

ROAD REPAIR AND ACCOUNTABILITY ACT OF 2017  
PROJECT BASELINE AGREEMENT

RIV 15 CAPM

Resolution **SHOPP-P-2526-06B**  
(to be completed by CTC)

**1. FUNDING PROGRAM**

- Active Transportation Program
- Local Partnership Program (Competitive)
- Solutions for Congested Corridors Program
- State Highway Operation and Protection Program
- Trade Corridor Enhancement Program

**2. PARTIES AND DATE**

- 2.1 This Project Baseline Agreement (Agreement) effective on **May 14, 2026** (will be completed by CTC), is made by and between the California Transportation Commission (Commission), the California Department of Transportation (Caltrans), the Project Applicant, **Caltrans**, and the Implementing Agency, **Caltrans**, sometimes collectively referred to as the "Parties".

**3. RECITAL**

- 3.1 Whereas at its **3/20/2026** meeting the Commission approved the **State Highway Operation and Protection Program** and included in this program of projects the **RIV 15 CAPM**, the parties are entering into this Project Baseline Agreement to document the project cost, schedule, scope and benefits, as detailed on the Project Programming Request Form attached hereto as **Exhibit A**, the Project Report attached hereto as **Exhibit B**, the Performance Metrics Form, if applicable, attached hereto as **Exhibit C**, as the baseline for project monitoring by the Commission.
- 3.2 The undersigned Project Applicant certifies that the funding sources cited are committed and expected to be available; the estimated costs represent full project funding; and the scope and description of benefits is the best estimate possible.

**4. GENERAL PROVISIONS**

The Project Applicant, Implementing Agency, and Caltrans agree to abide by the following provisions:

- 4.1 To meet the requirements of the Road Repair and Accountability Act of 2017 (Senate Bill [SB] 1, Chapter 5, Statutes of 2017) which provides the first significant, stable, and on-going increase in state transportation funding in more than two decades.
- 4.2 To adhere, as applicable, to the provisions of the Commission:
- Resolution **[REDACTED]**, "Adoption of Program of Projects for the Active Transportation Program", dated **[REDACTED]**
  - Resolution **[REDACTED]**, "Adoption of Program of Projects for the Local Partnership Program", dated **[REDACTED]**
  - Resolution **[REDACTED]**, "Adoption of Program of Projects for the Solutions for Congested Corridors Program", dated **[REDACTED]**
  - Resolution **G-26-33**, "Adoption of Program of Projects for the State Highway Operation and Protection Program", dated **3/20/2026**
  - Resolution **[REDACTED]**, "Adoption of Program of Projects for the Trade Corridor Enhancement Program", dated **[REDACTED]**

- 4.3 All signatories agree to adhere to the Commission's Guidelines. Any conflict between the programs will be resolved at the discretion of the Commission.
- 4.4 All signatories agree to adhere to the Commission's SB 1 Accountability and Transparency Guidelines and policies, and program and project amendment processes.
- 4.5 Caltrans agrees to secure funds for any additional costs of the project.
- 4.6 Caltrans agrees to report to Caltrans on a quarterly basis; on the progress made toward the implementation of the project, including scope, cost, schedule, and anticipated benefits/performance metric outcomes.
- 4.7 Caltrans agrees to prepare program progress reports on a on a semi-annual basis and include information appropriate to assess the current state of the overall program and the current status of each project identified in the program report.
- 4.8 Caltrans agrees to submit a timely Completion Report and Final Delivery Report as specified in the Commission's SB 1 Accountability and Transparency Guidelines.
- 4.9 Caltrans agrees to submit a timely Project Performance Analysis as specified in the Commission's SB 1 Accountability and Transparency Guidelines.
- 4.10 All signatories agree to maintain and make available to the Commission and/or its designated representative, all work related documents, including without limitation engineering, financial and other data, and methodologies and assumptions used in the determination of project benefits and performance metric outcomes during the course of the project, and retain those records for six years from the date of the final closeout of the project. Financial records will be maintained in accordance with Generally Accepted Accounting Principles.
- 4.11 The Inspector General of the Independent Office of Audits and Investigations has the right to audit the project records, including technical and financial data, of the Department of Transportation, the Project Applicant, the Implementing Agency, and any consultant or sub-consultants at any time during the course of the project and for six years from the date of the final closeout of the project, therefore all project records shall be maintained and made available at the time of request. Audits will be conducted in accordance with Generally Accepted Government Auditing Standards.

## 5. SPECIFIC PROVISIONS AND CONDITIONS

- 5.1 Project Schedule and Cost  
See Project Programming Request Form, attached as Exhibit A.
- 5.2 Project Scope  
See Project Report or equivalent, attached as Exhibit B. At a minimum, the attachment shall include the cover page, evidence of approval, executive summary, and a link to or electronic copy of the full document.
- 5.3 Performance Metrics  
See Performance Metrics Form, if applicable, attached as Exhibit C.
- 5.4 Additional Provisions and Conditions *(Please attach an additional page if additional space is needed.)*

### Attachments:

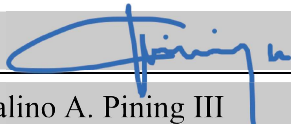
- Exhibit A: Project Programming Request Form  
Exhibit B: Project Report  
Exhibit C: Performance Metrics Form *(if applicable)*


SIGNATURE PAGE  
TO  
PROJECT BASELINE AGREEMENT


Project Name RIV 15 CAPM  
Resolution SHOPP-P-2526-06B  
*(to be completed by CTC)*

[Redacted Signature]  
[Redacted]  
Date  
[Redacted]  
Caltrans  
Project Applicant

[Redacted Signature]  
[Redacted]  
Date  
[Redacted]  
Caltrans  
Implementing Agency

  
Catalino A. Pining III  
Date 03/21/2026  
District Director  
California Department of Transportation

  
Cory Binns (Apr 27, 2026 14:58:36 PDT)  
Date 04/27/2026  
FOR Dina El-Tawansy  
Director  
California Department of Transportation

 for [Redacted]  
Tanisha Taylor  
Date 05/20/2026  
Executive Director  
California Transportation Commission

Baseline agreement information was extracted from Caltrans' project data systems. Project description, funding and performance measures are from CTIPS. Project delivery milestones are from PRSM. All information is current and accurate.

STATE OF CALIFORNIA • DEPARTMENT OF TRANSPORTATION

BASELINE AGREEMENT						Date:	03/25/26 04:14:41 PM
District	EA	Project ID		PPNO	Project Manager		
08	1L250	0819000168		3015U	ALARKAN, DALIA		
County	Route	Begin Postmile	End Postmile	Implementing Agency			
RIV	15	8.1	24.1	PA&ED	Caltrans		
				PS&E	Caltrans		
				Right of Way	Caltrans		
				Construction	Caltrans		
Project Nickname							
RIV 15 CAPM							
Location/Description							
In and near Murrieta and Lake Elsinore, from 1.4 miles south of Murrieta Hot Springs Road to 0.2 mile north of Nichols Road. Rehabilitate roadway, upgrade Transportation Management Systems (TMS) elements, Weigh in Motion (WIM) systems, lighting, and guardrail, construct facilities to Americans with Disabilities Act (ADA) standards. (G13 Contingency)							
Legislative Districts							
<b>Assembly:</b>	63, 71		<b>Senate:</b>	32		<b>Congressional:</b>	41, 48
PERFORMANCE MEASURES							
	Primary Asset	Good	Fair	Poor	New	Total	Units
Existing Condition	Pavement	25.3	57.1	13.5		95.9	Lane-miles
Programmed Condition	Pavement	95.9	0	0	0	95.9	Lane-miles
Project Milestone						Actual	Planned
Project Approval and Environmental Document Milestone						02/02/26	
Right of Way Certification Milestone							02/01/27
Ready to List for Advertisement Milestone							03/02/27
Begin Construction Milestone (Approve Contract)							01/16/29
FUNDING (Allocated amounts are shaded)							
Component	Fiscal Year	SHOPP					Total
PA&ED	24/25	1,899					1,899
PS&E	25/26	2,170					2,170
RW Support	25/26	236					236
Const Support	28/29	7,258					7,258
RW Capital	28/29	118					118
Const Capital	28/29	64,689					64,689
Total		76,370					76,370

# Memorandum

**To:** Rich Stone  
SHOPP  
HQ Financial Programming

**Date:** March 27, 2026

*Dalia Alarkan*

**From:** Dalia Alarkan, PE  
Project Manager  
District 8

**File:** 08-1L250  
0819000168  
08-Riv-15-8.10/24.10

**Subject: PROJECT STATUS UPDATE**

This memorandum is written to accompany the Baseline Agreement for the referenced project.

Since the Project Report was prepared, the estimate has been revised by the District's Design Team.

Current Funds (\$K):

Component	2026 SHOPP	Allocated	Current
PAED Support	\$1,899	\$1,899	\$1,899
PS&E Support	\$1,839	\$2,170	\$2,170
RW Support	\$200	\$236	\$236
Const. Support	\$7,258		\$7,258
RW Capital	\$62		\$118
Const. Capital	\$64,689		\$64,689

## Project Report For Project Approval

On Route RIV 015  
Between IN & NEAR MURRIETA, WILDOMAR & LAKE ELSINORE  
FROM 1.4 MI S/O MURRIETA HOT SPRINGS RD TO 0.2 MI  
And

I have reviewed the right of way information contained in this report and the right of way data sheet attached hereto, and find the data to be complete, current, and accurate:

*gg* Christine Senteno  
CHRISTINE L SENTENO, Acting Deputy District Director, Right of Way

### APPROVAL RECOMMENDED:

*AS* Dalia Alarkan  
DALIA ALARKAN, Project Manager

*JS* Antonía Toledo  
ANTONIA TOLEDO, Deputy District Director, Environmental

*WCH* Haissam Yahya  
HAISSAM YAHYA, Deputy District Director, Traffic Operations

*CQ* Jesus Galvan Jr  
JESUS GALVAN JR, Deputy District Director, Design

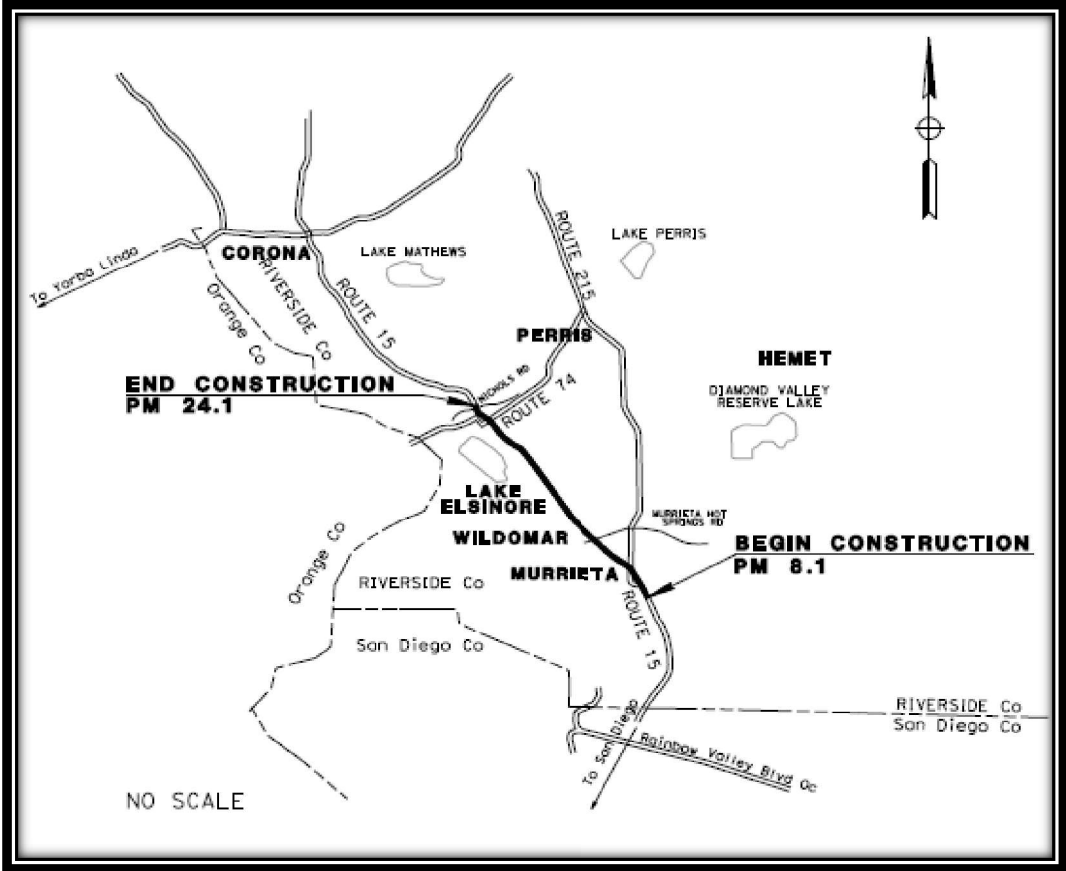
### PROJECT APPROVED:

Catalino A. Pinig III  
CATALINO A PINING III, District Director

02/02/2026

Date

### Vicinity Map



In Riverside County in Murrieta, Wildomar, and Lake Elsinore, from 1.4 miles south of Murrieta Hot Springs Road to 0.2 miles north of Nichols Road.

This project report has been prepared under the direction of the following registered civil engineer. The registered civil engineer attests to the technical information contained herein and the engineering data upon which recommendations, conclusions, and decisions are based.



---

RONALD Q. PHAM, REGISTERED CIVIL ENGINEER

12/02/2025

DATE



# Table of Contents

- 1. INTRODUCTION.....1**
- 2. RECOMMENDATION.....2**
- 3. BACKGROUND.....2**
- 4. PURPOSE AND NEED.....4**
  - 4A. Problem, Deficiencies, and Justification .....4
  - 4B. Regional and System Planning.....5
  - 4C. Traffic.....5
- 5. ALTERNATIVES.....7**
  - 5A. Build Alternative (Preferred).....7
  - 5B. No Build Alternative.....10
- 6. CONSIDERATIONS REQUIRING DISCUSSION.....10**
  - 6A. Hazardous Waste.....10
  - 6B. Value Analysis .....10
  - 6C. Resource Conservation.....10
  - 6D. Right of Way Issues .....10
  - 6E. Environmental Compliance .....10
  - 6F. Air Quality Conformity.....11
  - 6G. Title VI Considerations .....11
  - 6H. Noise Abatement Decision Report.....11
  - 6I. Life-Cycle Cost Analysis.....11
  - 6J. Reversible Lanes.....11
- 7. OTHER CONSIDERATIONS.....11**
  - 7A. Public Hearing Process.....11
  - 7B. Route Matters .....12
  - 7C. Transportation Management Plan .....12
  - 7D. Stage Construction .....12
  - 7E. Climate Change Considerations .....13
  - 7F. Asset Management .....13
  - 7G. Broadband and Advance Technology .....13
  - 7H. Complete Streets .....14
- 8. FUNDING, PROGRAMMING, AND ESTIMATE.....14**
- 9. DELIVERY SCHEDULE.....15**
- 10. RISKS.....16**
- 11. EXTERNAL AGENCY COORDINATION.....16**
- 12. PROJECT REVIEWS.....16**
- 13. PROJECT PERSONNEL.....17**
- 14. ATTACHMENTS.....18**

## 1. INTRODUCTION

### Project Description:

The project is located on Interstate 15 (I-15) from 1.4 miles south of Murrieta Hot Springs Road to 0.2 miles north of Nichols Road, Post Mile (PM) 8.1 to PM 24.1 in the cities of Murrieta, Wildomar, and Lake Elsinore, in Riverside County, Reference Attachment A, Location Map. The project scope includes the following:

- Perform individual slab replacement on mainline lanes
- 0.15' Hot Mix Asphalt – Type A (HMA-A) cold plane and overlay on shoulders, on-ramps, and off-ramps, including I-215/I-15 connectors
- Grind existing concrete pavement on all mainline lanes
- Upgrade Transportation Management Systems (TMS), including signals and ramp meter modifications
- Lighting Rehabilitation
- Install Conduits and Pull Boxes
- Eliminate sidewalk gaps on the south side of Wildomar Trail
- Convert the existing shoulder on the north side of Bundy Canyon Road to a Class II bike lane with signage and pavement markings
- Construct new curb ramps to meet the Americans with Disabilities Act (ADA) current standards
- Install Trash Capture Devices
- Upgrade Weigh in Motion (WIM) systems
- Install roadside safety improvements, including Maintenance Vehicle Pullouts (MVPs), paving beyond gore areas, and light-duty maintenance access trails
- Upgrade existing guardrail and end treatments to current standards, including anchor block installation and vegetation control (minor concrete).

<b>Project Limits</b>	08-RIV-15-PM 8.1/24.1	
<b>Number of Alternatives</b>	Build Alternative and No Build	
	<b>Current Cost Estimate:</b>	<b>Escalated Cost Estimate:</b>
<b>Capital Outlay Support</b>	\$10,085,000	\$10,885,000
<b>Capital Outlay Construction</b>	\$51,276,000	\$58,926,000
<b>Capital Outlay Right-of-Way</b>	\$117,500	\$117,500
<b>Funding Source</b>	SHOPP Pavement Preservation (20.XX.201.121)	
<b>Funding Year</b>	2026/2027	
<b>Type of Facility</b>	6-Lane Freeway	
<b>Number of Structures</b>	0	
<b>SHOPP Project Output</b>	95.9 Lane Miles of Pavement Preservation	
<b>Environmental Determination or Document</b>	Categorical Exemption (CE) for California Environmental Quality Act (CEQA) and Categorical Exclusion (CE) for National Environmental Policy Act (NEPA)	
<b>Legal Description</b>	In Riverside County, from 1.4 miles south of Murrieta Hot Springs Road in Murrieta to 0.15 mile north of Nichols Road in Lake Elsinore	
<b>Project Development Category</b>	5	

## 2. RECOMMENDATION

It is recommended that this project report be approved for the build alternative and authority be granted to proceed to the Plans, Specifications, and Estimate (PS&E) phase.

## 3. BACKGROUND

### **Project History:**

Project Initiation Proposal (PIP) No. 4656, initiated and prepared by the California Department of Transportation (Caltrans) District 8 Office of Maintenance, was approved on May 04, 2022 which identified the need to do slab replacement on mainline lanes, 0.20' cold plane and overlay on shoulders, on-ramps, and off-ramps including I-215/I-15 connectors. Early in phase 0, the Project Development Team (PDT) agreed to reduce the cold plane and overlay depth from 0.20' to 0.15', as this thickness was determined to adequately address existing pavement conditions while reducing material quantities and overall construction costs without compromising performance. The project also includes scope to grind the existing concrete pavement on all lanes of the mainline, upgrade TMS including signals and ramp meter modifications, upgrade lighting to current standards, eliminate sidewalk gaps, convert the existing shoulders on northside of Bundy Canyon Road to Class II bike lane with signage and pavement markings, construct new ADA curb ramps and refuge pads, upgrade WIM systems,

install roadside safety improvements including MVPs, paving beyond gore areas, and light duty maintenance access trails, upgrade existing guardrails and end treatments to current standards including vegetation control (minor concrete).

### **Community Interaction:**

Caltrans makes it a priority to engage the public, stakeholders, the media, and others on any project that the Department is developing. This generally includes one or more of the following actions: holding and attending public meetings, meeting with partner agencies, sending out virtual notifications via social media and or emails, and sending out notification letters to partner agencies.

Local agencies were notified by letters dated January 09, 2026, about the scope of the project as well as their input was requested. To date no public outreach events have been scheduled. However, more communications will be done in the subsequent phases.

During the construction phase of the project, Caltrans's Public Information Officers, in coordination with the Resident Engineer, will arrange and facilitate outreach programs to inform and engage residents, businesses, and agencies about the construction process and mainline/ramp closures.

### **Existing Facility:**

I-15 is a major truck/passenger route that starts at the junction of I-5 in San Diego, 10 miles north of the U.S./Mexican border, and ends at the U.S./Canadian border. The District 8 portion of the route starts at the Riverside/San Diego County Line and ends at the Nevada State Line. Existing drainage systems are in place along project limits and may require modification or enhancement to accommodate upgraded safety features and trash capture requirements.

The total length of I-15 in District 8 is 239 miles. The route varies in width from four to eight lanes within District 8. It is part of the Freeway and Expressway system, the National Highway System, the National Network, and the Strategic Highway Network, and is identified as the Strategic Interregional Road System Route in the Transportation Concept Report.

Within the project limits, the existing I-15 facility consists of three mainline lanes in each direction. The mainline travel lanes are constructed of concrete pavement, while the outside and inside shoulders are asphalt.

## **4. PURPOSE AND NEED**

### **Purpose:**

The purpose of this project is to preserve, repair, and extend the service life of the existing pavement and improve ride quality. This project will also upgrade lighting, TMS elements, WIM elements, guardrail, and end treatments with vegetation control, construct ADA curb ramps to the current standards, incorporate Complete Streets elements, install roadside safety

improvement to reduce the collision severity and to improve the safety of the traveling public and workers.

**Need:**

This segment of I-15 has deficiencies in the pavement condition due to the heavy and continuous traffic that has resulted in excessive areas of cracking and poor ride quality that are beyond routine maintenance and need repair to prevent further damage. Some TMS elements, WIM elements, guardrails and end treatments, and lighting within the project limits are either in poor condition or not in compliance with current standards. Other deficiencies in need of being addressed include Complete Streets elements, ADA curb ramps, and roadside safety improvements.

**4A. Problem, Deficiencies, and Justification**

The facility has been subjected to heavy traffic loads over a long period, which has distressed the existing pavement and caused it to deteriorate. Other facilities, such as pedestrian curb ramps and Metal Beam Guardrail (MBGR), are out of date and will be replaced to meet current standards. With this project, it is proposed to perform individual slab replacement, grind lanes, and cold plane and overlay asphalt concrete (AC) ramps and shoulders with HMA to extend the service life of the existing facility, maximize the productivity of the transportation system, and improve the ride quality along this segment of I-15.

**4B. Regional and System Planning**

Route I-15 begins at its junction with Interstate 5 (I-5) in San Diego County and ends at the United States/Canada International Border in the state of Montana. The District 8 portion of the route starts at the Riverside-San Diego County line and ends at the Nevada State line. The total length of I-15 in District 8 is 239 miles. The route varies from four to eight lanes in width. For the purposes of this study, I-15 is divided into 21 segments. Segments 1 through 18 traverse urbanized areas surrounding the greater Los Angeles Metropolitan area. Segments 19 through 21 serve traffic traveling to the rural desert and mountain areas of San Bernardino County and other states. I-15 is a major interstate goods-movement commuter corridor, which links to the Los Angeles Metropolitan area. It is a primary link between major economic centers and geographic regions. Weekend and holiday recreational traffic volumes on the route are exceptionally high since it serves as a connection to the city of Las Vegas and to the Colorado River area via Interstate 40 (I-40). The proposed project relates to the Route Concept Report, Regional Transportation Plans, and local planning documents. The proposed project is consistent with statewide, regional, and local planning goals and will be coordinated with governmental, regulatory, and private agencies in the area, if needed, to ensure consistency with specific local goals and objectives.

#### 4C. Traffic Data

<b>Mainline Traffic Data Information</b>					
Source: Caltrans Census Program					
<b>Year</b>	<b>2025</b>	<b>2029</b>	<b>2039</b>	<b>2049</b>	<b>2069</b>
<b>Annual Average Daily Traffic (AADT)</b>	132,100	136,500	148,100	160,600	186,000
<b>2-way Peak Hour Volume (PHV)</b>	9,900	10,200	11,000	12,000	13,900
<b>One-way PHV</b>	5,400	5,600	6,200	6,800	7,900
<b>Directional Split</b>	54%	55%	56%	57%	57%
<b>Truck % in ADT</b>	9%	10%	12%	13%	13%
<b>Truck % in PHV</b>	5%	6%	7%	8%	8%
<b>V/C Ratio</b>	0.91	0.95	0.65	0.71	0.85
<b>Level of Service</b>	E	E	C	C	D

#### Collision Analysis

The following TASAS Table B summarizes collision rates for the northbound (NB) and southbound (SB) I-15 mainline from postmiles PM 8.1 to PM 24.1 in Riverside County. The TASAS Table B report identified below was generated on 08/07/2025, and it depicts collision rates per Million Vehicle Miles (MVM) for segments greater or equal to 0.5 miles. The collision rate history spans 36 months, from the start date of October 1, 2021, to the end date of September 30, 2024, as recorded in the Traffic Accident Surveillance and Analysis System (TASAS) for the locations listed below.

Table 1: TASAS Table B 36 months Collision Rates ( 10/01/2021 – 09/30/2024 )							
Location	TOTAL No. of Collisions	ACTUAL Rates (per million vehicle miles)			AVERAGE Rates (per million vehicle miles)		
		Fatal	Fatal+ Injury	Total <sup>(1)</sup>	Fatal	Fatal+ Injury	Total <sup>(1)</sup>
Riv I-15 NB PM 8.1 – 24.1	359	0.004	0.11	0.33	0.006	0.38	1.16
Riv I-15 SB PM 8.1 – 24.1	407	0.008	0.16	0.37	0.006	0.38	1.16

Source: Caltrans, Traffic Accident Surveillance and Analysis System (TASAS). Data were retrieved on 08/07/2025.

Note:

All reported crashes (includes Property Damage Only (PDO) Collision)

Shading denotes that collision rates are higher than the statewide average for similar facilities. Table 1 (TASAS Table B 36 months Collision Rates (10/01/2021-09/30/2024)) summarizes and compares the actual collision rates to the average rates for similar facilities throughout the State. The Total collision rates include all reported collisions: Fatal, Injury, and Property Damage.

Detailed analysis per the TASAS Selective Accident Retrieval (TSAR) generated on 08/07/2025, shows the types of collision and primary collision factors for the studied segments were as follows:

Types of collision for segment NB Riv I-15 from PM 8.1-24.1 were:

- 170 collisions were Rear End,
- 94 collisions were Sideswipe,
- 65 collisions were Hit Object,
- 19 collisions were Overturn,
- 6 collisions were Broadside,
- 4 collisions were Other, and
- 1 collision was Head-On.

The primary collision factors for segment NB Riv I-15 from PM 8.1-24.1 were:

- Spreading,
- Improper Turn,
- Other Violations,
- Influence Alcohol,
- Other Than Driver,
- Unknown, and
- Follow too close.

Types of collision for segment SB Riv I-15 from PM 8.1-24.1 were:

- 185 collisions were Rear End,
- 92 collisions were Sideswipe,
- 92 collisions were Hit Object,
- 23 collisions were Overturn,
- 8 collisions were Other,
- 4 collisions were Broadside,
- 2 collisions were Auto-Pedestrian, and
- 1 collision was Head-On.

The primary collision factors for segment SB Riv I-15 from PM 8.1-24.1 were:

- Spreading,
- Improper Turn,
- Other Violations,
- Influence Alcohol,
- Other Than Driver,
- Unknown,
- Failure to Yield,
- Follow too Close.

### **Highway Safety Manual (HSM) Analysis**

Caltrans' Memorandum "Performance Based Decision-Making using Highway Safety Manual," dated April 4, 2022, guides performance-based safety analysis for highway design using the AASHTO Highway Safety Manual (HSM).

The HSM was considered and determined not to be applicable for this project because of the following reasons:

- There are no nonstandard design features proposed.
- There are no geometric or operational features that deviate from the existing conditions or from other project alternatives.
- There are no new interchanges or modifications to existing interchanges.

This project is expected to enhance motorist safety through the implementation of roadside safety measures, including the replacement of MBGR with MGS.

## 5. ALTERNATIVES

### 5A. Build Alternative – Programmable Project Alternative

#### Proposed Engineering Features:

Under this alternative, the following engineering features are proposed:

Refer to Attachment B for the Cross Section and Layouts.

- Perform individual slab replacement on mainline lanes
- Perform 0.40' Replace Asphalt Concrete Surfacing (Digouts) for ramps and shoulders.
- 0.15' cold plane and overlay on shoulders, and on-ramps and off-ramps at Murrieta Hot Springs Road, California Oaks Road, Wildomar Trail, Bundy Canyon Road, Main Street, Central Ave, Nichols Road, including I-215/I-15 connectors with HMA-A, for both directions
- Replace deteriorated base with 0.35' to 0.45' of Lean Concrete Base Rapid Setting (LCBRS) or Rapid Strength Concrete (RSC)
- Grind the existing concrete pavement on all lanes on the mainline
- Upgrade TMS including signals and ramp meter modifications at 19 locations between 8.1 to 24.1
- Upgrade lighting to the latest current standards
- Install 4 trash capture devices at PM 14.5/14.7, 21.35, 21.44, and 22.20
- Eliminate sidewalk gaps at Wildomar trail near PM 15.1
- Convert the existing shoulder on north side of Bundy Canyon Road to Class II bike lane with signages and pavement markings
- Construct new curb ramps to ADA standards (STAA Truck as the design vehicle)
- Upgrade WIM systems
- Install conduit and pull boxes outside Caltrans R/W boundaries at PM 22.3 (Central NB ramp), PM 16.89 (Lemon St.), and PM 16.29 (Bundy Canyon NB ramp).
- Install roadside safety improvements including MVPs, paving beyond gore areas, and light duty maintenance access trails
- Upgrade existing guardrail and end treatments to current MASH standards including anchor blocks and vegetation control (minor concrete)

#### Design Standards and Deviation from Design Standards:

Based on the proposed scope of work, this project will not alter or introduce new roadway geometry features, and no new nonstandard features are proposed. Therefore, it is anticipated that a design standard decision document will not be required. However, if there are newly proposed nonstandard features in subsequent phases of the project, these need to be documented in a design standard decision document.

Ramp Metering

The loop detectors at the ramps will be replaced due to the cold plane and overlay. The ramp meters may need timing adjustment.

California Highway Patrol (CHP) Enforcement Activities:

The project may impact California Highway Patrol (CHP) enforcement activities during construction. Stage construction and lane closure will be staged to minimize the impact.

Highway Planting and Irrigation:

There are no improvements anticipated for the existing landscape irrigation facilities at this time. Disturbed plantings and irrigation facilities will be repaired or replaced and coordinated during the design phase.

Erosion Control:

Disturbed soil areas adjacent to rock blankets, hardscape, mulch, and native soils will be treated with erosion control measures to match the surrounding areas per the direction of the district landscape architect.

Earth Retaining System:

This project does not propose any modification to existing earth retaining systems. All existing earth retaining systems will be protected in place.

Reversible Lanes:

This project does not qualify as a capacity-increasing or a major street or highway realignment project, and reversible lanes have not been considered.

Utility:

There is no utility impacts expected at this time. A list of utilities in the project limits was created by review of as-built plans. A DigAlert search will be conducted during PS&E to verify whether the utilities listed are still within the project limits. Utility companies will also be contacted to provide facility maps. The utilities within the project area are listed below:

Table 2 – Utilities Within the Project Area

Utility	Owner
Electrical	Southern California Edison
Electrical Overhead	Southern California Edison
Gas	Southern California Gas
Sewer	Santa Ana Watershed PRO High-Pressure Sewer
Water	Eastern Municipal Water District
Water	Elsinore Valley Municipal Water District
Water	Rancho California Water
Water	Western Municipal Water District
Water Company	City of Lake Elsinore
Telecommunication Utilities	Spectrum
Telecommunication Utilities	Frontier
Telecommunication Utilities	AT&T
Telecommunication Utilities	Crown Castle
Telecommunication Utilities	Charter-Spectrum

## **5B. No Build Alternative**

This alternative would leave the existing facility in its current condition, and no proposed improvement would be made. As a result, there is no cost associated with this alternative. This alternative does not address the purpose and need.

## **6. CONSIDERATIONS REQUIRING DISCUSSION**

### **6A. Hazardous Waste**

An Initial Site Assessment (ISA) checklist was completed on November 25, 2025. Based on the ISA determination, there is a potential for naturally occurring asbestos and aerially deposited lead (ADL). Refer to Attachment F for the ISA Checklist.

### **6B. Value Analysis**

A Value Analysis (VA) Study was conducted from August 18 to August 21, 2025. See Attachment J for the Final VA Study Report Cover Page and for the Stakeholder Implementation Action Form. The alternative that has been adopted from the VA Study into this project are 1, 3, 4, 5, and 6, which propose reducing the assumed slab depth for ISR from 0.95' to 0.75' in cost estimate, assuming 60% of the slabs will require drill and bond (dowels) to account for adjacent slabs that may be replaced, expanding work windows for construction to reduce overall duration, removing the mobile barrier system and utilizing additional speed reduction measures and stationary impact attenuators to protect workers and the public during ISR operations, and further expanding work windows for construction to reduce overall duration.

### **6C. Resource Conservation**

Preserving existing materials and facilities through salvaging and/or incorporating previously salvaged materials or facilities will be considered during the next phase and during construction.

### **6D. Right-of-Way Issues**

The capital right-of-way cost is \$117,500 which includes \$97,500 for potholing. All work will occur within the existing Caltrans right-of-way.

Refer to Attachment G for the R/W Data Sheet

### **6E. Environmental Compliance**

The project is categorically excluded under the National Environmental Policy Act (NEPA). The project is Categorical Exempt under Class 1 (c) of the state CEQA guidelines. The categorical exemption/categorical exclusion (CE/CE) for the project was completed on January 5, 2026. The completed CE/CE Determination Form is included in Attachment I.

## **6F. Air Quality Conformity**

The scope of the Project has been evaluated as an exempt project that falls under the broad category of a "pavement rehabilitation" project, listed under Table 1 of the Caltrans Carbon Monoxide Protocol or Table 2 of 40 CFR 93.126. Thus, no Air Quality study is needed.

## **6G. Title VI Considerations**

The implementation of the viable alternative will not result in disproportionately high or adverse impacts on minority or low-income neighborhoods or communities. Caltrans and Federal Highway Administration (FHWA) policies demonstrate a commitment to Title VI of the Civil Rights Act, which provides that no person in the United States shall, on the grounds of race, color, or national origin, be excluded from participation in, be denied the benefits of or be subjected to, discrimination under any program or activity receiving federal financial assistance.

## **6H. Noise Abatement Decision Report**

The project scope falls under Type III of the Caltrans Traffic Noise Protocol and noise impacts are not anticipated under 23 CFR 772.7. Therefore, a noise impact analysis is not required. However, Standard Specifications will be followed to minimize construction-generated noise impacts.

## **6I. Life-Cycle Cost Analysis**

Per Life-Cycle Cost Analysis (LCCA) Procedures Manual, Section 1.4 "Caltrans' Policy", and HDM Topic 619.1, pavement work on the State Highway System for minor pavement rehabilitation projects are exempted from LCCA.

## **6J. Reversible Lanes**

This project does not qualify as a capacity increasing project, and reversible lanes have not been considered.

# **7. OTHER CONSIDERATIONS AS APPROPRIATE**

## **7A. Public Hearing Process**

A Draft Environmental Document (DED) was not prepared for this project. As such, no formal public circulation or comment period was conducted. Consequently, a public hearing was not offered or held.

## **7B. Transportation Management Plan**

The primary objective of the Traffic Management Plans (TMP) is to develop the scope and cost for the potential strategies to be used to maintain safe traffic movement through the construction zone, as well as to minimize traffic delays.

A detailed TMP will be prepared during the next phase, which will include Traffic Control, Construction Zone Enhanced Enforcement Program (COZEEP) and Public Awareness Campaign (PAC). The cost of the TMP strategies is currently estimated to be \$1,090,000 and is included in the total project cost estimate. The cost estimate includes Portable Changeable Message Signs and Alternative Temporary Crash Cushion.

Refer to Attachment H for the TMP

## **7C. Stage Construction**

The staging plan will allow all lanes to remain open during construction with short-term mainline lane and ramp closures.

Since I-15 is not accessible to bicyclists or pedestrians, most construction activities will not impact those users. Temporary pedestrian access routes will be provided during curb ramp, sidewalk, and bridge rail work.

## **7D. Climate Change Considerations**

The State Highway System (SHS) and other transportation infrastructure in the state are at increasing risk of damage and impacts from climate change and associated extreme weather events. Caltrans must account for climate change in planning and investment decisions.

### Greenhouse Gas Reduction (GHG) Measures

The purpose of the project is to restore the facility to a state of good repair and it is not expected to result in increased operational emissions as no additional capacity will be added; however, GHG emissions would occur during the short-term construction process.

Caltrans Construction Emissions Tool (CAL-CET) was used to estimate construction GHG emissions. Total construction emissions of GHGs would be 1,951 metric tons CO<sub>2</sub>e over the approximately 250-day construction period.

The following GHG reduction measures will be incorporated as part of the construction process, as a means of contributing to Caltrans' goals and mandates to reduce GHG and consider climate change:

- Limit idling to 5 minutes for delivery and dump trucks and other diesel-powered equipment.
- Schedule truck trips outside of peak morning and evening commute hours.

## Adaptation Measures

The proposed project site is not located within the coastal zone and is not situated within an area prone to sea level rise, or flooding. The project represents the rehabilitation of the pavement. The risk associated with these factors has been determined to be low, and no adaptation measures are anticipated to be necessary in this regard.

## **7E. Asset Management**

See attachment E for the SHOPP performance measures.

## **7F. Broadband and Advance Technology**

Although no broadband and advanced technologies are planned in this project, the proposed improvements will not impact the ability to include broadband and advanced technologies such as wired broadband facilities, fueling or charging opportunities for zero-emission vehicles, vehicle-to-infrastructure communications for transitional or full autonomous vehicles, and supporting high-speed data infrastructure in the future.

## **7G. Complete Streets**

### Pedestrian Facilities

Two (2) existing ADA Facilities will be brought to the current ADA standard. Enhanced Wet Night Visibility pavement marking will be utilized at crosswalks to improve visibility.

### Bicycle Facilities

One bike lane, sidewalks, and refuge pads are to be constructed at Wildomar Trail.

### Transit Facilities

There are no transit facilities within the project limits. The proposed improvements on local streets will not impact the addition of transit facilities in the future.

## **7H. Storm Water Quality**

### Stormwater Compliance

A Storm Water Data Report (SWDR) was prepared for this project to meet the demands of the Storm Water Management Plan regarding controlling pollutant discharge and meeting permit requirement. The information in the SWDR prepared for Project Acceptance and Environmental Document (PA&ED) phase will be verified and updated during the subsequent phases of the project.

Refer to Attachment L for the SWDR

### Trash Capture

According to the pending update to the Caltrans Statewide Trash Implementation Plan, this project falls within a Significant Trash Generating Area (STGA). Therefore, it is necessary to implement trash capture in this project. Trash capture will be utilized in this project via three Gross Solids Removal Devices (GSRD) and one infiltration device.

### Permits

A Stormwater Data Report has been prepared for this project to meet the demand of the National Pollutant Discharge Elimination System (NPDES) General Permit For Stormwater Discharges Associated With Construction And Land Disturbance Activities (General Permit), Order WQ 2022-0057-DWQ NPDES No. CAS000002, And NPDES Statewide Stormwater Permit And Waste Discharge Requirements For State Of California Department Of Transportation, Order 2022-0033-DWQ NPDES No. CAS000003.

## **8. FUNDING, PROGRAMMING AND ESTIMATE**

### Funding

This project is a candidate for programming in the 2024 SHOPP under the 20.XX.201.121 – Minor Pavement Rehabilitation.

### Programming

The project Estimate to Complete (ETC) assignment hours summary report indicated the resources provided by each functional unit. These hours have been added to Project Resource and Schedule Management (PRSM), which has calculated the non-escalated dollar estimate. The escalated dollar for each phase will be programmed.

Fund Source 20.XX.201.121	Fiscal Year Estimate for the Programmable Alternative							
	Pavement Preservation (CAPM)					Total Escalated/Current Amount	Programmed Amount	Amount Needed (Programmed Less Escalated)
	Current	24/25	25/26	26/27	27/28			
Component	<i>In thousands of dollars (\$1,000)</i>							
PA&ED Support	\$1,899	\$1,899				\$1,899	\$1,899	
PS&E Support	\$2,047		\$2,152			\$2,152	\$1,839	\$313*
Right of Way Support	\$212		\$236			\$236	\$200	\$36*
Construction Support	\$5,927			\$6,598		\$6,598	\$6,598	
<b>Total Support</b>	<b>\$10,085</b>	\$1,899	\$2,388	\$6,598		\$10,885	\$10,536	\$349
Right of Way	\$118			\$118		\$118	\$56	\$62
Construction	\$51,158			\$58,808		\$58,808	\$58,808	
Total Capital	<b>\$51,275</b>			<b>\$58,926</b>		<b>\$58,926</b>	<b>\$58,864</b>	<b>\$62</b>
<b>Grand Total</b>	<b>\$61,360</b>	<b>\$1,899</b>	<b>\$2,388</b>	<b>\$65,524</b>		<b>\$69,811</b>	<b>\$69,400</b>	<b>\$411</b>

The support cost ratio for this project is 18.5%. The statewide support to cost ratio under program 20.XX.201.121, Pavement Preservation (CAPM), is at an average of 20%. The support cost ratio is within the statewide average.

\* This added cost will be achieved through higher allocation, and within 118% of the programmed amount.

A Project Change Request (PCR) was approved by the district and was submitted to Headquarters and the California Transportation Commission (CTC) to request for the right of way capital increase.

## 9. DELIVERY SCHEDULE

Project Milestones		Milestone Date (Month/Day/Year)	Milestone Designation (Target/Actual)
APPROVE PID	M010	06/15/2023	Actual
PROGRAM PROJECT	M015	07/15/2024	Actual
BEGIN ENVIRONMENTAL	M020	11/14/2024	Actual
PA & ED	M200	02/02/2026	Target
RIGHT OF WAY REQUIREMENT	M224	02/02/2026	Target
REGULAR RIGHT OF WAY	M225	02/02/2026	Target
DRAFT STRUCTURES PS&E	M378	12/30/2026	Target
PS&E TO DOE	M377	08/03/2026	Target
RIGHT OF WAY CERTIFICATION	M410	02/01/2027	Target
READY TO LIST	M460	03/02/2027	Target
CONST CONTR PACKAGE TO DES-OE	M475	09/05/2028	Target
HEADQUARTERS ADVERTISE	M480	09/26/2028	Target
AWARD	M495	12/13/2028	Target
APPROVE CONTRACT	M500	01/16/2029	Target

CONTRACT ACCEPTANCE	M600	07/15/2030	Target
END PROJECT EXPENDITURES	M800	01/16/2032	Target
FINAL PROJECT CLOSEOUT	M900	12/17/2032	Target

## 10. RISKS

The revised Risk Register (Attachment D) identifies 18 risks for the proposed project. These risks are related to Regulatory Permitting, Delivery Schedule, Pavement Strategy, and Overlapping Projects. The probability of these risks ranges from very low to moderate.

## 11. EXTERNAL AGENCY COORDINATION

### Federal Highway Administration (FHWA)

This PR has been reviewed by Caltrans' FHWA Liaison, Sergio Avila on 11/18/2025, and this project is eligible for federal aid funding.

Per the current Joint Stewardship and Oversight Agreement between the California Department of Transportation (Caltrans) and FHWA, dated August 26, 2024, this project is considered a Delegated Project. However, should any future situation/circumstance arise that will potentially classify the project for Risk Based Project Involvement (RBPI), Caltrans shall notify FHWA and FHWA will reassess this project to determine if project is selected for RBPI and identify the specific FHWA involvement activities.

## 12. PROJECT REVIEWS

Scoping team field review	<u>PDT (Attachment M)</u>	Date <u>                    </u>
Scoping team field review attendance roster attached.		
District Hydraulics	<u>Alan Bisi</u>	Date <u>11/06/2025</u>
District Design Liaison	<u>Sergio Avila</u>	Date <u>11/06/2025</u>
Materials Engineering	<u>Sittampalam Sathiskumar</u>	Date <u>11/06/2025</u>
District Landscape Architect	<u>Almabeth Anderson</u>	Date <u>11/06/2025</u>
District Environmental	<u>Amy Lee</u>	Date <u>11/06/2025</u>
District Traffic Operations	<u>Siva Sivakkolunthar</u>	Date <u>11/06/2025</u>
HQ Project Delivery Coordinator	<u>Jeffrey Rud II</u>	Date <u>11/06/2025</u>
Complete Streets	<u>Cuong Trinh</u>	Date <u>11/06/2025</u>
Project Manager	<u>Dalia Alarkan</u>	Date <u>11/06/2025</u>
District Safety Review	<u>Danny Pheng</u>	Date <u>11/06/2025</u>
District Traffic Manager	<u>Mauricio Santa Cruz</u>	Date <u>11/06/2025</u>
Constructability Review	<u>Ihab Boulos</u>	Date <u>11/06/2025</u>
District Stormwater Quality	<u>Greg Clark</u>	Date <u>11/06/2025</u>

**13. PROJECT PERSONNEL**

Name	Title	Division	Phone Number
Dalia Alarkan	Project Manager	Project Management	(909) 746-3554
Gilberto Juaregui	APM	Project Management	(909) 519-0376
Ronald Pham	Office Chief	Roadway Design	(909) 893-2480
Hazem Shekfa	Transportation Engineer	Roadway Design	(951) 323-9352
Brandon Nguyen	Transportation Engineer	Roadway Design	(213) 317-0077
Aung Naing	Office Chief	Traffic Design	(909) 518-8559
Sammy Korani	Transportation Engineer	Traffic Design	(951) 255-4541
David Gonzalez	Office Chief	Electrical Design	(909) 501-5832
Luis Penaloza	Transportation Engineer	Electrical Design	(909) 893-2464
Morgan Itzel	Landscape Associate	Landscape Architecture	(909) 665-3304
Sittampalam Sathis	Office Chief	Material Engineering	(213) 605-5538
Beatrice Torres	Transportation Engineer	Material Engineering	(909) 665-4128
Thomas Guglielmana	Branch Chief	Construction	(951) 232-3777
Ihab Boules	Office Chief	Constructability	(951) 232-7582
Amgad Benjamin	Transportation Engineer	Constructability	(909) 844-9731
Md Shaheed	Branch Chief	Risk Coordinator	(858) 688-0414
Kimberly Portillo	Assistant Risk Manager	Risk Management	(916) 879-7946
Hannah Duarte	Acting Branch Chief	Environmental Planning	(909) 472-5756
Amy Lee	Environmental Scientist	Environmental Planning	(909) 261-3977
Siva Sivakulam	Office Chief	Traffic Ops	(909) 255-2368
Greg Clark	Office Chief	Storm Water Quality	(213) 317-0017
Tariq Jouzi	Transportation Engineer	Storm Water Quality	(909) 893-2175
Nazek Kayali	Transportation Engineer	Storm Water Design	(909) 462-8714
Alan Bisi	Office Chief	Hydraulics Design	(909) 665-3677
Bijor Decena	Transportation Engineer	Hydraulics Design	(840) 587-1868
Max Lin	Associate R/W Agent	Right of Way	(909) 518-3887
Max Auyeung	Office Chief	Utility Engineering	(909) 518-7084
Truc Bui	Transportation Engineer	Utility Engineering	(909) 518-7479
Raymond Yip	Sr Transportation Engineer	Maintenance (Asset M)	(213) 310-2477
Jim Kennedy	Office Chief	Surveying	(909) 665-3269

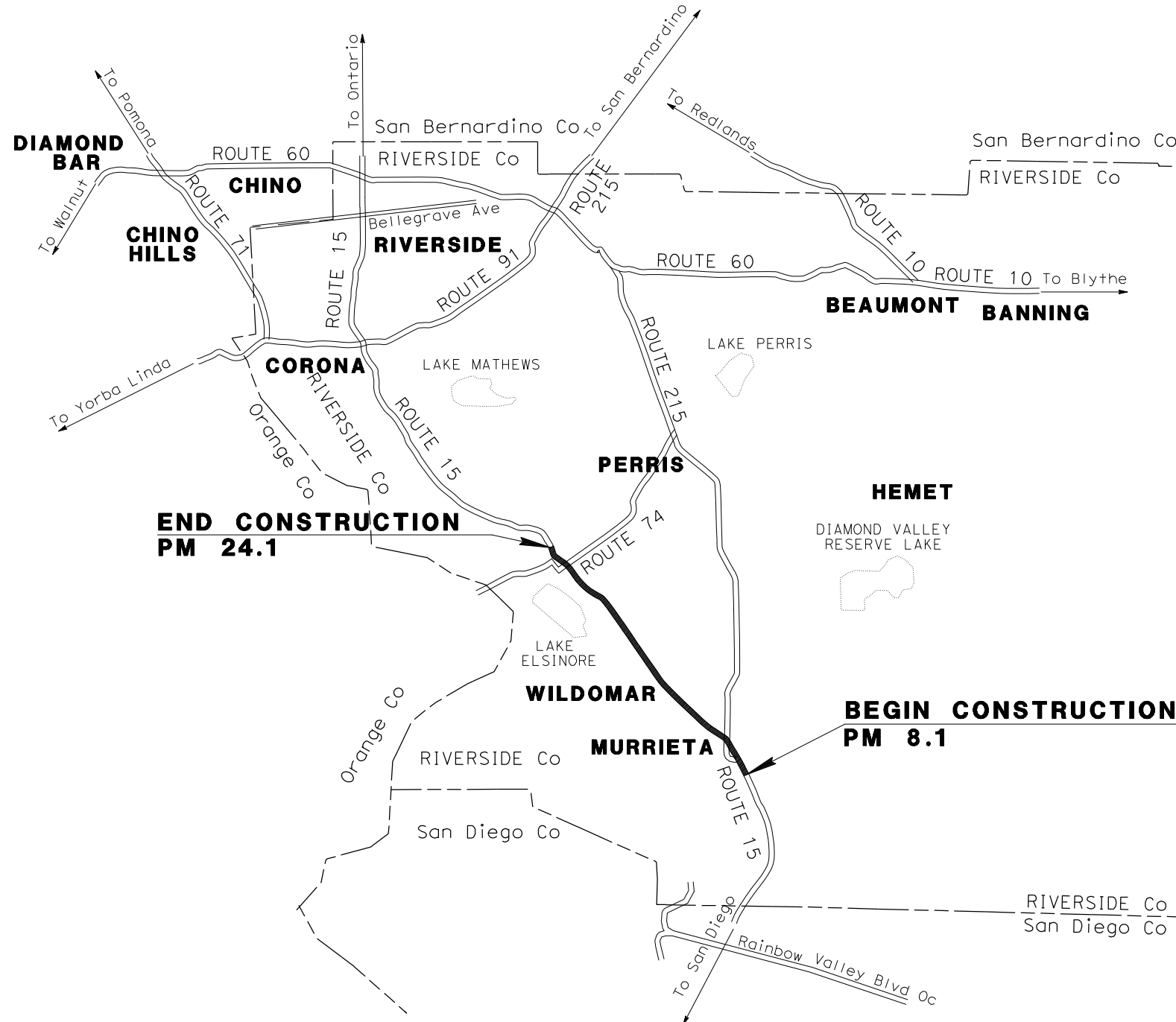
**ATTACHMENTS (Number of Pages)**

- A. Location Map (1)
- B. Typical Cross-Sections and Layouts (31)
- C. Preliminary Cost Estimate (10)
- D. Risk Register (4)
- E. SHOPP Performance Measures (1)
- F. ISA Checklist (2)
- G. Right of Way Data Sheet (10)
- H. Transportation Management Plan (TMP) Data Sheet (5)
- I. CEQA Categorical Exemption/NEPA Categorical Exclusion Determination Form (6)
- J. Final Value Analysis Study Report Cover Page (2)
- K. Complete Streets Decision Document (4)
- L. Storm Water Data Report Cover Page (1)
- M. Field Review Sign In Sheet (1)
- N. PRSM Quantities (1)

# Attachment A Location Map

**STATE OF CALIFORNIA**  
**DEPARTMENT OF TRANSPORTATION**  
**PROJECT PLANS FOR CONSTRUCTION ON**  
**STATE HIGHWAY**  
**IN RIVERSIDE COUNTY IN MURRIETA, WILDOMAR, AND**  
**LAKE ELSINORE FROM 1.4 MILES SOUTH OF MURRIETA**  
**HOT SPRINGS ROAD TO 0.2 MILE NORTH OF NICHOLS ROAD**

TO BE SUPPLEMENTED BY STANDARD PLANS DATED 2026



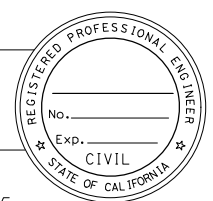
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

LOCATION MAP

PROJECT MANAGER

DESIGN MANAGER

PROJECT ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_  
 REGISTERED CIVIL ENGINEER



PLANS APPROVAL DATE \_\_\_\_\_  
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

CONTRACT No.	<b>08-1L2500</b>
PROJECT ID	<b>08190001680</b>

THE CONTRACTOR SHALL POSSESS THE CLASS (OR CLASSES) OF LICENSE AS SPECIFIED IN THE "NOTICE TO BIDDERS."

LAST REVISION 00-00-00  
 DATE PLOTTED => 13-JAN-2026  
 TIME PLOTTED => 08:26

Attachment B  
Typical Cross-Section &  
Layouts

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1	#	

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

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**NOTES:**

1. DIMENSIONS OF THE PAVEMENT STRUCTURES (STRUCTURAL SECTIONS) ARE SUBJECT TO TOLERANCES SPECIFIED IN THE STANDARD SPECIFICATIONS.
2. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
3. EXACT LOCATIONS OF INDIVIDUAL SLAB REPLACEMENTS TO BE DETERMINED BY THE ENGINEER.

**ABBREVIATION:**

HMA-A	HOT MIX ASPHALT (TYPE A)
LCBRS	LEAN CONCRETE BASE RAPID SETTING
PCC	PORTLAND CEMENT CONCRETE
AC	ASPHALT CONCRETE
AB	AGGREGATE BASE
AS	AGGREGATE SUBBASE
RAC-G	RUBBERIZED ASPHALT
RHMA-G	CONCRETE-GAP GRADED

**PAVEMENT STRUCTURE SECTIONS**

- 1 0.15' COLD PLANE  
0.15' RHMA-G
- 2 0.06' GRINDING EXISTING CONCRETE PAVEMENT
- 3 0.65' to 0.83' INDIVIDUAL SLAB REPLACEMENT (RSC)  
BASE BOND BREAKER  
0.35' to 0.45' LCBRS (As-needed)
- 4 0.30' HMA-A  
0.50' AB-Class 2

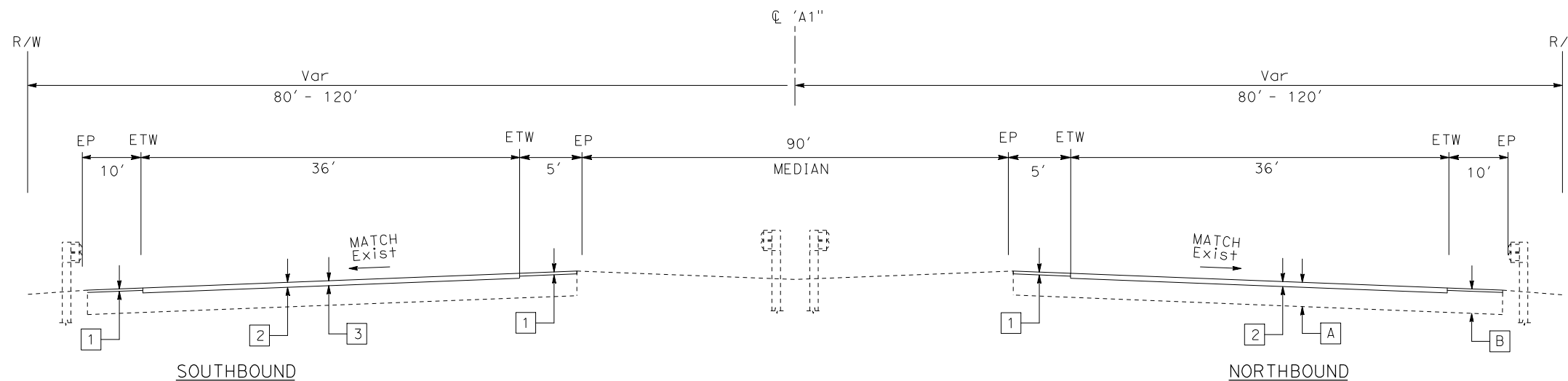
**Exist PAVEMENT STRUCTURE SECTIONS**

- A EXIST  
0.65' to 0.85' PCC  
0.35' to 0.45' PMCTB  
0.40' to 0.60' AS  
0.40' LCB
- B EXIST  
0.20' RAC-G  
0.75' to 0.95' AB  
0.10' to 0.20' AC  
0.40' to 0.60' AS
- C EXIST  
0.15' RAC-G  
0.65' AB  
0.25' to 0.35' AC  
0.55' to 0.90' AS

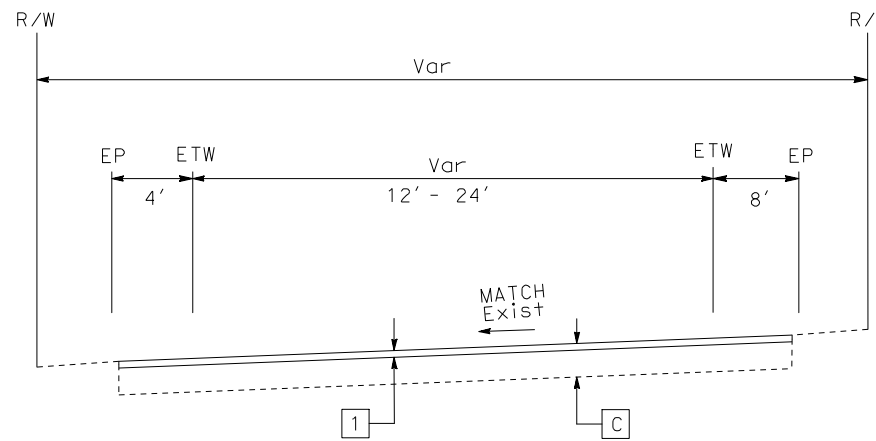
**DESIGN DESIGNATION (PM 8.1/24.1)**

ADT (2025)	132,100	D	54%
ADT (2069)	136,500	T	9%
DHV	5,400	V	70 mph
ESAL	126,077,276	TI <sub>20</sub>	14.5

PAVEMENT CLIMATE REGION: INLAND VALLEY



**ROUTE 15**  
PM 8.1 TO PM 24.1



**ON/OFF RAMP**  
**TYPICAL SECTION**

**TYPICAL CROSS SECTIONS**  
NO SCALE

**X-1**

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**

- NOTE:
- ALL DIMENSIONS AND ALL STRIPING ARE PRELIMINARY AND FOR CONCEPTUAL PURPOSE ONLY. ADDITIONAL DESIGN VARIATIONS MAY BE CONSIDERED IN SUBSEQUENT PHASES.
  - ALL RIGHT OF WAY DATA ARE APPROXIMATE. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

LEGEND:

- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY X' HMA-A

ABBREVIATIONS:

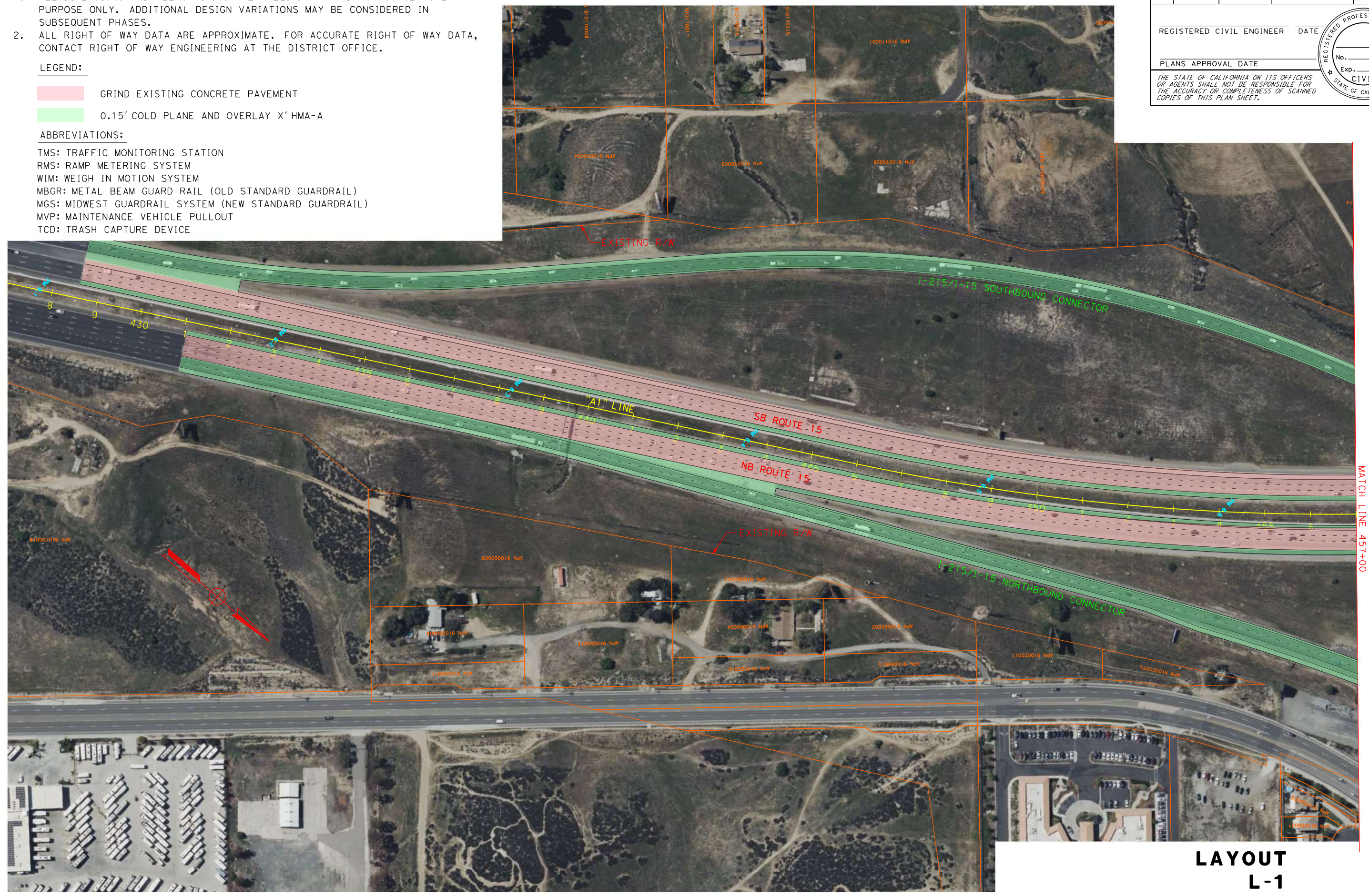
- TMS: TRAFFIC MONITORING STATION
- RMS: RAMP METERING SYSTEM
- WIM: WEIGH IN MOTION SYSTEM
- MBGR: METAL BEAM GUARD RAIL (OLD STANDARD GUARDRAIL)
- MGS: MIDWEST GUARDRAIL SYSTEM (NEW STANDARD GUARDRAIL)
- MVP: MAINTENANCE VEHICLE PULLOUT
- TCD: TRASH CAPTURE DEVICE

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_

PLANS APPROVAL DATE \_\_\_\_\_

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**LAYOUT L-1**

SCALE: 1" = 100'

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		





  

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

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**LEGEND:**

-  GRIND EXISTING CONCRETE PAVEMENT
-  0.15' COLD PLANE AND OVERLAY
-  UPGRADE EXISTING MBGR TO MGS
-  STAGING AREA






STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISOR	DATE
<b>Caltrans</b>		CHECKED BY		

**LAYOUT  
L-2**  
SCALE: 1" = 100'

LAST REVISION    DATE PLOTTED => 27-JAN-2026    00-00-00    TIME PLOTTED => 13:00

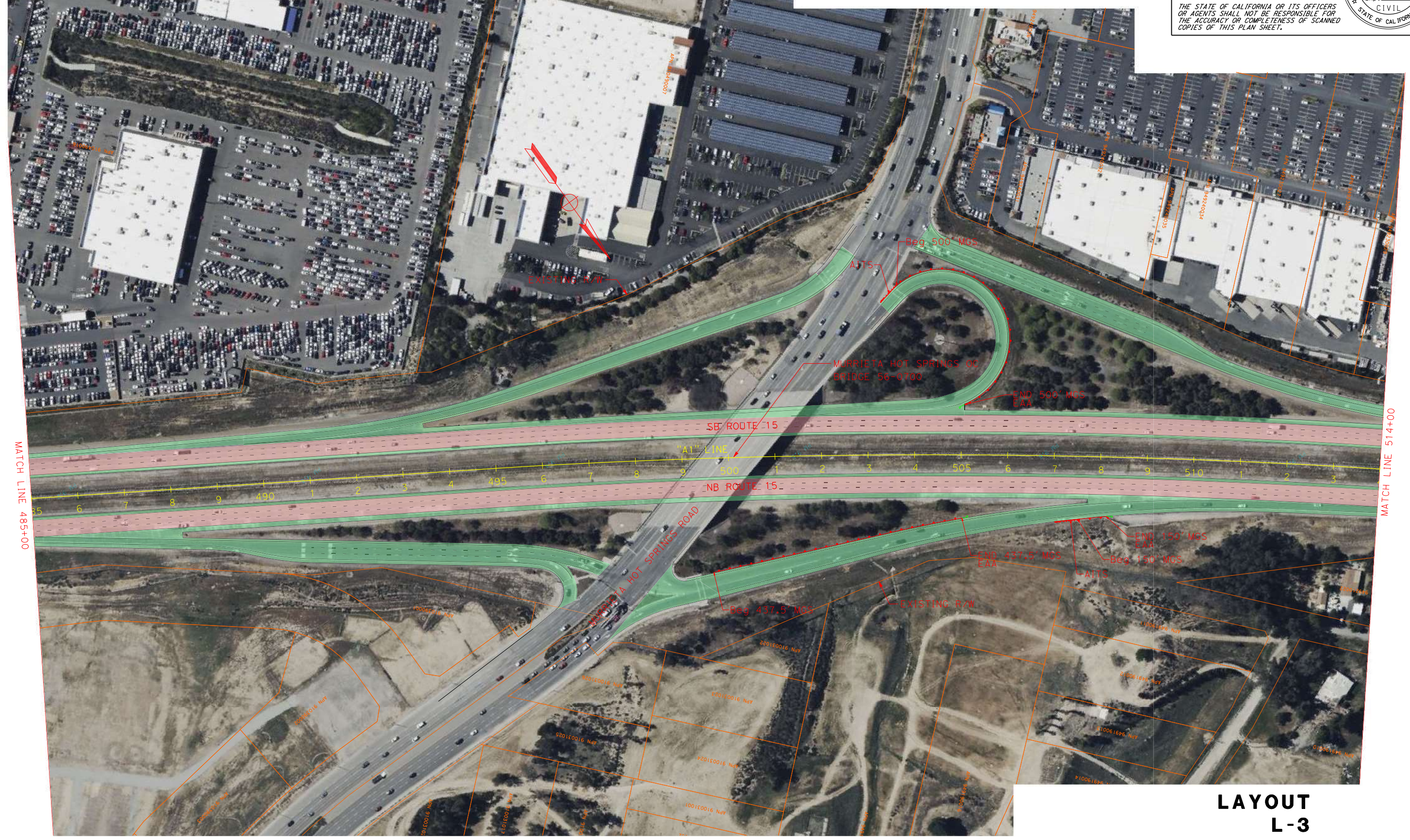
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**  
 FUNCTIONAL SUPERVISOR  
 CALCULATED-DESIGNED BY  
 CHECKED BY  
 REVISED BY  
 DATE REVISED

NOTE:  
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LEGEND:  
 GRIND EXISTING CONCRETE PAVEMENT  
 0.15' COLD PLANE AND OVERLAY  
 UPGRADE EXISTING MBGR TO MGS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
 PLANS APPROVAL DATE \_\_\_\_\_  
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**LAYOUT  
 L-3**

SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**

**NOTE:**

1. ALL DIMENSIONS AND ALL STRIPING ARE PRELIMINARY AND FOR CONCEPTUAL PURPOSE ONLY. ADDITIONAL DESIGN VARIATIONS MAY BE CONSIDERED IN SUBSEQUENT PHASES.
2. ALL RIGHT OF WAY DATA ARE APPROXIMATE. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

**LEGEND:**

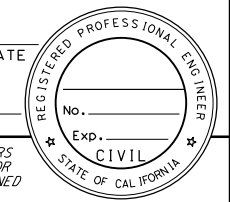
- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY
- UPGRADE EXISTING MBGR TO MGS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



**LAYOUT  
L-4**  
SCALE: 1" = 100'

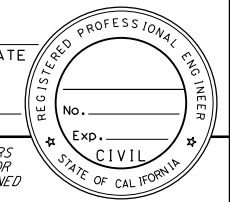
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

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NOTE:  
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 2. ALL RIGHT OF WAY DATA ARE APPROXIMATE. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

LEGEND:  
 GRIND EXISTING CONCRETE PAVEMENT  
 0.15' COLD PLANE AND OVERLAY  
 UPGRADE EXISTING MBGR TO MGS



**LAYOUT L-5**

SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
 Et-Catrans

REVISOR: [ ]  
 DESIGNED BY: [ ]  
 CHECKED BY: [ ]  
 SUPERVISOR: [ ]  
 TRANSPORTATION: [ ]

BORDER LAST REVISED 7/2/2010

USERNAME => DGN FILE => ... \Exhibits\1L250\_L05.dgn

RELATIVE BORDER SCALE IS IN INCHES




UNIT 2242

PROJECT NUMBER & PHASE 08190001580

LAST REVISION DATE PLOTTED => 1/12/2026  
 00-00-00 TIME PLOTTED => 11:31:39 AM

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**  
 FUNCTIONAL SUPERVISOR  
 CALCULATED-DESIGNED BY  
 CHECKED BY  
 REVISED BY  
 DATE REVISED

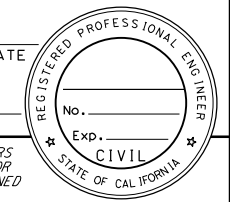
NOTE:  
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 2. ALL RIGHT OF WAY DATA ARE APPROXIMATE. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

LEGEND:  
 GRIND EXISTING CONCRETE PAVEMENT  
 0.15' COLD PLANE AND OVERLAY  
 UPDATE EXISTING MBGR TO MGS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
 PLANS APPROVAL DATE \_\_\_\_\_

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



**LAYOUT L-6**  
 SCALE: 1" = 100'

LAST REVISION DATE PLOTTED => 1/12/2026 00-00-00 TIME PLOTTED => 11:32:41 AM

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**  
 FUNCTIONAL SUPERVISOR  
 CALCULATED-DESIGNED BY  
 CHECKED BY  
 REVISED BY  
 DATE REVISED

**NOTE:**

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**LEGEND:**

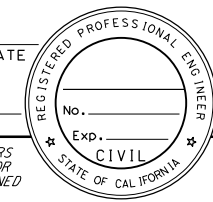
- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_

PLANS APPROVAL DATE \_\_\_\_\_

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.




**LAYOUT L-7**

SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**  
 FUNCTIONAL SUPERVISOR  
 CALCULATED-DESIGNED BY  
 CHECKED BY  
 REVISED BY  
 DATE REVISED

**NOTE:**

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**LEGEND:**

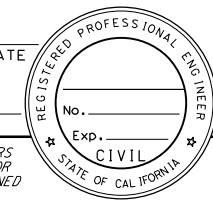
- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_

PLANS APPROVAL DATE \_\_\_\_\_

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.




**LAYOUT L-8**  
 SCALE: 1" = 100'

LAST REVISION DATE PLOTTED => 1/12/2026  
 00-00-00 TIME PLOTTED => 11:34:47 AM

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



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**LEGEND:**

- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY



**LAYOUT  
L-9**

SCALE: 1" = 100'




STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**  
 FUNCTIONAL SUPERVISOR  
 CALCULATED-DESIGNED BY  
 CHECKED BY  
 REVISED BY  
 DATE REVISIED

x  
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LAST REVISION    DATE PLOTTED => 1/12/2026  
 00-00-00    TIME PLOTTED => 11:35:49 AM

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**

NOTE:  
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LEGEND:  
 GRIND EXISTING CONCRETE PAVEMENT  
 0.15' COLD PLANE AND OVERLAY  
 UPGRADE EXISTING MBGR TO MGS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
 PLANS APPROVAL DATE \_\_\_\_\_  
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



**LAYOUT  
 L-10**  
 SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**

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**LEGEND:**

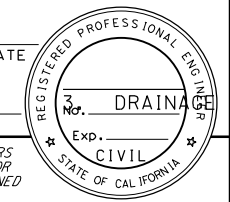
- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY
- UPGRADE EXISTING MBGR TO MGS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



**LAYOUT  
L-11**

SCALE: 1" = 100'

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISOR BY
<b>Caltrans</b>		CHECKED BY	DATE REVISED



**LAYOUT  
L-12**

SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**  
 FUNCTIONAL SUPERVISOR  
 CALCULATED-DESIGNED BY  
 CHECKED BY  
 REVISED BY  
 DATE REVISED

**NOTE:**

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2. ALL RIGHT OF WAY DATA ARE APPROXIMATE. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
3. DRAINAGE INLETS AND OUTLETS SHALL BE CONSIDERED FOR TCD.

**LEGEND:**

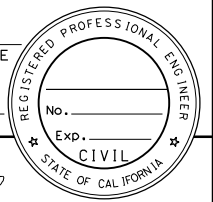
- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY
- PROPOSED MAINTENANCE VEHICLE PULLOUT (MVP)
- STAMPED CONCRETE PAVE PAVING BEYOND GORE AREA
- PROPOSED MAINTENANCE ACCESS PATH
- PROPOSED NEW SIDEWALK
- UPGRADE EXISTING MBGR TO MGS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



**LAYOUT**  
**L-13**  
 SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**

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**LEGEND:**

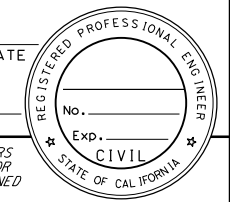
- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY
- STAMPED CONCRETE PAVE PAVING BEYOND GORE AREA

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_

PLANS APPROVAL DATE \_\_\_\_\_

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



**LAYOUT L-14**

SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**  
 FUNCTIONAL SUPERVISOR  
 CALCULATED-DESIGNED BY  
 CHECKED BY  
 REVISED BY  
 DATE REVISED

NOTE:  
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 2. ALL RIGHT OF WAY DATA ARE APPROXIMATE. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

LEGEND:  
 GRIND EXISTING CONCRETE PAVEMENT  
 0.15' COLD PLANE AND OVERLAY  
 PROPOSED MAINTENANCE VEHICLE PULLOUT (MVP)  
 UPGRADE EXISTING MBGR TO MGS

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
 PLANS APPROVAL DATE \_\_\_\_\_  
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



**LAYOUT  
 L-15**  
 SCALE: 1" = 100'

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

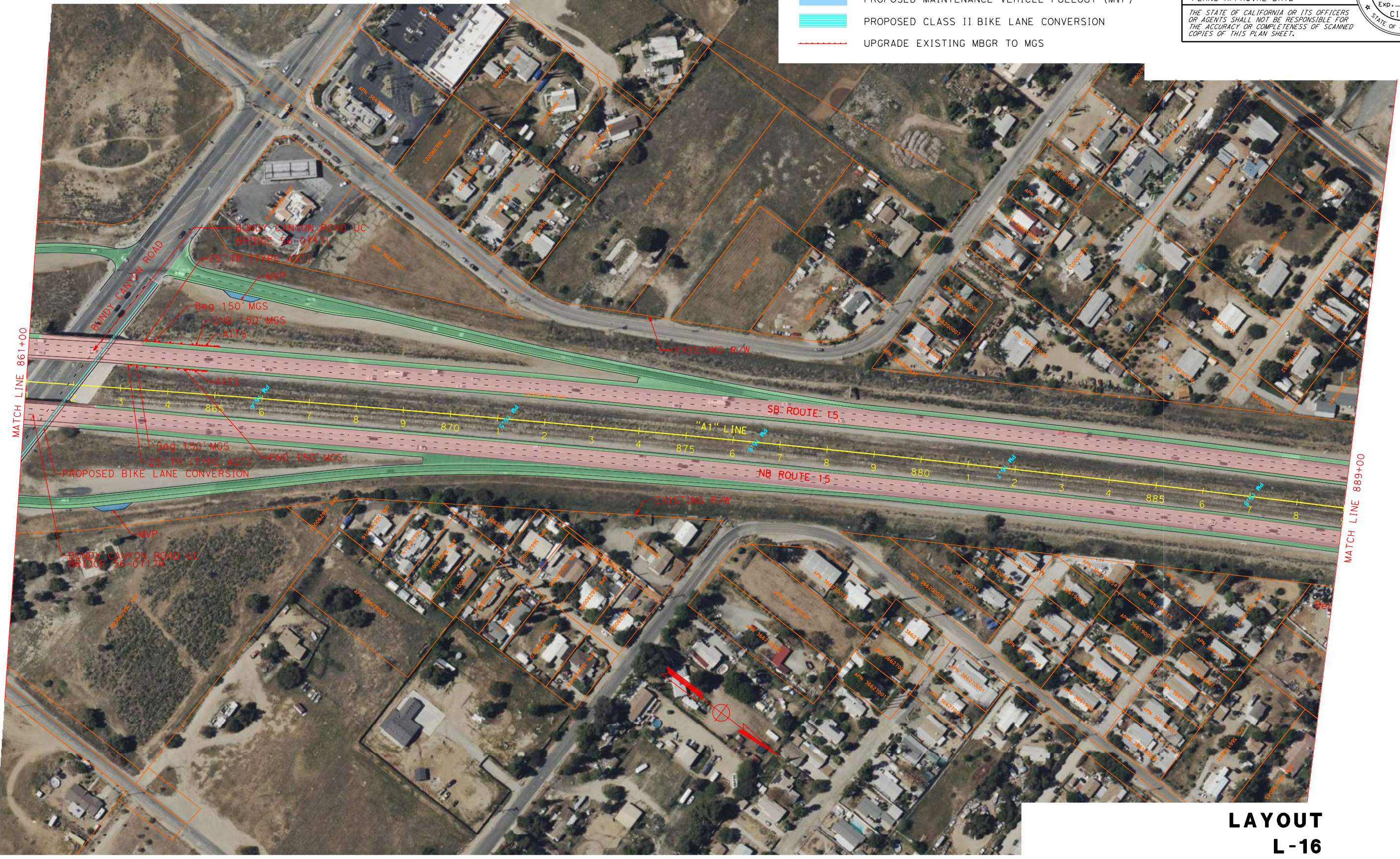
REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

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  - ALL RIGHT OF WAY DATA ARE APPROXIMATE. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

- LEGEND:
- GRIND EXISTING CONCRETE PAVEMENT
  - 0.15' COLD PLANE AND OVERLAY
  - PROPOSED MAINTENANCE VEHICLE PULLOUT (MVP)
  - PROPOSED CLASS II BIKE LANE CONVERSION
  - UPGRADE EXISTING MBGR TO MGS



**LAYOUT**  
**L-16**  
SCALE: 1" = 100'

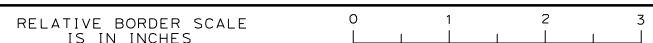
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**

REVISOR BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 CALCULATED/DESIGNED BY: \_\_\_\_\_ CHECKED BY: \_\_\_\_\_

FUNCTIONAL SUPERVISOR: \_\_\_\_\_

BORDER LAST REVISED 7/2/2010

USERNAME => ... \Exhibits\1L250\_L16.dgn



UNIT 2242

PROJECT NUMBER & PHASE 08190001680

LAST REVISION DATE PLOTTED => 1/12/2026  
 00-00-00 TIME PLOTTED => 11:42:58 AM

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**

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 DATE REVISED

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FUNCTIONAL SUPERVISOR

DEPARTMENT OF TRANSPORTATION

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**LEGEND:**

- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY
- UPGRADE EXISTING MBGR TO MGS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_

PLANS APPROVAL DATE \_\_\_\_\_

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

REGISTERED PROFESSIONAL ENGINEER  
 No. \_\_\_\_\_  
 Exp. \_\_\_\_\_  
 CIVIL  
 STATE OF CALIFORNIA



**LAYOUT L-17**

SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**

REVISOR BY  
 DATE REVISED

CALCULATED-DESIGNED BY  
 CHECKED BY

FUNCTIONAL SUPERVISOR

DEPARTMENT OF TRANSPORTATION

**NOTE:**

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**LEGEND:**

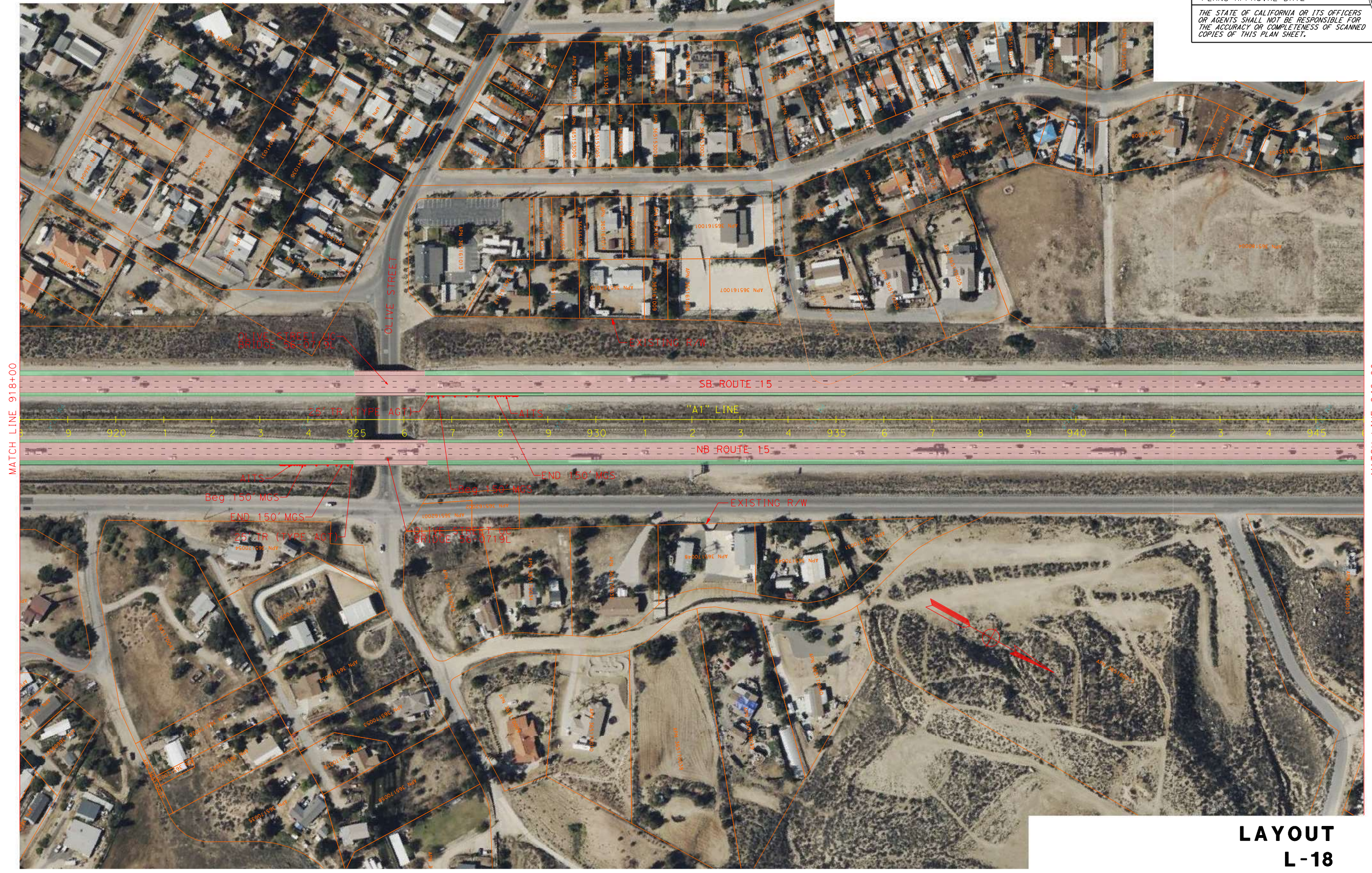
- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY
- UPGRADE EXISTING MBGR TO MGS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_

PLANS APPROVAL DATE \_\_\_\_\_

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**LAYOUT L-18**

SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**

**NOTE:**

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**LEGEND:**

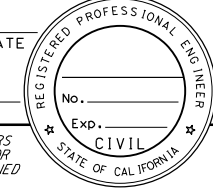
- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_

PLANS APPROVAL DATE \_\_\_\_\_

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

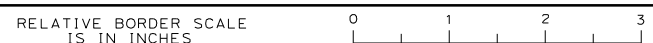



**LAYOUT L-19**

SCALE: 1" = 100'

BORDER LAST REVISED 7/2/2010

USERNAME => DGN FILE => ... \Exhibits\1L250\_L19.dgn




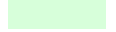
UNIT 2242

PROJECT NUMBER & PHASE 08190001580

LAST REVISION | DATE PLOTTED => 1/12/2026  
 00-00-00 | TIME PLOTTED => 11:46:20 AM

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**  
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NOTE:  
 1. ALL DIMENSIONS AND ALL STRIPING ARE PRELIMINARY AND FOR CONCEPTUAL PURPOSE ONLY. ADDITIONAL DESIGN VARIATIONS MAY BE CONSIDERED IN SUBSEQUENT PHASES.  
 2. ALL RIGHT OF WAY DATA ARE APPROXIMATE. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.

LEGEND:  
 GRIND EXISTING CONCRETE PAVEMENT  
 0.15' COLD PLANE AND OVERLAY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		
REGISTERED CIVIL ENGINEER			DATE		
PLANS APPROVAL DATE					
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					



**LAYOUT  
L-20**

SCALE: 1" = 100'

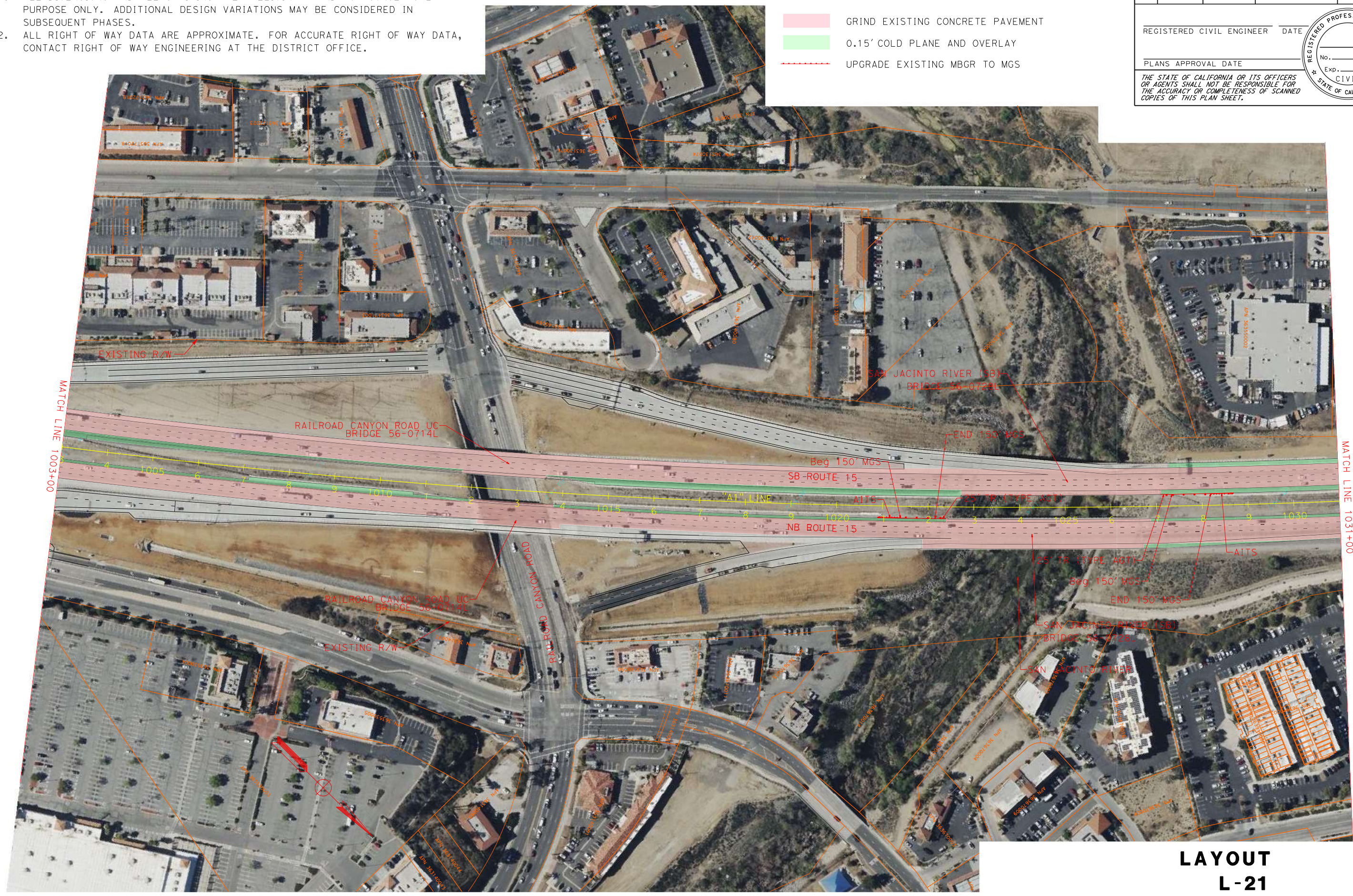
STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**  
 FUNCTIONAL SUPERVISOR  
 CALCULATED-DESIGNED BY  
 CHECKED BY  
 REVISED BY  
 DATE REVISED

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LEGEND:  
 GRIND EXISTING CONCRETE PAVEMENT  
 0.15' COLD PLANE AND OVERLAY  
 UPGRADE EXISTING MBGR TO MGS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
 PLANS APPROVAL DATE \_\_\_\_\_  
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



**LAYOUT  
 L-21**  
 SCALE: 1" = 100'

LAST REVISION DATE PLOTTED => 27-JAN-2026  
 00-00-00 TIME PLOTTED => 13:10

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

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**LEGEND:**

- GRIND EXISTING CONCRETE PAVEMENT
- 0.15' COLD PLANE AND OVERLAY
- UPGRADE EXISTING MBGR TO MGS



**LAYOUT  
L-22**

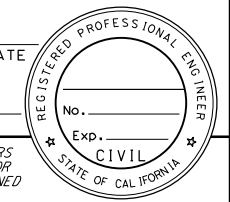
SCALE: 1" = 100'

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Caltrans
FUNCTIONAL SUPERVISOR
CALCULATED-DESIGNED BY
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REVISOR
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REVISION



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LAST REVISION    DATE PLOTTED => 1/12/2026    00-00-00    TIME PLOTTED => 11:49:29 AM

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		
REGISTERED CIVIL ENGINEER			DATE		
PLANS APPROVAL DATE					
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LEGEND:  
 GRIND EXISTING CONCRETE PAVEMENT  
 0.15' COLD PLANE AND OVERLAY



**LAYOUT  
 L-23**  
 SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  


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 CALCULATED/DESIGNED BY: \_\_\_\_\_ CHECKED BY: \_\_\_\_\_  
 FUNCTIONAL SUPERVISOR: \_\_\_\_\_

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

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- LEGEND:
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  - 0.15' COLD PLANE AND OVERLAY
  - STAMPED CONCRETE PAVE PAVING BEYOND GORE AREA
  - UPGRADE EXISTING MBGR TO MGS



**LAYOUT  
L-24**

SCALE: 1" = 100'

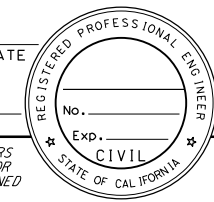
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 DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		


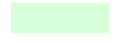


REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_

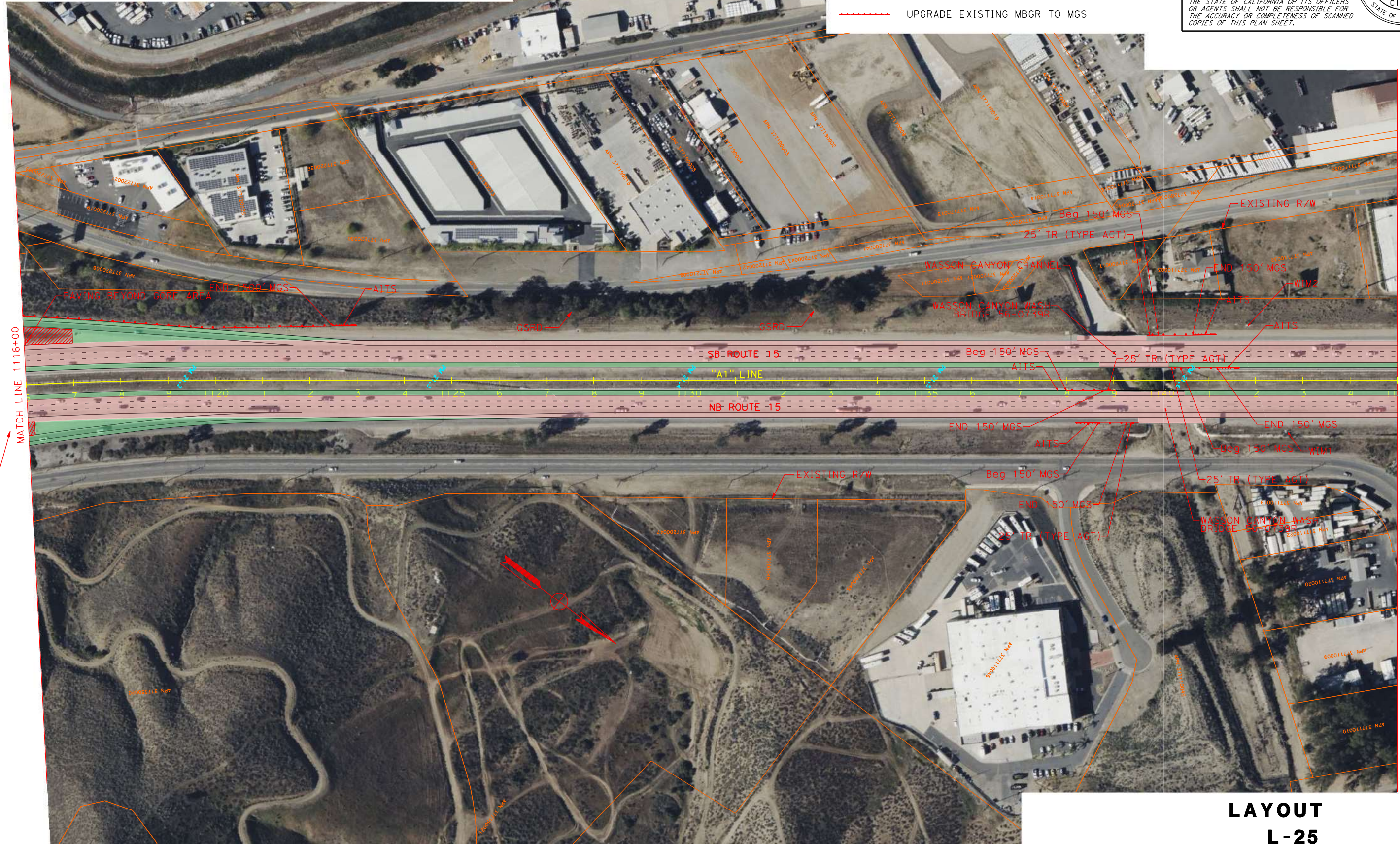
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- LEGEND:
-  GRIND EXISTING CONCRETE PAVEMENT
  -  0.15' COLD PLANE AND OVERLAY
  -  STAMPED CONCRETE PAVE PAVING BEYOND GORE AREA
  -  UPGRADE EXISTING MBGR TO MGS



**LAYOUT  
L-25**  
SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

**Caltrans**

REVISIONS:

NO.	BY	DATE	DESCRIPTION

FUNCTIONAL SUPERVISOR

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USERNAME => ... \Exhibits\1250\_L25.dgn



UNIT 2242

PROJECT NUMBER & PHASE 08190001580

LAST REVISION DATE PLOTTED => 1/12/2026  
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STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**

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 DATE REVISED

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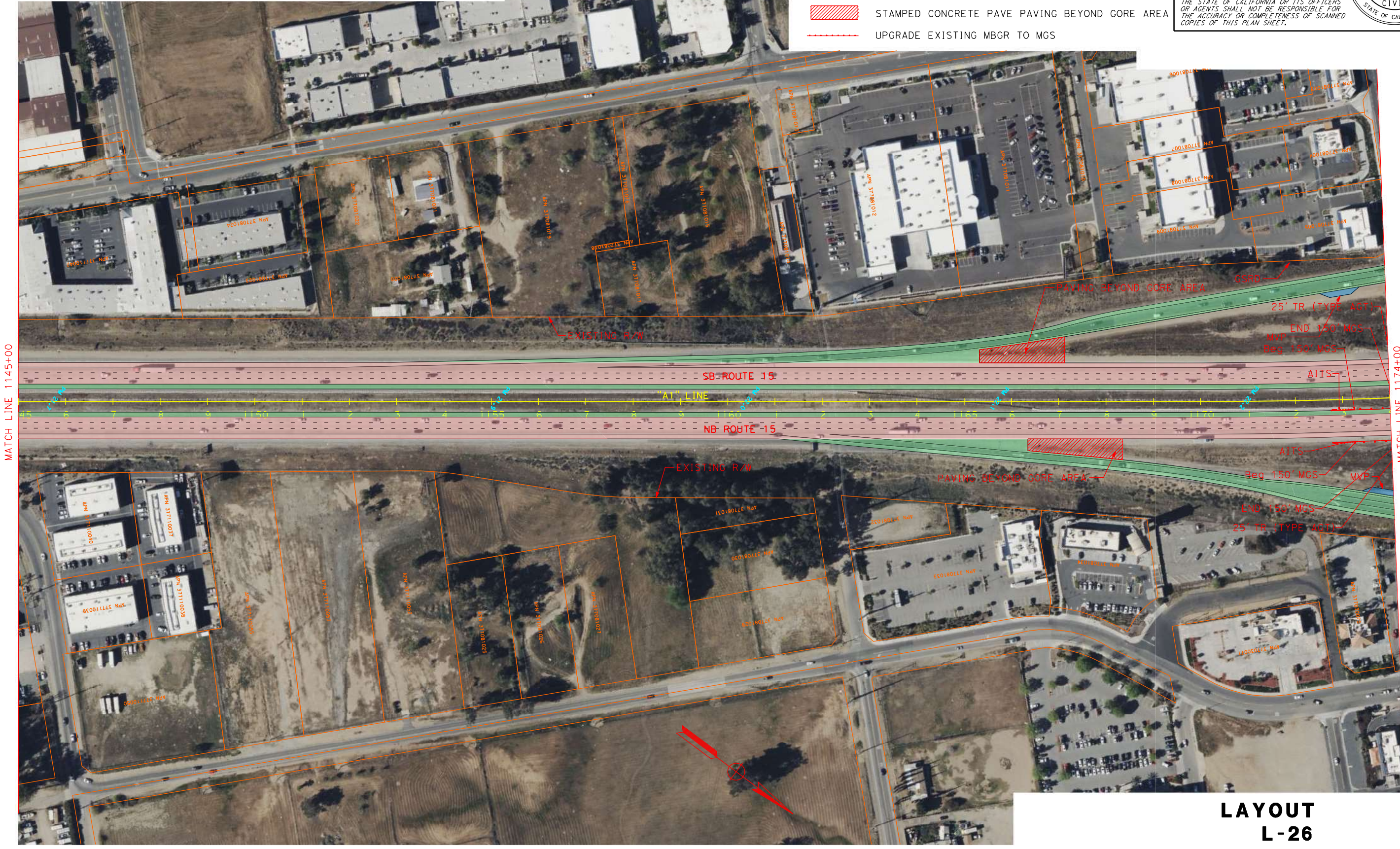
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- LEGEND:
- GRIND EXISTING CONCRETE PAVEMENT
  - 0.15' COLD PLANE AND OVERLAY
  - PROPOSED MAINTENANCE VEHICLE PULLOUT (MVP)
  - STAMPED CONCRETE PAVE PAVING BEYOND GORE AREA
  - UPGRADE EXISTING MBGR TO MGS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
 PLANS APPROVAL DATE \_\_\_\_\_

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**LAYOUT  
 L-26**

SCALE: 1" = 100'

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
**Caltrans**  
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 DATE REVISED  
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NOTE:  
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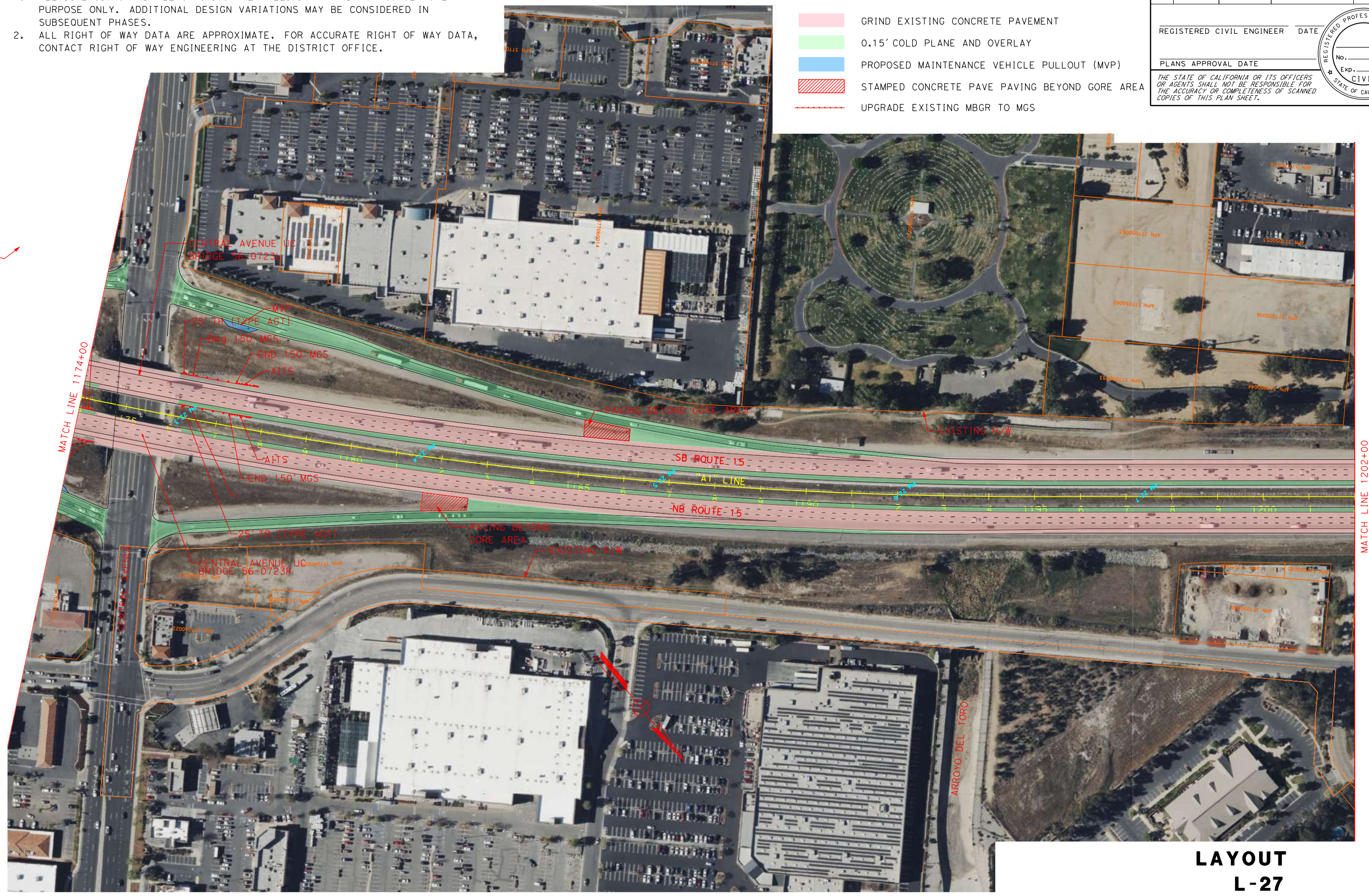
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_

PLANS APPROVAL DATE \_\_\_\_\_

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REGISTERED PROFESSIONAL ENGINEER  
 No. \_\_\_\_\_  
 Exp. \_\_\_\_\_  
 CIVIL  
 STATE OF CALIFORNIA


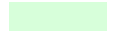


**LAYOUT  
 L-27**

SCALE: 1" = 100'

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**Caltrans**

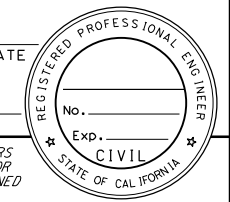
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 0.15' COLD PLANE AND OVERLAY

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
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**LAYOUT  
 L-28**  
 SCALE: 1" = 100'

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Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

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  - 0.15' COLD PLANE AND OVERLAY
  - PROPOSED MAINTENANCE VEHICLE PULLOUT (MVP)
  - STAMPED CONCRETE PAVE PAVING BEYOND GORE AREA



**LAYOUT  
L-29**

SCALE: 1" = 100'

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UNIT 2242

PROJECT NUMBER & PHASE 08190001680

LAST REVISION DATE PLOTTED => 1/12/2026  
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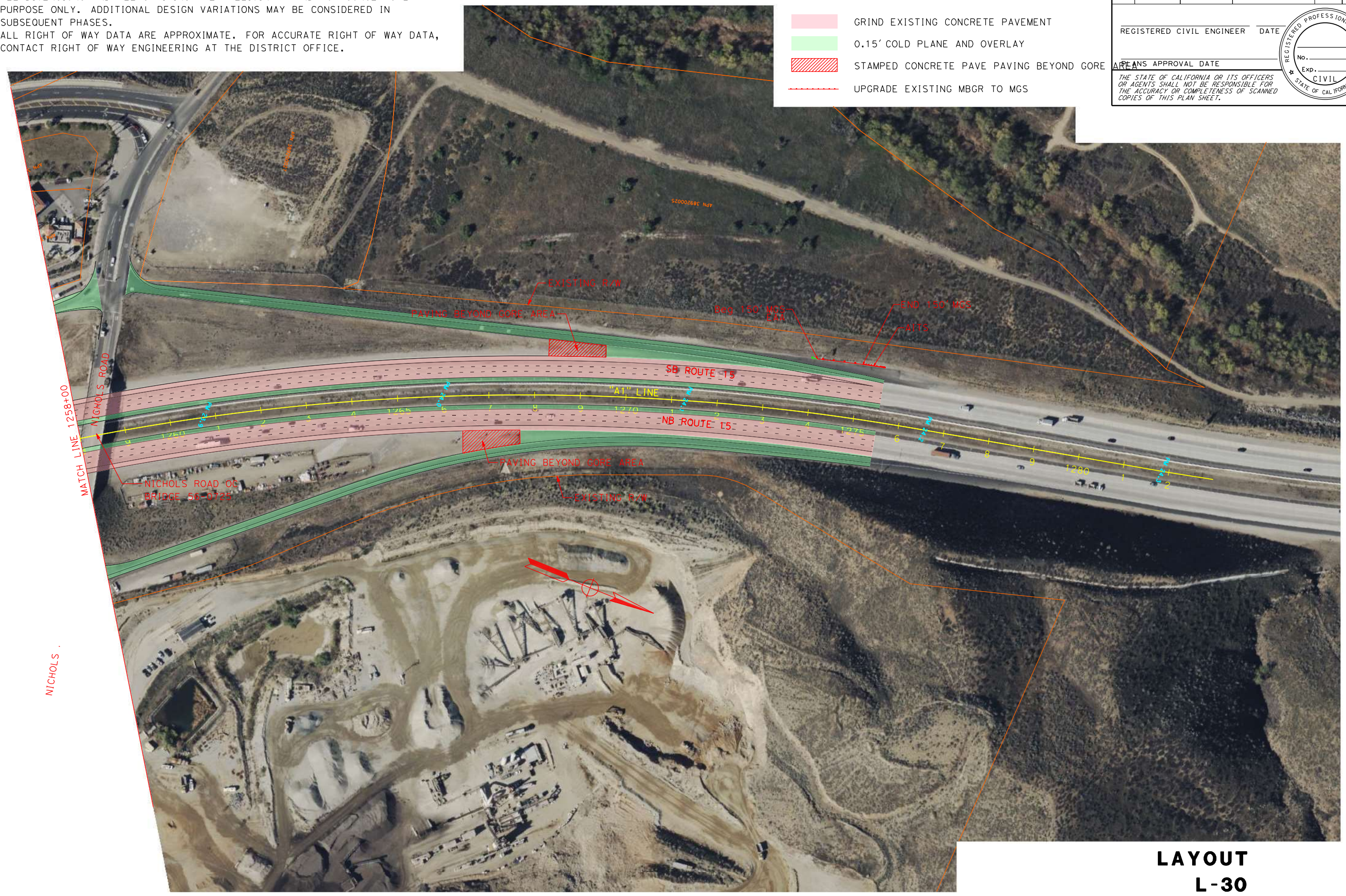
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- STAMPED CONCRETE PAVE PAVING BEYOND GORE
- UPGRADE EXISTING MBGR TO MGS

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
08	Riv	15	8.1/24.1		

REGISTERED CIVIL ENGINEER DATE \_\_\_\_\_  
 APPROVAL DATE \_\_\_\_\_  
 No. \_\_\_\_\_  
 Exp. \_\_\_\_\_  
 CIVIL  
 STATE OF CALIFORNIA

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**LAYOUT  
 L-30**  
 SCALE: 1" = 100'

Attachment C  
Preliminary Cost Estimate

## PLANNING COST ESTIMATE

EA: 1L250

EA: 1L250 PID: 0819000168

PID: 0819000168

District-County-Route: 08-RIV-15

PM: 8.1-24.1

Type of Estimate : PIR

Program Code : 20.XX.201.121

Project Limits : I-15 from PM 8.1 to PM 24.1

Project Description: I-15 CAPM

**Scope :** 1) Random slabs replacement on mainline lanes. 2) Cold plane and overlay on mainline shoulders, on-ramps, and off-ramps including I-215/I-15 connectors. 3) Grind the existing concrete pavement on all lanes on the mainline. 4) Upgrade lighting to the latest current standards. 5) Upgrade Transportation Management Systems (TMS). 6) Upgrade Weight in Motion (WIM) elements. 7) Eliminate sidewalk gaps. 8) Install roadside safety improvements including Maintenance Vehicle Pullouts (MVPs), paving beyond gore areas, and light duty maintenance access trails. 9) Upgrade existing guardrails and end treatments to latest current standards including vegetation control. 10) Install ramp meters. 11) Install ADA curb ramps

Alternative : Build Alternative

### SUMMARY OF PROJECT COST ESTIMATE

	Current Year Cost	Escalated Cost	
TOTAL ROADWAY COST	\$ 51,158,000	\$ 58,808,000	
TOTAL STRUCTURES COST	\$ -	\$ -	
SUBTOTAL CONSTRUCTION COST	\$ 51,158,000	\$ 58,808,000	
TOTAL RIGHT OF WAY COST	\$ 117,500	\$ 117,500	
<b>TOTAL CAPITAL OUTLAY COSTS</b>	<b>\$ 51,276,000</b>	<b>\$ 58,926,000</b>	
PA/ED SUPPORT	\$ 1,899,000	\$ 1,899,000	
PS&E SUPPORT	\$ 2,047,000	\$ 2,152,000	
RIGHT OF WAY SUPPORT	\$ 212,000	\$ 236,000	
CONSTRUCTION SUPPORT	\$ 5,927,000	\$ 6,598,000	
<b>TOTAL SUPPORT COST</b>	<b>\$ 10,085,000</b>	<b>\$ 10,885,000</b>	

<b>TOTAL PROJECT COST</b>	<b>\$ 61,400,000</b>	<b>\$ 69,900,000</b>	
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Programmed Amount

Month / Year  
 Date of Estimate (Month/Year) \_\_\_\_\_ 6 / 2025  
 Estimated Construction Start (Month/Year) \_\_\_\_\_ 11 / 2027  
 Number of Working Days = 250  
 Estimated Mid-Point of Construction (Month/Year) \_\_\_\_\_ 8 / 2028  
 Estimated Construction End (Month/Year) \_\_\_\_\_ 5 / 2029

Number of Plant Establishment Days

<i>Estimated Project Schedule</i>	
<i>PID Approval</i>	6/30/2023
<i>PA/ED Approval</i>	2/2/2026
<i>PS&amp;E</i>	8/3/2026
<i>RTL</i>	3/2/2027
<i>Begin Construction</i>	1/2/2028

Estimate Concurred by:

Office Engineer / Cost Estimate Certifier	Date	Phone
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Estimate Concurred by:

Project Manager	Date	Phone
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**SECTION 1: EARTHWORK**

Item code		Unit	Quantity		Unit Price (\$)		Cost
190101	Roadway Excavation	CY	4,597	x	58.00	= \$	266,626
17010X	Clearing & Grubbing	LS	1	x	20,000.00	= \$	20,000
100100	Develop Water Supply	LS	1	x	5,000.00	= \$	5,000
XXXXXX	Some Item	Unit		x		= \$	-

<b>TOTAL EARTHWORK SECTION ITEMS</b>	<b>\$</b>	<b>291,700</b>
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**SECTION 2: PAVEMENT STRUCTURAL SECTION**

Item code		Unit	Quantity		Unit Price (\$)		Cost
420201	Grind Existing Concrete Pavement	SQYD	772,382	x	5.00	= \$	3,861,910
390132	Hot Mix Asphalt (Type A)	TON	41,794	x	119.00	= \$	4,973,486
397005	Tack Coat	TON	189.0	x	734.00	= \$	138,726
398200	Cold Plane Asphalt Concrete Pavement	SQYD	410,531	x	4.00	= \$	1,642,124
731627	Minor Concrete (Curb, Sidewalk, and Curb Ramp)	CY	75	x	730.00	= \$	54,750
731519	Minor Concrete (Stamped Concrete)	SQFT	68,102	x	18.60	= \$	1,266,697
832070	Vegetation Control (Minor Concrete)	SQYD	440	x	120.00	= \$	52,800
411105	Individual Slab Replacement (RSC)	CY	18,863	x	495.00	= \$	9,337,185
260203	Class 2 Aggregate Base	CY	1,971	x	115.00	= \$	226,665
280015	Lean Concrete Base Rapid Setting	CY	1,971	x	376.00	= \$	741,096
394073	Place Hot Mix Asphalt Dike	LF	1,000	x	11.00	= \$	11,000
414201	Joint Seal (Silicone)	LS	1	x	200,000.00	= \$	200,000
730070	Detectable Warning Surface	SQRT	48	x	50.00	= \$	2,400
733000	Pre/Post Construction Survey	EA	4	x	1,400.00	= \$	5,600
390095	Replace Asphalt Concrete Surfacing	CY	1,000	x	350	= \$	349,500

<b>TOTAL PAVEMENT STRUCTURAL SECTION ITEMS</b>	<b>\$</b>	<b>22,864,000</b>
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**SECTION 3: DRAINAGE**

Item code	Unit	Quantity	Unit Price (\$)	Cost
XXXXXX Drainage Works	LS	1	x 988,288.00 = \$	988,288

<b>TOTAL DRAINAGE ITEMS \$ 988,300</b>
--

**SECTION 4: SPECIALTY ITEMS**

Item code	Unit	Quantity	Unit Price (\$)	Cost
070030 Lead Compliance Plan	LS	1	x 5,000.00 = \$	5,000
080050 Progress Schedule (Critical Path Method)	LS	1	x 10,000.00 = \$	10,000
141120 Treated Wood Waste	LB	157,680	x 0.35 = \$	55,188
410096 Drill and Bond (Dowel Bar)	EA	36,864	x 21.00 = \$	774,144
839752 Remove Guardrail	LF	8,087	x 11.50 = \$	93,001
832007 Midwest Guardrail System (Wood Post)	LF	8,087	x 35.00 = \$	283,045
839584 Alternative In-line Terminal System	EA	2	x 6,400.00 = \$	12,800
016055 Transition Railing (Type AGT)	EA	29	x 6,250.00 = \$	181,250
832006 MIDWEST GUARDRAIL SYSTEM (STEEL POST)	LF	1,250	x 45.00 = \$	56,250
839311 Double Thrie Beam Barrier ( Wood Post)	LF	250	x 70.00 = \$	17,500
839580 End Anchor Assembly ( Type SFT-M)	EA	2	x 2,400.00 = \$	4,800
839775 Remove Concrete Barrier ( Type K)	LF	250	x 95.00 = \$	23,750
120320 Temporary Barrier System	LF	1,500	x 45.00 = \$	67,500
014105 ALTERNATIVE TEMPORARY CRASH CUSHION TL-3	EA	2	x 4,200.00 = \$	8,400
810190 Guard Railing Delineator	EA	30	x 35.00 = \$	1,050
XXXXXX Some Item	Unit			

<b>TOTAL SPECIALTY ITEMS \$ 1,593,700</b>
---

**SECTION 5: ENVIRONMENTAL****5A - ENVIRONMENTAL MITIGATION**

Item code	Unit	Quantity		Unit Price (\$)		Cost
146002 Contractor-Supplied Biologist	LS	1	x	411,125.00	= \$	411,125
160110 Temporary High-Visibility Fence	LS	1	x	55,000.00	= \$	55,000
<i>Subtotal Environmental Mitigation</i>						<b>\$ 466,125</b>

**5B - LANDSCAPE AND IRRIGATION**

Item code	Unit	Quantity		Unit Price (\$)		Cost
210430 Hydroseed	SQFT	10,890	x	0.10	= \$	1,089
210252 Bounded Fiber Matrix (SQFT)	SQFT	104,550	x	0.35	= \$	36,593
<i>Subtotal Landscape and Irrigation</i>						<b>\$ 37,682</b>

**5C - EROSION CONTROL**

Item code	Unit	Quantity		Unit Price (\$)		Cost
210010 MOVE-IN/MOVE-OUT	EA	1	x	3500.00	\$	3,500
<i>Subtotal Erosion Control</i>						<b>\$ 3,500</b>

**5D - NPDES**

Item code	Unit	Quantity		Unit Price (\$)		Cost
130201 Storm Water Pollution Prevention Plan	LS	1	x	12,000.00	= \$	12,000
130100 Job Site Mangement	LS	1	x	150,000.00	= \$	150,000
130312 Pre-Storm Event Inspection Report	EA	37	x	1,000.00	= \$	37,000
130322 During & Post Storm Event Inspection Report	EA	24	x	1,600.00	= \$	38,400
130331 Stormwater Annual Report	EA	2	x	3,000.00	= \$	6,000
130900 Temporary Concrete Washout	LS	1	x	30,000.00	= \$	30,000
XXXXXX Trash Capture Devices	LS	1	x	800,000.00	= \$	800,000
130650 Temporary Gravel Bag Berm	LF	1,000	x	15.00	= \$	15,000
130710 Temporary Construction Entrance	EA	6	x	3,000.00	= \$	18,000
130640 Temporary Fiber Roll	LF	2,000	x	8.00	= \$	16,000
130620 Temporary Drainage Inlet Protection	EA	20	x	300.00	= \$	6,000
130560 Temporary Soil Binder	SQYD	20,000	x	1.00	= \$	20,000
<i>Subtotal NPDES</i>						<b>\$ 1,148,400</b>

<b>TOTAL ENVIRONMENTAL</b>	<b>\$ 1,655,800</b>
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**Supplemental Work for NPDES**

066595 Water Pollution Control Maintenance Sharing	LS	1	x	12,000.00	= \$	12,000
066596 Additional Water Pollution Control	LS	1	x	12,000.00	= \$	12,000
066597 Stormwater Sampling and Analysis	LS	1	x	3,000.00	= \$	3,000
<i>Subtotal Supplemental Work for NDPS</i>						<b>\$ 27,000</b>

\*Applies to all SWPPPs and those WPCPs with sediment control or soil stabilization BMPs.

\*\*Applies to both SWPPPs and WPCP projects.

\*\*\* Applies only to project with SWPPPs.

**SECTION 6: TRAFFIC ITEMS**

**6A - Traffic Electrical**

Item code	Unit	Quantity	Unit Price (\$)	Cost
870111 Inductive Loop Detector (EA)	EA	12	x 1,700.00 = \$	20,400
870200 Lighting System	LS	1	x 133,000.00 = \$	133,000
871899 Weigh-In-Motion	LS	1	x 893,000.00 = \$	893,000
872002 Modifying Lighting System	LS	1	x 661,400.00 = \$	661,400
872134 Modifying Ramp Metering Systems	LS	1	x 75,400.00 = \$	75,400
872135 Modifying Traffic Monitoring Stations (WIM)	LS	1	x 480,000.00 = \$	480,000
872139 Modifying Signal System	LS	1	x 72,000.00 = \$	72,000
870009 Maintaining Existing Traffic Management System Elements During Construction	LS	1	x 5,000.00 = \$	5,000
<b>Subtotal Traffic Electrical</b>				<b>\$ 2,340,200</b>

**6B - Traffic Signing and Striping**

Item code	Unit	Quantity	Unit Price (\$)	Cost
810230 Pavement Marker (Retroreflective)	EA	18,300	x 4.30 = \$	78,690
810190 Guard Railing Delineator	EA	250	x 40.00 = \$	10,000
820760 Furnish Single Sheet Aluminum Sign (0.080" - Unframed)	SQFT	10	x 18.00 = \$	180
820840 Roadside Sign - One Post	EA	2	x 700.00 = \$	1,400
840516 Thermoplastic Pavement Marking (Enhanced Wet Night Visibility)	SQFT	19,000	x 6.00 = \$	114,000
840655 Paint Traffic Stripe (1-Coat)	LF	169,000	x 1.00 = \$	169,000
846007 6" Thermoplastic Traffic Stripe (Enhanced Wet Night Visibility)	LF	537,000	x 1.00 = \$	537,000
846009 8" Thermoplastic Traffic Stripe (Enhanced Wet Night Visibility)	LF	8,100	x 1.50 = \$	12,150
846013 12" Thermoplastic Traffic Stripe (Enhanced Wet Night Visibility)	LF	29,100	x 3.00 = \$	87,300
847077 8" Traffic Stripe Tape With Contrast (Warranty) (Broken 12-3)	LF	7,000	x 4.00 = \$	28,000
847098 8" Traffic Stripe Tape with Contrast (Broken 12-3)	LF	7,000	x 4.00 = \$	28,000
847222 6" Traffic Stripe Tape with Contrast (Warranty) (Broken 36-12)	LF	260,000	x 2.50 = \$	650,000
847224 8" Traffic Stripe Tape with Contrast (Warranty)	LF	7,200	x 12.00 = \$	86,400
810120 Remove Pavement Marker	EA	4,000	x 2.00 = \$	8,000
846030 Remove Thermoplastic traffic Stripe	LF	45,000	x 1.00 = \$	45,000
846035 Remove Thermoplastic Pavement Marking	SQFT	810	x 7.00 = \$	5,670
847200 Remove 6" Traffic Stripe	LF	36,300	x 2.00 = \$	72,600
<b>Subtotal Traffic Signing and Striping</b>				<b>\$ 1,933,390</b>

**6C - Traffic Management Plan**

Item code	Unit	Quantity	Unit Price (\$)	Cost
128651 Portable Changeable Message Sign (EA)	EA	14	x \$ 4,500 = \$	63,000
014105 Alternative Temporary Crash Cushion	EA	33	x \$ 5,000 = \$	165,000
XXX Some Items	LS		x = \$	-
<b>Subtotal Traffic Management Plan</b>				<b>\$ 228,000</b>

**6C - Stage Construction and Traffic Handling**

Item code	Unit	Quantity	Unit Price (\$)	Cost
120090 Construction Area Signs	LS	1	x 41,000.00 = \$	41,000
120100 Traffic Control System	LS	1	x 1,707,000.00 = \$	1,707,000
120103 Stationary Impact Attenuator Vehicle	Day	296	x 750.00 = \$	222,000
120204 Portable Radar Speed Feedback Sign System Day	EA	296	x 110.00 = \$	32,560
120165 Channelizer (Surface Mounted)	EA	240	x 50.00 = \$	12,000
120320 Temporary Barrier System	LF	6,700	x 75.00 = \$	502,500
124000 Temporary Pedestrian Access Route	LS	1	x 20,000.00 = \$	20,000
129010 Mobile Barrier System	Day	192	x 2,880.00 = \$	552,960
129108 Temporary Crash Cushion TL-3	EA	33	x 5,000.00 = \$	165,000
180170 Delineator (Class 1)	EA	100	x 75.00 = \$	7,500
xxxxx Some Items			x =	
<b>Subtotal Stage Construction and Traffic Handling</b>				<b>\$ 3,262,520</b>

<b>TOTAL TRAFFIC ITEMS</b>	<b>\$ 7,764,200</b>
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**SECTION 7: DETOURS**

Includes constructing, maintaining, and removal

Item code	Unit	Quantity	Unit Price (\$)	Cost
XXXXXX Some Item	LS		x	= \$ -
<b>TOTAL DETOURS</b>				<b>\$ -</b>

SUBTOTAL SECTIONS 1 through 7    \$    35,157,700

**SECTION 8: MINOR ITEMS****8A - Americans with Disabilities Act Items**

ADA Items

0.5%    \$    175,789

**8B - Bike Path Items**

Bike Path Items

0.5%    \$    175,789

**8C - Other Minor Items**

Other Minor Items

5.0%    \$    1,757,885

Total of Section 1-7    \$    35,157,700    x    6.0%    =    \$    2,109,462

**TOTAL MINOR ITEMS    \$    2,109,500**

**SECTIONS 9: ROADWAY MOBILIZATION \***

Item code	Unit	Quantity	Unit Price (\$)	Cost
999990		Total Section 1-8	\$ 37,267,200	x 9% = \$ 3,365,229
<b>TOTAL ROADWAY MOBILIZATION</b>				<b>\$    3,365,300</b>

**SECTION 10: SUPPLEMENTAL WORK**

Item code	Unit	Quantity	Unit Price (\$)	Cost
066670	LS	1	x \$382,800	= \$ 382,800
066094	LS	1	x \$10,000	= \$ 10,000
066919	LS	1	x \$15,000	= \$ 15,000
066610	LS	1	x \$50,000	= \$ 50,000
066860	LS	1	x \$10,000	= \$ 10,000
066015	LS	1	x \$19,200	= \$ 19,200
066070	LS	1	x \$100,000	= \$ 100,000
XXXXXX	Unit		x	= \$ -
<i>Cost of NPDES Supplemental Work specified in Section 5D</i>				<i>= \$ 27,000</i>
Total Section 1-8		\$ 37,267,200	3%	= \$ 1,118,016
<b>TOTAL SUPPLEMENTAL WORK</b>				<b>\$    1,732,100</b>

**SECTION 11: STATE FURNISHED MATERIALS AND EXPENSES**

Item code	Unit	Quantity		Unit Price (\$)	=	Cost
066105 Resident Engineers Office	LS	1	x	503,000.00	=	\$503,000
066063 Traffic Management Plan - Public Information	LS	1	x	50,000.00	=	\$50,000
066578 Portable Changeable Message Signs	LS	1	x	24,000.00	=	\$24,000
066064 Radar Speed Message Sign (Specter Sign)	LS	1	x	60,000.00	=	\$60,000
120105 Automated Workzone Information System (AWIS)	LS	1	x	120,000.00	=	\$120,000
066062 COZEEP Contract	LS	1	x	1,040,000.00	=	\$1,040,000
066065 Freeway Service Patrol for Construction	LS	1	x	108,240.00	=	\$108,240
066916 Construction General Permit Fees	LS	1	x	1,130.00	=	\$1,130
Total Section 1-8		\$ 37,267,200		2%	=	\$ 745,344

**TOTAL STATE FURNISHED \$2,651,800**

**SECTION 12: TIME-RELATED OVERHEAD**

Total of Roadway and Structures Contract Items excluding Mobilization \$37,267,200 (used to calculate total TRO)  
 Total Construction Cost (excluding TRO and Contingency) \$45,016,400 (used to check if project capital cost is greater than \$5 million including contingency)

Estimated Time-Related Overhead (TRO) Percentage (0% to 10%) = **4%**

Item code	Unit	Quantity		Unit Price (\$)	=	Cost
090100 Time-Related Overhead	WD	250	X	\$5,963	=	\$1,490,700

**TOTAL TIME-RELATED OVERHEAD \$1,490,700**

**SECTION 13: ROADWAY CONTINGENCY\***

Risk Amount from Risk Register		(for Known Risks)	0%	\$0
Additional or Residual Contingency		(for Unknown/Undefined Risks)	20%	\$9,301,420
Total Section 1-12	\$	46,507,100	x <b>10%</b>	= \$4,650,710

**TOTAL CONTINGENCY\* \$4,650,800**

**II. STRUCTURE ITEMS**

**Bridge 1**

DATE OF ESTIMATE	00/00/00		00/00/00		00/00/00
Bridge Name	XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX
Bridge Number	57-XXX		57-XXX		57-XXX
Structure Type	XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX
Width (Feet) [out to out]	0 LF		0 LF		0 LF
Total Bridge Length (Feet)	0 LF		0 LF		0 LF
Total Area (Square Feet)	0 SQFT		0 SQFT		0 SQFT
Structure Depth (Feet)	0 LF		0 LF		0 LF
Footing Type (pile or spread)	XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX
Cost Per Square Foot	\$0		\$0		\$0

<b>COST OF EACH</b>	<b>\$0</b>		<b>\$0</b>		<b>\$0</b>
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**Building 1**

DATE OF ESTIMATE	00/00/00		00/00/00		00/00/00
Building Name	XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX
Bridge Number	57-XXX		57-XXX		57-XXX
Structure Type	XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX
Width (Feet) [out to out]	0 LF		0 LF		0 LF
Total Building Length (Feet)	0 LF		0 LF		0 LF
Total Area (Square Feet)	0 SQFT		0 SQFT		0 SQFT
Structure Depth (Feet)	0 LF		0 LF		0 LF
Footing Type (pile or spread)	XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX		XXXXXXXXXXXXXXXXXXXX
Cost Per Square Foot	\$0		\$0		\$0

<b>COST OF EACH</b>	<b>\$0</b>		<b>\$0</b>		<b>\$0</b>
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<b>TOTAL COST OF BRIDGES</b>	<b>\$0</b>
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<b>TOTAL COST OF BUILDINGS</b>	<b>\$0</b>
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<b>Time-Related Overhead</b>	10%	<b>\$0</b>
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<b>STRUCTURES MOBILIZATION</b>	10%	<b>\$0</b>
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<b>STRUCTURES CONTINGENCY*</b>	25%	<b>\$0</b>
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<b>TOTAL COST OF STRUCTURES</b>	<b>\$0</b>
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Estimate Prepared By: \_\_\_\_\_  
 XXXXXXXXXXXXXXXXXXXX ----- Division of Structures

\_\_\_\_\_ Date

**III. RIGHT OF WAY**

Fill in all of the available information from the Right of Way Data Sheet.

			<i>Current Value Future Use</i>		<i>Escalated Value</i>
A)	A1)	Acquisition, including Excess Land, Fees, Damages, Goodwill	\$		\$ 0
	A2)	Railroad Acquisition	\$		\$ 0
	A3)	Federal Lands - Special Use	\$		\$ 0
B)	B1)	Acquisition of Offsite Mitigation	\$		\$ 0
C)	C1)	Utility Relocation (State Share)	\$		\$ 0
	C2)	Potholing (Design Phase)	\$	97,500	\$ 97,500
D)		RAP and/or Last Resort Housing	\$		\$ 0
E)		Clearance & Demolition	\$		\$ 0
F)		Title and Escrow	\$		\$ 0
G)		Project Permit Fees	\$	20,000	\$ 20,000
H)		Condemnation Settlements <u>0%</u>	\$		\$ 0
I)		Design Appreciation Factor <u>0%</u>	\$		\$ 0
J)		Utility Relocation (Construction Cost)	\$		\$ 0

K) 

<b>TOTAL RIGHT OF WAY ESTIMATE</b>	<b>\$117,500</b>
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L) 

<b>TOTAL R/W ESTIMATE: Escalated</b>	<b>\$117,500</b>
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M) 

<b>RIGHT OF WAY SUPPORT</b>	<b>\$200,000</b>
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Support Cost Estimate  
Prepared By \_\_\_\_\_ Project Coordinator<sup>1</sup> \_\_\_\_\_ Phone \_\_\_\_\_

Utility Estimate Prepared  
By \_\_\_\_\_ Utility Coordinator<sup>2</sup> \_\_\_\_\_ Phone \_\_\_\_\_

R/W Acquisition Estimate  
Prepared By \_\_\_\_\_ Right of Way Estimator<sup>3</sup> \_\_\_\_\_ Phone \_\_\_\_\_

Note: Items G &amp; H applied to items A + B

<sup>1</sup> When estimate has Support Costs only<sup>2</sup> When estimate has Utility Relocation<sup>3</sup> When R/W Acquisition is required

# Attachment D

## Risk Register

**RISK REGISTER CERTIFICATION (ACCOUNTABILITY CHECKPOINTS) FORM**

PPM-0001 (REV 03/2016)

The risk register is to be approved and signed-off by the District Deputies\* listed below for all scalability levels. By signing this form, you are certifying that you have reviewed the risks documented in the register and agree that they have been managed to the extent possible by the PDT.

<u>Project Information</u> <input type="radio"/> Capital Project <input type="radio"/> Major Maintenance Project (Check One)		Total Estimated Const Cost:	\$53,180,000
Project ID/District-EA	0819000168		1L250
Project Description	GRIND ALL LANES, SLAB REPLACEMENT, 0.2 MILL & OVERLAY ON RAMPS & SHOULDERS, LIGHTING, ITS &		
Project Manager (PM)	Dalia Alarkan		
Risk Management Coordinator	Md Shaheed		
<input type="checkbox"/> No Risk Register Certification Required -- Check box if project is less than \$1 million in total cost and risk register not prepared. Sign below and submit this form with PID, PA&ED, PS&E submittals, and RE Handoff Files (as applicable).			
Project Manager Signature			

<u>PID (Recommended for Capital Projects Only excluding Minor Projects)</u>			
Project Manager	<u>Electronically signed by Elaheh Hadipour</u>	Date:	April 19, 2023
Deputy District Director, Planning	<u>Electronically signed by Ray I Desselle</u>	Date:	April 19, 2023
Deputy District Director, Design	<u>Electronically signed by Mahmuda Akhter</u>	Date:	April 25, 2023
Deputy District Director, Construction	<u>Electronically signed by Christy Connors</u>	Date:	April 25, 2023
Deputy District Director, Right of Way	<u>Electronically signed by Rebecca Guirado</u>	Date:	April 19, 2023
Deputy District Director, Environmental	<u>Electronically signed by Kurt R Heidelberg</u>	Date:	April 21, 2023
Deputy District Director, Project Management	<u>Electronically signed by Bassem W Barsoum</u>	Date:	April 26, 2023

<u>PA&amp;ED (Required for Capital Projects Only)</u>			
Project Manager	<u>Sign</u>	<u>Electronically signed by DALIA ALARKAN</u>	Date: January 16, 2026
Deputy District Director, Design	<u>Sign</u>	<u>Electronically signed by JESUS GALVAN JR</u>	Date: January 16, 2026
Deputy District Director, Construction	<u>Sign</u>	<u>Electronically signed by CHRISTY CONNORS</u>	Date: January 20, 2026
Deputy District Director, Right of Way	<u>Sign</u>	<u>Electronically signed by CHRISTINE SENTENO</u>	Date: January 21, 2026
Deputy District Director, Environmental	<u>Sign</u>	<u>Electronically signed by ANTONIA TOLEDO</u>	Date: January 16, 2026
Deputy District Director, Project Management	<u>Sign</u>	<u>Electronically signed by CATHERINE BARRON</u>	Date: January 23, 2026

<u>Prior to PS&amp;E (Required for Capital Projects and Major Maintenance Projects)</u>			
Project Manager		Date:	
Deputy District Director, Design		Date:	
Deputy District Director, Construction		Date:	
Deputy District Director, Right of Way		Date:	
Deputy District Director, Environmental		Date:	
Deputy District Director, Project Management		Date:	

<u>RE File Hand-off (Recommended for Capital Projects and Major Maintenance Projects)</u>			
Project Manager		Date:	
Deputy District Director, Construction		Date:	



## EA 1L250 QUALITATIVE RISK REGISTER

EA 1L250		Phase: 1		RIV 015 PM: 8.1/ 24.1		PM: Dalia Alarkan		Const Capital Estimate: \$53,180K		Project Description: GRIND ALL LANES, SLAB REPLACEMENT, 0.2 MILL & OVERLAY ON RAMPS & SHOULDERS, LIGHTING, ITS & WIM ELEMENTS, COMPLETE STREETS ELEMENTS, ROADSIDE SAFETY IMP.			Project Location: IN & NEAR MURRIETA, WILDOMAR & LAKE ELSINORE FROM 1.4 MI S/O MURRIETA HOT SPRINGS RD TO 0.2 MI N/O NICHOLS RD					
Program Code: 201.121 / CAPM		M460 Target: 3/2/27				ARM: Kimberly Portillo		R/W Capital Estimate: \$118K										
Risk No.	Status	Type	Date of Origin	Updated	Category	Title	Risk Statement	Root Cause/Relevancy/Current Status /Assumptions/Comments/Triggers	Probability	Cost Impact				Schedule Impact		Response Strategy	Response Actions	Risk Owner
			Originator							Ph	Impact	Ph	Impact	Ph	Impact			
5	Active	Threat	1/11/2023	1/15/2026	Environmental	2081 Permit	Should any special status species protected under the California Endangered Species Act (CESA) be impacted in a way that would qualify as 'take', a 2081 permit may be required, which may impact the cost and schedule.	Because of the minimal impacts of the project to non-disturbed areas, it was determined a 2081 permit was not anticipated. However, there is habitat and historical presence within the Biological Study Area (BSA) for several CESA protected species, including but not limited to San Bernardino Merriam's Kangaroo Rat ( <i>Dipodomys merriami parvus</i> ), Least Bell's Vireo ( <i>Vireo bellii pusillus</i> ) (can be impacted by noise, specially during nesting season (February to September)), and California Orcutt grass ( <i>Orcuttia California</i> ). If any of these species are impacted during project activities, a 2081 permit will need to be pursued. It may take 150 to 210 days to process 2081 permits depending on various factors. The permit cost can be between \$30-40K. This risk will be further evaluated during PSE. A Contractor Supplied Biologist (CSB) will perform monitoring.	Low	0				0		Mitigate	Bio has included protocol surveys done by consultants in the MCCE along with cost for CSB and temporary fencing for any ESA. Bio will coordinate with our CDFW liaison to determine measures to reduce the risk of 'take'. If a special status species is discovered within the project site, all construction activities must stop and the Caltrans Biologist and RE must be notified. The monitor will provide employee training to ensure no State Take of the species during construction. Cost and schedule adjustment are contingent on whether a 2081 will be needed. Work can continue in other areas while permit might be in process.	Chun-Sheng Wang
			1										1					
			2								9	Moderate	2					
			3							Low	4	Low	3	Moderate				
									11-30%									
6	Active	Threat	1/11/2023	1/6/2026	Environmental	Nesting Birds	There is potential for nesting birds, protected from harassment under Migratory Bird Treaty Act, in construction work area. During construction, if nesting birds are identified within the construction work area, this may create work window restrictions that would result in construction schedule delays and capital cost increases.	Several protected species, including Least Bell's Vireo ( <i>Vireo Bellii Pusillus</i> ), and Coastal California Gnatcatcher ( <i>Poliopitila California</i> ) have historical habitat in the Biological Study Area. The Coastal California Gnatcatcher has critical habitat from PM 23.7 to 24.1. No work can occur in a buffer zone surrounding an active bird nest (Bird nesting season is from February 1 to September 30), as identified in Caltrans Standard Specs. Any impacts to these species may require mitigation and coordination with the USFWS and CDFW.	Moderate	0				0		Mitigate	To mitigate this risk, Least Bell's Vireo, Coastal California Gnatcatcher, and other bird surveys were conducted during PAED. The results from these surveys determined that construction windows are necessary. Environmental has included a CSB and temporary high visibility fencing to mitigate impacts in the MCCE.	Chun-Sheng Wang
			1										1					
			2								9		2					
			3							Very Low	4	Low	3	Low				
									31-50%									
8	Active	Threat	2/8/2023	1/15/2026	Traffic Design	Traffic Handling	The TMP has not been finalized yet, and there is uncertainty related to estimating traffic handling. Thereby, the cost may increase, and the schedule may be delayed.	We may need to use the shoulder for traffic during construction and/or we may need ramp closures. Cost may increase due to re-stripping or additional signs. Existing freeway has 3 lanes in each direction.	Low	0				0		Mitigate	Traffic Design will work with DTM and Construction and will come up with a strategy for traffic handling during PSE. PM may have to adjust any cost increase as necessary.	Aung Naing
			1										1	Very Low				
			2								9		2					
			3								4	Low	3	Very Low				
									11-30%									
10	Active	Threat	2/8/2023	1/15/2026	Environmental	Hazardous Material	Hazardous material investigation for aerially deposited lead (ADL) and lead-based paint (LBP) will be performed to determine hazardous content in soil and roadway striping. If elevated levels of any of the above hazardous materials are detected, special handling and disposal of contaminated soil/material may become necessary which would impact cost and schedule.	Results from Environmental Engineering's soil investigation for ADL and LBP evaluation will be complete in early 2026. Elevated levels of ADL in this major truck/passenger route is likely to be found, and if so, an item for lead compliance plan will be required and disposal cost may need to be captured.	Low	0				0		Mitigate	Appropriate SSPs or NSSPs will be provided. If additional funds will be needed for handling and disposal of contaminated soil, and will be captured in the project's cost estimate. PM may need to adjust the cost as necessary.	Paul Phan
			1										1					
			2								9		2					
			3							Low	4	Low	3	Very Low				
									11-30%									



## EA 1L250 QUALITATIVE RISK REGISTER

EA 1L250		Phase: 1		RIV 015 PM: 8.1/ 24.1		PM: Dalia Alarkan		Const Capital Estimate: \$53,180K		Project Description: GRIND ALL LANES, SLAB REPLACEMENT, 0.2 MILL & OVERLAY ON RAMPS & SHOULDERS, LIGHTING, ITS & WIM ELEMENTS, COMPLETE STREETS ELEMENTS, ROADSIDE SAFETY IMP.		Project Location: IN & NEAR MURRIETA, WILDOMAR & LAKE ELSINORE FROM 1.4 MI S/O MURRIETA HOT SPRINGS RD TO 0.2 MI N/O NICHOLS RD						
Program Code: 201.121 / CAPM		M460 Target: 3/2/27				ARM: Kimberly Portillo		R/W Capital Estimate: \$118K										
Risk No.	Status	Type	Date of Origin	Updated	Category	Title	Risk Statement	Root Cause/Relevancy/Current Status /Assumptions/Comments/Triggers	Probability	Cost Impact				Schedule Impact		Response Strategy	Response Actions	Risk Owner
			Originator							Ph	Impact	Ph	Impact	Ph	Impact			
11	Active	Threat	2/8/2023	1/15/2026	Project Mgmt	Increase in Material Cost due to Market Fluctuations	Because of current trends and market situation, we may have significant increases in material and labor costs during the project development and delivery phases. This may impact project capital cost.	This risk is considered since there are large quantities of PCC and AC are involved and a small change in unit cost will impact the project cost. Also, there is a possibility that unit cost may increase and contractor's bids may come higher than the engineer's estimate. Higher cost of transporting materials to job site might increase the bids as well.	Moderate	0				0		Mitigate	Design will further evaluate this risk during PSE and will update the cost estimate using recent unit costs and contract cost database. PM may need to adjust the cost as necessary.	Dalia Alarkan
			1										1					
			2								9		2					
			3								4	Low	3					
										31-50%								
15	Active	Threat	7/29/2024	1/15/2026	Construction	Additional Slabs	If pavement damage increases as a result of construction staging/traffic handling, additional slabs will need to be replaced. This will increase cost and may impact schedule.	During construction, traffic will be diverted onto existing lanes as other lanes are being replaced. These lanes may be further damaged in the next few years and also during the project's construction.	Low	0				0		Mitigate	The program allows for a maximum of 20% slab replacement. RE will work with the contractor to select the most suitable slabs for replacement. If it is found that additional slab replacements are needed then contingency may be used to address this risk.	Joe Lopez
			1										1					
			2								9		2					
			3							Low	4	Moderate	3	Low				
										11-30%								
16	Active	Opportunity	11/25/2024	1/26/2026	Design	Overlapping Projects	As a result of overlapping projects 0J082 and 0F310, some work is being omitted from 1L250 that may be included in the above mentioned projects. The overlapping scope in 0F310 includes MVP's, ramp paving, guardrail, lighting, signals, and ramp metering at the I-15/Central Avenue I/C. The overlapping scope in 0J280 includes repaving the existing shoulder from PM 21.5 to 24.1. This may reduce project costs and schedule.	Multiple elements of the Central Avenue I/C and the shoulder pavement between PM 21.5 and 24.1 are outdated and require replacement and/or rehabilitation. It is currently being assumed that projects 0J082 and 0F310 will be approved, therefore overlapping scope may be eliminated from 1L250. If these projects are not approved, an environmental reevaluation will be required which may take 30-90 days.  0J082: App Con: 7/1/2027 CCA: 12/21/2030 1L250: App Con: 1/16/2029 CCA: 7/15/2030 0F310: App Con: 2/29/2028 CCA: 2/21/2030	Moderate	0				0		Accept	The 1L250 PDT will continue getting updates from the respective overlapping project teams on their approval status. If they are not approved, the 1L250 PDT will add the scope back into the project and the PM will need to adjust cost and schedule as necessary.	Ronald Pham
			1							Low			1	Low				
			2								9		2					
			3							Low	4	Low	3	Low				
										31-50%								
18	Active	Threat	1/27/2025	1/15/2026	Public Affairs	Public Convenience/Traffic Control	Due to the location of this project, any planned lane closures, full closures, or traffic impacts should carefully evaluate their effects on local businesses, nearby recreational areas, and commuters. If local businesses are impacted, this may affect project cost and schedule.	I-15 serves as a vital corridor for both local and regional commuters. Within the project limits, there are numerous businesses and residents, and access to these must be taken into account when planning project work and developing a traffic management plan. Additionally, coordination with partnering agencies is essential, particularly during peak travel periods. It is also recommended to limit work during major holiday travel times to minimize disruptions.	High	0				0		Mitigate	To minimize disruptions during the I-15 project, a traffic management plan will ensure access to businesses and residences while maintaining commuter flow, with work scheduled during off-peak hours and major holiday travel avoided. Coordination with partnering cities/agencies and clear public communication will help streamline operations and address potential conflicts, especially during peak periods. Continuous monitoring and real-time adjustments will be implemented to optimize traffic flow and address emerging issues efficiently.	Carolina Rojas
			1										1					
			2								9		2					
			3							Very Low	4	Very Low	3	Low				
										51-70%								



### EA 1L250 QUALITATIVE RISK REGISTER

EA 1L250		Phase: 1		RIV 015 PM: 8.1/ 24.1		PM: Dalia Alarkan		Const Capital Estimate: \$53,180K		Project Description: GRIND ALL LANES, SLAB REPLACEMENT, 0.2 MILL & OVERLAY ON RAMPS & SHOULDERS, LIGHTING, ITS & WIM ELEMENTS, COMPLETE STREETS ELEMENTS, ROADSIDE SAFETY IMP.		Project Location: IN & NEAR MURRIETA, WILDOMAR & LAKE ELSINORE FROM 1.4 MI S/O MURRIETA HOT SPRINGS RD TO 0.2 MI N/O NICHOLS RD					
Program Code: 201.121 / CAPM		M460 Target: 3/2/27				ARM: Kimberly Portillo		R/W Capital Estimate: \$118K									
Risk No.	Status	Type	Date of Origin	Updated	Category	Title	Risk Statement	Root Cause/Relevancy/Current Status /Assumptions/Comments/Triggers		Probability	Cost Impact		Schedule Impact		Response Strategy	Response Actions	Risk Owner
			Originator								Ph	Impact	Ph	Impact			
19	Active	Threat	7/8/2025	1/26/2026	Materials Engineering	Winter Weather	When surface temperatures drop below 50 degrees F, specially during nighttime and winter season, RHMA-G paving (mainline shoulders and ramps) cannot be performed and this could lead to Construction delay. This may impact project cost and schedule.  M500 = 1/16/29 WD = 250		Low	0			0		Mitigate	RE will monitor the weather conditions and coordinate with the contractor to plan the work accordingly. PM may adjust the construction schedule as needed and utilize contingency funding to cover cost increase.	Sittampalam Sathiskumar
													1				
											9		2				
			3							Low	4	Low	3	Very Low			
20	Active	Threat	12/1/2025	1/26/2026	Materials Engineering	Digout Quantities	If pavement conditions worsen by construction start date, then digout quantities may need to be increased within the areas where cold plane and overlay are proposed (especially the ramps). Increased digout quantities could result in increasing project cost and it may also impact schedule.  M500: 1/16/2029		Low	0			0		Mitigate	Early coordination with Maintenance, Construction, and Materials Engineering will be done during PSE to refine the quantities of digouts, if necessary. Currently, existing loops are being evaluated by Constructability. PM may have to adjust cost and schedule.	Sittampalam Sathiskumar
													1				
											9		2				
			3							Low	4	Low	3	Very Low			
21	Active	Threat	12/1/2025	1/15/2026	Materials Engineering	Oil Price Index	Due to uncertainty in oil price, crude oil index fluctuation could impact asphalt concrete prices. This may lead to an increase in construction capital cost.  Price index fluctuations are currently included in the project estimate. However, the factored cost is based on the oil index of the present month.  The actual cost may vary substantially since the crude oil prices for 2027 cannot be predicted with accuracy.  The oil price index may impact the following items:  - HMA-A: 41,794 TON - Tack Coat: 189 TON		Moderate	0			0		Mitigate	Revised estimates should also be done in PSE phase to address crude oil price fluctuations through 2027. PM may need to adjust project cost through contingency.	Sittampalam Sathiskumar
													1				
											9		2				
			3								4	Low	3				

Attachment E  
SHOPP Performance  
Measure

### SHOPP Project - Accomplishment - Performance Measures - Benefits

District: 08 Tool ID: 19697 Project ID: 0819000168 EA: 1L250 Co-Rte-PM: RIV-015-8.1/24.1 (Primary Location)  
 Res In PID WP: 06/13/22 Project Manager: Dalia Alarkan

Save to Excel

- Bridge
  Pavement
  Drainage
  Facilities
  Signs and Lighting
  Mobility
  Roadside
  Bicycle and Pedestrian Infrastructure
  Sustainability /Climate Change
  Advance Mitigation /Mitigation
  Major Damage & Betterments
  Green-house Gases
  Relinquishment

#### Performance & Accomplishments (PPC)

ActID	Activity Detail	Performance Objective	Unit of Measurement	Quantity	Pre-Good	Pre-Fair	Pre-Poor	New	Post-Good	Post-Fair	Post-Poor	HQ Program Review - Agree with District?	HQ Comment	Review Date	Performance Change Date After Review	Comment
1	B26 Concrete Pavement Minor Rehab (CAPM)	Pavement Class I	Lane Miles	95.870	25.266	57.082	13.522		95.870						11/12/25	
2	E07 Guard Rail	No Performance Objective in the SHSMP	Linear Feet	9855.000			9,855.000		9,855.000							
3	E24 Lighting - Rehabilitation	Lighting Rehabilitation	Each	66.000			66.000		66.000			Yes	Provide the IMMS IDs of the assets being programmed	11/29/22		
4	E55 Proactive Safety Vehicles	Proactive Safety	Annual Fatal & Serious Injury Collisions	0.740			0.740		0.740			Yes		06/29/22		
5	E99 Other Safety Activity	No Performance Objective in the SHSMP -		53.000			53.000		53.000							Upgrade end treatments of the existing guardrails. List provided by Kevin Chen.
6	F06 Ramp Meter	No Performance Objective in the SHSMP	Each	6.000	6.000				6.000							
7	F14 Weigh in Motion System	Weigh-In-Motion Scales	Stations	2.000			2.000		2.000			Yes		01/13/22		PM 21.6 NB & SB, 37- ELSINORE SB 38- ELSINORE NB
8	F23 ADA - New Curb Ramp Installed	No Performance Objective in the SHSMP	Each	4.000				4.000				Yes		07/19/23		
9	F43 ADA - Deficient Elements	ADA Pedestrian Infrastructure	Deficient Elements	4.000				4.000								
10	F46 TMS Technology Component	Transportation Management Systems	Each	6.000	6.000				6.000							
11	G07 Worker Safety - Safe Access	Roadside Safety Improvements	Locations	20.000			20.000		20.000			Yes	Well documented in PID!	07/24/23		Locations are provided in the PIR
12	G09 Worker Safety - Miscellaneous Paving/Treatment	Roadside Safety Improvements	Locations	16.000			16.000		16.000			Yes	Well documented in PID!	07/24/23		Locations are provided in the PIR
13	G10 Worker Safety - Vegetation Control	Roadside Safety Improvements	Locations	20.000			20.000		20.000			Yes	Well documented in PID!	07/24/23		Locations are provided in the PIR
14	H06 Bikeway Class II	No Performance Objective in the SHSMP	Linear Feet	550.000			550.000		550.000			Yes	In PID	07/21/23		
15	H21 Sidewalks (less than 8 feet)	No Performance Objective in the SHSMP	Linear Feet	350.000			350.000		350.000			Yes	In PID	07/21/23		
16	H32 Is any Location Within the Project Limits Ped/Bike Accessible?	No Performance Objective in the SHSMP	Yes/No	Yes								Yes		11/24/21		Yes
17	H63 Bicycle and Pedestrian Infrastructure	Bicycle and Pedestrian Infrastructure	Linear Feet	900.000			900.000		900.000							
18	N01 Qualitative	No Performance Objective in the SHSMP -														No change in operations, CE/CE

# Attachment F

## ISA Checklist

# INITIAL SITE ASSESSMENT (ISA) CHECKLIST – ESR Rev. #2

DATE: **11/25/2025**

**PROJECT INFORMATION**

District 08 County RIV Route 15 Postmile 8.1/24.1 EA: 1L250 PN: 0819000168

Description of Work:

**The project is located in the cities of Murrieta, Wildomar, and Lake Elsinore, in Riverside County. The project scope includes the following:** Random slab replacement on mainline lanes; 0.20' cold plane and overlay on shoulders, on-ramps, and off-ramps including I-215/I-15 connectors; Grind existing concrete pavement on all mainline lanes; Upgrade 19 Transportation Management Systems (TMS), 12 signals, and 6 Ramp Metering Systems (RMS); Upgrade 66 existing lighting elements to current standards; Eliminate sidewalk gaps at south side of Wildomar Trail; Convert the existing shoulder on north side of Bundy Canyon Road to Class II bike lane with signages and pavement markings; Construct 2 new curb ramps and 2 new refuge pads to Americans with Disabilities Act (ADA) current standards; Upgrade 2 Weigh in Motion (WIM) systems; Install roadside safety improvements including 13 Maintenance Vehicle Pullouts (MVPs), paving beyond gore areas, and light duty maintenance access trails; Upgrade existing guardrail and end treatments to current standards including vegetation control (minor concrete) and connections to existing bridges (anchor blocks); Install trash capture devices onto 5 existing drainage systems. All work will remain within the limits of the existing right-of-way. No Fees or easements will be required. ESR Rev. #1 applied the following changes: The locations of lighting work have been finalized. The lighting work includes replacing 20 Soffit lights (underneath bridges) and 23 light poles; the trash capture device locations have been updated, in total, trash capture locations include 13 inlets, 7 outlets, and one infiltration area; the depth of cold plane and overlay of ramps and shoulders has been reduced from 0.20' to 0.15'; the locations of cold plane and overlay have not changed. ESR Rev. #2 changed the following: Trash capture devices at Culvert ID 560150001480 have been removed from the project scope; added electrical scope: install conduit and pull boxes outside Caltrans R/W boundaries at PM 22.3 (Central NB ramp), PM 16.89 (Lemon St.), and PM 16.29 (Bundy Canyon NB ramp). (Note: Local roads referenced by nearest freeway PM). The work includes Bridge work, Road cut/fill, Detours, Grinding, and Drainage/culverts.

Project Engineer:	<u>Marc Mitri</u>	Telephone:	<u>(909) 501-9299</u>
Environmental Coordinator:	<u>Amy Lee</u>	Telephone:	<u>(909) 806-3234</u>
Date ISA Needed:	<u>12/18/2025</u>		

Attach the project location map and an aerial photo to this checklist to show the location of proposed R/W and all known and/or potential hazardous waste sites.

1. Project Features: New R/W? **NO** Excavation? **YES** Railroad Involvement? **NO**  
Structure Demolition/Modification? **NO** Utility Relocation? **TBD**
2. Project Setting: Rural - **NO** Urban - **YES**  
Current Land Uses: Existing Highway  
Adjacent Land Uses: Commercial, Residential  
(Industrial light industry, commercial, agriculture, residential, other)
3. Check Federal, State, and local environmental and health regulatory agency records as necessary to see if any known hazardous waste site is in or near the project area. If a known site is identified, show its location on the attached map and attach additional sheets as needed to provide all information available pertinent to the proposed project. IS PROJECT AFFECTING SITES LISTED ON CORTESE LIST? IF YES, DESCRIBE SITE: \_\_\_\_\_
4. \_\_\_\_\_
5. Conduct Field Inspection GeoTracker, EnviroStor & Mineral hazards Info Maps Date: 12/07/2022

Storage Structures/Pipelines:	Contamination: (spills, leaks, illegal dumping, etc.)	Hazardous Materials: (asbestos, lead, etc.)
USTs <u>NO</u>	Surface Staining <u>NO</u>	Buildings <u>NO</u>
Surface tanks <u>NO</u>	Oil Sheen <u>NO</u>	Sprayed-on <u>NO</u>
Sumps <u>NO</u> Ponds <u>NO</u>	Odors <u>NO</u>	Fireproofing <u>NO</u>
Drums <u>NO</u> Basins <u>NO</u>	Vegetation damage <u>NO</u>	Pipe Wrap <u>NO</u>
Transformers <u>NO</u>	Other _____	Friable Tile <u>NO</u>
Landfill <u>NO</u>		Acoustical Plaster <u>NO</u>
Other _____		Serpentine <u>NO</u>
		Paint <u>NO</u> Other _____

Other comments and/or observations:

GeoTracker and EnviroStor online database systems showed no hazardous contaminated sites at or near the project site. An aerially deposited lead (ADL) and Title 22 metals survey is scheduled for the project on December 2025, anticipated study completion is by end of January 2026. New SSPs may be added below based on the findings in the site survey. The section of the I-15 FWY within the project limits has recently been striped and hence, no lead paint study is warranted.

Include the following Standards Special Provisions in the PS&E package:

- SSP 6-1.0B:** Conditions for use of local material from non-commercial source, ADL spec pending completion of ADL survey.
- SSP 14-9.02:** NESHAP notification.
- SSP 14-11.14** for the removal and disposal of Treated Wood Waste (TWW) from signposts and/or guardrail posts.
- SSP 87-21.03D:** For the removal of Vehicle Sensor Nodes (VSN) and Electrical Systems (SSP shall be edited by the electrical engineer).
- All three SSPs below require a lead compliance plan (LCP) and item #070030 for LCP.
- ADL spec pending completion of ADL survey.
- SSP 36-4:** Residue Containing Lead from Paint and Thermoplastic,
- SSP 84-9.03B:** Separate removal of yellow traffic stripe and pavement marking with non-hazardous residue.



Attachment G  
Right of Way Data Sheet

**M E M O R A N D U M**

To: JEFFREY LAMBERT  
Design I

From: GUSTAVO GUTIERREZ  
RW Project Coordination

Date: November 7th, 2025-Revision  
File: 08-Riv 15 PM – 8.1 / 24.1  
Project: Capital Preventative  
Maintenance  
E.A./P.N.: 1L2500 / 0819000168

We have completed an estimate of the right of way costs for the above-referenced project based on the request received on **February 3, 2025**, and the following assumptions and limiting conditions:

- Mapping received did not provide sufficient detail to determine the limits of the right of way requirements and/or to determine damages to the remainder parcels impacted by the project.
- Additional right of way requirements may be anticipated but are not defined due to the preliminary nature of the early design requirements.
- We have determined that there are no right of way functional involvements in the proposed project at this time as currently designed.
- Due to the preliminary nature of the project scope/mapping, utility estimate was provided without the benefit of As-Built maps or potholing.
- Other: Available as built shows utilities within the project limit. UEW will conduct further investigation during phase 1. **PCR needed for Capital increase**

Right of Way Engineering will require a minimum of 0 months after receiving final Right of Way Requirements to deliver Right of Way Appraisal mapping (M224).

Right of Way will require a minimum of 12 months prior to certification of the subject project after receiving final Right of Way Appraisal maps, necessary environmental clearances, and approved freeway agreements (M225).

Shorter lead times may lead to additional Right of Way resources, an increased number of eminent domain actions and possibly result in missing the certification date. Any of these actions may reflect adversely on the District's other programs or the Department's and/or District's public image.

\*NOTE: THE WORKPLAN WILL BE SENT SEPARATELY AND ARE BASED ON THE INFORMATION PROVIDED WITH THE DATA SHEET REQUEST. IF THERE IS A CHANGE IN SCOPE, A REVISED DATA SHEET AND WORKPLAN WILL BE PROVIDED.

Attachments:

- [XX] Right of Way Data Sheet
- [XX] Utility Information Sheet
- [XX] Railroad Information Sheet
- [XX] Government Lands Information Sheet
- [ ] M.C.C.E.

EVNT RW	_____
COST RW1 - 6	_____
TEXT TI	_____
SCAN	_____
CLASS	_____
AGRE	_____
TPRC	_____

11/10/2025

**RIGHT OF WAY DATA SHEET**

(Form #)

Current 9-Phase Programming: \$ 56,000.00

1. Right of Way Cost Estimate:

	Value
A. Acquisition, including Excess Lands, Damages, Goodwill, Major Rehabilitation, and Permits to Enter Railroad	\$ 0.00
Federal Lands – Special Use	\$ 0.00
B. Acquisition of Offsite Mitigation.	\$ 0.00
C. Utility - Relocation (State share)	\$ 0.00
- Potholing (130 Potholes @ \$750.00)	\$ 97,500.00
D. RAP	\$ 0.00
E. Clearance/Demolition	\$ 0.00
F. Title and Escrow Fees	\$ 0.00
G. Project Permit Fees	\$ 20,000.00
H. Condemnation Costs	\$ 0.00
<b>Total R/W Estimate:</b>	<b>\$ 117,500.00</b>

2. Anticipated Date of Right of Way Certification February 01, 2027

3. Parcel Data:

Type	Dual/Appr	Utility Involvement	<b>RR Involvement</b>	<b>NO</b>
X _____	_____	U4-1 _____	C&M Agreement	_____
A _____	_____	-2 _____	Svc Contract	_____
B _____	_____	-3 _____	OE Clearances/ Clauses	_____
C _____	_____	-4 _____	LIC/ROE	_____
D _____	_____	U5-7 <u>4</u> _____		
		-8 _____		
Total Parcels _____		-9 _____	<b>Federal Lands</b>	<b>NO</b>
			Number of Parcels	_____

**Misc. R/W Work**

RAP Displacement	_____
Clear/Demo	_____
Const Permits	_____
Condemnation	_____
Permits to Enter-ENV	_____

Areas: Right of Way: S.F. \_\_\_\_\_  
 Excess: S.F. \_\_\_\_\_  
 No. Excess Land Parcels: \_\_\_\_\_

**RIGHT OF WAY DATA SHEET**

(Form #)

4. Are there major items of Construction Contract Work?  
Yes \_\_\_\_ No X (If yes, explain.)

5. Provide a general description of the right of way and excess lands required (zoning, use, major improvements, critical or sensitive parcels, etc.).

Type and Number of Parcels: Total Number of Larger Parcels 0

Fee \_\_\_\_\_  
Easements \_\_\_\_\_

6. Is there an effect on assessed valuation?  
Yes \_\_\_\_ Not Significant \_\_\_\_ No X (If yes, explain.)

7. Are utility facilities or rights of way affected?  
Yes \_\_\_\_ No X (See attached Utility Information Sheet)

The following checked items may seriously impact lead time for utility relocation:

- Longitudinal policy conflict(s).
- Environmental concerns impacting acquisition of potential easements.
- Power lines operating in excess of 50 KV and substations.

8. Are railroad facilities or rights of way affected? Yes \_\_\_\_ No X  
(See attached Railroad Information Sheet)

9. Were any previously unidentified sites with hazardous waste and/or material found?  
Yes \_\_\_\_ Non-Evident X  
(If yes, attach memorandum per R/W Manual, Chapter 4, Section 4.01.10.00.)

10. Are State or Federal rights of way affected?  
Yes \_\_\_\_ No X (See attached Government Lands Information Sheet)  
Agencies Involved: \_\_\_\_\_  
Rights/Permissions Required: \_\_\_\_\_

11. Are RAP displacements required? Yes \_\_\_\_ No X  
No. of single-family \_\_\_\_ No. of business/nonprofit \_\_\_\_  
No. of multi-family \_\_\_\_ No. of farms \_\_\_\_  
Based on Draft/Final Relocation Impact Statement/Study dated \_\_\_\_\_, it is anticipated that sufficient replacement housing (will/will not) be available without Last Resort Housing.

12. Are there material borrow and/or disposal sites required?  
Yes \_\_\_\_ No X (If yes, explain.)

13. Are there potential relinquishments and/or abandonments?  
Yes \_\_\_\_ No X (If yes, explain.)

14. Are there existing and/or potential Airspace sites?  
Yes \_\_\_\_ No X (If yes, explain.)

**RIGHT OF WAY DATA SHEET**

(Form #)

15. Is it anticipated that all Right of Way work will be performed by CALTRANS staff?  
 Yes  X  No   (If no, discuss.)

Evaluations prepared by:

Right of Way Estimator: STEPHEN HENSLEY, Associate Right of Way Agent  
 Railroad Coordinator: LYND SAY CAMPANELLA, Associate Right of Way Agent  
 Utility Coordinator: BRENDA FLORES, Right of Way Agent  
 Federal Lands: ANDREW FILAMOR, Associate Right of Way Agent  
 Right of Way Engineering: CARLOS URIBE, Transportation Land Surveyor

Reviewed By:

Max Lin  
 MAX LIN  
 Project Coordinator  
 District 8, Right of Way

Reviewed By:

Gustavo Gutierrez  
 GUSTAVO GUTIERREZ  
 Senior RW Agent, Project Coordination  
 District 8, Right of Way

Date: 02/27/2025

Date: 02/28/2025

I have personally reviewed this Right of Way Data Sheet and all supporting information. I certify that the probable Highest and Best Use, estimated values, escalation rates, and assumptions are reasonable and proper subject to the limiting conditions set forth, and I find this Data Sheet complete and current.

Christine Senteno  
 CHRISTINE SENTENO  
 Project Delivery Manager  
 District 8, Right of Way

Rebecca Guirado  
 REBECCA GUIRADO,  
 Deputy District Director  
 District 8, Right of Way and Land Survey

Date: 03/06/2025

Date: 03/06/2025

REVISIONS			APPROVAL	
No.	Date	Reason for Revision	Project Coordinator	Sr. RW Agent
1	07/03/2025	Utility potholing decreased. Cost estimate updated.	<i>ML</i>	<i>gg</i>
2	11/07/2025	Utility potholing decreased from 230 to 130. Cost estimate updated.	<i>ML</i>	<i>gg</i>

# RIGHT OF WAY DATA SHEET

(Form #)

**This utility estimate was prepared using “project specific” data and unit values. This information is not to be utilized for the updating or preparation of this, or any other Right of Way Cost Report or Utility Information Sheet.**

## UTILITY INFORMATION SHEET

1. List of utility companies in the project area:

**AT&T Transmission, AT&T Distribution, Eastern Municipal Water District, Elsinore Valley Municipal Water District, Crown Castle, Rancho California Water, Santa Ana Watershed PRO High-Pressure Sewer, Southern California Gas Distribution, Spectrum, Southern California Edison Distribution, Southern California Edison Trans Telecom, Frontier, Western Municipal Water District.**

2. Type and name of utilities in conflict and agreements required:

**Notices to Owners and Utility Agreements are not expected.**

3. Is any facility a longitudinal encroachment in existing or proposed access controlled right of way?  
Explain

Disposition of longitudinal encroachment(s):

- None
- Relocation required.
- Exception to policy needed.
- Other. Explain

4. Additional information concerning utility involvement on this project. Is there any special circumstances/facilities requiring additional lead time?

**The estimated number of potholes was reduced from 230 to 130, by UEW. Maintenance advised Electrical Design that the foundation of Streetlight did not need to be replaced. Therefore, UEW removed all potholes at the streetlight replacement location.**

**If the foundation of streetlight must be replaced, then that would cause the number of potholes to increase in Phase 1. This estimate also assumes no Trash Capture device is installed and there is no hydraulic improvement on the project.**

**Should design need pre-construction utility(ies) verification, positive location, or relocation of utility(ies); The right of Way Utility Coordinator (UC) must be contacted and provided with geometric base maps and written request for utility Owners (UOs) for verification and corrections. The UC will then provide Design with the updated maps of U-Sheets. Design will then determine all utility conflicts that require positive location and/or relocation [See Design Task D283 (220.D.)].**

5. Potholing costs: \$ **97,500 (130PH X \$750).**

Total estimated cost of State's obligation for utility relocation on this project:  
(Phase 9 funding) \$**0.00.**

**RIGHT OF WAY DATA SHEET**

Capital Preventative

Maintenance

1L2500 / 0819000168

(Form #)

Facility Owner	Type of Relocation (facility)	Quantity (i.e., LF of waterline, # of manholes, # poles, etc.)	Cost of Each relocation	Total Cost of relocations	Estimated Grand Total including contingency
<b>N/A</b>					

Utility Involvement

U4-1 \_\_\_ total number of expected owner expense involvements

-2 \_\_\_ total number of expected State expense involvements-conventional highway, no Federal aid


-3 \_\_\_ total number of expected State expense involvements-freeway, no Federal aid

-4 \_\_\_ total number of expected State expense involvements-conventional or freeway, with Federal aid

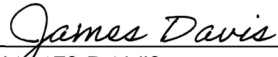
U5-7 **8** \_\_\_ total number of expected utility verifications, which will not result in involvements

-8 \_\_\_ total number of expected utility verifications, 50% which will result in involvements, and 50% will not

-9 \_\_\_ total number of expected utility verifications, which will result in involvements

Prepared By:   
 BRENDA FLORES  
**Right of Way Utility Estimator**

Date: 11/7/2025

Reviewed By:   
 JAMES DAVIS  
 Senior Right of Way Agent, Utilities

Date: 11/7/2025

**RIGHT OF WAY DATA SHEET**

(Form #)

RAILROAD INFORMATION SHEET

1. Describe railroad facilities or rights of way affected.  
**Per the scope of work, there is no railroad involvement anticipated. There are no railroad tracks located within the vicinity of the project.**
  
2. When branch lines or spurs are affected, would acquisition and/or payment of damages to businesses and/or industries served by the railroad facility be more cost effective than construction of a facility to perpetuate the rail service? Yes \_\_\_ No **X** (If yes, explain.)
  
3. Discuss types of agreements and rights required from the railroads. Are grade crossings requiring service contracts, or grade separations requiring construction and maintenance agreements involved?  
**None**
  
4. Remarks (non-operating railroad right of way involved?):  
**None**
  
5. 4-Phase Cost: \$   **0**    
Explanation: (Flagging)
  
- 9-Phase Cost: \$   **0**    
Explanation: (ROE, Svc Contract)
  
6. PMCS Input Information
 

RR Involvement	<u>  <b>NO</b>  </u>
C&M Agreement	<u>          </u>
SVC Contract	<u>          </u>
OE Clearances/ Clauses	<u>          </u>
LIC/ROE	<u>          </u>

Anticipated Lead time: **No additional anticipated lead time.**

Prepared By: Lyndsay Campanella  
LYNDSAY CAMPANELLA  
Right of Way Railroad Coordinator

Date: 02/20/2025

Reviewed By:   
AIDEE ARPON  
Senior Right of Way Agent, Acquisitions

Date: 2/20/2025

**RIGHT OF WAY DATA SHEET**

(Form #)

FEDERAL LANDS INFORMATION SHEET

**Are Federal Lands involved?**

Yes  No  (If "Yes," provide the following information.)

Agencies Involved:

_____ Army Corps of Engineers	_____ GSA	_____ US Postal Service
_____ BIA	_____ National Parks	_____ Veterans Administration
_____ BLM	_____ US Fish & Wildlife	_____ Other _____
_____ Dept. of Parks & Recreation	_____ US Forest Service	_____ Other _____

Rights/Permissions Required:

_____ Cooperative Work Agreement	_____ Letter of Concurrence	_____ Right of Way Grant
_____ Cost Recovery	_____ Letter of Consent	_____ Special Use Permit
_____ Courtesy Letter	_____ Mineral Agreement	_____ Timber Sale
_____ Easement	_____ Perfection of Title	_____ Transfer of Jurisdiction
_____ Highway Easement	_____ Right of Entry	_____ Other _____

9-Phase Cost Anticipated (if any) \_\_\_\_\_

Explanation:

No federal lands involvement within project limits.

Remarks: (if multiple agencies, please comment on what rights are needed from each)

Anticipated Lead time: \_\_\_\_\_

Prepared By:   
ANDREW FILAMOR  
Right of Way Federal Lands Coordinator

Date: 2/19/2025

Reviewed By:   
AIDEE ARPON  
Senior Right of Way Agent, Acquisitions

Date: 2/20/2025

# Memorandum

**To:** CHRISTINE SENTENO  
OFFICE CHIEF  
RIGHT OF WAY PROJECT COORDINATOR

**Date:** January 27, 2025

**File:** 08-Riv-15 PM 8.1/24.1  
In Murrieta, Wildomar &  
Lake Elsinore  
PN: 0819000168  
201.121 CAPM

**From:** RONALD PHAM   
Office Chief  
Design I

**Subject:** **RIGHT OF WAY DATA SHEET REQUEST**

Design I is preparing the Project Report for the above-referenced project. The purpose of this pavement rehabilitation project is to grind all lanes, perform random slab replacement, mill & overlay on ramps & shoulders, lighting, upgrade ITS and WIM elements, complete streets elements, improve roadside safety, and upgrade existing end treatments and guardrails.

Attached for your use are the Right of Way (R/W) Data Request Form, a preliminary plan set, and the Utility Data Assessment (UDA).

Currently, the Project Approval & Environmental Document approval date is scheduled for February 2<sup>nd</sup>, 2026.

Please provide us with the Right of Way Data Sheet by 2/27/2025.

Should you have any questions or need additional information, please contact Jeffrey Lambert, Project Engineer, at (909) 893-2289 or myself at (909) 501-9299.

RONALD PHAM  
January 27, 2025  
Page 2

Attachments

1. Right of Way Date Sheet Request Form
2. Preliminary Plan Set
3. UDA

c: DAlarkan, Project Manager  
JGalvan, DDD-Design  
CQuach, Design Manager

MM/TF

Attachment H  
Transportation Management  
Plan



<b>TMP Elements</b>	EA #/ID#	1L250/0819000168	Date	1/8/2026
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**Note: A checkmark in the box means** you need to include this in the project unless staging, material, or work hour changes eliminate the need for the item. A **?** in front means TMP anticipates this - please check into this. A blank box means the item is not needed at this time based on the information received.

Public Affairs officer's 1st. & last name	Phone number
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<b>1</b>	<p><b>Public Information/Public Awareness Campaign (PAC).</b> Developer: Remember to obtain the estimate from Public affairs by contacting Emily Leinen. Procedure is in the file under 3- TMP matters</p>	<table border="1"> <tr> <td style="text-align: center;">Estimated Cost</td> </tr> <tr> <td style="text-align: center;">\$ 50,000</td> </tr> </table>	Estimated Cost	\$ 50,000
Estimated Cost				
\$ 50,000				
	<p>BEES 066063 (Traffic Management Plan-Public Information). Cost to be reduced by Public Affairs (PA) and Construction Liaison (CL) only. Show under <b>State Furnished</b> as the total of PA+CL.</p>			

- 1.01  Include Rideshare information in PA/CL project material to encourage vehicles reduction in work area
- 1.02  Brochures and Mailers
- 1.03  Media Releases (& minority media sources)
- 1.04  Paid Advertising
- 1.05  Public Meetings/PAC Mtgs./Speakers Bureau (show cost also for room rental)
- 1.06  Hand deliver notices to vicinity
- 1.07  Broadcast fax service
- 1.08  Coordinate with TMC about the traffic incident management plan over 75 Million project
- 1.09  Visual Information (videos, slide shows, etc.)
- 1.10  Local cable TV and News
- 1.11  Traveler Information System (Internet)
- 1.12  Internet, E-mail, Social Media
- 1.13  Notification to targeted groups:
  - Revised Transit Schedules/maps
  - Rideshare organizations
  - schools
  - organizations representing people with disabilities
  - bicycle organizations
- 1.14  Include PA/CL/Consultant resources in WPS
- 1.15  Commercial traffic reporters/feeds - e.g. brief Traffic Information people (TIP) group
- 1.16  Insert SSP's
 

"A representative of the Contractor, at Superintendent level or higher, and authorized to commit the Contractor, shall attend and participate in all Public Awareness Campaign meetings. Time commitment for the meeting(s) varies from two to four hours per month."

<b>Section 1 Total</b>	\$ 50,000
------------------------	-----------

**2 Motorist Information Strategies**  
**Project team needs to coordinate with Traffic Design!**

- 2.1  Existing Overhead Changeable Message Signs (Stationary)
 

New Installation (Stationary) - BEES 860532 CHANGEABLE MESSAGE SIGN SYSTEM - list locations
- 2.2  Lane Closure System Website
- 2.3  Caltrans Highway Information Network (CHIN)
- 2.4  Portable Radar Speed Feedback Sign System Day BEES 120204 (approx. EA @ \$50,000)
- 2.5  Bicycle and pedestrian information, e.g. Detour maps
- 2.6  Automated Workzone Information System (AWIS) BEES 120105

<b>Section 2 Total</b>	\$ -
------------------------	------

**3 Incident Management**

- 3.1  CHP's Construction or Maintenance Zone Enhanced Enforcement Program - COZEPP or MAZEPP. BEES 066062 - show under "State or Agency furnished" in the Cost Estimate.

Make sure to consider the LC hours and add CHP driving time to/from their office

**Day COZEPP:** To protect active closures

# of days	hours/day	CHP vehicles	# of officers.	Rate/Hr.
20	8	1	1	\$ 250

\$ 40,000

**Night COZEPP:** To protect active closures

# of nights	hours/night	CHP vehicles	# of officers. Nights need 2 per car	Rate/Hr.
200	10	1	2	\$ 250

\$ 1,000,000

<b>TMP Elements</b>	EA #/ID#	1L250/0819000168	Date	1/8/2026
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3.2  **Tow Truck Service for Construction** \$/hr./truck  
 BEES 018142 - Tow Truck Service \$70

A For service within the regular Tow Truck hours

# of trucks	# of days	Hours per day	
<input type="text"/>	<input type="text"/>	<input type="text"/>	\$0

B For service outside the regular Tow Truck hours

# of trucks	# of days	Hours per day	
<input type="text"/>	<input type="text"/>	<input type="text"/>	\$0

**Section 3 Total** \$ 1,040,000

**4 Construction Strategies**

Contact DTM, at 909-383-6262, to get Delay Calculations, Lane Requirement Charts (LRC), Table Z and Special events list. Inform DTM of any concerns/commitments regarding special LC days, times, seasons, events; environmental restrictions; if work may be affected by snow and low or high temperatures. E.g. excessive heat may delay HMA operations lane openings which may increase traffic impact when vehicles overheat in the queue; etc. If traffic volumes vary significantly between seasons, consider 2 sets of LRCs to avoid CCOs.

4.1 This TMP presumes that work is planned as below. If different, TMP needs to be revised. The Project Engineer shall ensure all appropriate lane requirement charts are included.

- Day
- Night
- Weekend

4.2 Expected facility closures and requirements

- Flagging
- Shoulder
- Lane
- Local Street
- Ramp
- Connector\*
- Extended Weekend Closures\*
- Total Facility Closures\*

\*Consult with TMP developer and the DTM regarding COZEEP & other costs. Provide proposed detour and traffic diversion plans for review.

**CAUTION:** If the Lane Requirement Chart (LRC) for full mainline closures, of one or both directions on a highway or freeway, does not show the maximum number of allowable closures, the PS&E shall not be certified by DTM/TMP.

4.3  BEES 066008 Incentives

4.4  BEES 120101 Traffic Control Supervisors (DAY)

# of days w/ Active Complex Temporary Traffic Control	# of days w/o No Active Complex Temporary traffic control	# of nights w/o 55 hour Weekend Closure	# of 55 Hour Weekend Closure
0			

# of days	# of nights	% Workdays	WNI (Weekly Nighttime inspections)	WWZSR (Weekly Work Zone Safety Review)	% Contingency Days	Quantity Estimate	Rate/Hr.
0	0	0	0	0	0	0	\$ 900

4.5  Strictly enforce construction CPM schedule

4.6  10-Min. Delay Penalty Contact DTM at 909-383-6423 for 10 Min. Delay Penalty Calculations.

**Section 4 Total** \$ -

**5 Demand Management (DM)**

Project team needs to coordinate with RCTC/SBCTA

5.1  A co-op will be executed - mentioned in PSR or PR.

Instead of a co-op, 15% is added to the cost of DM elements since the payment to the local agency will be routed through the contractor.

Instead of a co-op, the local agency will make their own arrangements with RCTC/SBCTA. PA/CL or local agency need to inform commuters through RCTC/SBCTA. Funds part of PA/CL.

5.2  HOV Lanes/Ramps (New or Convert)

5.3  Park-and-Ride Lots

5.4  Parking Management/Pricing (Coordination with local agency is required)

5.5  BEES 066067 Rideshare Promotion

**Section 5 Total** \$ -

<b>TMP Elements</b>	EA #/ID#	1L250/0819000168	Date	1/8/2026
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<b>6</b>	<b>Alternate Route Strategies</b>
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<p><b>Caution</b> - signed detours may require environmental clearance. Traffic diversion may increase available work hours. Please work with Traffic Design.</p>
---

- 6.1  Add Capacity to Freeway connector
- 6.2  Ramp Closures
- 6.3  Temporary Highway Lanes or Shoulder Use
- 6.4  Parking Restrictions
- 6.5  Street Improvements
  - State R/W - Signals, Widen, etc.
  - Local R/W - Signals, Widen, etc. co-op or permit may be needed
- 6.6  Local Street USE - co-op or Permit may be needed
- 6.7  Traffic Control Officers (see 3.1 COZEEP)
- 6.8  Signed detour - using State routes
- 6.9  Signed detour - using local streets and roads. Coordinate with corresponding local agency.
- 6.10  Adjust signals
- 6.11  Temporary bicycle or pedestrian facilities

<b>Section 6 Total</b>	<b>\$</b>	-
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<i>TMP Estimate</i>					
<i>Developed by</i>	<i>0</i>	<i>EA#/ID#</i>	<i>1L250/0819000168</i>	<i>Date</i>	<i>1/8/2026</i>
<p><b>TMP developer:</b> Amounts under the cost column will automatically be copied from the TMP elements</p>					
<b>TMP Elements</b>					<b>Cost</b>
1. Public Information					\$50,000
2. Motorist Information Strategies					\$0
3. Incident Management					\$1,040,000
4. Construction Strategies					\$0
5. Demand Management (DM)					\$0
6. Alternate Route Strategies					\$0
<b>Total TMP Estimate</b>					<b>\$ 1,090,000</b>

Attachment I  
CEQA Categorical  
Exemption/NEPA  
Categorical Exclusion  
Determination Form



**CEQA EXEMPTION / NEPA CATEGORICAL EXCLUSION  
DETERMINATION FORM (rev. 06/2022)**

**Project Information**

**Project Name (if applicable):** Riv 15 CAPM

**DIST-CO-RTE:** 08-RIV-15

**PM/PM:** 8.100/24.100

**EA:** 08-1L250

**Federal-Aid Project Number:**

**Project Description**

Full project description in continuation page.

**Caltrans CEQA Determination** (Check one)

- Not Applicable** – Caltrans is not the CEQA Lead Agency
- Not Applicable** – Caltrans has prepared an IS or EIR under CEQA

Based on an examination of this proposal and supporting information, the project is:

- Exempt by Statute.** (PRC 21080[b]; 14 CCR 15260 et seq.)
- Categorically Exempt. Class 1(c).** (PRC 21084; 14 CCR 15300 et seq.)
  - No exceptions apply that would bar the use of a categorical exemption (PRC 21084 and 14 CCR 15300.2). See the [SER Chapter 34](#) for exceptions.
- Covered by the Common Sense Exemption.** This project does not fall within an exempt class, but it can be seen with certainty that there is no possibility that the activity may have a significant effect on the environment (14 CCR 15061[b][3].)

**Senior Environmental Planner or Environmental Branch Chief**

Hannah Duarte		1/5/26
Print Name	Signature	Date

**Project Manager**

Dalia Alarkan	<i>Dalia Alarkan</i>	01/05/2026
Print Name	Signature	Date



CEQA EXEMPTION / NEPA CATEGORICAL EXCLUSION DETERMINATION FORM

Caltrans NEPA Determination (Check one)

Not Applicable

Caltrans has determined that this project has no significant impacts on the environment as defined by NEPA, and that there are no unusual circumstances as described in 23 CFR 771.117(b). See SER Chapter 30 for unusual circumstances. As such, the project is categorically excluded from the requirements to prepare an EA or EIS under NEPA and is included under the following:

23 USC 326: Caltrans has been assigned, and hereby certifies that it has carried out the responsibility to make this determination pursuant to 23 USC 326 and the Memorandum of Understanding dated April 18, 2022, executed between FHWA and Caltrans. Caltrans has determined that the project is a Categorical Exclusion under:

- 23 CFR 771.117(c): activity (c)(27)
23 CFR 771.117(d): activity (d)(Enter activity number)
Activity Enter activity number listed in Appendix A of the MOU between FHWA and Caltrans

23 USC 327: Based on an examination of this proposal and supporting information, Caltrans has determined that the project is a Categorical Exclusion under 23 USC 327. The environmental review, consultation, and any other actions required by applicable Federal environmental laws for this project are being, or have been, carried out by Caltrans pursuant to 23 USC 327 and the Memorandum of Understanding dated May 27, 2022, and executed by FHWA and Caltrans.

Senior Environmental Planner or Environmental Branch Chief

Hannah Duarte
Print Name
Signature
Date 1/5/26

Project Manager/ DLA Engineer

Dalia Alarkan
Print Name
Signature
Date 01/05/2026

Date of Categorical Exclusion Checklist completion (if applicable): N/A
Date of Environmental Commitment Record or equivalent: 1/5/2026

Briefly list environmental commitments on continuation sheet if needed (i.e., not necessary if included on an attached ECR). Reference additional information, as appropriate (e.g., additional studies and design conditions).



## CEQA EXEMPTION / NEPA CATEGORICAL EXCLUSION DETERMINATION FORM

### Continuation sheet:

The project will construct the following improvements on Interstate 15 (I-15) from 1.4 miles south of Murrieta Hot Springs Road to 0.2 mile north of Nichols Road in the Cities of Murrieta, Wildomar, and Lake Elsinore:

- Random slab replacement on mainline
- 0.15' cold plane and overlay on shoulders, on-ramps, and off-ramps, including I-215/I-15 connectors
- Grind existing concrete pavement on all mainline lanes
- Upgrade 19 transportation management systems, 12 signals, and 6 ramp metering systems
- Replace 20 soffit lights and 23 light poles
- Eliminate sidewalk gaps on south side of Wildomar Trail
- Convert the existing shoulder on the north side of Bundy Canyon Road to a Class II bike lane with signage and pavement markers.
- Construct 2 new curb ramps and 2 new refuge pads to current American with Disabilities Act (ADA) standards
- Upgrade 2 Weigh in Motion Systems
- Install roadside safety improvements
  - o 13 maintenance vehicle pull outs
  - o Paving beyond gore areas
  - o Light duty maintenance access trails
- Upgrade existing guardrail and end treatments to current standards, including vegetation control (minor concrete) and connection to existing bridges (anchor blocks)
- Install trash capture devices onto 4 existing drainage systems
- Install conduit and pull boxes at Central northbound ramp (PM 22.3), Lemon Street (PM 16.89), and Bundy Canyon northbound ramp (PM 16.29).

### Air Quality

Per memo from Caltrans Environmental Engineering on December 12, 2025, the project falls under exempt project types listed under Table 1 of Caltrans Carbon Monoxide Protocol or Table 2 of CFR 93.126. Therefore, no air quality study is needed and no significant impacts to air quality are anticipated.

### Biology

Per No Effect Memo completed on December 29, 2025, there will be No effect to any species listed on the USFWS Species List and there will be no take of any species on the CDFW CNDDDB species list. The following measures will be implemented to avoid and/or minimize impacts to biological resources.

**BIO-1:** Preconstruction Nesting Bird Survey: If project activities cannot avoid the nesting season, Feb 1 – Sept 30, then preconstruction nesting bird surveys must be conducted 3 days prior to construction by a qualified contractor-supplied biologist (CSB) to locate and avoid nesting birds. If an active avian nest is located, a no construction buffer may be established and monitored by a



## CEQA EXEMPTION / NEPA CATEGORICAL EXCLUSION DETERMINATION FORM

qualified biologist and/or project activities can be moved to occur outside of the nesting bird timeframe.

- BIO-2:** Preconstruction Burrowing Owl Survey: Two burrowing owl preconstruction surveys must be performed: by a qualified contractor-supplied biologist (CSB) one survey 14-30 days prior to project activities, and one survey 24 hours prior to project activities, within the MSHCP survey area.
- BIO-3:** Worker Environmental Awareness Program (WEAP): A contractor supplied biologist (CSB) must present a biological resource information program/WEAP prior to project activities to all personnel that will be present within the project limits for longer than 30 minutes at any given time.
- BIO-4:** Preconstruction Surveys: Preconstruction Crotch's bumble bee surveys must be conducted by a Qualified Biologist 3 days prior to construction activities within areas of appropriate nesting and foraging habitat. If an active Crotch's bumble bee nest is located, a no construction buffer shall be established and monitored by the Qualified Biologist; the Resident Engineer must be contacted and additional measures and/or agency coordination may be required.
- BIO-5:** Rare Plant Surveys, Flagging and Fencing: Within 1 week prior to construction, a preconstruction survey must be conducted by a qualified contractor-supplied biologist for narrow and endemic plants within Criteria Cell 4166, between PM 9.8 and 10.8. Narrow and endemic plants must be flagged for visual identification to construction personnel for work avoidance. Narrow and endemic plants detected that feature multiple plants in a single location must be fenced with Environmentally Sensitive Area (ESA) temporary fencing.

### Cultural

Per screened undertaking memo completed on December 30, 2025, the project has no potential to affect Historic Properties eligible for or listed on the National Register of Historic Places. The following measures will be implemented to avoid and/or minimize impacts to cultural resources.

- CR-1:** If buried cultural resources are encountered during Project Activities, it is Caltrans policy that work stop within 60 feet of the area until a qualified archaeologist can evaluate the nature and significance of the find.
- CR-2:** In the event that human remains are found, the county coroner shall be notified and ALL construction activities within 60 feet of the discovery shall stop. Pursuant to Public Resources Code Section 5097.98, if the remains are thought to be Native American, the coroner will notify the Native American Heritage Commission (NAHC) who will then notify the Most Likely Descendent (MLD). The person who discovered the remains will contact the District 8 Division of Environmental Planning; Gabrielle Duff, DEBC: (909) 501-5142 and Julie Scrivner, DNAC: (909) 260-8265. Further provisions of PRC 5097.98 are to be followed as applicable.

### Hazardous Waste



## CEQA EXEMPTION / NEPA CATEGORICAL EXCLUSION DETERMINATION FORM

Per Initial Site Assessment (ISA) Checklist completed on November 25, 2025, the project has a low risk for potential hazardous waste involvement. The following measures will be implemented to avoid and/or minimize impacts from hazardous waste:

**HAZ-1:** Include SSP 6-1.0B conditions for use of local material from non-commercial sources in the PS&E package.

**HAZ-2:** Include SSP 14-9.02 for NESHAP notification in the PS&E package.

**HAZ-3:** Include SSP 14-11.14 for the removal and disposal of Treated Wood Waste from signposts and/or guardrail posts in the PS&E package.

**HAZ-4:** Include SSP 87-21.03D for the removal of Vehicle Sensor Nodes and Electrical Systems in the PS&E package.

**HAZ-5:** Include SSP 36-4 for residue containing lead from paint and thermoplastic striping in the PS&E package.

**HAZ-6:** Include SSP 84-9.03B for separate removal of yellow traffic stripe and pavement marking with non-hazardous residue in the PS&E package.

**HAZ-7:** Include bid item #070030 for a lead compliance plan in the PS&E package.

### Noise

Per email from Caltrans Environmental Engineering on December 22, 2025, the project scope falls under Type III of the Caltrans Traffic Noise Analysis Protocol and traffic noise impacts are not anticipated under 23 CFR 772.7. The following measure will be implemented to avoid noise impacts from construction:

**NOI-1:** Include SSP 14-8.02 for noise control and monitoring for work in a residential or urban area in the PS&E package.

### Paleontology

Per email from Caltrans Paleontology on December 23, 2025, no paleontological studies are required and no significant impacts to paleontological resources are anticipated.

### Greenhouse Gas

The project is not capacity increasing and would not increase the vehicle capacity in Riverside County. Because the project would not increase the number of travel lanes, no increase in vehicle miles traveled (VMT) would occur. Therefore, no increase in operational GHG emissions is anticipated. Construction of the project would temporarily increase GHG emissions during the construction phase of the project. Per memo from Caltrans Environmental Engineering, completed December 4, 2025, the Caltrans Construction Emissions Tool (CAL-CET) was used to estimate construction GHG emissions for the project. Construction of the project is estimated to last 250 working days and generate a total of 1951 metric tons of CO<sub>2</sub>-equivalent (CO<sub>2</sub>e). While the proposed project will result in GHG emissions during construction, it is anticipated that the project will not result in any increase in operational GHG emissions. The proposed project does not conflict with any applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases. With implementation of the



## CEQA EXEMPTION / NEPA CATEGORICAL EXCLUSION DETERMINATION FORM

following construction GHG reduction measures, the impact would be less than significant.

**GHG-1:** Limit idling to 5 minutes for delivery and dump trucks and other diesel-powered equipment.

**GHG-2:** Schedule truck trips outside of peak morning and evening commute hours.

Attachment J  
Final value Analysis Study  
Report Cover Page



# Final Value Analysis Study Report



## D8 RIV-15 CAPM Project

EA 1L250; PN 00819000168

08-RIV-15 (8.1/24.1)

Contract No. 53A0282

Task Order No. 1500

*October 2025*

Prepared by  
Value Management Strategies, Inc.





Date: October 20, 2025  
To: Nivine Georges, District 8 DVAC  
Subject: Final VA Study Report  
***RIV-15 CAPM (T.O 1500)***

Dear Ms. Georges,

Value Management Strategies, Inc. is pleased to submit this Final VA Study Report for the referenced project. This report summarizes the results and events of the virtual study conducted August 18-21, 2025, and the implementation meeting on October 2, 2025, using the Teams virtual meeting platform for District 8, California.

It was a pleasure working with District 8 on this project, and I look forward to the next one. If you have any questions or comments concerning this final report, please do not hesitate to contact me at (360) 616-0181 or email [john.corcoran@vms-inc.com](mailto:john.corcoran@vms-inc.com).

Sincerely,

Value Management Strategies, Inc.

A handwritten signature in blue ink that reads "John Corcoran". The signature is written in a cursive style and is positioned above the printed name and title.

John Corcoran, PE, CVS®, PMP  
Certified Value Specialist, SAVE International No. 201909001  
VA Study Team Leader

Copy: (PDF) Addressee  
(PDF) Dalia Alarkan, Project Manager  
(PDF) Jarek Kusz, Office of Innovative Design and Delivery  
(PDF) Erika Barrick, HQ VA Program Administrator

Attachment K  
Complete Street Decision  
Document

### Complete Streets Decision Document (CSDD)

- 1) Is the project located entirely on a facility where bicyclists and pedestrians are legally prohibited and the project does not involve a shared use path, pedestrian/bicycle structure or work impacting a local road crossing or interchange? (For example, a project including freeway mainline and ramp work, not including the ramp connection with the minor road, where the project freeway segment legally prohibits bicyclists and pedestrians.)

NO - Proceed to Question 2  
 YES - Stop here. The project is exempt from further complete streets evaluation. Sign and attach to the Project Initiation Document (PID).

- 2) Is the primary project purpose to address assets that are outside of the roadbed where pedestrian and bicycle travel is not affected, and proposed project will not affect future pedestrian and bicycle facilities? Examples may include culvert outfalls, storm water treatment facilities, bridge substructure or scour mitigation, planting or vegetation removal, retaining walls, etc.

NO - Continue to Question 3  
 YES - Stop here. The project is exempt from further complete streets evaluation. Sign and attach to PID.

- 3) Has a Transportation Planning Scoping Information Sheet (TPSIS) been completed for this project?

NO – Proceed to Question 4  
 YES – Skip to Question 5 (Note: TPSIS is attached to the PID)

- 4) Which of the following planning documents were consulted to determine bicycle, pedestrian or transit needs? Select all that apply and proceed to Question 5.

- a. District Active Transportation Plan
- b. Other Caltrans or local/regional agency bike/ped/transit/safe routes to school plans
- c. ADA Transition Plan/Grievances (consult with the District ADA Coordinator)
- d. Corridor planning documents
- e. Other (list here) \_\_\_\_\_

- 5) Based on the reviews completed in Question 4 or identified in the TPSIS, after a review of the roadway geometrics, or identified by the PDT, are there any bicycle, pedestrian, or transit needs, deficiencies or opportunities for improvement identified for the project location?

NO – Provide brief description of findings: \_\_\_\_\_  
Stop here. The project meets the requirements for consideration of Complete Streets elements. Sign and attach to the PID.

YES – Describe them here and proceed to Question 6: There are opportunities for improvements to incorporate Complete Streets elements such as: Fill 500 feet sidewalk gap on south side of Central Ave UC, widen approximately 800 feet sidewalk on each side of Bundy Canyon Road UC, and 800 feet sidewalk gap on each side of Wildomar Trail; add 1,100 LF of Class II Bike Lane on the north side of Bundy Canyon Rd UC.

\_\_\_\_\_

\_\_\_\_\_

- 6) Based on the needs identified in Question 5, what would be the preferred complete streets elements to address those needs (e.g. road diet, separated bikeway, reconstructed sidewalk, etc.)? Resources include the Complete Streets Elements Toolbox, the Contextual Guidance for Bikeway Facility Selection, the Bikeway Facility Selection Guidance Memorandum, etc. List them in the table below and

provide a rough estimated cost to construct preferred project complete streets elements (including right-of-way and support costs) and proceed to Question 7.

FACILITY TYPE	UNIT	QUANTITY	ESTIMATED TOTAL COST
Class II Bike Lane	LF	1,100	\$3,480
Sidewalk	LF	3,700	\$334,057

7) Was there any known public and stakeholder opposition to any preferred complete streets elements identified for the project? Provide response and proceed to Question 8.

NO  
 YES – Describe the opposition position here: \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

8) Does the programmable project alternative/project scope include all the complete streets elements identified in Question 6?

NO - Proceed to Question 9  
 YES - Stop here. The project has met the requirements for consideration of complete streets elements. Sign and attach to PID.

9) Does the project include any of the complete streets elements that are identified in Question 6? Or are there any proposed incremental improvements related to the complete streets elements in Question 6? Provide response and proceed to Question 10.

NO – The programmable project alternative does not include any complete streets elements, and therefore does not address identified needs for complete streets elements.  
 YES – List them here:

FACILITY TYPE	UNIT	QUANTITY	ESTIMATED TOTAL COST
Class II Bike Lane (North side of Bundy Canyon Rd)	LF	550	\$1,740
Sidewalk (South side of Wildomar Trail)	LF	350	\$31,600
ADA Curb Ramps (South side of Wildomar Trail)	EA	2	\$140,000
Refuge Pads (South side of Wildomar Trail)	EA	2	\$1,850

10) Does the project funding have constraints that would preclude the ability to incorporate additional complete streets elements into the project (For example, cannot combine funding with other sources.)? Provide response and proceed to Question 11.

NO  
 YES – Describe the constraints here: \_\_\_\_\_

11) Provide a rationale and justification for not including all the recommended complete streets elements into the project: (Consider the engineering justification, right-of-way constraints, environmental impacts, etc.) This project is including all the recommended Complete Streets elements, but the quantity of the sidewalk and Class II bike lane are less than the recommended quantities because of the following

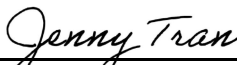
reasons: 1) Central Ave UC is covered under EA 0F310, 2) Existing width of sidewalks on Bundy Canyon Road UC is approximately 5 feet. To make existing sidewalk standard (6 feet), the existing sidewalk needs to be widened that will require cutting into the slope of the bridge abutment and require retaining wall. This will impact project cost and schedule. Also, this is a pavement project. We cannot include Structure items (retaining wall). 3) There is no room to construct sidewalk on the north side of Wildomar Trail, and it will require bridge widening, 4) The north side of Bundy Canyon is only 550 feet not 1,100 feet as stated in the PIP to construct Class II Bike Lane. In addition, two (2) ADA curb ramps and two (2) refuge pads will be added due to construction of new sidewalk on the south side of Wildomar Trail.

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Prepared by:

  
\_\_\_\_\_  
Jenny Tran, PID Preparer in responsible charge  
Planning

Concurred by:

  
\_\_\_\_\_  
Alexa Pok  
District Complete Streets Coordinator


2/2/2023  
\_\_\_\_\_  
Date

*CG*   
\_\_\_\_\_  
Ray I. Desselle  
Deputy District Director, Planning

02/09/2023  
\_\_\_\_\_  
Date

*CQ*   
\_\_\_\_\_  
Mahmuda Akhter  
Acting Deputy District Director, Design or  
Division Chief, Design/Project Development

02/14/2023  
\_\_\_\_\_  
Date

  
\_\_\_\_\_  
Rebecca Guirado  
Acting District Director

02/15/2023  
\_\_\_\_\_  
Date

Distribution: Attach completed original CSDD to PID and email to HQ Division of Design at CSDD@dot.ca.gov

**Revalidation of CSDD at PA&ED**

Does the project scope defined in the project approval document include the complete streets elements identified in Question 6 or 9 of this CSDD and the PID?

       NO – Prepare a Superseding CSDD (answer Questions 1 through 11) replacing the original CSDD, obtain all certified and concurrence signatures below, and attach the superseding CSDD to the project approval document. Email superseding CSDD to HQ Division of Design at CSDD@dot.ca.gov.  
  X   YES – Certify there are no changes to the scope of complete streets elements with only the project engineer certification signature below on the original approved CSDD and attach the CSDD to the project approval document. Email revalidated CSDD to HQ Division of Design at CSDD@dot.ca.gov.

Certified by:



\_\_\_\_\_  
Ronald Pham, P.E.  
Design I/Caltrans D8

12/01/2025  
\_\_\_\_\_  
Date

Concurred by: *(Include concurrence signatures only if a Superseding CSDD is prepared.)*

\_\_\_\_\_  
Name  
District Complete Streets Coordinator

\_\_\_\_\_  
Date

\_\_\_\_\_  
Name  
Deputy District Director, Planning

\_\_\_\_\_  
Date

\_\_\_\_\_  
Name  
Deputy District Director, Design or  
Division Chief, Design/Project Development

\_\_\_\_\_  
Date

\_\_\_\_\_  
Name  
District Director

\_\_\_\_\_  
Date

Attachment L  
Strom Water Data Report  
Cover Page

### Long Form – Stormwater Data Report



Dist-County-Route: 08-RIV-15  
Post Mile Limits: 8.1/24.1  
Type of Work: Capital Preventive Maintenance  
Project ID (EA): 0819000168 (1L2500)

Phase:  PID  PA/ED  PS&E

Applicable Caltrans Post Construction Treatment Requirement: 2012  2022   
Regional Water Quality Control Board(s): Santa Ana (Region 8) & San Diego (Region 9)  
Total Disturbed Soil Area: 5 acres PCTA: Not applicable  
Alternative Compliance (acres): Not applicable ATA 2 (50% Rule)? Yes  No   
Estimated Const. Start Date: 11/01/2027 Estimated Const. Completion: 05/01/2029  
Risk Level: RL 1  RL 2  RL 3  WPCP  Other: \_\_\_\_\_  
Is (M)WELO applicable? Yes  No   
Is the Project within a TMDL watershed? Yes  No   
Does the project require trash treatment? Yes  No   
Notification of ADL reuse (if yes, provide date): Yes  Date: \_\_\_\_\_ TBD

*This Report has been prepared under the direction of the following Licensed Person. The Licensed Person attests to the technical information contained herein and the date upon which recommendations, conclusions, and decisions are based. Professional Engineer or Landscape Architect stamp required at PS&E only.*

*Haider Al-Khafaji* 01/05/2026  
Haider Al-Khafaji, Registered Project Engineer Date

*I have reviewed the stormwater quality design issues and find this report to be complete, current and accurate:*

*Dalia Alarkan* 01/05/2026  
Dalia Alarkan, Project Manager Date

*Donald Larson* 01/05/2026  
Donald Larson, District Maintenance Stormwater Coordinator Date

*Almabeth Anderson* 01/05/2026  
Almabeth Anderson, District Landscape Architect Date

*Gregory Clark* 01/08/2026  
Gregory Clark, District Stormwater Coordinator Date

*tj* 1/8/2026

Attachment M  
Field Review Sign-In Sheet

Contract Number-Project I.D. Number 08-1L250 / 0819000168	County, Route, Post Mile RIV, 015, 8.1 - 24.1	Federal Aid Number
Project Description GRIND ALL LANES, SLAB REPLACEMENT, 0.2 MILL & OVERLAY ON RAMPS & SHOULDERS, LIGHTING, ITS & WIM ELEMENTS, COMPLETE STREETS ELEMENTS, ROADSIDE SAFETY IMP, UPRGR END TREATMENTS & GUARDRAILS		

**Instructions:** The Project Engineer (PE) shall coordinate with the Project Manager to facilitate the field meetings with representatives from the eight Divisions. The joint field meetings shall be held pre-design/Phase 0 and within 2 weeks of the 60% design plans in Phase 1. The PE shall submit the completed sign-in sheet(s) with the design package to the District Office Engineer (DOE). Incomplete submittals will not proceed and DOE will forward to the Design Manager.

Phase 0

Division	Print Name	Signature	Date
Planning			
Design	MORGAN ITZEL - LAND ARCH.		
Maintenance	(Maint. Design) Harmeh Saigh	Harmeh Saigh	6/26/25
	JEFFREY LAMBERT		6/26/25
Construction	Amgad Benjamin		6/26/25
Right of Way	Brenda Flores		6/24/25
Environmental	Amy Lee		6/26/25
PPM			
Traffic Operations	HUU NGU		06/26/2025
Traffic Design	Sammy Korani		6/26/25

Phase 0

Division	Print Name	Signature	Date
Design	Max		
Maintenance	Heather Duncan		
Construction			
Right of Way			
Environmental			
PPM			
Traffic Operations			

Electrical | Luis Peratoza 6-26-25  
 JOHN VASQUEZ 6-26-25  
 Leo Martinez 6-26-25

# Attachment N

## PRSM Quantities

# PRSM PRODUCTION



HOME | FAVORITES |

Properties

Team

Tasks

Risks/Issues/Changes

Hierarchy

Processes

Collaboration

Project: 0819000168 - 1L250 PAVEMENT REPAIR - *Properties - Main - C*

[Open in Scheduler](#)

[Actions](#)

## General

M200 Note : Must enter values for these fields when M200 is reached

Aggregate Base (CY)

Asphalt Concrete (TON)

Bar Reinforcing Steel (LB)

Imported Borrow (CY)

Cement Concrete (CY)

Roadway Excavation (CY)

Structural Concrete (CY)

Structural Steel (LB)

[Save](#)

[Save And Return](#)

[Return](#)