curb extensions, Americans with Disabilities Act ramps, conflict striping, widen sidewalks, and add a stop control at one intersection.

This project received its ATP CON phase allocation in January 2025, however this project also contains federal earmark funds which require an Authorization to Proceed (E-76). The City had an approved 2023 Federal Transportation Improvement Program (FTIP) funding with proper programming. However, when the City submitted the E-76 for the CON phase, it was discovered the 2025 FTIP was incorrect. The corrected FTIP was submitted to the Los Angeles Metropolitan Authority and was approved in May 2025. With approval of the FTIP, the City can then submit their E-76 for the CON phase. The City anticipates receiving the E-76 in September 2025, and advertising the contract in October 2025. This extension allows time for proper federal procedures to advertise and award a construction contract.

Therefore, the City is requesting a 6-month time extension for the period of contract award for the CON phase from July 31, 2025 to January 31, 2026.

Project: 3
Applicant: City of Wildomar
County: Riverside
PPNO: 1286B
District: 8
Allocation Amount: \$77,000
Phase: Construction Non-Infrastructure
Allocation Date: 01/31/2025
Allocation Resolution: FATP-2425-13
Number of Months Requested: 12 Months
Extended Deadline: 07/31/2026
Department Recommendation: Support

Bundy Canyon Active Transportation Corridor (BCATC)

The City of Wildomar (City) requests a 12-month time extension for the period of contract award for the Construction Non-Infrastructure (CON-NI) phase of the Bundy Canyon Active Transportation Corridor (BCATC) project. The City has experienced delays during the CON phase.

The project proposes to construct a Class I shared bicycle and pedestrian path. In addition it proposes to provide outreach events that would educate and encourage residents and high school students to use active modes of transportation safely.

The CON phase of the project received a 20-month project allocation time extension in May 2022. The City requested and received its CON phase allocation in March 2024. The CON-NI component was originally planned to take place after the construction work was near completion. The outreach events planned would be most beneficial once the infrastructure is near completion. The events will provide on-site learning experience and photos of the new facilities for training and presentation. This will be instrumental in engaging students and residents to visibly see the changes and how the project benefits their community. As a result of the CON phase schedule delay, the contract award for the CON-NI phase will need to be pushed back to line up with construction completion.

Therefore, the City is requesting a 12-month time extension for the period of contract award for the CON-NI phase from July 31, 2025 to July 31, 2026

Project: 4
Applicant: City of National City
County: San Diego
PPNO: 1395
District: 11
Allocation Amount: \$5,185,000
Phase: Construction
Allocation Date: 03/21/2025
Allocation Resolution: FATP-2425-17
Number of Months Requested: 12 Months
Extended Deadline: 09/30/2026
Department Recommendation: Support

8th Street and Roosevelt Ave Active Transportation Corridor, National City

The City of National City (City) requests a 12-month time extension for the period of contract award for the CON phase of the 8th Street and Roosevelt Ave Active Transportation Corridor, National City project. The City has experienced delays with utility relocation coordination.

The project proposes to construct a multi-use path and sidewalk, install lighting, landscape and make various intersection adjustments for safety improvements.

The project is currently facing a substantial and unforeseeable delay due to gas and electrical relocation issues involving San Diego Gas and Electric (SDG&E). Despite proactive coordination, SDG&E has encountered significant internal constraints and has confirmed that they require additional time to review the 100 percent improvement plans. Following this initial review, SDG&E will perform a critical comparison check of the final, signed improvement plans. Only after this comparison can they proceed with scheduling and executing the gas and electric relocation design work. According to SDG&E, this next phase that comprises design preparation, permit acquisition, crew scheduling, and the physical construction necessary to resolve utility conflicts will take several additional months. These relocations are needed before the City can proceed with its own construction efforts. The City is requesting the additional time for SDG&E review and finalization of designs. Following finalization the City will need to obtain necessary permits, schedule crews, and complete relocation construction.

Therefore, the City is requesting a 12-month time extension for the period of contract award for the CON-NI phase from September 30, 2025 to September 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8c.(4), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT COMPLETION FOR
LOCALLY-ADMINISTERED ACTIVE TRANSPORTATION PROJECTS, OFF THE
STATE HIGHWAY SYSTEM, PER ATP GUIDELINES
WAIVER 25-128**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project completion for two locally-administered Active Transportation Program (ATP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of project completion for two locally-administered ATP projects, for the time periods as identified in the attachment.

BACKGROUND:

In December 2021 and March 2022, the Commission allocated \$10,514,000 for two locally-administered ATP projects. The responsible agencies are unable to complete the projects by their construction completion deadlines. The attachment describes the details of the projects and the explanation for the delays. The respective agencies request an extension and the planning agencies concur.

Current ATP Guidelines adopted by the Commission stipulate that a local agency has up to 36 months from the time of contract award to contract acceptance. The local agency has 180 days after the contract acceptance to prepare and submit the final invoices and reports to the Department. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Completion
Active Transportation Program**

Project Number: 1
Applicant: City of Santa Cruz
County: Santa Cruz
District: 5
PPNO: 3058A
Allocation Amount: \$8,634,000
Phase: Construction
Allocation Date: 12/09/2021
Allocation Resolution: FATP-2122-09
Award Date: 05/11/2022
Number of Months Requested: 3 Months
Extended Deadline: 08/31/2025
Department Recommendation: Support

Santa Cruz Rail Trail Segment 7 Phase 2 Construction

The City of Santa Cruz (City) requests a 3-month time extension to complete the Construction (CON) phase of the Santa Cruz Rail Trail Segment 7 Phase 2 Construction project. The City has experienced delays completing construction.

The project proposes to construct a bicycle and pedestrian multi-use trail adjacent to existing railroad tracks.

The project went through two extremely long and intense rainy seasons in the winter of 2023 and 2024 that resulted in State and Federal declared disaster areas. This caused a delay in the construction project for over six months. The City had a soft opening of the trail segment to the public on May 9, 2025, and has scheduled a ribbon cutting ceremony on May 21, 2025. The City is requesting the additional time for project close out and to be able to capture final expenses as they work with consultants and the contractor to finalize contract close out work, including final punch list items and as-builts.

Therefore, the City is requesting a 3-month time extension to complete the CON phase from May 31, 2025 to August 31, 2025.

Project Number: 2
Applicant: Southern California Association of Governments
County: Various
District: 7
PPNO: 5899
Allocation Amount: \$1,880,000
Phase: Construction Non-Infrastructure
Allocation Date: 03/17/2022
Allocation Resolution: FATP-2122-20
Award Date: 08/12/2022
Number of Months Requested: 12 Months
Extended Deadline: 08/31/2026
Department Recommendation: Support

Sustainable Communities Program

The Southern California Association of Governments (SCAG) requests a 12-month time extension to complete the Construction Non-Infrastructure (CON-NI) phase of the Sustainable Communities Program project. The City has experienced delays completing CON-NI.

The project proposes to implement Active Transportation Network Visions projects, Active Transportation Plan projects, and a Safe Routes to School Plan.

The consultant originally selected, in September 2022, to deliver the plan did not provide satisfactory performance. Approximately 20 percent of the project scope was completed. As a result, in July 2023, the contract was canceled. A new Request for Proposal (RFP) to complete the remainder of the project was due September 2024, but due to a lack of proposals, the RFP deadline was extended to October 2024. A new contract was executed in April 2025. The project has one year remaining. Additionally, the plan was intended to be a joint project effort with the Orange County Transportation Authority (OCTA) and Los Angeles County (County). This was anticipated to begin the summer of 2023. However, due to the project consultant cancellation, OCTA and the County issued separate RFPs. Their plans are expected to be complete by July 2026. SCAG is requesting the additional time to complete and implement their active transportation and Safe Routes to School plans.

Therefore, SCAG is requesting a 12-month time extension to complete the CON-NI phase from August 31, 2025 to August 31, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8d.(5), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT DEVELOPMENT
EXPENDITURE FOR LOCALLY-ADMINISTERED ACTIVE TRANSPORTATION
PROGRAM PROJECTS, OFF THE STATE HIGHWAY SYSTEM, PER ATP
GUIDELINES
WAIVER 25-129**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project development expenditure for two locally-administered Active Transportation Program (ATP) projects, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project development expenditure for two locally-administered ATP projects, for the time period as identified in the attachment.

BACKGROUND:

In March 2023, the Commission allocated \$803,000 for two locally-administered ATP projects. In accordance with ATP Guidelines (Guidelines), the deadline for expenditures for the projects is June 30, 2025. The responsible agencies have been unable to complete the projects component per the Timely Use of Funds (TUF) policy. The attachment describes the details of the projects and the explanations for the delays.

Current Guidelines, adopted by the Commission, stipulate that the funds programmed for all components of local grant projects are available for expenditure only until the end of the second fiscal year following the year of allocation. The Commission may approve a waiver to the TUF deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Development Expenditure
Active Transportation Program**

Project Number: 1
Applicant: County of Santa Barbara
County: Santa Barbara
District: 5
PPNO: 2992
Allocation Amount: \$543,000
Remaining Balance: \$272,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 03/23/2023
Allocation Resolution Number: FATP-2223-23
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Modoc Road Multimodal Path Gap Closure (Phase II)

The County of Santa Barbara (County) requests a 12-month time extension for the period of project development expenditure for the Plans, Specifications, and Estimate (PS&E) phase of the Modoc Road Multimodal Path Gap Closure (Phase II) project. The County has experienced delays with the project due to weather and resource impacts.

The project proposes to construct a multi-use path along Modoc Road.

The County experienced significant storm damage during the 2023-24 and 2024-25 winter seasons, that resulted in dozens of large disaster sites. The work to repair the damage is still ongoing. This has impacted the County's ability to assign resources to the project resulting in delays with the contract bid process. In addition, as a result of the storms, the County is proactively anticipating design modifications that are not known until after the construction contractor breaks ground. The County is requesting the additional time to prepare for design modifications after a contractor is approved for construction. There is a concurrent project contract award time extension request for the Construction phase on this month's Commission agenda.

Therefore, the County is requesting a 12-month time extension for the period of project development expenditure for the PS&E phase from June 30, 2025 to June 30, 2026.

Project Number: 2
Applicant: National City
County: San Diego
District: 11
PPNO: 1441
Allocation Amount: \$260,000
Remaining Balance: \$76,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 03/23/2023
Allocation Resolution Number: FATP-2223-23
Number of Months Requested: 6 Months
Extended Deadline: 12/31/2025
Department Recommendation: Support

Highland Avenue Inter-City Bike Connection

National City (City) requests a 6-month time extension for the period of project development expenditure for the PS&E phase of the Highland Avenue Inter-City Bike Connection project. The City has experienced delays with the right of way process.

The project proposes to construct Class IV and II bicycle facilities, traffic calming, pavement markings, signing, signal modifications, and Americans with Disabilities Act curb ramps.

The City submitted an Encroachment Permit permit to the Department for a commercial property in October 2024. The City received comments back from the Department in February 2025 and resubmitted with responses in May 2025. When it has been finalized, the design team will need to finalize right of way coordination with the owners. The commercial property owner is not local which has led to slow coordination. The City anticipates right of way coordination to be completed by November 2025. Right of Way Certification will be completed by January 2026. The City is requesting the additional time to complete the permitting process and complete the PS&E phase.

Therefore, the City is requesting a 6-month time extension for the period of project development expenditure for the PS&E phase from June 30, 2025 to December 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8a.(22), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT ALLOCATION FOR
LOCALLY-ADMINISTERED LOCAL TRANSPORTATION CLIMATE ADAPTATION
PROGRAM PROJECTS, OFF THE STATE HIGHWAY SYSTEM,
PER LTCAP GUIDELINES
WAIVER 25-130**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project allocation for two locally-administered Local Transportation Climate Adaptation Program (LTCAP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission extend the period of project allocation for two locally-administered LTCAP projects, for the time periods as identified in the attachment.

BACKGROUND:

The agencies will not be able to request allocation of funds by the June 30, 2025 deadline. The attachment shows the details of the projects and the explanations for the delays.

The 2023 LTCAP Guidelines adopted by the Commission stipulate that funds that are programmed for all components of locally implemented grant projects are available for allocation only until the end of the fiscal year. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months.

Attachment

**Time Extension/Waiver – Project Allocation
Local Transportation Climate Adaptation Program**

Project Number: 1
Applicant: Mendocino County
County: Mendocino
District: 1
PPNO: 4888
Allocation Amount: \$6,000,000
Phase: Plans, Specifications, and Estimate
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Redmeyer Road Extension project

Mendocino County (County) requests a 12-month time extension for the period of project allocation for the Plans, Specifications, and Estimate (PS&E) phase of Redmeyer Road Extension project. The County experienced delays during the Project Approval and Environmental Document (PA&ED) phase.

The project proposes constructing a 2-lane road with paved shoulders to extend Redmeyer Road as well as a local bridge structure over the Russian River.

During the PA&ED phase, the Department required an extended phase 1 study be completed due to the location and the possibility of encountering cultural resources. This additional study extended the schedule due to the length of the study area needed, intrusion into private property requiring permission to enter (PTE), field work to complete the study, and environmental permits to perform the field work for the study. Additionally, the 2024-2025 rain season resulted in high flows in the nearby Russian River, resulting in delays getting to the site to start the geotechnical and environmental studies. Finally, unanticipated stakeholder concerns were raised by a property owner and a local business about the initial roadway alignment. Coordination with those stakeholders caused delays, but ultimately, the County finalized and proceeded with an agreeable alignment, shifting slightly north of the initial proposal in order to minimize impacts to a private property.

The project anticipates obtaining the California Environmental Quality Act (CEQA) and National Environmental Policy Act (NEPA) clearances by June 2026.

Therefore, the County is requesting a 12-month time extension for the period of project allocation for the PS&E phase from June 30, 2025 to June 30, 2026.

Project Number: 2
Applicant: Mendocino County
County: Mendocino
District: 1
PPNO: 4889
Allocation Amount: \$6,000,000
Phase: Plans, Specifications, and Estimate
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

Brooktrails Second Access project

Mendocino County (County) requests a 12-month time extension for the period of project allocation for the PS&E phase of the Brooktrails Second Access project. The County experienced delays during the PA&ED phase.

The project proposes constructing a 2-lane road with paved shoulders as a second access to the Brooktrails Township.

The project originally included two alignment alternatives. After discussions between nearby landowners and the County, two additional alignment alternatives were added for consideration, increasing the amount of effort and therefore time to evaluate all four alternatives for viability. Four landowner coordination meetings have been held between October 2024 and April 2025 to get feedback on the alternatives as well as communicate the CEQA and NEPA processes. The Padula Trust owner, which is the landowner most impacted by the alternatives, was given the opportunity to review and comment on the geometric approval drawings. A PTE for environmental and engineering technical studies was also given to the landowner, however they have yet to sign. The PTE is currently the critical path item, which the County is working towards with the landowner.

The project anticipates obtaining CEQA and NEPA clearances by June 2026.

Therefore, the County is requesting a 12-month time extension for the period of project allocation for the PS&E phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8b.(6), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF CONTRACT AWARD FOR A TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM COMPONENT, PER TIRCP ALLOCATION POLICY WAIVER 25-131**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of contract award for the Transit and Intercity Rail Capital Program (TIRCP) (2022:21) Sonoma Regional Bus and Rail Connectivity Improvements (Purchase 4 Electric Buses) component (PPNO CP100A), in Sonoma County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of contract award for the TIRCP (2022:21) Sonoma Regional Bus and Rail Connectivity Improvements (Purchase 4 Electric Buses) component (PPNO CP100A), in Sonoma County, for the time period as identified in the attachment.

BACKGROUND:

In January 2025, the Commission allocated \$1,178,000 for the Construction (CON) phase of the TIRCP (2022:21) Sonoma Regional Bus and Rail Connectivity Improvements (Purchase 4 Electric Buses) component (CP100A). The responsible agency has been unable to award a third-party contract per the Timely Use of Funds (TUF) policy.

The current TIRCP Allocation Policy stipulates that the agency implementing a project component request a time extension if the project component will not be awarded within the specified timeline. Under the TUF policy, an extension can be requested for up to 20 months for each delivery milestone, where permissible by statute. The attachment describes the details of the project component and the explanation for the delays.

Attachment

**Time Extension/Waiver – Contract Award
Transit and Intercity Rail Capital Program**

Project Number: 1
Applicant: Sonoma County Transit
County: Sonoma
District: 4
PPNO: CP100A
Allocation Amount: \$1,178,000
Phase: Construction
Allocation Date: 01/31/2025
Allocation Resolution: TIRCP-2425-46
Number of Months Requested: 8 Months
Extended Deadline: 03/31/2026
Department Recommendation: Support

(2022:21) Sonoma Regional Bus and Rail Connectivity Improvements (Purchase 4 Electric Buses)

Sonoma County Transit (SCT) requests an 8-month time extension for the period of contract award for the Construction (CON) phase of the (2022:21) Sonoma Regional Bus and Rail Connectivity Improvements (Purchase 4 Electric Buses) component. SCT has experienced unanticipated delays in awarding the third-party contract.

The project component proposes to purchase four 40-foot battery-electric buses from Gillig LLC, which will be deployed by SCT throughout its service area in Sonoma County. The new battery electric buses will enhance transit connectivity and support ridership growth.

In January 2025, the Commission allocated \$1,178,000 for the CON phase of the component. However, in early April 2025, Gillig LLC informed SCT that they were pausing proposals for buses due to federal tariffs, thus hampering SCT's anticipated execution of the third-party contract by June 2025. Currently the project component is estimated to be ten percent complete. SCT plans on funding this component with local Transportation Development Act funds and signing a third-party contract with Gillig LLC by February 28, 2026. Delivery of the buses is then expected by January 2028 with project closeout in April 2028.

Therefore, SCT is requesting an 8-month time extension for the period of contract award for the CON phase from July 31, 2025 to March 31, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8c.(5), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT COMPLETION FOR
TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM COMPONENTS, PER
TIRCP ALLOCATION POLICY
WAIVER 25-132**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project completion for the Transit and Intercity Rail Capital Program (TIRCP) (2018:01) Purchase Zero Emission High Capacity Buses to Support Transbay Tomorrow and Clean Corridors Plan (Facilities and Bus Charging Infrastructure) component (PPNO 2320B), in Alameda County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of project completion for the TIRCP (2018:01) Purchase Zero Emission High Capacity Buses to Support Transbay Tomorrow and Clean Corridors Plan (Facilities and Bus Charging Infrastructure) component (PPNO 2320B), in Alameda County, for the time period as identified in the attachment.

BACKGROUND:

In January of 2022, the Commission allocated \$6,000,000 for the Construction (CON) phase of the TIRCP (2018:01) Purchase Zero Emission High Capacity Buses to Support Transbay Tomorrow and Clean Corridors Plan (Facilities and Bus Charging Infrastructure) component (PPNO 2320B). The responsible agency has been unable to complete the CON phase per the Timely Use of Funds (TUF) policy.

The current TIRCP Allocation Policy stipulates that the agency implementing a project component request a time extension if the project will not be completed within the specified timeline. Under the TUF policy, an extension can be requested for up to 20 months for each

delivery milestone, where permissible by statute. The attachment describes the details of the components and the explanation for the delays.

Attachment

**Time Extension/Waiver – Project Completion
Transit and Intercity Rail Capital Program**

Project Number: 1
Applicant: Alameda Contra Costa Transit District
County: Alameda
District: 4
PPNO: 2320B
Allocation Amount: \$6,000,000
Phase: Construction
Allocation Date: 01/27/2022
Allocation Resolution Number: TIRCP-2122-17
Award Date: 07/26/2022
Number of Months Requested: 20 Months
Extended Deadline: 03/31/2027
Department Recommendation: Support

**(2018:01) Purchase Zero Emission High Capacity Buses to Support Transbay
Tomorrow and Clean Corridors Plan (Facilities and Bus Charging Infrastructure)**

The Alameda Contra Costa Transit District (AC Transit) requests a 20-month time extension for the period of project completion for the Construction (CON) phase of the (2018:01) Purchase Zero Emission High Capacity Buses to Support Transbay Tomorrow and Clean Corridors Plan (Facilities and Bus Charging Infrastructure Project) component. AC Transit has experienced delays in completing the CON phase.

The project component, located in Alameda County, consists of building charging infrastructure for zero-emission buses at AC Transit's Division 2 Emeryville facility and Division 4 East Oakland facility.

To date, the CON phase of the Facilities and Bus Charging Infrastructure component is 50 percent completed. The component includes two charging facilities, Division 2 in Emeryville and Division 4 in East Oakland. AC Transit has completed one of the two charging facilities, the Division 2 Emeryville facility, which is 100 percent complete, functional, and operational. Construction work on the Division 4 charging facility has not begun and is currently at 0 percent as the agency has identified an unexpected need to locate a new powering source at the charging facility. AC Transit is actively working to resolve this issue with the design of a microgrid through another project component, Self Generation of Power at the East Oakland Facility (PPNO 2320C), allocated by the Commission in October 2024. The agency continues to make progress in delivering this infrastructure at the Division 4 East Oakland facility. Additional time is needed to continue the CON work on the battery electric bus charging infrastructure at AC Transit's Division 4 East Oakland facility.

During the CON phase, the project faced unexpected delays due to a third-party energy provider, Pacific Gas and Electric Company (PG&E), that initially confirmed capacity for the

charging infrastructure at AC Transit's District 4 East Oakland facility in April 2022. In November 2022, PG&E could no longer provide power, citing a grid issue, insufficient capacity, and unavailability of grid distribution. Between November 2022 and October 2024, when the Commission allocated the Self Generation of Power at the East Oakland Facility component, AC Transit was continuing negotiations with PG&E, assessing options, and determining a path forward. After considering options, including interconnection to PG&E, not operating battery electric buses, or running fuel cell electric buses, AC Transit resolved to self-generate the power necessary for charging infrastructure at AC Transit's Division 4 East Oakland facility to be in compliance with California Air Resources Board's Innovative Clean Transit mandate and minimize further delays of the project. Completion of the Self Generation of Power component by February 2027 will allow for completion of CON for this Bus Charging Facilities component by March 2027.

Therefore, AC Transit is requesting a 20-month time extension for the period of project completion for the CON phase from July 31, 2025 to March 31, 2027.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8d.(6), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT DEVELOPMENT
EXPENDITURE FOR A TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM
COMPONENT, PER TIRCP ALLOCATION POLICY
WAIVER 25-133**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project development expenditure for the Transit and Intercity Rail Capital Program (TIRCP) (2022:4) Sacramento Valley Station (SVS) Transit Center: Priority Projects (H Street Cycle Track Project On-Street Component) (PPNO CP094A), in Sacramento County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of project development expenditure for the TIRCP (2022:4) Sacramento Valley Station (SVS) Transit Center: Priority Projects (H Street Cycle Track Project On-Street Component) (PPNO CP094A), in Sacramento County, for the time period as identified in the attachment.

BACKGROUND:

In December 2022, the Commission allocated \$850,000 for the Project Approval and Environmental Document phase of the (2022:4) Sacramento Valley Station (SVS) Transit Center: Priority Projects (H Street Cycle Track Project On-Street Component) (PPNO CP094A). The responsible agency has been unable to complete the component per the Timely Use of Funds (TUF) policy.

The current TIRCP Allocation Policy stipulates that the agency implementing a project component request a time extension if the project component will not be completed within the specified timeline. Under the TUF policy, an extension can be requested for up to 20 months

for each delivery milestone, where permissible by statute. The attachment describes the details of the project component and the explanation for the delays.

Attachment

**Time Extension/Waiver – Project Development Expenditure
Transit and Intercity Rail Capital Program**

Project Number: 1
Applicant: City of Sacramento
County: Sacramento
District: 3
PPNO: CP094A
Allocation Amount: \$850,000
Remaining Balance: \$673,777
Phase: Project Approval and Environmental Document
Allocation Date: 12/08/2022
Allocation Resolution: TIRCP-2223-36
Number of Months Requested: 12 Months
Extended Deadline: 06/30/2026
Department Recommendation: Support

(2022:4) Sacramento Valley Station (SVS) Transit Center: Priority Projects (H Street Cycle Track On-Street Component)

The City of Sacramento (City) requests a 12-month time extension for the period of project development expenditure to complete the Project Approval and Environmental Document (PA&ED) phase of the (2022:4) Sacramento Valley Station (SVS) Transit Center: Priority Projects (H Street Cycle Track Project On-Street Component). The City has experienced unexpected delays in completing the PA&ED phase.

This component will construct a two-way cycle track on the south side of H Street, between 5th and 10th Streets. This will improve active transportation between downtown and the SVS. As part of a broader plan for SVS, this bikeway will provide low-stress bike routes between central city neighborhoods and the region's premier rail station, the SVS.

The primary delay is due to the additional time required to coordinate the analysis and evaluations needed to secure the City's Traffic Division's support for reducing the number of vehicle lanes from two to one. Other concurrent projects by partner agencies are being constructed at or near the location of the City's planned two-way cycle track. Sacramento County is completing construction of a new courthouse, and Sacramento Regional Transit (SacRT) is expanding the light rail system west of 7th Street by double-tracking the rails, which has introduced constraints to the existing roadway that the City must now address.

In October 2023, the City learned of SacRT's required realignment of the light rail expansion due to Sacramento County modifying frontage improvements of the new Sacramento County Courthouse. In January 2024, SacRT communicated their engineered track realignment design plans to the City. To accommodate both SacRT's and the County's projects, the City now proposes to reduce the vehicle lanes from two to one. This lane reduction may affect on-street parking and loading zones, requiring engagement with impacted stakeholders, which is critical to the success of the project.

Due to the lack of precedent for such a proposal, additional time is needed to complete the traffic analysis and 30 percent design, as well as coordination with stakeholders. The City has already taken steps to begin the process of contacting stakeholders to discuss the merits of the project and gather feedback. Meetings with the stakeholders are scheduled to take place in summer 2025 and continue through fall of 2025. Additionally, the City has completed traffic analyses to evaluate the impacts of the reduced space available for the two-way cycle track. The analyses have produced three plans that have been developed with a level of detail that can be presented to the public at the summer and fall meetings.

The option to reduce to one vehicle lane was in response to the space limitations posed by SacRT's new alignment and allows for the original scope for the two-way separated bikeway to be implemented using best practices and appropriate facility widths. Currently, the project has reached 50 percent completion of the PA&ED phase, which includes 30 percent plans, as well as the filing of the California Environmental Quality Act documentation.

Therefore, the City is requesting a 12-month time extension for the period of project development expenditure for the PA&ED phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8d.(7), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO EXTEND THE PERIOD OF PROJECT DEVELOPMENT
EXPENDITURE FOR A TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM
COMPONENT, PER TIRCP ALLOCATION POLICY
WAIVER 25-149**

ISSUE:

Should the California Transportation Commission (Commission) extend the period of project development expenditure for the Transit and Intercity Rail Capital Program (TIRCP) (2022:6) Making a Beeline for Electrification (Parking Deck Build Enhancements) component (PPNO CP082), in Los Angeles County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of project development expenditure for the TIRCP (2022:6) Making a Beeline for Electrification (Parking Deck Build Enhancements) component (PPNO CP082), in Los Angeles County, for the time period as identified in the attachment.

BACKGROUND:

In October 2022, the Commission allocated \$750,000 for the Plans, Specifications, and Estimate phase of the TIRCP (2022:6) Making a Beeline for Electrification (Parking Deck Build Enhancements) component (PPNO CP082). The responsible agency has been unable to complete the component phase per the Timely Use of Funds (TUF) policy. The current TIRCP Allocation Policy stipulates that the agency implementing a project component request a time extension if the project component will not be completed within the specified timeline. Under the TUF policy, an extension can be requested for up to 20 months for each delivery milestone, where permissible by statute. The attachment describes the details of the component and the explanation for the delays.

Attachment

**Time Extension/Waiver – Project Development Expenditure
Transit and Intercity Rail Capital Program**

Project Number: 1
Applicant: City of Glendale
County: Los Angeles
District: 7
PPNO: CP082
Allocation Amount: \$750,000
Remaining Balance: \$33,343,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 10/13/2022
Allocation Resolution: TIRCP-2223-30
Number of Months Requested: 20 Months
Extended Deadline: 02/28/2027
Department Recommendation: Support

(2022:6) Making a Beeline for Electrification (Parking Deck Build Enhancements)

The City of Glendale (City) requests a 20-month time extension for the period of project development expenditure for the Plans, Specifications, and Estimate (PS&E) phase of the Making a Beeline for Electrification (Parking Deck Build Enhancements) component. The City experienced delays during the PS&E phase.

The project component includes the design for a parking deck within the existing right of way, with a solar panel canopy over an existing bus parking area. The enhanced parking area will accommodate the necessary electrification infrastructure required to charge the battery-electric buses.

In May 2025, prior to finalizing their designs, the City was notified by their engineers that the cost estimates for the project component were nearly double the originally calculated amount.

Therefore, the current design plan will need to be adjusted and cannot be finalized by the June 30, 2025 deadline. The City requires additional time to further analyze the current design plan and make necessary revisions to keep costs within budget, which would require an extension for the period of project development expenditure.

Therefore, the City is requesting a 20-month time extension for the period of project development expenditure for the PS&E phase from June 30, 2025 to February 28, 2027.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(1), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **POST-FACT REQUEST TO AMEND THE PERIOD OF PROJECT COMPLETION
FOR A STATE HIGHWAY OPERATION AND PROTECTION PROGRAM
PROJECT, AS AN EXCEPTION TO THE SHOPP GUIDELINES
WAIVER 25-135, AMENDING WAIVER 24-147**

ISSUE:

Should the California Transportation Commission (Commission) approve a Post-Fact time extension amendment for the period of project completion for the State Highway Operation and Protection Program (SHOPP) Transportation Management Systems project (PPNO 3230), in San Joaquin County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve a Post-Fact time extension amendment for the period of project completion for the SHOPP Transportation Management Systems project (PPNO 3230), in San Joaquin County, for the time period as identified in the attachment.

BACKGROUND:

In August 2021, the Commission allocated \$6,566,000 for the Construction Capital and Construction Support phase for the SHOPP Transportation Management Systems project (PPNO 3230). This project was awarded in October 2021. In accordance with the SHOPP Guidelines (Guidelines), the deadline to complete the Construction Contract Acceptance for projects awarded in October 2021 is October 31, 2024. In October 2024, the project received an additional six months for project completion, extending the deadline to April 30, 2025. Because the Department did not meet this deadline, the Department is now requesting an exception to the Guidelines for a Post-Fact time extension amendment to complete construction. The attachment shows the details of the project and the delays that have resulted in this Post-Fact time extension amendment request.

Current Guidelines stipulate that the agency implementing a project request a time extension if the project will not be completed within 36 months after award. The Commission may approve waivers to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

**Post-Fact Amendment Time Extension/Waiver – Project Completion for
State Highway Operation and Protection Program**

Project Number: 1
County: San Joaquin
District: 10
PPNO: 3230
EA: 1C960
Route: 120
Allocation Amount: \$6,566,000
Phase: CON/CON Support
Allocation Date: 08/18/2021
Allocation Resolution: FP-21-04
Award Date: 10/20/2021
1st Waiver: Waiver 24-147
Approved: October 2024
Number of Months Requested: ~~6 Months~~ **12 Months**
Extended Deadline: ~~04/30/2025~~ **10/31/2025**
Department Recommendation: Support

SHOPP Transportation Management Systems project

The Department requests an additional 6 months, from 6 months to 12 months, for the period of project completion for the Construction (CON) phase of the SHOPP Transportation Management Systems project. The Department has experienced unexpected delays in completing construction.

The project is located on State Route (SR) 120, in and near Manteca and Lathrop, from Interstate 5 to SR 99. The project proposes to install fiber optic cable as part of the Department's Transportation Management System.

Initially, the project received a 6-month extension to resolve a damaged fiber optic line. The contract was awarded in October 2021 and the 36-month deadline to complete Construction Contract Acceptance (CCA) was October 31, 2024. During construction, the contractor ran over and damaged a fiber optic cable while working on the site. This damaged fiber optic line is required to be fully functional with the newly installed project elements. Based on communication with the Contractor and Construction Support team, replacement order and installation of the damaged line was anticipated to take an additional three to six months. The remaining project scope was to be completed concurrently.

Since then, the project experienced further delays in late March 2025, primarily due to the absence of factory test results for the fiber optic cables. These test results are essential for evaluating the performance of the installed fiber optic system as part of the project's acceptance process. The contractor contacted the fiber optic supplier to request a copy of the factory test results. Unfortunately, the supplier was unable to provide them, as the records were no longer available. After discussions between the Department and the contractor on how to proceed, the testing and inspection will rely on the delivered test results in lieu of the factory test results. The

delivered test result includes testing data collected by the contractor from the fiber optic cable at the time of delivery to the job site. The timing of this issue and impacts to the construction schedule were not fully known in time to request action at the May CTC meeting and resulted in a late submittal. The remaining work was further delayed by a heavy rain event in April 2025 that prevented testing and caused water accumulation in several conduit vaults. This resulted in further delays, as the water must be removed from the conduits before conduit sealing plugs can be installed. However, the sealing plugs specified in the plans are no longer available and locating and ordering the appropriate sealing plugs further delayed the project. A special order was placed for the appropriate sealing plugs, and they are expected to arrive in early fall of 2025. It is estimated that it will take the contractor three to four months to complete fiber optic testing and install the sealing plugs once the materials are delivered. Additional time is needed to complete this work and finalize the project.

Therefore, the Department is requesting an additional 6-month Post-Fact time extension amendment to allow sufficient time to complete construction and reach CCA from April 30, 2025 to October 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(2), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO AMEND THE PERIOD OF PROJECT ALLOCATION FOR
STATE HIGHWAY OPERATION AND PROTECTION PROGRAM PROJECTS, AS
AN EXCEPTION TO THE SHOPP GUIDELINES
WAIVER 25-136, AMENDING WAIVERS 24-125, 24-199 and 25-16**

ISSUE:

Should the California Transportation Commission (Commission) amend the period of project allocation for 10 State Highway Operation and Protection Program (SHOPP) projects, as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission amend the period of project allocation for 10 SHOPP projects, as identified in the attachment.

BACKGROUND:

The Department will not be able to request an allocation of funds by the previously approved deadline as detailed in the attachment. Current SHOPP Guidelines stipulate that the Commission may approve waivers to the Timely Use of Funds deadline, one-time only, for up to 20 months. This request is to provide an exception per the SHOPP Guidelines.

Attachment

**Amendment Time Extension/Waiver - Project Allocation
State Highway Operation and Protection Program**

Project Number: 1
County: Placer
District: 03
PPNO: 5290
EA: 4H980
Allocation Amount: \$4,160,000
Phase: Construction and Construction Support
1st Waiver: Waiver 24-125
Approved: August 16, 2024
Number of Months Requested: ~~12 months~~ **14 months**
Extended Deadline: ~~6/30/2025~~ **8/31/2025**
Department Recommendation: Support

EA 03-4H980 (Project ID: 0319000129): Pla-89, PM 8.9

The Department requests an amendment for an additional 2 months, from 12 months to 14 months, for the allocation of the Construction (CON) phase of the SHOPP Maintenance Facilities project in Placer County.

The Commission approved a 12-month time extension for project allocation at the August 2024 Commission meeting that will expire on June 30, 2025. The initial request was needed for the State Fire Marshal (SFM) to review the plans, and it was estimated that it would take SFM four to six months to complete the review process. Following submittal to SFM, the new SFM reviewing standards were being held to a different standard than previously encountered for this type of work, requiring five rounds of submittals for SFM approval.

Therefore, the Department is requesting an exception to the SHOPP Guidelines, for an additional two months to allocate CON from June 30, 2025 to August 31, 2025.

Project Number: 2
County: Santa Barbara
District: 05
PPNO: 2667
EA: 1H610
Allocation Amount: \$22,796,000
Phase: Construction and Construction Support
1st Waiver: Waiver 24-125
Approved: August 16, 2024
Number of Months Requested: ~~12 months~~ **14 months**
Extended Deadline: ~~6/30/2025~~ **8/31/2025**
Department Recommendation: Support

EA 05-1H610 (Project ID: 0516000097): SB-1, PM R36.3/49.2

The Department requests an amendment for an additional 2 months, from 12 months to 14 months, for the allocation of the CON phase of the SHOPP Safety Improvement project near Orcutt.

The Commission approved a 12-month time extension for project allocation at the August 2024 Commission meeting that will expire on June 30, 2025. The initial request was needed for the relocation of approximately 100 utility poles, the majority of which are owned by Pacific Gas and Electric Company (PG&E). The project is expected to reach Ready To List (RTL) on June 30, 2025, delaying the CON and CON Support (Sup) allocations to August 2025.

Therefore, the Department is requesting an exception to the SHOPP Guidelines, for an additional two months to allocate CON from June 30, 2025 to August 31, 2025.

Project Number: 3
County: Santa Cruz
District: 05
PPNO: 2843B
EA: 1J962
Allocation Amount: \$12,851,000
Phase: Construction and Construction Support
1st Waiver: Waiver 24-125
Approved: August 16, 2024
Number of Months Requested: ~~14 months~~ **19 months**
Extended Deadline: ~~8/31/2025~~ **1/31/2026**
Department Recommendation: Support

EA 05-1J962 (Project ID: 0523000198): SCR-1, PM 10.6/ 26.0

The Department requests an amendment for an additional 5 months, from 14 months to 19 months, for the allocation of the CON phase of the SHOPP Drainage Improvement project near Capitola and the City of Santa Cruz.

The Commission approved a 14-month time extension for project allocation at the August 2024 Commission meeting that will expire on August 31, 2025. The initial request was needed for Coastal Development Permits (CDPs) through three different jurisdictions. As conflicts were resolved, it was determined it was in the Department's best interest to combine this project under one Plans, Specifications, and Estimate (PS&E) package and later combine for construction with related second child project EA 05-1J961 as EA 05-1J96U. EA 05-1J961 was reacquired from the Santa Cruz Regional Transportation Commission to mitigate delays the second child was experiencing due to development with a non-SHOPP project EA 05-0C734. RTL is anticipated for mid-November 2025.

Therefore, the Department is requesting an exception to the SHOPP Guidelines, for an additional five months to allocate CON from August 31, 2025 to January 31, 2026.

Project Number: 4
County: Santa Cruz
District: 05
PPNO: 2775
EA: 1J830
Allocation Amount: \$10,344,000
Phase: Construction and Construction Support
1st Waiver: Waiver 24-125
Approved: August 16, 2024
Number of Months Requested: ~~12 months~~ **14 months**
Extended Deadline: ~~6/30/2025~~ **8/31/2025**
Department Recommendation: Support

EA 05-1J830 (Project ID: 0518000078): SCR-129, PM L0.0/ 0.6

The Department requests an amendment for an additional 2 months, from 12 months to 14 months, for the allocation of the CON phase of the SHOPP Pavement Improvement project near Watsonville.

The Commission approved a 12-month time extension for project allocation at the August 2024 Commission meeting that will expire on June 30, 2025. The initial request was needed for approximately 25-30 deeds to reach Right of Way Certification (R/W Cert). All parcels have been acquired. Meanwhile, progress in obtaining the Right of Entry (ROE) railroad agreement needs additional time to be fully executed. Also, this additional time will be used to resolve design challenges with traffic design and traffic handling plans. It is anticipated RTL will be reached on June 9, 2025.

Therefore, the Department is requesting an exception to the SHOPP Guidelines, for an additional two months to allocate CON from June 30, 2025 to August 31, 2025.

Project Number: 5
County: Kern
District: 06
PPNO: 6960
EA: 0W810
Allocation Amount: \$10,800,000
Phase: Construction and Construction Support
1st Waiver: Waiver 24-125
Approved: August 16, 2024
Number of Months Requested: ~~12 months~~ **19 months**
Extended Deadline: ~~6/30/2025~~ **1/31/2026**
Department Recommendation: Support

EA 06-0W810 (Project ID: 0617000303): Ker-155, PM R0.0/R1.3

The Department requests an amendment for an additional 7 months, from 12 months to 19 months, for the allocation of the CON phase of the SHOPP Pavement Improvement project in Delano.

The Commission approved a 12-month time extension for project allocation at the August 2024 Commission meeting that will expire on June 30, 2025. The initial request was needed for a construction and maintenance (C&M) agreement. In April 2025, a continued delay from the prolonged railroad coordination for which the railroad requested additional indemnification language has continued for several iterations and revisions. The agreement is now pending approval by both parties' legal counsel. For perspective, the railroad coordination began in November 2021. The revised RTL date is December 2025.

Therefore, the Department is requesting an exception to the SHOPP Guidelines, for an additional seven months to allocate CON from June 30, 2025 to January 31, 2026.

Project Number: 6
County: San Bernardino
District: 08
PPNO: 3021K
EA: 1N590
Allocation Amount: \$87,363,000
Phase: Construction and Construction Support
1st Waiver: Waiver 24-125
Approved: August 16, 2024
Number of Months Requested: ~~12 months~~ **19 months**
Extended Deadline: ~~6/30/2025~~ **1/31/2026**
Department Recommendation: Support

EA 08-1N590 (Project ID: 0823000021): SBD-15, PM R114.0/171.5

The Department requests an amendment for an additional 7 months, from 12 months to 19 months, for the allocation of the CON phase of the SHOPP Sustainability and Miscellaneous project near Baker.

The Commission approved a 12-month time extension for project allocation at the August 2024 Commission meeting that will expire on June 30, 2025. The initial request was needed to accommodate Brightline West's (BW) procurement schedule for their high-speed rail project. Additional time is needed to coordinate the procurement schedule with BW for finalizing design, advertising, and awarding to a contractor by winter 2025.

Therefore, the Department is requesting an exception to the SHOPP Guidelines, for an additional seven months to allocate CON from June 30, 2025 to January 31, 2026.

Project Number: 7
County: San Bernardino
District: 08
PPNO: 3017K
EA: 1L800
Allocation Amount: \$2,031,000
Phase: Plans, Specifications, and Estimate and Right of Way Support
1st Waiver: Waiver 24-125
Approved: August 16, 2024
Number of Months Requested: ~~12 months~~ **16 months**
Extended Deadline: ~~6/30/2025~~ **10/31/2025**
Department Recommendation: Support

EA 08-1L800 (Project ID: 0820000156): SBD-40, PM R104.6/R120.5

The Department requests an amendment for an additional 4 months, from 12 months to 16 months, for the allocation of the PS&E and R/W Sup phases of the SHOPP Bridge Formula Program project near Needles.

The Commission approved a 12-month time extension for project allocation at the August 2024 Commission meeting that will expire on June 30, 2025. The initial request was needed to complete a Future Consideration of Funding to complete the Environmental Certificate. Additionally, there has been an unanticipated delay associated with the Draft Environmental Document (DED). The project requires extension of Rock Slope Protection limits at various locations which has triggered an additional 45 days review period from United States Fish and Wildlife Services to obtain Desert Tortoise Programmatic Biological Opinion, causing additional changes to the DED, and further delaying completion of the Project Approval and Environmental Document phase.

Therefore, the Department is requesting an exception to the SHOPP Guidelines, for an additional four months to allocate PS&E and R/W Sup from June 30, 2025 to October 31, 2025.

Project Number: 8
County: Mono
District: 09
PPNO: 2645
EA: 37430
Allocation Amount: \$21,140,000
Phase: Construction and Construction Support
1st Waiver: Waiver 24-125
Approved: August 16, 2024
Number of Months Requested: ~~12 months~~ **14 months**
Extended Deadline: ~~6/30/2025~~ **8/31/2025**
Department Recommendation: Support

EA 09-37430 (Project ID: 0918000015): Mno-395, PM 50.6/53.1

The Department requests an amendment for an additional 2 months, from 12 months to 14 months, for the allocation of the CON phase of the SHOPP New Curb Ramps project near Lee Vining.

The Commission approved a 12-month time extension for project allocation at the August 2024 Commission meeting that will expire on June 30, 2025. The initial request was needed to coordinate with Mono County and the Lee Vining Public Utility District, to secure funding to add to Department plans, and the replacement of their water line. This coordination has proven to be complex and is taking more time than originally anticipated, resulting in the need to amend the project schedule by an additional two months. The reasoning for this delay is Mono County and the Lee Vining Public Utility District requires additional time to secure funding to pay for the Department's construction and support costs to replace the water/sewer line, sign a cooperative agreement, and provide PS&E approval. The board of supervisors recently approved funding and the cooperative agreement has been routed for final signature. The Department is currently working closely with the utility district to incorporate this work into the project plans. These tasks are expected to be completed by June 2025.

Therefore, the Department is requesting an exception to the SHOPP Guidelines, for an additional two months to allocate CON from June 30, 2025 to August 31, 2025.

Project Number: 9
County: Alpine
District: 10
PPNO: 1044
EA: 0J720
Allocation Amount: \$41,887,000
Phase: Construction and Construction Support
1st Waiver: Waiver 24-125
Approved: August 16, 2024
2nd Waiver: Waiver 24-199
Approved: December 6, 2024
Number of Months Requested: ~~6 months~~ ~~14 months~~ **16 months**
Extended Deadline: ~~4/31/2024~~ ~~8/31/2025~~ **10/31/2025**
Department Recommendation: Support

EA 10-0J720 (Project ID: 1018000271): Alp-4, PM R0.0/31.7

The Department requests an amendment for an additional 2 months, from 14 months to 16 months, for the allocation of the CON phase of the SHOPP Pavement Improvement project near Bear Valley.

The Commission approved a 6-month time extension for project allocation at the August 2024 Commission meeting and an amended time extension for an additional 8 months was approved at the December 2024 Commission meeting that will expire on August 31, 2025. The initial request was needed for obtaining concurrence from the Cultural Study Office and State Historic Preservation Officer. The amended time extension was needed for the Resolution of Necessity (RON) process of parcel no. 17537. In the meantime, the R/W team has acquired four signed agreements, removed six parcels, and received the RON approval at the January 2025 Commission meeting. An additional 2-month delay is due to the utility easement deed preparation for PG&E and a request for Federal Highway Administration (FHWA) review sent in late April 2025 that carries a very high risk. The anticipated RTL date is August 13, 2025.

Therefore, the Department is requesting an exception to the SHOPP Guidelines, for an additional two months to allocate CON from August 31, 2025 to October 31, 2025.

Project Number: 10
County: Calaveras
District: 10
PPNO: 3494
EA: 1K820
Allocation Amount: \$8,657,000
Phase: Construction and Construction Support
1st Waiver: Waiver 24-125
Approved: August 16, 2024
2nd Waiver: Waiver 25-16
Approved: January 31, 2025
Number of Months Requested: ~~9 months~~ ~~14 months~~ **16 months**
Extended Deadline: ~~3/31/2025~~ ~~8/31/2025~~ **10/31/2025**
Department Recommendation: Support

EA 10-1K820 (Project ID: 1019000164): Cal-26, PM 18.1

The Department requests an amendment for an additional 2 months, from 14 months to 16 months, for the allocation of the CON phase of the SHOPP Safety Improvement project near Mokelumne Hill.

The Commission approved a 9-month time extension for project allocation at the August 2024 Commission meeting and an amended time extension for an additional 5 months was approved at the January 2025 Commission meeting that will expire on August 31, 2025. The initial request was needed to finalize six property acquisitions. The continued work with the Stanislaus National Forest Service (SNFS) that began in September 2023 to expedite the required Temporary Construction Easement (TCE) permits was not successful, in part due to forest fires at the time. Incorporation of the permit into the contract documents was initially estimated to take three months but this non-standard specification is taking longer than estimated. An additional two months is needed to prepare for an RTL of August 20, 2025, delaying the CON and CON Sup allocations to October 2025.

Therefore, the Department is requesting an exception to the SHOPP Guidelines, for an additional two months to allocate CON from August 31, 2025 to October 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(3), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO AMEND THE PERIOD OF PROJECT COMPLETION FOR
STATE HIGHWAY OPERATION AND PROTECTION PROGRAM PROJECTS, AS
AN EXCEPTION TO THE SHOPP GUIDELINES
WAIVER 25-137, AMENDING WAIVERS 25-18, 25-23, AND 25-89**

ISSUE:

Should the California Transportation Commission (Commission) amend the period of project completion for three State Highway Operation and Protection Program (SHOPP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission amend the period of project completion for three SHOPP projects, for the time periods as identified in the attachment.

BACKGROUND:

In June, October 2020, and August 2021 the Commission allocated \$124,920,000 in Construction Capital and Support for the three SHOPP projects. The projects have received previous time extensions for the period of project completion. However, the Department is requesting an exception to the SHOPP Guidelines (Guidelines) for additional time for the period of project completion. The attachment shows the details of the projects and the delays that have resulted in the amended time extension requests.

Current Guidelines stipulate that the Department request a time extension if the project will not be completed within 36 months after award. The Commission may approve waivers to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

**Amendment Time Extension/Waiver – Project Completion for
State Highway Operation and Protection Program**

Project Number: 1
County: Orange
District: 12
PPNO: 2859B
EA: 0P42U
Route: Var
Allocation Amount: \$20,720,000
Phase: CON/CON Support
Allocation Date: 06/24/2020
Award Date: 09/15/2020
1st Waiver: Waiver 23-133
Approved: August 2023
2nd Waiver: Waiver 25-18
Approved: January 2025
Number of Months Requested: ~~16 Months~~ ~~20 Months~~ **26 Months**
Extended Deadline: ~~01/31/2025~~ ~~05/31/2025~~ **11/30/2025**
Department Recommendation: Support

SHOPP Mobility project

The Department requests an additional 6 months, from 20 months to 26 months, for the period of project completion for the Construction (CON) phase of the SHOPP Mobility project. The Department has experienced unexpected delays in completing construction.

This project is in the Cities of Orange, Anaheim, Santa Ana, Fullerton, and Buena Park on Interstate 5, and on State Route (SR) 57 and SR 91. The project intends to install and modify Intelligent Transportation System (ITS) elements. This project has been combined with SHOPP project EA 12-0P672 under EA 12-0P42U for construction.

Initially, the project experienced delays due to the COVID-19 Pandemic (Pandemic). The project proposed to install and modify ITS elements within Department facilities, and at various locations along various routes. This work required close construction coordination to keep existing elements operational, while new elements were being installed. During these early months of the Pandemic, the project team was unable to have in-person meetings to effectively support and address issues in a timely manner. As a result, there were delays in mobilizing equipment and procuring materials. The procurement of materials was further complicated and delayed, due to shortages and longer-than-normal lead times for electrical components. A major project component involved replacing changeable message signs along the routes. These were initially slated to be Department-furnished items but were modified to be contractor-furnished. This required processing and approval of a public interest finding and modifications to the plans and specifications. This delayed the project by six months. In addition, the installation of other project elements at various locations were also delayed due to severe storm events limiting construction activities.

Construction was well under way, and the Department anticipated reaching Construction Contract Acceptance (CCA) by January 2025. However, four additional months were required due to vandalism of ITS elements, constructability concerns and resolving discrepancies in the plans. The project experienced significant vandalism of both existing and newly installed ITS elements. This required the contractor to spend additional time to investigate the damage and implement the warranted repair. During construction, the contractor also identified constructability concerns with the contract plans due to uneven terrain. As a result, additional time was needed to prepare the revised contract plan. In addition, it was discovered that a conduit run and pull box for fiber optic cables, which were supposed to be present in the field per the project contract plans, were missing. The Department anticipated resolving these issues and reaching CCA by May 2025.

Since then, the project has experienced unexpected issues that are causing further delays. The project continues to be plagued by vandalism, copper wire theft, and damage to pull boxes. This has stopped the progress of work while the damage is repaired. The construction team is taking aggressive measures to prevent future vandalism. Some of these measures include utilizing tamper resistant bolted traffic pull boxes, buried pull boxes in concrete and welded pull boxes. Another issue includes some of the proposed equipment on the traffic signal poles. A special lowering ring used to hold cameras isn't working as expected. Fixing this issue requires hiring a specialized contractor and will result in adding more time to the schedule. In addition, the project includes installing three communication devices called Roadside Units (RSU) on poles that are being built under a separate contract (EA 12-0734U). The poles installation is expected to be completed in two months which is holding back the installation of the RSUs. In addition, the Department is waiting for Anaheim Public Utilities to connect two new power services. Without those connections, the project cannot power, inspect, and clear the transportation systems that are already installed. The Department is prioritizing the remaining work and anticipates completing all construction activities by November 2025.

Therefore, the Department is requesting an exception to the Guidelines for an additional six months to finalize CCA from May 31, 2025 to November 30, 2025.

Project Number: 2
County: Fresno
District: 6
PPNO: 6800
EA: 0U420
Route: 99
Allocation Amount: \$92,500,000
Phase: CON/CON Support
Allocation Date: 10/21/2020
Award Date: 02/25/2021
1st Waiver: Waiver 24-26
Approved: March 2024
2nd Waiver: Waiver 25-18
Approved: January 2025
3rd Waiver: Waiver 25-89
Approved: May 2025
Number of Months Requested: ~~5 Months 8 Months 10 Months~~ **13 Months**
Extended Deadline: ~~12/31/2024 03/31/2025 05/31/2025~~ **08/31/2025**
Department Recommendation: Support

SHOPP Roadway Preservation project

The Department requests an additional 3 months, from 10 months to 13 months, for the period of project completion for the CON phase of the SHOPP Roadway Preservation project. The Department has experienced unexpected delays in completing construction.

This project is located in and near the cities of Selma and Fowler, from McCall Avenue Undercrossing to 0.5 miles north of Merced Street Undercrossing. The project will replace pavement with Continuous Reinforced Concrete Pavement (CRCP), and update curb ramps to current standards.

At the time of construction allocation, the project received an additional five months for project completion. In March 2024, the project received a 5-month time extension; however, that extension omitted the additional five months provided at the time of allocation. Therefore, the project had a deadline of December 31, 2024 to reach CCA.

Initially, the project began construction in the spring of 2021 and was scheduled to be completed within three construction seasons. One of the main components for this project is concrete. Due to the supply chain issues, there was a nationwide cement shortage. This shortage minimized production rates for the CRCP operations as the frequency of loads was severely diminished. Further delays were encountered due to major winter storms that impacted the area in late 2022 and early 2023. The storms resulted in additional repairs to the existing facility and some recently completed work had to be redone. These impacts resulted in a delay of several months to the project.

The Department had moved to accelerate work wherever possible, and reach CCA by December 2024. However, during Cast-In-Drilled-Hole (CIDH) drilling operations for the sign piles, unsuitable material was discovered, which was an unforeseen soil condition. The soil

condition type caused the CIDH to cave in at multiple locations while pouring concrete. This caused an additional delay to the project schedule as mitigation efforts had to be used to prevent this from re-occurring. The Department anticipated completing the other bid items and the punch list in three months to reach CCA by March 2025. However, the rain and weather activities further delayed the project affecting the pavement. The pavement at the southbound on and off ramps of Manning Avenue failed after the cold planing and overlay were installed. This was due to their prolonged use for stage construction and weather events. Additional time was needed to correct the ramps before reaching CCA.

Since that time, the contractor has failed to prioritize this project, resulting in daily liquidated damages and delays to project completion. The main impacts have been to the paving operations to correct the on and off ramps. The project's schedule was nearing the extended deadline, and the rework had yet to be started. The Department has held elevated meetings with the contractor's management team to address the delays. As a result of the elevated meetings, the contractor submitted a schedule indicating that paving work is planned for the week of June 8, 2025. This schedule will allow the project to be completed by August 2025.

Therefore, the Department is requesting an exception to the Guidelines for an additional three months to finalize CCA from May 31, 2025 to August 31, 2025.

Project Number: 3
County: Alameda
District: 4
PPNO: 0050N
EA: 4A800
Route: 880
Allocation Amount: \$11,700,000
Phase: CON/CON Support
Allocation Date: 08/19/2021
Award Date: 03/14/2022
1st Waiver: Waiver 25-23
Approved: March 2025
Number of Months Requested: ~~3 Months~~ **9 Months**
Extended Deadline: ~~06/30/2025~~ **12/31/2025**
Department Recommendation: Support

SHOPP Bridge Preservation project

The Department requests an additional 6 months, from 3 months to 9 months, for the period of project completion for the CON phase of the SHOPP Bridge Preservation project. The Department has experienced unexpected delays in completing construction.

This project is located on SR 880, in the City of Oakland, near Lake Merritt Channel Bridge No. 33-0027 and 5th Avenue. This project proposes to replace the existing Hanlon Lead Railroad (HLR) Bridge that crosses Lake Merritt Channel. The replacement bridge will be constructed north of the existing HLR bridge. This is a mitigation project under combined EA 04-1706U.

Initially, the construction of this project was delayed since September 2022 due to delays in utility relocation and the Union Pacific Railroad (UPRR) maintenance contract. The utility relocation work involved two underground Kinder Morgan (KM) petroleum pipelines and an underground Lumen fiber optic line. These utilities were originally scheduled to be relocated before July 2022 by a single contract administered by KM. However, due to delays with obtaining environmental permits and contracting issues, KM could not relocate the two petroleum pipelines until December 2022. The remaining Lumen fiber optic line still required relocation. The Department was preparing to begin construction in January 2023, when UPRR informed the Department that they would proceed with their own project to replace two existing mainline bridge superstructures with two wider structures via a maintenance contract, while Lumen also pursued a new contractor to relocate the fiber optic line.

The Department elected to delay their railroad bridge construction contract until the summer of 2023 upon expected completion of the UPRR maintenance contract and Lumen fiber optic relocation. However, due to schedule delays, the UPRR maintenance contract and Lumen fiber optic conduit relocation contract were not completed until January 2024 and February 2024, respectively.

Due to changes resulting from UPRR's new mainline railroad bridges' alignments and its wider superstructure, UPRR requested the Department to resurvey and verify the proposed railroad bridge design, including verification of the centerline offset and clearance between the adjacent mainline bridge and the proposed bridge. The Department completed the survey in April 2024 and submitted the revised plans to UPRR in June 2024. The survey identified a new conflict between the Department's proposed railroad bridge foundation and the adjacent mainline bridge's steel barrier that UPRR contractor used to temporarily place UPRR signal and fiber optic lines during the maintenance contract.

The Department requested UPRR relocate these utilities to a permanent location and expedite the approval of the revised plans by early May 2025. The project was anticipated to begin construction in June 2025 with a target completion date of December 2025. However, due to the described delays, and the potential for further delays in the relocation of UPRR's utilities, additional time was needed to coordinate with UPRR and evaluate delivery options.

Since then, the Department has been working with UPRR to reach agreement on a mitigation alternative, including a potential lump sum payment or the reprogramming of another bridge project. The Department has also determined that additional time would be needed due to continued delays with UPRR and the significant cost and schedule risks to complete the construction contract. The Department seeks the additional time extension to December 2025 to reach project completion.

Therefore, the Department is requesting an exception to the Guidelines for an additional six months to finalize CCA from June 30, 2025 to December 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(4), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO AMEND THE PERIOD OF PROJECT DEVELOPMENT
EXPENDITURE FOR STATE HIGHWAY OPERATION AND PROTECTION
PROGRAM PROJECTS, AS AN EXCEPTION TO THE SHOPP GUIDELINES
WAIVER 25-138, AMENDING WAIVERS 24-03, 24-27 AND 24-148**

ISSUE:

Should the California Transportation Commission (Commission) amend the period of project development expenditure for three State Highway Operation and Protection Program (SHOPP) projects, for the time periods as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission amend the period of project development expenditure for three SHOPP projects, for the time periods as identified in the attachment.

BACKGROUND:

In January 2021, March 2021 and October 2021, the Commission allocated \$7,764,000 in Project Development Support for three SHOPP projects identified in the attachment. In accordance with SHOPP Guidelines (Guidelines), the deadline to complete the Plans, Specifications, and Estimate (PS&E) phases for projects allocated in January 2021, March 2021 and October 2021 is January 31, 2024, March 31, 2024, and October 31, 2024, respectively. However, the Department is requesting an exception to the Guidelines for additional time to complete project development expenditures. The attachment shows the details of the projects and the delays that have resulted in these amended time extension requests.

Current Guidelines stipulate that the implementing agency request an expenditure time extension if the project phase will not be completed within 36 months of allocation for the pre-construction phases. The Commission may approve waivers to the timely use of funds deadline, one-time only, for up to 20 months.

Attachment

"Improving lives and communities through transportation."

**Amendment Time Extension/Waiver – Project Development Expenditure for
State Highway Operation and Protection Program**

Project Number: 1
Applicant: Department of Transportation
County: San Luis Obispo
District: 5
PPNO: 2650
EA: 1H440
Route: 1
Allocation Amount: \$3,718,000
Remaining Balance: \$12,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 01/28/2021
1st Waiver: Waiver 24-03
Approved: January 2024
Number of Months Requested: ~~18 Months~~ **20 Months**
Extended Deadline: ~~07/31/2025~~ **09/30/2025**
Department Recommendation: Support

SHOPP Bridge Rehabilitation and Replacement project

The Department requests an additional 2 months, from 18 months to 20 months, for the period of project development expenditure for the PS&E phase of the SHOPP Bridge Rehabilitation and Replacement project. The Department has experienced unexpected delays in awarding this project.

This project is located on State Route (SR) 1, in and near the City of Guadalupe, at the Santa Maria River Bridge No. 49-0042; also, in Santa Barbara County Postmiles 50.3 to 50.6. The project proposes to replace the bridge. The existing bridge will be used for traffic handling during construction and will be demolished soon after. The project proposes to realign the roadway and construct the new bridge adjacent to the existing structure.

Initially, the United States Army Corps of Engineers (USACE) indicated that piers for the new bridge would be allowed on the existing Santa Maria River levee, similar to the existing bridge. Following completion of 60 percent of PS&E in July 2022, the USACE indicated that the piers would no longer be allowed within the levee. As a result, the bridge was redesigned, lengthened, and raised to ensure that the current pier placement would not interfere with existing levees. This effort resulted in an 18-month delay. While the Department made every effort to complete the redesign work efficiently, the schedule and resources originally planned were insufficient. The project received supplemental funding for this phase at the March 2024 Commission meeting.

Since then, the project was advertised in April 2025 and is expected to reach approved contract by September 2025. The additional time is needed to allow for further review, award, and to finalize contract processing to begin the construction activities.

Therefore, the Department is requesting an exception to the Guidelines for an additional two months to allow enough time to award the project, from July 31, 2025 to September 30, 2025.

Project Number: 2
Applicant: Department of Transportation
County: Monterey
District: 5
PPNO: 2850
EA: 1K020
Route: 1
Allocation Amount: \$1,046,000
Remaining Balance: \$289,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 10/14/2021
1st Waiver: Waiver 24-148
Approved: October 2024
Number of Months Requested: ~~9 Months~~ **14 Months**
Extended Deadline: ~~07/31/2025~~ **12/31/2025**
Department Recommendation: Support

SHOPP Major Damage (Permanent Restoration) project

The Department requests an additional 5 months, from 9 months to 14 months, for the period of project development expenditure for the PS&E phase of the SHOPP Major Damage (Permanent Restoration) project. The Department has experienced unexpected delays in completing the PS&E phase.

This project is located on SR 1 near Gorda, from north of White Creek Bridge to south of Los Burros Road. The project proposes to realign the highway, replace temporary safety features with permanent safety devices and install erosion control measures.

Initially, completion of the PS&E phase was delayed due to acquisition of the required Right of Way (RW). The property owner was not cooperative, and the parcel was acquired through condemnation. The Resolution of Necessity was adopted at the October 2023 Commission meeting followed by a hearing, and final order of possession was received on July 27, 2024. The RW Certification was expected to be completed by September 1, 2024. The California Coastal Commission (CCC) was not willing to issue the Coastal Development Permit (CDP) until RW was acquired. As part of the CDP, a 3-year plant establishment period to remove invasive plants was included. The CDP application was submitted on August 16, 2024, and expected to be approved by February 1, 2025. In addition, last minute design changes occurred due to presence of the underground (U/G) utilities, identified prior to the 95 percent constructability review. Due to misleading information from the utility owner, additional efforts and resources was required to protect the U/G utility line in place.

Since then, the project required a CDP from the CCC. The process to obtain this permit requires more time than initially anticipated. The final CDP was granted in March 2025 at the CCC hearing. Due to the additional time required to obtain the CDP, the Ready to List (RTL) was delayed to the end of April 2025, further delaying the other milestones. The additional time is requested to complete the PS&E phase by December 2025. There is a concurrent Greater Than 120 request on this month's Commission agenda.

Therefore, the Department is requesting an exception to the Guidelines for an additional five months to allow sufficient time to complete the PS&E phase, from July 31, 2025 to December 31, 2025.

Project Number: 3
Applicant: Department of Transportation
County: Kern
District: 6
PPNO: 6803
EA: 0U290
Route: 184
Allocation Amount: \$3,000,000
Remaining Balance: \$146,000
Phase: Plans, Specifications, and Estimate
Allocation Date: 03/25/2021
1st Waiver: Waiver 24-27
Approved: March 2024
Number of Months Requested: ~~17 Months~~ **20 Months**
Extended Deadline: ~~08/31/2025~~ **11/30/2025**
Department Recommendation: Support

SHOPP Roadway Preservation project

The Department requests an additional 3 months, from 17 months to 20 months, for the period of project development expenditure for the PS&E phase of the SHOPP Roadway Preservation project. The Department has experienced unexpected delays in completing the PS&E phase.

This project is located on SR 184, in and near the City of Bakersfield, from Dunnsmere Street to Breckenridge Road. The project proposes to upgrade curb ramps to current standards, install bike lanes, cold plane pavement, place pavement, construct sidewalks, bus turnouts, and improve pedestrian crossings as complete streets elements, drainage improvements, traffic signal system upgrades, and Railroad (RR) crossing upgrades.

The project was initially delayed due to issues with Union Pacific Railroad (UPRR). The project's roadwork crossed over the UPRR facility. The work included upgrading the traffic signal, RR crossing arms, other miscellaneous facilities, and signal coordination. This required coordination with the UPRR as well as the acquisition of easements on UPRR's RW. The Department was regularly meeting and coordinating with the RR representatives to move the process along, but it was time-consuming. UPRR had indicated that completing the Construction and Maintenance Agreement, issuing the permit, and RW acquisition would take up to 18 months to complete. As a result, the Department requested that a 20-month extension be granted for the Construction Capital and Support allocations at the June 2023 Commission meeting.

Since then, the project achieved RTL in December 2024. However, the advertisement was delayed due to discovery of additional Pacific Gas and Electric (PG&E) facilities within the project limits. This discovery and remediation effort required additional coordination with PG&E. To support this work, the Department utilized consultants to assist with development of the utility plans. A revised utility certification was processed for the project documenting the added PG&E facilities. The project was then advertised in April 2025. The target bid opening is June 2025, and contract approval is expected in July 2025. The additional time is being requested to provide sufficient time to process all of consultant invoices incurred through the bidding phase.

Therefore, the Department is requesting an exception to the Guidelines for an additional three months to allow enough time to process and finalize the PS&E phase, from August 31, 2025 to November 30, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(6), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO AMEND THE PERIOD OF PROJECT DEVELOPMENT
EXPENDITURE FOR A STATE-ADMINISTERED SENATE BILL 1 LOCAL
PARTNERSHIP PROGRAM (FORMULAIC) PROJECT, ON THE STATE HIGHWAY
SYSTEM, PER LPP (FORMULAIC) GUIDELINES
WAIVER 25-140, AMENDING WAIVER 24-99**

ISSUE:

Should the California Transportation Commission (Commission) amend the period of project development expenditure for the State-Administered Senate Bill 1 (SB 1) Local Partnership Program (LPP) (Formulaic) Marin Sonoma Narrows (MSN) Contract B7 project (PPNO 0361M), in Marin County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission amend the period of project development expenditure for the State-Administered SB 1 LPP (Formulaic) Marin Sonoma Narrows (MSN) Contract B7 project (PPNO 0361M), in Marin County, for the time period as identified in the attachment.

BACKGROUND:

In June 2022, the Commission allocated \$3,500,000 for the Right of Way (RW) Capital phase of the State-Administered SB 1 LPP (Formulaic) Marin Sonoma Narrows (MSN) Contract B7 project (PPNO 0361M). In accordance with the LPP (Formulaic) Guidelines (Guidelines), the deadline to complete the project development expenditure for projects allocated in June 2022 is June 30, 2024. The Department received a 12-month time extension for the RW Capital phase in June 2024 with an extended deadline of June 30, 2025. Because the Department will not meet this deadline, the Department is requesting a time extension amendment for additional time to complete the RW Capital phase. The attachment shows the details of the project and the delays that have resulted in this time extension amendment request.

Current Guidelines stipulate that funds allocated for project development costs must be expended by the end of the second fiscal year following the fiscal year of allocation. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months.

Attachment

**Amendment Time Extension/Waiver – Project Development Expenditure for
Local Partnership Program (Formulaic)**

Project Number: 1
Applicant: Department of Transportation
County: Marin
District: 4
PPNO: 0361M
EA: 26472
Route: 101
Allocation Amount: \$3,500,000
Remaining Balance: \$2,860,000
Phase: Right of Way Capital
Allocation Date: 06/30/2022
1st Waiver: Waiver 24-99
Approved: June 2024
Number of Months Requested: ~~12 Months~~ **16 Months**
Extended Deadline: ~~06/30/2025~~ **10/31/2025**
Department Recommendation: Support

Marin Sonoma Narrows (MSN) Contract B7 project

The Department requests an additional 4 months, from 12 months to 16 months, for the period of project development expenditure for the Right of Way (RW) Capital phase of the Marin Sonoma Narrows (MSN) Contract B7 project. The Department has experienced unexpected delays in completing the RW Capital phase.

The project is located on State Route (SR) 101, in the City of Novato, from south of the Franklin Avenue Overhead to south of the Marin/Sonoma County Line. The project will widen SR 101 to construct a southbound High Occupancy Vehicle (HOV) lane from south of the Marin/Sonoma County Line to south of the Franklin Avenue Overhead and northbound HOV lane from north of the Atherton Avenue Overcrossing to south of the Marin/Sonoma County line. The project includes roadway and bridge widening for HOV lanes and installing standard shoulders. The project will also upgrade the horizontal and vertical roadway alignment, modify the Redwood Landfill Interchange ramps, and re-stripe Redwood Boulevard for Class II bike lanes.

Initially, while working on the RW Capital phase, the project faced unanticipated delays due to negotiations regarding mitigation. As a result, the project received a 12-month extension in June 2024. The Regional Water Quality Control Board (RWQCB) 401 permit required the Department to mitigate 44 acres of impervious area. Due to limited space within the State right of way, the project was limited to 20 acres of onsite mitigation. The remaining 24 mitigation acres need to be pursued off-site. The delay is attributed to working with the RWQCB and incorporating recent adjustments to the mitigation ratios. The RWQCB was seeking an increase to the original 1:1 ratio for off-site mitigation. This was not anticipated when the project was programmed. After extended coordination, the Department eventually received concurrence from the RWQCB to reduce the ratio to closer to the initial 1:1 ratio. However, this coordination between the Department and the RWQCB resulted in a 6-month delay.

In addition, the Department also partnered with California State Parks for the riparian and channel offsite mitigation, and with the County of Marin (County) for the non-vegetation channel. The riparian trees and habitat impacted by the project were initially going to be mitigated at the Trione-Annadel State Park. However, community concerns and the then recent wildfire in the area resulted in State Parks canceling the project. The Department experienced delays in acquiring a new site. For the non-vegetation channel, the initial mitigation site within the Deer Island Basin Complex owned by the County became unavailable in Winter 2022. This delayed the preparation of a cooperative agreement for the new riparian mitigation site. These delays have collectively impacted the completion of the RW Capital phase work for the project.

Since then, the project encountered further delays with the stormwater mitigation, riparian and non-riparian channel issues and will not meet the June 2025 deadline. The Department partnered with the County to deliver alternative stormwater compliance projects to meet the acreage requirement. However, due to the difference in quality of pollutants between the State facility and the local road, the required acreage increased to just over 27 acres from 21. Since this is more than originally planned for, the County must evaluate and consider new projects to account for the shortfall. This process was further delayed by funding and delivery challenges that pushed some of the proposed alternative stormwater projects beyond the original schedule.

The conceptual design for the riparian channel, habitat and tree mitigation was provided by Point Blue and included a key component of an exclusion fence to prevent cattle from entering the mitigation site. However, the landowner was not in agreement to the full exclusion of the site from cattle grazing. As a result, additional coordination and negotiation with the property owner was needed to address the concerns and coordinate with the RWQCB. A new conceptual plan and cost estimate that allowed for managed grazing within the mitigation area and ensured protection of the mitigation plantings was provided by Point Blue in February 2025.

Mitigation for the non-riparian channel, required by the United States Army Corps of Engineers (USACE) and the RWQCB, was not included in Point Blue's original mitigation project. Potential mitigation sites were explored in collaboration with USACE, North Marin Water District, the County Public Works, Marin Agricultural Land Trust, California State Parks, National Park Service, and the Sonoma Land Trust; however, no suitable location was identified. In April 2024, a property acquired for the project was found to contain wetlands and a non-riparian channel suitable for mitigation and restoration. Initial site assessments began in April 2024, followed by coordination with RWQCB and USACE to confirm eligibility. A wetland delineation was completed in December 2024, and soil testing and property surveys were conducted in February 2025. The site plan is now in the final design phase. The identification, analysis, and ongoing design development have resulted in additional delays. The project was approved for an allocation amendment in May 2025 to allow for non-proportional spending. This will allow the work to continue uninterrupted and utilize the Local Partnership Program (LPP) (Formulaic) funding through October 31, 2025. The remaining RW activities will be delivered with matching local funds. At completion, expenditures will be reviewed to ensure accurate proportional spending. The Department continues to address these items and anticipates completing the RW Capital phase by October 2025.

Therefore, the Department is requesting an additional 4-month time extension for the period of project development expenditure for the RW phase to allow enough time to utilize the LPP (Formulaic) funding from June 30, 2025 to October 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(8), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO AMEND THE PERIOD OF PROJECT DEVELOPMENT
EXPENDITURE FOR A STATE-ADMINISTERED MULTI-FUNDED SENATE BILL 1
TRADE CORRIDOR ENHANCEMENT PROGRAM AND PROPOSITION 1B STATE
ROUTE 99 PROJECT, ON THE STATE HIGHWAY SYSTEM
WAIVER 25-142, AMENDING WAIVER 24-67**

ISSUE:

Should the California Transportation Commission (Commission) amend the period of project development expenditure for the State-Administered multi-funded Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) and Proposition 1B (Prop 1B) State Route 99 (SR 99) South Madera 6 Lane project (PPNO 6297), in Madera County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission amend the period of project development expenditure for the State-Administered multi-funded SB 1 TCEP and Prop 1B SR 99 South Madera 6 Lane project (PPNO 6297), in Madera County, for the time period as identified in the attachment.

BACKGROUND:

In August 2021, the Commission allocated \$7,719,000 in project development support for the Plans, Specifications, and Estimate and Right of Way (RW) (Support and Capital) phases of the State-Administered multi-funded SB 1 TCEP and Prop 1B SR 99 South Madera 6 Lane project (PPNO 6297). In accordance with the TCEP Guidelines (Guidelines), the deadline to complete the RW Support and Capital phases for the project is June 30, 2024. In May 2024, the project received a 12-month time extension for project development expenditure for the RW Support and Capital phases extending the deadline to June 30, 2025. This project will not be able to meet the deadline, and the Department is requesting a project development time

extension amendment for the RW Support and Capital phases. The attachment shows the details of the project and the delays that have resulted in a time extension request.

Current Guidelines stipulate that funds allocated for project development costs must be expended by the end of the second fiscal year following the fiscal year of allocation. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months.

Attachment

**Amendment Time Extension/Waiver – Project Development Expenditure for
Multi-funded SB 1 TCEP and Prop 1B SR 99**

Project Number: 1
Applicant: Department of Transportation
County: Madera
District: 6
PPNO: 6297
EA: 0H220
Route: 99
Allocation Amount: \$7,719,000 (SB 1 TCEP: \$4,659,000 and Prop 1B SR 99: \$3,060,000)
Remaining Balance: \$2,970,000
Phase: Right of Way Support and Capital
Allocation Date: 08/19/2021
1st Waiver: Waiver 24-67
Approved: May 2024
Number of Months Requested: ~~12 Months~~ **41 Months**
Extended Deadline: ~~06/30/2025~~ **11/30/2027**
Department Recommendation: Support

South Madera 6 Lane project

The Department requests an additional 29 months, from 12 months to 41 months, for the period of project development expenditure for the Right of Way (RW) Support and Capital phases of the South Madera 6 Lane project. The Department has experienced unexpected delays in completing the RW Support and Capital phases.

The project is located on State Route (SR) 99, from north of the Fresno County line and Madera County line to south of Avenue 7 and north of Avenue 12. The project will improve goods movement and passenger travel along SR 99 by widening the median and increasing the number of lanes from four lanes to six lanes. The project will also upgrade drainage, construct drainage basins, add median barrier, and increase vertical clearance at one structure.

Initially, the project was delayed due to design modifications, utility relocation efforts, and parcel acquisition efforts. During the acquisition process, property owners requested changes which required modification to the project design. These changes required identifying additional properties to be acquired, which had extended the originally estimated timeline beyond the June 30, 2024, deadline to complete the RW Support and Capital phases. These additional properties and the modifications to the original parcels required more time for the utility verification process, conflict determinations and possible relocation. To determine the exact locations for the buried utilities, potholing was needed to verify and determine if the utilities required relocation. The potholing contract that was developed had expired. Additional time was required for executing a new contract and allowing the contractor to perform the positive utility location. Also, there were several parcels proceeding with the condemnation process. The Resolutions of Necessity (RONs) were addressed at the March 2024, May 2024, and June 2024 Commission meetings. The Department anticipated completing the RW Support and Capital phases by June 30, 2025.

Since then, the project has taken longer than expected due to the expiration and reissuance of a potholing contract, conflicts with a private gas main, design modifications, and the identification of additional impacted properties.

The potholing contract had to be reissued before positive utility verification and relocation efforts could resume. Although this delay was identified earlier, the reissuance took longer than anticipated. This delay impacted coordination with the utility companies on relocation efforts, and moved several milestone dates out. The milestones include key target dates such as Notice-To-Owner, and Utility Agreement approvals. Adding to the delays was a recently discovered conflict between a private gas main and one of the planned drainage basins. The Department's Right of Way team is working closely with the property owner to minimize impacts and determine whether the gas main can be relocated. This unresolved issue is delaying Pacific Gas and Electric Company's work until the new gas service location for this gas main is established. In addition, during the property acquisition process, some affected landowners requested changes to the project's design. These changes led to the identification of additional properties for acquisition, further extending the original timeline. Appraisal reports are currently being updated and the RON for the properties is scheduled for the August 2025 Commission meeting. Should the property owners contest the RONs, the projects would move through the condemnation process and result in further delays. Coordination efforts and finding workable solutions to the right of way needs have taken longer than anticipated. The process involves balancing project needs, utility relocations, and minimizing impacts to the land use aspects of the affected parcels. The Department continues to work diligently with the property owners and anticipates the issues to be resolved by November 2027.

Therefore, the Department is requesting an additional 29-month time extension for the period of project development expenditure to allow enough time to finalize the RW Support and Capital phases, from June 30, 2025 to November 30, 2027.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(9), Action Item

Prepared By: James R. Anderson, Chief
Division of Financial Programming

Subject: **REQUEST TO AMEND THE PERIOD OF PROJECT ALLOCATION FOR A
LOCALLY-ADMINISTERED SENATE BILL 1 TRADE CORRIDOR
ENHANCEMENT PROGRAM PROJECT, ON THE STATE HIGHWAY SYSTEM
WAIVER 25-143, AMENDING WAIVER 24-64**

ISSUE:

Should the California Transportation Commission (Commission) amend the period of project allocation for the locally-administered Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) Interstate 605/Valley Boulevard Interchange Improvements Project (PPNO 4382), in Los Angeles County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission amend the period of project allocation for the locally-administered SB 1 TCEP Interstate 605/Valley Boulevard Interchange Improvements Project (PPNO 4382), in Los Angeles County, for the time period as identified in the attachment.

BACKGROUND:

In May 2024, the Commission approved a 12-month time extension for the period of project allocation for the SB 1 TCEP Interstate 605/Valley Boulevard Interchange Improvements Project (PPNO 4382). The local agency will not be able to request allocation for funds by the June 30, 2025, deadline and is requesting a time extension amendment for project allocation. The attachment provides the details of the project and the explanation for the delays.

Current SB 1 TCEP Guidelines adopted by the Commission, stipulate that funds programmed are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the timely use of funds deadlines, one-time only, for up to 12 months.

Attachment

**Amendment Time Extension/Waiver – Project Allocation
Trade Corridor Enhancement Program**

Project Number: 1
Applicant: Los Angeles County Metropolitan Transportation Authority
County: Los Angeles
District: 7
PPNO: 4382
Allocation Amount: \$33,570,000
Phase: Construction
1st Waiver: Waiver 24-64
Approved: May 2024
Number of Months Requested: ~~12 Months~~ **24 Months**
Extended Deadline: ~~06/30/2025~~ **06/30/2026**
Department Recommendation: Support

Interstate 605/Valley Boulevard Interchange Improvements Project

The Los Angeles County Metropolitan Transportation Authority (LA Metro) requests an additional 12 months, from 12 months to 24 months, for the period of project allocation for the Construction (CON) phase of the Interstate 605/Valley Boulevard Interchange Improvements Project. LA Metro experienced delays in finalizing the project's Ready to List package.

This project is located on the Interstate 605 (I-605)/Valley Boulevard Interchange in Los Angeles County. The project proposes to improve mobility and alleviate capacity constraints. The project includes construction of retaining walls, and sound walls, as well as curb ramps, sidewalks, driveways, signals (traffic and railroad), railroad gates and other vehicular/pedestrian/railroad facilities that will be upgraded to comply with current standards.

In May 2024, the Commission approved a 12-month extension to accommodate delays related to securing Right of Way (RW) Certification. There has also been necessary coordination with multiple agencies (third parties) to ensure the protection of the Union Pacific Railroad (UPRR) and Southern California Regional Rail Authority facilities situated within the project area.

UPRR and the Southern California Regional Rail Authority operate facilities adjacent to the project, which require protection in place during construction. In addition, the City of Industry and Los Angeles County oversee local arterial roads within the project boundaries, necessitating third-party agreements for clarifying responsibilities and ensuring smooth coordination during development. Stakeholder discussions have identified potential street and railroad improvements at the I-605/Valley Boulevard interchange that influenced the project timeline. LA Metro has begun holding regular meetings with partners such as the Department and the San Gabriel Valley Council of Governments to enhance coordination and minimize potential delays and costs.

Although the Department is expected to approve the final design plans soon, further delays have hindered the finalization of third-party agreements and RW activities. Currently, the RW Certification is projected for May 2026. The project requires 13 parcels from 8 different owners, and LA Metro has utilized Resolutions of Necessity to facilitate acquisitions through condemnation, securing 6 parcels thus far. Out of the remaining seven parcels, one is involved in litigation, two are in escrow, two are expected to be donated by the City of Industry, and two await decisions from the UPRR. Significant challenges include ongoing legal discussions regarding temporary construction easements with a property owner, who is also currently in litigation with the Department, as this parcel along the southbound I-605/Valley Boulevard on-ramp's abandonment affects access for the Department's maintenance crews; the court hearing date of November 17, 2025. Furthermore, an offer for two permanent easements to UPRR is pending approval of the final signal design, which is anticipated to take up to 12 months before a Construction and Maintenance Agreement can be executed.

LA Metro's coordination efforts with Southern California Edison (SCE) and the LA County Department of Public Works (LADPW) on this project, also resulted in delays to project timelines due to the wildfires and winter storms in January 2025. Both agencies redirected personnel to focus on the immediate recovery efforts, resulting in the unavailability of engineers and technical staff typically involved in LA Metro's projects. SCE prioritized staff to reassessing shut-off gas and power lines, while LADPW had staff focused on inspecting wildfire-damaged buildings and clearing debris. Consequently, essential reviews and approvals for RW certification for this project were delayed.

Although the commitment of SCE and LADPW during this critical time has been vital, it led to a significantly delay in the RW processes and raised concerns regarding the project schedule, underscoring the need for effective solutions moving forward.

Therefore, LA Metro is requesting an additional 12 months for the period of project allocation for the CON phase from June 30, 2025 to June 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(16), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO AMEND THE PERIOD OF PROJECT ALLOCATION FOR A
LOCALLY-ADMINISTERED SENATE BILL 1 TRADE CORRIDOR
ENHANCEMENT PROGRAM RAIL PROJECT
WAIVER 25-125, AMENDING WAIVER 24-80**

ISSUE:

Should the California Transportation Commission (Commission) amend the period of project allocation for the locally-administered Senate Bill 1 (SB 1) Trade Corridor Enhancement Program (TCEP) Stockton Diamond Grade Separation Rail project (PPNO 9883), in San Joaquin County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission amend the period of project allocation for the locally-administered SB 1 TCEP Stockton Diamond Grade Separation Rail project (PPNO 9883), in San Joaquin County, for the time period as identified in the attachment.

BACKGROUND:

In June 2023, the Commission approved a 12-month time extension for the period of project allocation for the SB 1 TCEP Stockton Diamond Grade Separation Rail project (PPNO 9883). In May 2024, the Commission approved a second time extension for an additional 12 months for the period of project allocation with a deadline of June 30, 2025.

The local agency will not be able to request an allocation for funds by the June 30, 2025, deadline and is requesting additional time for period of project allocation. The attachment provides the details of the project and the explanation for the delays.

Current SB 1 TCEP Guidelines (Guidelines) stipulate that funds programmed for all phases are available for allocation only until the end of the fiscal year programmed. The Commission may approve a waiver to the timely use of funds deadline, one-time only, for up to 12 months. The responsible agency is requesting an exception to the Guidelines for additional time for the period of project allocation.

Attachment

**Amendment Time Extension/Waiver – Project Allocation
Trade Corridor Enhancement Program**

Project Number: 1
Applicant: San Joaquin Regional Rail Commission
County: San Joaquin
District: 10
PPNO: 9883
Allocation Amount: \$100,000,000
Phase: Construction
1st Waiver: Waiver 23-120
Approved: June 2023
2nd Waiver: Waiver 24-80
Approved: May 2024
Number of Months Requested: ~~12 Months~~ ~~24 Months~~ **30 Months**
Extended Deadline: ~~06/30/2024~~ ~~06/30/2025~~ **12/31/2025**
Department Recommendation: Support

Stockton Diamond Grade Separation Rail

The San Joaquin Regional Rail Commission (SJRRC) requests an additional 6 months, from 24 months to 30 months, for the period of project allocation for the Construction (CON) phase of the Stockton Diamond Grade Separation Rail project. SJRRC has experienced unforeseen delays during the project's Right of Way (RW) and Plans, Specifications, and Estimate (PS&E) phases.

The Stockton Diamond Grade Separation Rail project is located at the intersection of the heavily-trafficked Union Pacific Railroad (UPRR) and Burlington Northern Santa Fe (BNSF) Railway mainlines within the City of Stockton (City) in San Joaquin County. This is one of the busiest at-grade railway junctions in California. The construction of the grade separation will provide for an uninterrupted flow of rail traffic through the current at-grade crossing and allows for the current and anticipated future volume of freight and passenger trains to travel through the City more efficiently, thus reducing grade crossing blockage times.

In June 2023, the Commission approved a 12-month time extension for the period of project allocation for the CON phase of this project, due to delays in the delivery of the PS&E phase. These delays were caused by intensive design reviews and approvals from UPRR and BNSF Railroads, both of which have limited resources for project reviews and approvals. Additionally, utility relocation was delayed longer than originally anticipated due to extensive coordination with the utility owners. SJRRC previously expected to request CON allocation in spring 2024.

In May 2024, the Commission approved a second time extension for an additional 12 months for the period of project allocation for the CON phase, due to delays in the PS&E and RW phases and the need for new environmental clearance. These delays were due to a late-stage design modification required by UPRR, which mandated that the Mormon Slough Box Culvert shift from an open-bottom to a closed-bottom design. As UPRR will own and maintain the culvert, SJRRC was obligated to comply, prompting notification to the National Oceanic and Atmospheric Administration (NOAA). This modification triggered a formal consultation process with NOAA and delayed National Environmental Policy Act revalidation. As a result, RW certification and the TCEP CON funding allocation were postponed to October 2024.

Since the approval of the second time extension, additional unforeseen delays have occurred due to complications in executing the RW Certificate.

Multi-party construction and maintenance agreements have been identified, that need to be executed during the RW phase with UPRR, BNSF, the Department, and the City, prior to the start of construction. These agreements were previously planned to be executed during the CON phase after CON allocation. This change to the timing of the construction management (C&M) agreements has triggered a reprogram of the portion of the Transit and Intercity Rail Capital Program (TIRCP) funding from the CON phase to the RW phase. While negotiating agreements with a single railroad is common, working with two Class I railroads has proven much more complex than anticipated, delaying the drafting and review of agreement terms. These agreements must be executed to enable SJRRC to certify the RW, as the RW certification cannot proceed without them. The C&M agreement involving the Department is currently being drafted and is expected to be shared with UPRR by July 2025. The C&M agreement involving UPRR and BNSF is currently under review by the railroads.

Relocation of remaining tenants on current UPRR right-of-way is also awaiting resolution of the relocation agreements. Furthermore, separate agreements with the railroads and the Department, and with the City have also introduced complexities to understanding property rights and ownership. Negotiations over budgets and construction phasing for self-performed work by the host railroads are still ongoing.

The project has federal funding with the Department as the primary recipient, and its footprint affects the State Highway System RW. As a result, the Department in association with SJRRC must document and certify RW compliance for the project, including those areas not within the Department RW. This condition has resulted in recent unexpected requirements to document and certify compliance for the entire project, adding several months of delay in completing the RW phase.

There are 13 utility lines that are impacted by the project. The majority of the utilities relocation require design and/or construction to be self-performed by the utility owners. In many cases, the utility owners need to complete the design prior to completing the utility agreements with SJRRC. Utility agreements are one of the requirements for the RW Certification.

By fall 2024, SJRRC had reached agreement on most terms with the utility owners. However, the agreements had to be revised to incorporate the Department's standard utility agreement language, which is primarily designed for highway projects and not fully suited to the unique scope and complexity of this project. As a result, additional coordination with both the utility owners and the Department was necessary to finalize the agreements, leading to several months of delay in completing the RW phase. SJRRC submitted the final utility agreements to the Department in late May 2025 and are awaiting approval. Five utility agreements are required for this project.

SJRRC has worked closely with UPRR, BNSF, the Department, and the City to complete and approve 13 design packages for structures, tracks, and engineering elements. The project design phase is concurrent with the design phase for the large SJRRC Valley Rail program, which includes a portfolio of over 20 improvements along both UPRR and BNSF lines to expand capacity to the Altamont Corridor Express service. This extensive workload in the San Joaquin region has placed a heavy review burden on the host railroads.

In addition, when SJRRC engineers re-estimated the construction costs in October 2024 at 90 percent design, they found that the project cost had increased by \$100,000,000 compared to the previously estimated cost at 60 percent design. This increase is attributed to several reasons summarized below:

- A longer construction duration based on UPRR requirements for track/signal construction phasing:
 - Requirement to maintain freight train operation
 - Requirement of increased traffic control
- Increased costs for municipal (City) and third-party (Fiber/CalWater) utility relocation after 60 percent design completion
- A higher contingency amount to cover the unforeseen conditions on this complex project, such as delays in coordinating with multiple stakeholders, extended negotiations or design approvals.

Of the original \$104,000,000 funding gap identified at the 60 percent design stage, \$101,000,000 has been secured through a combination of \$32,000,000 in future TCEP capacity, \$55,000,000 from TIRCP Cycle 7, and \$14,000,000 from the Congestion Mitigation and Air Quality Program, leaving a \$3,000,000 shortfall. The additional \$100,000,000 identified at the 90 percent design milestone brings the total remaining funding gap to \$103,000,000. To address this, SJRRC is actively working with the Commission, the Department, and the California Transportation Agency to reprioritize existing programmed TIRCP funds and pursue additional State and Federal grant funding opportunities. The status of several key State and Federal grant opportunities is expected to be known by late 2025.

SJRRC has been collaborating with the host railroads and stakeholders to complete the design reviews and obtain approvals. The remaining three design packages are currently under

review, with approvals expected in the second quarter of 2025. The bid document development is also underway and is expected to be completed in the third quarter of 2025.

SJRRC is facilitating negotiations between the Department and UPRR regarding State Route 4 railroad undercrossing to resolve the outstanding items related to the construction and maintenance agreement and RW Certification.

Although it's common for host railroads to initiate construction and maintenance agreements, SJRRC has taken the proactive step of drafting a multi-railroad agreement with BNSF and UPRR to encourage negotiations and expedite the agreement process. The draft is currently under review by both railroads.

SJRRC is collaborating with the Department to review and revise all utility relocation agreements. This effort is being undertaken to ensure that each agreement fully complies with the Department's standard utility relocation agreement requirements. SJRRC is also working closely with the Department to review all utility relocation design packages. These revisions aim to ensure that the design packages meet all applicable Department guideline requirements.

SJRRC is aggressively pursuing funding opportunities to ensure the construction phase is fully funded and can proceed without delay. As part of this effort, SJRRC has conducted a detailed review of phased capital priorities across the broader TIRCP-funded Valley Rail Program and has identified a potential funding solution by shifting funds from later phases of the Valley Rail Program to address the remaining funding gap for the Stockton Diamond Grade Separation project.

SJRRC, UPRR, BNSF, the Department, and the City have all made significant investments in this project. TCEP funding is an essential fund source of the overall funding plan. If the time extension request is not approved, other secured funding sources, including federal funds, may be jeopardized, leading to significant delays. This project is well positioned for implementation: environmental clearance has been secured, design is 100 percent complete awaiting final approvals, and RW is nearing completion. Additionally, utility owners and host railroads have affirmed their commitments for the upcoming phases.

Therefore, SJRRC is requesting an additional six months for the period of project allocation for the CON phase from June 30, 2025 to December 31, 2025.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(12), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO AMEND THE PERIOD OF CONTRACT AWARD FOR A
TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM COMPONENT
WAIVER 25-146, AMENDING WAIVER 24-191**

ISSUE:

Should the California Transportation Commission (Commission) amend the period of contract award for the Transit and Intercity Rail Capital Program (TIRCP) (2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure Treasure Island Electrification) component (PPNO CP075A), in San Francisco County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission extend the period of contract award for the TIRCP (2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure Treasure Island Electrification) component (PPNO CP075A), in San Francisco County, for the time period as identified in the attachment.

BACKGROUND:

In May 2024, the Commission allocated \$14,581,000 for the Construction (CON) phase of the (2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure) (2022:17) San Francisco Zero Emissions High-Frequency Ferry Network (Shoreside Charging Infrastructure) component (PPNO CP075A). In December 2024, the Commission approved a 7-month time extension for the period of contract award. There is a concurrent allocation amendment to revise the component name to (2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure Treasure Island Electrification) and deallocate \$12,381,000 from the CON phase due to the component being split into three separate components on this month's Commission agenda.

The responsible agency has been unable to complete the component per the Timely Use of Funds policy. This is a request to amend the previously approved contract award time extension, per the TIRCP Allocation Policy. The attachment describes the details of the project and the explanation for the delays.

Attachment

**Amendment Time Extension/Waiver – Contract Award
Transit and Intercity Rail Capital Program**

Project Number: 1
Applicant: San Francisco Bay Area Water Emergency Transportation Authority
County: San Francisco
District: 4
PPNO: CP075A
Allocation Amount: \$14,581,000
Phase: Construction
Allocation Date: 05/17/2024
Allocation Resolution: TIRCP-2324-58
1st Waiver: Waiver 24-191
Approved: December 2024
Number of Months Requested: ~~7 Months~~ **20 Months**
Extended Deadline: ~~06/30/2025~~ **07/31/2026**
Department Recommendation: Support

**(2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure
Treasure Island Electrification)**

The San Francisco Bay Area Water Emergency Transportation Authority (WETA) requests an additional 13 months, from 7 months to 20 months, for the period of contract award for the Construction (CON) phase of the (2020:17) Expansion of WETA Ferry Service (Shoreside Charging Infrastructure Treasure Island Electrification) component. WETA has experienced unexpected delays in awarding the contract.

The awarded scope of work for the component includes the modification of standard floats to house a bank of batteries that will allow vessels to charge while they are docked. This is the first project of this kind at this scale in the nation.

In December 2024, WETA experienced a delay in awarding the contract due to the complexity of applying United States building restrictions to this type of marine project, as the project was modeled after and will be built with many European components, and there is extremely limited precedence for this. WETA is now requesting additional time due to the bids received being approximately double the original engineering estimates. WETA needs more time to engage in negotiations and value engineering efforts to bring the costs down, minimize the need for additional funding, and ensure the successful delivery of the system as originally planned. WETA anticipates awarding a contract by July 31, 2026.

Therefore, WETA is requesting an additional 13 months to allow sufficient time to award the contract for the CON phase from June 30, 2025 to July 31, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(13), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO AMEND THE PERIOD OF PROJECT COMPLETION FOR A
TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM COMPONENT
WAIVER 25-147, AMENDING WAIVER 23-160**

SUMMARY:

Should the California Transportation Commission (Commission) amend the period of project completion for the Transit and Intercity Rail Capital Program (TIRCP) (2018:10) Los Angeles Region Transit System Integration and Modernization Program (Network Integration) component (PPNO CP030), in Los Angeles County, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation recommends that the Commission amend the period of project completion for the TIRCP (2018:10) Los Angeles Region Transit System Integration and Modernization Program (Network Integration) component (PPNO CP030), in Los Angeles County, for the time period as identified in the attachment.

BACKGROUND:

In December 2018, the Commission allocated \$7,000,000 for Construction (CON) of the TIRCP (2018:10) Los Angeles Region Transit System Integration and Modernization Program (Network Integration) component (PPNO CP030). In October 2023, the Commission approved a 20-month time extension to complete CON, however the responsible agency has been unable to complete the component phase per the Timely Use of Funds policy. This is a request to amend the previously approved project completion time extension. The attachment describes the details of the component and the explanation for the delay.

Attachment

**Amendment Time Extension/Waiver – Project Completion
Transit and Intercity Rail Capital Program**

Project Number: 1
Applicant: Los Angeles County Metropolitan Transportation Authority
County: Los Angeles
District: 7
PPNO: CP030
Allocation Amount: \$7,000,000
Phase: Construction
Allocation Date: 12/06/2018
Award Date: 02/15/2019
1st Waiver: Waiver 23-160
Approved: October 2023
Number of Months: ~~20 Months~~ **34 Months**
Extended Deadline: ~~07/31/2025~~ **09/30/2026**
Department Recommendation: Support

**(2018:10) Los Angeles Region Transit System Integration and Modernization Program
(Network Integration)**

The Los Angeles County Metropolitan Transportation Authority (Metro) requests an additional 14 months, from 20 months to 34 months, for the period of project completion for the Construction (CON) phase of the (2018:10) Los Angeles Region Transit System Integration and Modernization Program (Network Integration) component. Metro experienced unavoidable delays in completing the CON phase.

The component proposes the development of three studies, including the Metrolink San Bernardino Line – Gold Line Task Force study, the Los Angeles County Rail Network Integration study, and the High Desert Intercity Rail Corridor (HDC) Service Development Plan (SDP).

The Network Integration component has progressed from 88 percent to 94 percent completion since the 20-month project completion time extension was approved in October 2023. At that time, only one of three studies had been completed, and the HDC project had not received corridor identification, which was necessary for the HDC SDP to be considered for review and approval by the Federal Railroad Administration (FRA). Once the HDC project received corridor identification, the HDC SDP was submitted to the FRA for comment. At that time, Metro was informed that the FRA was developing a new scope of work for the HDC SDP under the corridor ID program and could not review it until the full scope was approved by the FRA. Metro created a gap analysis to compare completed items of the HDC SDP scope of work with the new items from the FRA approved scope of work. Metro now has FRA approval on the scope of work required to complete the HDC SDP. With the additional time being requested, Metro will prepare the revised technical documents with the HDC SDP and submit a draft for FRA review in the first eight months, allow for a one-month FRA review period,

two months to revise the documents, one month for FRA back-checking, and two months for coordination to approve the final HDC SDP by September 30, 2026.

Therefore, Metro is requesting an additional 14 months for the period of project completion for the CON phase from July 31, 2025 to September 30, 2026.

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: June 26-27, 2025

From: STEVEN KECK, Chief Financial Officer

Reference Number: 2.8v.(14), Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **REQUEST TO AMEND THE PERIOD OF PROJECT COMPLETION FOR A
TRANSIT AND INTERCITY RAIL CAPITAL PROGRAM COMPONENT
WAIVER 25-134, AMENDING WAIVER 23-186**

ISSUE:

Should the California Transportation Commission (Commission) amend the period of project completion for the Transit and Intercity Rail Capital Program (TIRCP) (2018:13) Peninsula Corridor Electrification Expansion Project (Bike Parking Enhancements) component (PPNO CP039), in various counties, for the time period as identified in the attachment?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission amend the period of project completion for the TIRCP (2018:13) Peninsula Corridor Electrification Expansion Project (Bike Parking Enhancements) component (PPNO CP039), in various counties, for the time period as identified in the attachment.

BACKGROUND:

In August 2020, the Commission approved an allocation of \$1,475,000 for the Construction (CON) phase of the TIRCP (2018:13) Peninsula Corridor Electrification Expansion Project (Bike Parking Enhancements) component (PPNO CP039). In January 2021, the Commission approved a supplemental allocation for \$1,500,000, increasing the total allocated TIRCP funds to \$2,975,000. In December 2023, the Commission approved a 20-month time extension to complete the CON phase, however, the responsible agency has been unable to complete the component per the Timely Use of Funds (TUF) policy. This is a request to amend the previously approved project completion time extension. The attachment describes the details of the project and the explanation for the delays.

Attachment

**Amendment Time Extension/Waiver – Project Completion
Transit and Intercity Rail Capital Program**

Project Number: 1
Applicant: Peninsula Corridor Joint Powers Board
County: Various
District: 4
PPNO: CP039
Allocation Amount: \$1,475,000
Phase: Construction
Allocation Date: 08/13/2020
Allocation Resolution: TIRCP-2021-02
Award Date: 01/29/2021
1st Waiver: 23-186
Approved: December 2023
Number of Months Requested: ~~20 Months~~ **40 Months**
Extended Deadline: ~~08/31/2025~~ **04/30/2027**
Department Recommendation: Support

(2018:13) Peninsula Corridor Electrification Expansion Project (Bike Parking Enhancements)

The Peninsula Corridor Joint Powers Board (PCJPB) requests an additional 20 months, from 20 months to 40 months, for the period of project completion for the Construction (CON) phase of the (2018:13) Peninsula Corridor Electrification Expansion Project (Bike Parking Enhancements) component. PCJPB has experienced unexpected delays in the CON phase.

The project proposes to enhance ridership and safeguard capacity on the electrified system by implementing a wayside bike parking program. The project will procure and install up to 1,150 on-demand e-lockers at up to 24 Caltrain stations. This component also includes networking equipment and software related to electronic payments and usage monitoring to determine where to prioritize additional lockers. Minor site preparations are required, including the installation of concrete pads for the e-lockers.

In August 2020, the Commission approved an allocation of \$1,475,000 in TIRCP funds to complete the CON phase of the component. In January 2021, the Commission approved a supplemental allocation of \$1,500,000, increasing the total allocated amount for the CON phase of this component to \$2,975,000. In December 2023, a 20-month time extension to complete the CON phase was granted due to delays with the e-locker vendor supplying the e-lockers, however, additional time is needed after reaching 40 percent completion of the CON phase during the initial time extension.

Due to varying site conditions at each station, the construction and size of the modular bike rooms may vary and be offset with the installation of additional e-lockers to achieve sufficient bicycle parking. Currently, the agency experienced an unforeseen delay with the installation of the modular bike rooms at multiple locations due to varying reasons to address unique site conditions.

The Sunnyvale station modular bike room faced delays coming to an agreement with the utility easement holder where the bike room was intended to be built. The bike room had to move locations in the station and has been subsequently redesigned entirely. The Menlo Park station modular bike room faced delays due to lengthy discussions with the city of Menlo Park requesting the station to fit the physical characteristics of the historic area. The Redwood City station modular bike room faced delays due to destructive behaviors from local unhoused peoples occurring at the original site. Subsequently, the station had to be redesigned in a more visible location with additional security measures.

The work can be completed with the existing funds, but additional time is necessary to accommodate the installation of the remaining modular bike rooms and e-lockers. The delivery and installation schedule of the modular bike rooms and e-lockers is on track and no further delays are expected. PCJPB will be able to install all remaining e-lockers by April 30, 2027.

Therefore, PCJPB is requesting an additional 20 months to allow sufficient time to complete the CON phase from August 31, 2025 to April 30, 2027.