

MEMORANDUM

To: CHAIR AND COMMISSIONERS
CALIFORNIA TRANSPORTATION COMMISSION

CTC Meeting: May 14-15, 2026

From: STEVEN KECK, Chief Financial Officer

Reference Number: 4.21, Action Item

Prepared By: Dee Lam, Chief
Division of Local Assistance

Subject: **ACTIVE TRANSPORTATION PROGRAM – PROJECT SCOPE AMENDMENT–
SANTA FE DRIVE CORRIDOR BIKE AND PEDESTRIAN IMPROVEMENTS
PROJECT
RESOLUTION ATP-P-2526-14**

ISSUE:

Should the California Transportation Commission (Commission) approve a project scope amendment for the Cycle 3A Active Transportation Program (ATP) Santa Fe Drive Corridor Bike and Pedestrian Improvements project (PPNO 1299A), in San Diego County, to amend the scope?

RECOMMENDATION:

The California Department of Transportation (Department) recommends that the Commission approve a project scope amendment for the Cycle 3A ATP Santa Fe Drive Corridor Bike and Pedestrian Improvements project (PPNO 1299A), in San Diego County to amend the scope.

DISCUSSION:

In October 2017, the Commission approved the Cycle 3A Statewide Component of the 2017 ATP Augmentation – Statewide and Small Urban and Rural Components, which included \$996,000 for the Santa Fe Drive Corridor Bike and Pedestrian Improvements project (PPNO 1299A), under Resolution G-17-29.

The Santa Fe Drive Corridor Bike and Pedestrian Improvements project (PPNO 1299A) is located in the City of Encinitas (City), along the north and south sides of Santa Fe Drive, between Windsor Road and El Camino Real, within the vicinity of San Dieguito Academy High, Ocean Knoll Elementary, Oak Crest Middle School, and Ada Harris Elementary. The project will construct new sidewalk, curb, gutter and curb ramps and will also include installation of new bicycle lanes, pedestrian activated Rectangular Rapid Flashing Beacon, and striping and signage along the corridor.

The City is requesting approval for a scope change to remove the sidewalk along the north side of Santa Fe Drive between Crest Drive and El Camino Real due to its direct conflict with a major existing drainage channel. The City is also proposing to enhance the original scope by including Class II bicycle lanes, an Americans with Disabilities Act (ADA) ramp, timing improvements, pedestrian push buttons, and bicycle detections.

ANALYSIS:

The Department's analysis has deemed the City's request to be a significant change to the original scope.

The revised scope removes 1,359 linear feet of sidewalks and adds 380 linear feet of non-buffered Class II bicycle lanes, 1,547 linear feet of 2-foot buffered Class II bicycle lanes, 1 ADA ramp, 2 timing improvements, 2 upgraded pedestrian push buttons, and 2 bicycle detections. Although the sidewalk along the north side of Santa Fe Drive between Crest Drive and El Camino Real is being removed from the scope, the sidewalk along the south side of Santa Fe Drive between Crest Drive and El Camino Real will remain in the scope to provide the continuous east-west route for pedestrians.

ANALYSIS RECOMMENDATION:

Based on the Commission's scope change guidance, as well as the analysis of the proposed scope changes, the Department's recommendation is to support this scope change request because it still aims to deliver the active transportation need and purpose as identified in the project application. The project will continue to link the eastern portion of the corridor with important community destinations that include transit stops, schools, shopping centers, recreational facilities (Encinitas Community Park) and medical facilities (Scripps Hospital).

BACKGROUND:

Per the ATP Guidelines, the Commission will consider changes to the approved scope submitted in the project application to assist agencies in implementing their ATP projects and to maximize the overall benefits of the ATP. Scope change requests must be submitted to the Department as soon as an implementing agency has identified a need for one and must include documentation that supports the requested change. The Department will make a recommendation to the Commission for action. The Commission will not consider recommending the allocation of any additional project phases until the scope change request has been approved.

Attachments

Attachment 1

Exhibit 25-D Request for
Project Scope Change



Exhibit 25-D: Request for Project Scope Change (Local ATP Projects)

The Request for Project Scope Change (Local ATP Projects) form, Exhibit 25-D, is used when the implementing agency is unable to complete the approved project scope as programmed. This Project Scope Change Request Form is intended to support the California Transportation Commission (CTC) "Policy on Project Amendments and Advance Project Allocations", adopted on August 18, 2016.

To: *Bryan Ott, Acting DLAE* Date: 10/30/2025
District Local Assistance Engineer PPNO: 1299A
Caltrans, Office of Local Assistance Federal PROJECT #: 1118000207
2050 Taylor Street ATP ID #: ATPSB1L-5446(025)
Bldg 1, 2nd Fl, MS 124
San Diego, CA 92110-2737

Project Name (Per CTC programming): Santa Fe Drive Corridor Bike and Pedestrian Improvements

Approved Project Description (As submitted in Application): The project will construct new sidewalk, curb, gutter, and curb ramps and will also include pavement improvements for new bike lanes and traffic calming features along Santa Fe Drive.

Approved Project Limits (As submitted in Application): In Encinitas along north and south of Santa Fe Drive between Windsor/Bonita Road & El Camino Real, in vicinity of San Dieguito Academy High, Ocean Knoll ES, Oak Crest MS & Ada Harris

For Federally Funded Projects: Written MPO Concurrence (see Item #7)

Current FTIP/FSTIP Description: N/A

Current FTIP/FSTIP Limits: N/A

Dear Micheal Stewart:

The City of Encinitas respectfully submits this letter to formally request approval for a major scope change for the Santa Fe Drive Corridor Bike and Pedestrian Improvements Project.

Consistent with the California Transportation Commission's (CTC) "Policy on Project Amendments and Advance Project Allocations", adopted on August 18, 2016, we request that Caltrans consider, and the CTC approve the proposed Project Amendments as documented below:



REQUIRED ELEMENTS (PER CTC'S PROJECT AMENDMENT POLICY):

1. An explanation of the proposed scope change:

Proposed Change #1 – Removal of 1,349 LF of proposed sidewalk from the project scope:

A segment of unimproved roadway on the north side of Santa Fe Drive, between Crest Dr and El Camino Real that was originally proposed as new 5'-wide sidewalk is being removed from the project scope due to the infeasibility of its construction and associated cost constraints as it conflicts with an existing storm drain concrete open channel. In this same area, sidewalk will be constructed on the south side of Santa Fe Drive, with crosswalks at both Crest Drive to the west and El Camino Real to the east that will allow for pedestrians and bicyclists to safely cross the street and maintain east-west connectivity through the corridor, in alignment with the original project application objective. With the deletion of this segment of sidewalk, no bike lanes will be impacted, and pedestrians will have safe access to cross Santa Fe Drive at both El Camino Real and at Crest Drive to safely travel east-west through the corridor, as originally intended with the project application.

Proposed Change #2 – Increased Pedestrian and Bike Improvements:

Following a more detailed evaluation of the corridor, additional improvements have been identified as necessary to improve safety and accessibility for non-motorized users. The proposed scope change will add the following to the project:

- 1 New ADA Curb Ramp
- 4,459 LF of New Bike Lanes (Bike lanes will be installed along the corridor where no bike lanes existed as of the project application in 2016)
- 2 new intersection timing improvements, upgraded pedestrian push buttons, and bike detection improvements

2. The reason for the proposed scope change:

Proposed Change #1 – Removal of 1,349 LF of proposed sidewalk from the project scope:

The proposed segment of sidewalk has been found to be in direct conflict with an existing drainage channel; the alignment of the sidewalk would need to be placed in the exact location as the open drainage channel. In order to construct the new sidewalk, it would require demolition of the existing at-grade drainage channel, installation of new curb inlets, installation of a new, buried RCP drainage pipe system, and revisions to both the upstream and downstream invert elevations (flowlines) of the existing storm drain system to accommodate burial of the at-grade channel to tie it back into the existing drainage system. Additional work to accommodate the sidewalk in this location would include construction of retaining walls due to the steep side slopes, as well as right-of-way acquisition to allow for the placement of the retaining walls. These additional storm drain improvements were not identified as necessary during the initial scoping of the project in 2016. Upon more detailed evaluation of the project site based on field surveys and potholing of existing utilities, it has become apparent that the cost to install the sidewalk in this location, with the additional storm drain improvements, is infeasible from a budgetary



perspective. These unanticipated costs would require significant City funding and major redesign efforts to allow for the sidewalk to be placed.

Proposed Change #2 – Increased Pedestrian and Bike Improvements:

Following careful evaluation of project conditions, stakeholder input that was collected through community workshops and public hearings, and updated technical analyses, the City has determined various improvements that will be added to the scope to enhance the effectiveness of the new infrastructure installed in the project. These improvements include an additional curb ramp and upgrades to traffic signals to allow for more crossing time and more accessible push buttons. The original ATP project application was prepared in July 2016 and awarded in 2018; since that time, the City has incorporated several of the proposed project elements, including curb ramps, bike lanes, Rapid-Rectangular Flashing Beacon (RRFB) pedestrian crossing at Balour Drive, and a 4-way controlled Stop at the intersection of Crest Drive and Santa Fe Drive. These improvements complement and support the ATP Cycle 3 application that was submitted for this project in 2016, aligning with the City’s objective to create a safer and more connected east-west corridor on Santa Fe Drive. The proposed scope changes presented herein intend to progress the original application and ensure that the City achieves the original outputs that were identified in 2016 for this project to create a comprehensively connected east-west corridor along Santa Fe Drive for pedestrians and bicyclists.

3. The impact the proposed scope change would have on the overall cost of the project:

The proposed project scope change will increase the overall estimate from \$1,388,461 to \$1,783,250, accounting for inflation that occurred from the time of the original estimate and current unit costs, and accounting for additional materials and labor required for the identified additional improvements. While the project estimate has increased since the original estimate was created, the City is prepared to cover the difference in cost with local funds and is not requesting additional ATP Cycle 3 State funding for this project as part of this scope change request.

4. An estimate of the impact the proposed scope change would have on the potential of the project to increase walking and bicycling as compared to the benefits identified in the project application (increase or decrease in benefit);

Location/Element	Approved Scope	Proposed Change	Change in Walking/Bicycling Benefit
Project wide	New Sidewalk	6,407 LF to 5,048 LF	Decrease
Project wide	New ADA Curb Ramp	23 to 24	Increase
Project wide	New Bike Lanes	2,532 to 4,459 LF	Increase
Project wide	Signalized Intersection – Timing Improvements	0 to 2	Increase



Local Assistance Program Guidelines

Exhibit 25-D

Request for Project Scope Change (Local ATP Projects)

Project wide	Signalized Intersection – Upgraded Push Buttons	0 to 2	Increase
Project wide	Signalized Intersection – Bike Detection	0 to 2	Increase

5. **An estimate of the impact the proposed scope change would have on the potential of the project to increase the safety of pedestrians and bicyclists as compared to the benefits identified in the project application (increase or decrease in benefit);**

Location/Element	Approved Scope	Proposed Change	Change in Safety Benefit
Project wide	New Sidewalk	6,407 LF to 5,048 LF	Decrease
Project wide	New ADA Curb Ramp	23 to 24	Increase
Project wide	New Bike Lanes	2,532 to 4,459 LF	Increase
Project wide	Signalized Intersection – Timing Improvements	0 to 2	Increase
Project wide	Signalized Intersection – Upgraded Push Buttons	0 to 2	Increase
Project wide	Signalized Intersection – Bike Detection	0 to 2	Increase

6. **An explanation of the methodology used to develop the aforementioned estimates; and**
 The original project outputs were developed through visual assessment of the corridor at the time of the ATP Cycle 3 project application (2016) and community coordination efforts during the preparation of the City's Safe Routes to School Plan prepared in 2013-2015. Quantities were estimated based on the corridor length and the number of intersections within the project area, and a cost estimate was prepared using those quantities and average unit costs available at the time the application was submitted. The updated outputs and estimates reflect a more recent comprehensive assessment informed by a field survey of the corridor.
7. **For projects programmed in the Metropolitan Planning Organization (MPO) component, evidence of MPO approval and the MPO rationale for their approval.**

NA

ADDITIONAL QUESTIONS:

8. **Does this scope change require revalidation of your environmental document?** No
 If yes, what is the actual/estimated date of revalidation? NA
9. **Explain the additional public outreach efforts you have made with respect to this proposed scope change and provide a summary of the public response to these efforts:**



Local Assistance Program Guidelines

Exhibit 25-D
Request for Project Scope Change (Local ATP Projects)

The original project scope was shared with the community at-large during the initial project scoping in 2019. After the City Council selected a design firm for the project, City staff organized a Community Participation Plan (CPP) workshop, hosted on March 21, 2021, and followed up with a survey on October 20, 2021. A summary of the responses received by the City from the community is provided as reference.

Since that time, City staff have presented the project at various City Council meetings and have held community input meetings to gather feedback regarding the scope of this project. On May 13, 2025, the City of Encinitas held a public input meeting for the Santa Fe Drive Corridor Improvements project, during which many residents expressed a strong desire for the City to construct the eastern segment as soon as possible due to the lack of pedestrian and bicycle infrastructure. The ATP project, as currently proposed, is specifically designed to respond to this feedback and to provide the community with infrastructure that supports safe, multimodal travel. Attachment 3 highlights comments from members of the public expressing approval of the proposed new infrastructure on the eastern segment of Santa Fe Drive. In addition, staff have shared project updates with the City Council and received direction on scope changes for this segment and have provided updates to the school district and the schools fronting the project every two months, including notification of scope changes at School District Liaison Committee meetings.

REQUIRED Attachments: (check boxes of attached required documents)

- Original plans/workplan with changes highlighted
- Revised plans/workplan
- Original Detailed Engineer’s Est. with changes highlighted
- Revised Detailed Engineer’s Est.
- Written MPO Concurrence (see Item #7)
- Additional Revised Application Documentation

Required revisions to the Project’s Description and/or Limits:

The proposed Project Amendments documented above will require the following changes to the Project’s Description and/or Limits:

Proposed changes to the Project Description: NA

Proposed changes to the Project Limits: NA

For Federally Funded Projects:

Proposed changes to the FTIP/FSTIP Description: NA

Proposed changes to the FTIP/FSTIP Limits: NA



Project Delivery Status:

The following is a side-by-side comparison of the original project schedule and the current project schedule. The explanations for each milestone date change is listed below:

Original CTC Allocation Dates: (as programmed by the CTC when the application was approved for funding):
PA&ED: 6/15/2018 PS&E: 12/31/2020 R/W: 12/31/2020 CON: 6/30/2021 CON-NI: 6/30/2021

Actual/Currently Anticipated CTC Allocation Dates: (at the time of this request)
PA&ED: 6/15/2018 PS&E: 3/25/2021 R/W: 3/25/2021 CON: 3/23/2023 CON-NI: 3/23/2023

Explanation for milestone changes:

A 20-month time extension for the allocation of the CON phase of work was requested due to an extended PAED completion date and approved extension requests for the PS&E and R/W phases of work. This scope change will not impact any of the currently approved project allocation milestones.

Local Agency Certification:

This Request for Scope Change has been prepared in accordance with the *Procedures for Administering Local Projects in the Active Transportation Program (ATP)*. I certify that the information provided in the document is accurate and correct. I understand that if the required information has not been provided this form will be returned and the request may be delayed. Please advise us as soon as the scope change has been approved.

You may direct any questions to:

Badr Suleiman at 760.943.2291
(name) (phone number)

Signature: **Badr Suleiman** Title: Engineer II Date: 01/15/2025

Agency/Commission: City of Encinitas

Attachments:

1. Original Plans with Changes Highlighted
2. Original Cost Estimate with Changes Highlighted/Revised Cost Estimate
3. Community Engagement Efforts
4. Updated Application Documents



Local Assistance Program Guidelines

Exhibit 25-D
Request for Project Scope Change (Local ATP Projects)

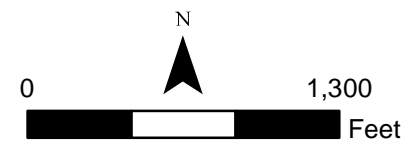
Distribution:

- (1) Original -DLAE
- (2) Copy – Division of Local Assistance, Headquarters ATP Program Manager
- (3) Copy – MPO/RTPA/County Transportation Commission



SANTA FE DRIVE CORRIDOR PEDESTRIAN & BIKE IMPROVEMENTS
ATTACHMENT D- EXISTING CONDITIONS

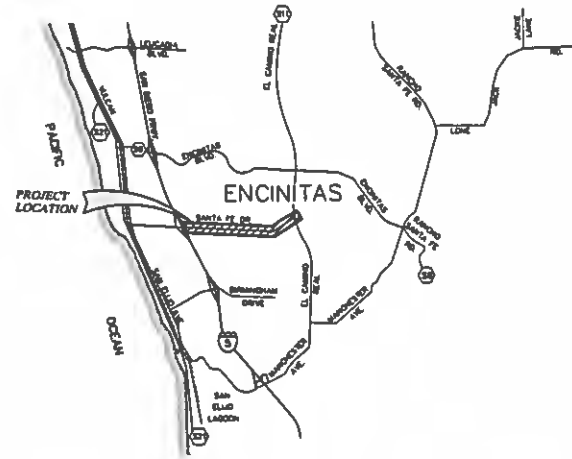
ATTACHMENT D



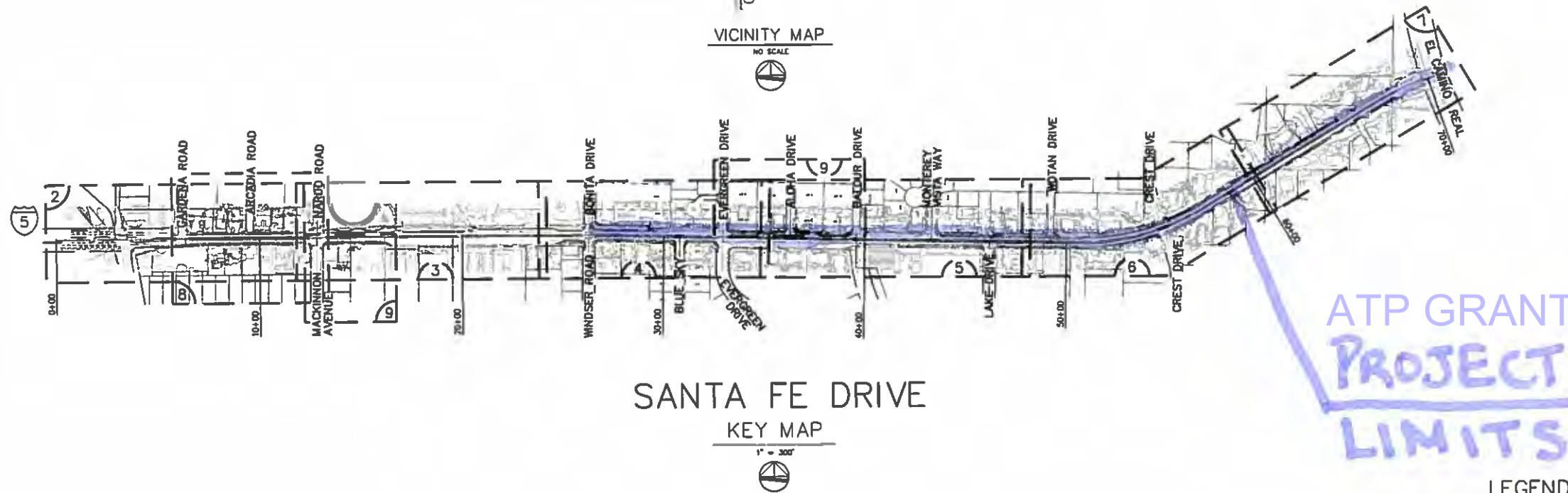
1 inch = 800 feet

Revised on 01.07.2025
For ATP Major Scope Change

ALIGNMENT STUDY SANTA FE DRIVE CITY OF ENCINITAS



VICINITY MAP
NO SCALE



SANTA FE DRIVE
KEY MAP
1" = 300'

ATP GRANT
PROJECT
LIMITS

ABBREVIATIONS

AC	= ASPHALT CONCRETE	PRC	= POINT OF REVERSED CURVE
ARHW	= ASPHALT RUBBERIZED HOT MIX	PVC	= POLYVINYL CHLORIDE PIPE
BC	= BEGIN CURVE	PWMT	= PAVEMENT
BW	= BOTTOM OF WALL	(R)	= RADIAL BEARING
CAB	= CRUSHED AGGREGATE BASE	RCP	= REINFORCED CONCRETE PIPE
CMP	= CORRUGATED METAL PIPE	REQD.	= REQUIRED
EC	= END CURVE	R/W	= RIGHT OF WAY
EP	= EDGE OF PAVEMENT	SORSO	= SAN DIEGO REGIONAL STANDARD DRAWINGS
EXST.	= EXISTING	ST LT	= STREET LIGHT
FH	= FIRE HYDRANT	TB	= TOP OF BERM
FL	= FLOWLINE	TC	= TOP OF CURB
FS	= FINISHED SURFACE	TW	= TOP OF WALL
HORIZ.	= HORIZONTAL	VERT.	= VERTICAL
IE	= PIPE INVERT ELEVATION	X-GTR	= CROSS GUTTER
LIP	= GUTTER LIP	(72.89)	= EXISTING ELEVATION
PCC	= PORTLAND CEMENT CONCRETE		
PCR	= POINT OF CURB RETURN		
PP	= POWER POLE		

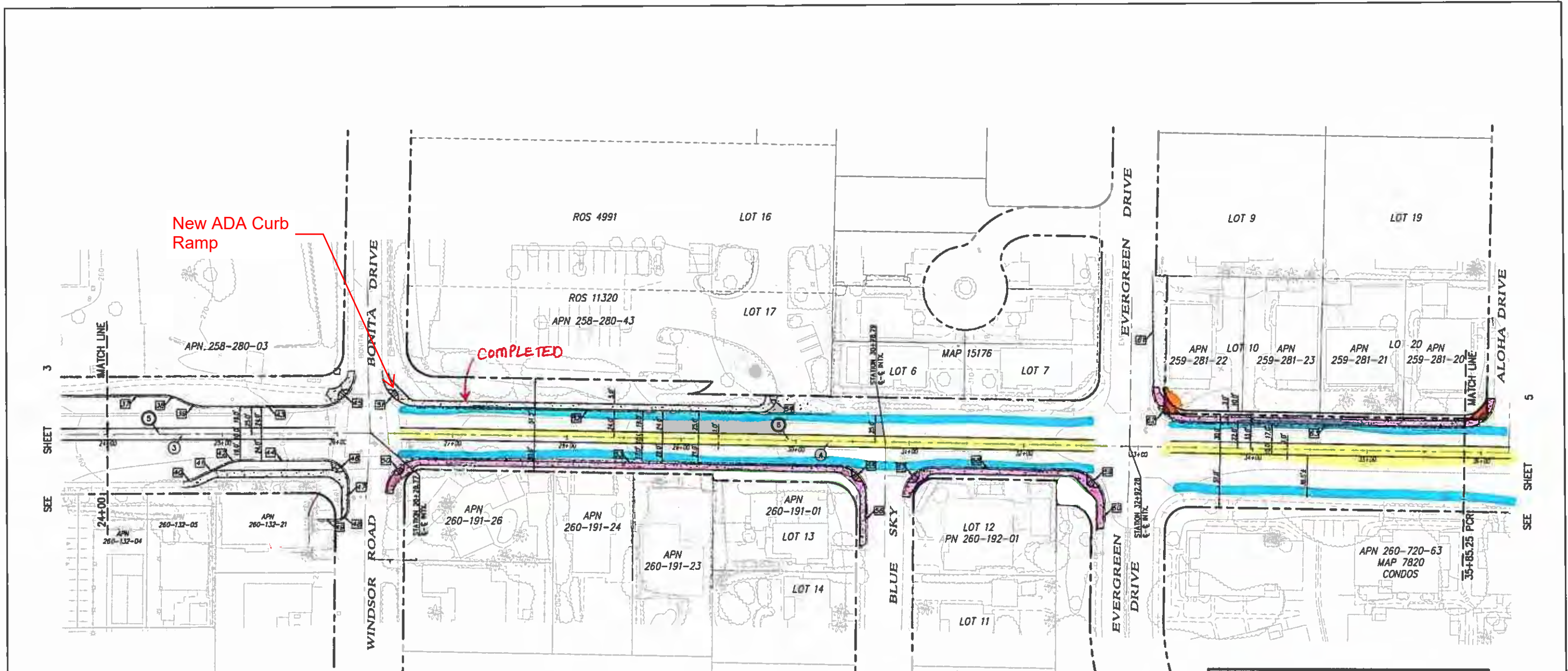
SHEET INDEX

SHEET 1	TITLE SHEET
SHEET 2	STATION 0+00 - 12+00
SHEET 3	STATION 12+00 - 24+00
SHEET 4	STATION 24+00 - 35+35.16
SHEET 5	STATION 35+35.16 - 48+00
SHEET 6	STATION 48+00 - 60+00
SHEET 7	STATION 60+00 - 70+36.57
SHEET 8	DETAILS
SHEET 9	DETAILS

LEGEND

PUBLIC RIGHT-OF-WAY	---
PROPERTY LINES	----
EASEMENT LINES	----
CENTERLINES	----
STATIONING	+
EXST. CONTOURS	----
CONCRETE CURB & GUTTER	----
CONCRETE SIDEWALK	----
CONCRETE CROSS GUTTER	----
PEDESTRIAN RAMP	----

REVISIONS	APPROVED	DATE	REFERENCES	DATE	BENCHMARK	SCALE	SPECIAL DISTRICT	DESIGNED BY	DRAWN BY	CHECKED BY	APPROVALS	CITY OF ENCINITAS ENGINEERING DEPARTMENT	DRAWING NO.	
					ELEV: 243.238 DATUM: NAVD 88 DESCRIPTION: STATION POINT "1035", AS SHOWN ON RECORD OF SURVEY NO. 18416. A STREET WELL MONUMENT AT THE CENTER OF CUL-DE-SAC AT THE EAST END OF ORANGE VIEW DRIVE.	HORIZONTAL AS SHOWN VERTICAL AS SHOWN		DESIGNED BY MICHAEL H. SMITH	DRAWN BY Dr.	CHECKED BY Dr.	RECOMMENDED BY: LEROY BODAS DATE: 9/30/13 R.C.E. NO.: 22312	APPROVED BY: _____ DATE: _____ R.C.E. NO.: _____	PUBLIC IMPROVEMENTS ALIGNMENT STUDY FOR: SANTA FE DRIVE WORK PROJECT NO.	0090-SI SHEET 1 OF 9



SANTA FE DRIVE

STATIONING NOTE: ALL STATIONING IS BASED ON THE EXISTING RIGHT-OF-WAY CENTERLINE-EQUATIONS AND DATA FOR THE CONSTRUCTION CENTERLINE ARE PROVIDED FOR REFERENCE.

Legend

- New Sidewalk
- New Bike Lane
- Lane Restriping
- Bio-Retention/Landscaping
- ROW Acquisition

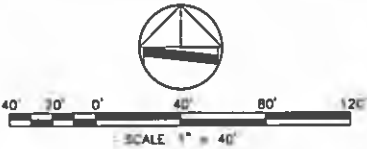
No ROW Acquisition required

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46	90°47'37"	20.00'	31.69'	6" TYPE G CURB & GUTTER
47	N00°45'19"E	21.18'	21.18'	6" TYPE G CURB & GUTTER
48	90°00'00"	10.00'	15.71'	6" TYPE G CURB & GUTTER
49	N89°14'41"W	16.01'	16.01'	6" TYPE G CURB & GUTTER
50	90°30'00"	30.00'	47.39'	6" TYPE G CURB & GUTTER

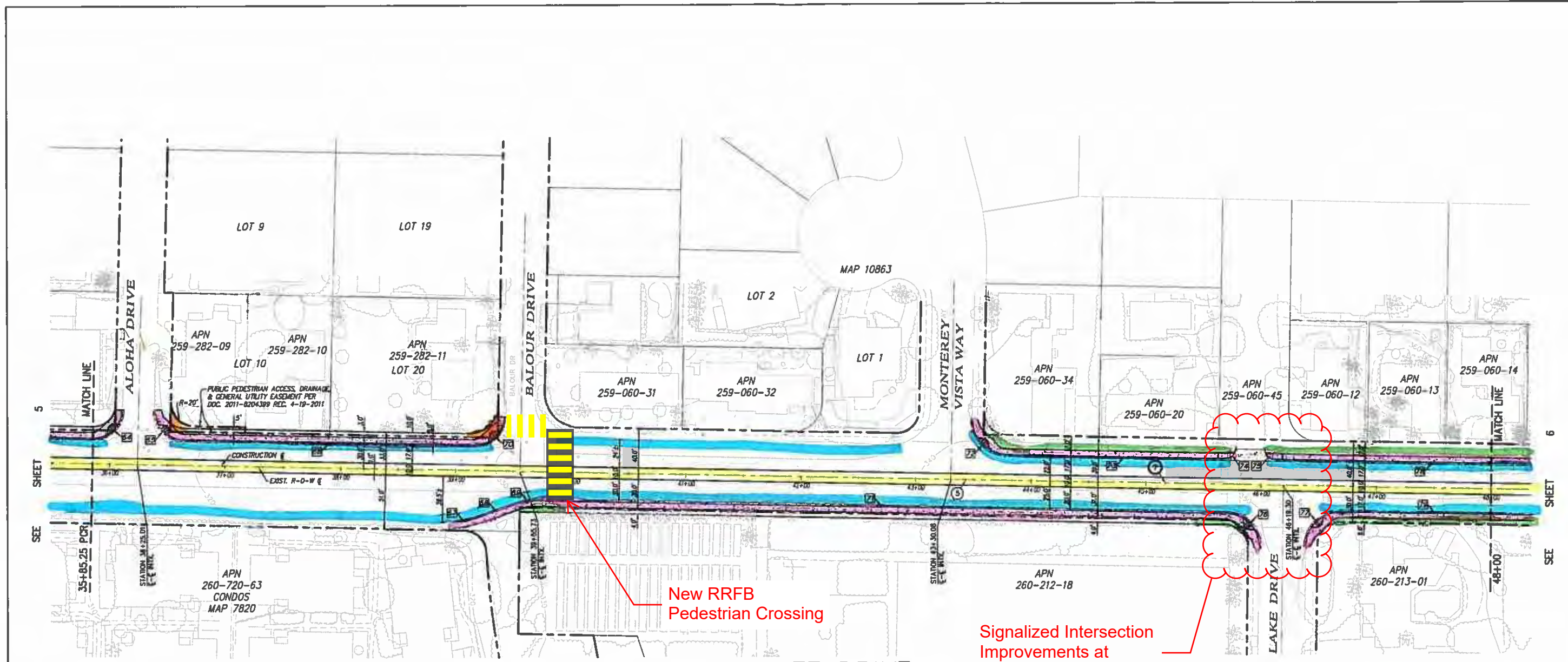
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54	89°59'52"	15.00'	23.56'	6" TYPE G CURB & GUTTER
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CONSTRUCTION CENTERLINE DATA				
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6	N88°44'41"W	---	956.48'	CONST. 1.00' N. OF E. CL.

CENTERLINE DATA				
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3	S89°57'42"W	---	228.77'	EXIST. R-G-W CENTERLINE
4	N88°44'41"W	---	956.48'	EXIST. N-G-W CENTERLINE



REVISIONS	APPROVED	DATE	REFERENCES	DATE	BENCHMARK	SCALE	SPECIAL DISTRICT	DESIGNED BY	DRAWN BY	CHECKED BY	APPROVALS	CITY OF ENCINITAS ENGINEERING DEPARTMENT	DRAWING NO.	
					ELEV: 243.238 DATUM: NAVD 88 DESCRIPTION: STATION POINT "1035", AS SHOWN ON RECORD OF SURVEY NO. 18418. A STREET WELL MONUMENT AT THE CENTER OF CUL-DE-SAC AT THE EAST END OF ORANGE VIEW DRIVE.	HORIZONTAL AS SHOWN VERTICAL AS SHOWN		MICHAEL H. SMITH			RECOMMENDED BY: LEROY BODAS DATE: _____ R.C.E. NO: 65090 EXP: 9/30/13	APPROVED BY: _____ DATE: _____ R.C.E. NO: 22312	STATION 24+00 - 35+83.25 FOR SANTA FE DRIVE WORK PROJECT NO.	0090-SI SHEET 4 OF 9



SANTA FE DRIVE

STATIONING NOTE: ALL STATIONING IS BASED ON THE EXISTING RIGHT-OF-WAY CENTERLINE-EQUATIONS AND DATA FOR THE CONSTRUCTION CENTERLINE ARE PROVIDED FOR REFERENCE.

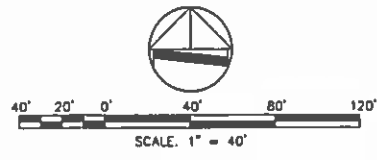
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68	N71°12'48"E	-	40.80'	6" TYPE G CURB & GUTTER
69	20°02'31"	50.00'	17.49'	6" TYPE G CURB & GUTTER
70	89°48'37"	25.00'	39.19'	6" TYPE G CURB & GUTTER
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74	90°01'37"	10.00'	15.71'	CURB LINE, PED. RAMP
75	73°23'48"	10.00'	12.81'	CURB LINE, PED. RAMP
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CENTERLINE DATA				
NO.	BEARING/DELTA	RADIUS	LENGTH	REMARKS
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Legend

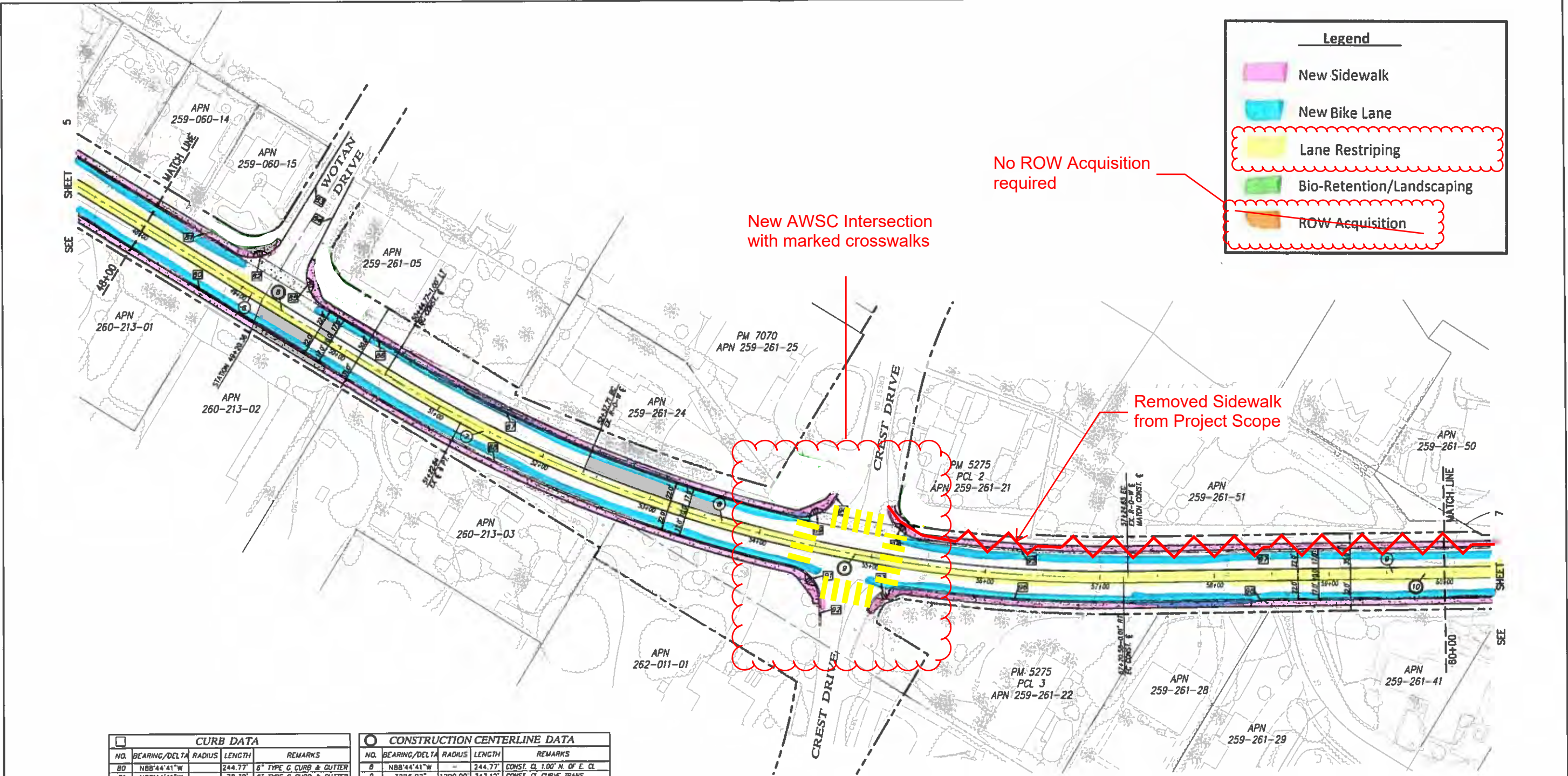
- New Sidewalk
- New Bike Lane
- Lane Restriping
- Bio-Retention/Landscaping
- ROW Acquisition



REVISIONS	APPROVED	DATE	REFERENCES	DATE	BENCHMARK ELEV: 243.238 CATUM: NAD83 DESCRIPTION: STATION POINT "1035", AS SHOWN ON RECORD OF SURVEY NO. 18416. A STREET WELL MONUMENT AT THE CENTER OF CUL-DE-SAC AT THE EAST END OF ORANGE VIEW DRIVE.	SCALE HORIZONTAL AS SHOWN VERTICAL AS SHOWN	SPECIAL DISTRICT	DESIGNED BY US MICHAEL H SMITH	DRAWN BY OT DATE: R.C.E. NO. 65090 EXP 9/30/13	CHECKED BY US	APPROVALS RECOMMENDED BY: LEROY BODAS DATE: R.C.E. NO. 22312	APPROVED BY: DATE: R.C.E. NO.	CITY OF ENCINITAS ENGINEERING DEPARTMENT STATION 35+85.25 = 48+00 FOR: SANTA FE DRIVE WORK PROJECT NO.	DRAWING NO. 0090-SI SHEET 5 OF 9
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Legend

- New Sidewalk
- New Bike Lane
- Lane Restriping
- Bio-Retention/Landscaping
- ROW Acquisition



No ROW Acquisition required

New AWSC Intersection with marked crosswalks

Removed Sidewalk from Project Scope

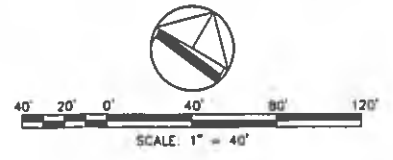
CURB DATA				
NO.	BEARING/Delta	RADIUS	LENGTH	REMARKS
80	N88°44'41"W		244.77'	6" TYPE G CURB & CUTTER
81	N88°44'41"W		78.39'	6" TYPE G CURB & CUTTER
82	90°00'41"	30.00'	47.13'	6" TYPE G CURB & CUTTER
83	N01°14'37"E		111.66'	6" TYPE A AC DKE
84	N03°01'23"W		80.21'	6" TYPE A AC DKE
85	89°59'19"	30.00'	47.12'	6" TYPE G CURB & CUTTER
86	N88°44'41"W		78.43'	6" TYPE G CURB & CUTTER
87	18°16'27"	1178.00'	375.71'	6" TYPE G CURB & CUTTER
88	18°36'08"	1222.00'	396.74'	6" TYPE G CURB & CUTTER
89	71°44'25"	30.00'	37.56'	6" TYPE G CURB & CUTTER
90	N11°42'27"E		7.31'	6" TYPE G CURB & CUTTER
91	75°42'22"	30.00'	39.64'	6" TYPE G CURB & CUTTER
92	N31°38'26"W		22.48'	6" TYPE G CURB & CUTTER
93	77°52'34"	30.00'	40.78'	6" TYPE G CURB & CUTTER
94	71°35'53"	30.00'	37.49'	6" TYPE G CURB & CUTTER
95	8°40'07"	1178.00'	178.23'	6" TYPE G CURB & CUTTER
96	8°57'14"	1222.00'	190.97'	6" TYPE G CURB & CUTTER
97	N58°59'17"E		279.42'	6" TYPE G CURB & CUTTER
98	N58°59'17"E		279.42'	6" TYPE G CURB & CUTTER

CONSTRUCTION CENTERLINE DATA				
NO.	BEARING/Delta	RADIUS	LENGTH	REMARKS
8	N88°44'41"W		244.77'	CONST. CL 1.00' N. OF E. CL
9	32°16'02"	1200.00'	347.12'	CONST. CL CURVE TRANS.
10	N58°59'17"E		279.42'	CONST. CL CONCORDANT WITH EX. CL

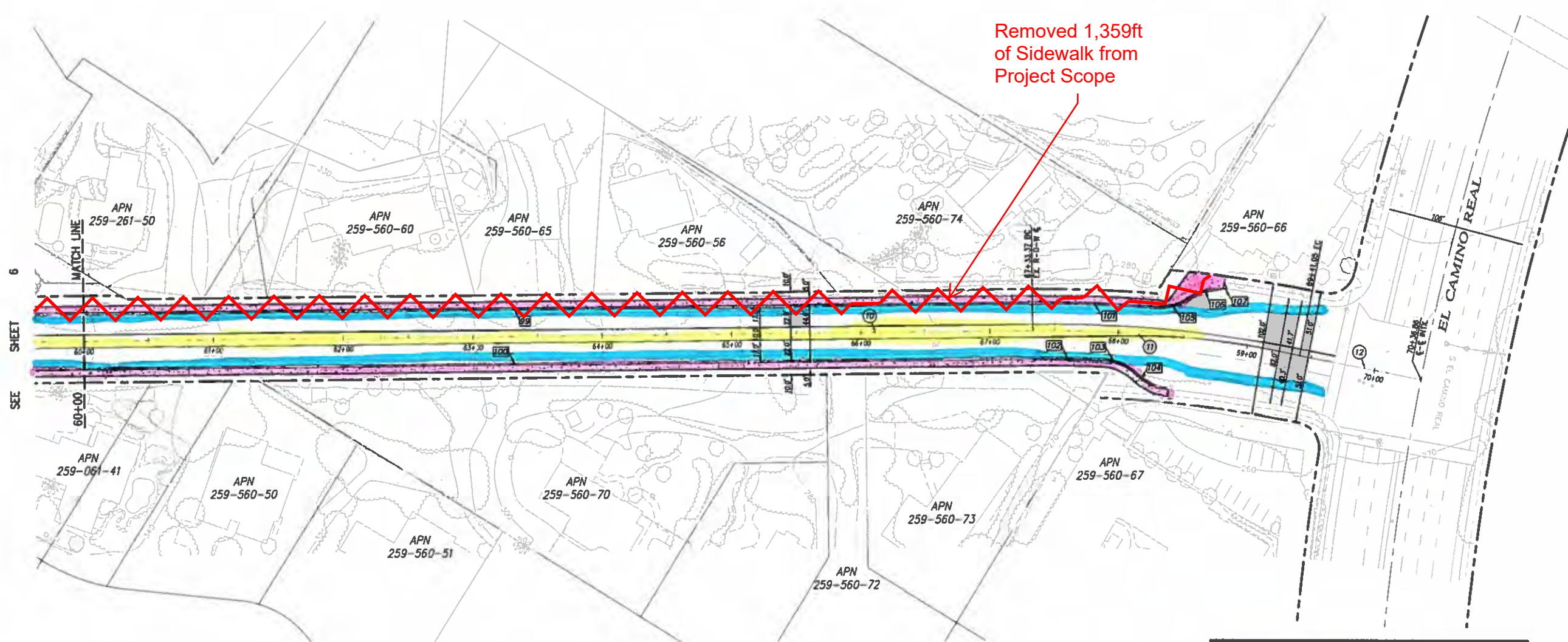
CENTERLINE DATA				
NO.	BEARING/Delta	RADIUS	LENGTH	REMARKS
6	N88°44'41"W		322.84'	EXIST. R-O-W CENTERLINE
7	N82°14'17"E		114.87'	EXIST. R-O-W CENTERLINE
8	23°15'00"	1200.00'	486.95'	EXIST. R-O-W CENTERLINE
9	N58°59'17"E		275.35'	EXIST. R-O-W CENTERLINE

SANTA FE DRIVE

STATIONING NOTE: ALL STATIONING IS BASED ON THE EXISTING RIGHT-OF-WAY CENTERLINE-EQUATIONS AND DATA FOR THE CONSTRUCTION CENTERLINE ARE PROVIDED FOR REFERENCE.



REVISIONS	APPROVED	DATE	REFERENCES	DATE	BENCHMARK	SCALE	SPECIAL DISTRICT	DESIGNED BY	DRAWN BY	CHECKED BY	APPROVALS	CITY OF ENCINITAS ENGINEERING DEPARTMENT	DRAWING NO.
					ELEV: 243.238 DATA: NAVD 88 DESCRIPTION: STATION POINT "1035", AS SHOWN ON RECORD OF SURVEY NO 18416. A STREET WELL MONUMENT AT THE CENTER OF CUL-DE-SAC AT THE EAST END OF ORANGE VIEW DRIVE.	HORIZONTAL AS SHOWN VERTICAL AS SHOWN		MICHAEL H. SMITH	DATE: 9/30/13 R.C.E. NO. 65090 EXP. 9/30/13	RECOMMENDED BY: LEROY BODAS DATE: 22312 R.C.E. NO. 22312	APPROVED BY: _____ DATE: _____ R.C.E. NO.: _____	STATION 48+00 - 60+00 FOR: SANTA FE DRIVE WORK PROJECT NO.	0090-SI SHEET 6 OF 9



Removed 1,359ft
of Sidewalk from
Project Scope

SANTA FE DRIVE

STATIONING NOTE: ALL STATIONING IS BASED ON THE EXISTING RIGHT-OF-WAY CENTERLINE-EQUATIONS AND DATA FOR THE CONSTRUCTION CENTERLINE ARE PROVIDED FOR REFERENCE.

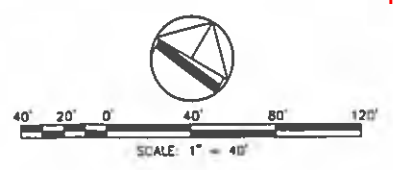
Legend

- New Sidewalk
- New Bike Lane
- Lane Restriping
- Bio-Retention/Landscaping
- ROW Acquisition

No ROW Acquisition
required

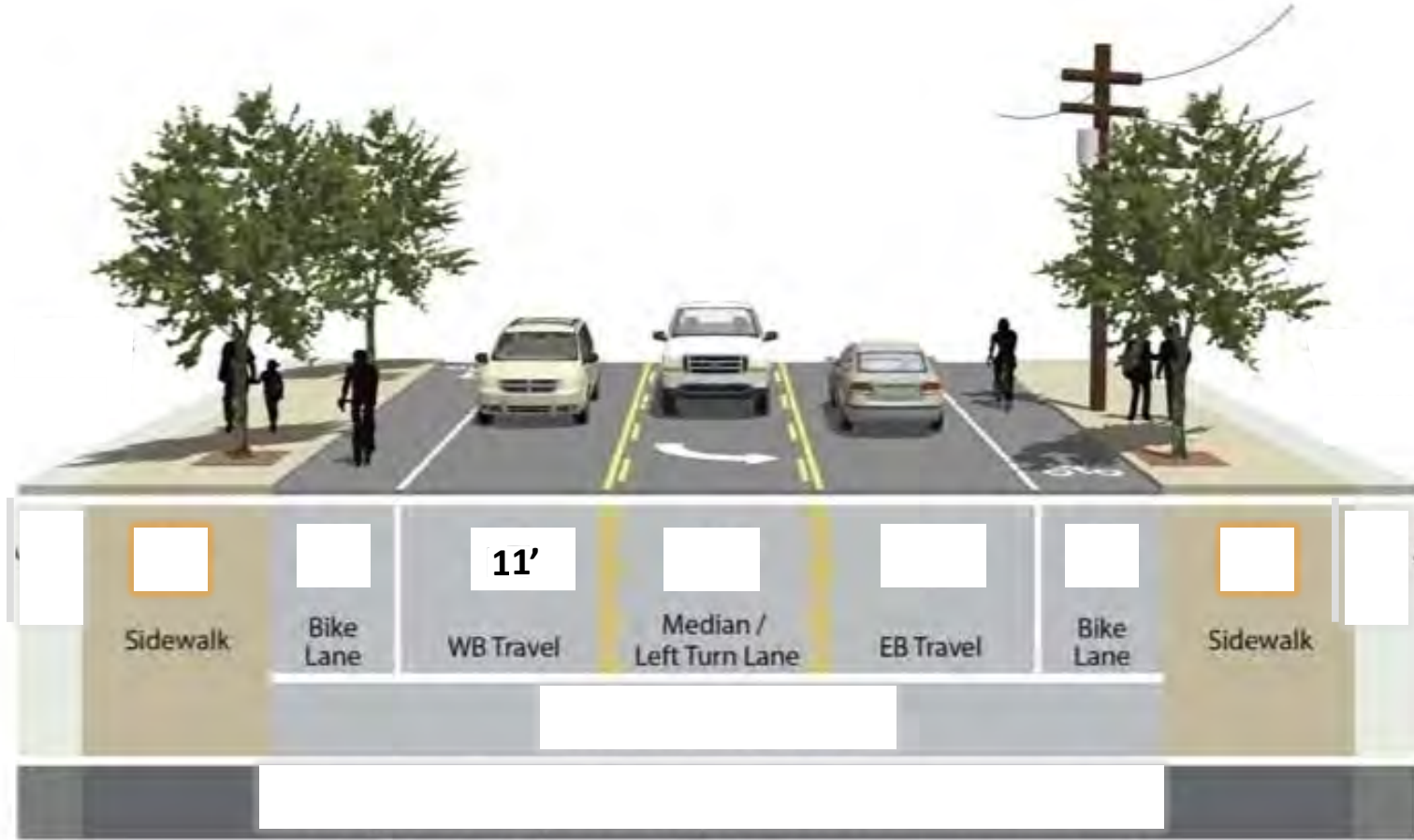
CURB DATA				
NO.	BEARING/DELTA	RADIUS	LENGTH	REMARKS
99	N58°59'17"E		733.37'	6" TYPE G CURB & GUTTER
100	N58°59'17"E		733.37'	6" TYPE G CURB & GUTTER
101	S19°09'	1022.00'	94.88'	6" TYPE G CURB & GUTTER
102	S109°03'	978.00'	53.78'	6" TYPE G CURB & GUTTER
103	41°54'9"	40.00'	29.26'	6" TYPE G CURB & GUTTER
104	39°45'24"	40.00'	27.76'	6" TYPE G CURB & GUTTER
105	39°01'36"	40.00'	27.25'	6" TYPE G CURB & GUTTER
106	41°24'14"	40.00'	28.91'	6" TYPE G CURB & GUTTER
107	N65°03'19"E		2.67'	6" TYPE G CURB & GUTTER

CENTERLINE DATA				
NO.	BEARING/DELTA	RADIUS	LENGTH	REMARKS
10	N58°59'17"E		733.37'	EXIST. R-O-W CENTERLINE
11	N11°53'56"	1000.00'	207.68'	EXIST. R-O-W CENTERLINE
12	N70°53'13"E		95.81'	EXIST. R-O-W CENTERLINE



REVISIONS	APPROVED	DATE	REFERENCES	DATE	BENCHMARK	SCALE	SPECIAL DISTRICT	DESIGNED BY	DRAWN BY	CHECKED BY	APPROVALS	CITY OF ENCINITAS ENGINEERING DEPARTMENT	DRAWING NO.	
					ELEV: 243.238 DATUM: NAVD 88 DESCRIPTION: STATION POINT "1035", AS SHOWN ON RECORD OF SURVEY NO. 18418, A STREET WELL MONUMENT AT THE CENTER OF CUL-DE-SAC AT THE EAST END OF ORANGE VIEW DRIVE.	HORIZONTAL AS SHOWN VERTICAL AS SHOWN		MICHAEL H. SMITH			RECOMMENDED BY: LEROY BODAS DATE: 9/30/13 R.C.E. NO.: 65090 EXP. 9/30/13	APPROVED BY: _____ DATE: _____ R.C.E. NO.: 22312	STATION 60+00 - 70+36.57 FOR: SANTA FE DRIVE WORK PROJECT NO.	0090-SI SHEET 7 OF 9

Attachment E-4: Santa Fe Drive Typical Cross-Section



Item No.	Unit of Measure	Approximate Quantity	Item	Unit Price (figures)	Total (figures)
1	LS	1	Mobilization and Demobilization	\$ 60,000.00	\$ 60,000.00
2	LS	1	Traffic Control and Implementation	\$ 10,000.00	\$ 10,000.00
3	LS	1	Stormwater Pollution Preventaion Plan	\$ 10,000.00	\$ 10,000.00
4	LS	1	Clearing and Grubbing	\$ 1,000.00	\$ 1,000.00
5	CY	138	Unclassified Excavation and Haul	\$ 45.00	\$ 6,210.00
6	CY	15	Backfill and Compaction	\$ 25.00	\$ 375.00
7	LF	1349	8" PCC Curb per SDRSD G-01	\$ 20.00	\$ 26,980.00
8	LF	3878	6" PCC Curb and Gutter per SDRSD G-02, Type G	\$ 30.00	\$ 116,340.00
9	LF	130	6" PCC Rolled Curb and Gutter per SDRSD G-4A, Type B	\$ 20.00	\$ 2,600.00
10	LF	57	PCC Variable Height Curb	\$ 40.00	\$ 2,280.00
11	SF	2610	Cross/Longitudianl Gutter	\$ 15.00	\$ 39,150.00
12	EA	19	PCC Curb Ramps	\$ 2,000.00	\$ 38,000.00
13	EA	26	PCC Ramps	\$ 2,000.00	\$ 52,000.00
14	SF	1057	PCC Bus Pad	\$ 20.00	\$ 21,140.00
15	SF	1871	PCC Driveway	\$ 12.00	\$ 22,452.00
16	SF	13347	PCC Sidewalk/Bike Path	\$ 10.00	\$ 133,470.00
17	CY	707	Class II Crushed Aggegate Base	\$ 100.00	\$ 70,700.00
18	TON	338	AC Pavement	\$ 150.00	\$ 50,700.00
19	EA	1	Adjust Manhole to Grade	\$ 1,000.00	\$ 1,000.00
20	EA	8	Adjust Water Valve/Gas Valve/Water Meter to Grade	\$ 500.00	\$ 4,000.00
21	EA	1	Adjust/Relocate Fire Hydrant	\$ 1,250.00	\$ 1,250.00
22	LS	1	Stormwater Improvements	\$ 25,000.00	\$ 25,000.00
23	LS	1	Biofiltration Cells	\$ 18,250.00	\$ 18,250.00
24	LS	1	Striping and Pavement Markings	\$ 25,000.00	\$ 25,000.00
25	LS	1	Signage	\$ 6,000.00	\$ 6,000.00
26	EA	1	Trash Can (Install Only)	\$ 1,000.00	\$ 1,000.00
27	EA	9	Relocate Mailbox	\$ 1,000.00	\$ 9,000.00
28	LS	1	Traffic Signal Modifications	\$ 125,000.00	\$ 125,000.00
29	EA	1	Adjust Electrical/Communications Pull Box to Grade	\$ 1,300.00	\$ 1,300.00
30	CY	913	Retaining Wall Foundation	\$ 100.00	\$ 91,300.00
31	SF	674	CMU Retaining Wall	\$ 10.00	\$ 6,740.00
32	LS	1	Landscaping	\$ 130,000.00	\$ 130,000.00
33	LS	1	Irrigation	\$ 17,500.00	\$ 17,500.00
34	LS	1	Install 1" Potable Water Service and 1" Recycled Water Meter	\$ 5,000.00	\$ 5,000.00
35	LS	1	1 Year Plant Maintenance and Establishment	\$ 31,500.00	\$ 31,500.00
36	LS	1	Field Orders	\$ 100,000.00	\$ 100,000.00

Total	\$ 1,262,237.00
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Item No.	Unit of Measure	Approximate Quantity	Item	Unit Price (figures)	Total (figures)
1	LS	1	Mobilization and Demobilization	\$ 100,000.00	\$ 100,000.00
2	LS	1	Traffic Control and Implementation	\$ 100,000.00	\$ 100,000.00
3	LS	1	Stormwater Pollution Prevention Plan	\$ 60,000.00	\$ 60,000.00
4	LS	1	Clearing and Grubbing	\$ 100,000.00	\$ 100,000.00
5	CY	138	Unclassified Excavation and Haul	\$ 100.00	\$ 13,800.00
6	CY	15	Backfill and Compaction	\$ 50.00	\$ 750.00
7	LF	1349	8" PCC Curb per SDRSD G-01	\$ 35.00	\$ 47,215.00
8	LF	3549	6" PCC Curb and Gutter per SDRSD G-02, Type G	\$ 35.00	\$ 124,215.00
9	LF	130	6" PCC Rolled Curb and Gutter per SDRSD G-4A, Type B	\$ 35.00	\$ 4,550.00
10	LF	57	PCC Variable Height Curb	\$ 55.00	\$ 3,135.00
11	SF	1904	Cross/Longitudinal Gutter	\$ 30.00	\$ 57,120.00
12	EA	17	PCC Curb Ramps	\$ 5,000.00	\$ 85,000.00
13	EA	26	PCC Ramps	\$ 4,000.00	\$ 104,000.00
14	SF	620	PCC Bus Pad	\$ 20.00	\$ 12,400.00
15	SF	1871	PCC Driveway	\$ 15.00	\$ 28,065.00
16	SF	12,122	PCC Sidewalk	\$ 15.00	\$ 181,830.00
17	CY	600	Class II Crushed Aggregate Base	\$ 100.00	\$ 60,000.00
18	TON	200	AC Pavement	\$ 200.00	\$ 40,000.00
19	SY	20650	Slurry Seal	\$ 3.00	\$ 61,950.00
20	EA	1	Adjust Manhole to Grade	\$ 2,000.00	\$ 2,000.00
21	EA	8	Adjust Water Valve/Gas Valve/Water Meter to Grade	\$ 1,000.00	\$ 8,000.00
22	EA	1	Adjust/Relocate Fire Hydrant	\$ 5,000.00	\$ 5,000.00
23	LS	1	Drainage Improvements	\$ 60,000.00	\$ 60,000.00
24	LS	1	Biofiltration Cells	\$ 100,000.00	\$ 100,000.00
25	LS	1	Striping and Pavement Markings	\$ 75,000.00	\$ 75,000.00
26	LS	1	Signage	\$ 20,000.00	\$ 20,000.00
27	EA	1	Trash Can (Install Only)	\$ 1,000.00	\$ 1,000.00
28	EA	9	Relocate Mailbox	\$ 1,000.00	\$ 9,000.00
29	LS	1	Intersection Improvements	\$ 125,000.00	\$ 125,000.00
30	EA	1	Adjust Electrical/Communications Pull Box to Grade	\$ 1,300.00	\$ 1,300.00
31	CY	34	Retaining Wall Foundation	\$ 1,000.00	\$ 34,000.00
32	SF	674	CMU Retaining Wall	\$ 80.00	\$ 53,920.00
33	LS	1	Landscaping	\$ -	\$ -
34	LS	1	Irrigation	\$ -	\$ -
35	LS	1	Install 1" Potable Water Service and 1" Recycled Water Meter	\$ 5,000.00	\$ 5,000.00
36	LS	1	1 Year Plant Maintenance and Establishment	\$ -	\$ -
37	LS	1	Field Orders	\$ 100,000.00	\$ 100,000.00

Total	\$ 1,783,250.00
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*Final Costs are subject to change pending receipt of construction Bids



Application Part 4: Project Details

INFRASTRUCTURE TYPE (Only Intended for Infrastructure Projects)

Note: When quantifying the amount of Active Transportation improvements proposed by the project, **do not double-count the improvements** that benefit both Bicyclists and Pedestrians (i.e. new RRFB/Signal should only show as a Pedestrian or Bicycle Improvement).

Bicycle Improvements

What % of the BICYCLE related project cost are going towards closing a "Gap" in infrastructure? 44 %
(As opposed to cost going towards "improving" existing bicycle infrastructure: i.e. Class 2 to Class 4) 4459

New Bike Lanes/Routes:	Class 1: _____ Linear Feet	Class 2: 2,532 <u>4459</u> Linear Feet
	Class 3: _____ Linear Feet	Class 4: _____ Linear Feet
Signalized Intersections:	New Bike Boxes: <u>0</u> Number	Timing Improvements: <u>0</u> Number
Un-Signalized Intersections:	New RRFB/Signal: <u>0</u> Number	Crossing-Surface Improvements: _____ Number
Mid-Block Crossing:	New RRFB/Signal: <u>0</u> Number	Crossing-Surface Improvements: <u>0</u> Number
Lighting:	Intersection: <u>0</u> Number	Roadway Segments: <u>0</u> Linear Feet
Bike Share Program:	New Station: <u>0</u> Number	New Bikes: <u>0</u> Number
Bike Racks/Lockers:	New Racks: <u>0</u> Number	New Secured Lockers: <u>0</u> Number
Other Bicycle Improvements:	#1: _____ #: _____	#2: _____ #: _____

Pedestrian Improvements

What % of the PEDESTRIAN related project cost are going towards closing a "Gap" in infrastructure? 100 %
(As opposed to cost going towards "improving" existing pedestrian infrastructure.) 5048

Sidewalks:	New (4' to 8' wide): 6,407 <u>5048</u> Linear Feet	New (over 8' wide): <u>0</u> Linear Feet
	Widen Existing: <u>0</u> Linear Feet	Reconstruct/Enhance Existing: <u>0</u> Linear Feet
	New Barrier Protected (Barrier, parking, functional-planter, etc.): _____ Linear Feet	
ADA Ramp Improvements:	New Ramp (none exist): 23 <u>24</u> Number	Reconstruct Ramp to Standard: <u>0</u> Number
Signalized Intersections:	New Crosswalk: <u>0</u> Number	Enhance Existing Crosswalk: <u>4</u> Number
	Ped-Heads: <u>0</u> Number	Shorten Crossing: <u>0</u> Number
	Timing Improvements: <u>0</u> Number	
Un-Signalized Intersections:	New Traffic Signal: <u>0</u> Number	New Roundabout: <u>0</u> Number
	New RRFB/Signal: <u>2</u> Number	Crossing-Surface Improvements: <u>15</u> Number
	Shorten Crossing: <u>0</u> Number	
Mid-Block Crossing:	New RRFB/Signal: <u>0</u> Number	Crossing-Surface Improvements: <u>0</u> Number
Lighting:	Intersection: <u>0</u> Number	Roadway Segments: <u>0</u> Linear Feet
Pedestrian Amenities:	Benches: <u>0</u> Number	Trash Cans: <u>0</u> Number
	Shade Trees: <u>0</u> Number	Shade Tree Type: _____
Other Ped Improvements:	#1: <u>Upgrade signalized intersections to include timing improvements, push buttons and bike detection: 2</u> #: _____	#2: _____ #: _____

Multi-use Trail Improvements

Vehicular-Roadway Traffic-Calming Improvements

Road Diets:	Remove Travel Lane: <u>0</u> Linear Feet	Remove Right-Turn Pocket: <u>0</u> Number
Speed Feedback Signs:	Speed Feedback Signs: <u>0</u> Number	
Signalized Intersections:	Timing Improvements: <u>0</u> Number	New Roundabout: <u>0</u> Number
Un-Signalized Intersections:	New Traffic Signal: <u>0</u> Number	New Roundabout: <u>0</u> Number
Other Traffic-Calming Improvements:	#1 _____ #: _____	#2: _____ #: _____

Comment	Date Provided	Method
<p>1.Additional Mid-Block Crossing Outreach – Videos/Content on How to Use.</p> <p>2.People using the bike lanes for both E-W direction. Potential preventions or more education on 1-way bike lanes?</p> <p>3.Better lighting or bike lane indication on the WB Bike Lane as it intersections with PCC ramp at the mid-block. No clear guidance for bikers to re-align as they cross the ramp.</p> <p>a.Discussed thermoplastic arrows along the path of travel.</p> <p>4.Grade of Speed table ramps at both ends. Few complaints coming in and if possible, a smoother transition at each end of the speed table would be desired.</p> <p>a.Potential repaving the entry to the driveway to fix failing asphalt.</p> <p>5.WB Bus Pull-in Bay has very tight taper for the NCTD bus to pull-in, therefore, the bus doesn't hug the curb as it should. Cars have to go around.</p> <p>a.Discussed trimming the easterly bulb out to create more room for the bus to enter the pull-in area.</p> <p>6.More Green Paint in general on the bike paths.</p> <p>7.Adding 25MPH School Speed Limit Signs between Intersections.</p> <p>8. Flashing light pedestrian crossing that was installed in the entrance driveway of SDA does not make sense. The signs are behind the crossing path, and cars would not see it until they turn into the driveway which would be too late. It is also across the sidewalk from the bike lane so no bicyclist would use it. It seems like a waste of funds.</p>	4/23/2025	In-person
<p>I rode the pathway westbound yesterday for the first time.</p> <p>Some of the pavement surface is poor and looks like it is undone as well as some paint at crossings. It really is not finished yet, right? See photos below.</p> <p>I also rode eastbound yesterday like I usually do from Balour to El Camino Real on Santa age Dr. And the fact that this council has likely put the brakes on the eastern phase of the project is very disappointing and concerning to me. As a pedestrian, I see a lot of pedestrians using that segment, and there's nowhere for them to be, but either on a side goat path or walking in the bike lane. I plan to write to the council once again to share from my vantage point.</p>	4/16/2025	Email
<p>Unfortunate that the Eastern Phase is being hung up due to the Western Phase challenges. We need sidewalks going to el camino real.</p>	4/28/2025	Phone Call
<p>Dear Mr.Sulieyman</p> <p>This letter is to provide feedback on the Sante Fe Drive project.</p> <p>When exiting the parking lot at Venture Church located across the street from SDA, oncoming traffic cannot be seen when cars are parked in the angled parking.</p> <p>I almost was T-boned yesterday creeping out a few inches at a time to see the east bound traffic passing the church.</p> <p>A major accident will undoubtedly happen most likely with injuries. The east bound drivers are driving 40+MPH as they pass the church exit.</p> <p>Easiest solution is to get rid of the angled parking in front of the church so drivers exiting the church parking lot can see the oncoming traffic.</p> <p>Yours truly,</p>	4/29/2025	Letter
<p>Now that we have completed the observation period I would like to give a short synopsis of my thoughts concerning the West Santa Fe Drive project. Given the fact that I drive and cycle Santa Fe Dr. frequently, that I own a rental condo in Park Place, and that I am a member of the MTSC, I think my views should be considered relevant.</p> <p>Santa Fe Dr. is one of five major E-W major arteries in the town of Encinitas. As the major feeding artery to Scripps Encinitas Hospital it is especially important. On top of that it is also the street that adjoins our large and prestigious high school, San Dieguito Academy. While safety is always our first consideration, mobility is an increasingly important consideration in the evaluation of our roads as we undergo considerable population (and traffic) increase.</p> <p>The changes to Santa Fe Drive were driven by a former mayor who used the excuse of a tragic e-bike teen death on S. El Camino Real to justify changes to Santa Fe Dr. that she sought. They may exist, but I have yet to see any justification for making the expensive (both to construct and, if decided upon, to redesign and deconstruct) design that has been built. Be that as it may, I think my qualifications listed above allow me to say that this present design serves neither mobility or safety. As it now stands the newly constructed design will prove to be, by far, the most problematic part of safe traffic flow on the entire length of Santa Fe Dr. and probably the entire city of Encinitas.</p> <p>I have ridden and driven Santa Fe Drive many times during the construction and post construction phase in order to try to understand the logic behind this design. Although this is far from a systematic review of the current design I would like to list just some of the problems I see with it.</p> <p>1) At the west end of the construction sidewalks were widened to the point that the EB bicycle lanes could not be accommodated. Bike lanes are directed on to the sidewalk where cyclists, including heavy and fast e-bikes, will be competing with pedestrians and refuse containers for space. While cycling in California on sidewalks is legal and can be approved by a municipality, it is generally considered to be an inferior solution and to be avoided if at all possible because of the inherent danger of mixing 15 mph bicycles with 3 mph pedestrians.</p>	5/12/2025	Email

<p>Mayor Ehlers, Deputy Mayor Lyndes, and City Manager Campbell - In addition to completing the survey and coming to tonight's community workshop about the Santa Fe Dr Corridor Project, I decided to sit down and write you an email as well with a few more of my thoughts.</p> <p>First, the entire project (both completing the western phase and starting the eastern phase) should continue. The traffic calming features in the western phase have been effective, even with the project 'not fully baked'. In my experience, in a car, on a bike, and walking the section between Nardo/Mackinnon and Bonita/Windsor, the narrower lanes are creating friction as intended (slowing drivers down perhaps even without them realizing it), people have figured out the reverse angle parking and are doing it, including some very large trucks and Sprinter type vans, and the separated bikeways are being utilized by students and beyond. As the landscaping grows up, the corridor will be a lot more interesting, beautiful, and will have much needed shade as well. The transit stop is improved and even the quick build mid-block crossing is already an asset creating a safer crossing location used not only by SDA students, but pickleballers and others as well.</p> <p>Anyone saying that the entire western phase project should be scrapped away is being fiscally irresponsible + shortsighted from my vantage point and likely does not understand the intent of the project - encourage active transportation (walking, riding, and rolling) and provide a better transit set up as well. As you know, school zones are congested during high use drop off and pick up times and during special events. Here, there, and everywhere. Avoid all school locations if you can during these timeframes to be a good community member. And please understand that hundreds of students are riding their bikes/ebikes to school replacing car trips and that is a good thing for car drivers too - less cars on the road and less competition for that all important easy parking space.</p> <p>Safe Routes to School (& Everywhere) has benefits for everyone. We need better infrastructure, continued education, and consistent law enforcement presence to create safer streets together. And drivers who care and look out for other road users, especially vulnerable road users like bike riders and walkers.</p> <p>The construction of the western phase was far from perfect. The contractor (TriGroup) was so over their 120 working day commitment it was laughable at times. Very frustrating. Road users did not obey the detour signs and did not understand/want to understand that it was a construction zone. The protracted timeframe did not help matters. Many parked illegally in the reverse angle parking spaces</p>	5/13/2025	Email
<p>To whom it may concern:</p> <p>My husband and I have lived at 1408 Santa Fe Drive for 28 years. We have seen so many accidents in that time. Getting in and out of our driveway has become truly dangerous. One neighbor was hit while trying to exit our driveway. I was hit turning right off Santa Fe into our driveway. I had my signal on, brakes lights on and still I was rear-ended and sent airborne to the bottom of my driveway. I was also almost run into exiting my driveway by a teenager on an ebike who was flying down (to the East) Santa Fe in the bike lane, going against traffic. Had she actually hit my car she probably would have ended up on the hood of my car or in the street.</p> <p>II know my neighbors have requested a mirror be placed on the South side of Santa across from our driveway but it has always been rejected.</p> <p>As far as the improvements to Santa Fe, they are a disaster. Simply viewing the road looks like a hodge podge of colors, lines, flashing lights, zebra stripes and green bike lanes that go nowhere. I was behind a dually truck one evening turning left onto Santa Fe from Nardo. The driver obviously thought the bike lane paint was on the road as he made the turn. His rear tires ended upon the sidewalk next to the green line.</p> <p>I asked my 16 year old grandson (new driver) to drive me down Santa Fe and tell me his thoughts. Here they are:</p> <ol style="list-style-type: none"> 1. There is too much going on. He couldn't concentrate on driving while watching the yellow lines, cross stripes, flashing lights, people walking and bikes going from the street up onto the bike paths, not to mention the traffic signal up ahead. 2, The lanes are too tight. He felt he could hit a parked car, the zebra curbs, or be hit by an oncoming vehicle that had crossed the center line to avoid all of the above on the other side. 3. Once past the high school the green bollards appear to once again compress the driving lane. There's just too much in that space. 4. It makes no sense in that tight area to ask young, teen-age drivers to try to back into parking spaces that go against the flow of traffic. Everyone behind them has to back-up - they get impatient, honk their horns and try to pass the driver by crossing the center line. <p>I am not an engineer but I feel like common sense and a lot of money went awry with this project. When visitors come over they are astonished that the changes that now exist were ever allowed to be made.</p>	5/13/2024	Email
<p>Please do not install planter boxes or backup parking on eastern phase. Remove the planter boxes, backup parking on west side. They cannot see cars coming on santa fe. People are parking parallel on the backup parking. The side streets are being used for alternative route. Use speed tables or dips for slowdown. Cars are not driving the speed limit, please put traffic back on Santa Fe.</p>	5/13/2025	Community Meeting
<p>Concern about signal timing, it has not been completed. Eastbound is being held up, at least 16 cars backed-up because the loop detectors have not been replaced. Once new loop detectors are placed, it should be fixed. Redundance in travel lane when it there's a bike lane. Kids on motorized bikes will used travel lane. Motorized bikes are not following rules of the road, no law enforcement is being</p>	5/13/2025	Community Meeting
<p>Warning signal, eastbound bikelane is dangerous and unsafe design. Northside can be dangerous if cobble gets on the lane, the landing will land me into the vegetation. The view of cars exiting cannot see bikes because cars are blocking view. Kids will not know what they are doing. The acceleration is steep.</p>	5/13/2025	Community Meeting
<p>Start eastern phase as soon as possible, and complete as designed. Narrow lanes will slow cars, reverse parking will require slow down parking. The appreciate the approved safety. Concern: Sharrows on an arterial street. Complete Protected intersection on western</p>	5/13/2025	Community Meeting
<p>Good westbound improvements. I feel very safe, spend money to improve the eastside corridor. I like it, thank you so much.</p>	5/13/2025	Community Meeting
<p>I feel safe walking around now. But walking from windor to I-5 feels unsafe. The mail notification was not provided. Drainage is a concern with higher curb. Why not build the sidewalk up higher on the Southside, east of Windsor?</p>	5/13/2025	Community Meeting
<p>Design had noble goals, but failed because of unforeseen consequences. Eastbound bikes are not using the bike lanes. People parking wrong in the Back-in Space. People being stuck in the planter boxes. Due to narrow EB Lanes, causing trouble to navigate.</p>	5/13/2025	Comment Cards

Yes, improve pavement condition (roadway and bikeway). Complete intersection work - pause created by council created an unsafe condition. Address ASAP. Yes to cutouts to help peds to/from cars. Start East Phase ASAP!	5/13/2025	Comment Cards
Improve, dont remove. Finish Intesection & East Side. Lanes should be straighter at intersection. Curbs should be shorter & beveled if possible. Dont put bike bars in the apron.	5/13/2025	Comment Cards

SANTA FE ON-LINE SURVEY TEXT RESPONSES

IP Address (REDACTED)	3. Provide your input on any project features you would like the City to keep, modify, add or remove.	4. Please provide any general feedback of the project here:
	<p>The narrowed lanes are far too narrow making cars drive outside of the lanes. The reverse parking slows traffic and causes many cars to nearly collide. The design requires bikes to cross with cars making it less safe for bikers. The amount of terrible design is evident by its need to cover the entire road in different colors, green, black, white. Intuitive design does not require so much visual mess to further the idea.</p>	<p>This project solved zero of the problems on Santa Fe. It causes more congestion, and is less safe for pedestrians, bikers and cars. The pickle ball players now walk on Santa Fe with no protections as they have no parking spots and the traffic in/out of the parking lot is dangerous and they cannot see past the now diagonally parked cars! I would never park on this street due to the need to reverse into the spot. Please hire a professional to build these designs, someone with a proven portfolio. This project and the 101 have made our city go from a place of historic beauty and local culture to nightmare urban sprawl looking. They look cheap, not thoughtful and don't work</p>
	<p>Please widen the streets. It's causing drivers to not drive in the lines for the fear of hitting the curbs. This is extreme dangerous.</p>	
	<p>Love the large/ wide sidewalks</p>	
	<p>I just biked through the project for the first time today and I felt very safe! It is a huge improvement from what was there before. I also drive on Santa Fe frequently since I live nearby and all the cars are accurately using the back in angled parking, which is nice to see. I like how the narrow lanes slow down traffic in a busy school zone. My son will walk to San Dieguito Academy next year and, with this new project, I know he will be more safe. Building the east phase as soon as possible would also help. Currently he would have to walk on the shoulder of Santa Fe where there is no sidewalk to get to school.</p>	
	<p>Angled back in parking. especial for young drivers and older drivers. oncoming traffic and bikes can be seen better.</p>	<p>Took a while to complete.</p>
	<p>Residents on Santa Fe were not notified that the upgrades would disrupt receiving mail. Without notice, our mail has not been delivered since the installation of the green cones separating the bike lane from traffic. The city defers the issue to the Post Office & the Post Office defers the issue to the city. We've heard through word of mouth that a multiunit mailbox will be installed at a yet to be determined location. Those units are known to be insecure especially if installed separate from the residences. This is again done without the voice of the residents.</p>	<p>Seek out the voices of those directly impacted (residents, businesses, etc) rather than those who merely drive on the road.</p>
	<p>Insainley impractical and creates even worse traffic on Santa Fe after school gets out, should not take 15+ minutes to get fo the highway</p>	<p>Stop making the city undrivable</p>

SANTA FE ON-LINE SURVEY TEXT RESPONSES

IP Address (REDACTED)	3. Provide your input on any project features you would like the City to keep, modify, add or remove.	4. Please provide any general feedback of the project here:
	If the intent was to slow traffic by the school, you've succeeded. But you have basically re-defined what is a major E-W conduit to the freeway and the hospital accessibility.	
	I'm very happy about the bike lanes and sidewalk. I am worried that the more narrow lanes and reverse angle parking will cause a lot of congestion	
	Road is too narrow with the ridiculous back in parking	
	Remove the reverse angle parking, add more space for cars to travel!!!	It doesn't make sense to have reverse parking only accessible if you are driving eastbound on the street
	It is a complete disaster!! Nothing is working and it is a hazard to have so close to a school. Waste.	A complete wasteland
	Please stop wasting tax dollars to create even more congestion on the streets!	
	All the features should be kept in place. The pavement issues should be addressed. The parts of the project stopped for "pause and observe" should be completed asap, namely the two intersections. Unsafe conditions currently at those two intersections where the separated bikeways abruptly end. Southeast corner at Mackinnon and Nardo is a mess. Yes, TriGroup was a subpar contractor working way way way over it's 120 working day timeframe, but why did we as a city accept both their lack of performing to timeframes and the quality of their work?	If TriGroup bids again for City of Encinitas projects, I sure hope there is a way to ensure they are not the lowest bidder and therefore given more work. Also, the eastern phase of the Santa Fe Dr project needs to be done. It is very different than the western phase, more straightforward, and much needed. Do it !!! It would be irresponsible not to at this point. Both fiscally and safetywise.
	Keep all features and complete the project as designed.	Better communication needed throughout. Ditch the contractor. They did a poor job overall. Delays and lack of understanding have a really bad first impression.
	Reverse parking is impossible when someone is ALWAYS too close to your back end to even attempt, and it blocks traffic. Lanes are way too narrow. Why have planters where there could be more spaces?!	
	REMOVE:1) All non- parallel parking to allow for more lanes (emergency and traffic flow) 2)Planters 3)The confusing painted stripes 4) Remove all obstacles that impede traffic as Cardiff only has about 4 east/west streets and Sanra Fe us used for emergency and medical office traffic as well as numerous schools, senior rehab center, churches etc.	This new project has created an environment in which I am frequently NOT SEEN AS A PEDESTRIAN and have had numerous close calls almost being g struck by cars while I'm in a cross walk. The non parking near corners has not been enforced and new curb with fire hydrant at Sata Fe and Blue Sky is NOT EVEN PAINTED RED. It seems to be big liability NOT to have an emergency center lane for all the busy sections of Santa Fe. Please remove all ridiculous back in parking g and oversized planters to return to a safer flow of critical traffic.
	I am not fond of having back-in parking the only option, the lanes are entirely too narrow and the planters take up and enormous amount of space. I think this all presents potential issues with a school and hospital on Santa Fe. Emergency vehicles will be challenged trying to get through here when there is traffic.	
	The angled parking plus, a dedicated and approved use of traffic lanes for bikes is just too much. Why not just tell drivers they can't use the street?!	Whoever thought angled parking would work needs to go back to class on how to assess possible problems.
	The general population is NOT smart enough for these reverse angled parking spots. Please know your audience and try to make the problem better, not worse!	Unrelated, but you guys sure did ruin 101 with your last waste of tax payer money!

SANTA FE ON-LINE SURVEY TEXT RESPONSES

IP Address (REDACTED)	3. Provide your input on any project features you would like the City to keep, modify, add or remove.	4. Please provide any general feedback of the project here:
	Scrap the entire project and start over	This is dangerous for cars, pedestrians, bicyclists, and emergency vehicles that travel on this road. Anyone who has business on this street now avoids it because of the chaos due to the poor design.
	Start over because this is a complete circus.	
	Remove back in parking, make travel lanes wider, take bike land off sidewalk.	I now avoid traveling on Santa Fe when possible. Limited visibility, traffic backs up.
	Leave it alone, it's not broken don't fix it, it has been working well for years	The city has f-ed up the corridor from McKinnon to Windsor, now admit it was a mistake and reverse the damage.
	Miss the wide streets. Parking dividers jet way too far into street. Backing in parking seems more dangerous than helpful.	I also worry about ambulance traffic not being able to transport emergencies quickly due to congestion. I also witness the bus drivers limited ability to make the turns from SDA East and onto Santa Fe heading West.
	It would be nice to return it to what it was beforehand. It's now an overpriced, dangerous eyesore.	I can't even imagine how many accidents will be caused by your lame brained back in parking on Santa Fe! We live on Blue Sky dr. at Santa Fe dr. Traffic is even more congested every afternoon thanks to narrowing Santa Fe dr.as well, andeven more commuters come rocketing down our street to avoid the light at Windsor/Bonita and Santa Fe.
	With only 1 lane of traffic in each direction, it is disruptive to the traffic flow when someone stops to reverse into the reverse angle parking. It would be better suited if traffic could move over to another lane to allow the person space and time to park in that configuration, or simply park straight in.	It took much longer than the expected timeline, and the planned safety components were, disappointingly, not all included. We are very much looking forward to the continuation to the East Corridor phase and are hoping it will be completed in a timely manner. Further, given the recent communications from the city, we are concerned that work may not proceed at all - the messaging has not been clear.
	The reverse parking is dangerous. The potential for tragedy is palpable. The separated bike lanes are not being used by cyclists, at least the times I have driven when cyclists are present. The narrowed lanes are ridiculous. Another tragedy waiting to happen. Ambulance, firetruck etc during heavy traffic waiting for someone trying to back into a parking spot? People could die while waiting for their ambulance to get through. I think the reverse angled parking needs to go away. That alone would make the street safer. The landscaping is mostly attractive, however I question the wisdom of large, loose rocks available to be picked up and thrown. We need to acknowledge that people can act impulsively when angered, and the city is providing ammunition (literally) to someone who is unhinged and angry at the traffic that these changes will inevitably cause. And just wait till someone falls on one of those spiky agaves. The landscaping is a great idea, but there are some aspects that need to be reconsidered.	The city has been trying to convince people to get out of their cars and ride bikes or walk, so is reducing the size and number of lanes to encourage (force?) people out of their cars. But then the city adds an increased number of parking spaces at the expense of safe lane width so people can park their cars. The double message here is astounding. The good people of Encinitas are expected to accept large housing projects with little or no parking to help achieve these goals, but then safe lane configurations are sacrificed for additional parking spaces? Makes no sense. I am beyond grateful that we have a new City Council who seems more qualified and sensible about how the city needs to be run moving forward, and correct the egregious decisions made by the former mayor and her appointed acolytes. . I have great confidence that this City Council can and will do whats safe, sensible and necessary to correct the poor decisions made in regards to the Santa Fe Dr project
	The lanes are so narrow. My car drives in the middle bumps the entire way down.	
	Birmingham and I-5 intersection west of the 5. Getting off the freeway, coming south, is at times, impossible to take a left and head east on Birmingham.	I think Encinitas is over reacting to change on Santa Fe. It could have been done better but it works. Ideally, I would like enforcement for ensuring back in parking, but other than that, it's fine.

SANTA FE ON-LINE SURVEY TEXT RESPONSES

IP Address (REDACTED)	3. Provide your input on any project features you would like the City to keep, modify, add or remove.	4. Please provide any general feedback of the project here:
	The Santa Fe West project has narrowed the lanes too much, remove the reverse angle parking - it feels like an accident waiting to happen when I am driving down that road. My commute to the hospital (Scripps) and adequate access for ambulances/fire department has been affected	Do not like these changes
	Make the parking parallel	This project is a disaster. It has increased my commute and made increasingly dangerous to drive up or down Santa Fe. There are numerous traffic concerns with this project due to the layout and manner of parking.
		Change it all back. It is a safety disaster and visual sensory overload. Google maps now tells me to use Melba instead of Santa Fe because Santa Fe is congested. Cars do not fit in the parking spots. People driving eastbound partly drive in the center lane because the eastbound lane is too narrow. There is a bike lane literally on the sidewalk in one section. People need put their trash bins in the bike lane. There are so many issues and safety hazards, people will get injured.
	Remove the green post in front of our houses so we can get our mail delivered again and get rid of the protected bike lane. Riders are still in the street not utilizing them at all. Make it so we don't have to waist gas and time getting our mail, the narrowed lanes has increased congestion and the speed of traffic going up and down Santa Fe has not changed saying that it has calmed traffic is a joke. The back in parking is extremely dangerous.	Essentially, you have made Santa Fe more dangerous just like what has happened along the san elijo corridor. In the future, maybe listen to the resident that reside in the area instead of the city planners and/or contractors that don't live in the area.
	Widen traffic lanes Have enforcement of speed limit and e bike safety laws	Poorly planned
	I participated with input prior to completion and it was completed as stated. I believe it is a huge enhancement and we live on Santa Fe Drive and look forward to the East corridor being completed.	other than the bike lane on the north side being a little rough and the traffic calming narrow lanes, the rest is great.
		This project sucks! Traffic has gotten worse. Terrible use of space
	Remove it all. There needs to be more space for vehicles. The back in spots make zero sense. This is dangerous and a liability for everyone especially all the teenage drivers of SDA and emergency vehicles.	Liability over the years will far exceed the cost to fix this now.
	Eliminate reverse-in parking; eliminate landscape features; both to increase vehicle lanes, esp in emergencies.	Please rethink this project. Emergency vehicles use Santa Fe daily. Reverse in parking stops traffic and is not used properly. With the impending housing project on Santa Fe, this will impact the traffic even further. Emergency vehicles need more space to navigate this area. Lanes are too narrow; emergency vehicles have no where to pass.
	Reverse parking in such a congested area is very unsafe for all. Traffic will be a mess.	This plan is more dangerous for pedestrians, bikes and cars. Traffic is/will be a nightmare. We need wider car lanes and sidewalks.

SANTA FE ON-LINE SURVEY TEXT RESPONSES

IP Address (REDACTED)	3. Provide your input on any project features you would like the City to keep, modify, add or remove.	4. Please provide any general feedback of the project here:
	At the intersections, the transitioning into the bike lane is bumpy and of low quality. It's a bummer that the contractor didn't do a better job there. Maybe this can be smooth out. And it would be better to not have the curbs of 1 inch height there. Maybe the contractor cut corners or it wasn't in their contract but the surfacing of the bike lane for the most part isn't that good. That could be improved one day if budget allows.	I definitely like the upgrades very much but I think the quality of the work in some instances is inferior. At the intersections the transition into the bike lane is bumpy and there is a curb. For biking it would have been better not to have a curb of an inch or so but a smooth transfer. Please continue the east side of Santa Fe Dr. Our kids can't safely ride their bikes to/from school there at all. And there is no side walk. I appreciate all these improvements that were done! Thank you!
	Please remove the reverse parking spots. They are so dangerous.	
	Its awful! It needs to be demolished. I hear people talking about it all the time and it's not liked by anyone. Who came up with this ridiculous road? Did they forget there's a hospital?	
	I had signal lights to exit SDA. Protected crosswalks for all crosswalks around SDA. No more merge left green lights.	
	Remove the project completely. It is a danger to public safety. This is an improper deeply flawed street design modification on a major collector street which THE best direct access to the hospital and freeway from New Encinitas.	This design is dangerous to the public and I find it hard to believe it was designed by an actual traffic engineer. Reverse angle parking has its place but this ROW is not wide enough to correctly allow it and yet it was crammed in. The stupid giant planters / spades are nonsensical and eliminate valuable parking spaces which could be used by the school and the businesses on that block. Why? The separate bike lane is useless as people will be walking on it and crossing over it from the cars to the sidewalk/school forcing bicycle traffic onto the street which has narrow lanes and no shoulders. Please over some the institutional ego, admit it was a mistake, and scrape it.
	The parking spaces are a nightmare. Single lane has made that road more congested.	Waste of tax payer money
	Remove all of it and make the lanes wider. This is ridiculous how narrow it is.	Bad design. The road is for cars not bikes.
	Traffic will increase with the lanes allowing bicyclists to use the traffic lanes and not bike lanes, specifically minors who do not understand road laws. Also safety concerns have increased with the confusion and congestion created with the project with multiple lanes being allowed for bikes and backing in parking.	N/a
	Remove it all	
	The slanted parking is horrible.	The construction took a really long time and traffic was bad. Now pulling out of evergreen is even more challenging.

Project Scope Change Request

Caltrans Analysis and Recommendations

Submittal Date: March 25, 2026

Implementing Agency: City of Encinitas
ATP ID: ATP3A-11-068S
Federal Project No.: 5446 (025)
PPNO: 1299A

Project Name: Santa Fe Corridor Bike and Pedestrian Improvements

Existing Project Description (ATP Database): The project will construct new sidewalk, curb, gutter and curb ramps and will also include pavement widening and installation of new bike lanes, pedestrian activated rectangular rapid flashing beacon (RRFB) and striping and signage along the corridor.

Existing Project Limits (ATP Database): The project is located in the City of Encinitas, along the north and south sides of Santa Fe Drive, between Windsor Road and El Camino Real, within the vicinity of San Dieguito Academy High, Ocean Knoll Elementary, Oak Crest Middle School, and Ada Harris Elementary.

Caltrans' Recommendations

Level of Scope Change: Major
Recommendation: Support

Scope to be Changed

The following is a brief explanation of the proposed scope change:

- The proposed scope change will remove 1,359 linear feet of the north side sidewalk on Santa Fe Drive between Crest Drive and El Camino Real.
- The proposed scope change will increase the Class II Bike Lanes (Non-Buffered) and Class II Bike Lanes (2-ft Buffered) by 380 linear feet and 1,547 linear feet, respectively. In total, the Class II bike lanes will increase from 2,532 linear feet to 4,459 linear feet.
- The proposed scope change will add timing improvements, upgraded pedestrian push buttons, and bike detections at the Santa Fe Drive/Crest Drive and Santa Fe Drive and Lake Drive Intersection.

Below is the summary of revised quantities from the proposed scope change:

Items Name	Original Quantity	Revised Quantity
Class II Bike Lanes (Non-Buffered)	2,532 Linear Feet	2,912 Linear Feet
Class II Bike Lanes (2-ft Buffered)	0 Linear Feet	1,547 Linear Feet
New Sidewalk	6,407 Linear Feet	5,048 Linear Feet
New ADA Ramps	23	24
Timing Improvements	0	2
Signalized Intersection - Upgrade Push Buttons	0	2
Signalized Intersection - Bike Detection	0	2

Reasons for the Scope Change

The originally proposed north side sidewalk on Santa Fe Drive between Crest Drive and El Camino Real has been found to be in direct conflict with a major existing drainage channel. To construct the new sidewalk, it would require demolition of the existing at-grade drainage channel, installation of new curb inlets, installation of a new drainage pipe system, and revisions to both the upstream and downstream drainage flowline elevations. Additional work to accommodate the sidewalk in this location would include construction of retaining walls due to the steep side slopes, as well as right-of-way acquisition to allow for the placement of the retaining walls. These additional storm drain improvements were not identified as necessary during the initial scoping of the project in 2016. Upon more detailed evaluation of the project site based on field surveys and potholing of existing utilities, it has become apparent that the cost to install the sidewalk in this location, with the additional storm drain improvements, is infeasible from a budgetary perspective. These unanticipated costs would require significant City of Encinitas (City) funding and major redesign efforts to allow for the sidewalk to be placed.

Following careful evaluation of project conditions, stakeholder input that was collected through community workshops and public hearings, and updated technical analyses, the City has determined various improvements that will be added to the scope to enhance the effectiveness of the new infrastructure installed in the project. These improvements include an additional curb ramp and upgrades to traffic signals to allow for more crossing time and more accessible push buttons.

Summary of Caltrans Analysis

Caltrans' recommendation is to support this scope change request because it still aims to deliver the active transportation need as identified in the project application. The project will still link the eastern portion of the corridor with important community destinations that include transit stops, schools, shopping centers, recreational facilities (Encinitas Community Park) and medical facilities (Scripps Hospital).

An estimate of the impact the proposed scope change would have on the potential of the project to increase walking and bicycling as compared to the benefits identified in the project application (increase or decrease in benefit):

- Overall, when compared to the original scope, the proposed changes are estimated to have a similar potential increase in walking and biking due to the proposed change will still provide continuous east-west route for pedestrians and bicyclists.

An estimate of the impact the proposed scope change would have on the potential of the project to increase the safety of pedestrians and bicyclists as compared to the benefits identified in the project application (increase or decrease in benefit):

- Overall, the proposed changes are estimated to have similar potential to increase the safety of pedestrians and bicyclists when compared to the original scope as the project's proposed changes remain similar to the type of improvements from the original scope.

Caltrans' Coordination with Requesting Agency

Caltrans HQ and District Local Assistance staff participated in a web meeting with the City and CTC ATP staff on 09/11/2025 and 12/16/2025. In addition, Caltrans HQ and District Local Assistance staff coordinated with the City throughout the preparation of this analysis.

Impact to Project Cost

The proposed scope change will result in an increase in the overall cost of the project construction. The City will utilize local funds for the difference in cost from the ATP funding.

Impact to Project Schedule

There is no change to the current project schedule.

Attachments:

1. Exhibit 25-D: Request for Project Scope Change